

GENERAL NOTES

1. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE SAFETY REGULATIONS. ALL CONSTRUCTION SHALL BE COMPLETED FOLLOWING CURRENT CITY STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. CONTRACTOR WILL BE REQUIRED TO PROVIDE NOTICE TO UTILITY COMPANIES A MINIMUM OF SEVENTY-TWO (72) HOURS PRIOR TO ANY EXCAVATION, AS FOLLOWS:

KANSAS ONE-CALL 687-2470

THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF AN EMERGENCY:

AT&T	1-316-246-8464
BLACK HILLS ENERGY (GAS)	1-800-694-8989
CITY OF WICHITA WATER & SEWER	1-316-219-8921
CITY OF WICHITA STORMWATER	1-316-268-4090
CITY OF WICHITA TRAFFIC	1-316-268-4034
COX COMMUNICATIONS	1-888-249-3530
KANSAS GAS SERVICE	1-888-482-4950
EVERGY	1-800-544-4857

3. UTILITY SERVICE LINES, POLES, ETC. ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR OR UNLESS THE PLANS SPECIFICALLY IDENTIFY A UTILITY TO BE ADJUSTED BY ITS OWNER DURING CONSTRUCTION. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.
4. RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND EXCESS EXCAVATION WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE AND SITE LOCATION. LOCATIONS, IN THE OPINION OF THE ENGINEER, THAT WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WILL REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS WILL REQUIRE ADDITIONAL ARCHAEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION.
5. TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED BY THE CONTRACTOR WITH THE CITY ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.
6. THE CONTRACTOR SHALL GIVE ALL PROPERTY OWNERS AND/OR TENANTS OF DEVELOPED PROPERTY ADJUTING THE CONSTRUCTION OF THIS PROJECT A MINIMUM OF TEN (10) DAYS NOTICE PRIOR TO START OF CONSTRUCTION.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS.
8. IF TRAFFIC WILL BE IMPACTED BY CONSTRUCTION, A TRAFFIC CONTROL PLAN MUST BE SUBMITTED AND APPROVED BY THE CITY TRAFFIC ENGINEER AT traffic@wichita.gov BEFORE CONSTRUCTION CAN BEGIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL MEASURES TO FACILITATE CONSTRUCTION. ALL CONSTRUCTION ZONE MARKINGS AND SIGNAGE SHALL CONFORM TO THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS PUBLISHED BY THE US DEPT. OF TRANSPORTATION. FEDERAL HIGHWAY ADMINISTRATION. ALL COSTS ASSOCIATED WITH CONSTRUCTION MARKINGS AND SIGNAGE SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
9. THE ENGINEERING DIVISION SHALL FIELD LOCATE WATER VALVES ONE TIME DURING CONSTRUCTION WHEN REQUESTED BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PRESERVE SUCH FIELD LOCATIONS DURING THE CONSTRUCTION PROCESS. WATER VALVES, VALVE BOXES OR FIRE HYDRANTS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY CONTRACTOR AT HIS OWN EXPENSE. VALVE BOXES AND WATER METERS WITHIN THE PROJECT LIMITS SHALL BE ADJUSTED TO MATCH FIELD GRADES BY THE CONTRACTOR.
10. THE CONTRACTOR SHALL NOTIFY THE INSPECTING ENGINEER AND DAWNITA REINHARDT AT 650-0740 WITH THE CITY OF WICHITA WITH THE ANTICIPATED CONSTRUCTION START DATE AND NOTIFY THEM OF PROJECT COMPLETION. STAKING AND INSPECTION FOR THIS PROJECT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
11. ALL AREAS DISTURBED DURING CONSTRUCTION THAT WILL NOT BE UNDER PROPOSED PAVEMENT SHALL BE SEEDED AND MULCHED. COST SHALL BE CONSIDERED SUBSIDIARY TO PROJECT SEEDING.
12. CONTRACTOR SHALL LIMIT THE EXTENT OF TRENCH OPEN OVERNIGHT AND WEEKENDS TO LESS THAN 50 FEET.
13. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS COMPANIES AND IS EITHER FROM COMPANY UTILITY DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED.
https://safety.fhwa.dot.gov/roadway_dept/countermeasures/reduce_crash_severity/
14. ALL TRAFFIC CONTROL DEVICES IN THE WORK ZONE (INCLUDING MARKINGS AND SIGNS) AND THEIR INSTALLATION AND MAINTENANCE SHALL COMPLY WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). ALL TRAFFIC CONTROL DEVICES IN THE TRAVELED WAY OR CLEAR ZONE SHALL BE CRASHWORTHY (NCHRP REPORT 350 OR MASH COMPLIANT).
https://safety.fhwa.dot.gov/roadway_dept/countermeasures/reduce_crash_severity/
15. ALL CONSTRUCTION EQUIPMENT, INCLUDING VEHICLES, MATERIALS, AND DEBRIS, SHALL BE STORED OUTSIDE OF THE CLEAR ZONE. WHERE THIS CANNOT BE ACHIEVED THE CONTRACTOR SHALL PLACE APPROPRIATE SIGNS, OBJECT IDENTIFIERS, AND/OR BARRICADES IN COMPLIANCE WITH MUTCD.
16. EXCEPT WHEN REQUIRED FOR SAFETY, TRAFFIC CONTROL SHALL NOT BLOCK ANY LANES OR SIDEWALKS WHEN WORK IS NOT BEING PERFORMED.
17. THIS PROJECT INCLUDES A CERTAIN AMOUNT OF ROLL TYPE CURB CONSTRUCTION. ROLL CURBS SHALL BE DEPRESSED THROUGH ALL DRIVEWAY OPENINGS WHEN SUCH DRIVES ARE CONSTRUCTED AS A PART OF THE PROJECT. NO MORE THAN 2 DRIVES 20 FEET IN WIDTH OR EQUIVALENT COMBINATIONS THEREOF ARE TO BE CONSTRUCTED WITH THIS PROJECT.
18. TRANSITION CURB FROM FULL HEIGHT COMBINATION CURB AND GUTTER TO ROLL TYPE COMBINATION CURB AND GUTTER IS TO BE PAID AS BID FOR LINEAL FEET COMBINED CURB AND GUTTER (3 5/8" ROLL).
19. A SAW CUT OF AT LEAST ONE-HALF THE DEPTH OF THE EXISTING SURFACE COURSES OR ONE-FOURTH THE DEPTH OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ABUTS AN EXISTING SURFACE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THAT SURFACE OR PAVEMENT IS REQUIRED. SAW JOINT TO FACILITATE REMOVAL WITHIN THREE (3) FEET OF EXISTING JOINTS WILL NOT BE PERMITTED AND FOR SUCH INSTANCES THE LIMITS OF REMOVAL SHALL EXTEND TO THE EXISTING JOINT. SUCH SAW CUTS WILL NOT BE PAID FOR
20. DIRECTLY AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE REMOVAL OF SURFACE OR PAVEMENT.
21. FOLLOW THE LINK BELOW FOR CONSTRUCTION DETAILS ON SPECIFIC CITY OF WICHITA STANDARD DETAILS: <http://www.wichita.gov/PWU/Pages/Regulations.aspx>
22. DEVELOPER FOR THIS PROJECT IS:
PERFECTION SIGNATURE PROPERTIES, LLC
443 N MAIZE RD
WICHITA, KS 67212
SCOTT LEHNER/JASON RONK
316.729.1900

PAVING & INCIDENTAL DRAINAGE PLANS FOR PHASE 1 - PART B

COURTYARDS AT THE MOORINGS

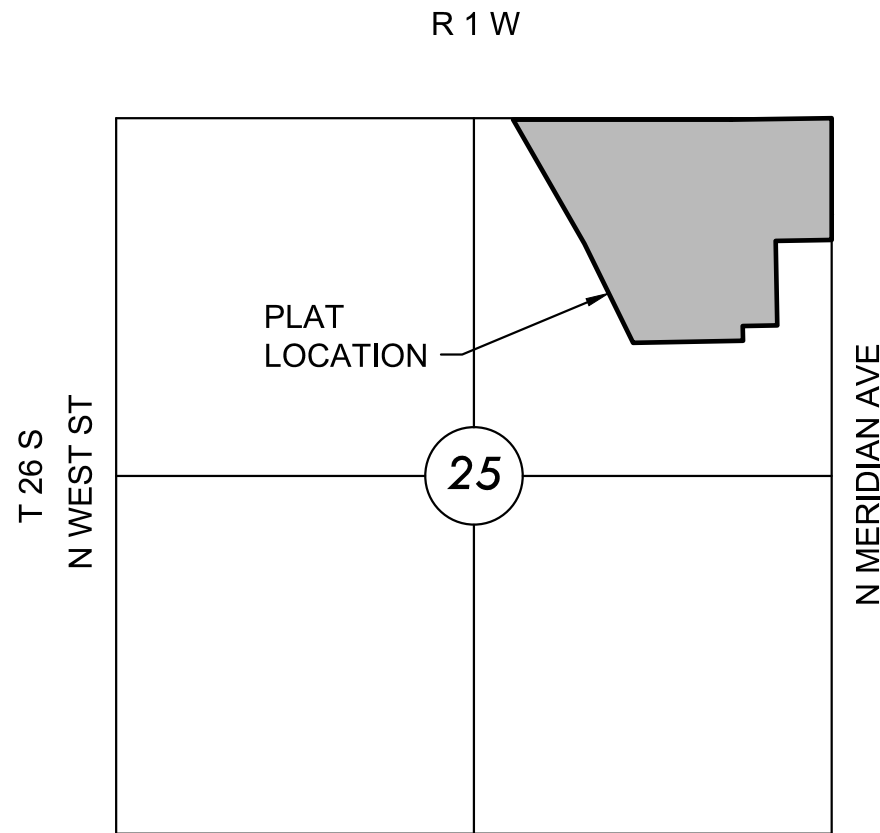
PROJECT NO. 472-2022-085820B

AN ADDITION TO THE CITY OF WICHITA, SEDGWICK COUNTY, KANSAS
GARY JANZEN, P.E. - CITY ENGINEER

ORG CODE 47471022

MUNIS NO. E2069

LOTS 1-10, BLOCK 1; LOTS 15-63, BLOCK 2; LOTS 1-5, BLOCK 3; LOTS 1-4, BLOCK 4; LOTS 1-5, BLOCK 5; LOTS 1-8, BLOCK 6; LOTS 1-10, BLOCK 7; LOTS 1-4, BLOCK 8; LOTS 1-5, BLOCK 9; LOTS 1-3, BLOCK 10; LOTS 1-4, BLOCK 11; LOTS 1-5, BLOCK 12; LOTS 1-4, BLOCK 13; LOTS 1-4, BLOCK 14; LOTS 1-8, BLOCK 15; LOTS 1-8, BLOCK 16; LOTS 1-8, BLOCK 17; LOTS 1-4, BLOCK 18; LOTS 1-5, BLOCK 19; LOTS 1-8, BLOCK 20 &
LOTS 1-16, BLOCK 1 COURTYARDS AT THE MOORINGS SECOND



VICINITY MAP

No Scale

INDEX TO DRAWINGS

SHEET NO.	DESCRIPTION
01	TITLE SHEET
02-09	PAVING DETAILS
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30	BUBBLE TABLE
31	EROSION CONTROL MAP
32-36	BMP DETAILS
37-47	CROSS SECTIONS
48-51	FINAL PLAT

BENCHMARKS

CP100
N: 1714419.007 E: 1638538.082 EL: 1328.839
PK-NAIL

CP101
N: 1713560.118 E: 1638541.17 EL: 1329.886
CHISELED "X"

BM 101
N: 1713560.118 E: 1638541.17 EL: 1329.886(88)
CHISELED "X"

DATUM

The Horizontal Datum is based on the Kansas Coordinate System of 1983, NAD83(2011), EPOCH: 2010.0000, South Zone. Coordinates shown have been modified to the ground using a combined adjustment factor of 1.0001200144. State Plane coordinates can be calculated by multiplying the shown values by 0.99988.

All elevations shown are based on the NAVD 88 vertical datum, Geoid12b.

NOTE

All Control Points shown have elevations established using standard surveying procedures and can be used as temporary benchmarks. When using a Control point as a temporary benchmark, it is recommended that cross-checks be made to other control points or benchmarks to confirm elevations prior to use.

PROJECT LENGTH

PORTWEST ST.	480 L.F.
PORTWEST CIR.	162 L.F.
DRIFTWOOD ST.	1048 L.F.
N. COBBLESTONE ST.	75 L.F.
HARBORSIDE DR.	483 L.F.
SANDKEY ST.	543 L.F.
ALLEY 1	333 L.F.
ALLEY 2	313 L.F.
ALLEY 3	287 L.F.
ALLEY 4	366 L.F.
ALLEY 5	300 L.F.
ALLEY 6	156 L.F.
ALLEY 7	266 L.F.

EARTHWORK SUMMARY

STREET
EXCAVATION 2,500 C.Y.
COMPACTED FILL (95%) 2,500 C.Y.

ADDITION GRADING
EXCAVATION (CONT FURN.) ON SITE 400 C.Y.
COMPACTED FILL (95%) 400 C.Y.

EXCESS MATERIAL TO BE PLACED & COMPACTED ON SITE.
LOCATION DETERMINED BY ENGINEER/OWNER'S DIRECTION



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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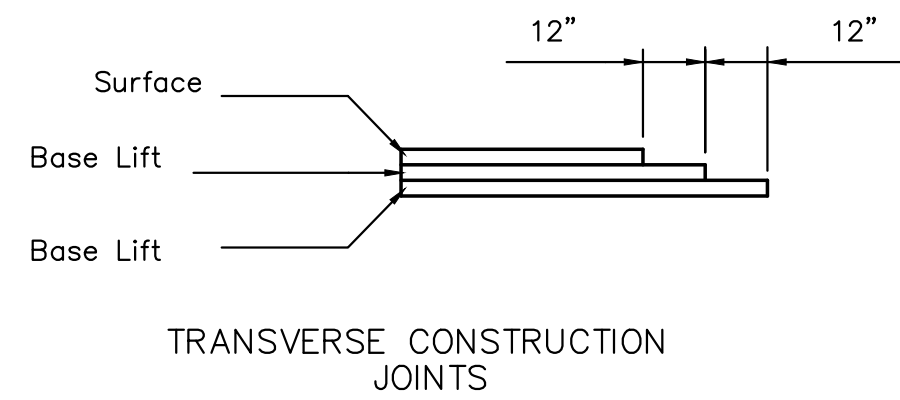
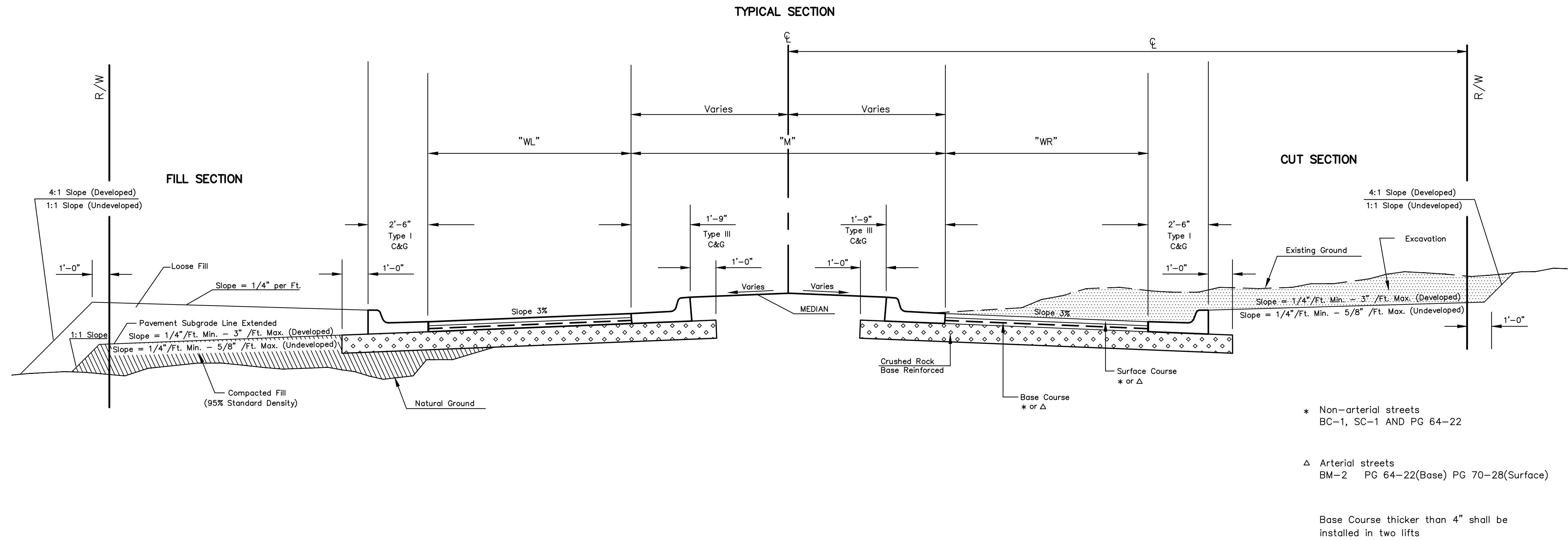
TITLE SHEET

PROJECT NO.	472-2022-085820B
DATE	SEPT. 2023
SCALE	1" = 150'

DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.



Transverse construction joints shall be constructed in flexible base pavement at locations where pavement joins existing flexible base pavement as show by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards of pavement.

GENERAL NOTES

Fabric base reinforcement shall be an approved grid. Fabric base reinforcement shall be installed in accordance with manufacturer's recommendations. Crushed rock shall be uniformly graded from 1 - 1/2" maximum size to not more than 10% passing a No. 200 sieve. Rock quality shall be the same as specified for coarse aggregate for concrete mixes.

Rock base is to be compacted and smoothed with a steel faced roller prior to placement of asphalt. Tack coat will not be applied to rock base.


A tack coat of emulsified asphalt (SC-1H or CSS-1H) shall be applied to an approximate rate of 0.05 gallons per square yard between each lifts of asphaltic material.

Bituminous base and asphaltic concrete wearing surface shall be placed with a laydown machine having automatic controls for line and grade.

Construction joints in each lift shall be staggered a minimum distance of one (1) foot from joints in preceding lifts and placed so that a joint will be constructed on the centerline of the top lift.

The asphaltic concrete pavement between the combined curb and gutter shall be paid as square yards of of pavement.

STREET NAME	"WL"	"M"	"WR"	STATION	CENTER LINE	ROW DIMENSION	MEDIAN DESCRIPTION	SLOPE	ROCK THICKNESS	PAVEMENT THICKNESS	COMMENTS
PORTWEST ST. TO DRIFTWOOD ST.	12'	0	12'	32+88.46 TO 48+29.87	29'	58'		3/8"/FT.	5"	5"	
DRIFTWOOD ST.	VARIES	0	VARIES	48+29.87 TO 48+91.79	VARIES	58'		3/8"/FT.	5"	5"	
HARBORSIDE DR.	12'	0	12'	10+38.19 TO 15+01.64	29'	58'		3/8"/FT.	5"	5"	
SANDKEY ST.	12'	0	12'	10+33.05 TO 15+56.18	29'	58'		3/8"/FT.	5"	5"	



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

REVISED: OCTOBER 2015

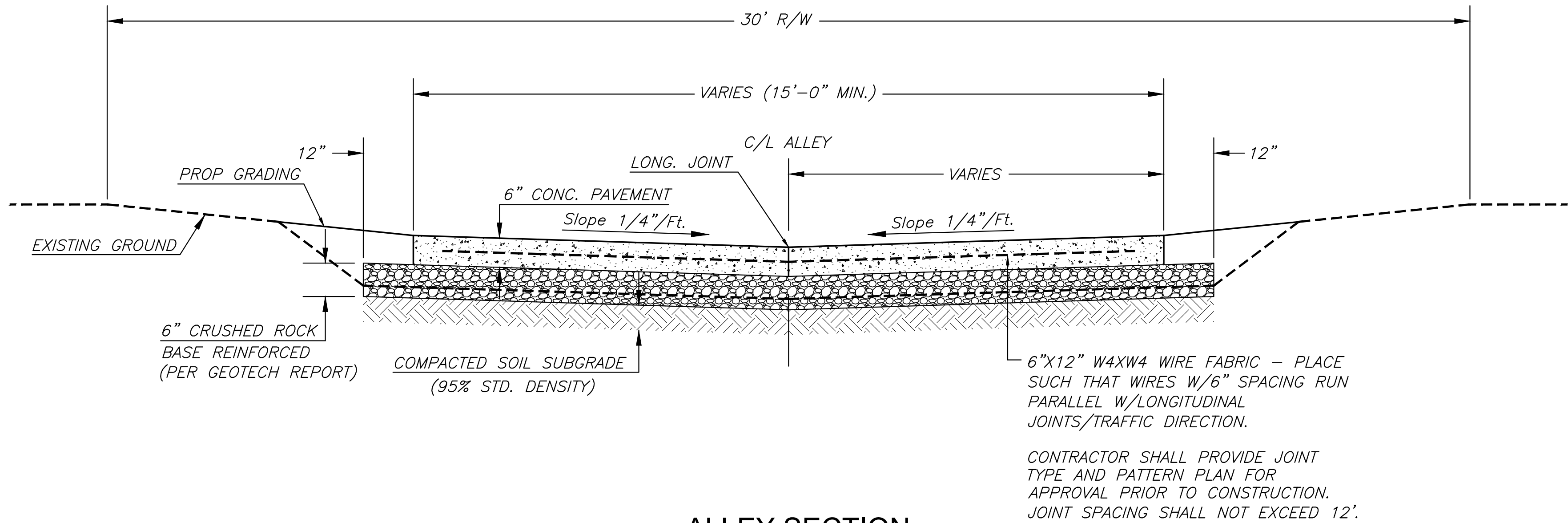
ASPHALT PAVING
DETAIL

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER 472-2022-085820B	OCA NUMBER ###	DATE SEPT. 2023
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 02 OF 51

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ALLEY SECTION

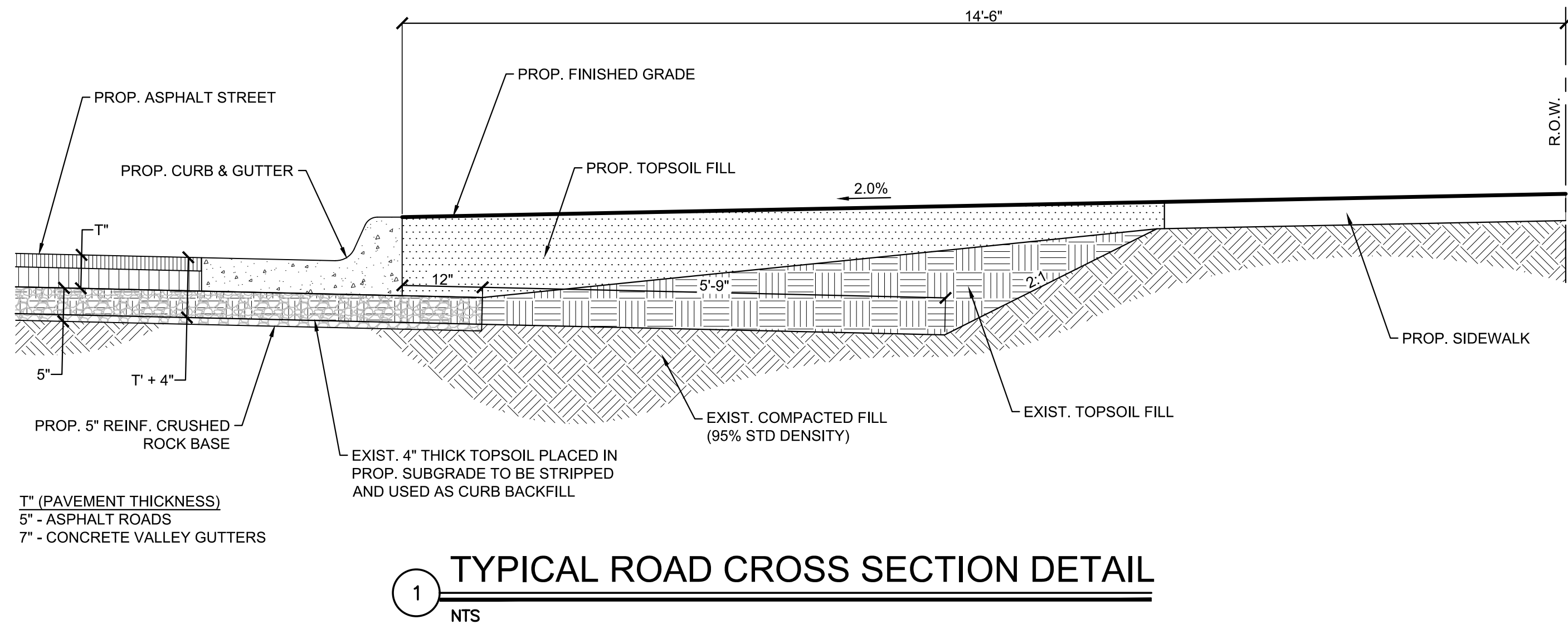
SCALE: NOT TO SCALE

PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
(ALLEY) PHASE 1A

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TYPICAL ALLEY SECTION		
PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	AS SHOWN	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE
NO.	REVISION	DATE
SHEET NO.		
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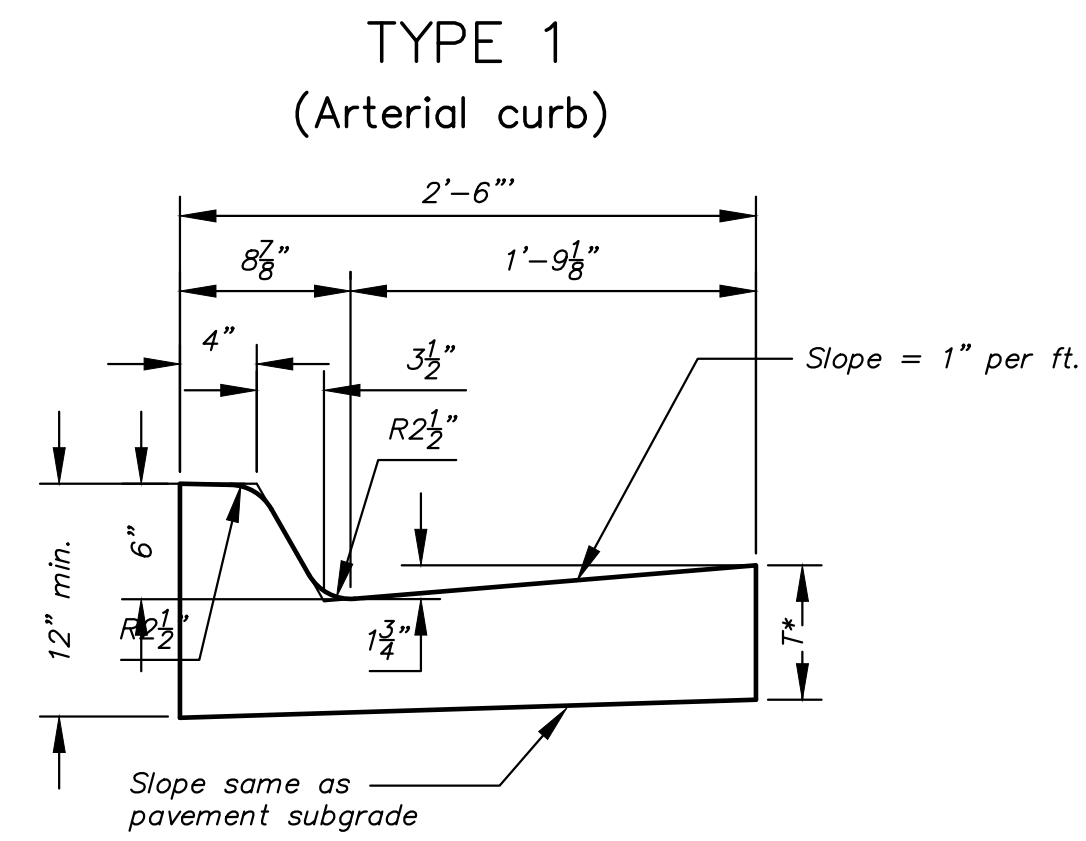
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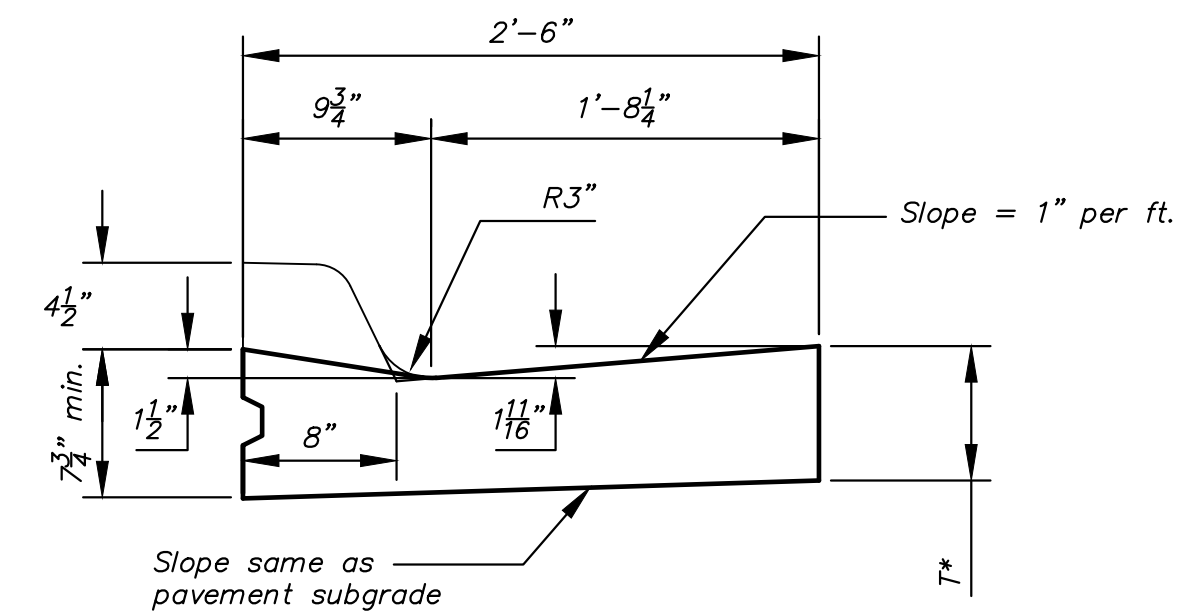
PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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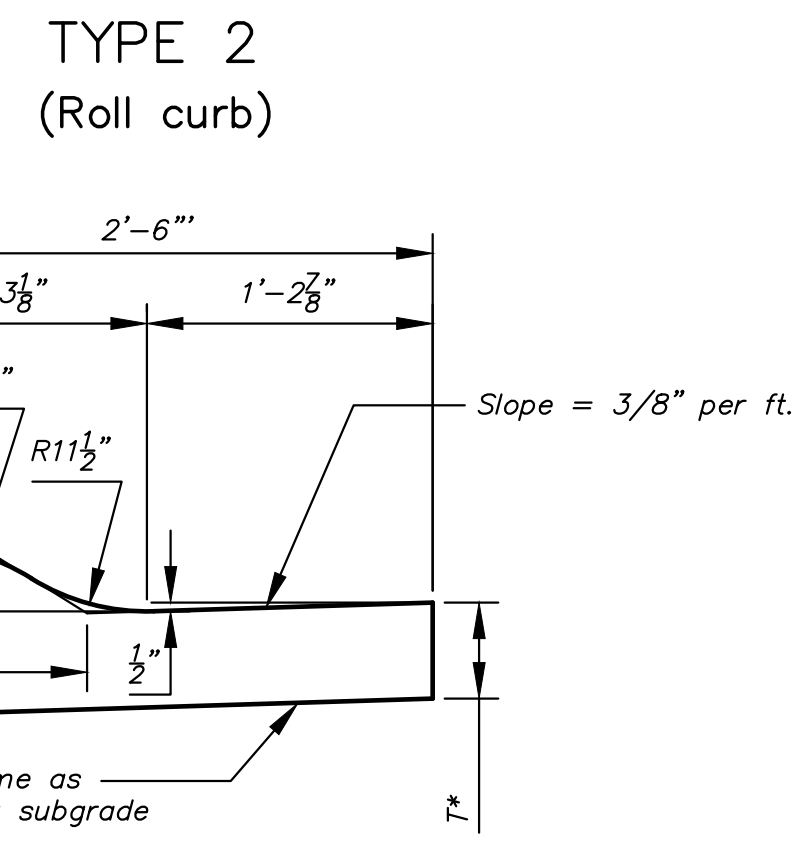
CURB BACKFILL DETAIL		
PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	AS SHOWN	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE
NO.	REVISION	DATE
SHEET NO.		
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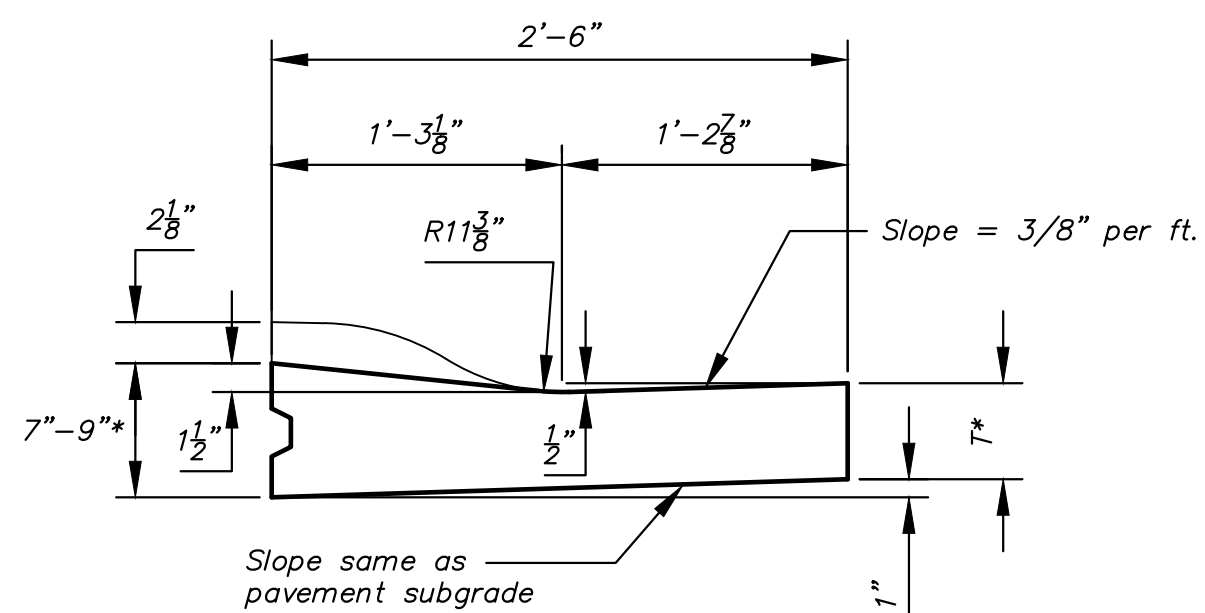
Combined Curb & Gutter (6")



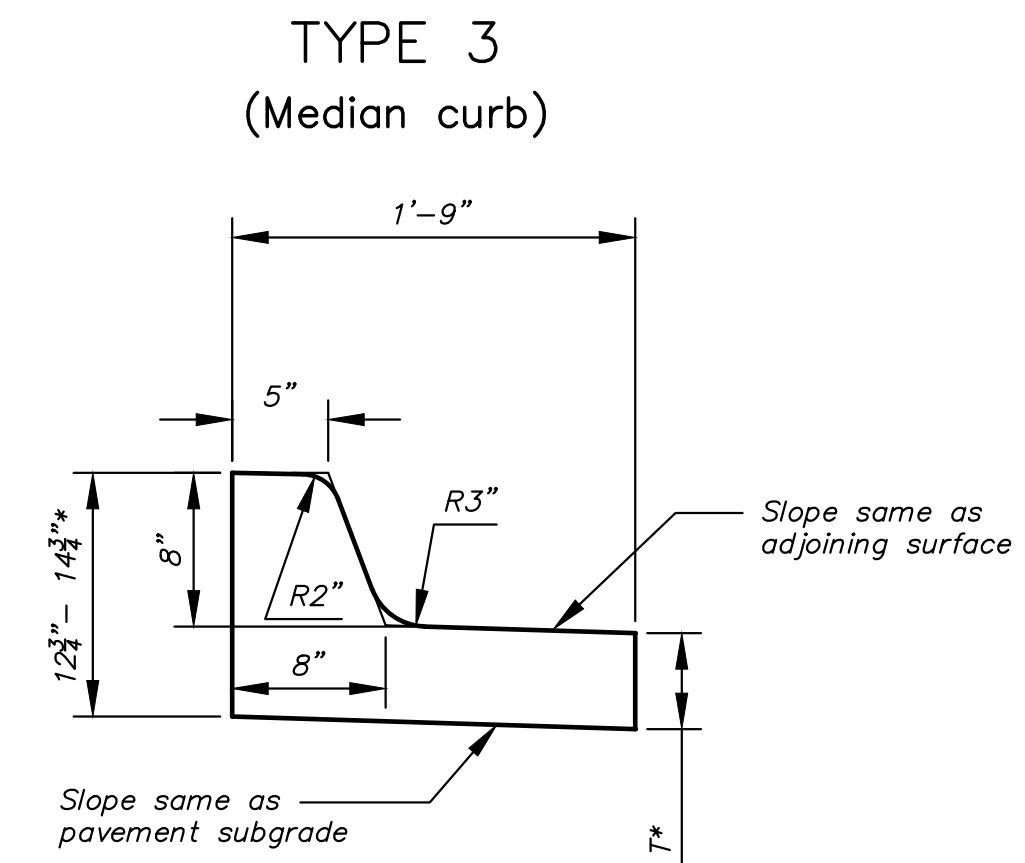
Combined Curb & Gutter (1 1/2")



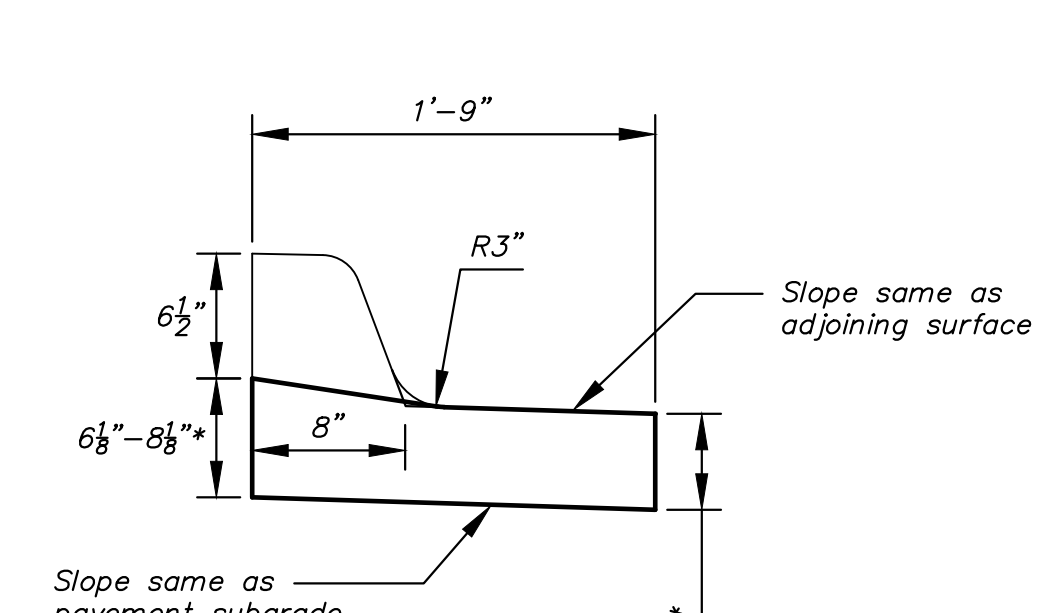
Combined Curb & Gutter (3 5/8")



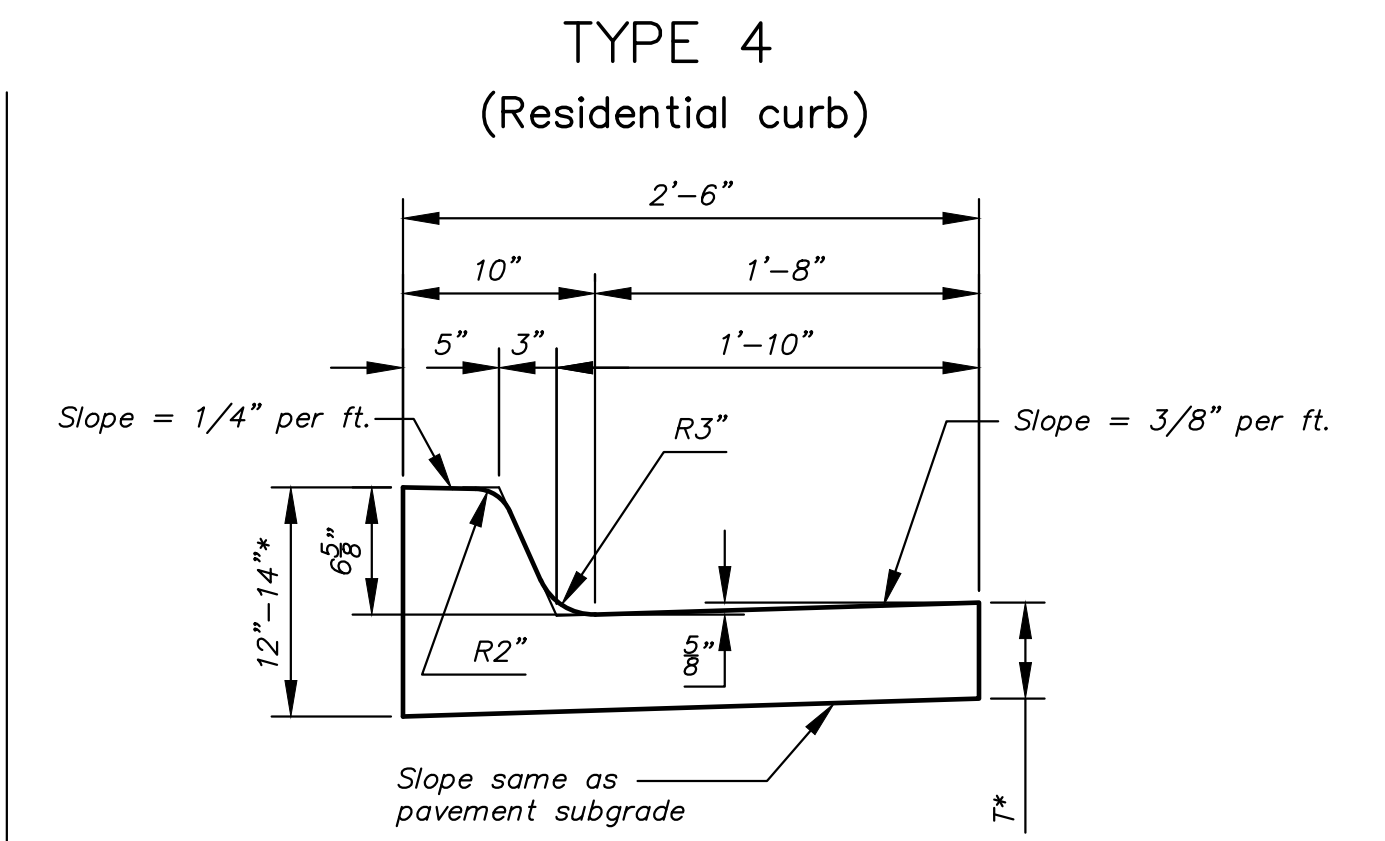
Combined Curb & Gutter (1 1/2")



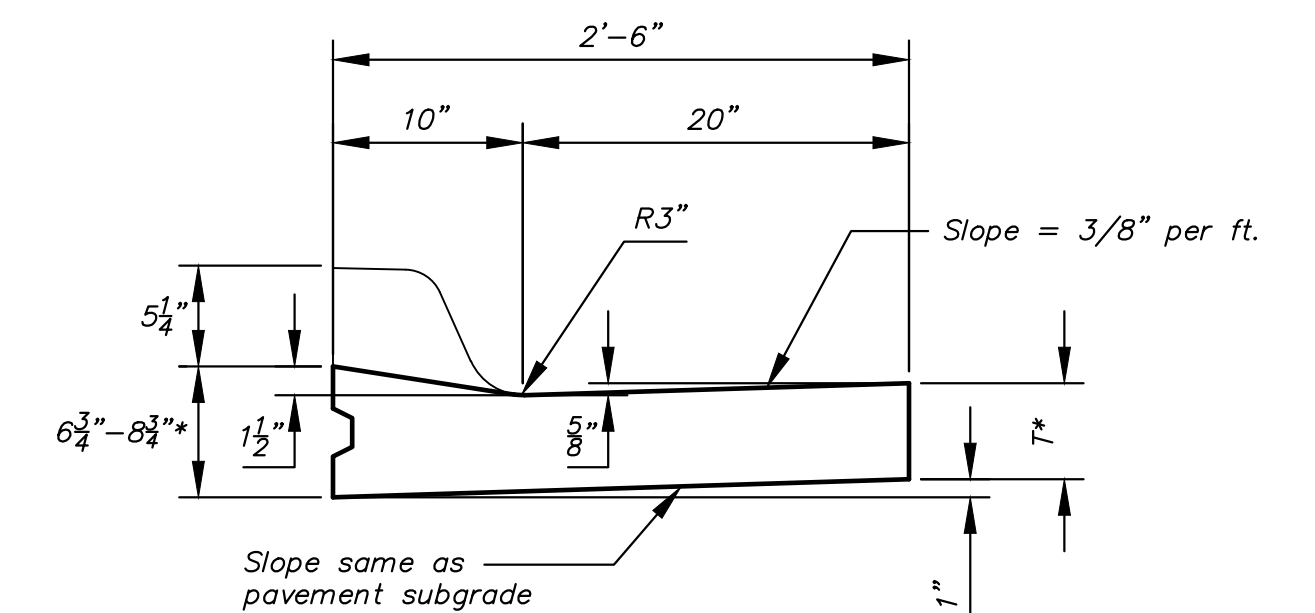
Combined Curb & Gutter (8")



Combined Curb & Gutter (1 1/2")



Combined Curb & Gutter (6 5/8")

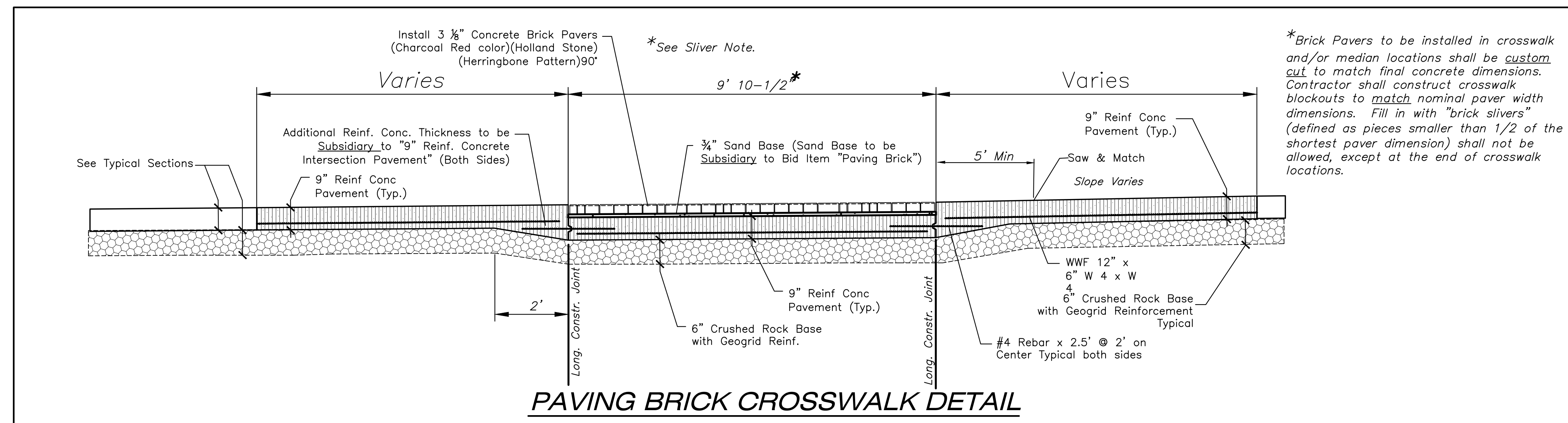
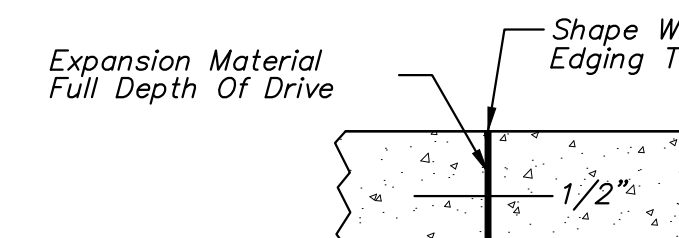
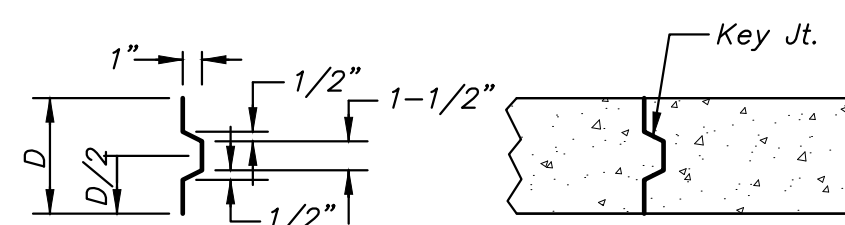
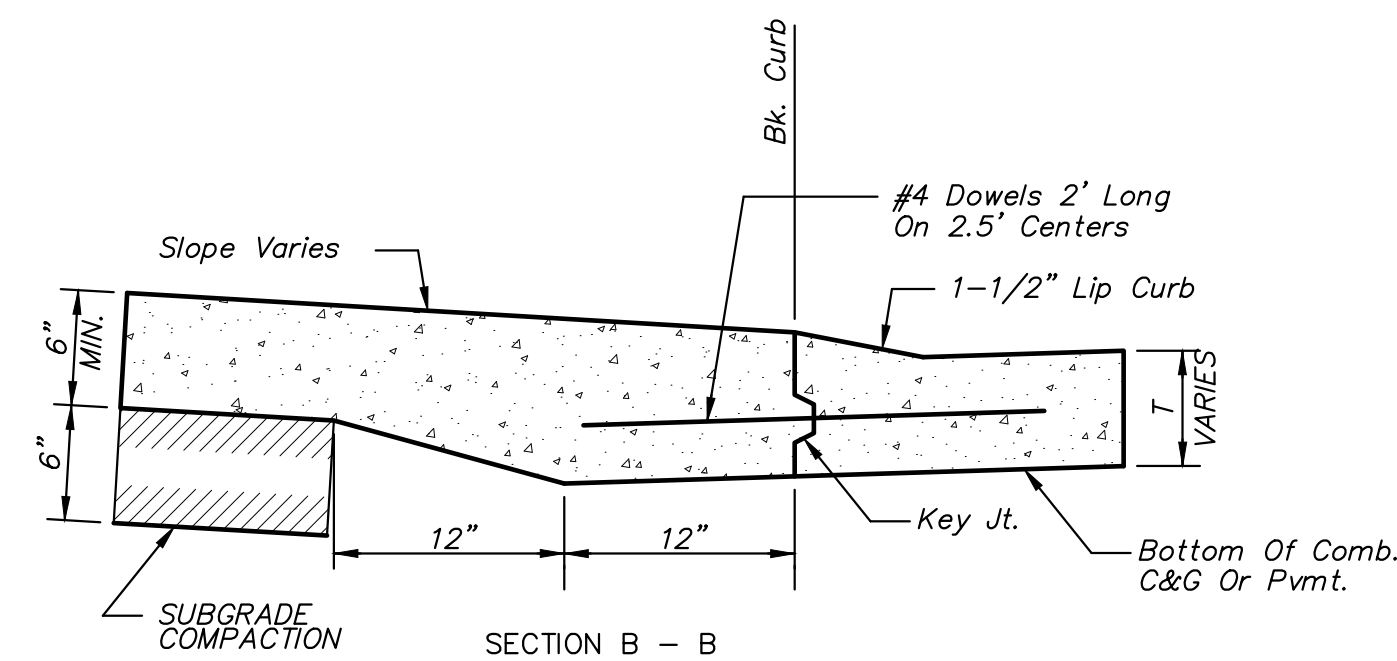


Combined Curb & Gutter (1 1/2")

T^* = Thickness of curb to adjust with pavement thickness

GENERAL NOTES

1. Expansion (isolation) joints shall be constructed a maximum of 300' apart and at all Pls, PCs, cul-de-sac quadrants, and ends of returns.
2. Contraction joints shall be constructed a minimum of 12' apart.
3. Joint sealer shall be required at all joints on arterial and industrial streets and at intersections on residential streets.



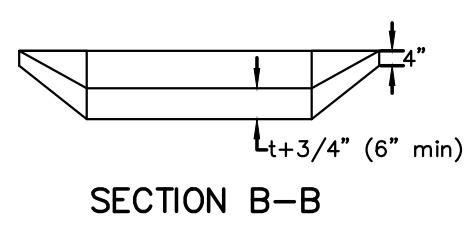
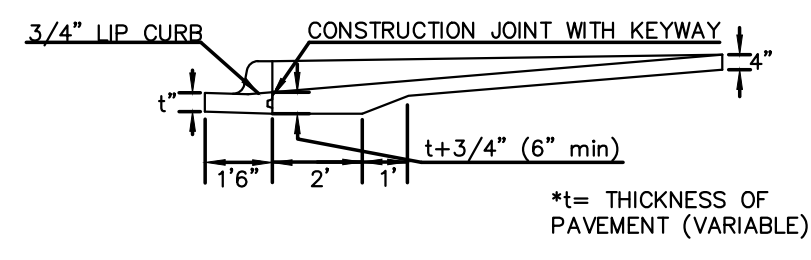
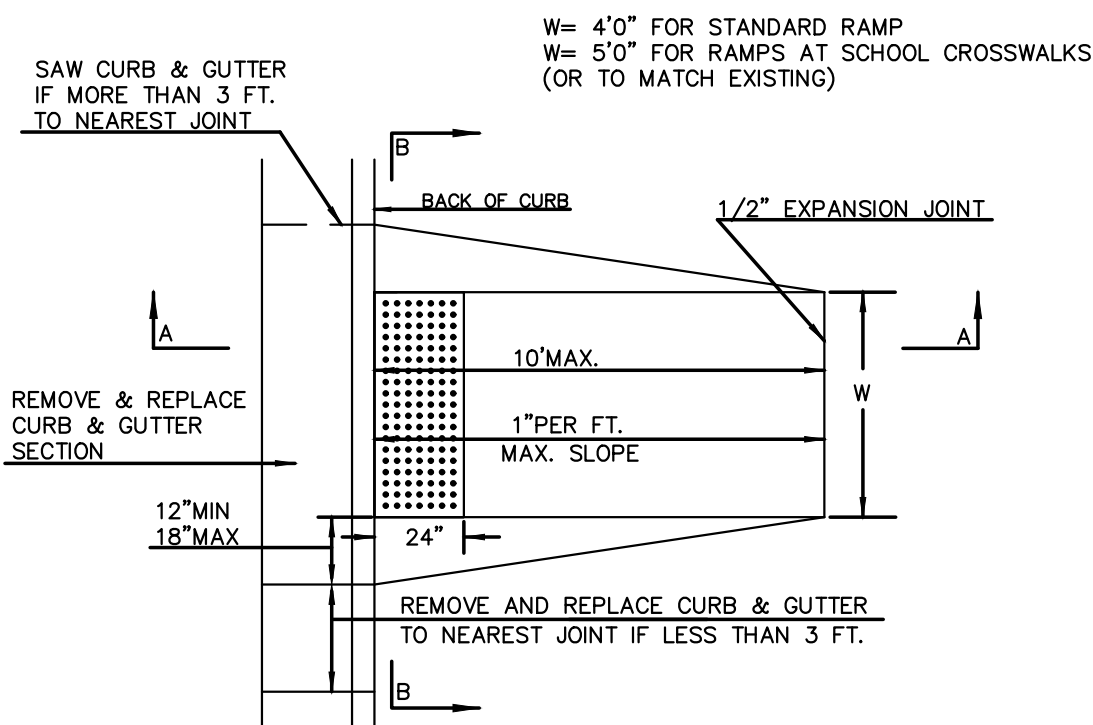
*CURB & GUTTER &
PAVING BRICK CROSSWALK
DETAILS*

CITY ENGINEER
GARY JANZEN, P.E.

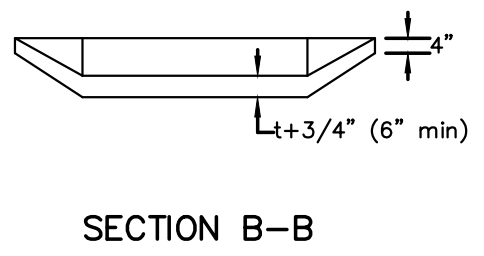
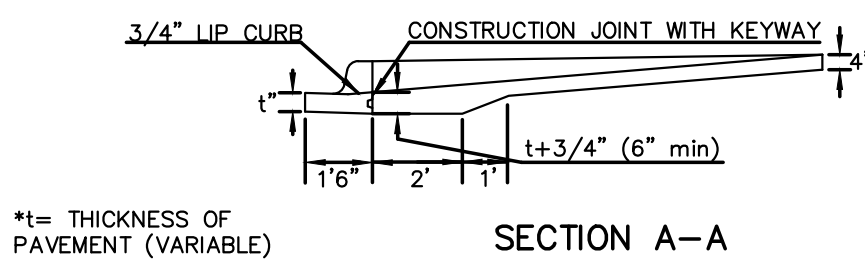
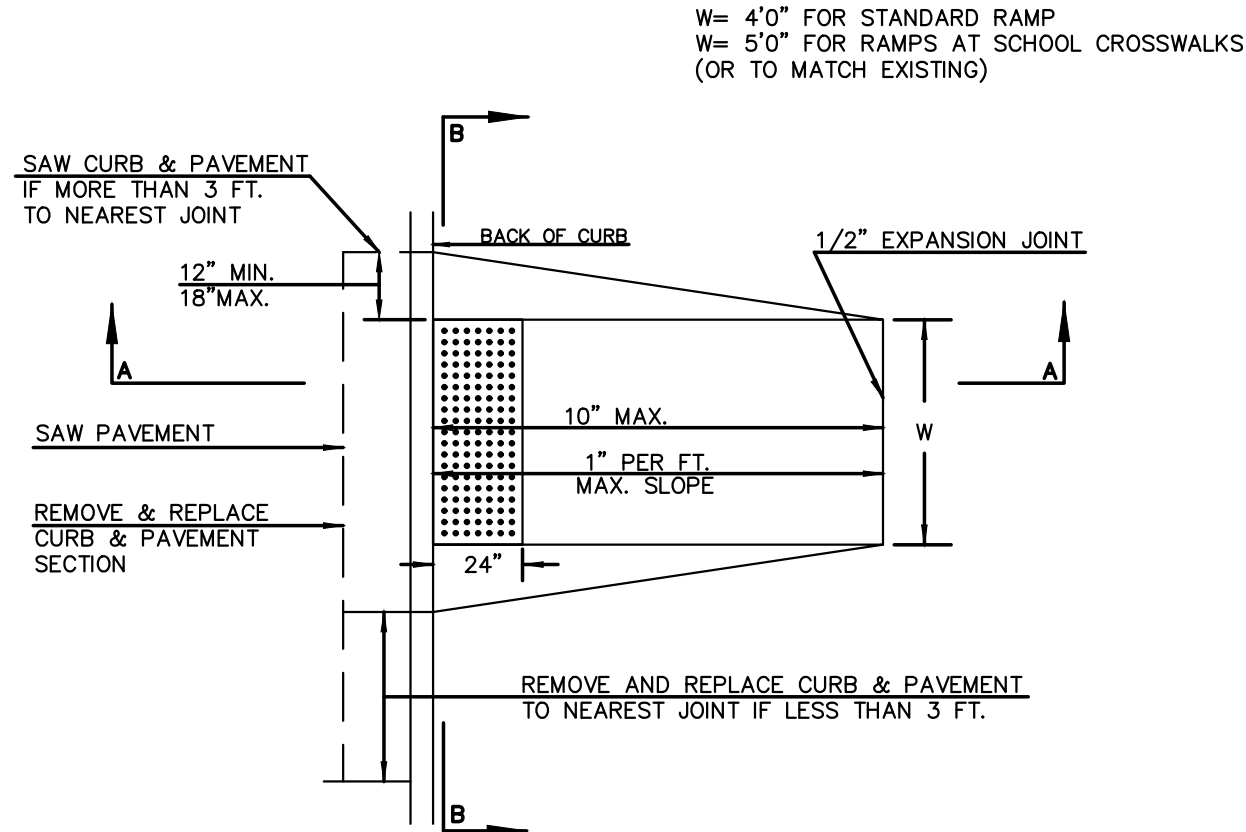
PROJECT NUMBER	OCA NUMBER	DATE
472-2022-085820B	####	SEPT. 2023

<p>CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501</p>	<p>SHEET</p> <p>05 OF 51</p>
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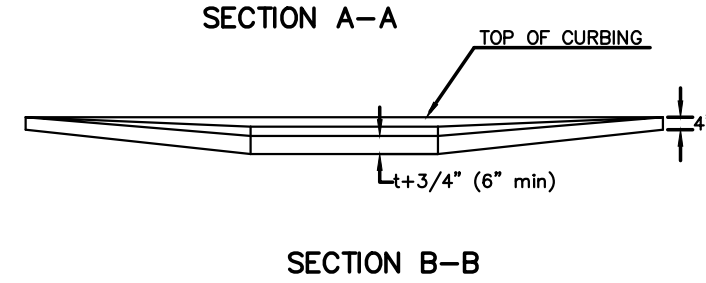
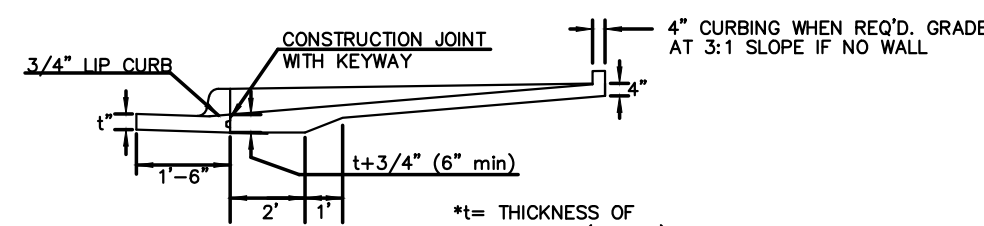
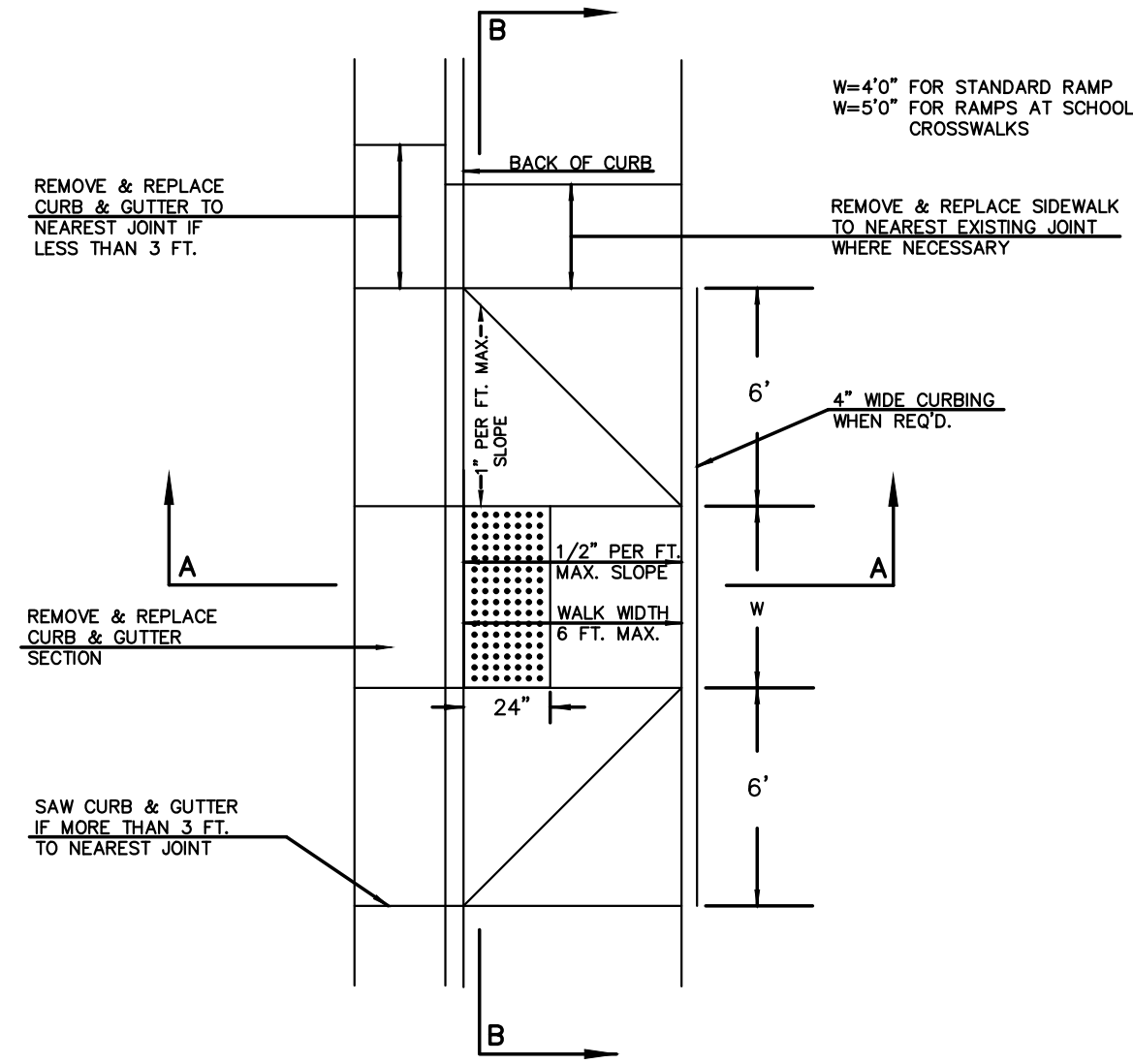
**STANDARD WHEELCHAIR RAMP
CONSTRUCTION DETAIL FOR STREETS
WITH COMBINED CURB & GUTTER
(TYPE A)**



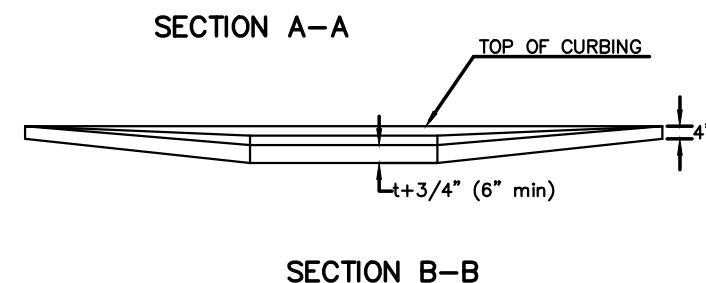
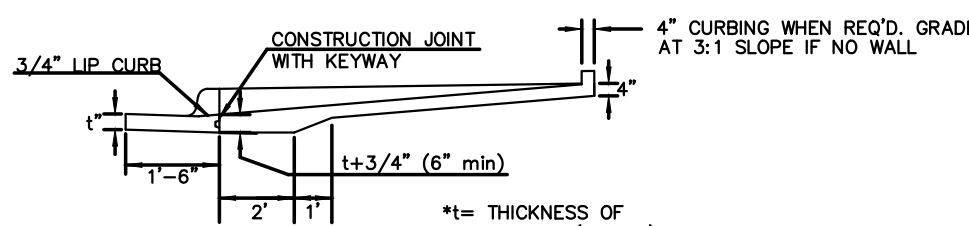
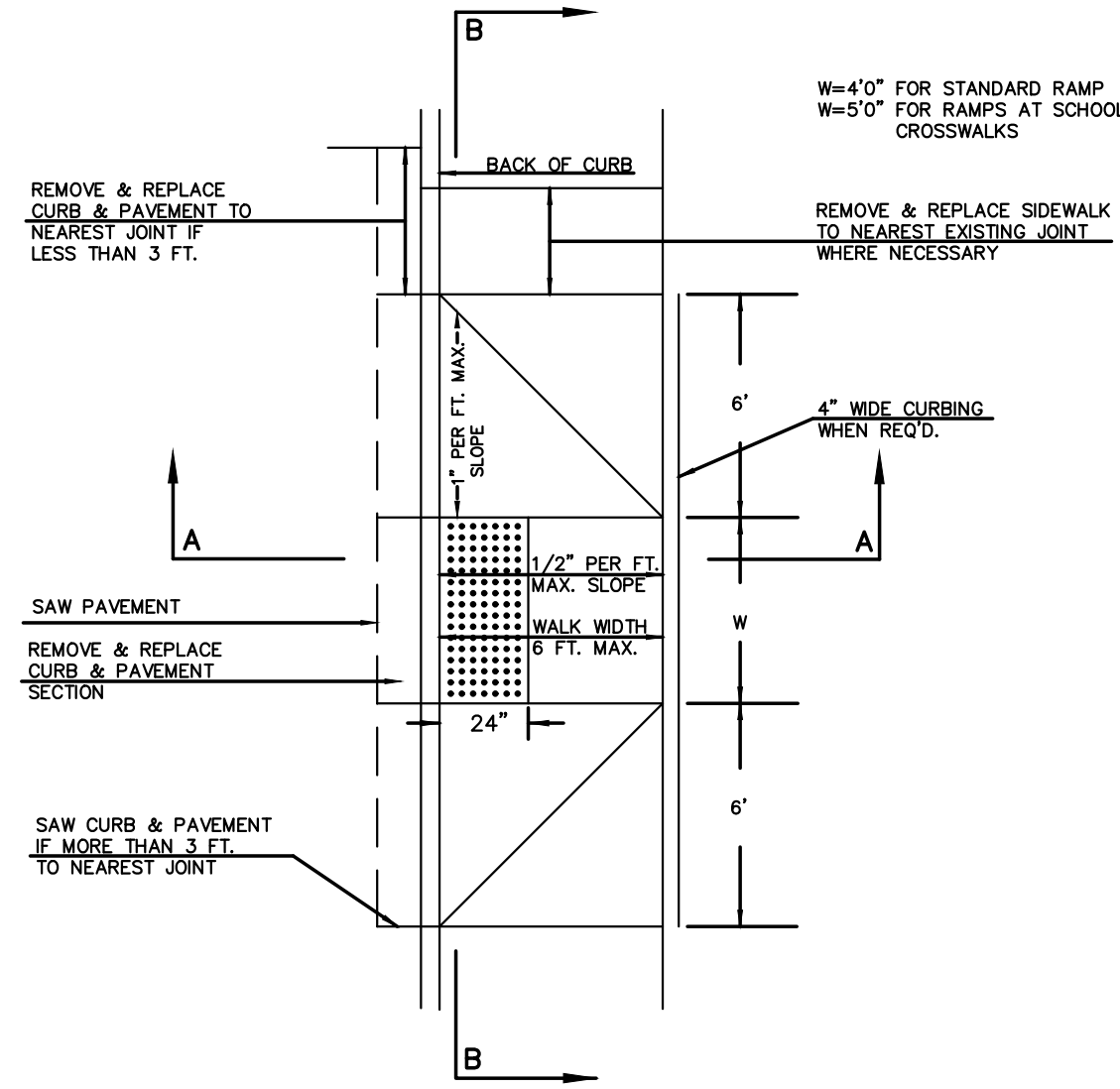
**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
CONCRETE STREETS WITH MONOLITHIC CURB
(TYPE A)**



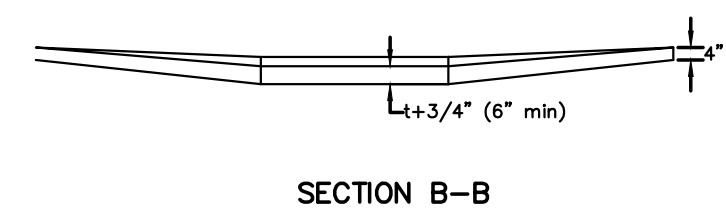
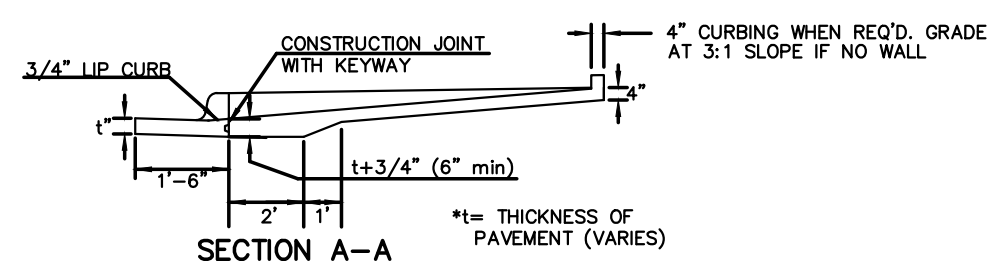
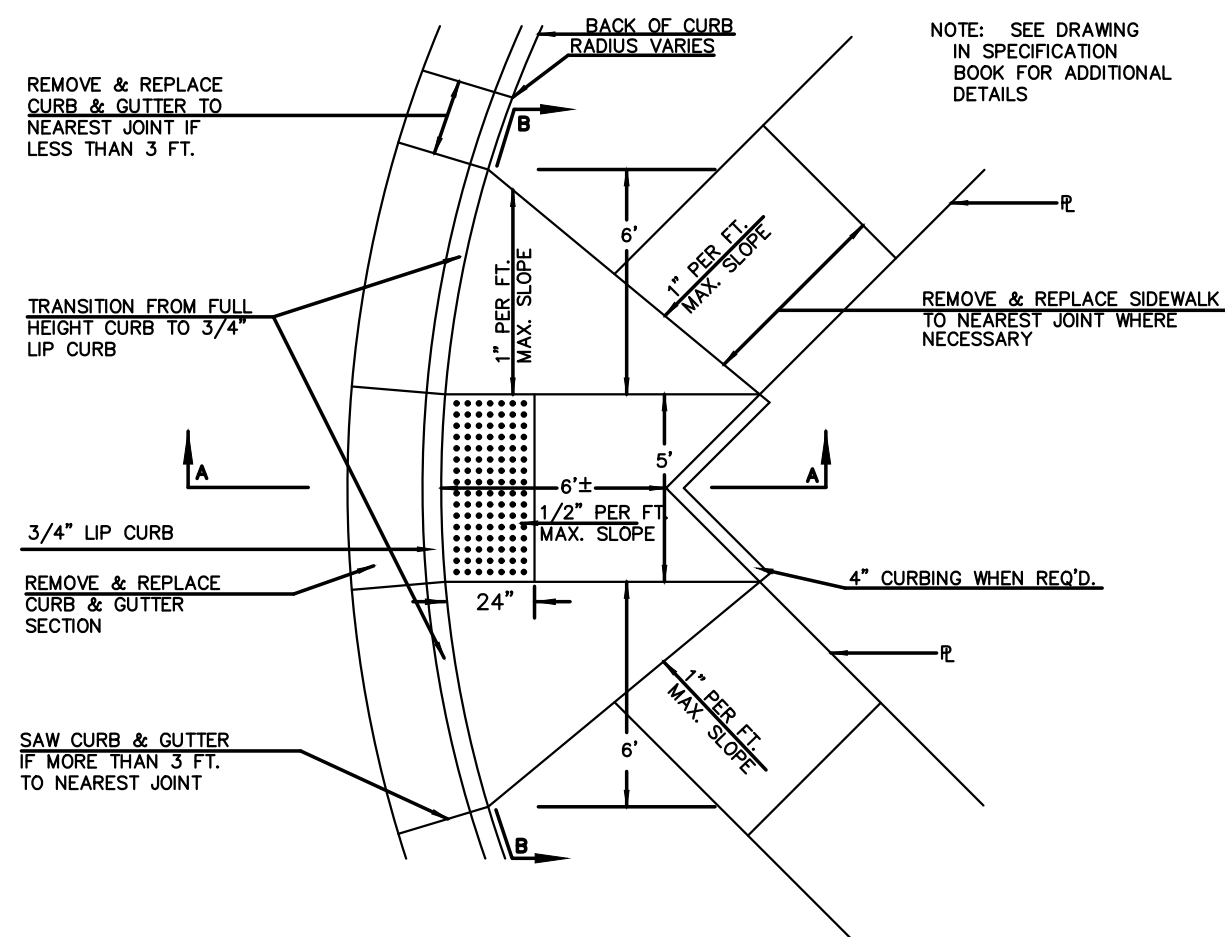
**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREETS WITH COMBINED CURB & GUTTER AND FULL WALK
(TYPE B)**



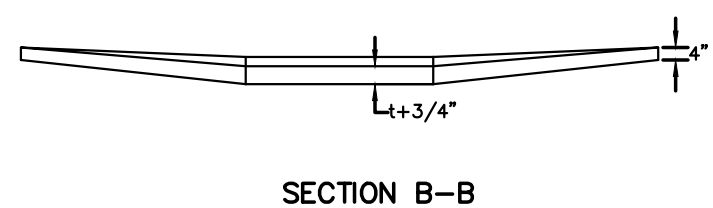
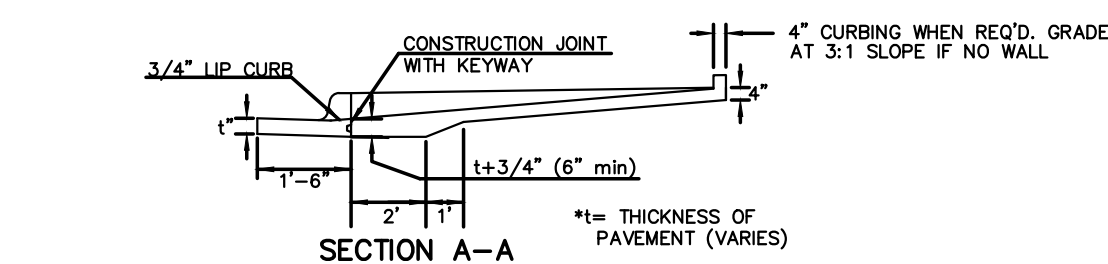
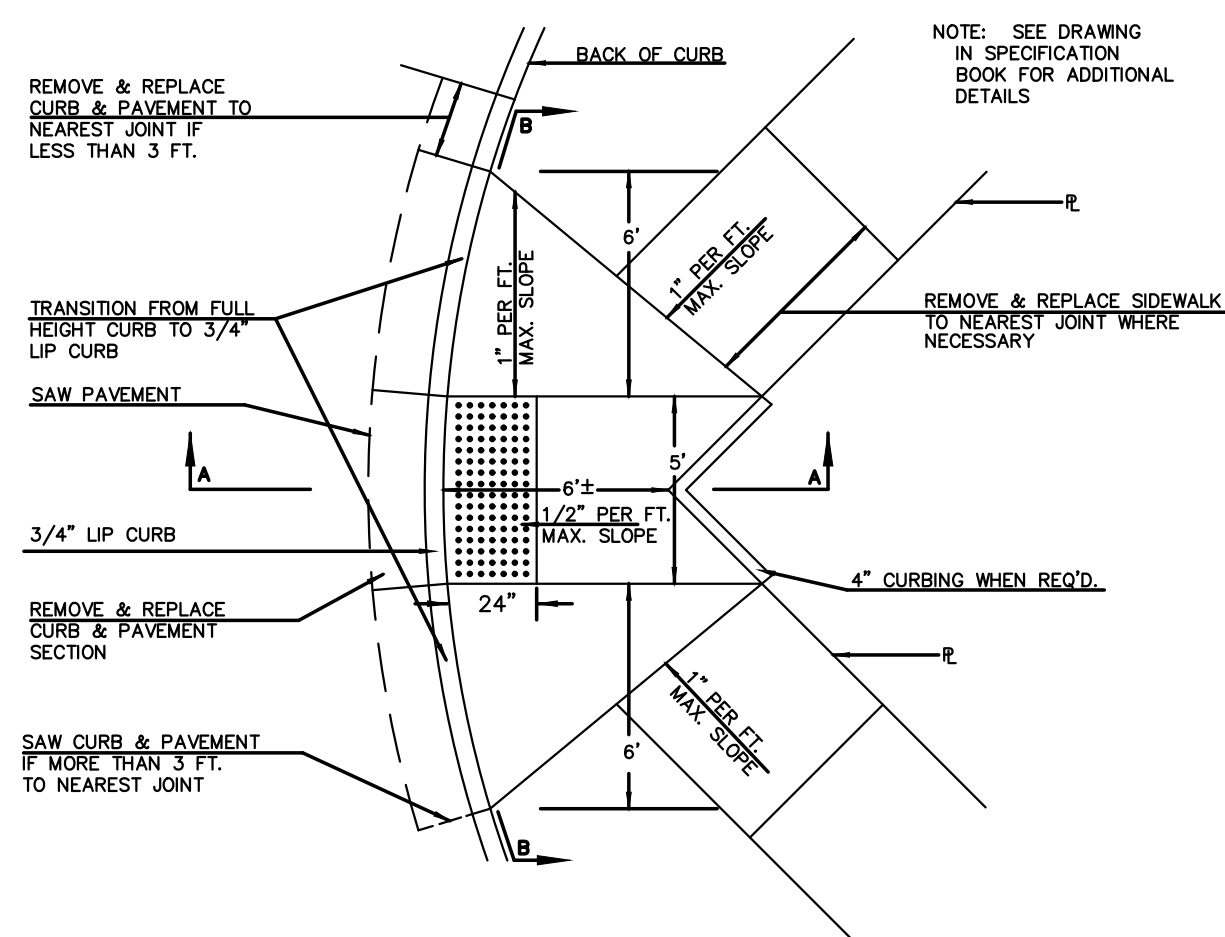
**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREETS WITH MONOLITHIC CURB AND FULL WALK
(TYPE B)**



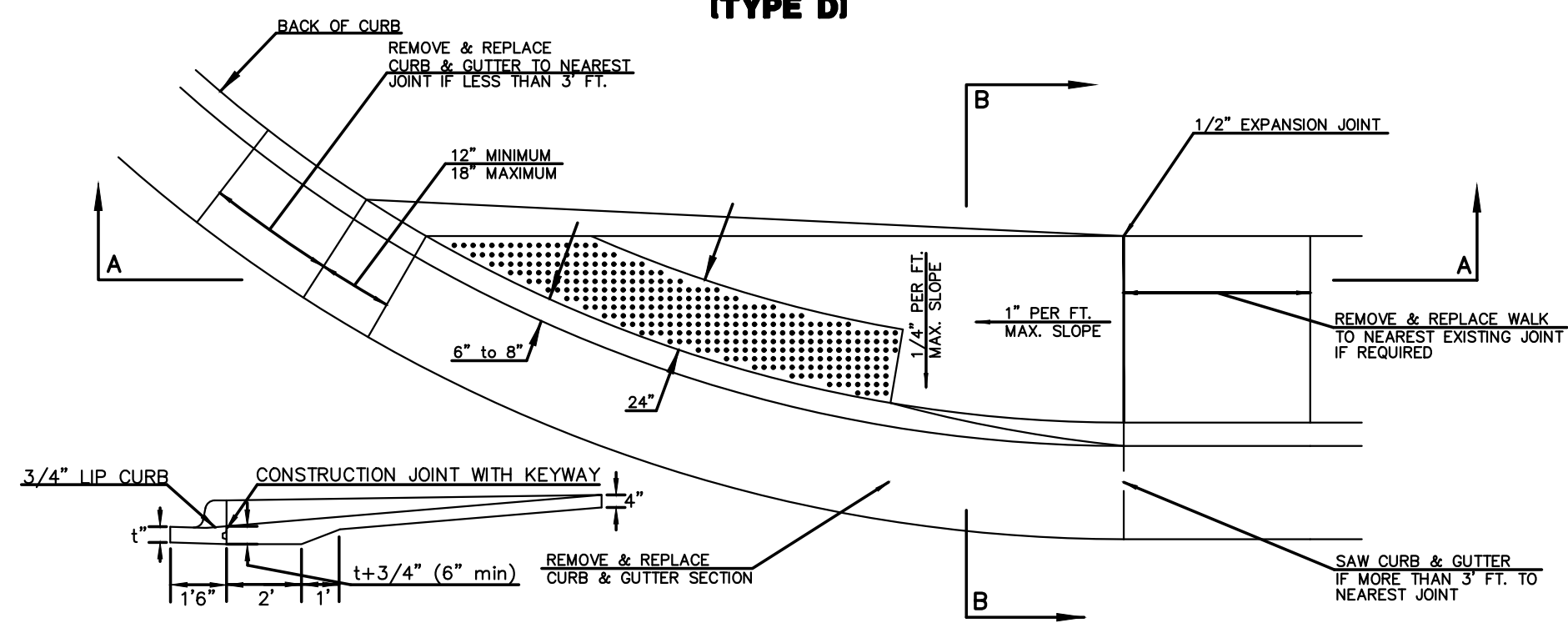
**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREET WITH COMBINED CURB AND GUTTER ON RADIUS
WITH 6'± FROM BACK OF CURB TO PROPERTY CORNER
(TYPE C)**



**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREET WITH MONOLITHIC CURB ON RADIUS
WITH 6'± FROM BACK OF CURB TO PROPERTY CORNER
(TYPE C)**

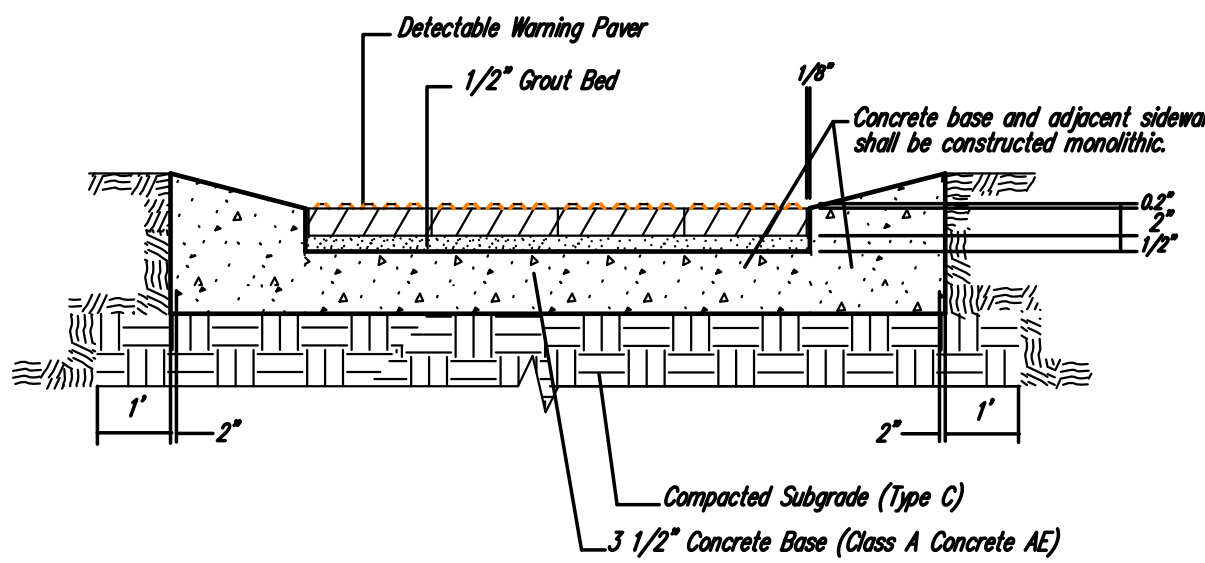
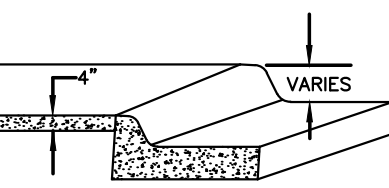
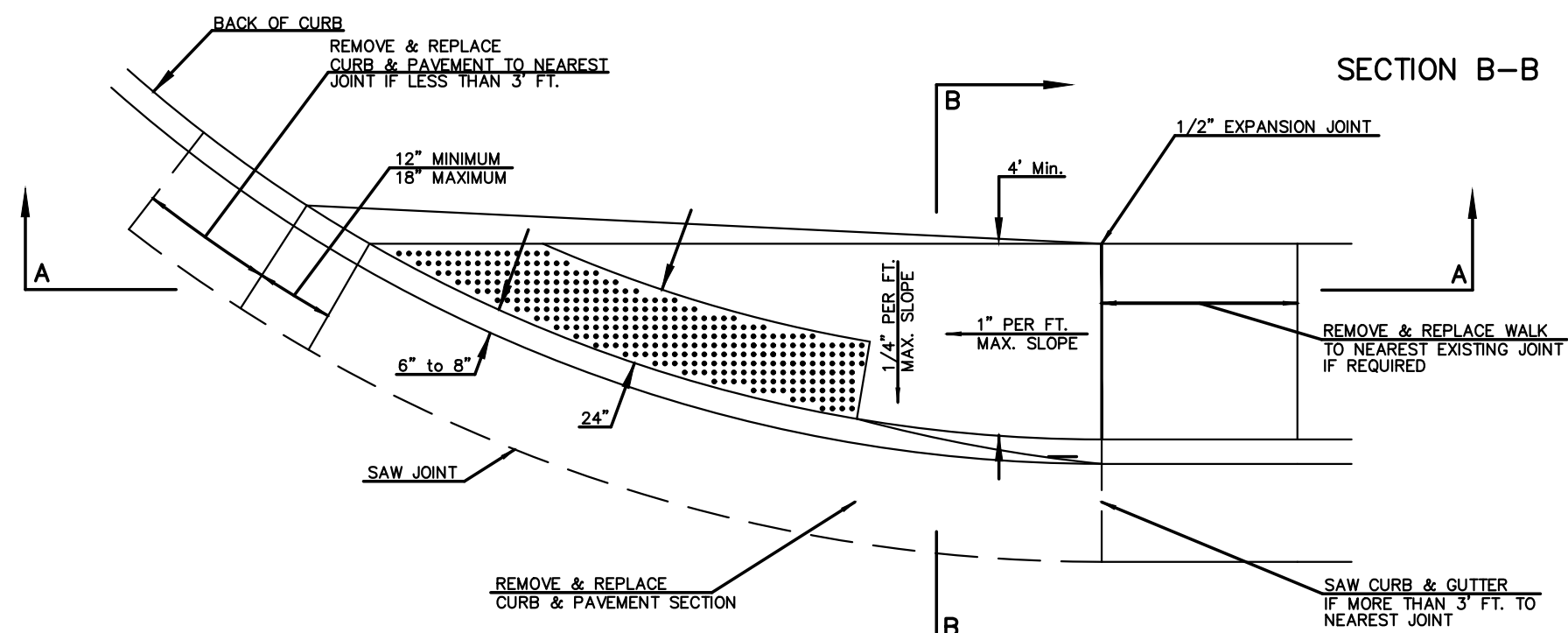


**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREETS WITH COMBINED CURB & GUTTER
WITH ONE FULL SIDEWALK
(TYPE D)**

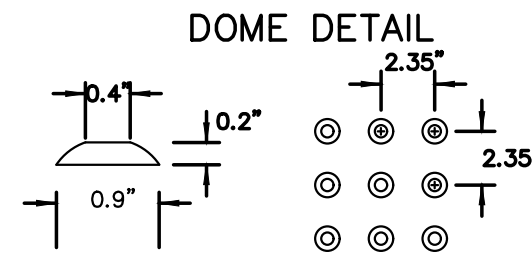


SECTION A-A

**STANDARD WHEELCHAIR RAMP CONSTRUCTION DETAIL FOR
STREETS WITH MONOLITHIC CURB
WITH ONE FULL SIDEWALK
(TYPE D)**



NOTE: HANOVER DETECTABLE WARNING PAVERS (OR AN APPROVED ALTERNATE) SHALL BE USED IN ALL WHEELCHAIR RAMPS. THE 11 3/4" RED 15" PAVES SHALL BE USED IN ALL APPLICATIONS.
HANOVER ARCHITECTURAL PRODUCTS
240 BENDER ROAD
HANOVER, PA 17331
1-717-637-0500
www.hanoverpavers.com



**WHEELCHAIR RAMP
DETAILS WITH
DETECTABLE WARNING**

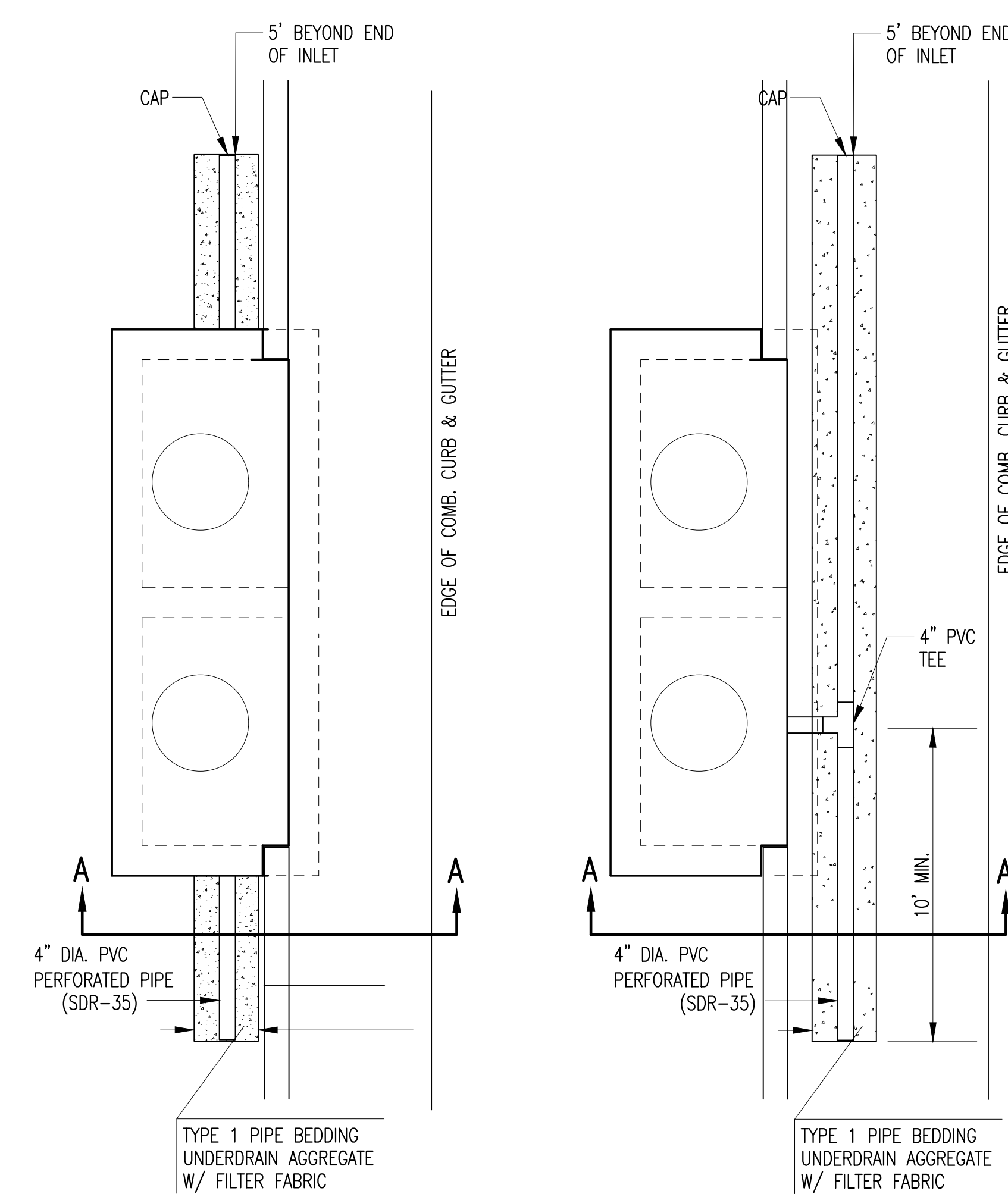
CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	OCA NUMBER	DATE
472-2022-085820B	####	SEPT. 2023

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

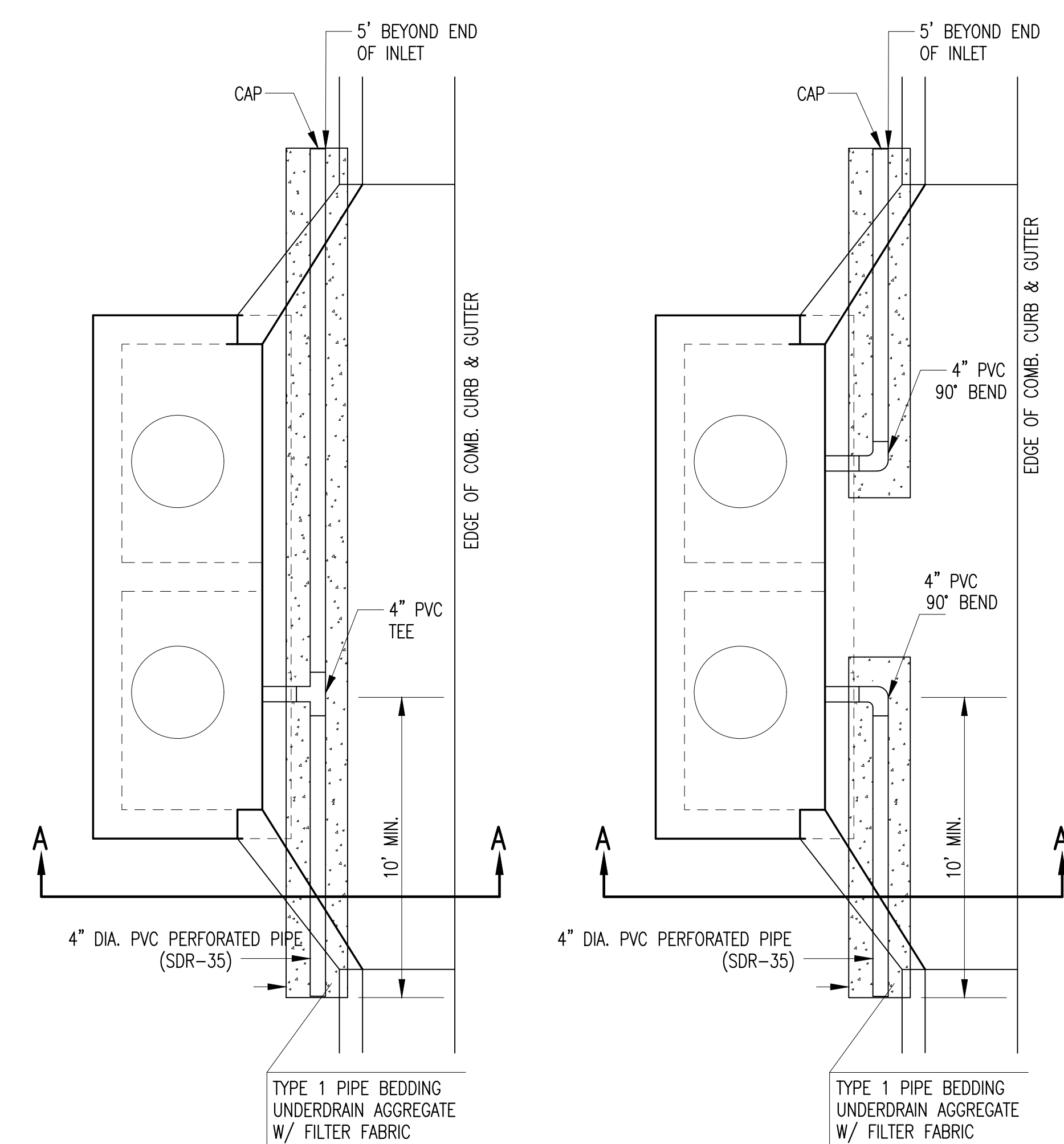
SHEET
07 OF 51

PAVEMENT UNDERDRAIN SHALL BE INSTALLED ON ALL CURB INLETS.



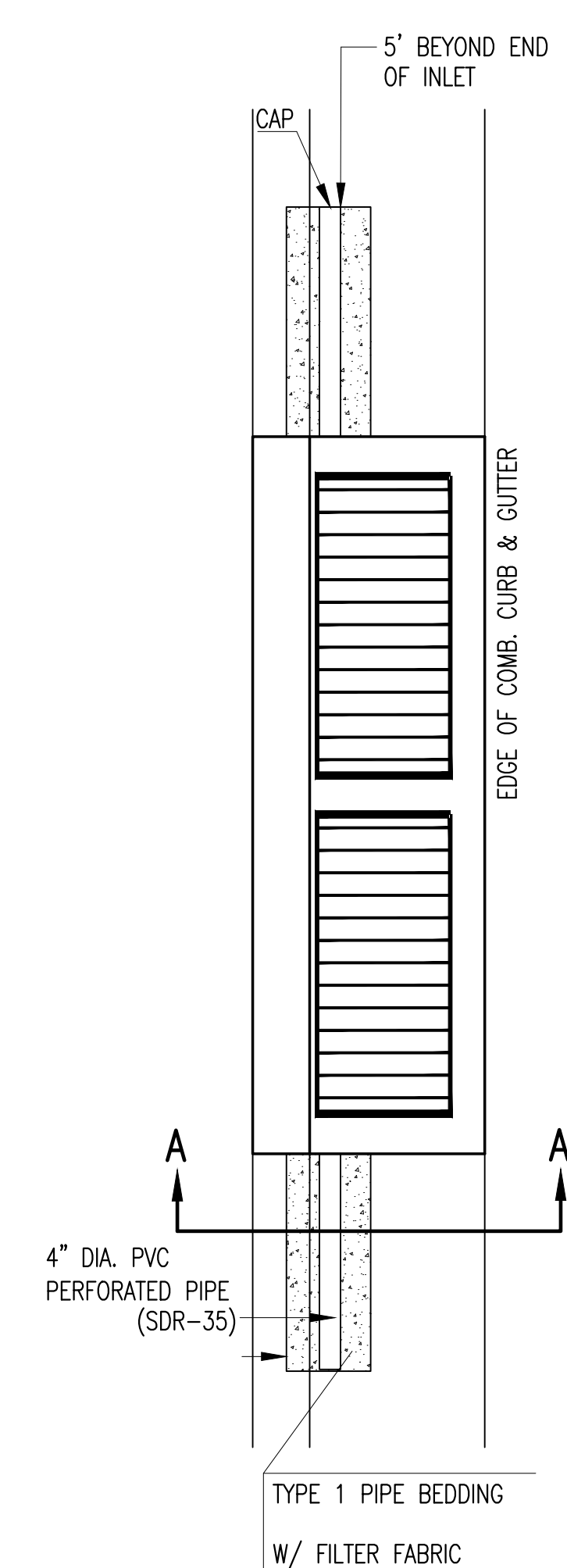
TYPE 1
OPTION 1

TYPE 1
OPTION 2

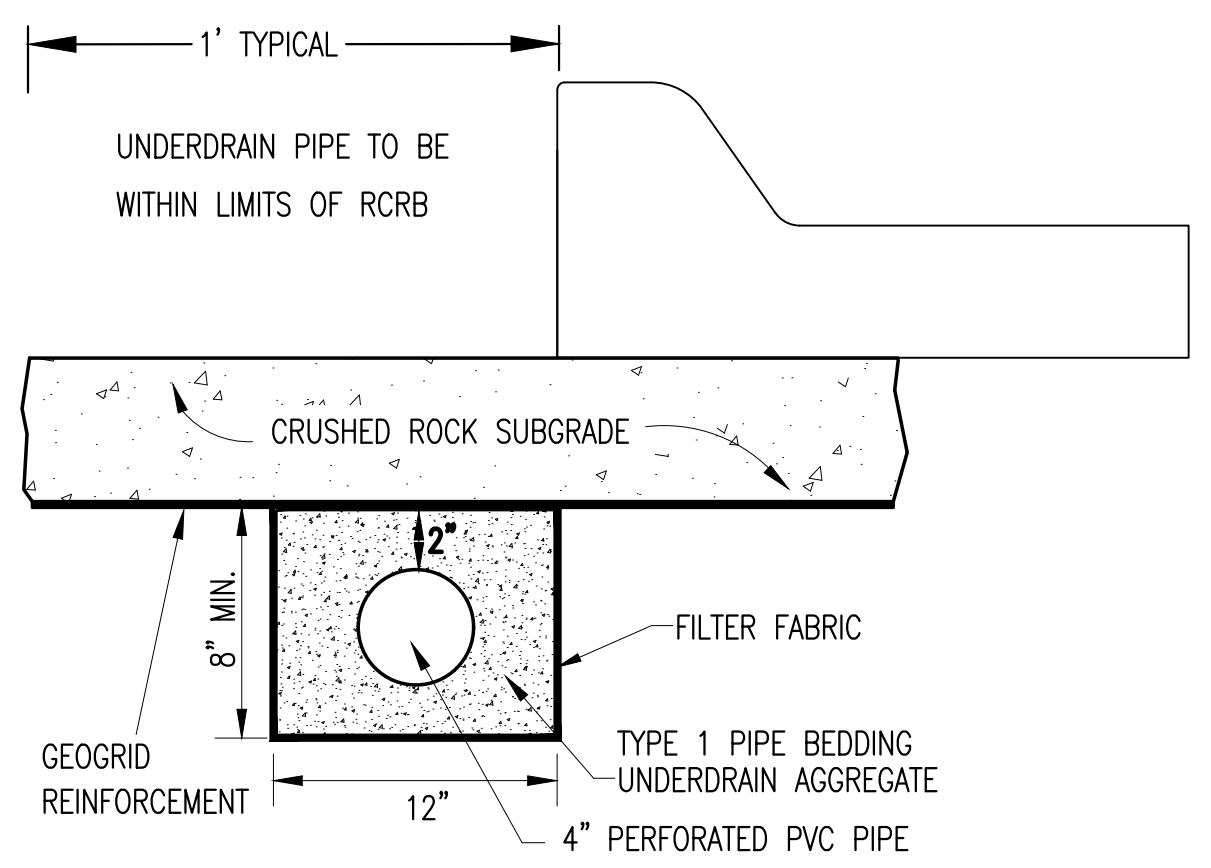


TYPE 1-A INLET
OPTION 1

TYPE 1-A INLET
OPTION 2



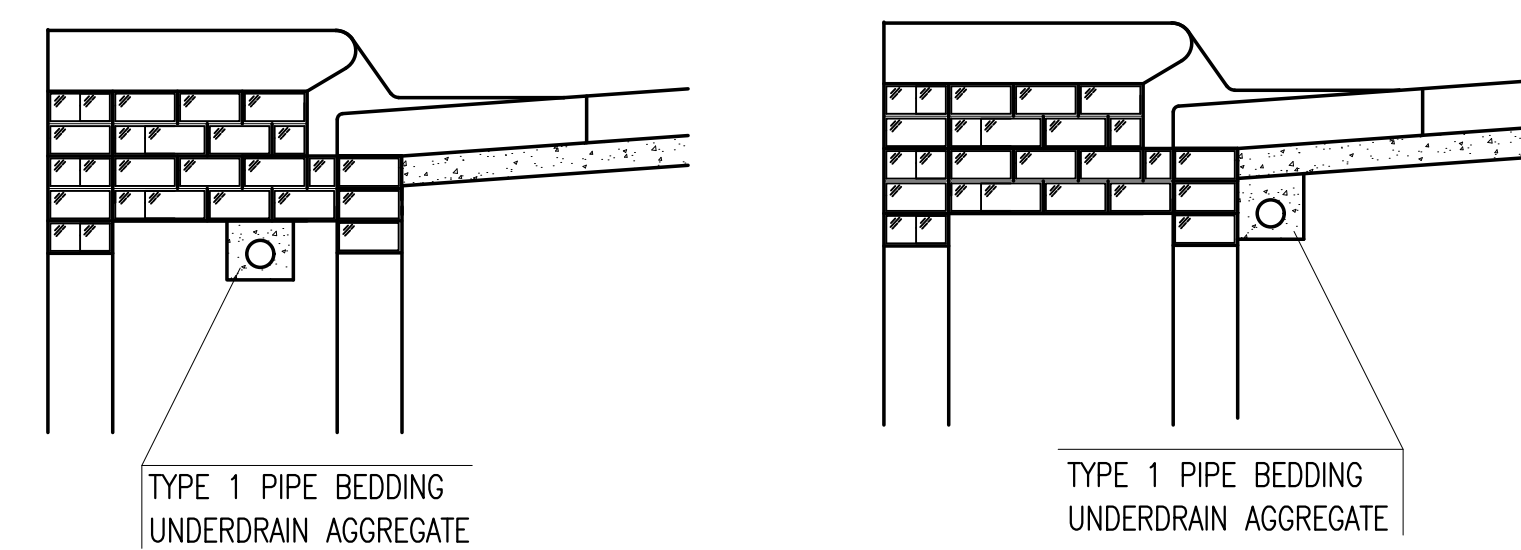
TYPE 2



SECTION A-A (TYPICAL)

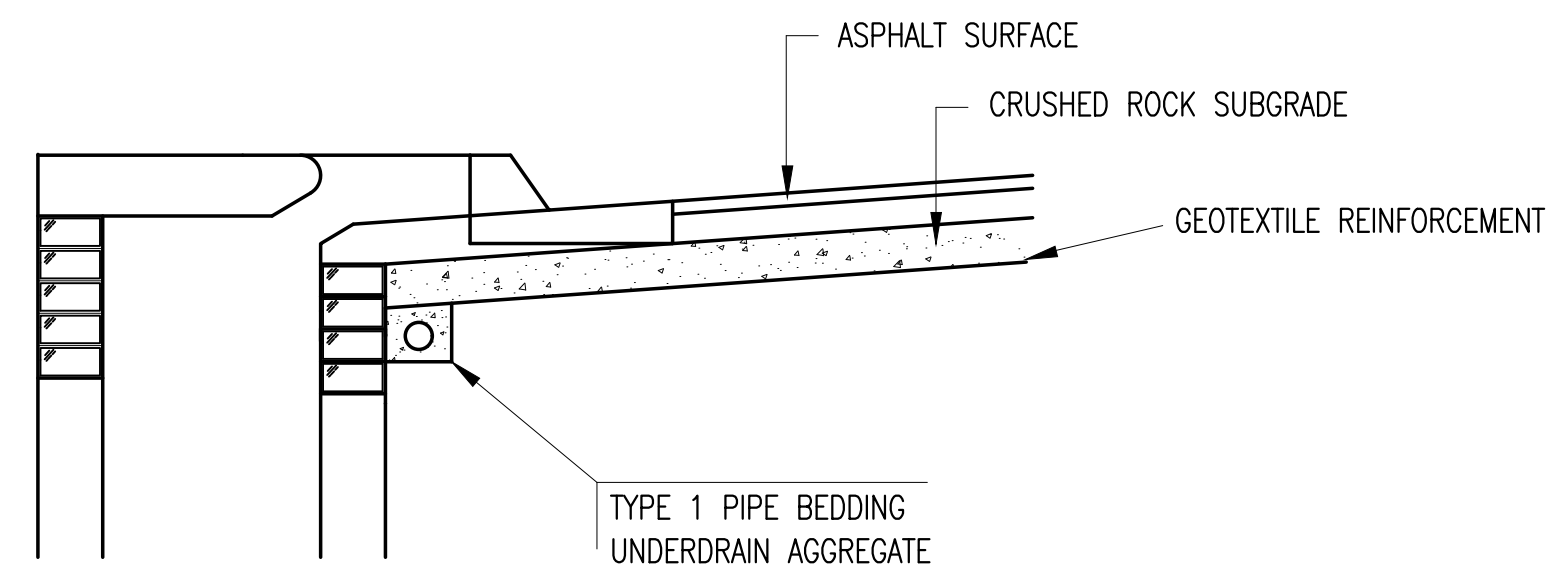
GENERAL NOTES

1. PAVEMENT CONTRACTOR WILL BE REQUIRED TO INSTALL SDR 35, 4" PERFORATED DRAIN PIPE AND TEE AS INDICATED IN THE DETAILS.
2. WHEN SWS CONSTRUCTED BY SEPARATE PROJECT, SWS CONTRACTOR SHALL INSTALL SDR 35, 4" DRAIN PIPE STUB ONLY THROUGH WALLS OF CURB INLETS AND CAP TO ALLOW FUTURE CONNECTION OF TEE AND ADDITIONAL DRAIN PIPE BY OTHERS.
2. UNDERDRAIN PIPE SHALL BE PAID AS A MEASURED QUANTITY BY THE LINEAL FOOT.



(MIN. 16 PERFORATIONS PER LIN. FT. @ 1/4" DIA.)
PERFORATIONS TO BE ON BOTTOM HALF

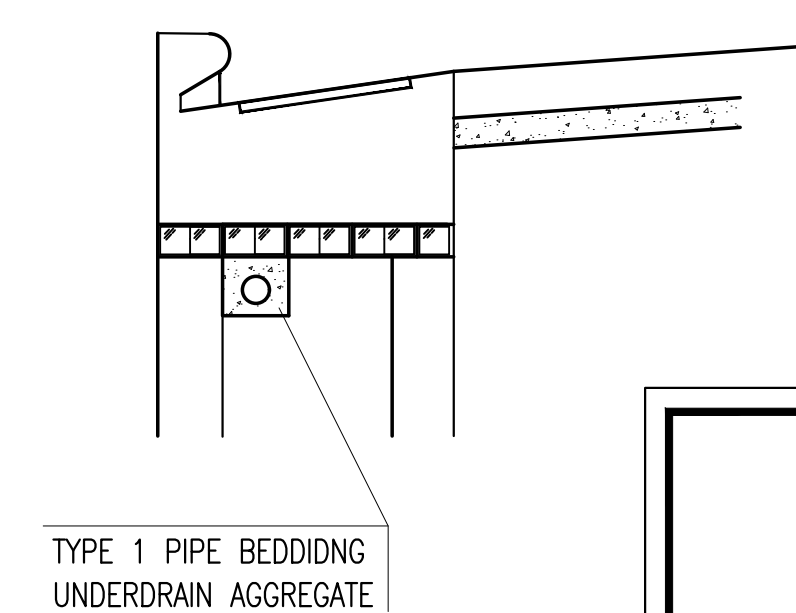
SECTION A-A




SECTION A-A

PAVEMENT UNDERDRAIN DETAIL

BID ITEM TO BE PROVIDED PER 4" PERFORATED UNDERDRAIN PIPE.

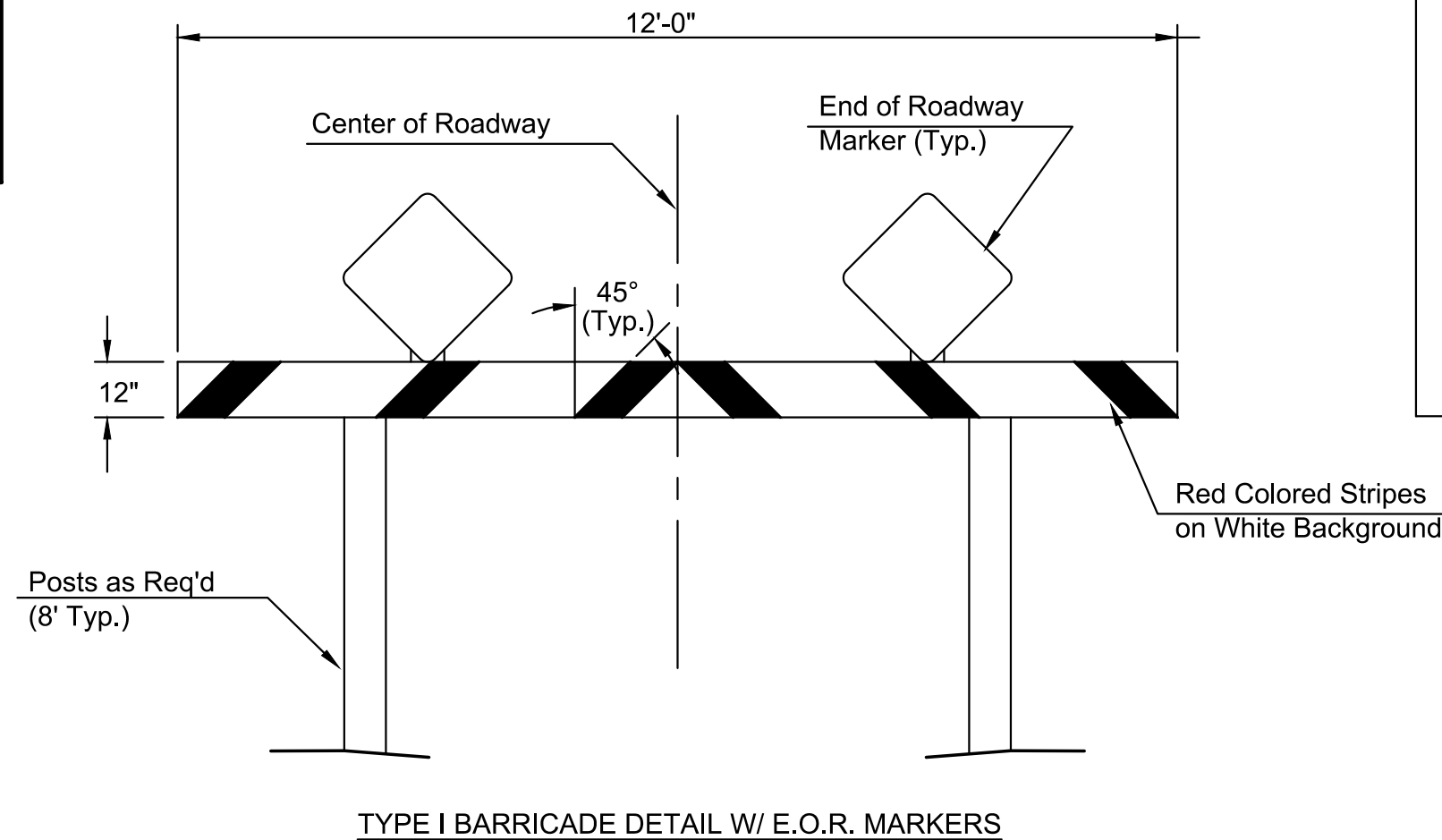
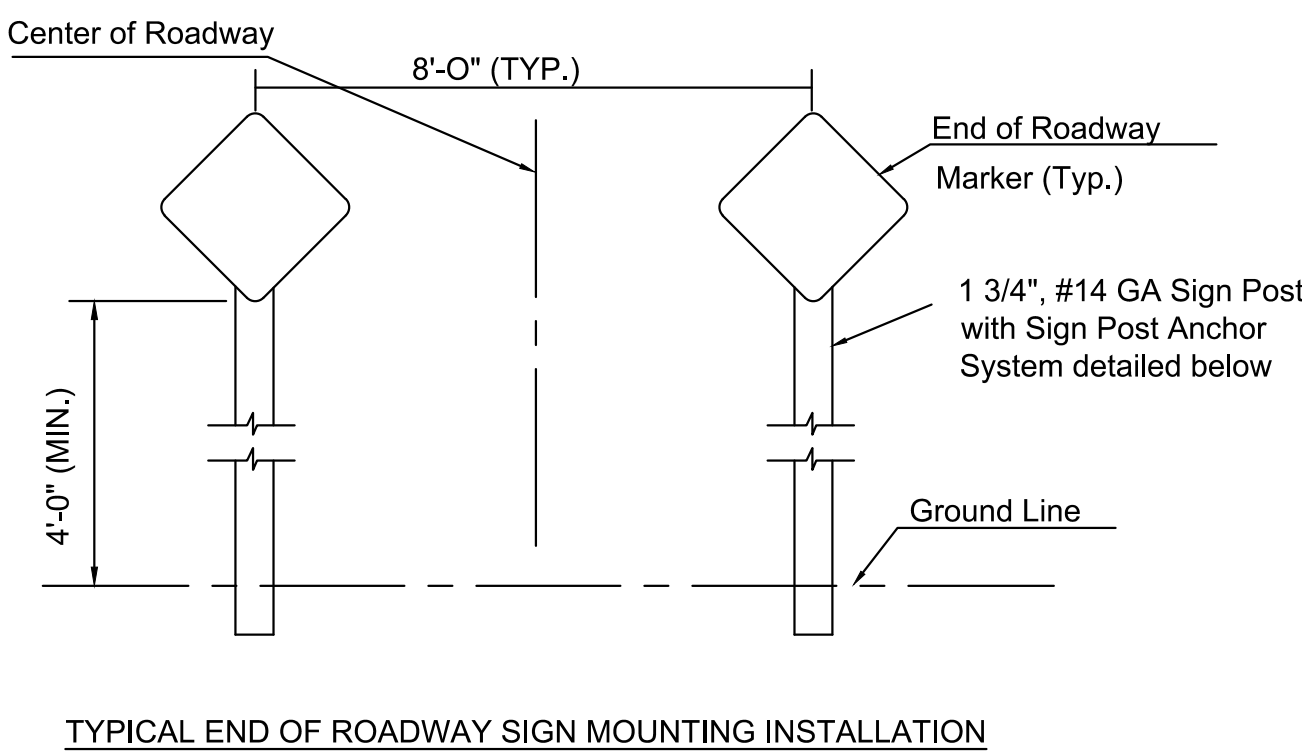
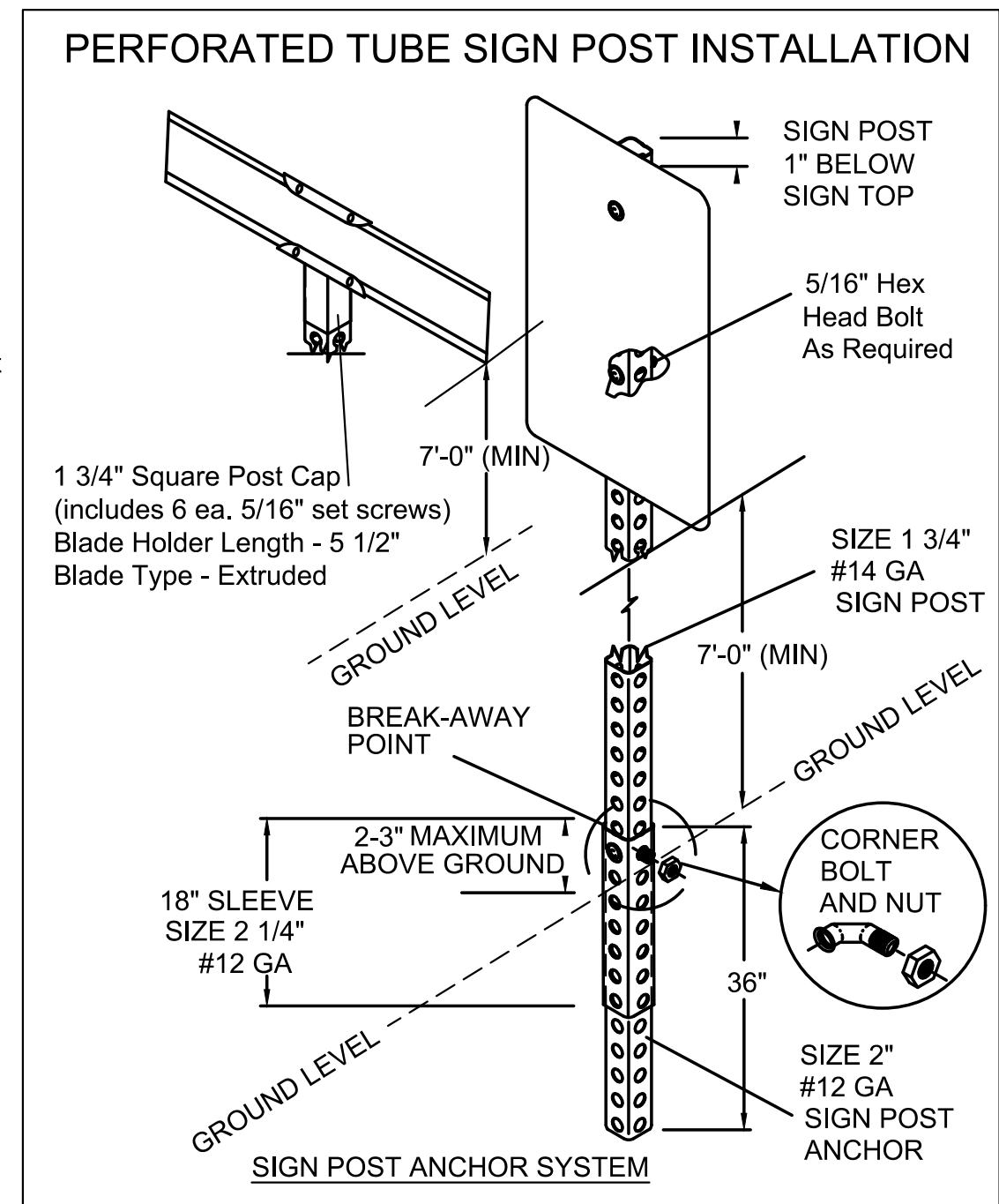
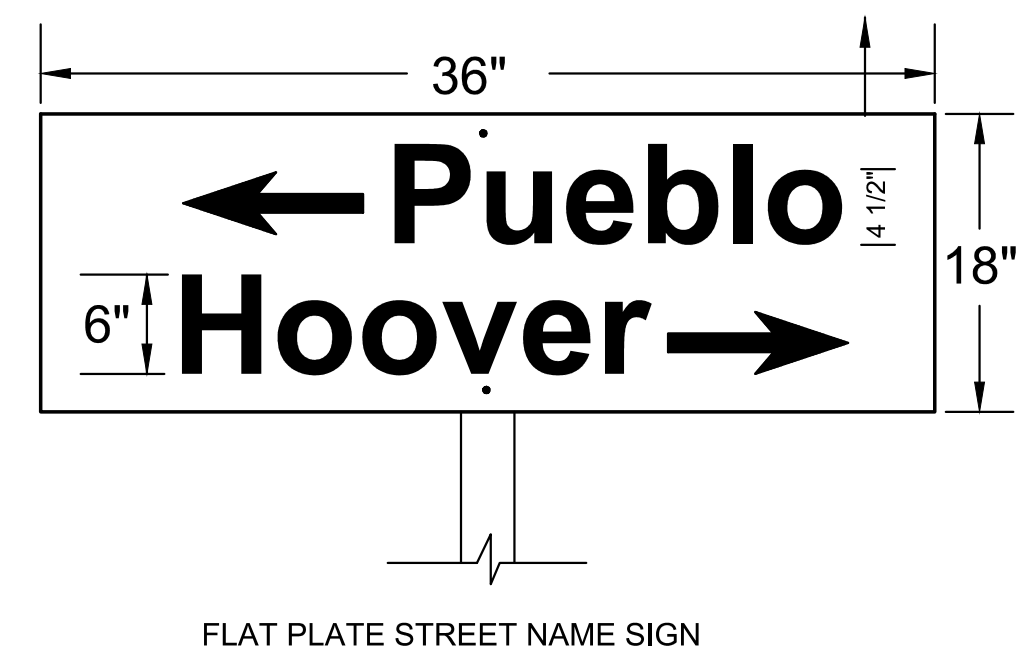
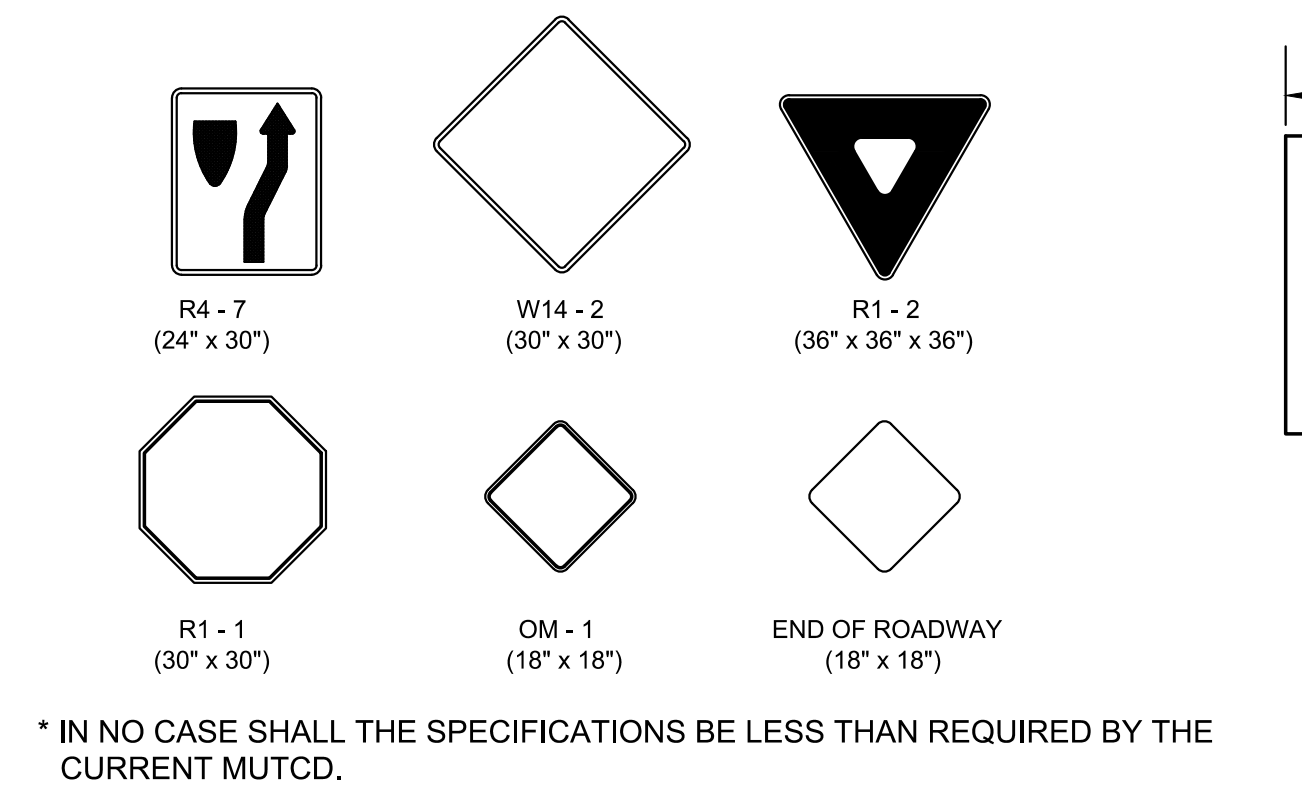
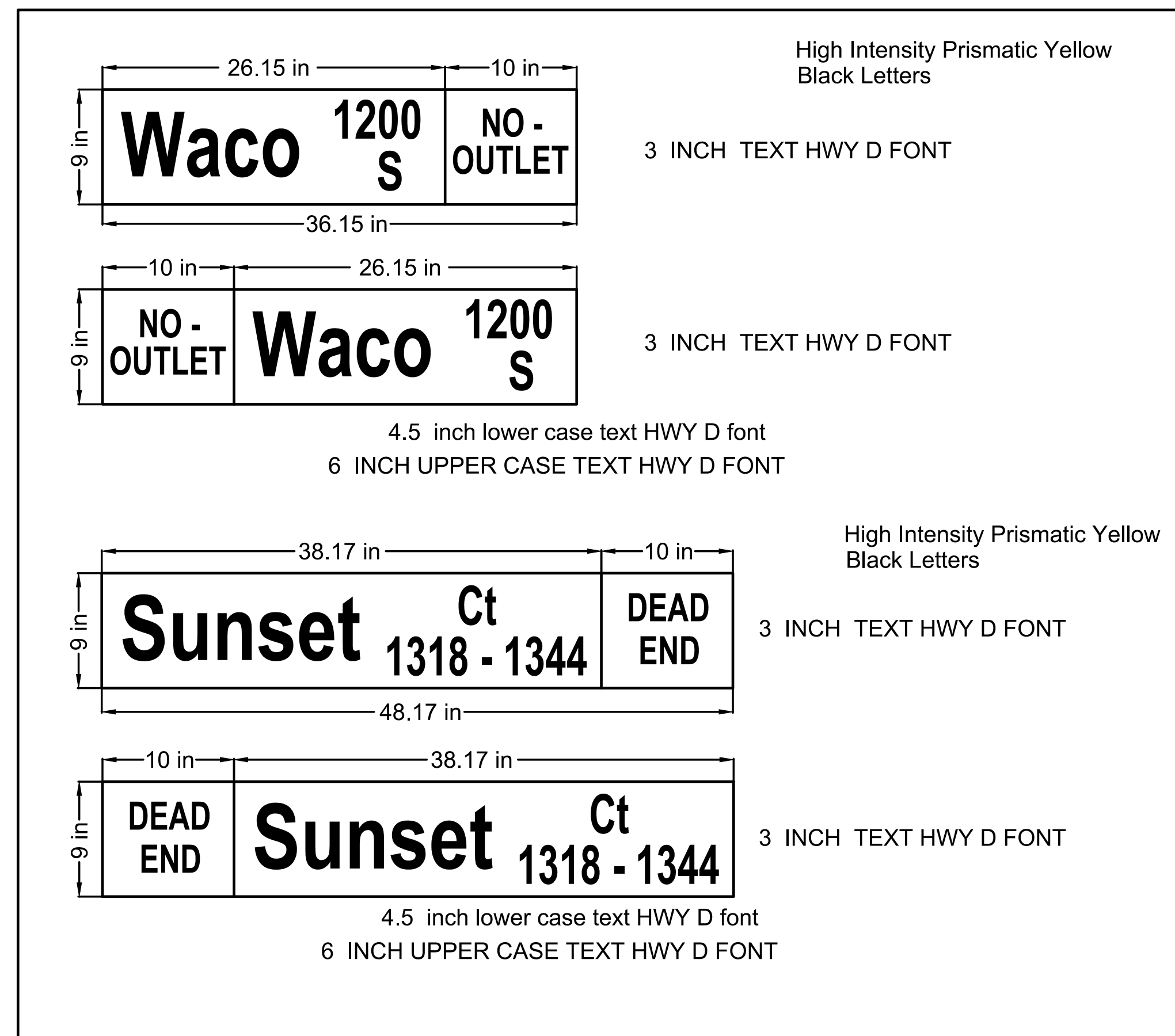


SECTION A-A

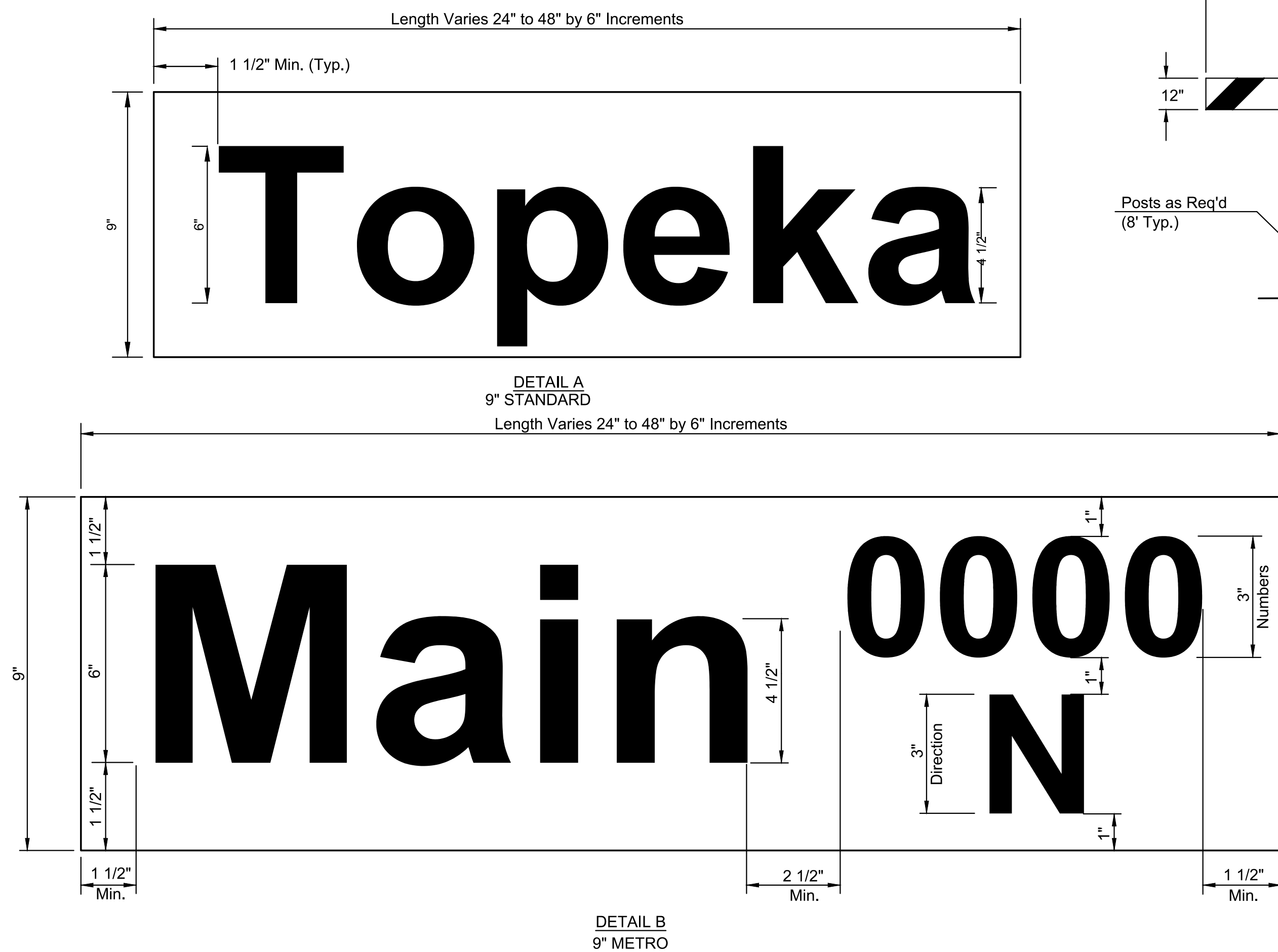
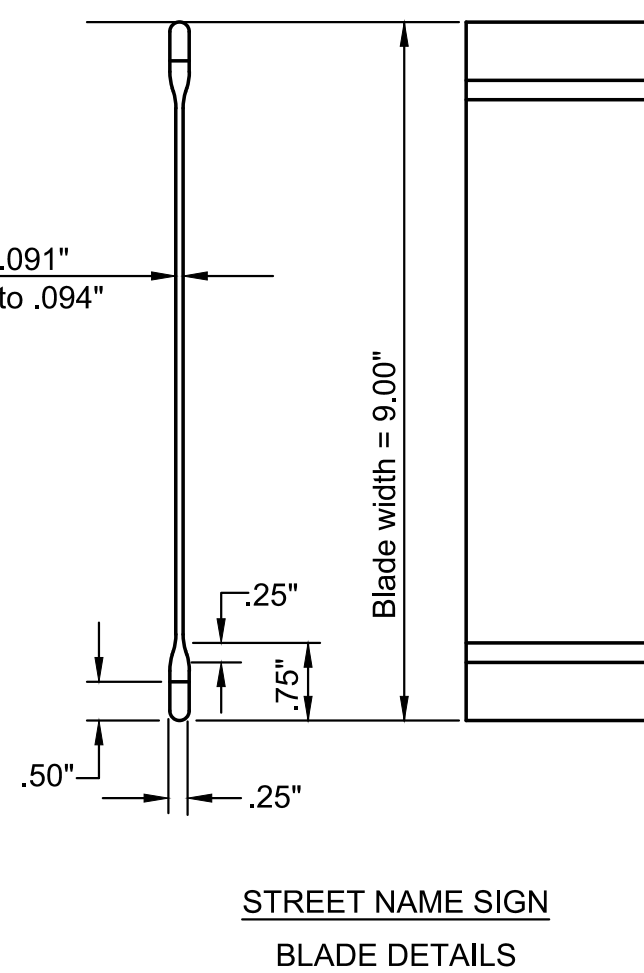



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

CURB INLET PAVEMENT UNDERDRAIN DETAIL		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER 472-2022-085820B	OCA NUMBER 47470122	DATE SEPT. 2023
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 08 OF 51

[illegible]

<i>STREET NAME</i>	NO. BLADES REQ'D	
	9" STD.	9" METRO
PORTWEST	1	
DRIFTWOOD	5	
COBBLESTONE	1	
PORTWEST CIR		1
HARBORSIDE	1	
SANDKEY	1	
TOTAL	10	

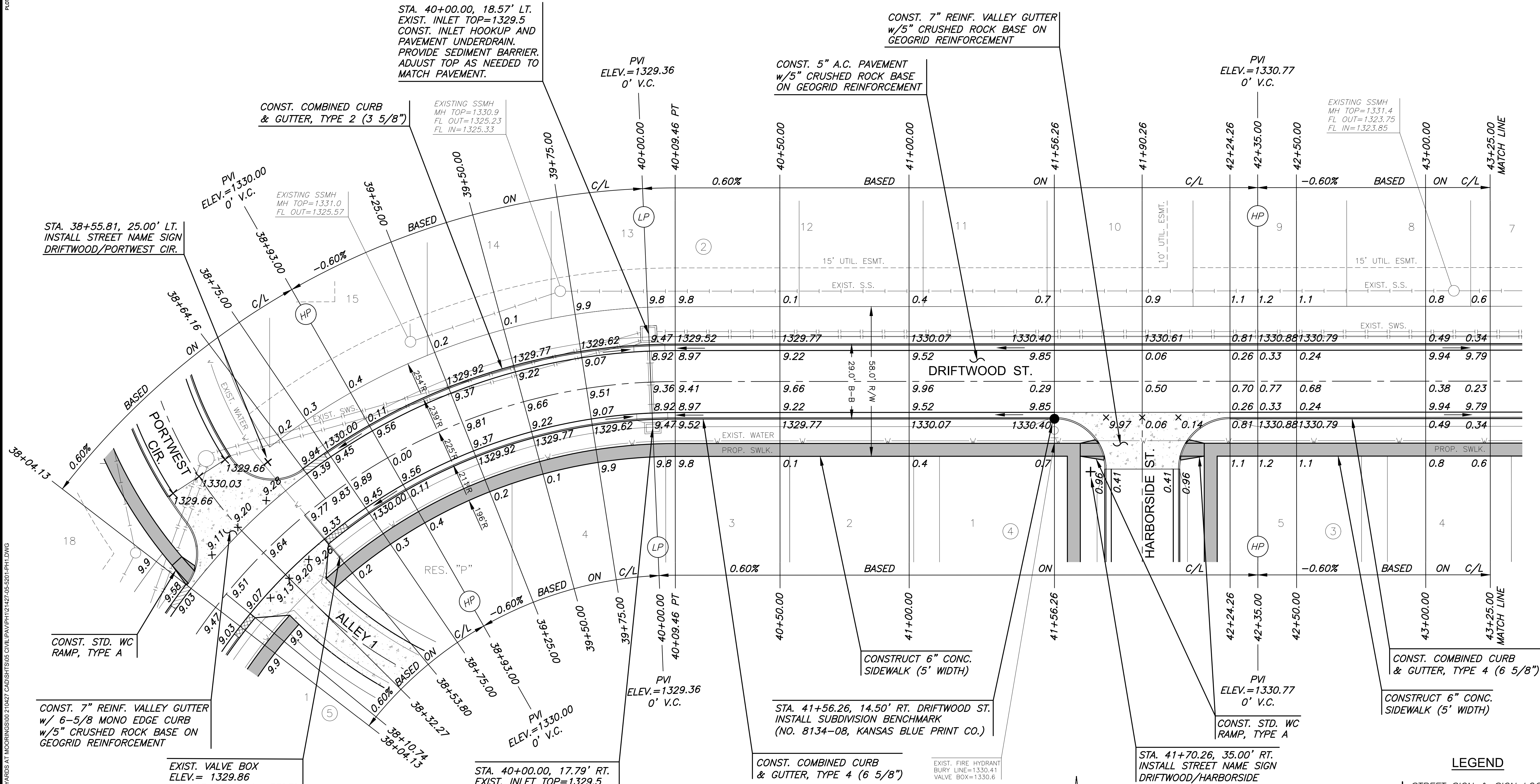


REVISED: December 2018 TM	Updated Edition year and Subsection Number in Notes
	<h1 style="margin: 0;">SIGN DETAILS</h1>
TRAFFIC ENGINEER	
PROJECT NUMBER	OCA NUMBER
472-2022-085820B	###
DATE	
SEPT. 2023	
CITY ENGINEER'S OFFICE	
CITY HALL - SEVENTH FLOOR	
455 NORTH MAIN STREET	
WICHITA, KANSAS 67202-1620	
(316) 268-4501	
SHEET	
09 OF 51	

J:\PROJECTS\2022\12101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SHITS\05 CIVIL\PAVPH121427-05-4201-PH1.DWG
PLOTTED: Thursday, September 14, 2023 @ 02:09PM

SUBDIVISION BENCH MARKS			
NO.	STREET AND STATION	FROM CL	DESCRIPTION
1	DRIFTWOOD ST., STA. 41+56.26	14.5' RT	TOP OF CURB AT FIRE HYDRANT
ELEVATIONS			

NOTE: CONTRACTOR SHALL INSTALL SUBDIVISION BENCH MARKS ("FLAT SURVEY MARKERS NO. 8134-08 3" TOP DIAMETER" PROVIDED BY KANSAS BLUE PRINT CO., INC.) COSTS TO BE CONSIDERED SUBSIDIARY TO CONSTRUCTION OF COMBINED CURB AND GUTTER.



- LEGEND**
- STREET SIGN & SIGN LOCATION (SEE SIGN ASSEMBLY TABLE FOR TYPE & STATION OF SIGN)
 - 10' TRANSITION TO LIP CURB
 - 10' TRANSITION TO TYPE 2 CURB

NOTE:
TOP OF CURB ELEVATIONS SHOWN
ARE FOR FULL CURB. CONSTRUCT
CURB AND GUTTER AS NOTED.



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

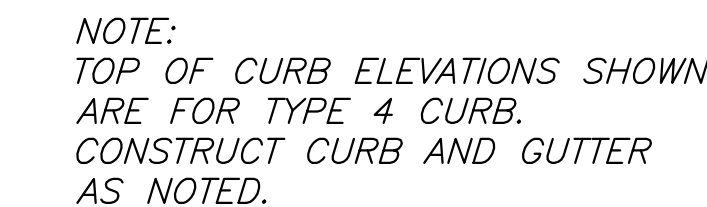
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DRIFTWOOD ST.

PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

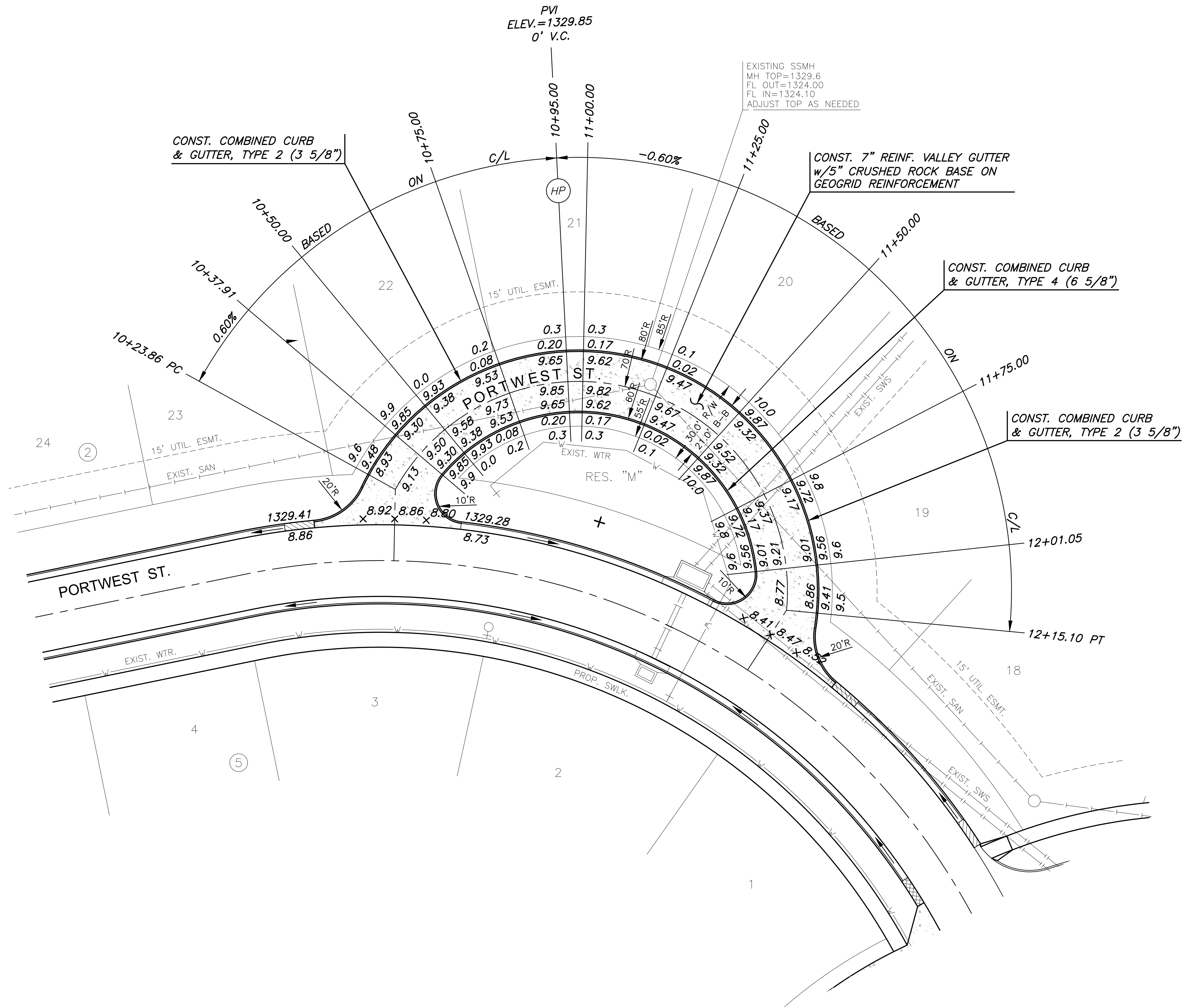
NO.	REVISION	DATE

SHEET NO.



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PLOTTED: Thursday, September 14, 2023 2:02:09PM

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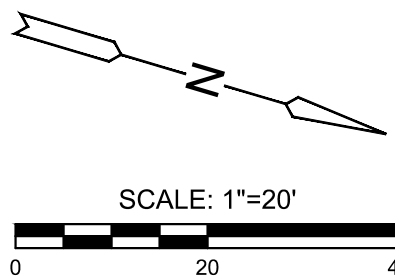
LEGEND

✚ STREET SIGN & SIGN LOCATION
(SEE SIGN ASSEMBLY TABLE
FOR TYPE & STATION OF SIGN)

10' TRANSITION TO LIP CURB

10' TRANSITION TO TYPE 2 CURB

NOTE:
TOP OF CURB ELEVATIONS SHOWN
ARE FOR TYPE 4 CURB.
CONSTRUCT CURB AND GUTTER
AS NOTED.



PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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PORTWEST ST.
BULB

PROJECT NO. 472-2022-085820B

DATE SEPT. 2023

SCALE 1" = 20'

DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

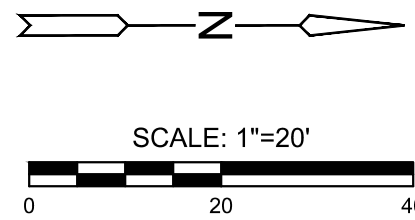
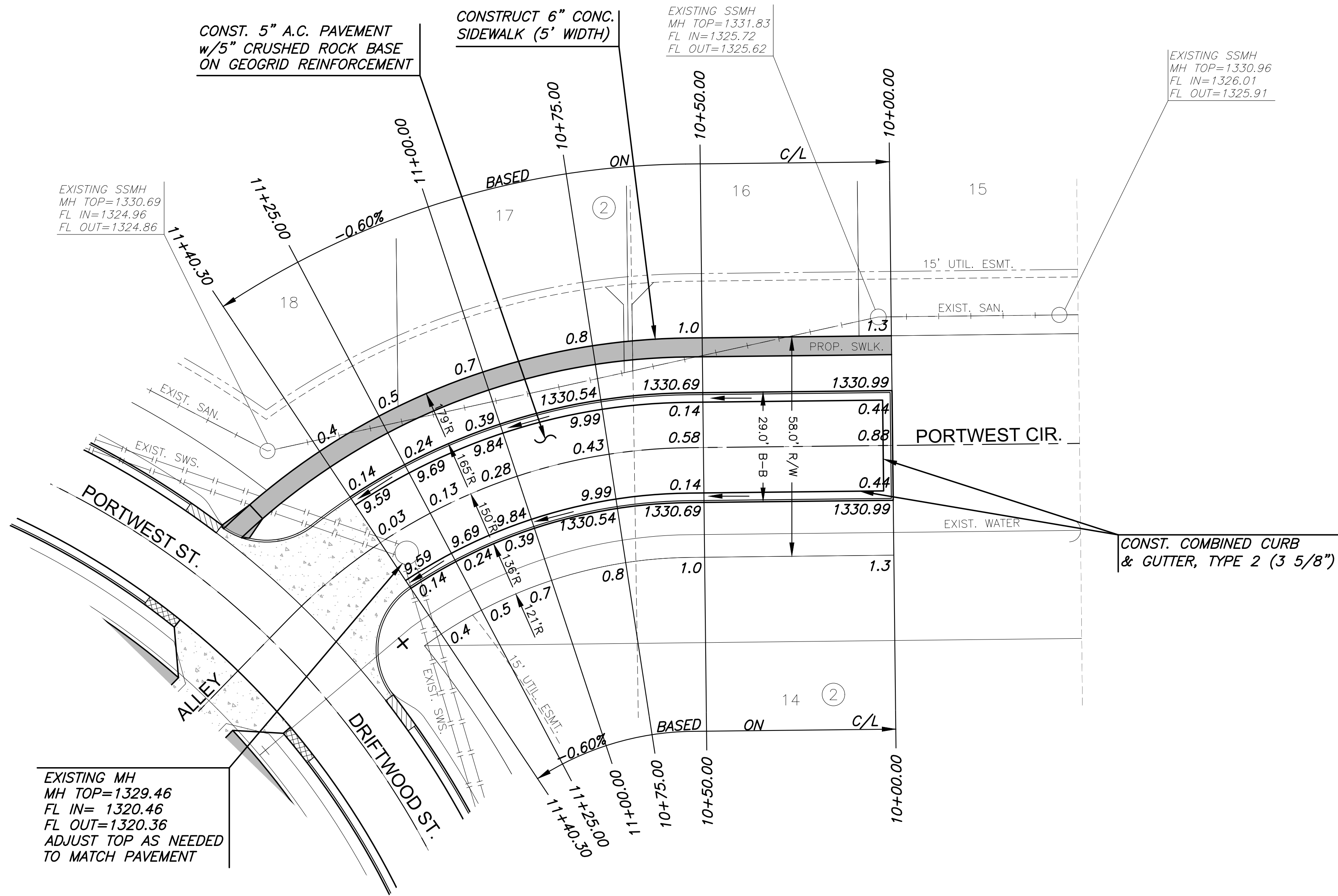
NO.	REVISION	DATE

SHEET NO.

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PLOTTED: Thursday, September 14, 2023 @ 02:13PM

SUBDIVISION BENCH MARKS				
NO.	STREET AND STATION	FROM CL	DESCRIPTION	ELEVATIONS
1	DRIFTWOOD ST., STA. 41+56.26	14.5' RT	TOP OF CURB AT FIRE HYDRANT	

NOTE: CONTRACTOR SHALL INSTALL SUBDIVISION BENCH MARKS ("FLAT SURVEY MARKERS NO. 8134-08 3" TOP DIAMETER" PROVIDED BY KANSAS BLUE PRINT CO., INC.) COSTS TO BE CONSIDERED SUBSIDIARY TO CONSTRUCTION OF COMBINED CURB AND GUTTER.



- LEGEND**
- ✚ STREET SIGN & SIGN LOCATION
(SEE SIGN ASSEMBLY TABLE
FOR TYPE & STATION OF SIGN)
 - 10' TRANSITION TO LIP CURB
 - 10' TRANSITION TO TYPE 2 CURB

NOTE:
TOP OF CURB ELEVATIONS SHOWN
ARE FOR FULL CURB. CONSTRUCT
CURB AND GUTTER AS NOTED.



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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PORTWEST CIR.

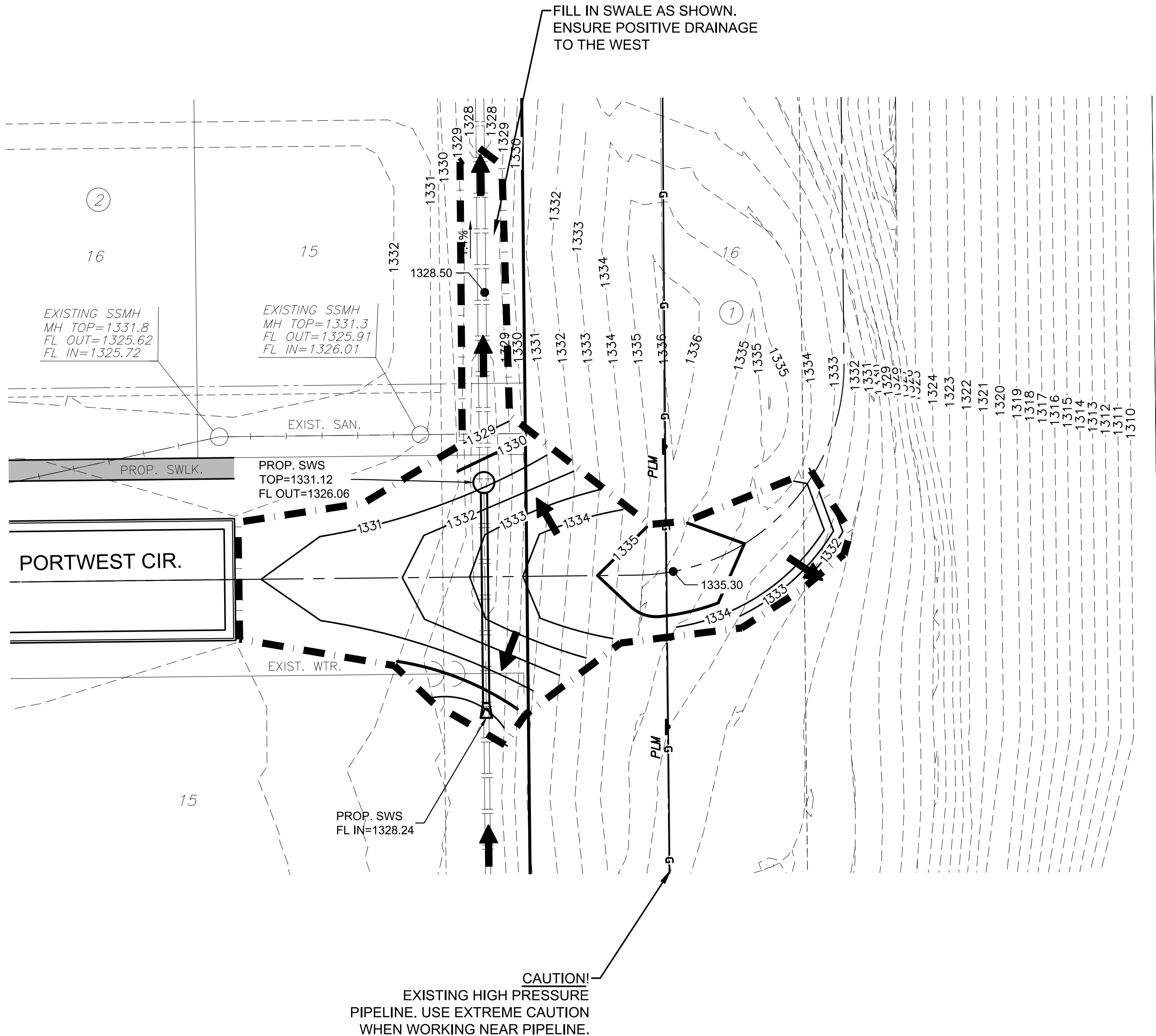
PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.
16 OF 51

GENERAL GRADING NOTES:

1. THIS IS DESIGN GRADING. ALL GRADES SHALL BE CONTOURED SMOOTHLY WITH GENTLE ROUNDING/SHAPING OF ALL AFFECTED LAND SURFACES. ABRUPT TRANSITIONS AT THE TOP OF SLOPES WHERE PROPOSED GRADES MEET EXISTING ARE NOT ACCEPTABLE. NOT ALL SLOPES ARE CONSTANT AND THEREFORE THE GRADING PLANS SHALL BE REFERRED TO FOR FINAL GRADE SHAPING. THE GRADING SHALL BE APPROVED BY MKEC'S LANDSCAPE ARCHITECT PRIOR TO THE ADDITION OF THE TOPSOIL LAYER.
2. EXISTING NATURAL AREAS (TREES & PASTURE) OUTSIDE OF THE NOTED LIMITS OF GRADING SHALL BE PRESERVED & SHALL BE OFF LIMITS TO ANY TYPE OF CONSTRUCTION ACTIVITY. TEMPORARY CONSTRUCTION FENCE SHALL BE ERECTED AROUND NOTED LOCATIONS PRIOR TO THE START OF CONSTRUCTION.
3. AS THE PROJECT NEARS COMPLETION, THE CONTRACTOR SHALL RIP (SCARIFY) ALL HAUL ROADS WITH AN AGRICULTURAL IMPLEMENT INTENDED FOR SUCH PURPOSES TO A DEPTH OF 18". MULTIPLE PASSES MAY BE NECESSARY TO THOROUGHLY ALLEVIATE COMPACTION.
4. EARTHWORK COMPACTION SHALL BE AT 95% STANDARD DENSITY. TESTING SHALL BE SUBSIDIARY TO PROJECT.



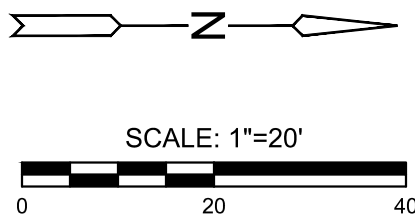
LEGEND

- 1250 --- EXISTING MAJOR CONTOUR
- 1250 --- EXISTING MINOR CONTOUR
- 1250 — PROPOSED MAJOR CONTOUR
- 1250 — PROPOSED MINOR CONTOUR
- — — EXISTING WATERLINE
- — — EXISTING SANITARY SEWER
- — — EXISTING STORMWATER SEWER
- — — PROPOSED STORMWATER SEWER
- --- LIMITS OF GRADING
- 1329.86 SPOT ELEVATIONS
- ← FLOW ARROW

EARTHWORK SUMMARY

EXCAVATION (CONT FURN.) (ON SITE) 400 C.Y.
COMPACTED FILL (95%) 400 C.Y.

CONTRACTOR TO COORDINATE WITH WITH ENGINEERS
REPRESENTATIVE FOR LOCATION OF ON-SITE SPOILS



PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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PORTWEST CIR.
GRADING

PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.



PAVING & INCIDENTAL DRAINAGE PLANS FOR **COURTYARDS AT THE MOORINGS** PHASE 1 - PART B

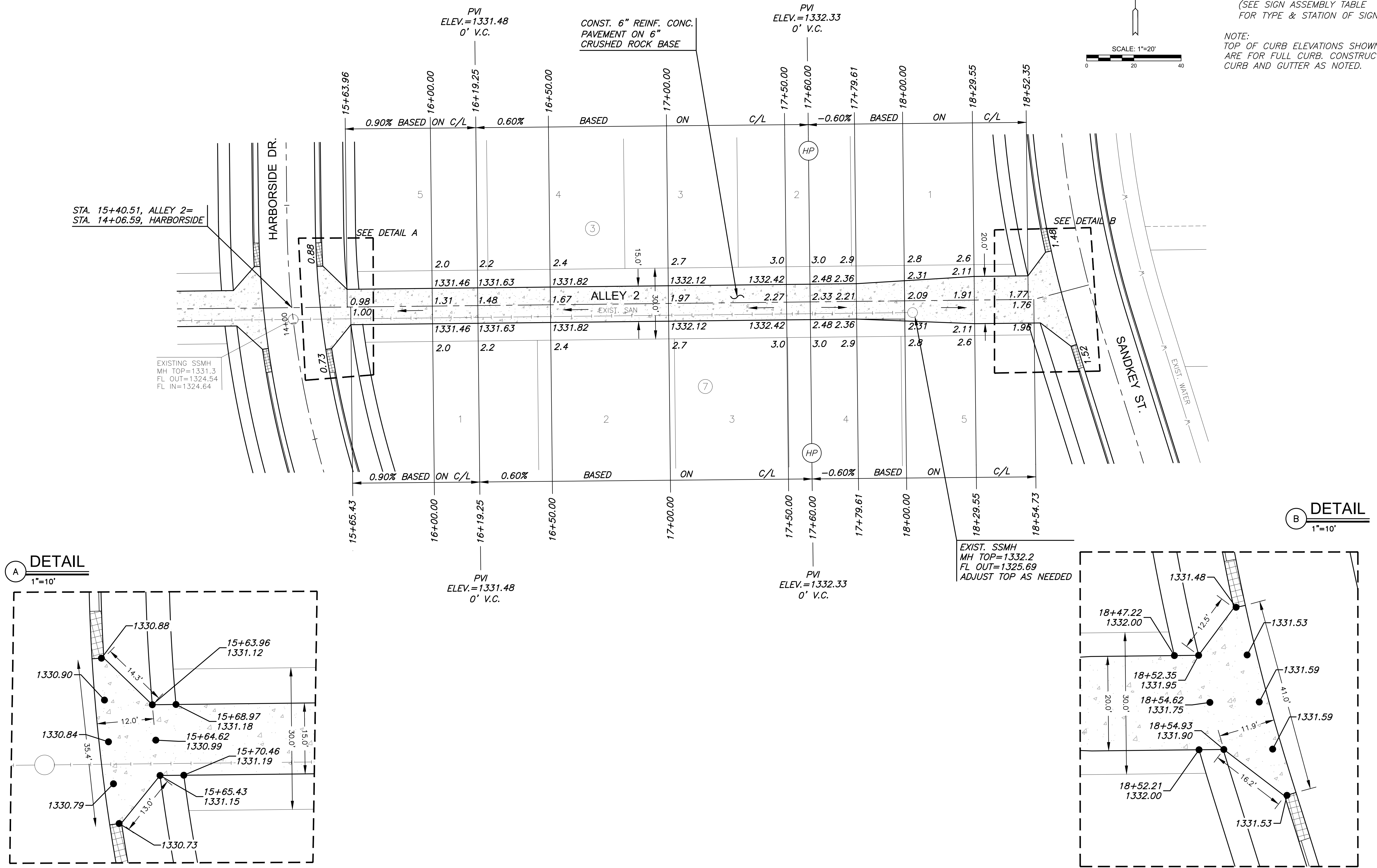
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ALLEY 1			
PROJECT NO.		472-2022-085820B	
DATE		SEPT. 2023	
SCALE		1" = 20'	
DESIGNED	DRAWN	CHECKED	
DFL	JWC	SPE	
NO.	REVISION	DATE	
SHEET NO.			
18 OF 51			

J:\PROJECTS\2021\101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SHOTS\05 CIVIL\PAVPH121427-05-5208-PH1.DWG
PLOTTED: Thursday, September 14, 2023 @ 02:17PM

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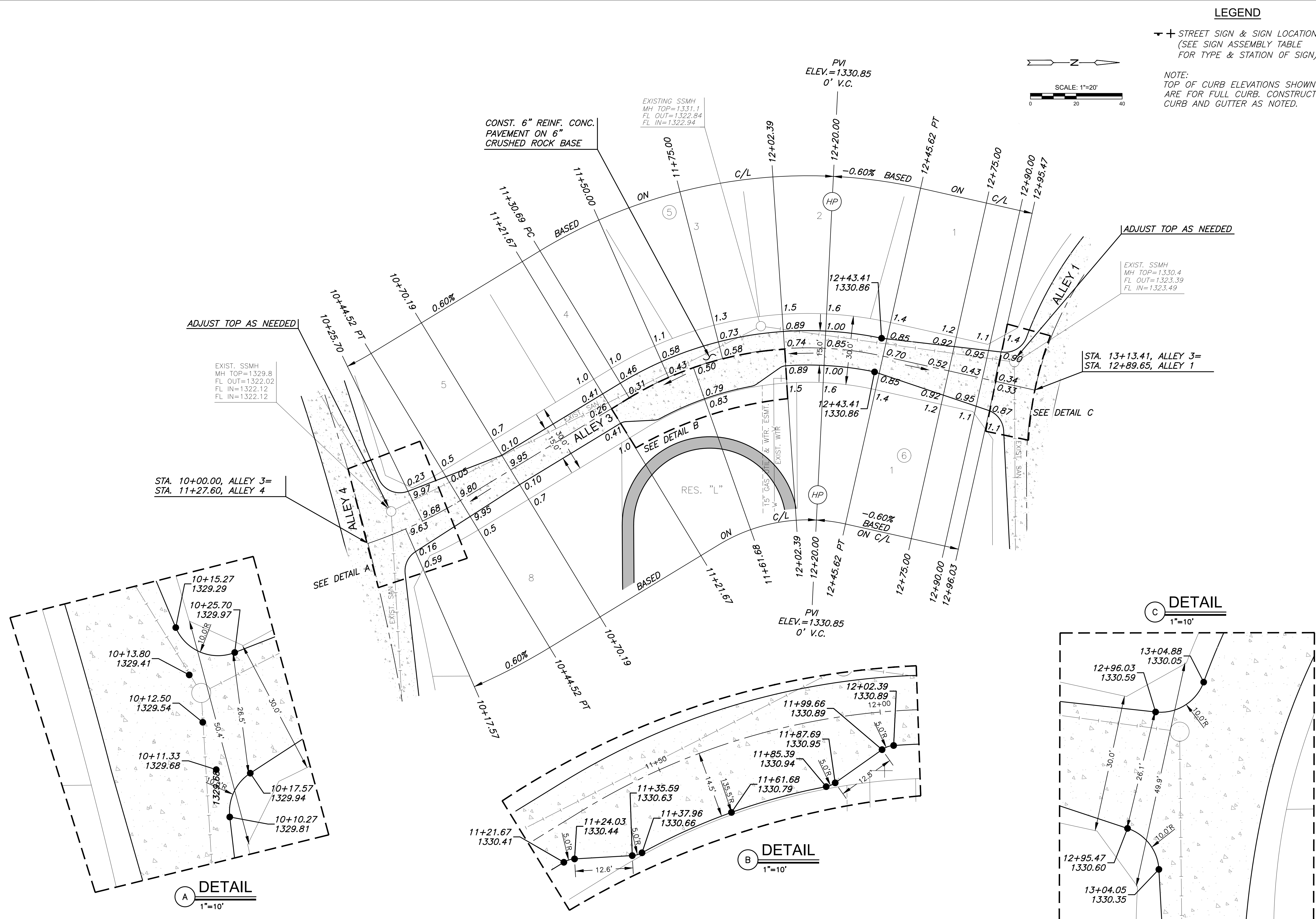
PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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ALLEY 2		
PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE
NO.	REVISION	DATE
SHEET NO.		
19 OF 51		



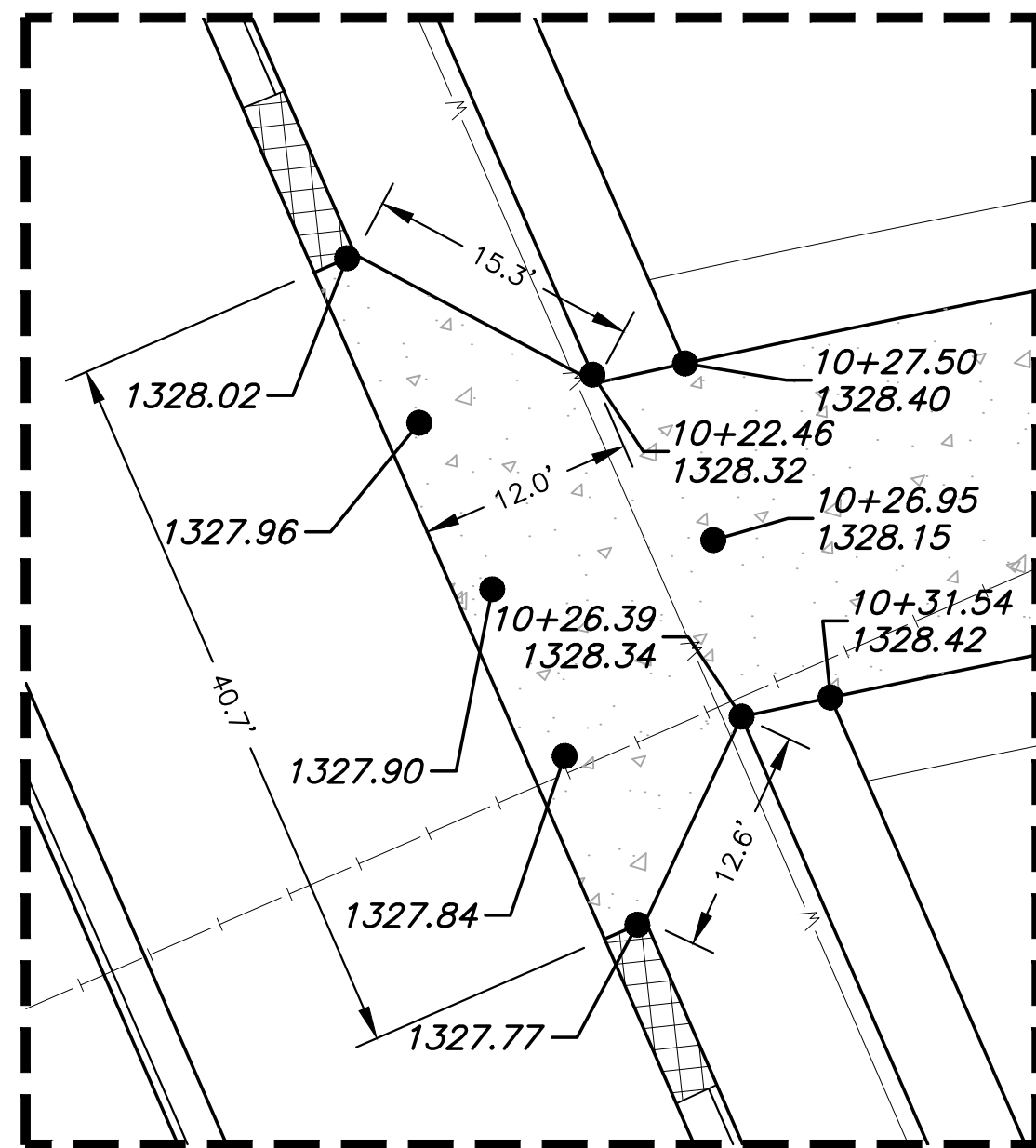
PAVING & INCIDENTAL DRAINAGE PLANS FOR COURTYARDS AT THE MOORINGS PHASE 1 - PART B

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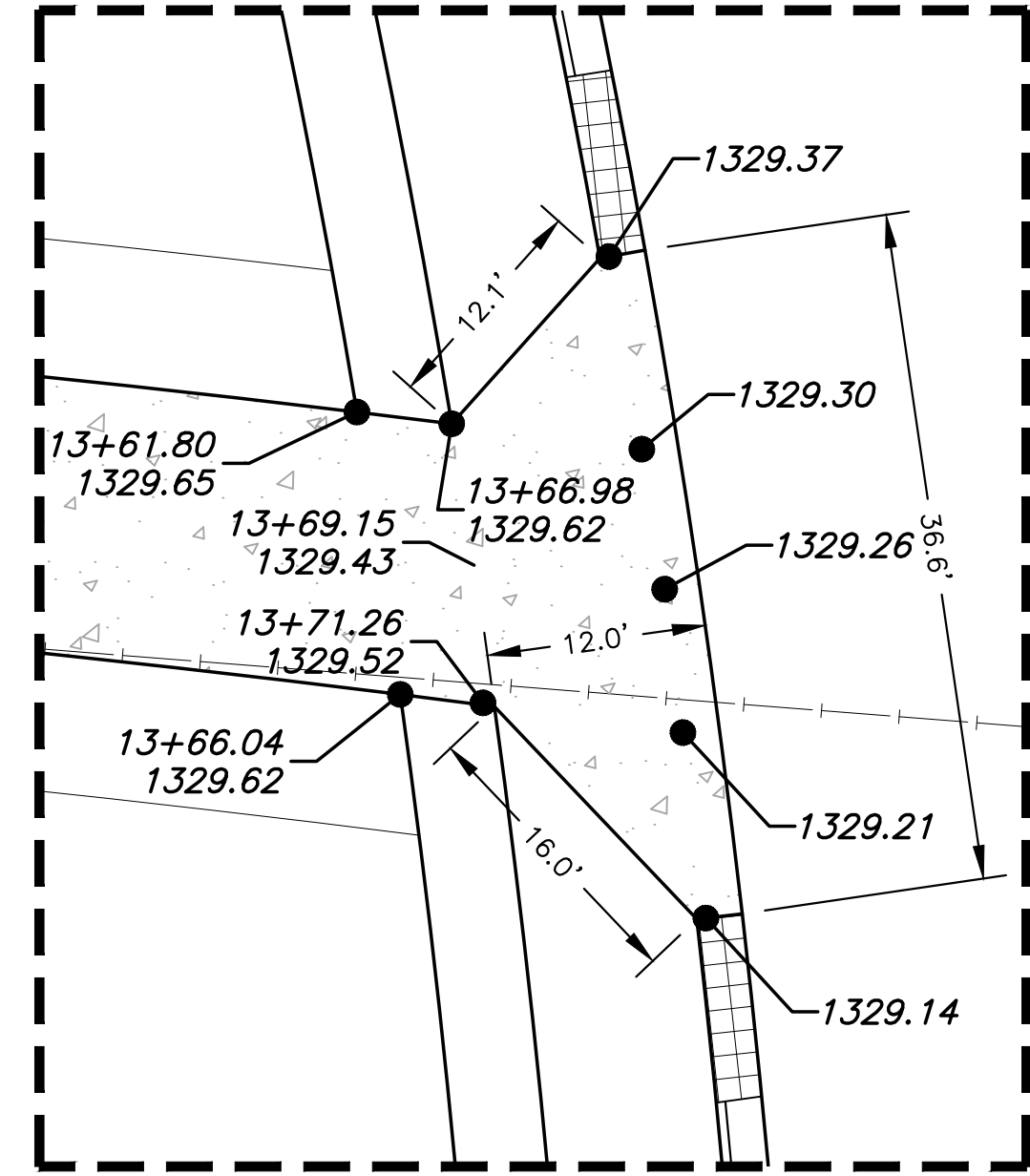
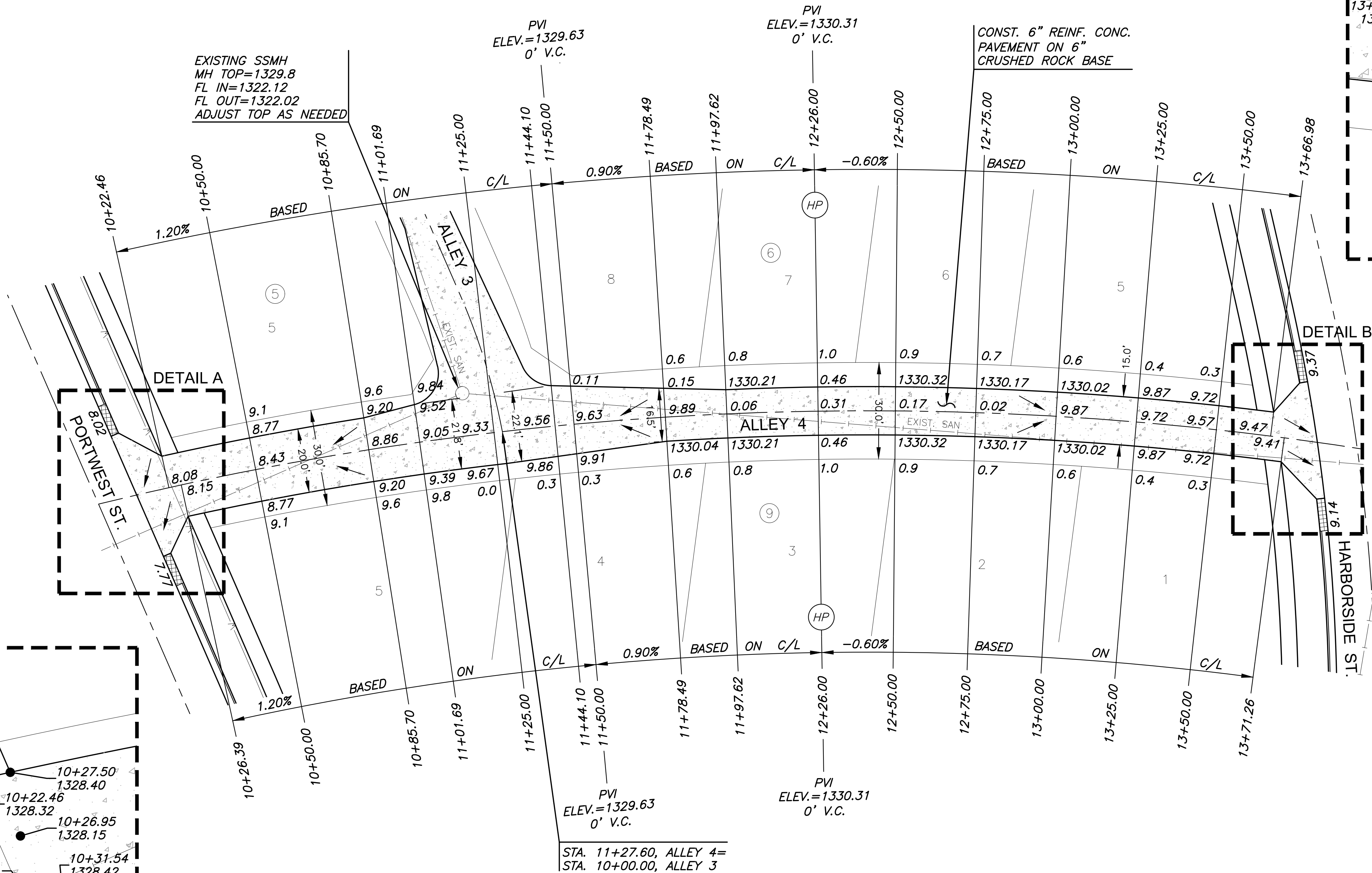
ALLEY 3		
PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE
NO.	REVISION	DATE
SHEET NO.		
20 OF 51		

PLOTTED: Thursday, September 14, 2023 @ 02:19PM

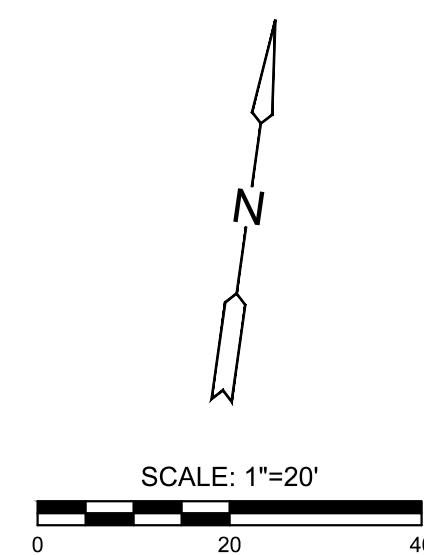
J:\PROJECTS\2021\101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SHTS\05 CIVIL\PAVPH121427-05-5208-PH1.DWG



A **DETAIL**
SCALE: 1" = 10'



B **DETAIL**
SCALE: 1" = 10'



LEGEND
+ STREET SIGN & SIGN LOCATION
(SEE SIGN ASSEMBLY TABLE
FOR TYPE & STATION OF SIGN)

NOTE:
TOP OF CURB ELEVATIONS SHOWN
ARE FOR FULL CURB. CONSTRUCT
CURB AND GUTTER AS NOTED.

PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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ALLEY 4

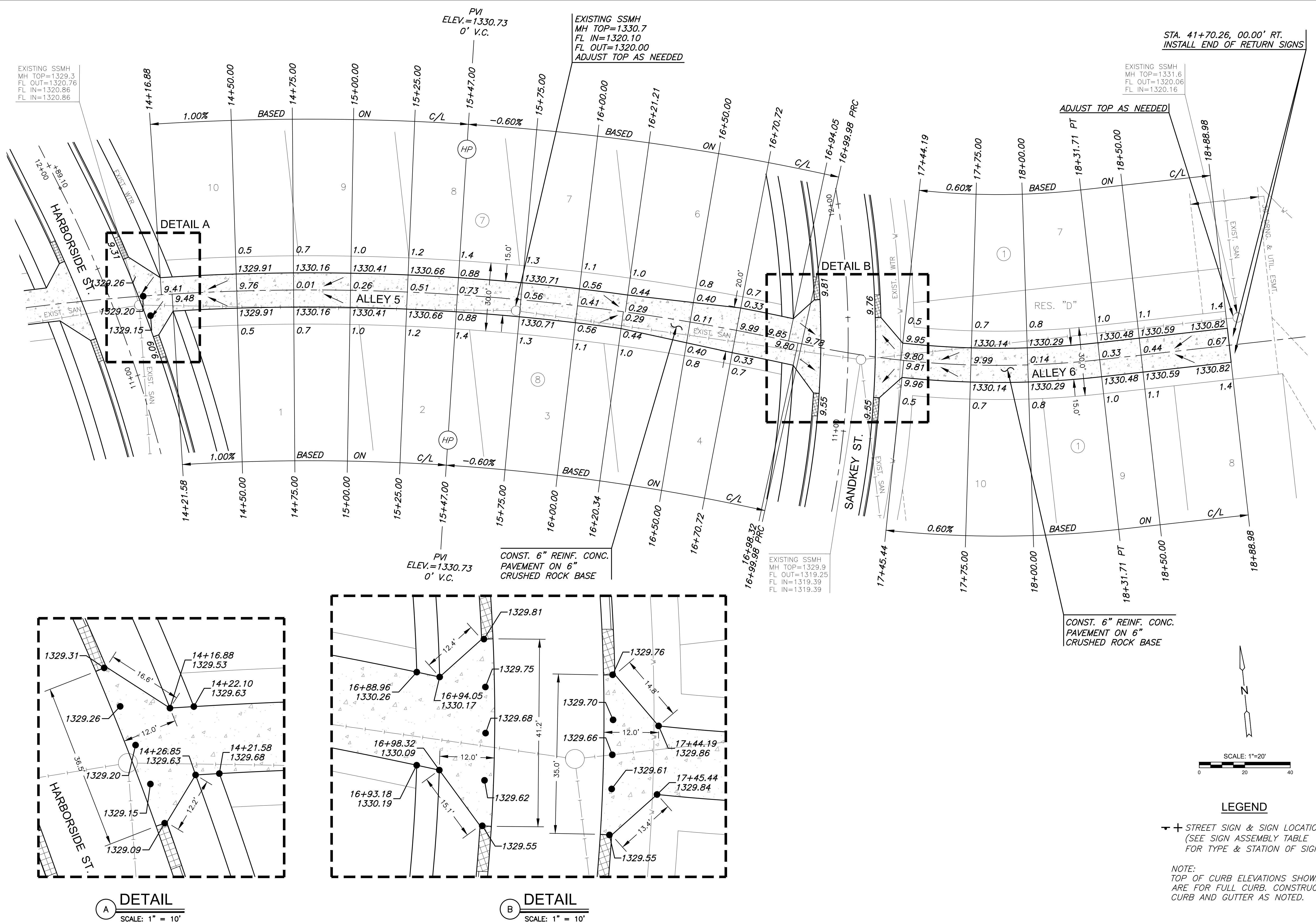
PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	1"=20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.

PLOTTED: Thursday, September 14, 2023 @ 02:20PM

J:\PROJECTS\2022\12101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SHOTS\05 CIVIL\PAVPH121427-05-2023-PH1.DWG



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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ALLEYS 5 & 6

PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	1" = 20'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

NO.	REVISION	DATE



NOTE:
TOP OF CURB ELEVATIONS SHOWN
ARE FOR FULL CURB. CONSTRUCT
CURB AND GUTTER AS NOTED.



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PLOTTED: Thursday, September 14, 2023 @ 02:22PM

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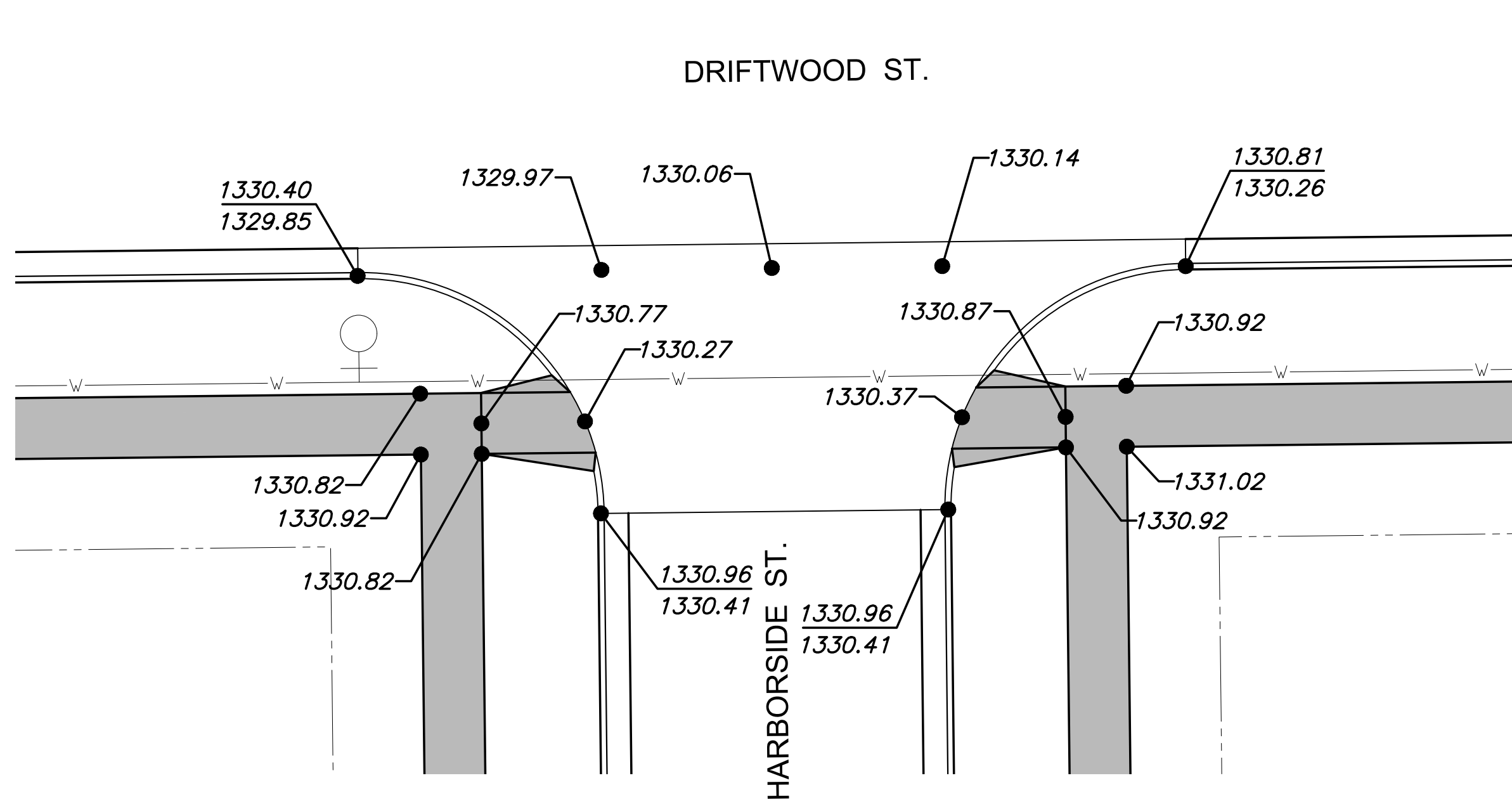
PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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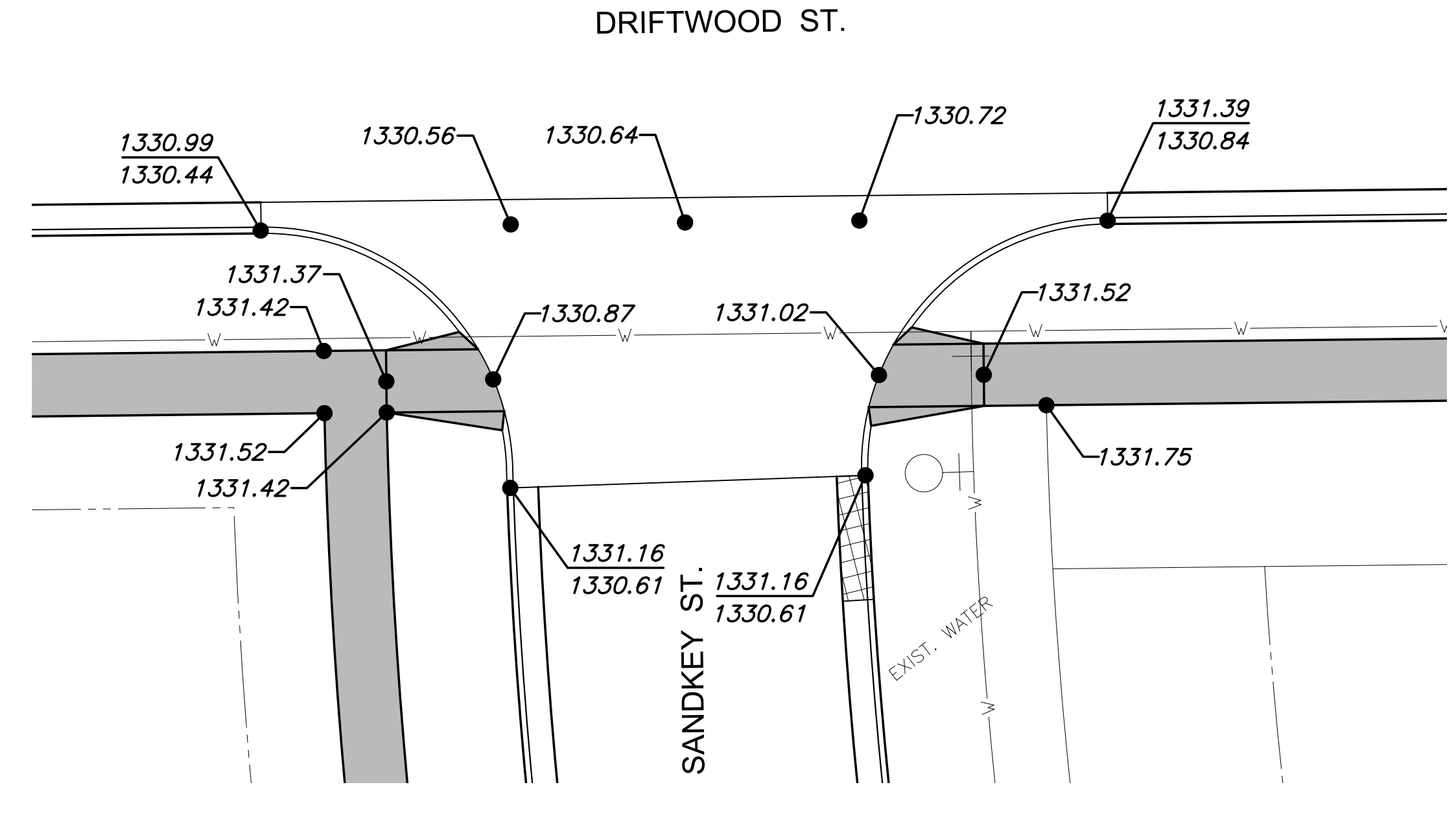
**INTERSECTION
DETAILS**

PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	1" = 10'	
DESIGNED	DRAWN	CHECKED
DFL	ABC	SPE

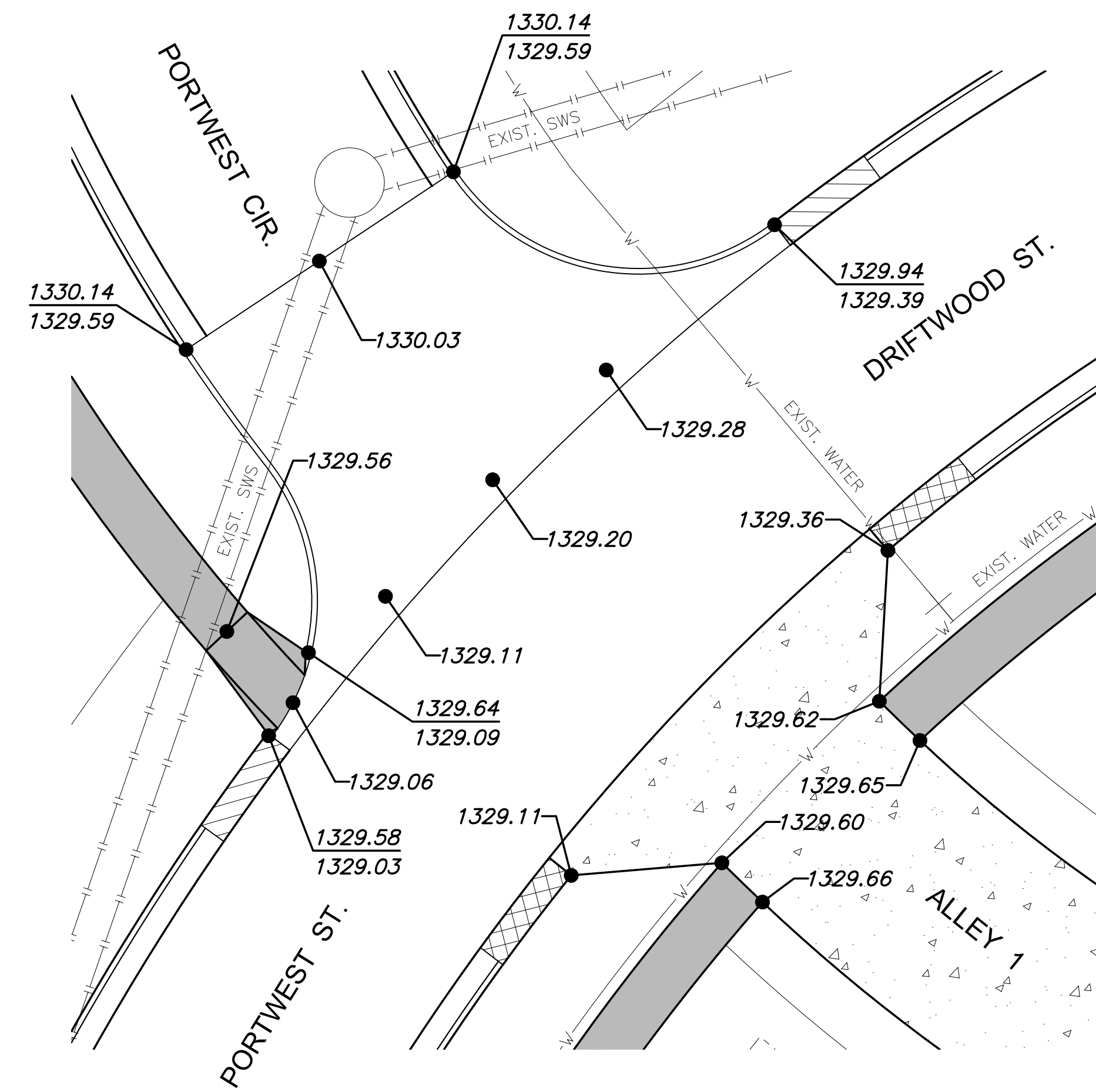
NO.	REVISION	DATE



A DETAIL
1"=10'



B DETAIL
1"=10'

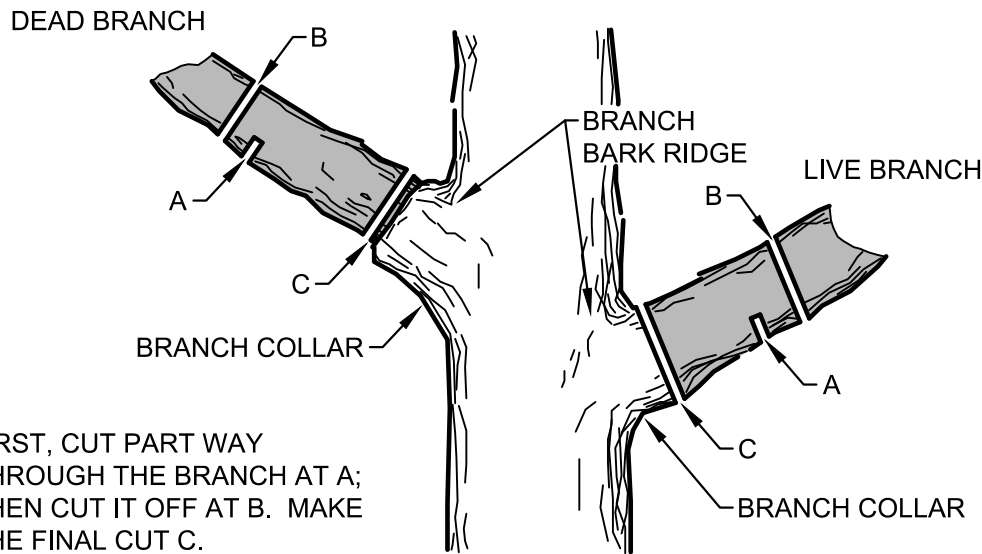


C DETAIL
1"=10'

J:\PROJECTS\2021\101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SHTS\05 CIVIL\PAVPH1121427-05-4500-PH1.DWG
PLOTTED: Thursday, September 14, 2023 @ 02:24PM

GENERAL TREE & NATURAL
AREA PROTECTION NOTES:

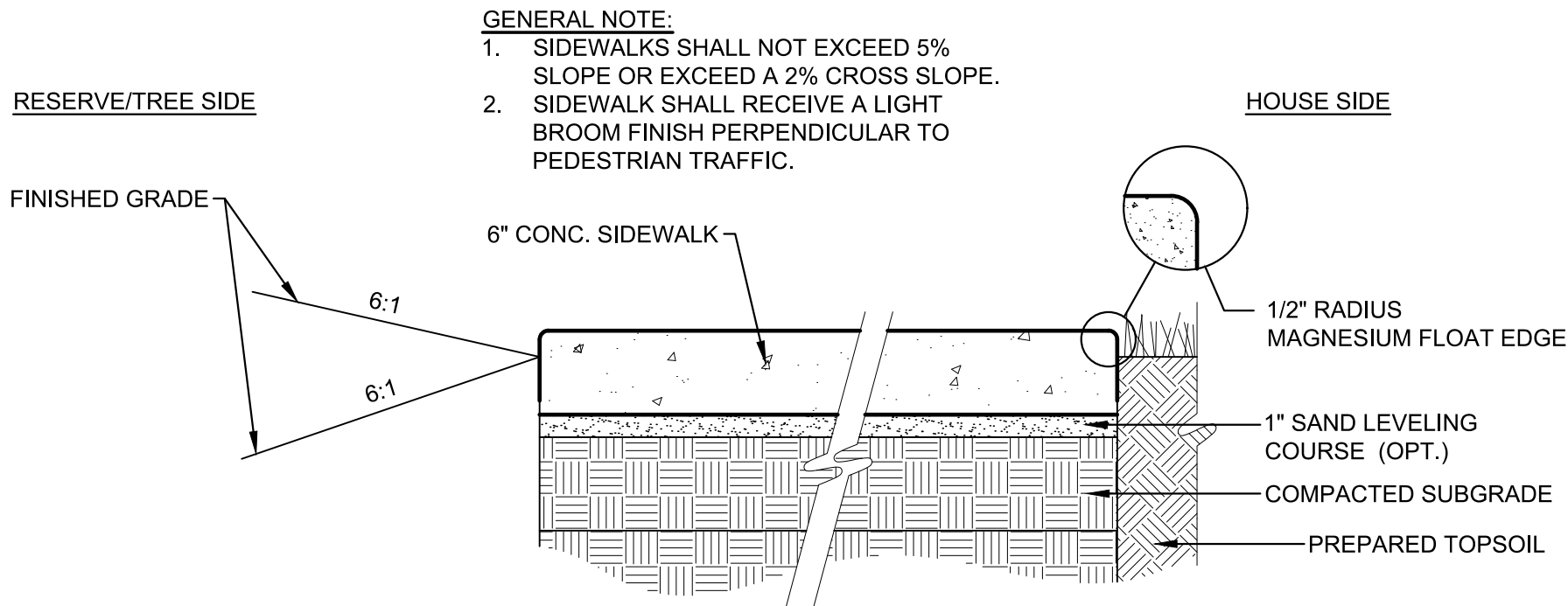
1. TREES AND NATIVE PRAIRIE PASTURE ARE AN IMPORTANT ASSET TO THE DEVELOPER. EVERY EFFORT SHOULD BE MADE TO PRESERVE EACH AND EVERY TREE AND PASTURE SPACE (UNLESS NOTED FOR REMOVAL) AS DIRECTED IN THESE PLANS AND NOTES.
2. THE NATIVE PRAIRIE PASTURE SHALL BE IMPACTED AS LITTLE AS POSSIBLE. CONSTRUCTION EQUIPMENT & STORED MATERIALS SHALL REMAIN OUT OF AREAS NOTED TO BE PRESERVED. CONSTRUCTION TRAFFIC SHALL BE LIMITED TO A CONFINED SPACE WITHIN THE STREET R.O.W. OR IMMEDIATE CONSTRUCTION AREA. VEHICLES SHALL NOT RANDOMLY CROSS PASTURE SPACES BUT RATHER STAY ON PREDEFINED ROUTES/ HAUL ROADS THAT THE OWNERS REPRESENTATIVE AGREES TO. IT IS NOT THE INTENTION OF THESE REQUIREMENTS TO MAKE THE PROJECT MORE DIFFICULT BUT RATHER TO THOUGHTFULLY PLAN HOW TO GET THE PROJECT BUILT WITHOUT DESTROYING THE AESTHETIC QUALITIES OF THE PROPERTY.
3. EQUIPMENT, CONSTRUCTION MATERIALS AND PEOPLE SHALL REMAIN OUT OF AND AWAY FROM TREE DIPLINES SO AS TO NOT COMPACT THE ROOT ZONE OR DAMAGE THE TREES. CHEMICAL SPILL DAMAGE SHALL BE PREVENTED BY FILLING GAS TANKS, CLEANING TOOLS, AND REPAIRING EQUIPMENT WELL OUTSIDE TREE PROTECTED ROOT ZONES. MIXING TRUCKS SHALL BE RINSED OUT OFF SITE. WHERE IT OCCURS THAT A CONSTRUCTION ROUTE OR A PROPOSED IMPROVEMENT OCCURS WITHIN A TREE'S PROTECTED ROOT ZONE, IT MAY BE NECESSARY, DEPENDENT UPON THE TYPE OF CONSTRUCTION AND EQUIPMENT USED, TO INSTALL A ROOT PROTECTION BRIDGE OR APPROVED EQUIVALENT. THIS MAY BE DETERMINED IN THE FIELD AT THE DIRECTION OF THE OWNER'S REPRESENTATIVE (REFER TO DETAIL THIS SHEET). DAMAGED TREES THAT ARE NOTED FOR PRESERVATION SHALL BE REPLACED OR THE DAMAGES AMENDED BY THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE WRITTEN RECOMMENDATIONS FROM A CERTIFIED ARBORIST TO THE OWNER'S REPRESENTATIVE AS TO HOW TO AMEND DAMAGED TREES AND ROOT ZONES.
4. ANY TREE THAT MUST HAVE BRANCHES REMOVED SHALL BE TRIMMED WITH SHARP INSTRUMENT/ TOOL THAT IS INTENDED FOR SUCH OPERATIONS. CONSULT LANDSCAPE ARCHITECT PRIOR TO TRIMMING. KNOCKING BRANCHES OFF WITH A BACKHOE OR OTHER SIMILAR MACHINE IS NOT ACCEPTABLE! REFER TO TREE TRIMMING DETAIL ON THIS SHEET FOR TRIMMING PROCEDURE.



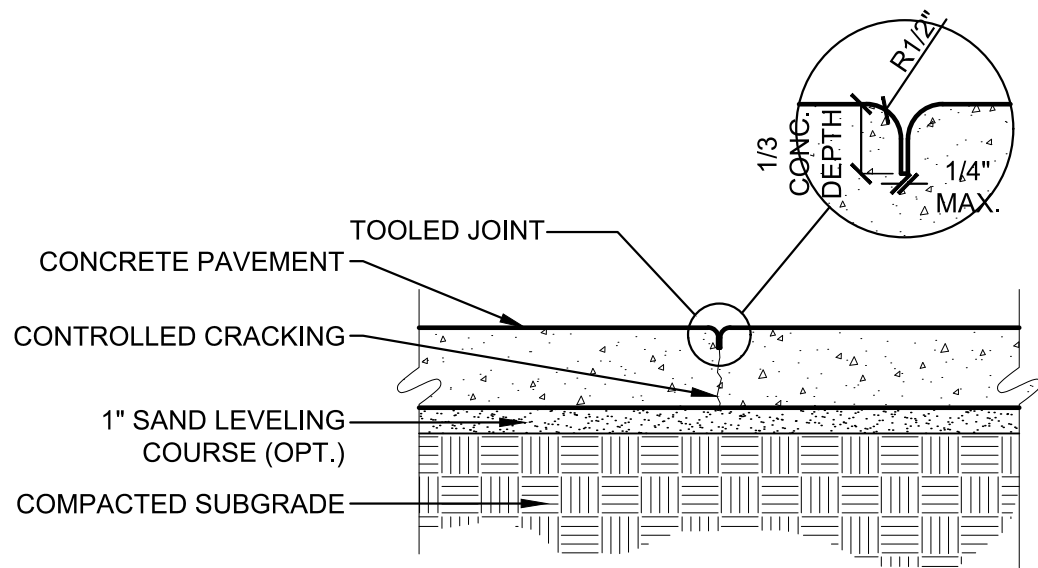
TREE TRIMMING NOTES:

1. NOTIFY MKEC PROJECT ENGINEER PRIOR TO COMMENCEMENT OF EASEMENT CLEARING & GRUBBING OPERATIONS. CONTRACTOR SHALL HAVE LIMITS OF REMOVAL DEFINED WITH FLAG & LATH. MKEC PROJECT ENGINEER WILL PROVIDE JUDGEMENT CALLS FOR TREES IN QUESTION. PROVIDE MKEC 24-HOUR NOTICE.
2. TREES MAY BE BURNED ON SITE. REGULATORY & PERMITTING APPROVALS SHALL BE SECURED BY CONTRACTOR.
3. TRIMMING OF BRANCHES SHALL NOT BE DONE WITH A BACKHOE!

1 TREE TRIMMING DETAIL
SCALE: NTS



2 6" CONCRETE SIDEWALK
SCALE: NTS



3 SIDEWALK TOOLED EDGE
SCALE: NTS

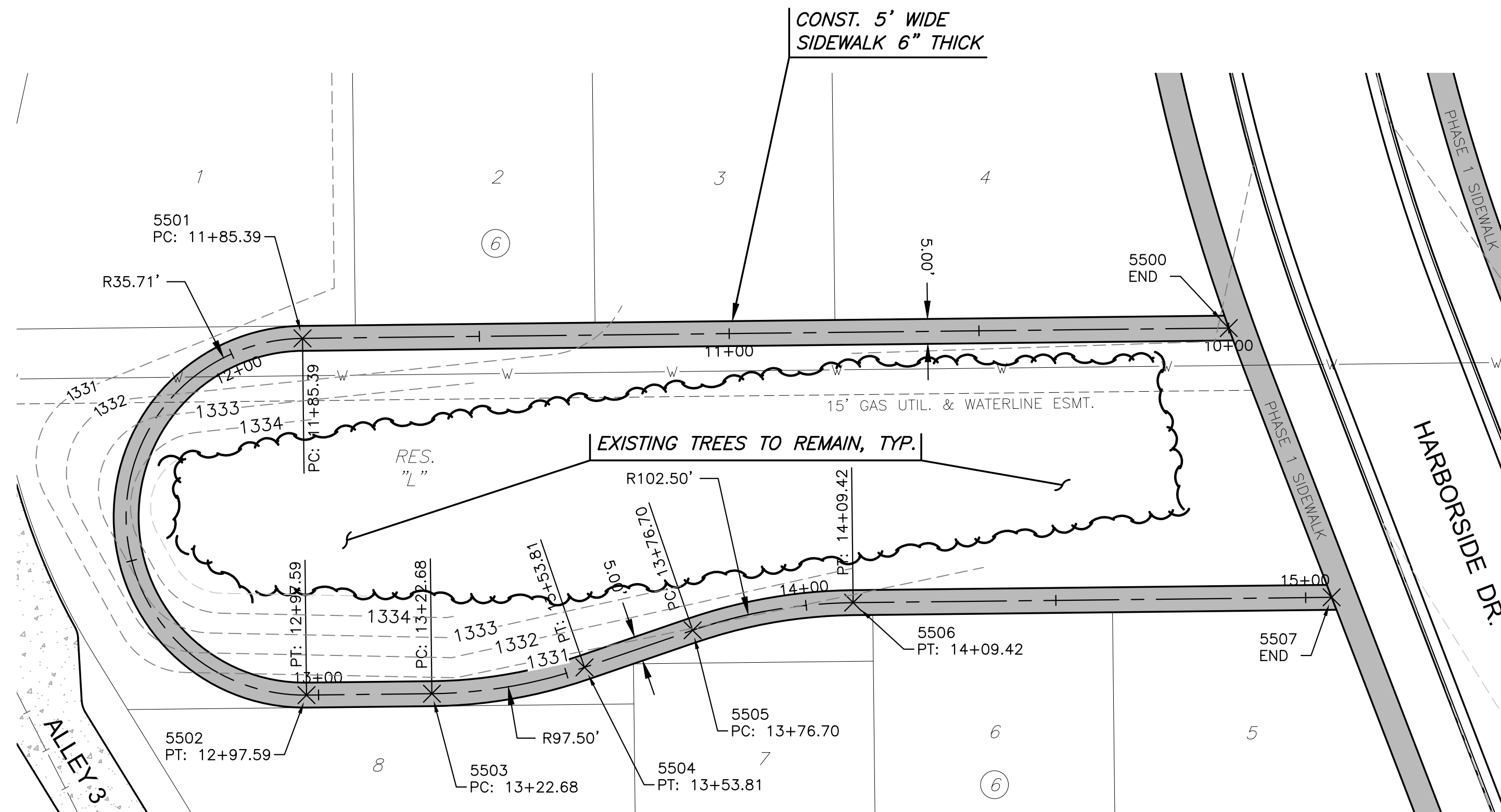
GRADING NOTES

PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	3/4" = 1'-0"	
DESIGNED	DRAWN	CHECKED

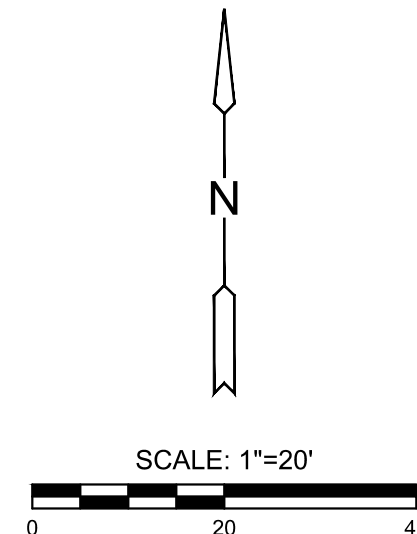
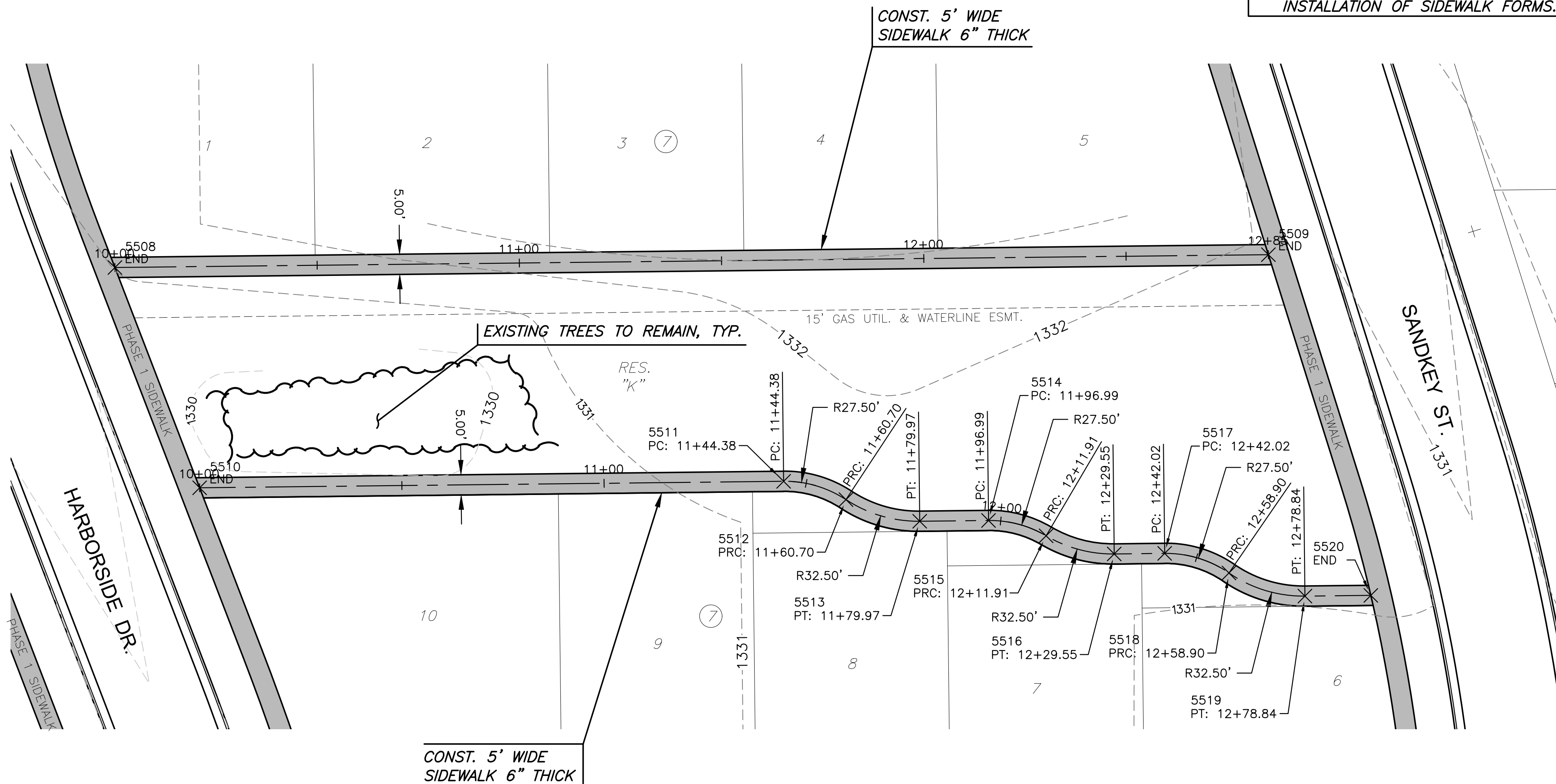
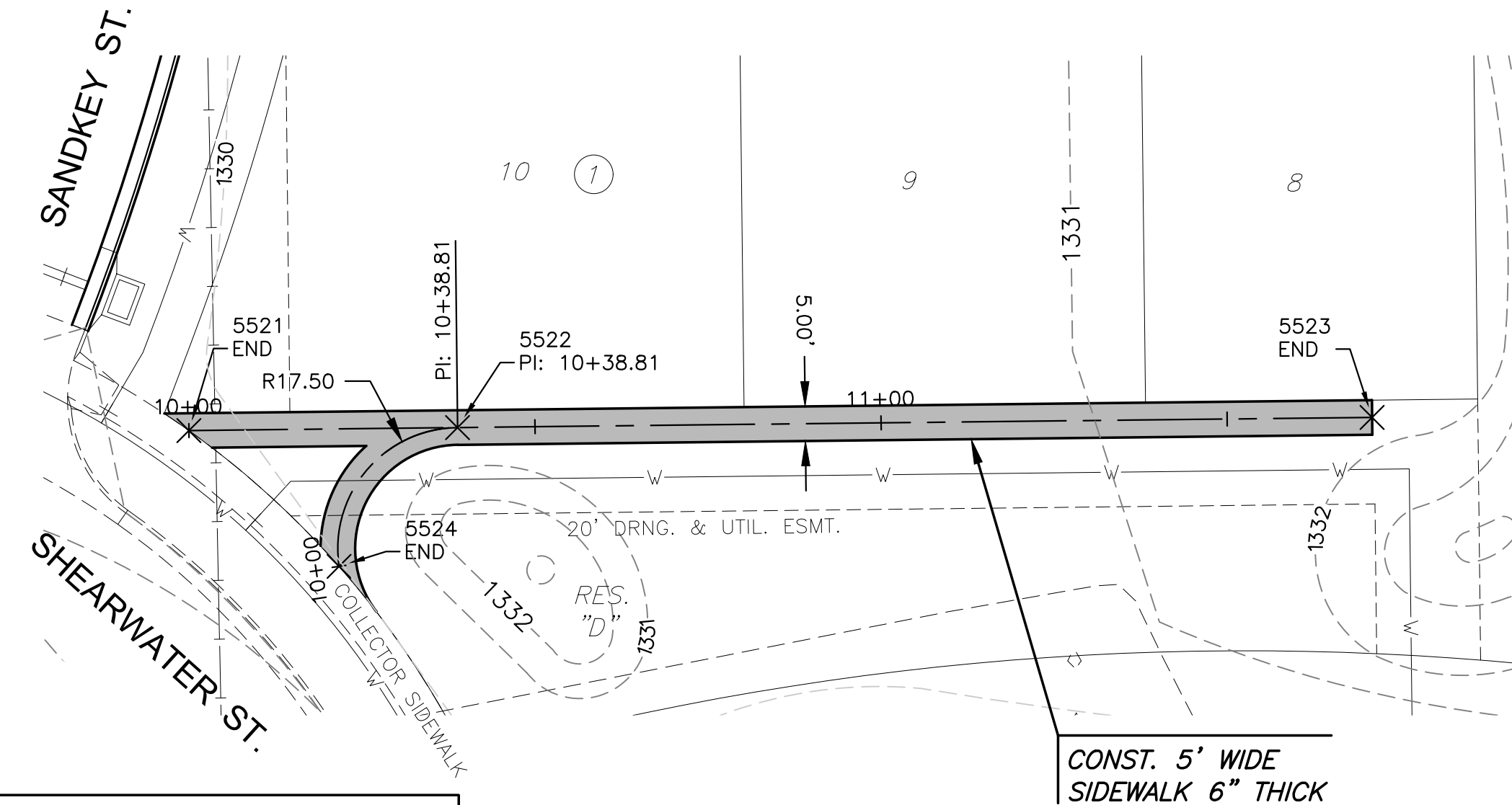
NO.	REVISION	DATE

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PLOTTED: Thursday, September 14, 2023 @ 02:24PM

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- NOTES:**
1. CONTRACTOR SHALL INSTALL SIDEWALK FORMS MIN. 1 WEEK PRIOR TO CASTING CONCRETE SIDEWALK.
 2. CONTRACTOR SHALL ACQUIRE MKEC'S LANDSCAPE ARCHITECT WRITTEN APPROVAL OF FORMWORK PRIOR TO SIDEWALK INSTALLATION.
 3. CONTRACTOR SHALL GIVE MKEC'S REPRESENTATIVE 2 DAYS NOTICE PRIOR TO INSTALLATION OF SIDEWALK FORMS.



PAVING POINTS				
Point #	Northing	Easting	Desc.	Elev.
5500	1714714.61	1637348.28	END	1331.12
5501	1714712.55	1637162.90	PC: 11+85.39	1331.90
5502	1714641.13	1637163.70	PT: 12+97.59	1330.97
5503	1714641.41	1637188.79	PC: 13+22.68	1331.01
5504	1714646.67	1637219.33	PT: 13+53.81	1331.04
5505	1714654.10	1637240.99	PC: 13+76.70	1331.05
5506	1714659.64	1637273.10	PT: 14+09.42	1331.05
5507	1714660.70	1637368.87	END	1330.78
5508	1714715.30	1637410.01	END	1331.00
5509	1714718.47	1637695.19	END	1331.88
5510	1714660.83	1637430.96	END	1330.65
5511	1714662.44	1637575.34	PC: 11+44.38	1331.77
5512	1714657.91	1637590.76	PRC: 11+60.70	1331.87
5513	1714652.56	1637608.98	PT: 11+79.97	1331.94
5514	1714652.75	1637626.00	PC: 11+96.99	1331.88
5515	1714648.96	1637640.24	PRC: 12+11.91	1331.80
5516	1714644.48	1637657.08	PT: 12+29.55	1331.72
5517	1714644.61	1637669.55	PC: 12+42.02	1331.67
5518	1714639.77	1637685.44	PRC: 12+58.90	1331.58
5519	1714634.05	1637704.23	PT: 12+78.84	1331.46
5520	1714634.23	1637720.52	END	1331.33
5521	1714386.01	1637760.34	END	1329.63
5522	1714386.44	1637799.15	PI: 10+38.81	1330.73
5523	1714387.91	1637931.28	END	1331.84
5524	1714366.39	1637782.03	END	1329.77

PAVING & INCIDENTAL DRAINAGE PLANS FOR
COURTYARDS AT THE MOORINGS
PHASE 1 - PART B

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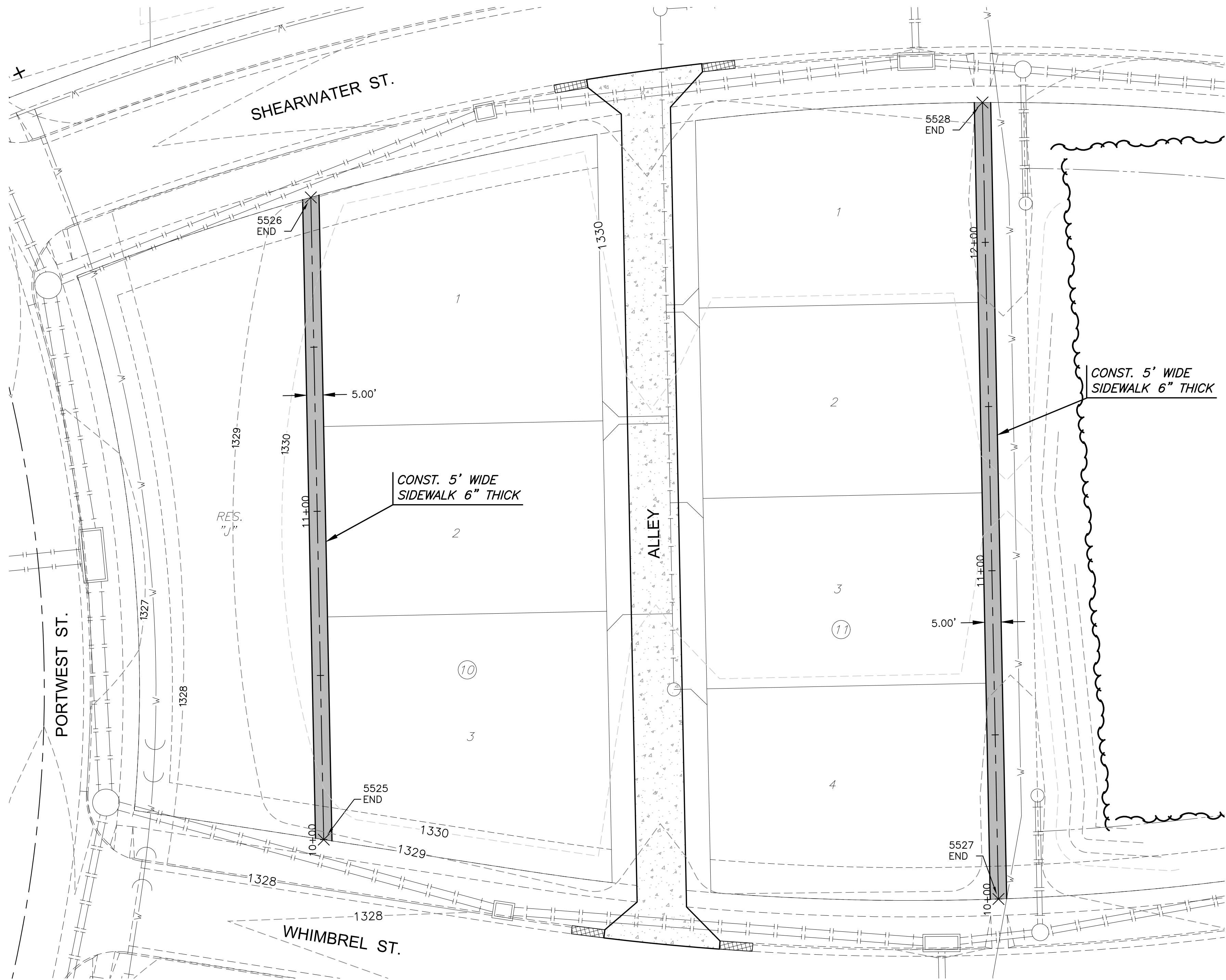
**SIDEWALK
LAYOUT 1 OF 2**

PROJECT NO.	472-2022-085820B		
DATE	SEPT. 2023		
SCALE	1" = 20'		
DESIGNED	DRAWN	CHECKED	
DFL	JWC	SPE	

NO.	REVISION	DATE

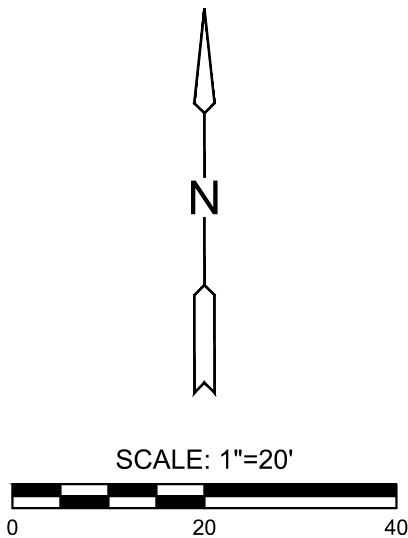
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PLOTTED: Thursday, September 14, 2023 @ 02:24PM

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PAVING POINTS				
Point #	Northing	Easting	Desc.	Elev.
5525	1714172.95	1637232.34	END	1328.55
5526	1714368.53	1637228.41	END	1328.66
5527	1714154.86	1637437.74	END	1328.87
5528	1714397.42	1637432.87	END	1328.91

- NOTES:**
- CONTRACTOR SHALL INSTALL SIDEWALK FORMS MIN. 1 WEEK PRIOR TO CASTING CONCRETE SIDEWALK.
 - CONTRACTOR SHALL ACQUIRE MKEC'S LANDSCAPE ARCHITECT WRITTEN APPROVAL OF FORMWORK PRIOR TO SIDEWALK INSTALLATION.
 - CONTRACTOR SHALL GIVE MKEC'S REPRESENTATIVE 2 DAYS NOTICE PRIOR TO INSTALLATION OF SIDEWALK FORMS.



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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SIDEWALK
LAYOUT 2 OF 2

PROJECT NO. 472-2022-085820B

DATE SEPT. 2023

SCALE 1" = 20'

DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

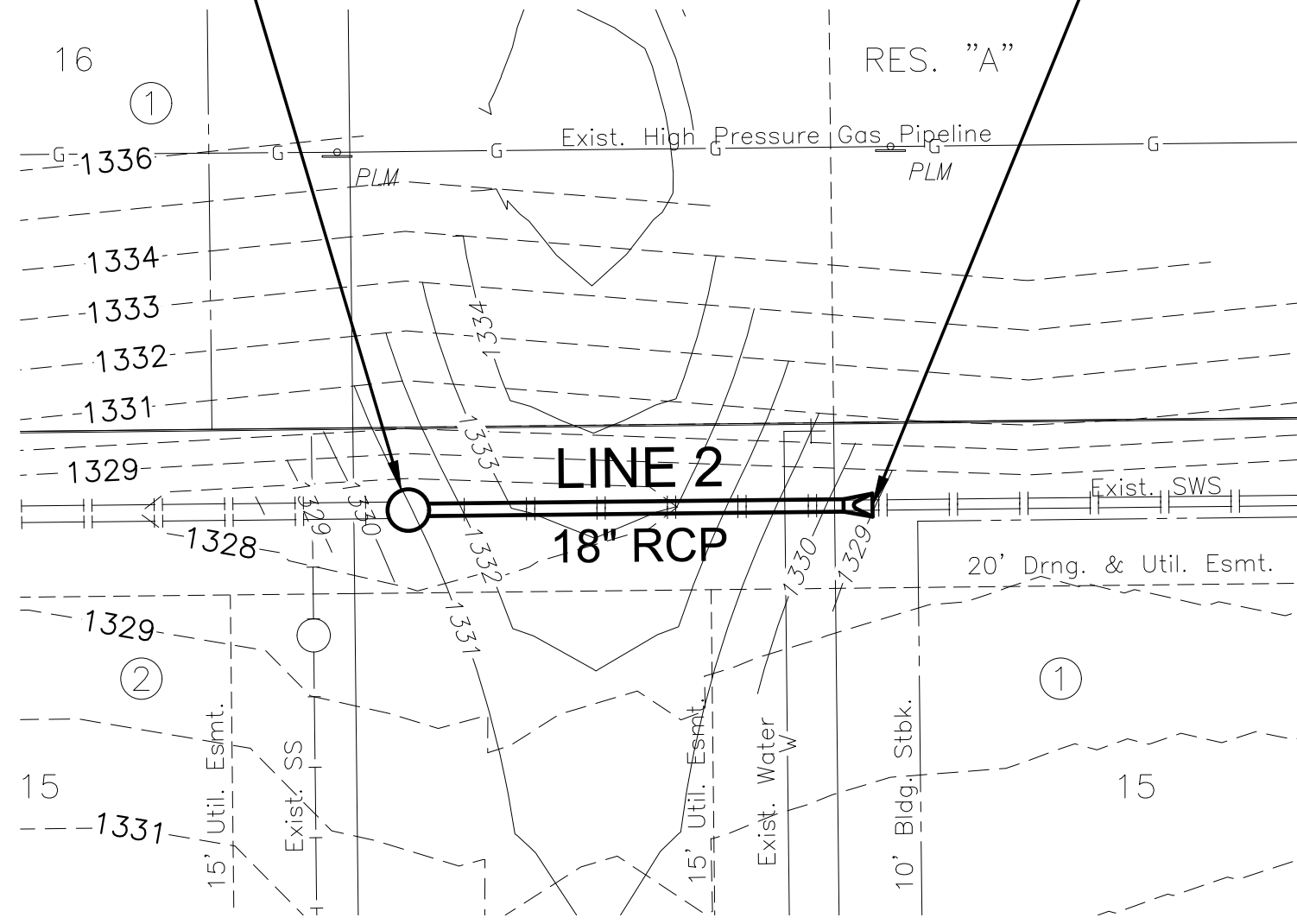
NO.	REVISION	DATE
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SHEET NO.

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PLOTTED: Thursday, September 14, 2023 @ 02:25PM

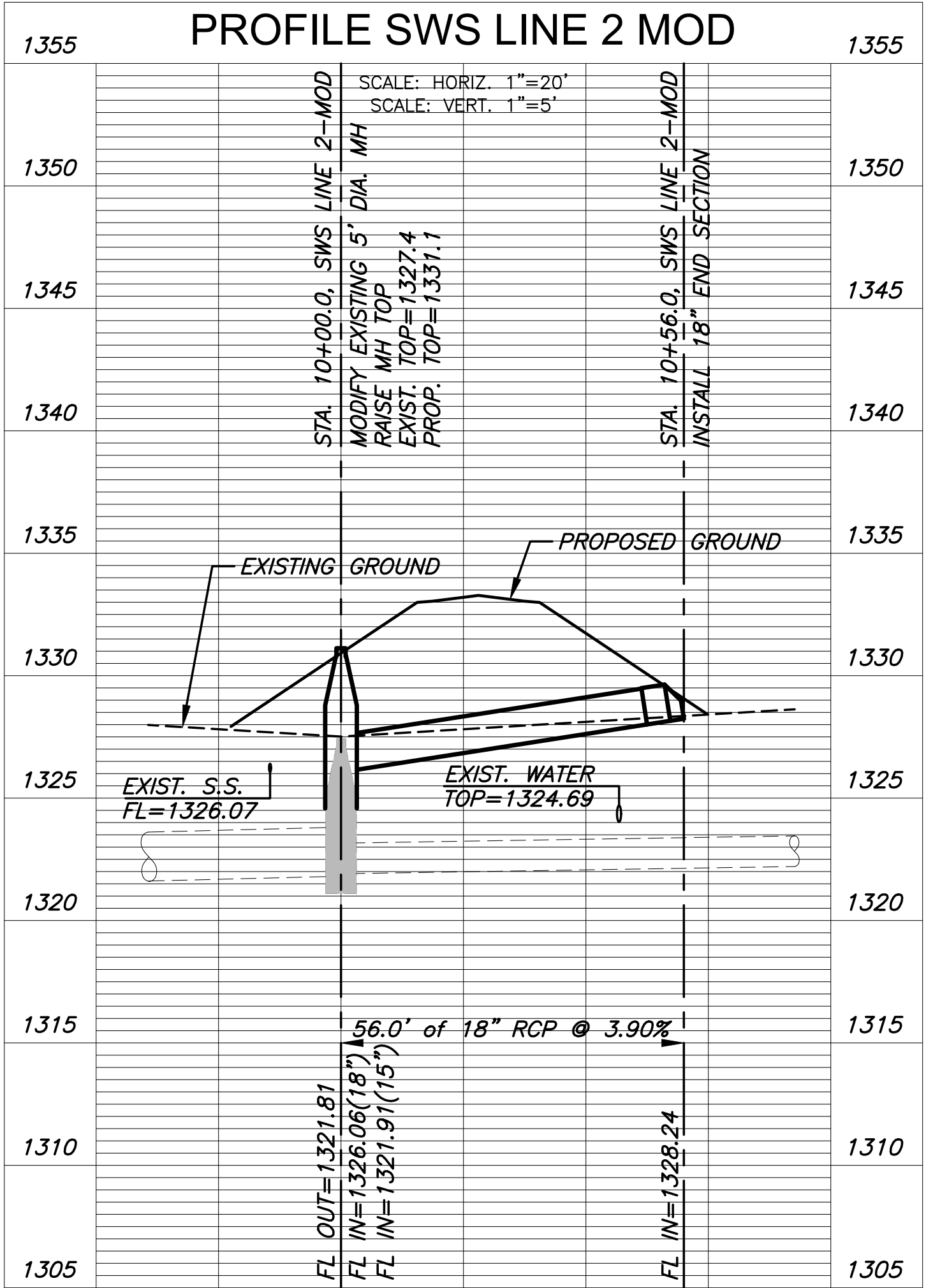
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STA. 10+00.0, SWS LINE 2-MOD
MODIFY EXISTING 5' DIA. MH
EXIST. MH TOP=1327.4
PROP. MH TOP=1331.1

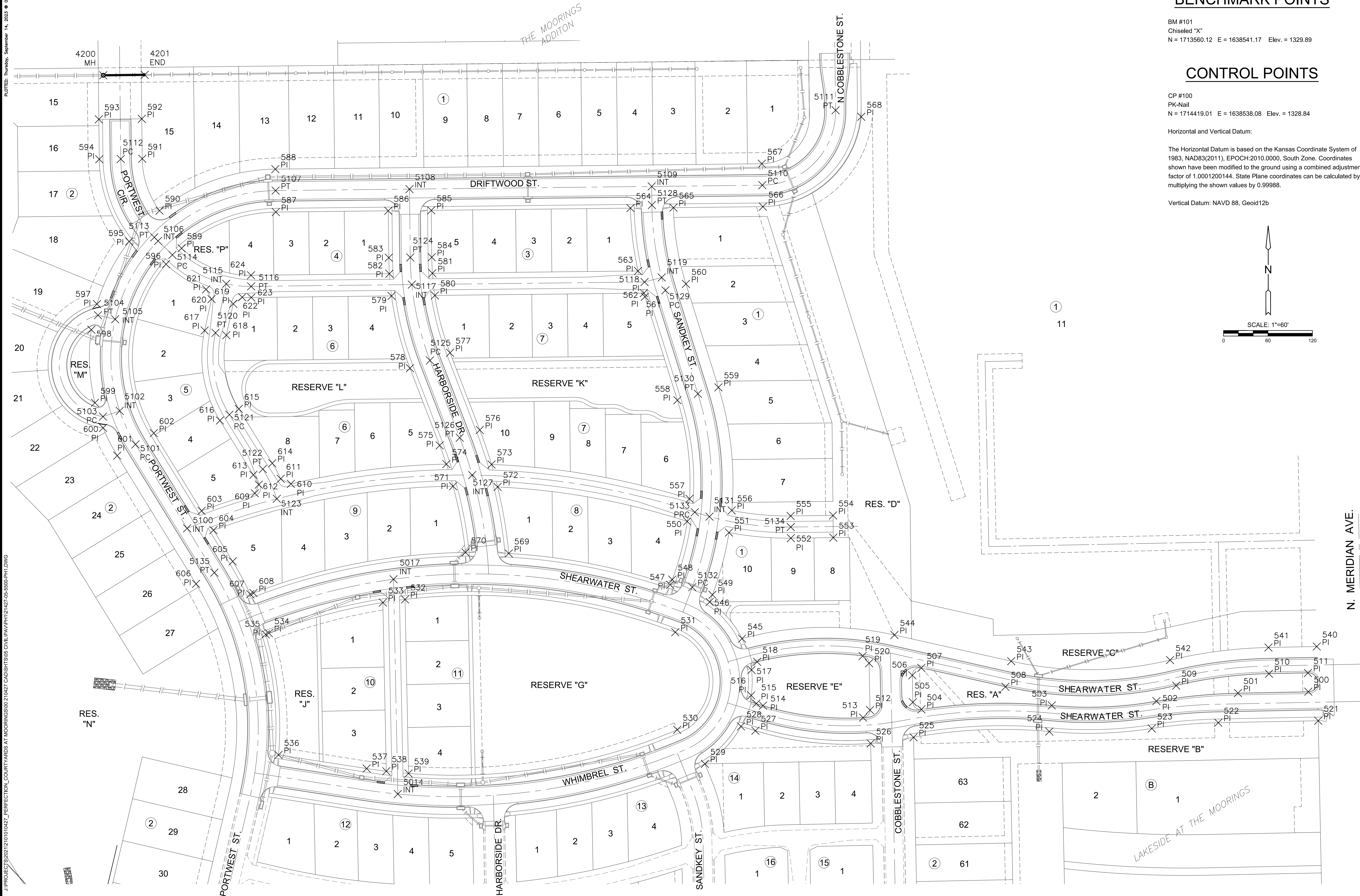


NOTE:
CONTRACTOR TO VERIFY THE
DEPTH AND LOCATION OF EXISTING
UTILITIES PRIOR TO CONSTRUCTION.

PLAN SWS LINE 2 MOD
PROFILE SWS LINE 2 MOD



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PLOTTED: Thursday, September 14, 2023 @ 02:25PM



BENCHMARK POINTS

BM #101
Chiseled "X"
N = 1713560.12 E = 1638541.17 Elev. = 1329.89

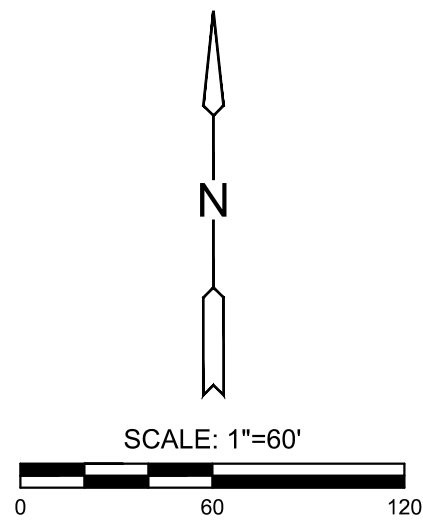
CONTROL POINTS

CP #100
PK-Nail
N = 1714419.01 E = 1638538.08 Elev. = 1328.84

Horizontal and Vertical Datum:

The Horizontal Datum is based on the Kansas Coordinate System of 1983, NAD83(2011), EPOCH:2010.0000, South Zone. Coordinates shown have been modified to the ground using a combined adjustment factor of 1.0001200144. State Plane coordinates can be calculated by multiplying the shown values by 0.99988.

Vertical Datum: NAVD 88, Geoid12b



PAVING & INCIDENTAL DRAINAGE PLANS FOR COURTYARDS AT THE MOORINGS PHASE 1 - PART B

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BUBBLE MAP

PROJECT NO.	472-2022-085820B	
DATE	SEPT. 2023	
SCALE	1"=60'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.

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PLOTTED: Thursday, September 14, 2023 @ 02:25PM

PAVING POINTS			
Point #	Northing	Easting	Desc.
5014	1714129.01	1637335.74	INT
5017	1714419.16	1637329.91	INT
5100	1714487.96	1637051.24	INT
5101	1714601.33	1636981.54	PC
5102	1714646.56	1636960.25	INT
5103	1714638.87	1636937.67	PC
5104	1714775.77	1636930.89	PT
5105	1714770.35	1636954.12	INT
5106	1714876.14	1637012.03	INT
5107	1714944.15	1637170.72	PT
5108	1714946.16	1637351.50	INT
5109	1714949.80	1637678.48	INT
5110	1714951.46	1637827.88	PC
5111	1715052.68	1637926.76	PT
5112	1714986.86	1636961.63	PC
5113	1714881.07	1637006.97	PT
5114	1714857.31	1637031.37	PC
5115	1714818.22	1637104.02	INT
5116	1714814.78	1637137.69	PT
5117	1714817.19	1637354.62	INT
5118	1714820.68	1637668.83	PI
5119	1714826.82	1637692.04	INT
5120	1714751.99	1637089.55	PT
5121	1714641.43	1637108.32	PC
5122	1714568.03	1637153.44	PT
5123	1714527.83	1637172.36	INT
5124	1714853.86	1637352.53	PT
5125	1714714.70	1637379.17	PC
5126	1714610.35	1637419.30	PT
5127	1714559.88	1637436.19	INT
5128	1714924.67	1637678.76	PT
5129	1714809.44	1637697.06	PC
5130	1714669.65	1637740.89	PT
5131	1714503.77	1637756.53	INT
5132	1714408.47	1637733.34	PC
5133	1714509.75	1637736.67	PRC
5134	1714489.70	1637866.26	PT
5135	1714428.11	1637088.03	PT

STORM WATER SEWER POINTS			
Point #	Northing	Easting	Desc.
4200	1715099.93	1636937.96	MH
4201	1715100.54	1636993.95	END

PLAT POINTS			
Point #	Northing	Easting	Desc.
500	1714267.28	1638565.02	PI
501	1714265.39	1638470.08	PI
502	1714254.56	1638359.93	PI
503	1714248.76	1638218.66	PI
504	1714243.41	1638042.62	PI
505	1714253.09	1638031.08	PI
506	1714289.38	1638030.35	PI
507	1714299.33	1638042.54	PI
508	1714273.84	1638156.09	PI
509	1714275.44	1638386.62	PI
510	1714291.48	1638511.94	PI
511	1714292.53	1638564.51	PI
512	1714242.45	1637973.28	PI
513	1714232.27	1637963.84	PI
514	1714248.49	1637828.83	PI
515	1714251.24	1637819.78	PI
516	1714262.27	1637812.80	PI
517	1714296.68	1637813.25	PI
518	1714307.60	1637820.81	PI
519	1714315.45	1637963.30	PI
520	1714305.73	1637972.01	PI
521	1714228.04	1638578.43	PI
522	1714225.35	1638443.48	PI
523	1714217.54	1638353.71	PI
524	1714213.41	1638215.33	PI
525	1714205.63	1638032.03	PI
526	1714197.37	1637974.19	PI
527	1714214.52	1637818.51	PI
528	1714220.45	1637799.01	PI
529	1714169.79	1637750.28	PI
530	1714215.82	1637712.96	PI
531	1714347.57	1637710.31	PI
532	1714391.92	1637345.46	PI
533	1714387.72	1637315.54	PI
534	1714346.37	1637161.61	PI
535	1714344.72	1637157.32	PI
536	1714181.86	1637174.70	PI
537	1714163.47	1637293.61	PI
538	1714159.84	1637320.12	PI
539	1714156.85	1637350.19	PI
540	1714328.27	1638576.43	PI
541	1714326.97	1638511.23	PI

PLAT POINTS			
Point #	Northing	Easting	Desc.
542	1714309.96	1638378.37	PI
543	1714308.48	1638163.87	PI
544	1714343.87	1638006.22	PI
545	1714339.00	1637800.50	PI
546	1714388.47	1637756.78	PI
547	1714411.61	1637703.52	PI
548	1714418.72	1637706.22	PI
549	1714398.21	1637760.47	PI
550	1714497.31	1637726.45	PI
551	1714482.26	1637782.83	PI
552	1714474.70	1637866.43	PI
553	1714475.33	1637923.70	PI
554	1714505.33	1637923.36	PI
555	1714504.69	1637866.09	PI
556	1714512.10	1637786.54	PI
557	1714527.81	1637729.55	PI
558	1714660.98	1637713.22	PI
559	1714678.33	1637768.56	PI
560	1714818.12	1637724.73	PI
561	1714800.77	1637669.38	PI
562	1714805.67	1637667.88	PI
563	1714835.58	1637660.06	PI
564	1714920.48	1637649.82	PI
565	1714921.12	1637707.82	PI
566	1714922.46	1637828.62	PI
567	1714980.45	1637827.20	PI
568	1715043.86	1637961.43	PI
569	1714454.04	1637485.52	PI
570	1714455.37	1637427.10	PI
571	1714544.86	1637410.27	PI
572	1714543.85	1637470.42	PI
573	1714574.23	1637462.21	PI
574	1714574.70	1637401.27	PI
575	1714599.94	1637392.24	PI
576	1714620.76	1637446.37	PI
577	1714725.11	1637406.23	PI
578	1714704.29	1637352.10	PI
579	1714801.88	1637327.23	PI
580	1714802.53	1637385.72	PI
581	1714832.50	1637382.40	PI
582	1714831.85	1637324.32	PI
583	1714853.54	1637323.53	PI

PLAT POINTS			
Point #	Northing	Easting	Desc.
584	1714854.18	1637381.52	PI
585	1714917.49	1637380.82	PI
586	1714916.84	1637322.82	PI
587	1714915.15	1637171.04	PI
588	1714973.15	1637170.39	PI
589	1714866.27	1637043.70	PI
590	1714917.06	1637013.98	PI
591	1714987.19	1636990.63	PI
592	1715041.07	1636990.03	PI
593	1715040.42	1636932.03	PI
594	1714986.54	1636932.63	PI
595	1714875.37	1636972.93	PI
596	1714844.78	1637022.76	PI
597	1714790.89	1636929.55	PI
598	1714756.18	1636921.93	PI
599	1714657.47	1636926.82	PI
600	1714623.70	1636937.84	PI
601	1714586.14	1636956.84	PI
602	1714616.52	1637006.25	PI
603	1714511.58	1637070.76	PI
604	1714485.50	1637086.79	PI
605	1714443.29	1637112.74	PI
606	1714412.92	1637063.33	PI
607	1714398.88	1637136.57	PI
608	1714400.52	1637140.85	PI
609	1714535.25	1637142.69	PI
610	1714547.98	1637191.38	PI
611	1714554.79	1637177.62	PI
612	1714547.01	1637148.03	PI
613	1714560.17	1637140.66	PI
614	1714575.88	1637166.22	PI
615	1714649.28	1637121.10	PI
616	1714633.57	1637095.54	PI
617	1714755.19	1637074.90	PI
618	1714748.79	1637104.21	PI
619	1714791.26	1637113.48	PI
620	1714797.66	1637084.18	PI
621	1714810.74	1637076.87	PI
622	1714800.10	1637125.57	PI
623	1714799.78	1637137.85	PI
624	1714829.78	1637137.52	PI

EVERY ATTEMPT HAS BEEN MADE TO INSURE ALL COORDINATE VALUES SHOWN ARE AN ACCURATE AND TRUE REPRESENTATION OF THE CURRENT PLANS. ALL VALUES ARE TO BE CONFIRMED WITH THE FINAL SIGNED PLAN SET BEFORE USE.



PAVING & INCIDENTAL DRAINAGE PLANS FOR

COURTYARDS AT THE MOORINGS

PHASE 1 - PART B

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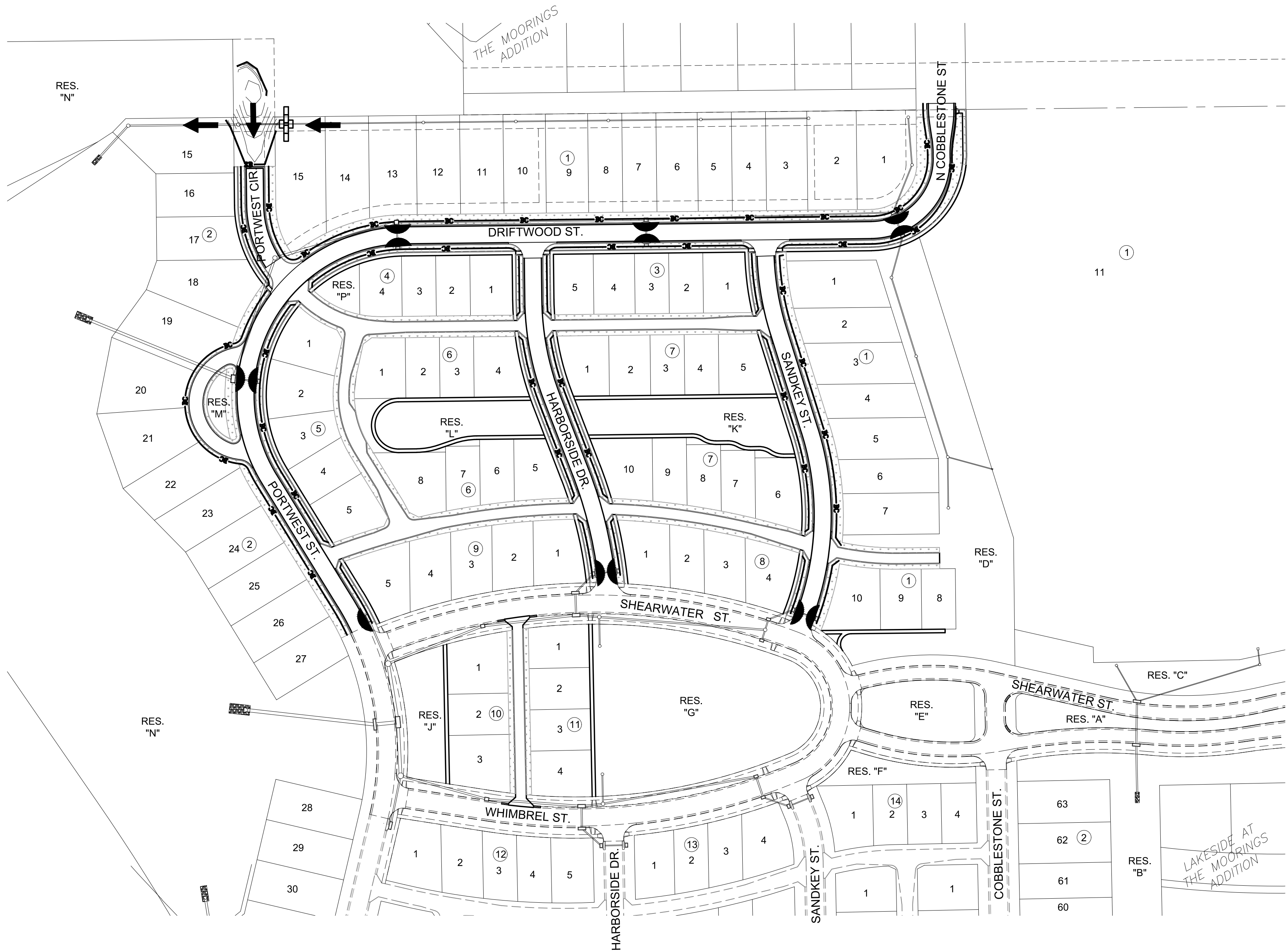
BUBBLE TABLE

PROJECT NO.	472-2022-085820B
DATE	SEPT. 2023
SCALE	AS SHOWN

NO.	REVISION	DATE

J:\PROJECTS\2021\101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SH105 CIVIL\SW\SPH1121427-05-4501.DWG
PLOTTED: Thursday, September 14, 2023 @ 02:25PM

J:\PROJECTS\2021\101010427_PERFECTION_COURTYARDS AT MOORINGS\00 210427 CAD\SH105 CIVIL\SW\SPH1121427-05-4501.DWG



EROSION CONTROL/SEEDING NOTES

1. EROSION CONTROL IS TO MEET ALL FEDERAL, STATE, COUNTY AND LOCAL CODE STANDARDS.
2. SEEDING: ALL AREAS DISTURBED WITH EXCEPTION OF PROPOSED STREET PAVEMENT BE SEEDED (COST SUBSIDIARY TO PROJECT) AND FERTILIZED AS FOLLOWS:
 - ANNUAL RYE @ 150 LBS./ACRE
 - SLOW RELEASE @ 150 LBS./ACRE
3. IN THE EVENT THAT A PORTION OF THE SITE WILL REMAIN DISTURBED FOR MORE THAN 14 DAYS, SEEDING SHALL BE INSTALLED.
4. ALL AREAS SHALL BE FINE GRADED AND SURFACE SHALL BE FREE FROM STICKS, SMALL STONES, AND OTHER EXTRANEIOUS MATERIALS.
5. CONTRACTOR SHALL PROVIDE EROSION PROTECTION THROUGHOUT PROJECT CONSTRUCTION. THE PLAN PROVIDED HERE IS FOR FINAL PROTECTION. VARIOUS PHASES OF THIS PLAN SHALL BE IMPLEMENTED OR MODIFIED TO CONTROL EROSION. MODIFICATIONS OF THE PLAN SHALL BE APPROVED BY THE OWNER'S REPRESENTATIVE.
6. SEEDING AREAS SHALL BE PREPARED FOR PLANTING WITH COMMON AGRICULTURAL TECHNIQUES. APPROVE WITH OWNER'S REPRESENTATIVE BEFORE PLANTING.
7. ALL SEED SHALL BE DISTRIBUTED WITH AN ACCEPTABLE DRILL INTENDED FOR SUCH OPERATIONS, OR OTHER EQUIPMENT APPROVED BY THE OWNER'S REPRESENTATIVE. SEEDING DEPTH SHALL BE 1/4 OF AN INCH.
8. ALL SEEDED AREAS SHALL BE IMMEDIATELY MULCHED WITH PRAIRIE HAY AT 2 TONS/ACRE. ANCHOR MULCH BY CRIMPING INTO TOPSOIL WITH SUITABLE MECHANICAL EQUIPMENT.
9. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AND IMPLEMENTING ALL EROSION CONTROL.
10. IN ORDER TO PREVENT SILT OR SEDIMENT FROM ENTERING ADJACENT PROPERTIES, APPROPRIATE BMP'S SHALL BE IMPLEMENTED WITHIN THE PROJECT.
11. ANY MUD TRACKED ONTO ADJACENT PAVED AREAS OR STREETS SHALL BE REMOVED AT THE END OF EACH WORK DAY.
12. PER THE REQUIREMENTS OF THE NOI/SWPPP, BMP INSPECTION REPORTS SHALL BE COMPLETED BY THE CONTRACTOR WEEKLY AND WITHIN 24 HOURS AFTER A 1/2" RAIN. REPORTS SHALL BE KEPT WITH THE SWPPP ON SITE.
13. CONTRACTOR SHALL PROVIDE A SIGN NEAR THE ENTRANCE WITH THE FOLLOWING INFORMATION:
 - A. CONTACT NAME AND INFORMATION
 - B. A COPY OF THE NOI
 - C. LOCATION OF SWPPP

LEGEND

- WATERLINE
- SANITARY SEWER
- STORMWATER SEWER
- FLOW ARROW
- CURB INLET PROTECTION (13)
- DITCH CHECK (1)
- SILT FENCE (APPROX. 220 LF)
- BACK OF CURB PROTECTION (APPROX. 5,220 LF)
- SEEDING LIMITS (APPROX. 2.0 ACRES)



PAVING & INCIDENTAL DRAINAGE PLANS FOR COURTYARDS AT THE MOORINGS PHASE 1 - PART B

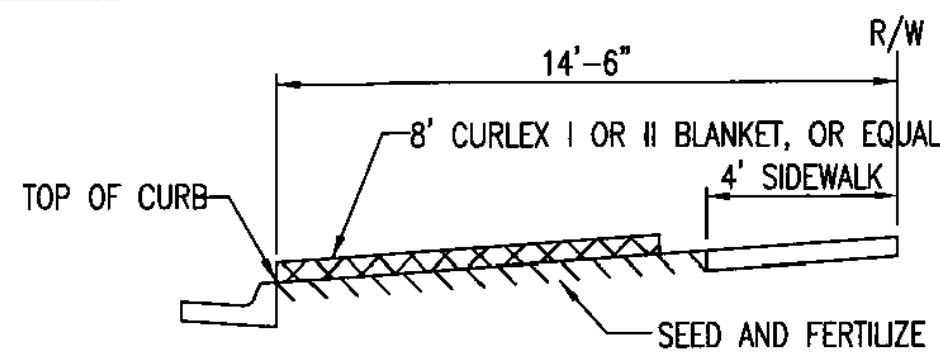
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EROSION CONTROL PLAN

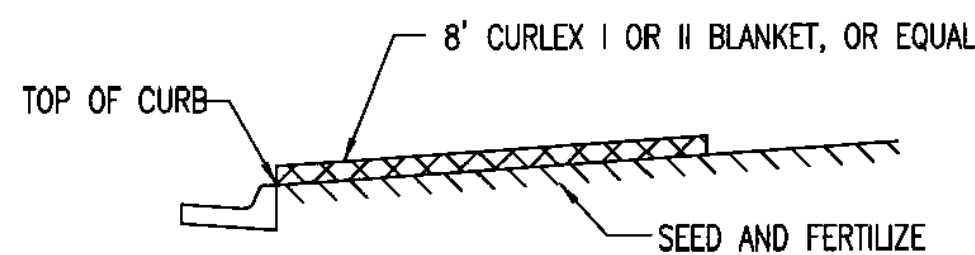
PROJECT NO.	472-2022-085820	
DATE	SEPT. 2023	
SCALE	1" = 100'	
DESIGNED	DRAWN	CHECKED
DFL	JWC	SPE

NO.	REVISION	DATE

SHEET NO.
31 OF 51

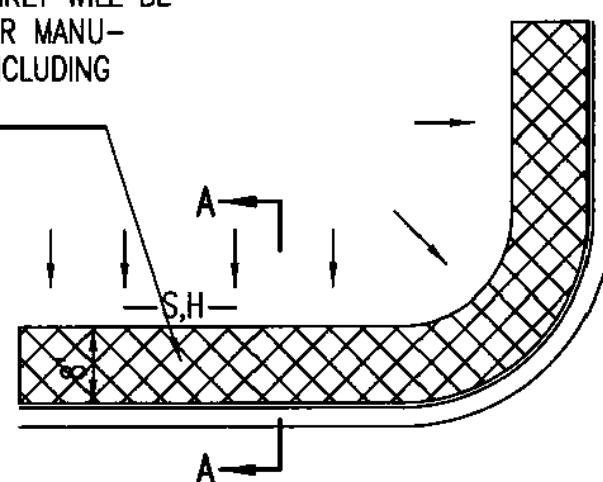


SECTION B-B

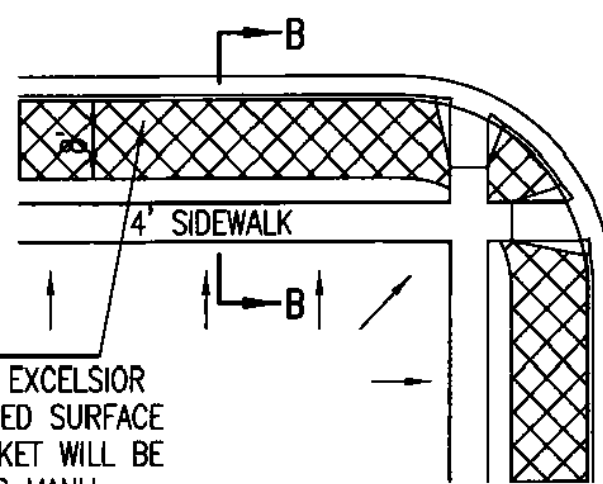


SECTION A-A

INSTALL 8' WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURERS RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)



SOUTH STREET

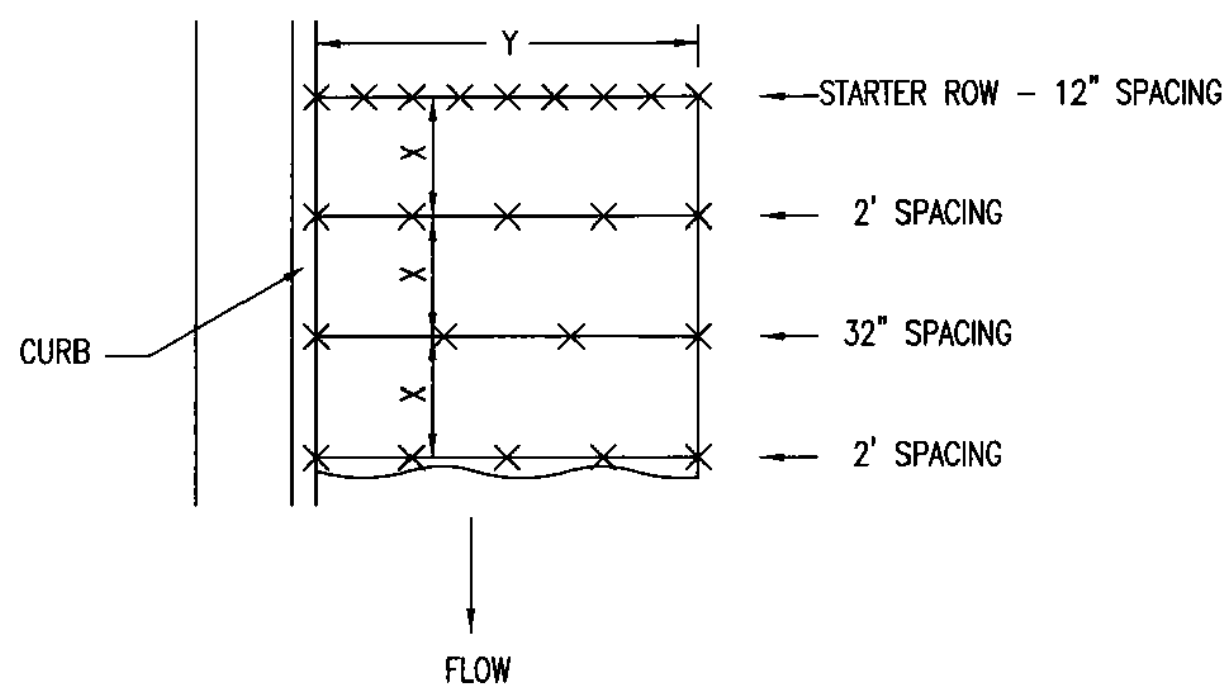


INSTALL 8' WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURERS RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)

GENERAL NOTES

- EXCELSIOR MAT TO BE INSTALLED WHEN SOD IS NOT SPECIFIED ON PROJECT.
- EXCELSIOR BLANKET TO BE INSTALLED OVER SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS.
- AFTER INSTALLATION OF EXCELSIOR BLANKET, AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB AND INTO THE GUTTER, SUPPLEMENTAL EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS NEEDED, TO FIX THE PROBLEM.

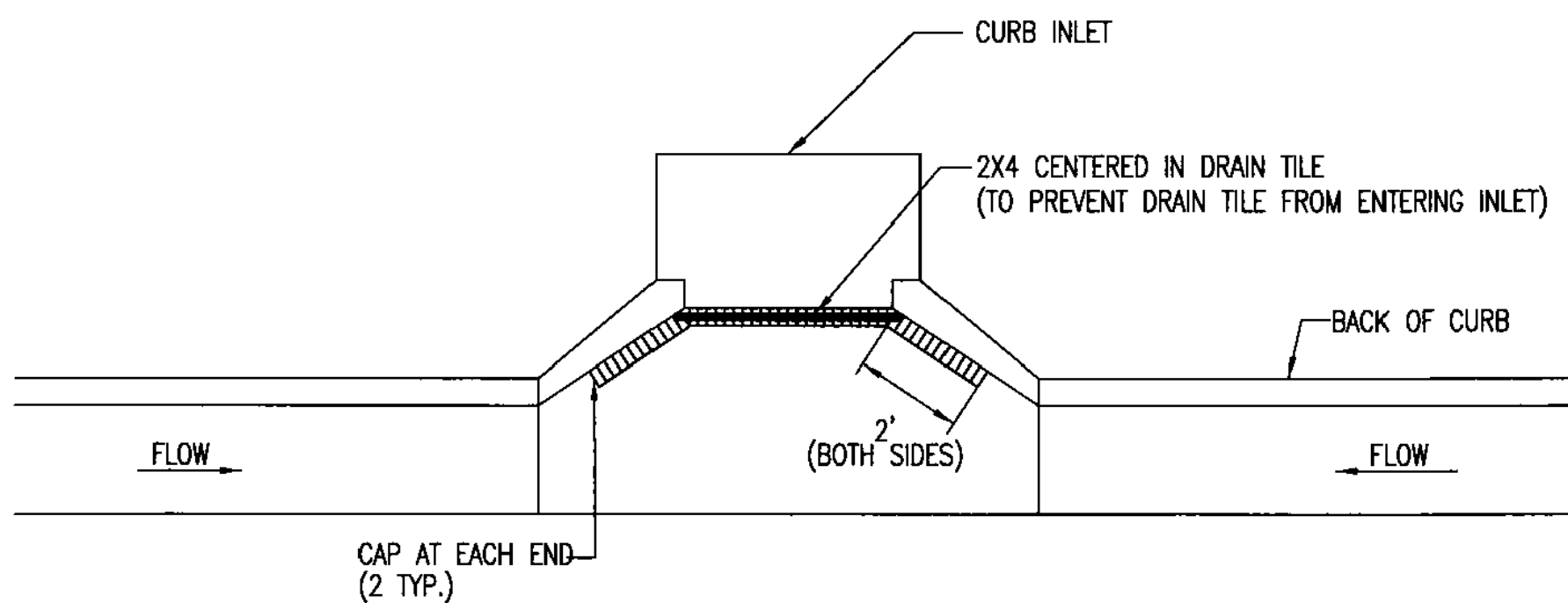
BACK OF CURB PROTECTION DETAIL



STAPLE PATTERN

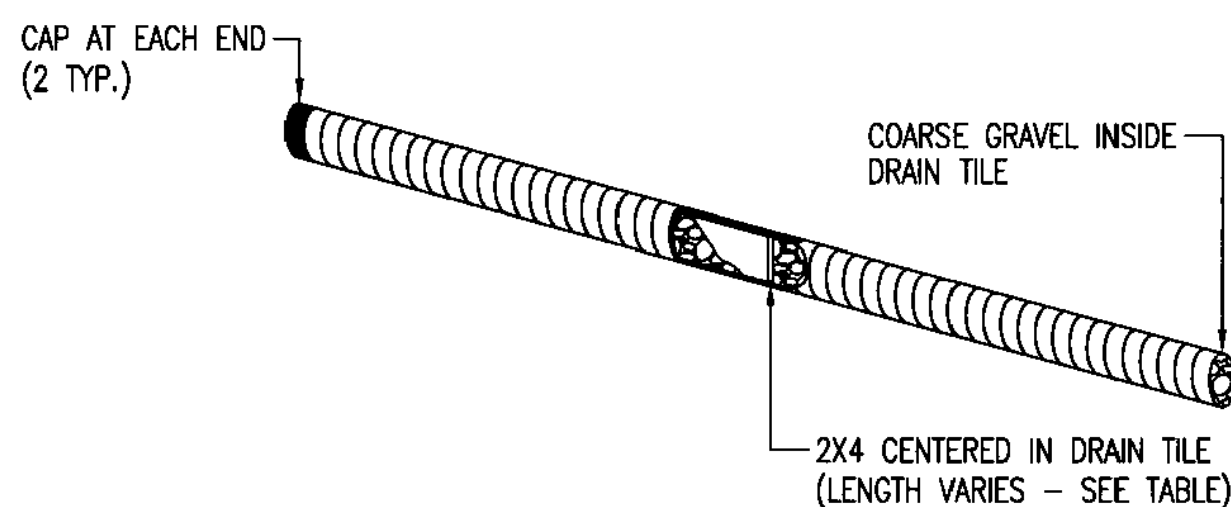
NOTES: USE 6" SEAM OVERLAP
(X & Y = RECOMMENDED BY MANUFACTURE)

DETAILS FOR APPROVED EROSION CONTROL MAT

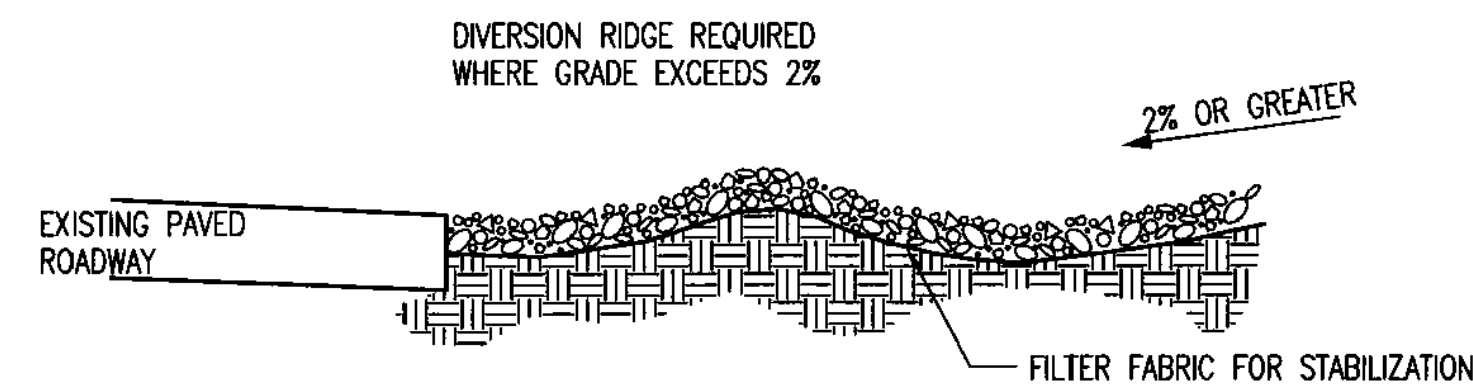


NOTE: PLACE 4" PERFORATED PVC PIPE, FILLED WITH 1/2"-1" DIA. GRAVEL, IN FRONT OF CURB INLET AS SHOWN.

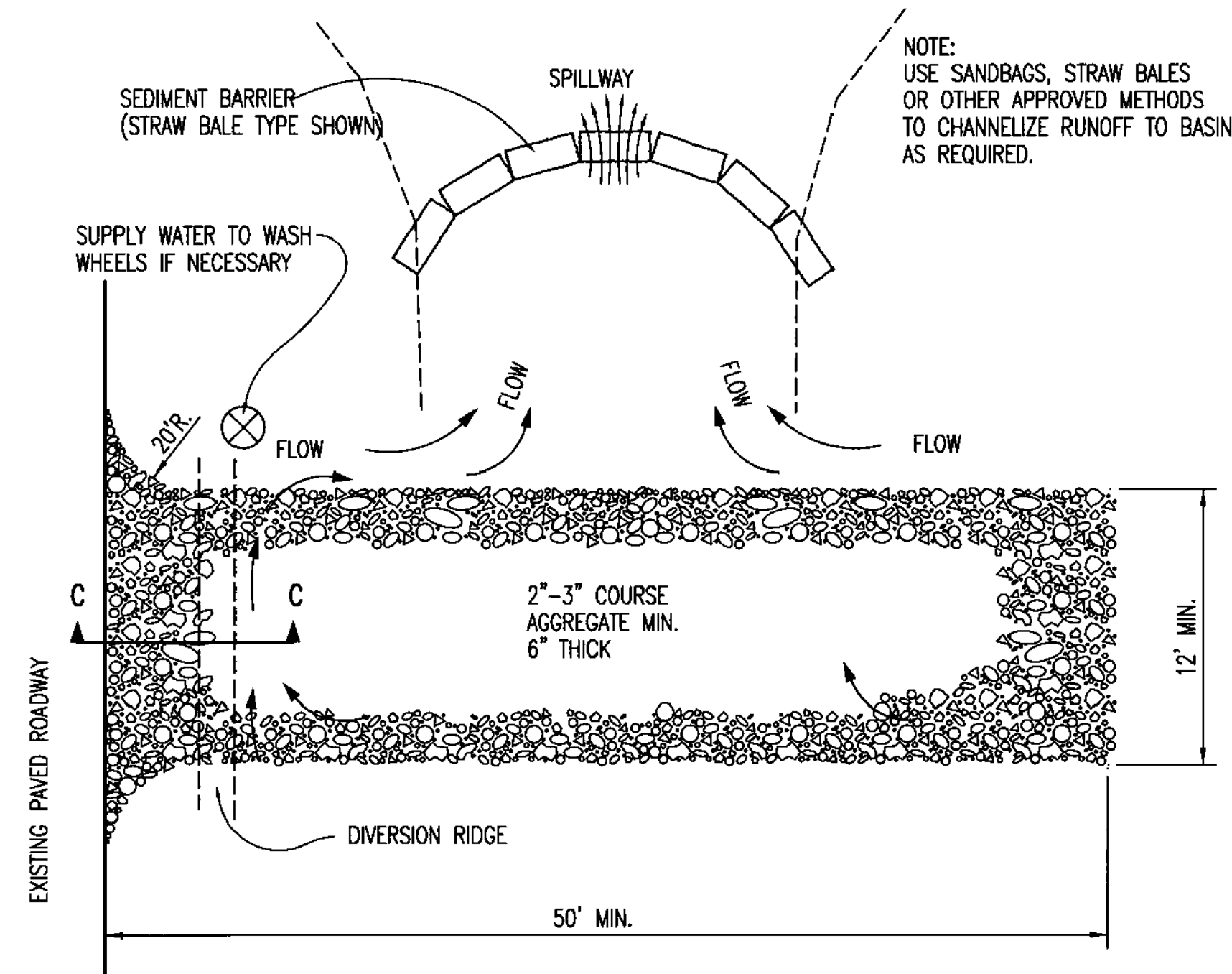
2X4 LENGTH	INLET TYPE	INLET OPENING
5'-6"	1-A	5'-0"
10'-6"	1-A	10'-0"
15'-6"	1-A	15'-0"



CURB INLET PROTECTION 4" PERFORATED PIPE W/ GRAVEL



SECTION C-C



STABILIZED CONSTRUCTION ENTRANCE

GENERAL NOTES

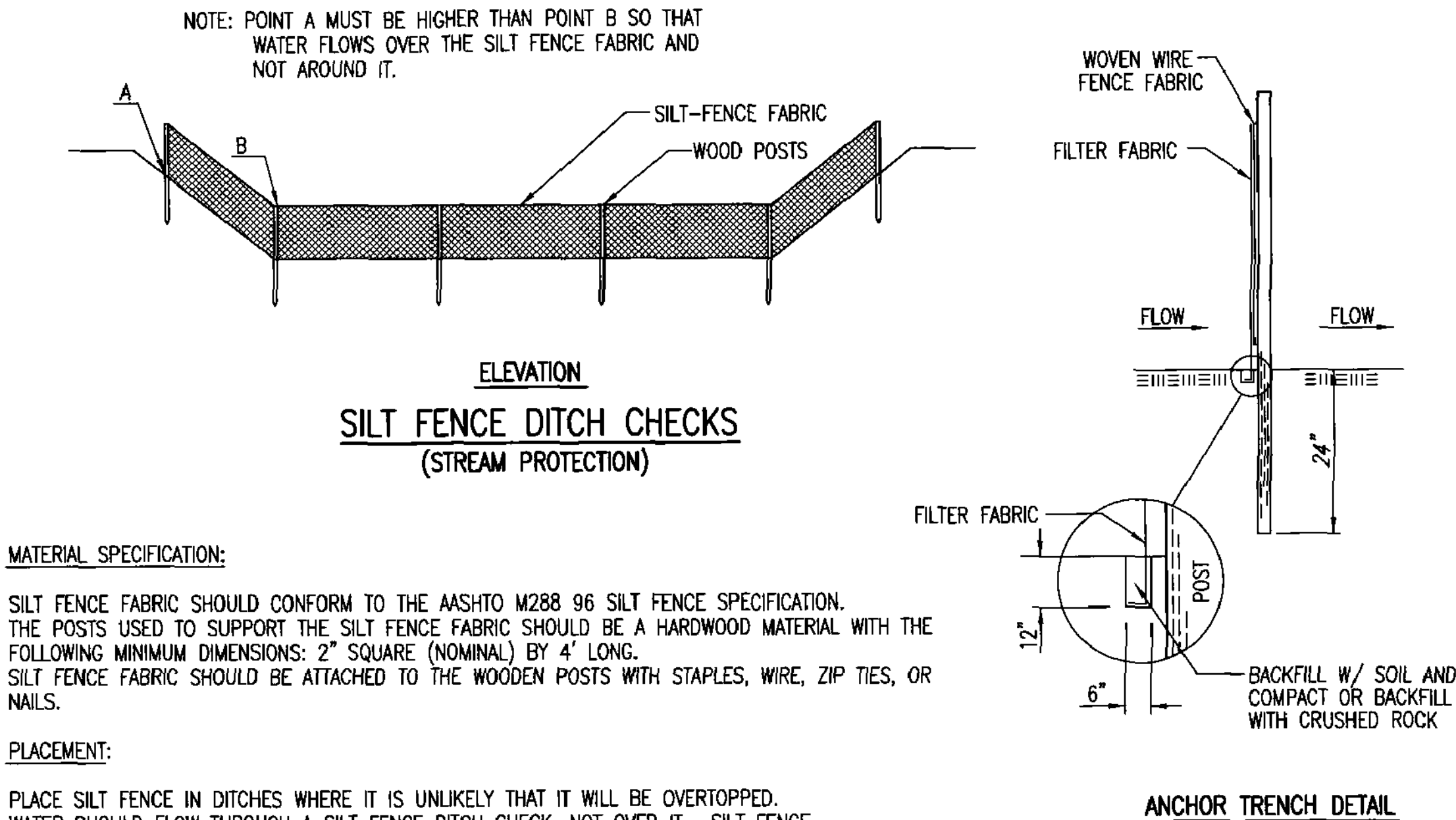
- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
- WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN, AS SHOWN ABOVE.
- DRIVE ENTRANCES ONTO RESIDENTIAL LOTS WILL NOT BE REQUIRED TO HAVE THE SEDIMENT BARRIER SHOWN, BUT WHEEL WASHING MAY BE REQUIRED IF STABILIZED ENTRANCE IS NOT SUFFICIENT TO KEEP MUD FROM BEING TRACKED ONTO ADJACENT STREET. ENTRANCE SHALL EXTEND FROM BACK OF CURB TO DWELLING.

REVISION DATE: MAY 2013



BACK OF CURB PROTECTION, CURB INLET PROTECTION AND CONSTRUCTION ENTRANCE

CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER 472-2022-0858208	OCA NUMBER ####	DATE SEPT. 2023
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 32 OF 51



MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK, NOT OVER IT. SILT FENCE DITCH CHECKS OFTEN FAIL WHEN OVERTOPPED. SILT FENCE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE SILT FENCE SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE TOP OF THE LOW POINT OF THE FENCE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. SILT FENCE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. SILT FENCE SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.

THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK DITCH GRADE (%)	SPACING CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS AT LEAST 12" DEEP BY 6" WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSTREAM SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE OF THE TRENCH. LINE TWO SIDES OF THE TRENCH WITH THE FABRIC AS SHOWN ON DETAIL. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE ON THE UPSLOPE SIDE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSTREAM OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 24". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

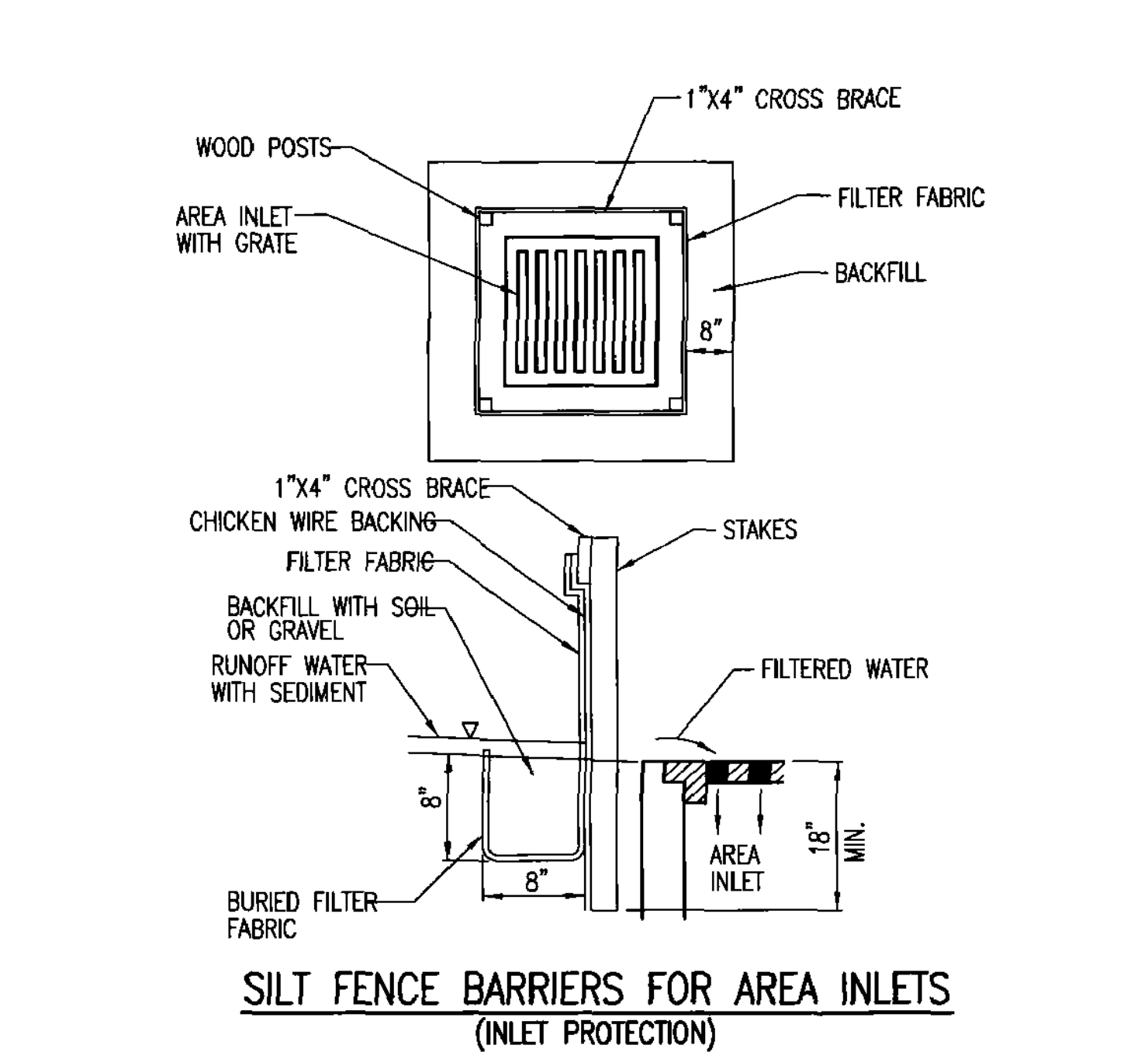
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK—NOT OVER IT. PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. SILT FENCE INSTALLATIONS QUICKLY DETERIORATE WHEN WATER OVERTOPS THEM. DO NOT PLACE SILT FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE A SILT FENCE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE SILT FENCE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE LOW POINT ON THE TOP OF THE FENCE. DO NOT PLACE SILT FENCE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

INSPECTION AND MAINTENANCE:

SILT FENCE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW AROUND THE DITCH CHECK?
DOES WATER FLOW UNDER THE DITCH CHECK?
DOES THE SILT FENCE SAG EXCESSIVELY?
HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE WIRE OR POLYMERIC MESH BACKING USED TO HELP SUPPORT THE SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. THE MATERIAL USED TO FRAME THE TOPS OF THE POSTS SHOULD BE 1" BY 4" BOARDS. SILT FENCE FABRIC AND SUPPORT BACKING SHOULD BE ATTACHED TO THE WOODEN POSTS AND FRAME WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE A SILT FENCE DROP INLET BARRIER IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. WATER SHOULD FLOW THROUGH SILT FENCE, NOT OVER IT. SILT FENCE BARRIERS FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. WHEN USED AS A BARRIER FOR AREA INLETS, SILT FENCE FABRIC AND POSTS MUST BE SUPPORTED AT THE TOP BY A WOODEN FRAME. WHEN A SILT FENCE BARRIER FOR AREA INLETS IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 8" DEEP BY 8" WIDE. DRIVE POSTS TO A DEPTH OF AT LEAST 18" AROUND THE PERIMETER OF THE AREA INLET. THE DISTANCE BETWEEN POSTS SHOULD BE 4' OR LESS. IF THE DISTANCE BETWEEN TWO ADJACENT CORNER POSTS IS MORE THAN 4', ADD ANOTHER POST(S) BETWEEN THEM. CONNECT THE TOPS OF ALL THE POSTS WITH A WOODEN FRAME MADE OF 1" BY 4" BOARDS. USE NAILS OR SCREWS FOR FASTENING. ATTACH THE WIRE OR POLYMERIC-MESH BACKING TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC LONG ENOUGH TO WRAP AROUND THE PERIMETER OF THE AREA INLET. ADD MORE LENGTH FOR OVERLAPPING THE FABRIC JOINT. PLACE THE EDGE OF THE FABRIC IN THE TRENCH, STARTING AT THE OUTSIDE EDGE OF THE TRENCH. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. ATTACH THE SILT FENCE TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. THE JOINT SHOULD BE OVERLAPPED TO THE NEXT POST.

NOTE: WHEN A SILT FENCE BARRIER FOR AREA INLET IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

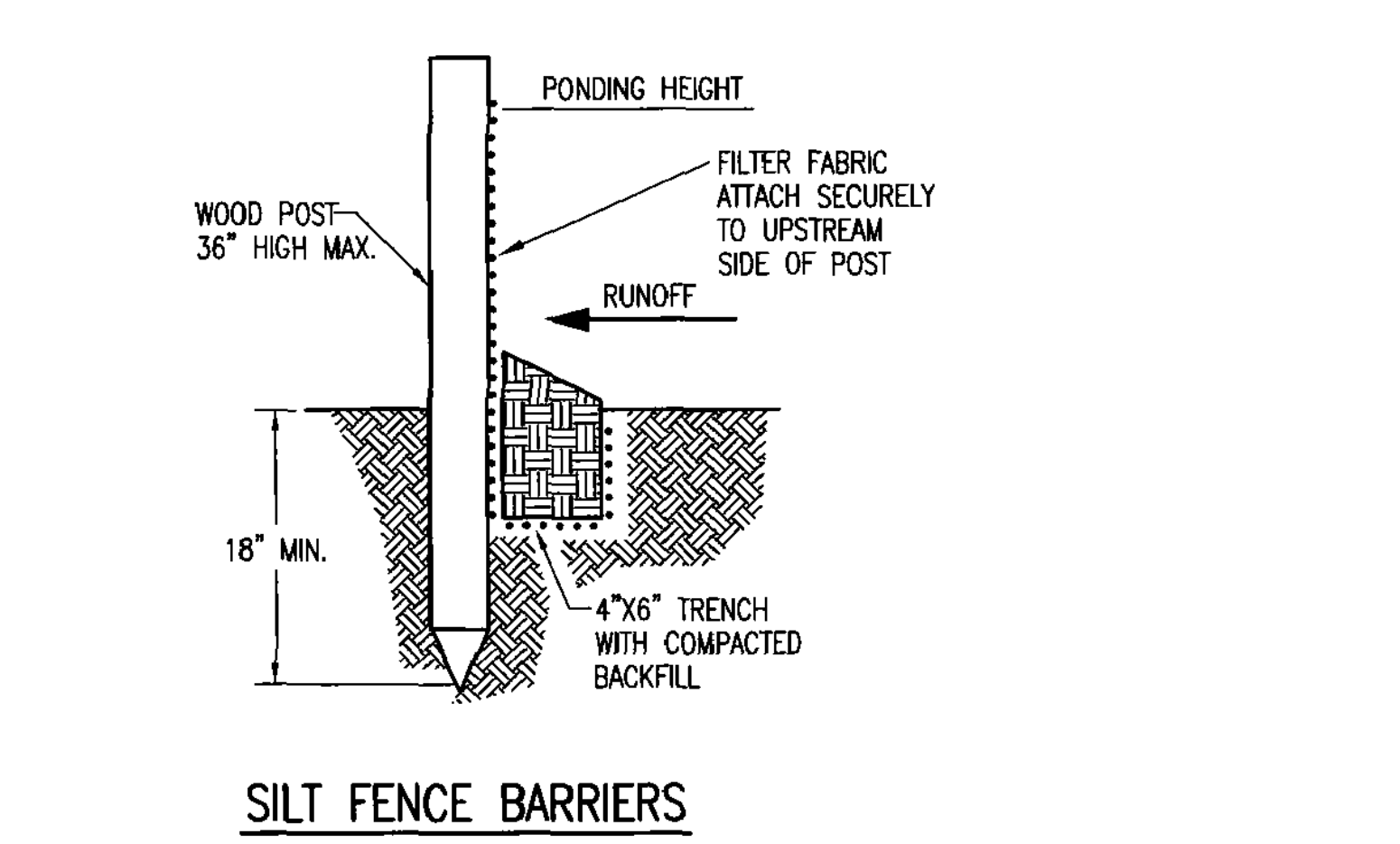
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE BARRIER FOR AREA INLET—NOT OVER IT. PLACE A SILT FENCE BARRIER FOR AREA INLET IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. SILT FENCE BARRIER FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. DO NOT PLACE POSTS ON THE OUTSIDE OF THE SILT FENCE BARRIER FOR AREA INLET. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESISTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT INSTALL SILT FENCE BARRIER FOR AREA INLETS WITHOUT FRAMING THE TOP OF THE POSTS. THE CORNER POSTS AROUND AREA INLETS ARE STRESSED IN TWO DIRECTIONS WHEREAS A NORMAL SILT FENCE IS ONLY STRESSED IN ONE DIRECTION. THIS ADDED STRESS REQUIRES MORE SUPPORT.

INSPECTION AND MAINTENANCE:

SILT FENCE BARRIER FOR AREA INLETS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW UNDER THE SILT FENCE?
DOES THE SILT FENCE SAG EXCESSIVELY?
HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, SILT FENCE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. SILT FENCE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 6" DEEP BY 4" WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT-FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE UPSLOPE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 18". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICABLE, DO NOT PLACE SILT FENCE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. WHEN THE FLOW CONCENTRATES, IT OVERTOPS THE BARRIER AND THE SILT FENCE SLOPE BARRIER QUICKLY DETERIORATES. DO NOT PLACE SILT-FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE SILT FENCE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT SUFFICIENTLY ANCHORED, IT WILL WASH OUT. SILT FENCE SLOPE BARRIERS MUST BE DUG INTO THE GROUND—SILT FENCE AT GROUND LEVEL DOES NOT WORK BECAUSE WATER WILL FLOW UNDERNEATH.

INSPECTION AND MAINTENANCE:

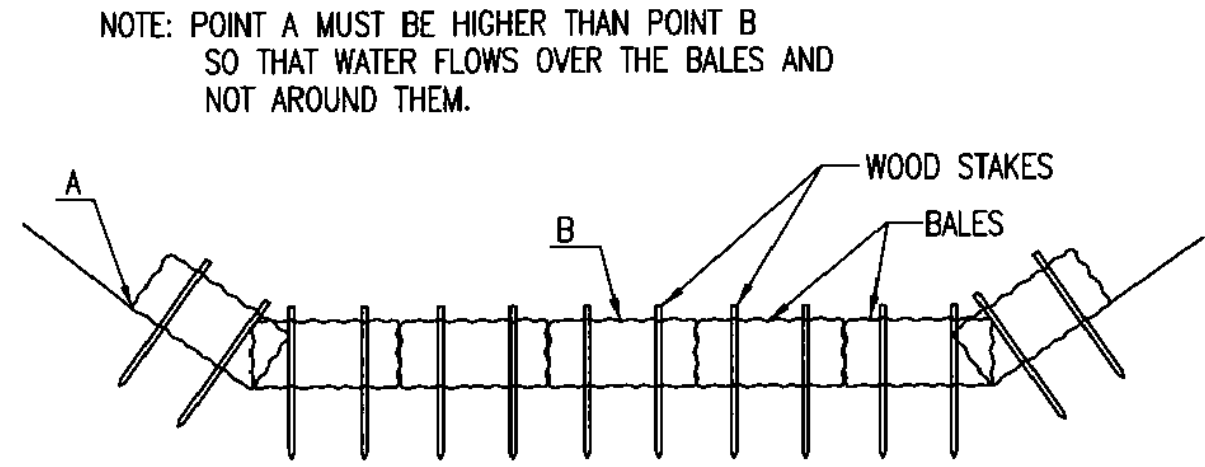
SILT FENCE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
DOES WATER FLOW UNDER THE SLOPE BARRIER?
DO THE SILT FENCES SAG EXCESSIVELY?
HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2013



SILT FENCE DITCH CHECK AND BARRIER DETAILS			
CITY ENGINEER GARY JANZEN, P.E.			
PROJECT NUMBER 472-2022-0858208	OCA NUMBER ###	DATE SEPT. 2023	
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501			SHEET 33 OF 51



STRAW BALE DITCH CHECKS

MATERIAL SPECIFICATION:

BALE DITCH CHECKS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. OPTIONAL: THE DOWNSTREAM SCOUR APRON SHOULD BE CONSTRUCTED OF A DOUBLE-NETTED STRAW EROSION-CONTROL BLANKET AT LEAST 6' WIDE. OPTIONAL: THE METAL LANDSCAPE STAPLES USED TO ANCHOR THE EROSION-CONTROL BLANKET SHOULD BE AT LEAST 8" LONG.

PLACEMENT:

BALE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE DITCH CHECK SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. STRAW BALE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. BALES SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED. THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK SPACING DITCH GRADE (%)	CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH-IT WILL BE USED LATER. OPTIONAL: ON THE DOWNSTREAM SIDE OF THE TRENCH, ROLL OUT A LENGTH OF EROSION-CONTROL BLANKET (SCOUR APRON) EQUAL TO THE LENGTH OF THE TRENCH. PLACE THE UPSTREAM EDGE OF THE EROSION-CONTROL BLANKET ALONG THE BOTTOM UPSTREAM EDGE OF THE TRENCH. THE EROSION CONTROL BLANKET SHOULD BE ANCHORED IN THE TRENCH WITH ONE ROW OF 8" LANDSCAPE STAPLES PLACED ON 18" CENTERS. THE REMAINDER OF THE EROSION-CONTROL BLANKET (THE PORTION THAT IS NOT LYING IN THE TRENCH) WILL SERVE AS THE DOWNSTREAM SCOUR APRON. THIS SECTION OF THE BLANKET SHOULD BE ANCHORED TO THE GROUND WITH 8" LANDSCAPE STAPLES PLACED AROUND THE PERIMETER OF THE BLANKET ON 18" CENTERS. THE REMAINDER OF THE BLANKET SHOULD BE ANCHORED USING TWO EVENLY SPACED ROWS OF 8" LANDSCAPE STAPLES ON 18" CENTERS PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSTREAM SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP AND EXTEND UPSTREAM NO MORE THAN 24".

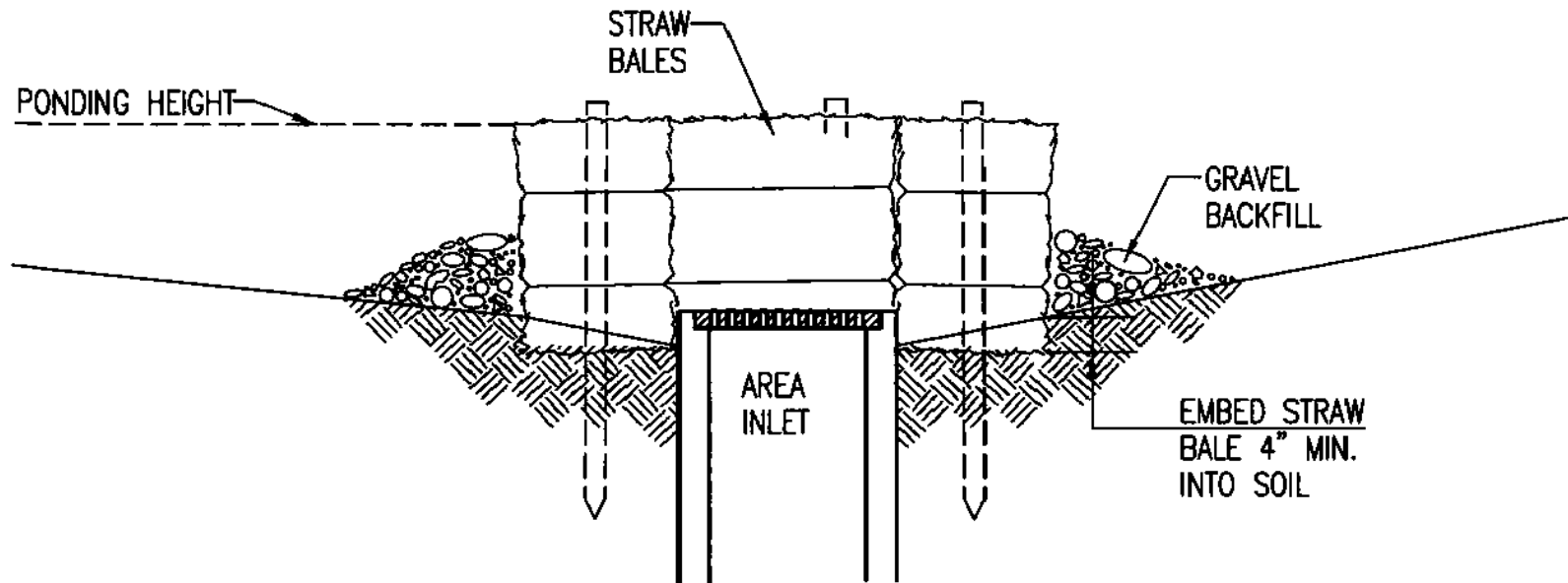
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

DO NOT PLACE A BALE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE BALE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH-CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. DO NOT PLACE BALE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT. BALE DITCH CHECKS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE CHECK.

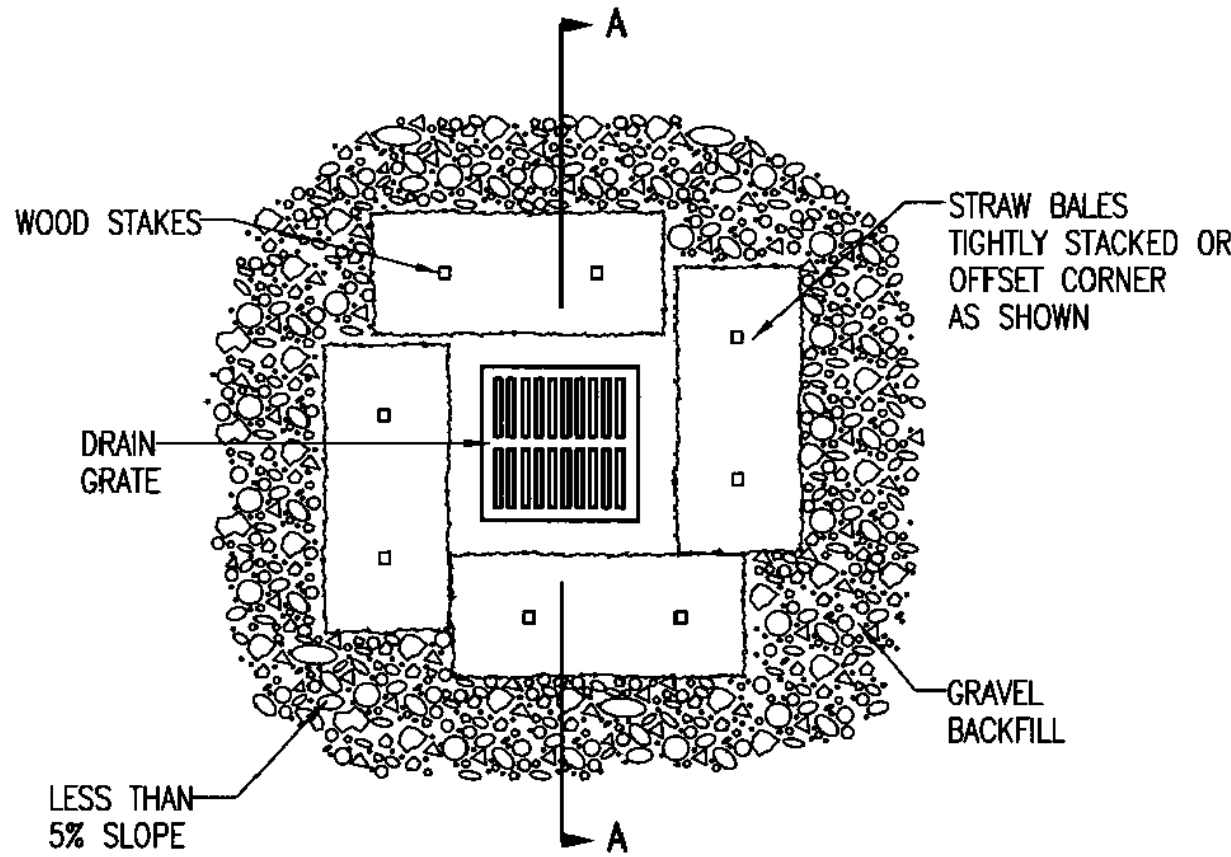
INSPECTION AND MAINTENANCE:

BALE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW AROUND THE DITCH CHECK?
DOES WATER FLOW UNDER THE DITCH CHECK?
DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
ARE ANY BALES AND/OR SCOUR APRONS (OPTIONAL) DISLODGED?
ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



SECTION A-A



STRAW BALE BARRIERS FOR AREA INLETS (INLET PROTECTION)

MATERIAL SPECIFICATION:

BALE AREA INLET BARRIERS SHOULD BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

BALE AREA INLET BARRIERS SHOULD BE PLACED DIRECTLY AROUND THE PERIMETER OF A DROP INLET. WHEN A BALE AREA INLET BARRIER IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 4" DEEP BY A BALE'S WIDTH WIDE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. SOME BALES MAY NEED TO BE SHORTENED TO FIT INTO THE TRENCH AROUND THE AREA INLET. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE RECEIVING SIDE OF THE BARRIER AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP. NOTE: WHEN A BALE AREA INLET BARRIER IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

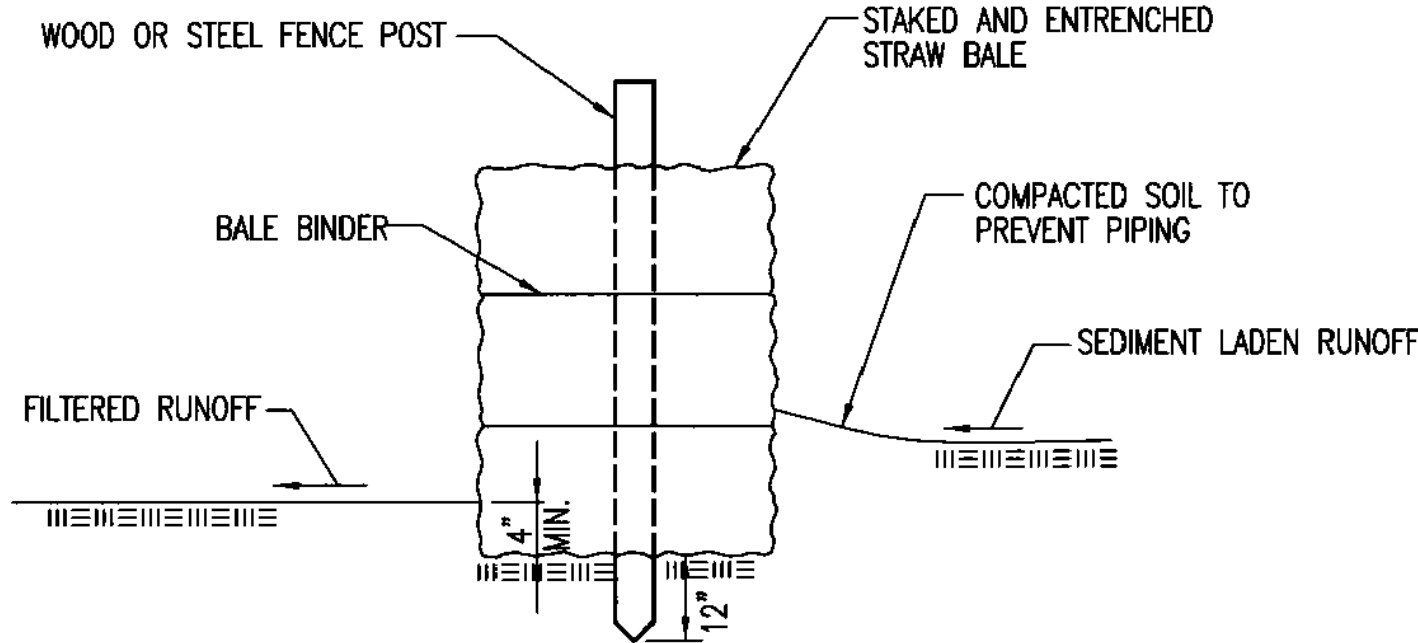
LIST OF COMMON PLACEMENT INSTALLATION MISTAKES TO AVOID:

BALES SHOULD BE PLACED DIRECTLY AGAINST THE PERIMETER OF THE AREA INLET. THIS ALLOWS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR. BALE AREA INLET BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE AREA INLET BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW UNDER THE AREA INLET BARRIER?
DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
ARE ANY BALES DISLODGED?
ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



STRAW BALE BARRIERS

MATERIAL SPECIFICATION:

BALE SLOPE BARRIERS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, BALE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. BALE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSLOPE SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

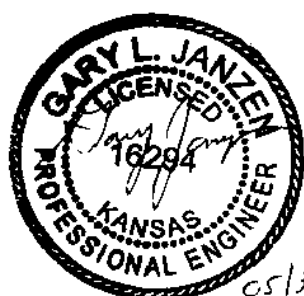
WHEN PRACTICAL, DO NOT PLACE BALE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. CONCENTRATED FLOW OVER A SLOPE BARRIER CREATES A SCOUR HOLE ON THE DOWNSLOPE SIDE OF THE BARRIER. THE SCOUR HOLE EVENTUALLY UNDERMINES THE BALES AND THE BARRIER FAILS. DO NOT PLACE BALE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT. BALE SLOPE BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
DOES WATER FLOW UNDER THE SLOPE BARRIER?
DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
ARE ANY BALES DISLODGED?
ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

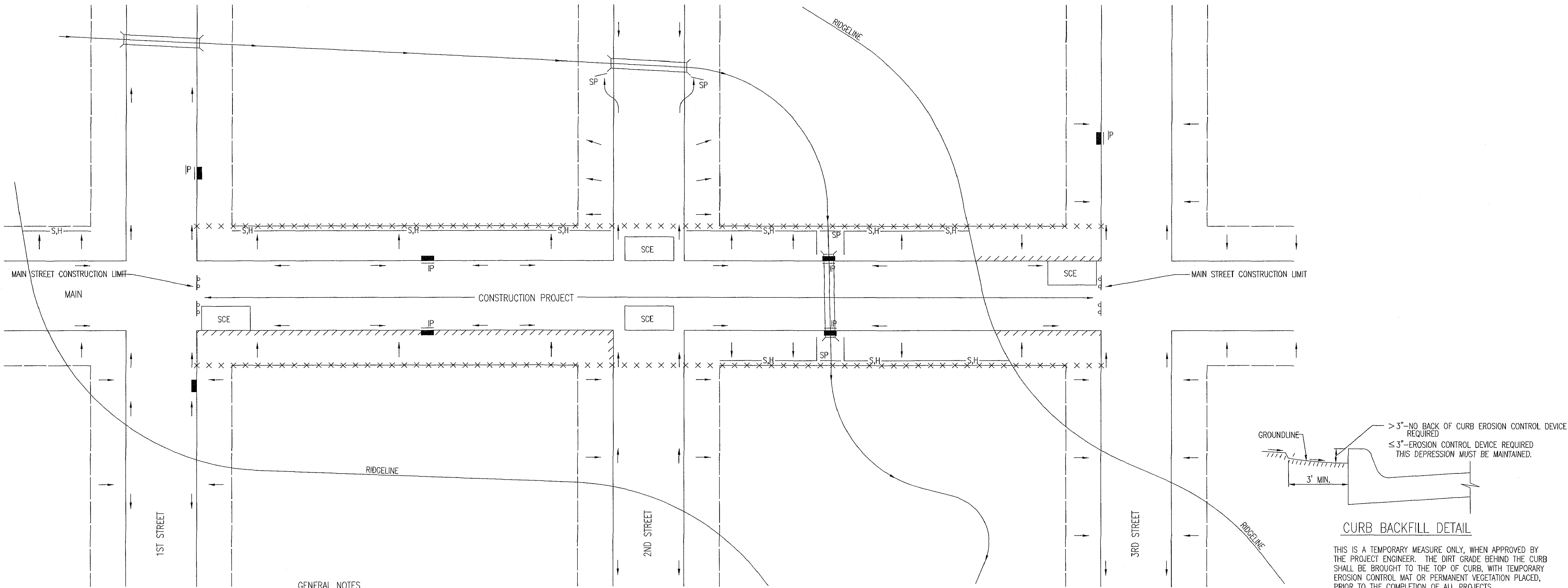
REVISION DATE: MAY 2013



STRAW BALE DITCH CHECK AND BARRIER DETAILS			
CITY ENGINEER GARY JANZEN, P.E.			
PROJECT NUMBER 472-2022-0858208	OCA NUMBER ####	DATE SEPT. 2023	
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501			SHEET 34 OF 51

GENERAL NOTES

1. THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPES OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
2. EROSION CONTROL DEVICES MUST BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PROCESS AND UNTIL THE DISTURBED EARTH IS RESTABILIZED.
3. IF THE PROJECT WILL DISTURB 1 ACRE OR MORE, A FEDERAL/STATE NPDES STORMWATER PERMIT IS REQUIRED. A DETAILED STORMWATER POLLUTION PREVENTION PLAN, IS REQUIRED. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED TO BE THE MINIMUM TO BE SHOWN IN THE POLLUTION PREVENTION PLAN.
4. FOR PROJECTS DISTURBING LESS THAN 1 ACRE, CONTRACTORS ARE ENCOURAGED TO PREPARE STORMWATER POLLUTION PREVENTION PLANS PRIOR TO CONSTRUCTION. EROSION CONTROL DEVICES MUST BE USED ON ALL PROJECTS.
5. FAILURE TO USE AND MAINTAIN EROSION CONTROL DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE CONTRACTOR TO THE PENALTIES PROVIDED FOR THEREIN.
6. THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT DEVICE OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED AS LONG AS THEY ARE EFFECTIVE AND MAINTAINED.

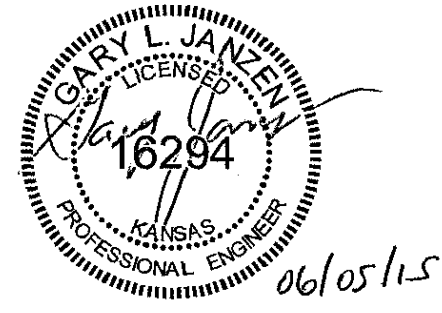



GENERAL NOTES

1. THE INTENT OF ALL EROSION CONTROL DEVICES IS TO KEEP ALL SEDIMENT CONFINED TO THE CONSTRUCTION SITE, AND OUT OF ALL UNDERGROUND PIPES, DITCHES, LAKES, AND OTHER DRAINAGE FACILITIES, AND OFF OF STREETS.
2. THE POINT OF COMPLIANCE IS GENERALLY THE RIGHT-OF-WAY LINES WITHIN THE LIMITS OF CONSTRUCTION.
3. EROSION CONTROL DEVICES WILL BE REQUIRED AT ALL POINTS ALONG THE PROJECT WHERE DISTURBED EARTH CAN DRAIN ONTO PRIVATE PROPERTY.
4. INLET PROTECTION DEVICES WILL BE REQUIRED WHEREVER WATER CAN DRAIN OFF THE PROJECT SITE INTO AN INLET, INCLUDING ANY SIDE STREET INLETS.
5. EROSION CONTROL DEVICES SHALL BE INSTALLED AT CREEK CROSSINGS SO AS TO PREVENT SEDIMENT FROM ENTERING THEREIN.
6. STABILIZED CONSTRUCTION ENTRANCES SHALL BE PROVIDED, AS NEEDED, TO PREVENT MUD FROM TRACKING ONTO STREETS NOT UNDER CONSTRUCTION AND ON STREETS WITHIN THE PROJECT LIMITS IF TRAFFIC IS BEING MAINTAINED THROUGH THE PROJECT.
7. ANY MUD TRACKED ONTO STREETS MUST BE REMOVED AT THE END OF EACH WORK DAY.
8. THE CONTRACTOR WILL BE REQUIRED TO PLACE EROSION CONTROL DEVICES BACK OF CURB, WHENEVER WATER CAN DRAIN OVER CURB, TO KEEP ERODED SOIL OUT OF THE GUTTERLINES, IN ACCORDANCE WITH THE FOLLOWING:
 - A. THE DEVICE REQUIRED WILL BE APPROVED EROSION CONTROL MAT LISTED ON THE CITY'S APPROVED MATERIAL LIST. SAID BLANKET SHALL BE PLACED OVER THE APPROPRIATE SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS. (SEE SOIL EROSION BMPs - BACK OF CURB SEDIMENT BARRIER DETAILS)
 - B. THIS DEVICE SHALL BE INSTALLED IMMEDIATELY WHENEVER THE CURB IS BACKFILLED TO WITHIN 3" OF THE TOP OF CURB. (SEE CURB BACKFILL DETAIL) OTHER BMP'S MAY BE REQUIRED AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB.
 - C. ADDITIONALLY, OTHER EROSION CONTROL DEVICES (HAY BALES, SILT FENCE, ETC.) WILL BE INSTALLED AT LOCATIONS OF CONCENTRATED FLOW RESULTING IN SEDIMENT OVERRUNNING THE MAT.
 - D. SHOULD THE PROJECT PLANS SPECIFY THAT THE RIGHT-OF-WAY IS TO BE SODDED, THE EXCELSIOR MAT WILL NOT BE REQUIRED SO LONG AS THE SOD IS PLACED WITHIN 48 HOURS AFTER CURB BACKFILL REACHES A HEIGHT OF 3" OR LESS FROM TOP OF CURB. (SEE CURB BACKFILL DETAIL)

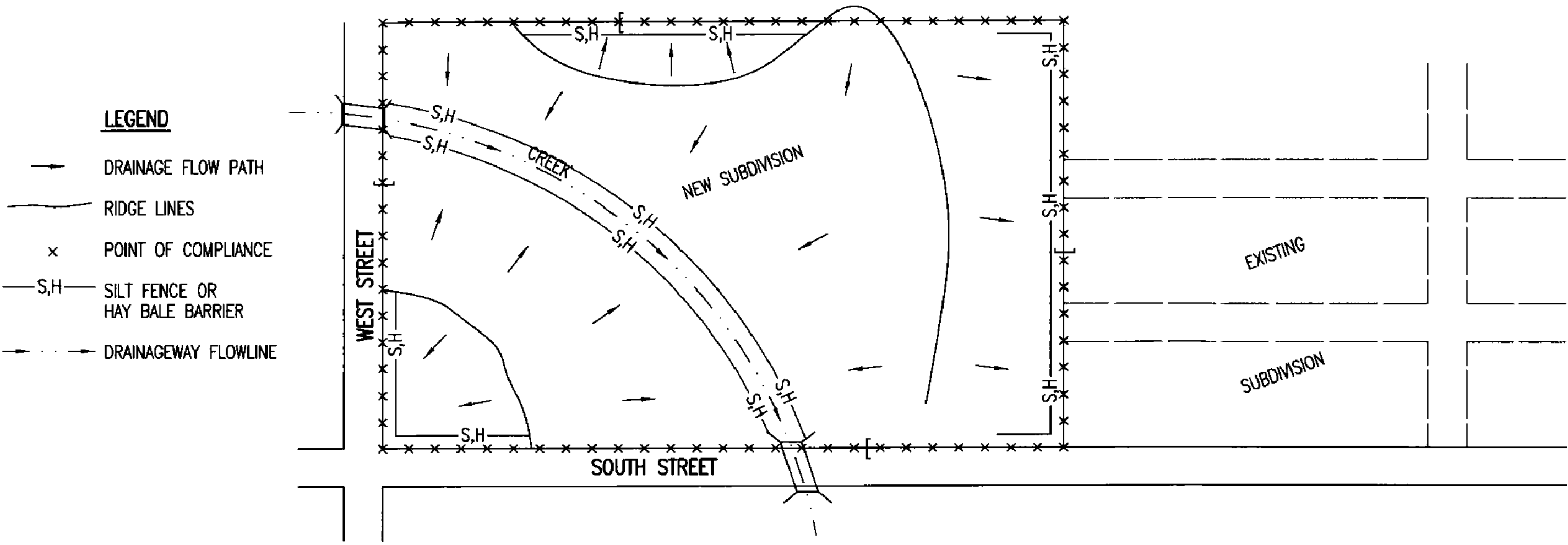
LEGEND

- R-O-W LIMITS
- > DRAINAGE FLOW PATH
- x x x x x R/W LIMIT WITHIN CONSTRUCTION LIMIT
- STORM WATER INLETS
- IP INLET PROTECTION
- S.H- SILT FENCE OR HAY BALE BARRIER
- SP STREAM PROTECTION
- SCE STABILIZED CONSTRUCTION ENTRANCE
- //// BACK OF CURB PROTECTION



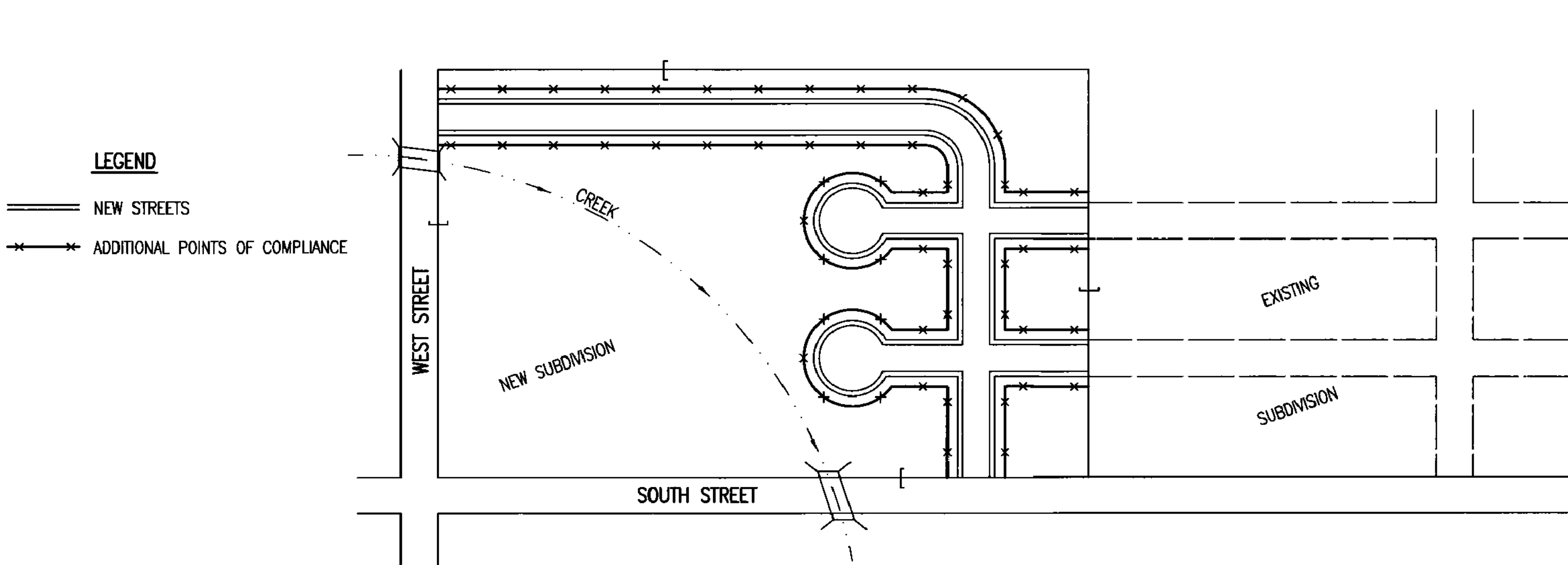
 CITY OF WICHITA PUBLIC WORKS & UTILITIES ENGINEERING DIVISION			
REVISION: JUNE 2015			
STREET IMPROVEMENT PROJECTS			
CITY ENGINEER GARY JANZEN, P.E.			
PROJECT NUMBER 472-2022-0858208	OCA NUMBER ####	DATE SEPT. 2023	
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501			SHEET 35 OF 51

PHASE 1 – INITIAL EARTHWORK AND UTILITIES (EXCEPT STORM SEWER)



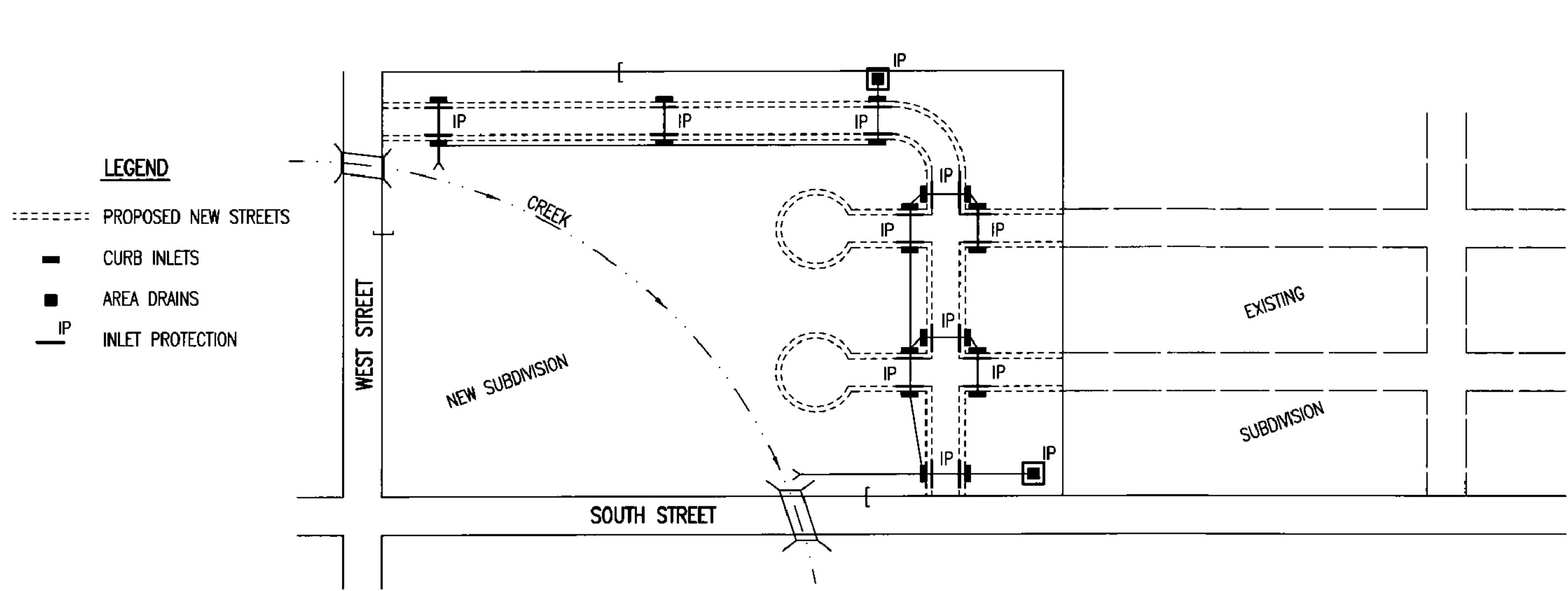
1. DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, THE POINTS OF COMPLIANCE ARE THE PERIMETER BOUNDARIES AND ANY DRAINAGE WAYS OR STORM SEWERS DRAINING THROUGH OR FROM THE SITE. SHOULD LAKES BE CONSTRUCTED WITHIN THE SUBDIVISION THAT WILL DISCHARGE DURING STORMS, THEY ARE ALSO A POINT OF COMPLIANCE.
2. HAY BALES OR SILT FENCE MUST BE CONSTRUCTED ALONG THE PROPERTY LINE WHERE ON SITE WATER CAN DRAIN OFF THE PROPERTY. THESE EROSION CONTROL DEVICES WILL ALSO BE INSTALLED ALONG ANY DRAINAGE DITCH OR LAKE THAT CAN DISCHARGE.
3. SHOULD SILT OR SEDIMENT ENTER THE DITCHES OR STREETS ON THE ADJACENT BOUNDARY STREETS, APPROPRIATE EROSION CONTROL DEVICES WILL BE PLACED WITHIN THE SUBDIVISION TO PREVENT THIS.
4. ANY MUD TRACKED ONTO ADJACENT STREETS WILL BE REMOVED WITHIN 48 HOURS OR BY FRIDAY AT 6:00 PM, WHICHEVER IS EARLIER.
5. CONTRACTORS WORKING WITHIN THE SITE WILL NOT BE REQUIRED TO USE INDIVIDUAL EROSION CONTROL DEVICES AS LONG AS THOSE SPECIFIED ABOVE ARE IN PLACE AND EFFECTIVE. CONTRACTORS WORKING ON THE BOUNDARY LINE STREETS OR ON ADJACENT PROPERTIES TO EXTEND UTILITIES ARE EXPECTED TO USE EROSION CONTROL DEVICES AT THEIR WORK LOCATIONS, AS NEEDED.
6. UTILIZE STABILIZED CONSTRUCTION ENTRANCE AT ENTRANCE AND EXIT ONTO ANY EXISTING PUBLIC STREETS.
7. IF THE INITIAL EARTH WORK AND UTILITIES ARE DONE AS PART OF A PUBLIC IMPROVEMENT PROJECT, THESE EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS SPECIFIED IN THE INDIVIDUAL PROJECT CONTRACTS. THE CONTRACTOR WILL MAINTAIN THE DEVICES UNTIL COMPLETION OF THE CONTRACT, AT WHICH TIME THE DEVELOPER WILL ASSUME MAINTENANCE RESPONSIBILITIES. IF THESE CONTRACTS ARE NOT PUBLIC IMPROVEMENT PROJECTS, THE DEVELOPER WILL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THESE DEVICES.
8. WITHIN 14 DAYS OF COMPLETION OF EARTHWORK ACTIVITIES IN ANY GIVEN AREA, THAT AREA SHALL BE TEMPORARILY OR PERMANENTLY SEEDED AND MULCHED.

PHASE 3 – STREET CONSTRUCTION



1. DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, NEW STREETS ARE INSTALLED. ALL EROSION CONTROL DEVICES INSTALLED DURING PHASE 1 AND 2 MUST STILL BE MAINTAINED. THE POINT OF COMPLIANCE NOW SHIFTS TO THE BACK OF CURB ALONG EACH STREET.
2. CURB OPENING INLET PROTECTION:
A. SUMP AREAS – INLET PROTECTION SHALL BE PROVIDED WHEN STREET SUBGRADE WORK IS COMPLETED.
B. NON-SUMP LOCATIONS – PROVIDE INLET PROTECTION AS SOON AS BASE COURSE ASPHALT IS INSTALLED, BEFORE THE SURFACE COURSE LIFT.
3. EROSION CONTROL DEVICES WILL BE REQUIRED BACK OF CURB WHEREVER WATER CAN FLOW OVER THE CURB AND THE CURB HAS BEEN BACKFILLED TO WITHIN 3" OR LESS OF THE TOP OF CURB (SEE CURB BACKFILL DETAIL). FOR CURBS NOT YET ENTIRELY BACKFILLED (3" OR MORE BELOW TOP OF CURB), ADDITIONAL DEVICES WILL BE REQUIRED AT POINTS WHERE WATER BREAKS OVER CURB WHICH COULD RESULT IN THE PLACEMENT OF SEDIMENT IN THE GUTTER.
4. SEE DETAIL SHEET FOR BACK OF CURB PROTECTION.
5. THE BACK OF CURB PROTECTION SPECIFIED ON THIS PLAN MAY HAVE TO BE SUPPLEMENTED WITH HAY BALE OR SILT FENCE EROSION CONTROL DEVICES AT LOCATIONS WHERE CONCENTRATED FLOW RESULTS IN SEDIMENT BEING CARRIED OVER THE EXCELSIOR MATS.
6. THE STREET CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING BACK OF CURB EROSION CONTROL DEVICES.
7. THE INDIVIDUAL LOT OWNERS WILL BE RESPONSIBLE FOR MAINTAINING THE BACK OF CURB EROSION CONTROL DEVICES IN FRONT OF THEIR LOTS UNTIL SUCH TIME AS ADJACENT DISTURBED EARTH IS STABILIZED WITH GRASS OR SOD.

PHASE 2 – INSTALLATION OF STORM SEWER

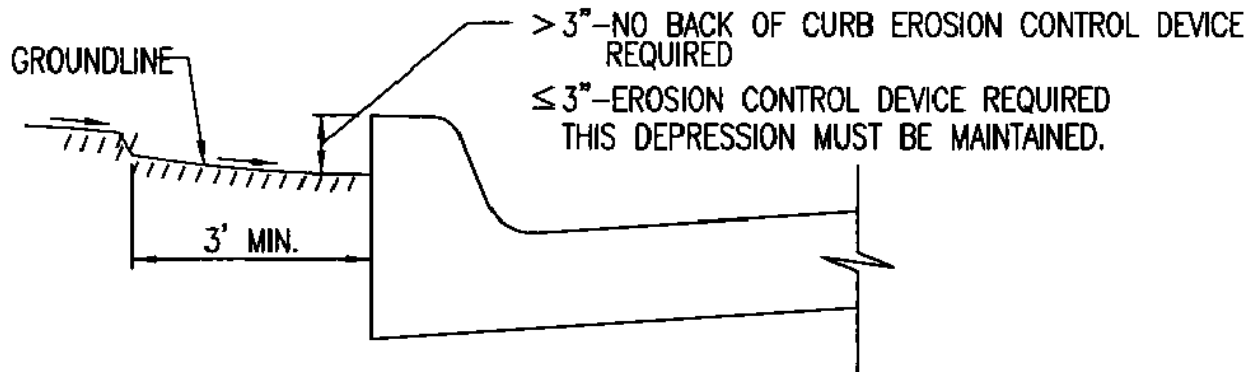


1. DURING THIS PHASE OF SUBDIVISION DEVELOPMENT, ALL EROSION CONTROL DEVICES REQUIRED IN PHASE 1 SHALL REMAIN IN PLACE AND BE MAINTAINED.
2. AS NEW STORM SEWERS, WITH INLETS, ARE INSTALLED, THE STORM SEWERS MUST NOW BE PROTECTED SO ALL NEW INLETS BECOME POINTS OF COMPLIANCE.
3. AREA DRAINS – AS SOON AS WATER CAN FLOW INTO THESE DRAINS, HAY BALE OR SILT FENCE PROTECTION WILL BE INSTALLED AROUND THEM.
4. CURB OPENING INLETS – AS SOON AS WATER CAN FLOW INTO THESE DRAINS, INLET PROTECTION DEVICES MUST BE INSTALLED. IF WATER CANNOT FLOW INTO CURB INLETS UNTIL STREET CONSTRUCTION IS COMPLETE, THEN STREET CONTRACTOR WILL INSTALL INLET PROTECTION. SEE PHASE 3 – STREET CONSTRUCTION.
5. THE STORM SEWER CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING THESE DEVICES.
6. THE SUBDIVISION DEVELOPER WILL MAINTAIN THESE EROSION CONTROL DEVICES ONCE INSTALLED.
7. ALL DISTURBED GROUND WILL BE FINAL GRADED AND TEMPORARILY OR PERMANENTLY SEEDED WITHIN 14 DAYS IF COMPLETION OF WORK IN ANY GIVEN PART OF THE SUBDIVISION.
8. ONCE ALL DISTURBED GROUND DRAINING TO AN INLET HAS BEEN RESTABILIZED WITH GRASS OR SOD, THE SUBDIVISION DEVELOPER WILL BE RESPONSIBLE FOR PERMANENTLY REMOVING THE INLET PROTECTION.

GENERAL NOTES

1. THE INTENT OF ALL EROSION CONTROL DEVICES IS TO PREVENT ERODED SOIL FROM ENTERING DITCHES, STORM SEWERS, LAKES, STREETS OR ANY OTHER OTHER DRAINAGE FEATURE.
2. THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPE OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
3. EROSION CONTROL DEVICES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS TO REMAIN EFFECTIVE. MAINTENANCE SHALL BE AS INDICATED ON SOIL EROSION BMP'S DETAIL SHEETS.
4. PERSONS DESTROYING EROSION CONTROL DEVICES SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING THEM OR INSTALLING SUITABLE REPLACEMENT DEVICES.
5. THE DEVELOPMENT OF ANY SUBDIVISION THAT DISTURBS 1 ACRE OR MORE WILL REQUIRE A FEDERAL/STATE NPDES STORMWATER PERMIT. THE PREPARATION OF A STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. EROSION CONTROL DEVICES ARE REQUIRED. THE DETAILS SHOWN ON THIS SHEET ARE THE MINIMUM STANDARDS TO BE SHOWN ON POLLUTION PREVENTION PLANS.
6. FOR SUBDIVISIONS SMALLER THAN 1 ACRE, SOIL EROSION DEVICES ARE REQUIRED. ALSO, DEVELOPERS AND CONTRACTORS ARE ENCOURAGED TO DEVELOP POLLUTION PREVENTION PLANS FOR EACH PROJECT PRIOR TO CONSTRUCTION.
7. FAILURE TO USE AND MAINTAIN SOIL EROSION DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE SUBDIVISION DEVELOPER AND CONTRACTORS TO THE PENALTIES PROVIDED THEREIN.
8. THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE DEVICES OTHER THAN THAT SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED SO LONG AS THEY ARE EFFECTIVE AND MAINTAINED.
9. A STABILIZED EARTH SURFACE IS DEFINED AS ONE THAT IS HARD SURFACED WITH CONCRETE, ASPHALT, OR THE LIKE, OR ONE ON WHICH 70% OF THE GRASS HAS GERMINATED ON THE ENTIRE SURFACE.

SEE DETAIL SHEET FOR
BACK OF CURB PROTECTION DETAIL



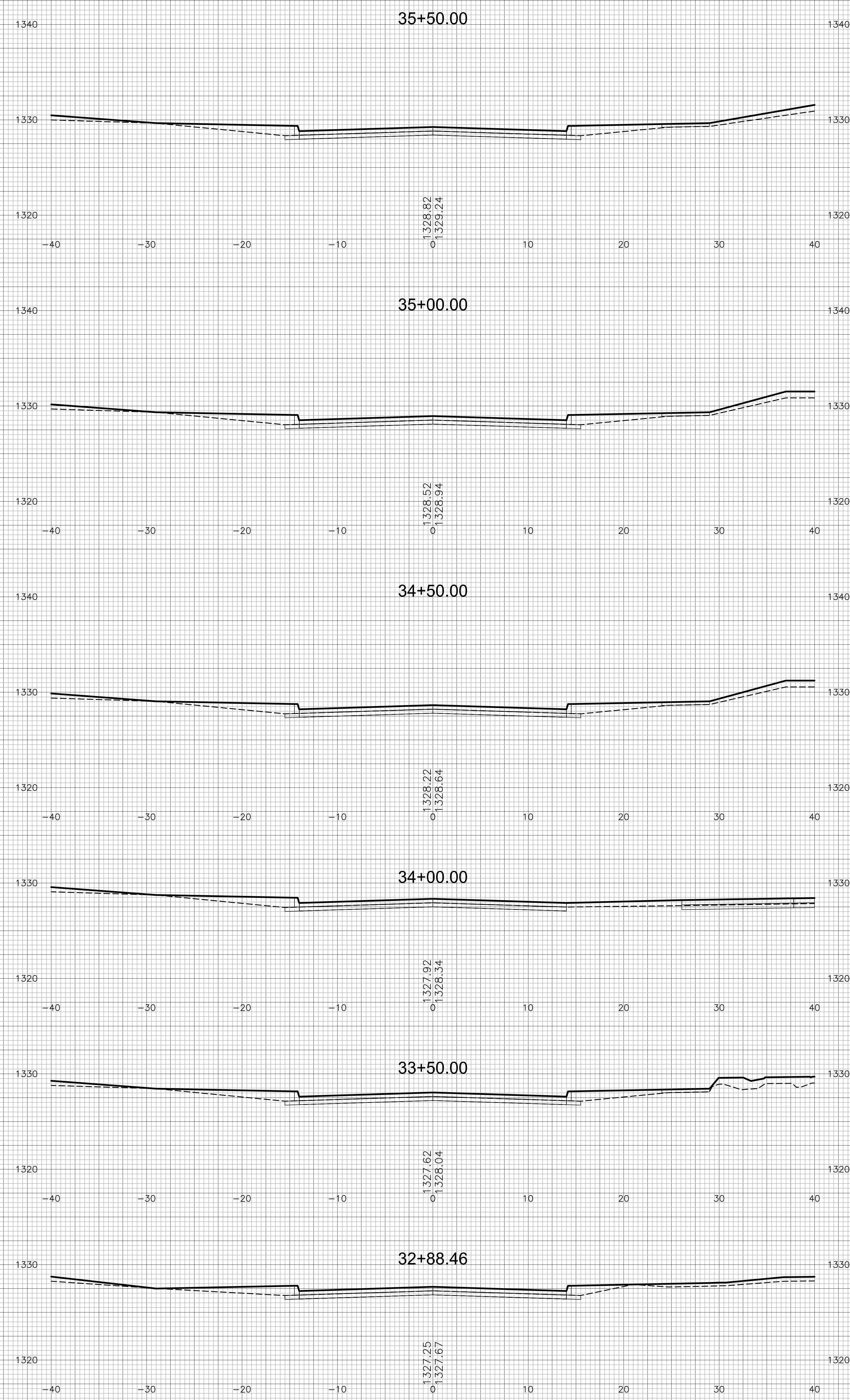
CURB BACKFILL DETAIL
(STREET CONSTRUCTION ONLY)

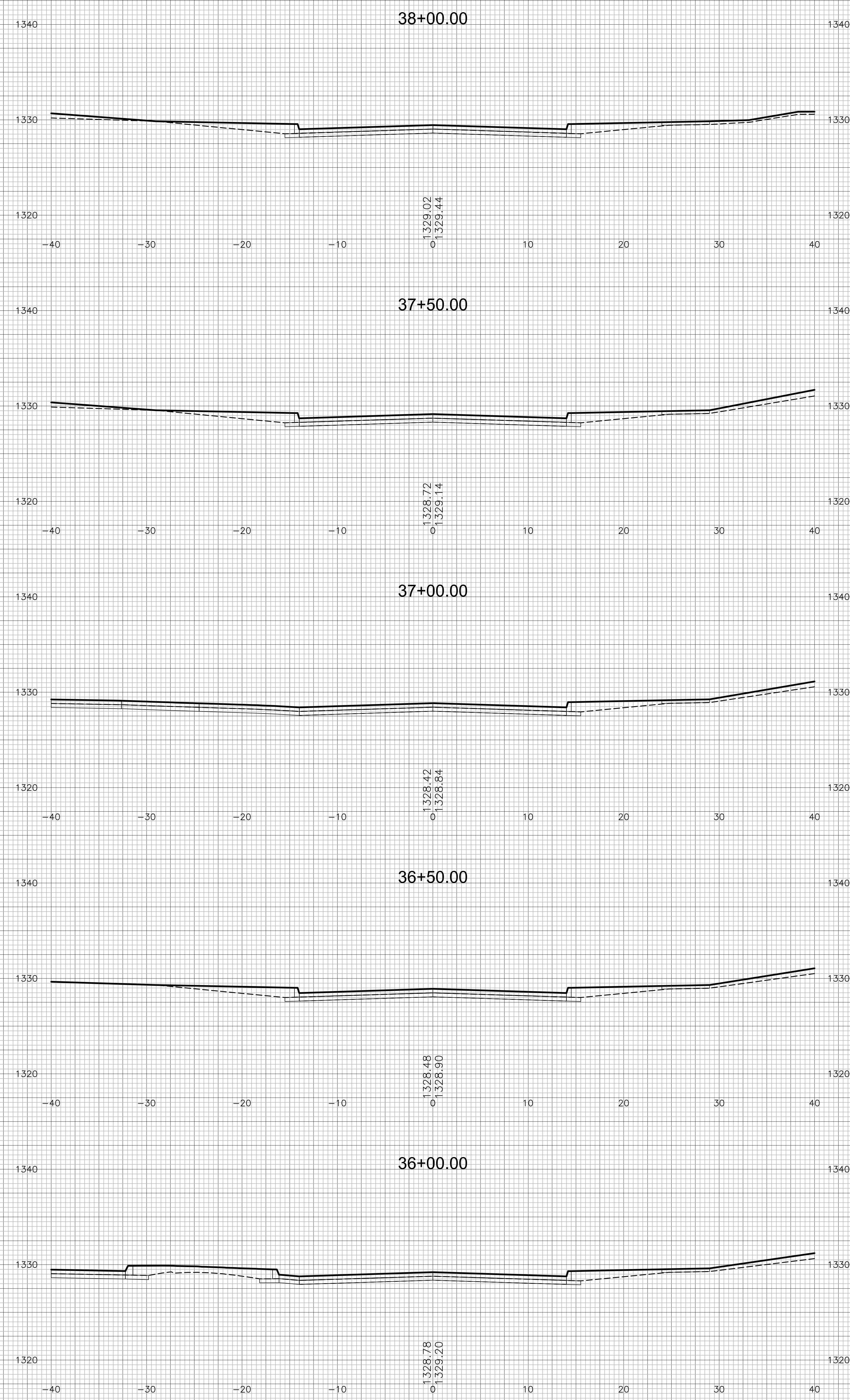
THIS IS A TEMPORARY MEASURE ONLY, WHEN APPROVED BY THE PROJECT ENGINEER. THE DIRT GRADE BEHIND THE CURB SHALL BE BROUGHT TO THE TOP OF CURB, WITH TEMPORARY EROSION CONTROL MAT OR PERMANENT VEGETATION PLACED, PRIOR TO THE COMPLETION OF ALL PROJECTS.

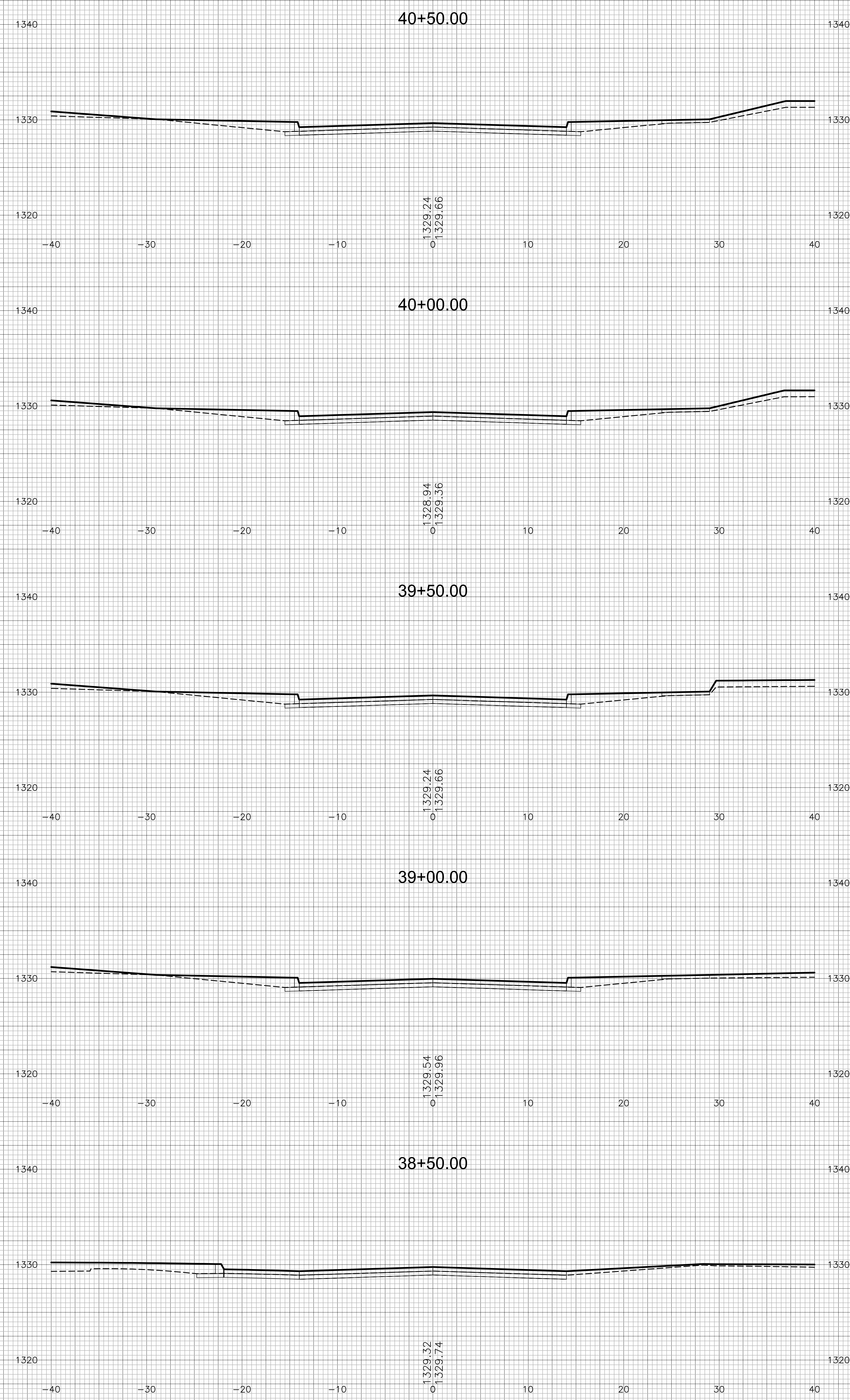
REVISION DATE: MAY 2013

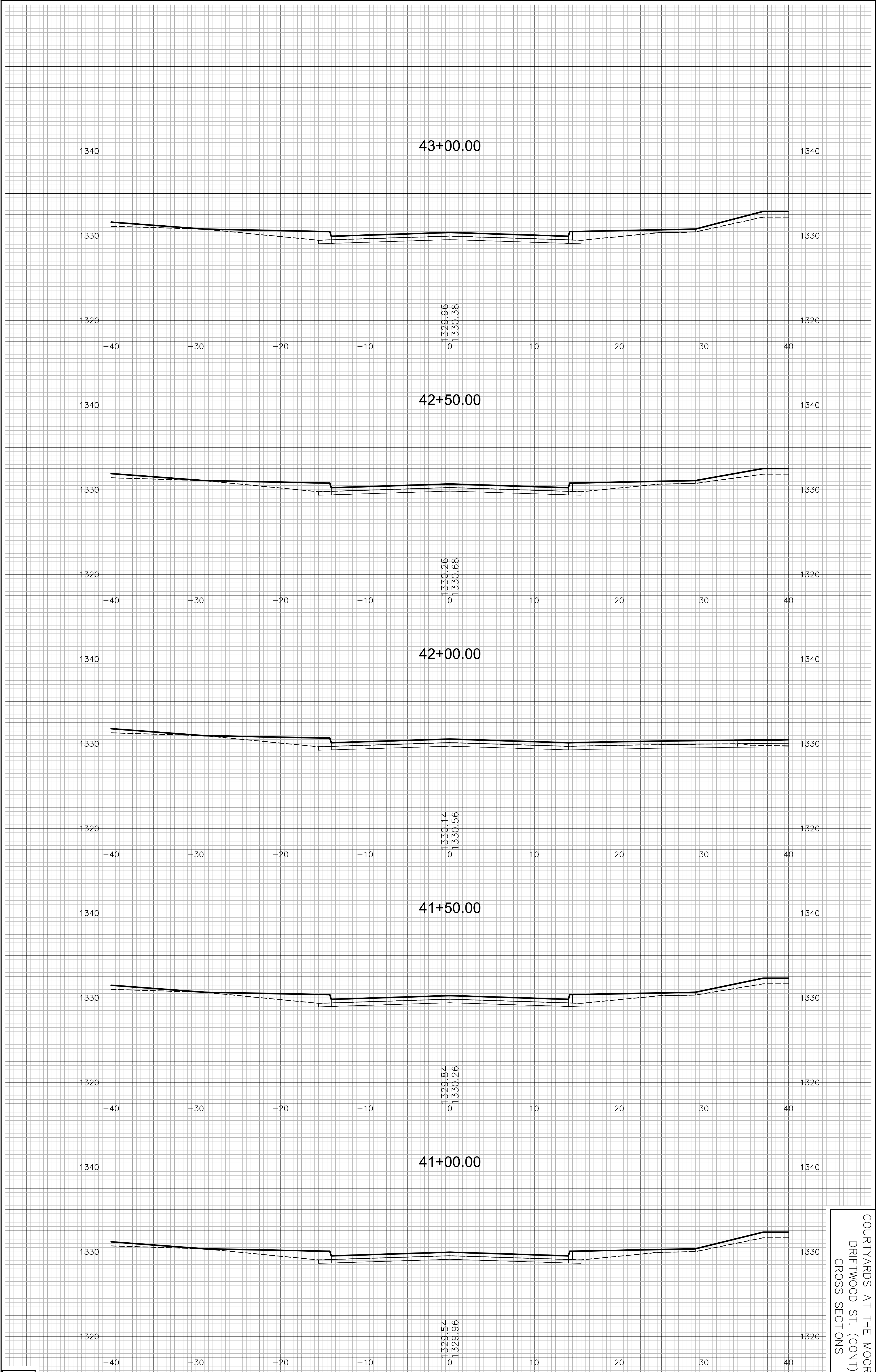


SUBDIVISION DEVELOPMENT PROCESS			
CITY ENGINEER GARY JANZEN, P.E.			
PROJECT NUMBER 472-2022-0858208	OCA NUMBER ####	DATE SEPT. 2023	
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501			SHEET 36 OF 51

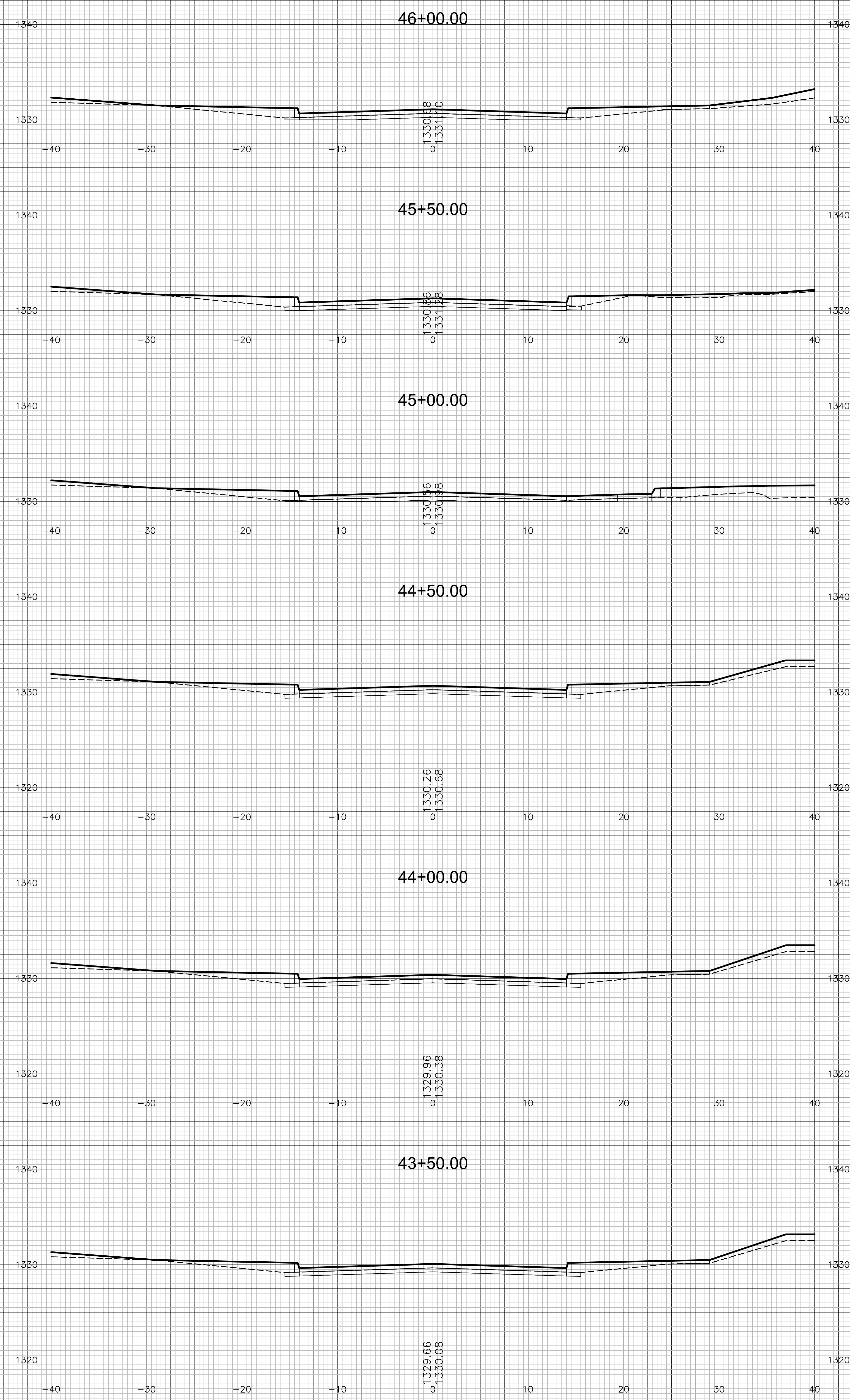






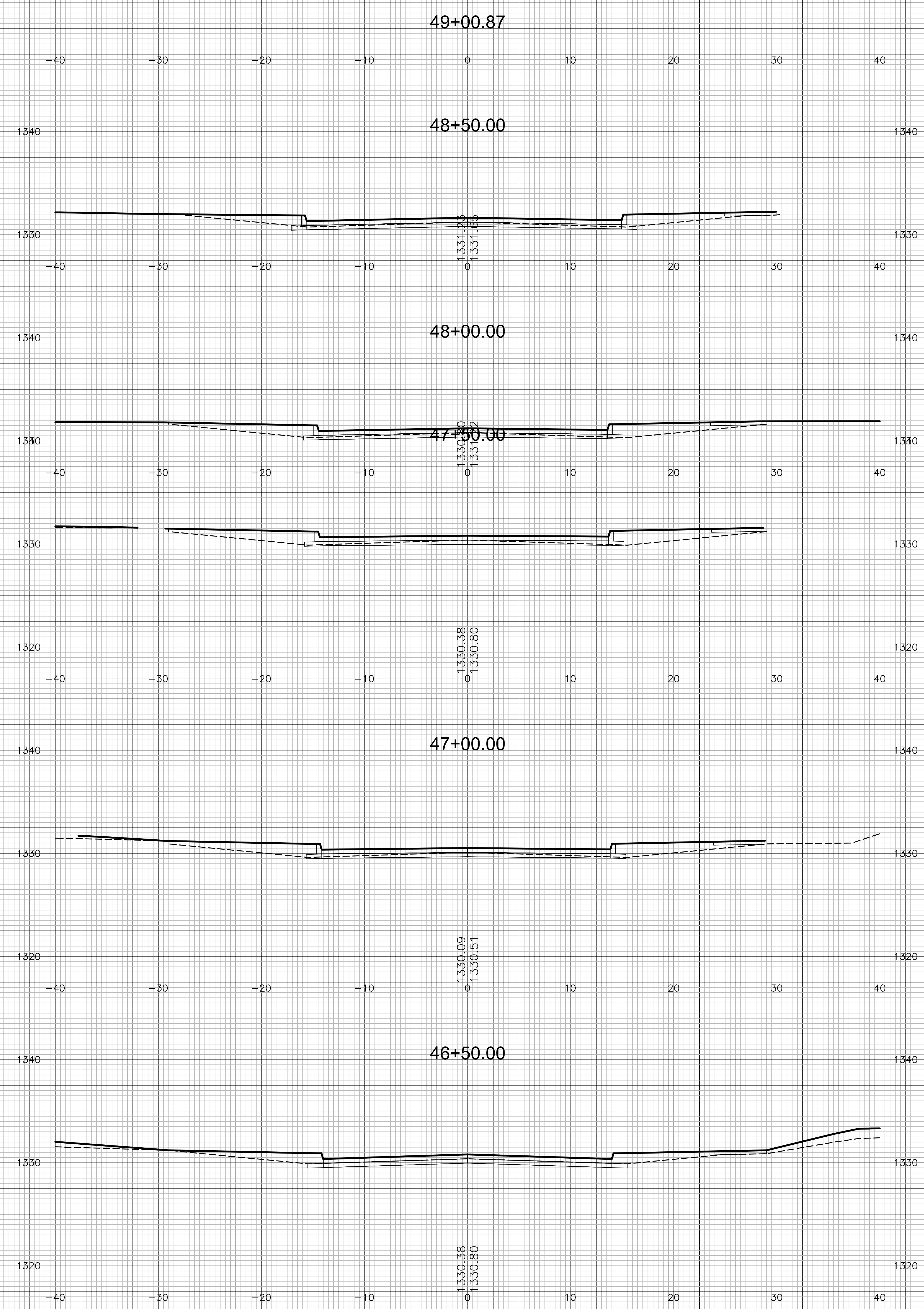


COURTYARDS AT THE MOORINGS
DRIFTWOOD ST. (CONT)
CROSS SECTIONS

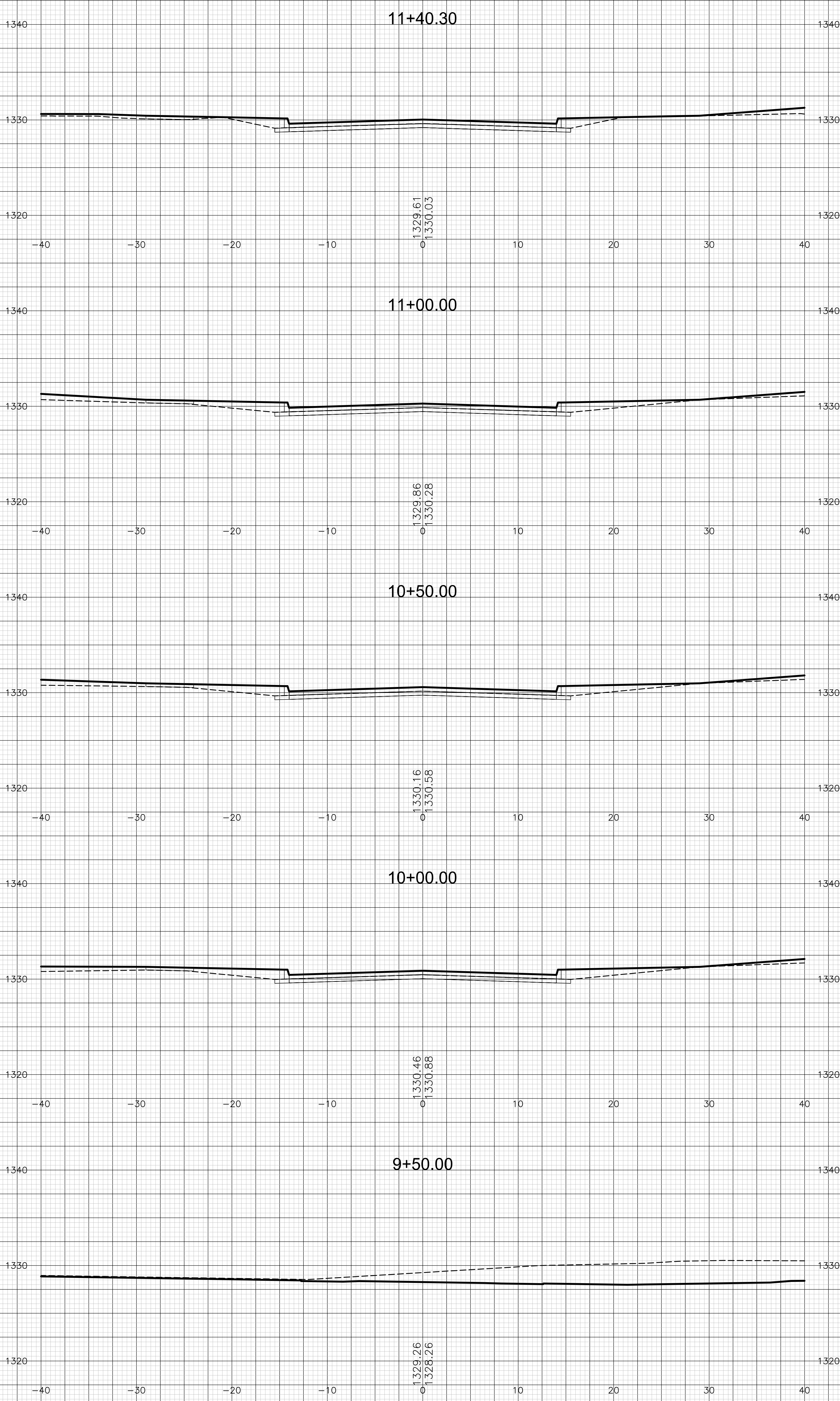


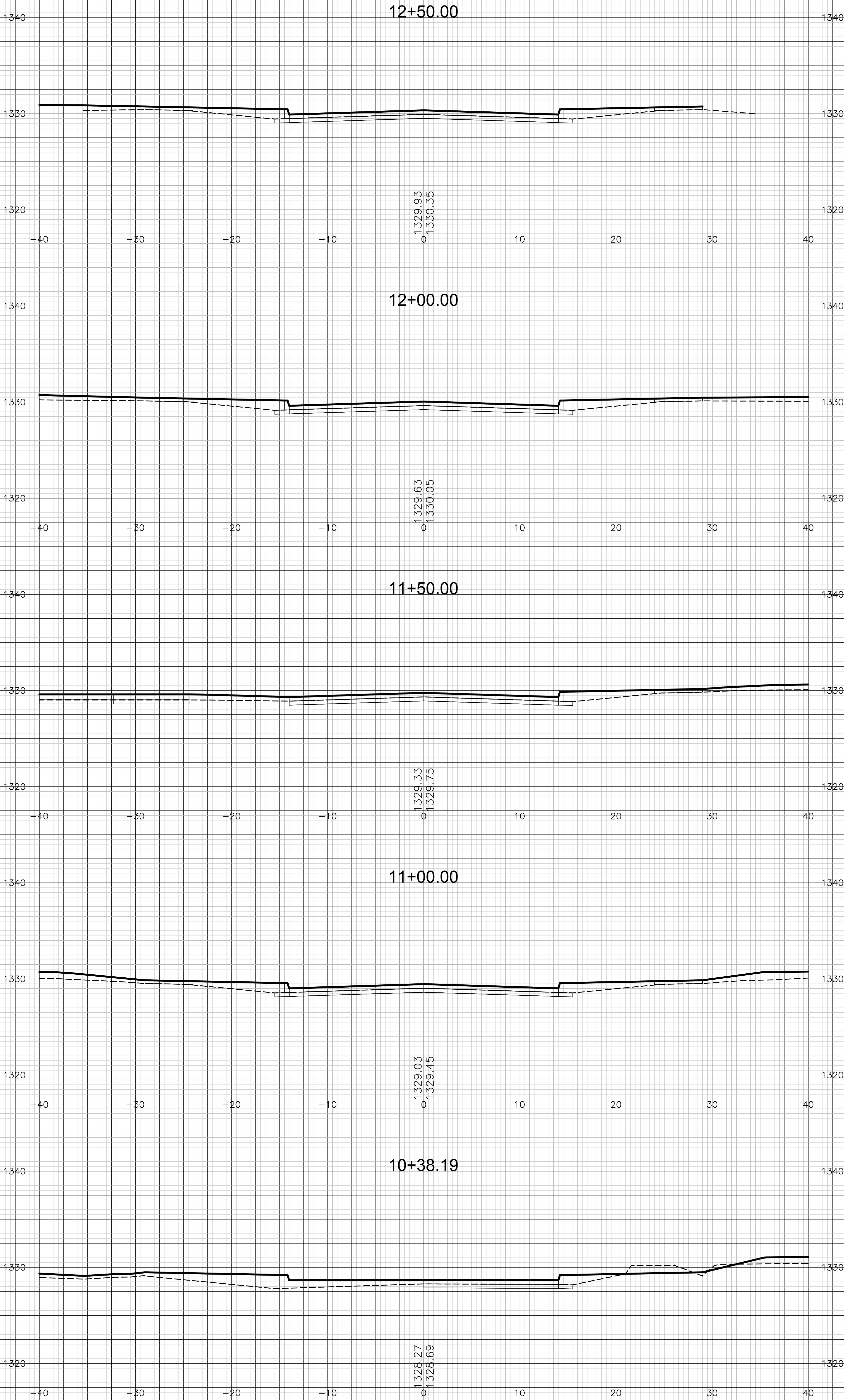
COURTYARDS AT THE MOORINGS
DRIFTWOOD ST. (CONT)
CROSS SECTIONS

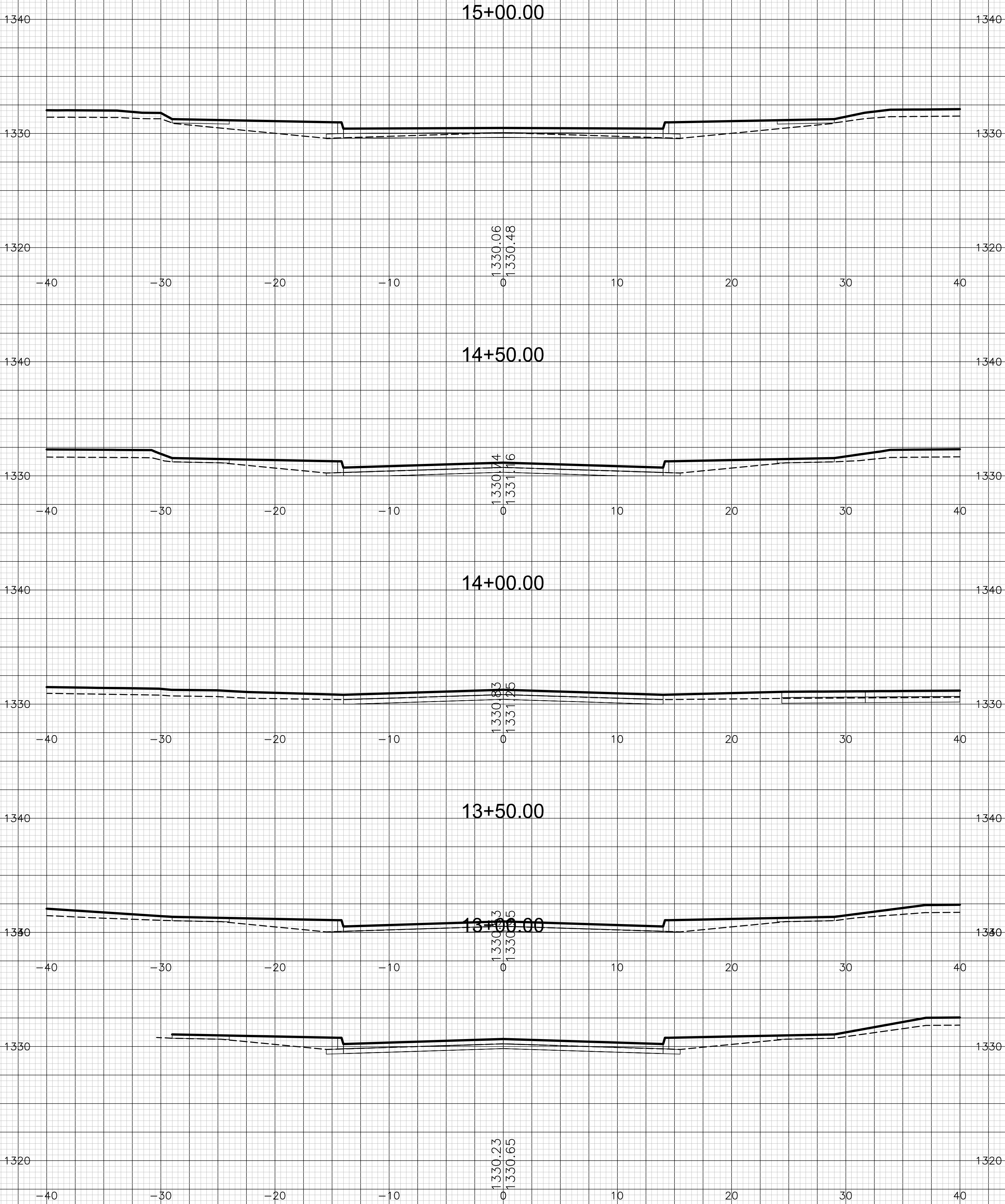
DRIFTWOOD ST.



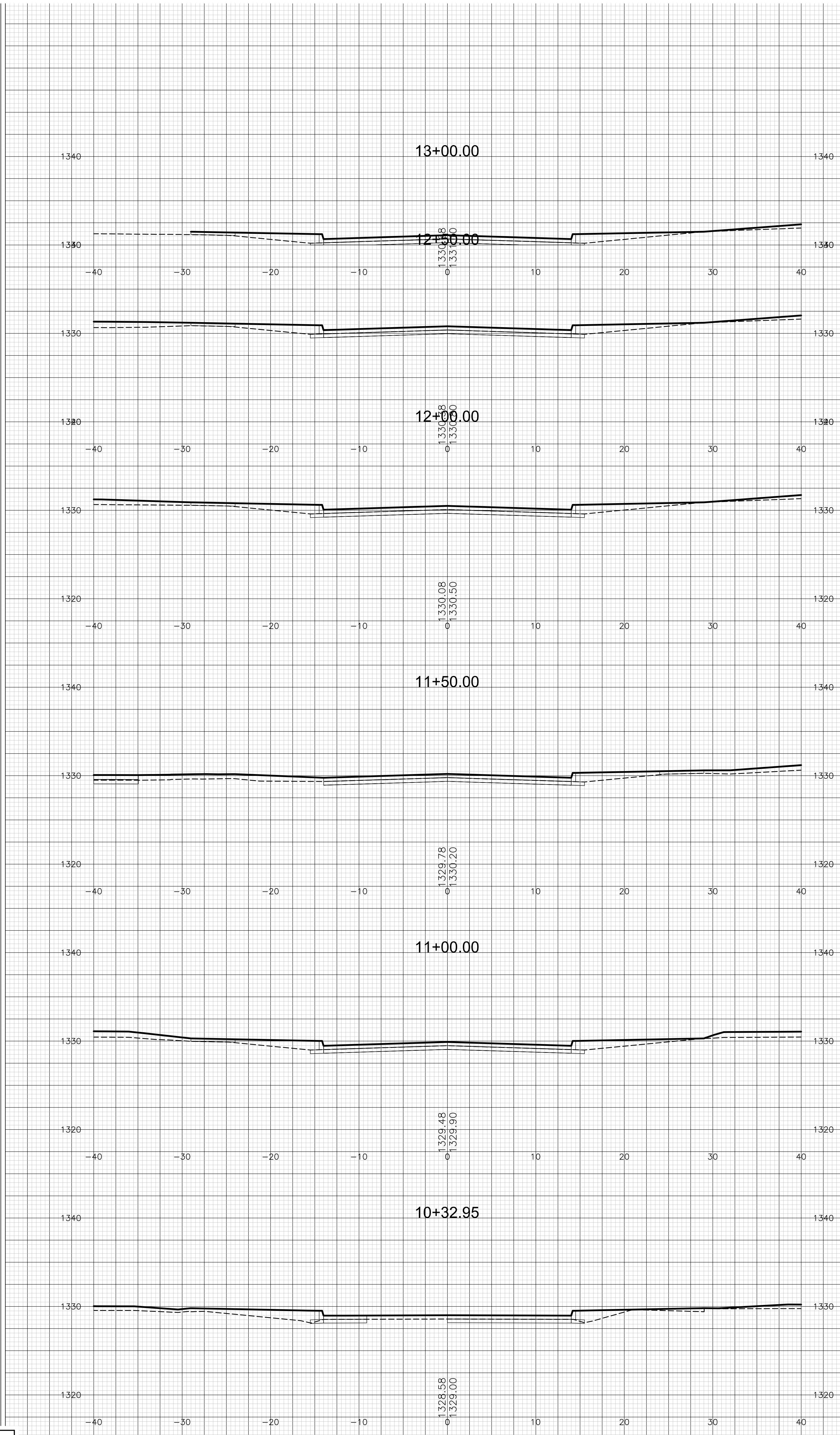
DRIFTWOOD ST.



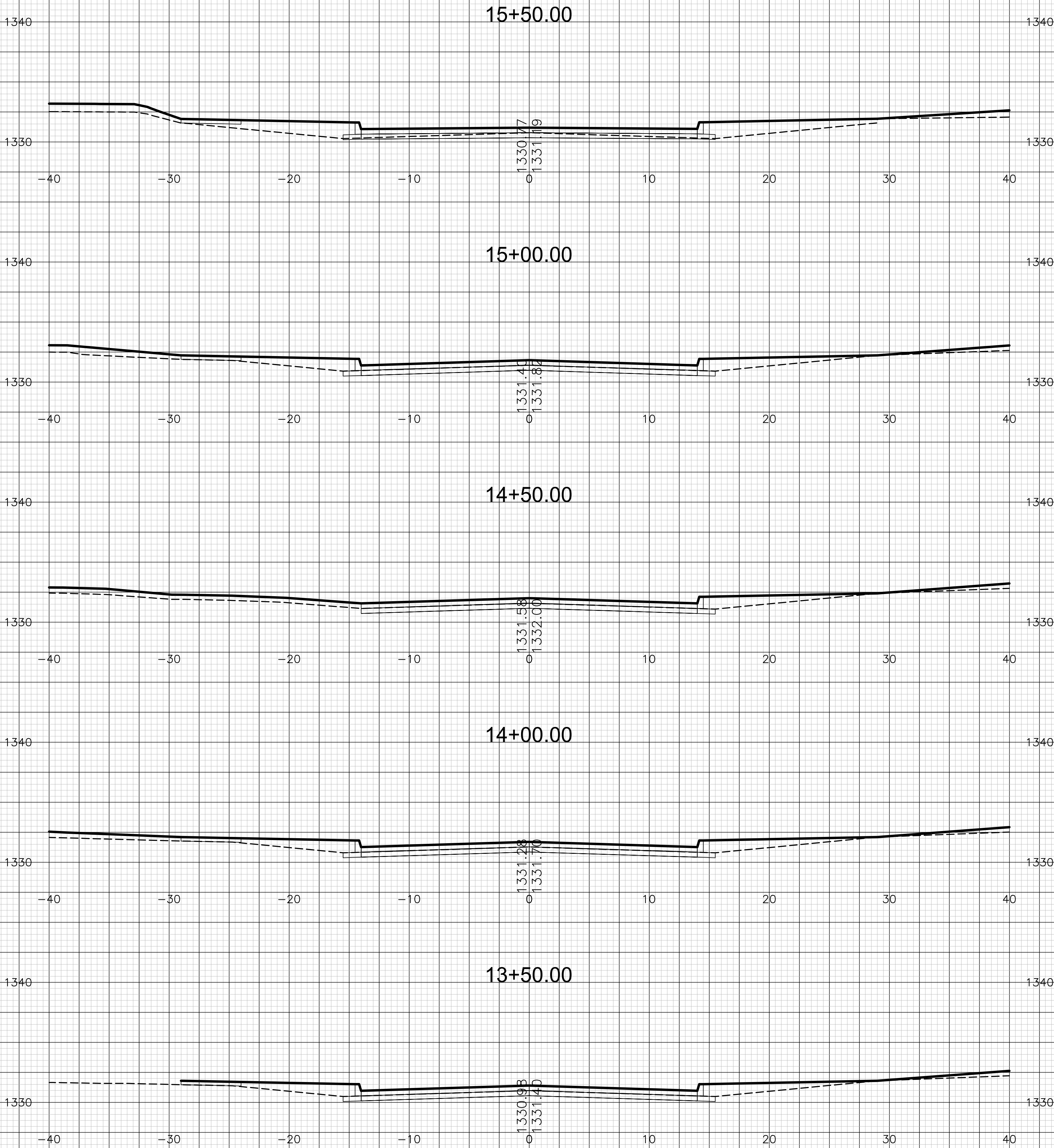




HARBORSIDE DR.

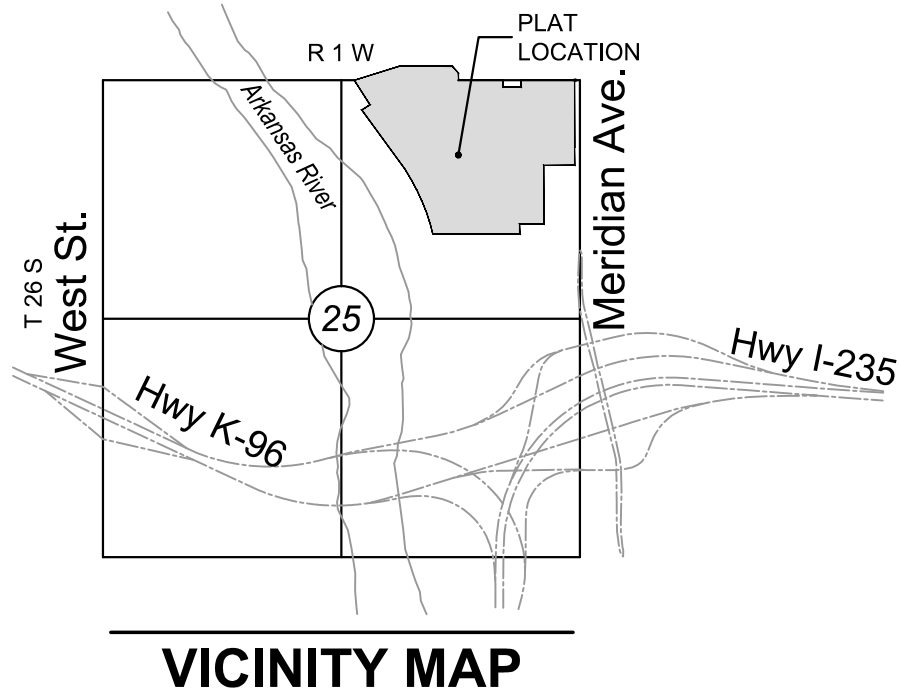


SANDKEY ST.



SANDKEY ST.

FINAL PLAT
COURTYARDS AT THE MOORINGS ADDITION
AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS



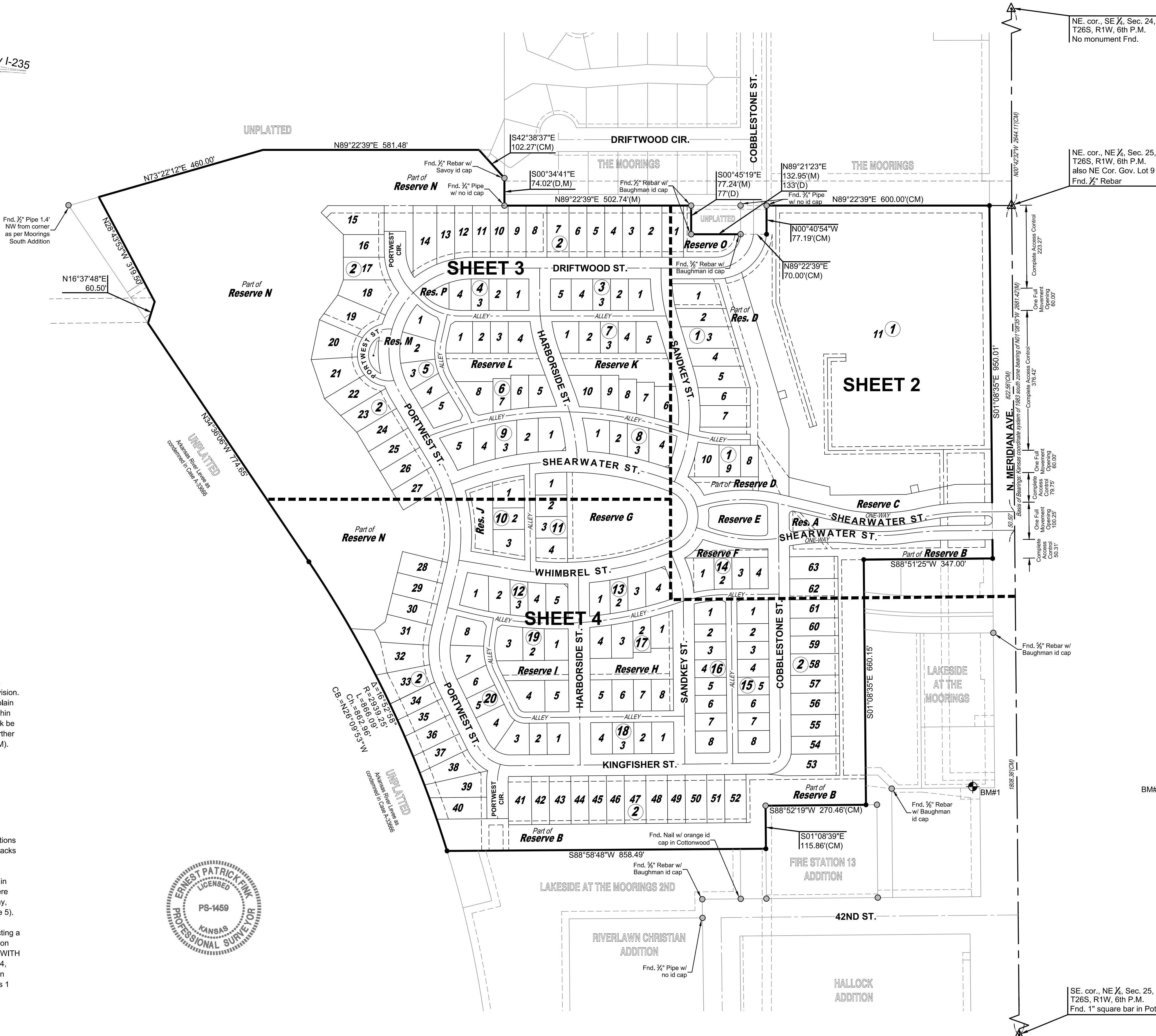
MINIMUM PAD ELEVATIONS LOWEST OPENINGS		
LOT(S) inclusive	BLOCK	ELEVATION NAVD 88
1-10	1	1329.4
1-63	2	1329.4
1-5	3	1329.4
1-4	4	1329.4
1-5	5	1329.4
1-8	6	1329.4
1-10	7	1329.4
1-4	8	1329.4
1-5	9	1329.4
1-3	10	1329.4
1-4	11	1329.4
1-5	12	1329.4
1-4	13	1329.4
1-4	14	1329.4
1-8	15	1329.4
1-8	16	1329.4
1-8	17	1329.4
1-4	18	1329.4
1-5	19	1329.4
1-8	20	1329.4

FLOODWAY NOTE:

FEMA floodplain and regulatory floodway boundaries are subject to periodic change and such change may affect the intended land use within the subdivision. Portions of the land within the plat boundaries are encumbered with a floodplain at the time of final platting. No permanent buildings shall be constructed within the floodplain, nor any fill, change in grade, creation of channel or other work be carried on without the written permission of the Floodplain Manager. For further information, see current applicable FEMA Flood Insurance Rate Maps (FIRM). See drainage plan/report on file with the City of Wichita for the base flood elevations and floodway/floodplain delineations.

NOTES:

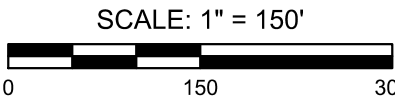
- This plat of "Courtyards at the Moorings Addition" is subject to the conditions of the Planned Unit Development PUD No. 95. The platted building setbacks are established with the PUD or as shown hereon.
- Alley Lot Access Controls: As to Lots 8, 9, and 10, Block 1, all Lots within Block 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, and 20, there shall be no motor vehicle access to said Lots to public street rights-of-way, except, by way of the platted public Alleys (see Owner's Certificate, Page 5).
- A portion of a Pipeline Easement recorded on Film 512, Page 167, affecting a portion of Reserve N; TOGETHER WITH a Pipeline Easement recorded on Film 514, Page 479, affecting Lots 14, 15, and 16, Block 2; TOGETHER WITH a portion of an Electric Utility Easement recorded on Film 1192, Page 314, affecting Lot 11, Block 1 and Lots 1 and 2, Block 2; TOGETHER WITH an Electric Utility Easement recorded on Film 1192, Page 315, affecting Lots 1 and 2 Block 2 are not depicted hereon.



LEGEND

Date of Survey: 12/20/2021

- △ = Section Corner Monument Found
- = Found 3/4" rebar w/ MKEC CLS 39 id. cap or see annotation for type
- = Set 3/4" rebar w/ MKEC CLS 39 id. cap
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- (M) = Measured
- (P) = Platted
- (D) = Described
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- (CD) = Calculated from Described
- Drng. = Drainage
- Util. = Utility
- Sdwb. = Sidewalk
- St. = Street
- Esmnt. = Easement
- 1 = Lot
- ① = Block



Basis of Bearings: Kansas coordinate system of 1983 south zone bearing of N01°08'35"W on the east line of Northeast Quarter, Section 25, Township 26 South, Range 1 West of the Sixth Principal Meridian, Sedgwick County, Kansas. This plat is surveyed and platted on NAD83 using Kansas state plane south zone coordinates, modified to the surface, having a combined adjustment scale factor of 1.000120014401728

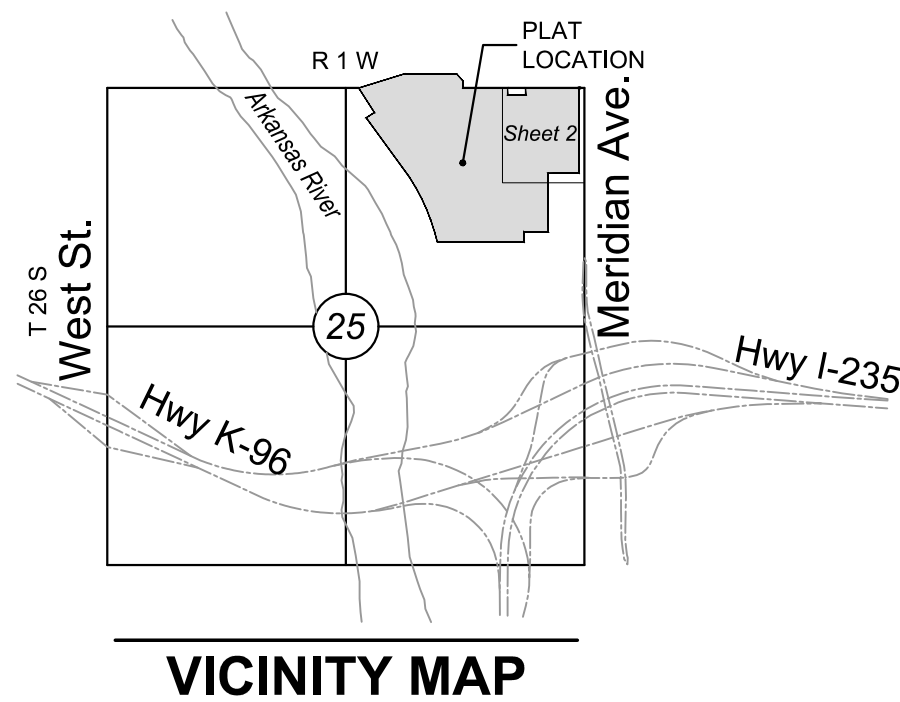
BENCHMARK

- BM#1 Chiseled "X" top of north curb on north access drive to Meritrust Credit Union from Meridian Ave. 45' northwest of sanitary manhole, 40 feet east of back of curb corner, 29 feet north of south access drive back of curb. Elev.=1329.89 NAVD88

FOR REFERENCE ONLY



FINAL PLAT
COURTYARDS AT THE MOORINGS ADDITION
AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

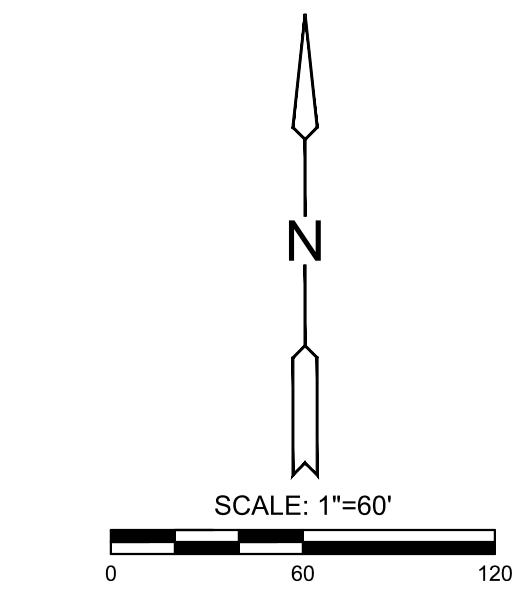
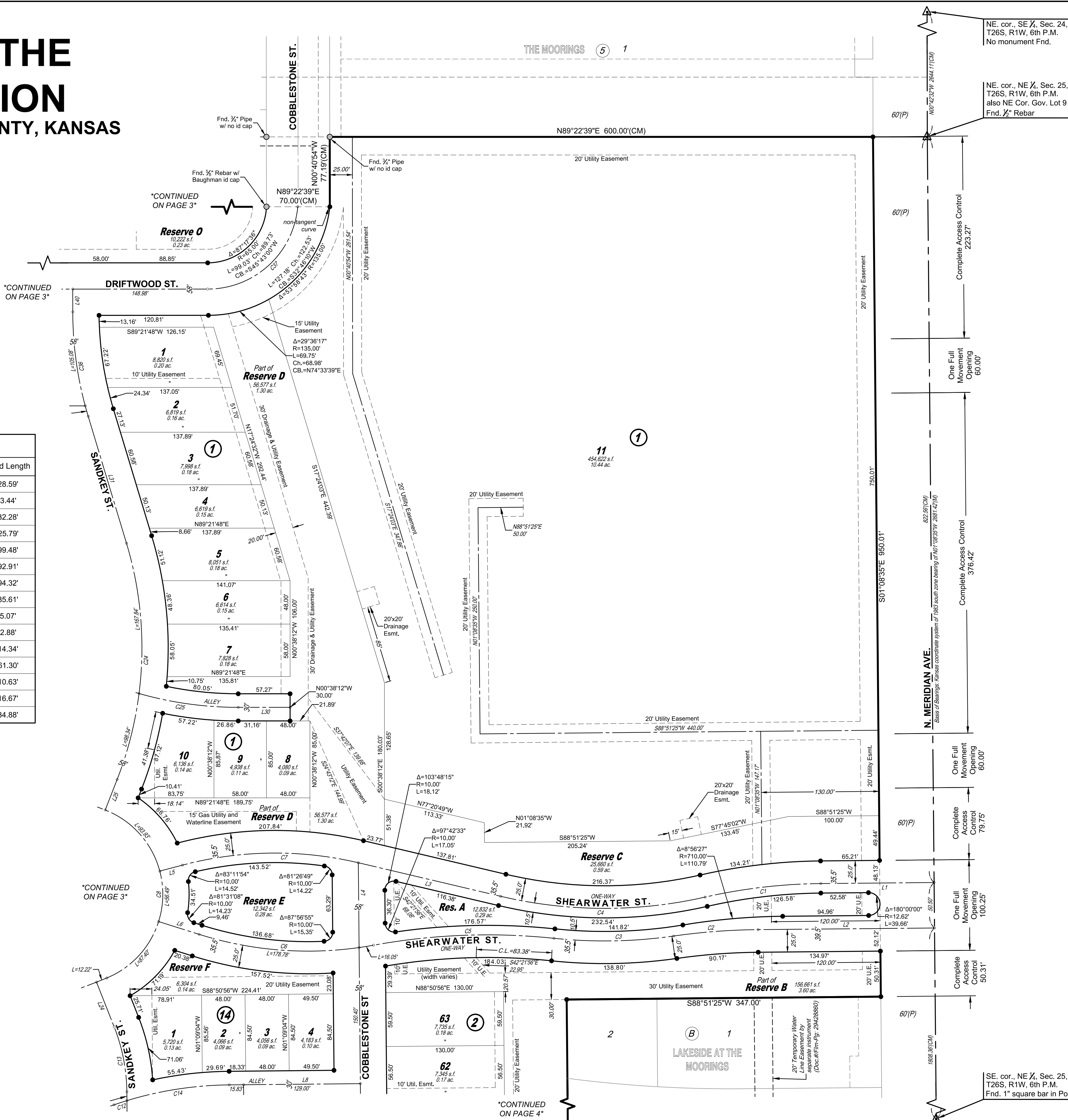


CENTERLINE TABLE		
Line #	Length	Direction
L1	125.21'	S88°51'25"W
L2	194.97'	S88°51'25"W
L3	161.58'	N77°20'49"W
L4	93.73'	N01°09'04"W
L5	25.84'	S74°32'14"W
L6	46.94'	N73°05'49"W
L8	144.83'	S88°50'56"W
L24	53.32'	S22°08'46"E
L25	36.10'	S20°43'08"W
L30	57.27'	N89°21'48"E
L31	146.50'	S17°24'32"E
L40	25.12'	S00°38'12"E

CENTERLINE CURVE TABLE					
Curve #	Length	Radius	Delta	Chord Direction	Chord Length
C1	128.84'	600.00'	12°18'11"	S82°42'19"W	128.59'
C2	93.51'	700.00'	7°39'14"	S85°01'47"W	93.44'
C3	132.62'	536.00'	14°10'33"	S88°17'27"W	132.28'
C4	227.76'	500.00'	26°05'57"	S89°36'12"W	225.79'
C5	200.16'	700.00'	16°22'59"	S87°11'14"W	199.48'
C6	194.83'	400.00'	27°54'26"	N87°03'02"W	192.91'
C7	196.28'	400.00'	28°06'56"	S88°35'42"W	194.32'
C8	237.82'	100.00'	136°15'36"	S01°09'04"E	185.61'
C12	15.07'	400.00'	2°09'30"	S71°20'07"W	15.07'
C13	73.29'	200.00'	20°59'41"	S11°38'55"E	72.88'
C14	114.73'	400.00'	16°26'03"	S80°37'54"W	114.34'
C24	266.18'	400.00'	38°07'40"	S01°39'18"W	261.30'
C25	110.98'	400.00'	15°53'49"	S82°41'18"E	110.63'
C36	117.09'	400.00'	16°46'20"	S09°01'22"E	116.67'
C37	148.03'	100.00'	84°48'44"	S46°57'26"W	134.88'

MINIMUM PAD ELEVATIONS LOWEST OPENINGS		
LOTS inclusive	BLOCK	ELEVATION NAVD 88
62-63	2	1329.4
1-10	1	1329.4
1-4	14	1329.4

BENCHMARK
BM#1 Chiseled "X" top of north curb on north access drive to Meritrust Credit Union from Meridian Ave. 45' northwest of sanitary manhole, 40 feet east of back of curb corner, 29 feet north of south access drive back of curb.
Elev.=1329.89 NAVD88



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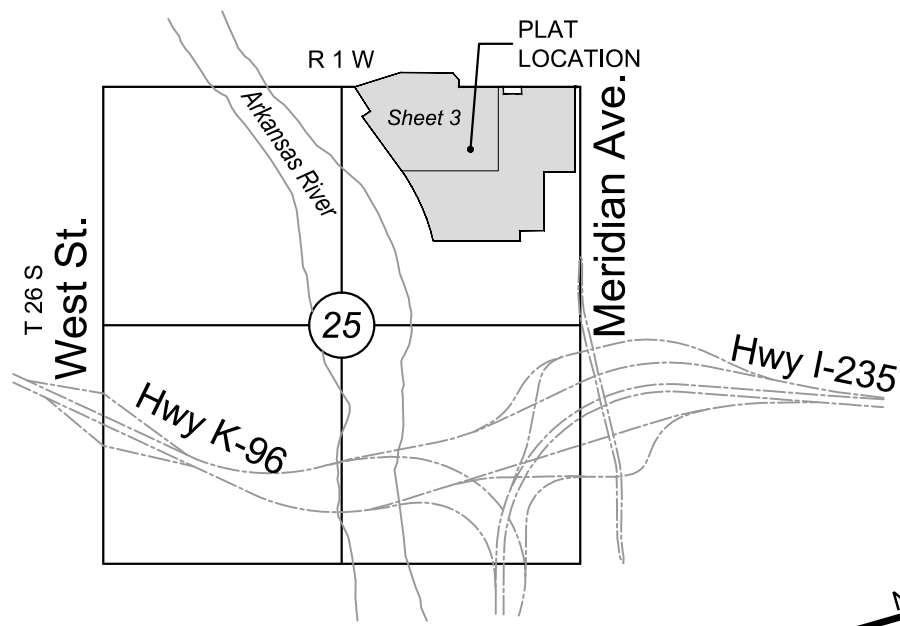
LEGEND

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FOR REFERENCE ONLY





VICINITY MAP

COURTYARDS AT THE MOORINGS ADDITION

AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

FINAL PLAT

VICINITY MAP

CENTERLINE TABLE

Line #	Length	Direction
L25	36.10'	S20°43'08"W
L26	32.61'	N69°01'46"E
L27	203.34'	S31°34'54"E
L28	86.16'	S31°34'54"E
L29	111.80'	N21°02'21"W
L30	57.27'	N89°21'48"E
L31	146.50'	S17°24'32"E
L32	339.04'	S89°21'48"W
L33	216.94'	S89°21'48"W
L34	67.79'	S12°19'08"W
L35	27.00'	N45°45'31"W
L36	7.06'	N45°45'31"W
L37	53.88'	N00°38'12"W
L38	92.31'	N00°38'12"W
L39	656.77'	S89°21'48"W
L40	25.12'	S00°38'12"E

CENTERLINE CURVE TABLE

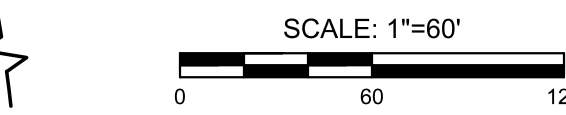
Curve #	Length	Radius	Delta	Chord Direction	Chord Length
C22	582.09'	800.00'	41°41'22"	N89°52'27"E	569.34'
C23	189.10'	600.00'	18°03'27"	N12°00'38"W	188.32'
C24	266.18'	400.00'	38°07'40"	S01°39'18"W	261.30'
C25	110.98'	400.00'	15°53'49"	S82°41'18"E	110.63'
C26	20.75'	400.00'	2°58'19"	S73°15'14"E	20.74'
C27	305.82'	1000.00'	17°31'20"	S80°31'45"E	304.63'
C28	394.16'	1000.00'	22°35'01"	N79°25'04"E	391.61'
C29	44.52'	200.00'	12°45'19"	S25°12'15"E	44.43'
C30	114.93'	150.00'	43°54'02"	S09°37'53"E	112.14'
C31	474.95'	225.00'	120°56'42"	S28°53'27"W	391.55'
C32	241.78'	70.00'	197°53'53"	S02°50'17"E	138.30'
C33	117.49'	150.00'	44°52'41"	N68°11'52"W	114.51'
C34	118.13'	150.00'	45°07'19"	N23°11'52"W	115.10'
C35	142.44'	400.00'	20°24'09"	N10°50'17"W	141.68'
C36	117.09'	400.00'	16°46'20"	S09°01'22"E	116.67'
C37	148.03'	100.00'	84°48'44"	S46°57'26"W	134.88'

MINIMUM PAD ELEVATIONS LOWEST OPENINGS		
LOTS	BLOCK	ELEVATION NAVD 88
1-10	1	1329.4
1-27	2	1329.4
1-5	3	1329.4
1-4	4	1329.4
1-5	5	1329.4
1-8	6	1329.4
1-10	7	1329.4
1-4	8	1329.4
1-5	9	1329.4
1	10	1329.4
1	11	1329.4

LEGEND

Date of Survey: 12/20/2021

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BENCHMARK

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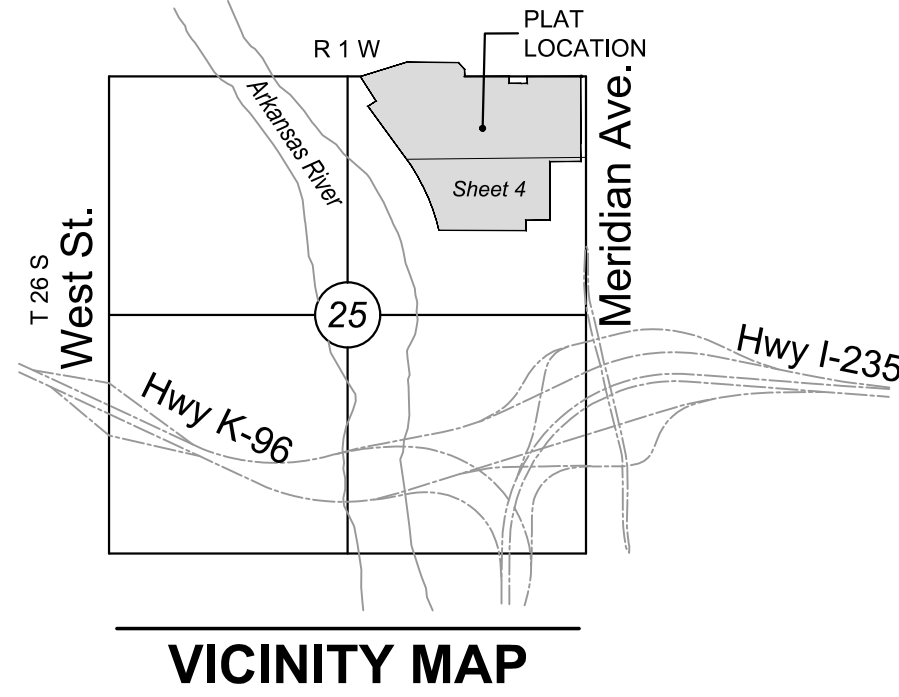
FOR REFERENCE ONLY



COURTYARDS AT THE MOORINGS ADDITION

AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

FINAL PLAT



VICINITY MAP

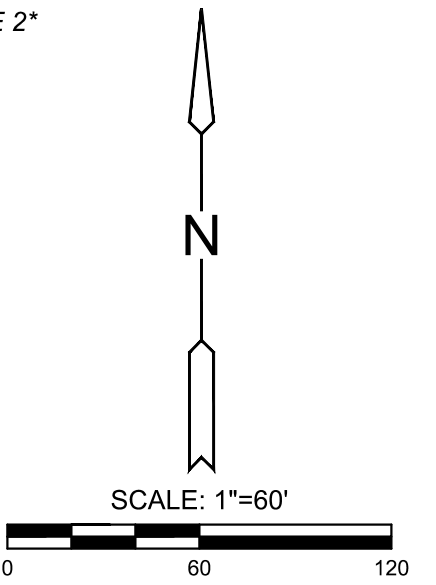
CENTERLINE TABLE		
Line #	Length	Direction
L6	46.94'	N73°05'49"W
L7	571.61'	N01°09'04"W
L8	144.83'	S88°50'56"W
L9	455.22'	N01°09'04"W
L10	692.80'	N88°50'56"E
L11	448.67'	S01°09'04"E
L12	281.69'	N88°50'56"E
L13	517.87'	N01°09'04"W
L14	147.41'	N88°50'56"E
L15	8.78'	S15°05'11"W
L16	95.75'	S01°09'04"E
L17	184.50'	S26°43'04"E
L18	114.82'	S26°43'04"E
L19	55.69'	S09°07'43"W
L20	55.63'	N76°49'12"W
L21	170.90'	S13°10'48"W
L22	148.34'	N81°12'32"W
L23	290.21'	N01°09'04"W
L24	53.32'	S22°08'46"E

CENTERLINE CURVE TABLE				
Curve #	Length	Radius	Delta	Chord Length
C3	132.62'	536.00'	14°10'33"	132.28'
C5	200.16'	700.00'	16°22'59"	199.48'
C6	194.83'	400.00'	27°54'26"	192.91'
C9	444.19'	800.00'	31°48'45"	438.50'
C10	301.66'	1000.00'	17°17'02"	300.52'
C11	272.96'	1000.00'	15°38'23"	272.12'
C12	15.07'	400.00'	2°09'30"	15.07'
C13	73.29'	200.00'	20°59'41"	72.88'
C14	114.73'	400.00'	16°26'03"	114.34'
C15	53.41'	34.00'	90°00'00"	48.08'
C16	42.51'	150.00'	16°14'15"	42.37'
C17	112.46'	100.00'	64°26'01"	106.62'
C18	38.24'	34.00'	64°26'01"	36.25'
C19	93.85'	150.00'	35°50'47"	92.32'
C20	139.27'	200.00'	39°53'52"	136.47'
C21	312.50'	400.00'	44°45'42"	304.61'

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1-5	12	1329.4
1-4	13	1329.4
1-4	14	1329.4
1-8	15	1329.4
1-8	16	1329.4
1-8	17	1329.4
1-4	18	1329.4
1-5	19	1329.4
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