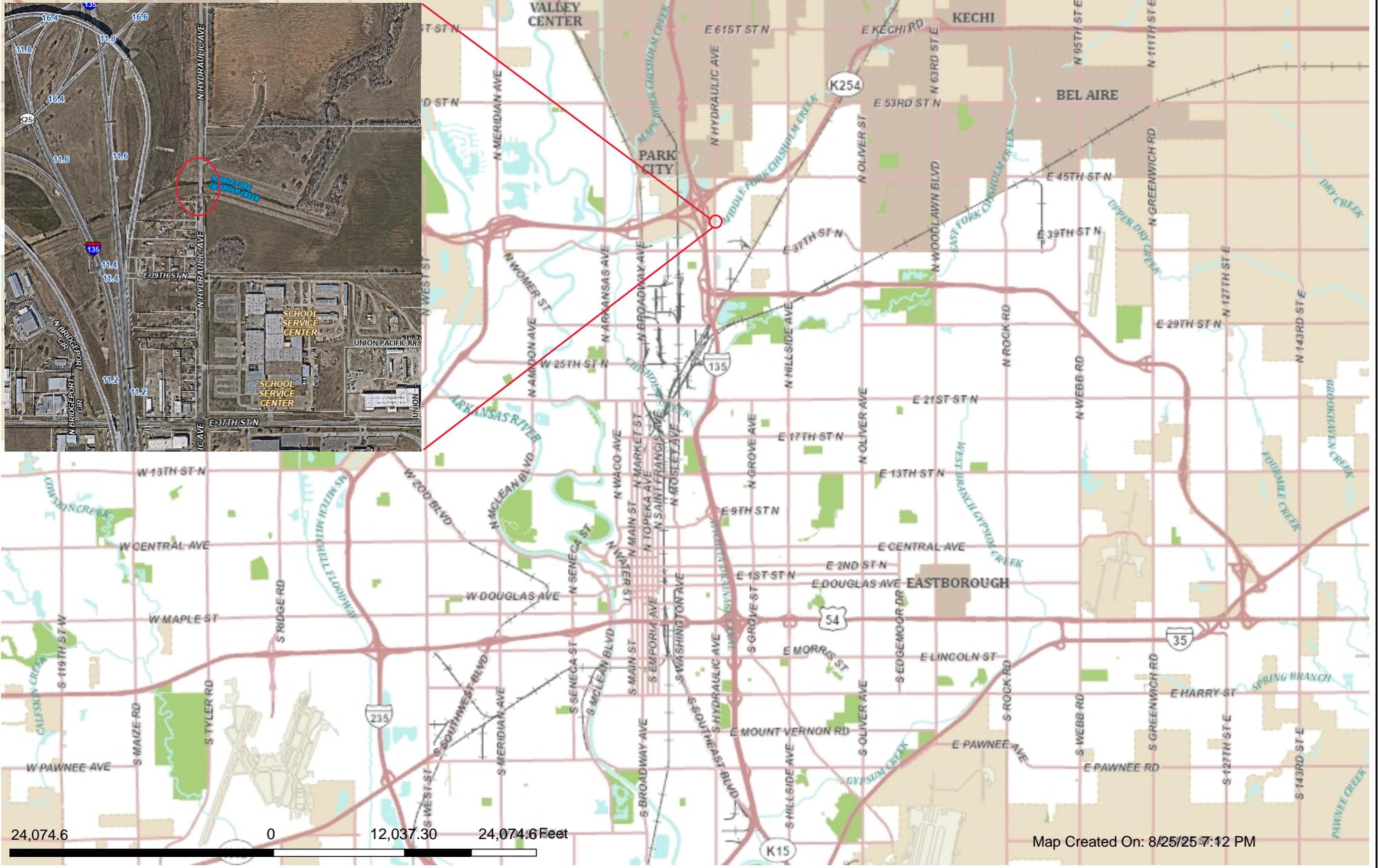


Project Location: N. Hydraulic over Middle Fork Chisholm Creek



24,074.6 0 12,037.30 24,074.6 Feet

Map Created On: 8/25/25 7:12 PM

This information is not an official record, and cannot be used as such. The user should rely only upon official records available from the custodian of records in the appropriate City and/or County department. Some data provided here and used for the preparation of these maps has been obtained from public records not created or maintained by the City of Wichita.

1: 144,448

N. Hydraulic over Middle Fork Chisholm Creek



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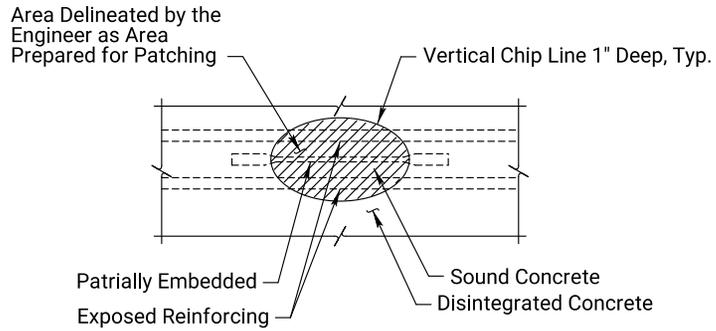


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REVISIONS		
BY		
APP'D		

Br. No. 823-K-2160

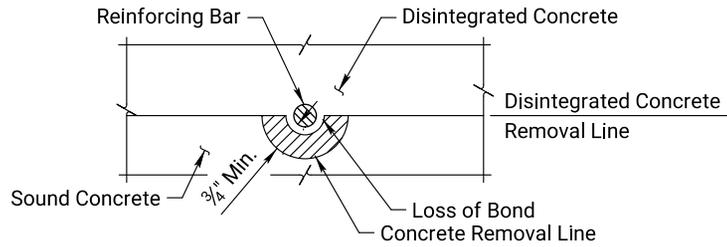
DECK PATCHING DETAIL

City of Wichita
 Proj. No. 194 530400878236086
 Sedgwick County
 0 of

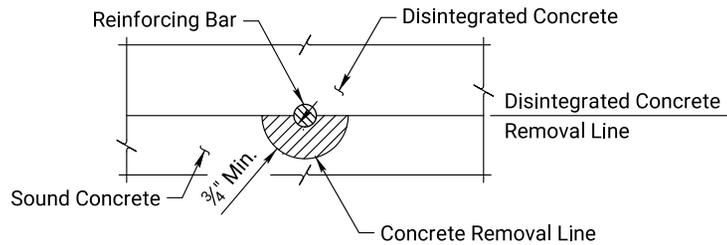


DETAIL 1

Area delineated by the Engineer as Prepared for Patching



DETAIL 2



DETAIL 3

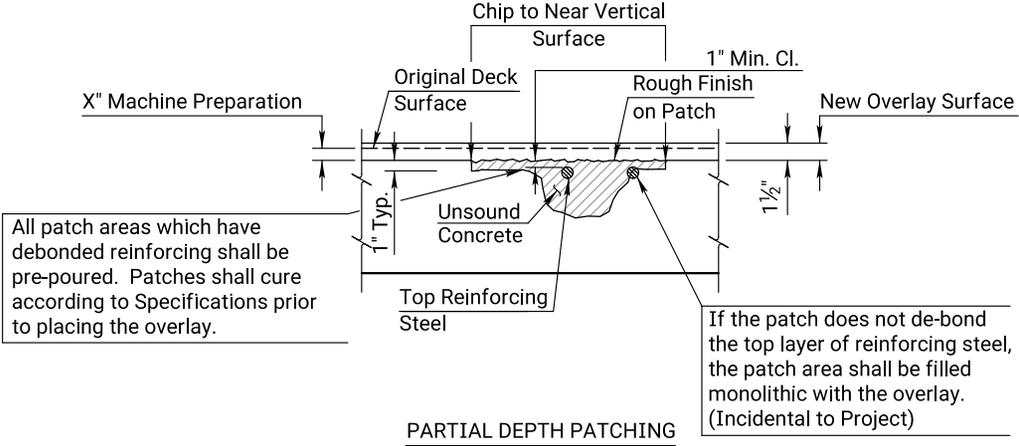
DECK PATCHING DETAILS

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DECK PATCHING DETAIL
(Portland Cement Concrete Overlay)

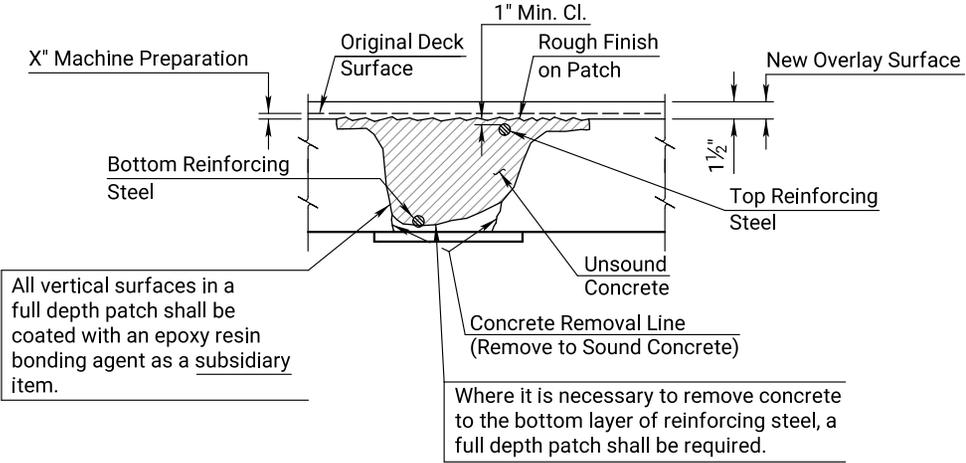
City of Wichita
Proj. No. 194 530400878236086
Sedgwick County 0 of



All patch areas which have debonded reinforcing shall be pre-poured. Patches shall cure according to Specifications prior to placing the overlay.

If the patch does not de-bond the top layer of reinforcing steel, the patch area shall be filled monolithic with the overlay. (Incidental to Project)

PARTIAL DEPTH PATCHING



All vertical surfaces in a full depth patch shall be coated with an epoxy resin bonding agent as a subsidiary item.

Where it is necessary to remove concrete to the bottom layer of reinforcing steel, a full depth patch shall be required.

FULL DEPTH PATCHING

DECK PATCHING DETAILS

MACHINE PREPARATION: This item shall consist of preparing the deck for an overlay by removing concrete from the roadway surface of the bridge deck to the depth as shown on the plans. See KDOT Specifications.

AREA PREPARED FOR PATCHING: This item shall consist of removing unsound concrete and asphalt patches from the bridge deck, cleaning reinforcing bars, and filling the removed patched areas with concrete. Quantity shown is an estimate of the areas involved. The exact areas shall be determined by tapping, before, during and after chipping operation to ensure that all unsound concrete has been removed. See KDOT Specifications.

FULL DEPTH PATCHING: Forms shall be provided to enable placement of the concrete in areas of full depth removal of bridge slab. The forms may be suspended from existing reinforcing bars by wire ties or a method approved by the Engineer. See KDOT Specifications for method of measurement and payment.

REINFORCING IN BRIDGE DECK: Care should be exercised to prevent cutting, stretching or damaging exposed reinforcing steel. Extreme care should be exercised to avoid breaking the bond between the reinforcing steel and concrete where bars are partially exposed yet remain anchored in sound concrete. Reinforcing steel damaged, cut or deteriorated shall be replaced as directed by the Engineer. Do not wedge chipping hammer bit against reinforcement. See table for replacement bar size and minimum splice length required. Replacement of bars damaged by the Contractor shall be at no cost to the State.

PORTLAND CEMENT CONCRETE OVERLAY: This item shall consist of cleaning the concrete surface and placing the "Portland Cement Concrete Overlay" to the dimensions specified on the plans. See KDOT Specifications.

OVERLAY CONSTRUCTION JOINTS: All construction joints in the overlay and the cold joint between the overlay and the curbs shall be cleaned by sandblasting and sealed with an approved cold applied joint sealant 72 hours after placement of the overlay.

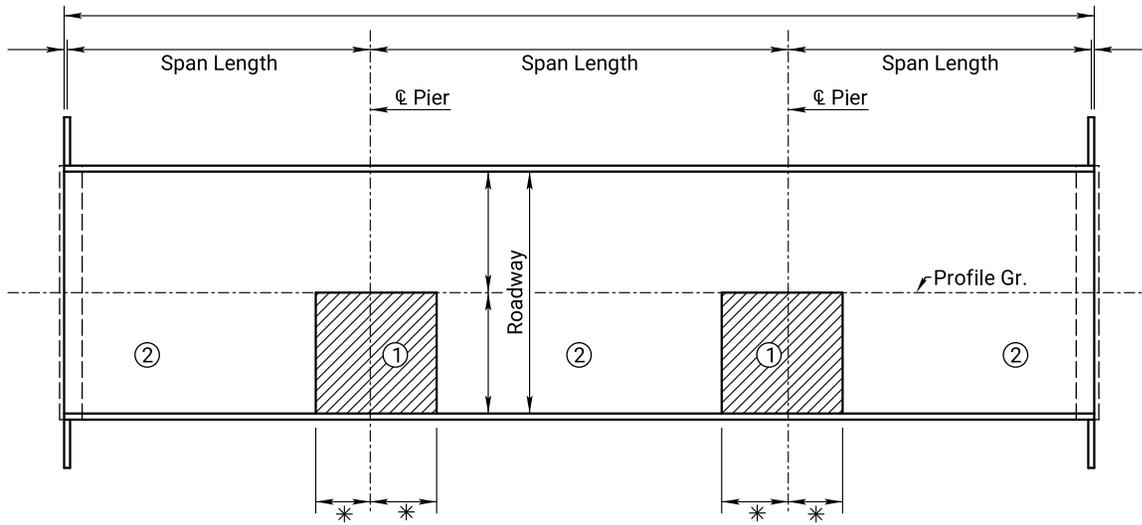
REPAIR OF EPOXY COATED REINFORCING STEEL: Replace any epoxy coating damaged or removed from the reinforcing steel during the concrete removal process. Thoroughly clean damaged areas with a stiff wire brush to remove dirt and damaged coating. Apply an approved patching material in accordance with the manufacturer's recommendations. Avoid dripping any patching material onto the existing concrete. See KDOT Specifications.

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APPROV.	

Br. No. 823-K-2160

DECK PATCHING SEQUENCE
TITLE TEXT

City of Wichita
Proj. No. 194 530400878236086
Sedgwick County
0 of



DECK PATCHING SEQUENCE
(Showing sequence for one lane only, other lane similar.)
* Span Length/4 or as directed by the Engineer.

PATCHING SEQUENCE: The concrete removal shall be completed in stages beginning with removal of deteriorated concrete in Area ①. The maximum size of a patch with fully debonded bars is limited to 4 ft. (along C) x 8 ft. (perpendicular to C) in Area ①. If more than 15 longitudinal bars in Area ① are debonded for a distance of greater than 4 feet along the bars, the concrete removal shall stop and the patch area filled with Concrete (Grade 4.0) (AE). The patches shall cure a minimum of 3 days before concrete removal resumes adjacent to newly placed patches. Following the completion of work in Area ①, concrete removal may begin in Area ②. Concrete removal shall not begin in Area ② until the final patches in Area ① have cured a minimum of 3 days. The maximum size of any full depth patch in Area ② shall be limited to 4 ft. x 8 ft. in any direction. All patching and overlay concrete shall be cured according to the Specifications prior to opening the lane to traffic.

RECAPITULATION OF QUANTITIES

ITEM	UNITS	QUANTITY		
Machine Preparation ()	Sq. Yds.			
Area Prepared for Patching	Sq. Yds.			
Area Prepared for Patching (Full Depth)	Sq. Yds.			
Portland Cement Concrete Overlay ()	Sq. Yds.			
Reinforcing Steel (Repair) (Grade 60) (Set Price)	Lbs.	1		
Reinforcing Steel (Repair) (Grade 60) (Epoxy) (Set Price)	Lbs.	1		
Material for Portland Cement Concrete Overlay (Set Price)	Cu. Yds.	1		
HMA-Commercial Grade (Class A)	Ton			
Bridge Deck Grooving	Sq. Yds.			

MINIMUM REBAR SPLICE LENGTHS

Existing Bar Size	Minimum Splice Lengths (inches)	
	Existing Gr. 40 ksi Bars	Existing Gr. 60 ksi Bars
#4	12"	16"
#5	13"	20"
#6	16"	24"
#7	20"	30"
#8	26"	39"
#9	33"	49"
#10	42"	62"
#11	51"	77"

Note: If splicing epoxy coated reinforcing steel, increase the above splice lengths by 20%.

■ Lap lengths are based on a Class B splice. Use the minimum splice length corresponding to the grade of the existing reinforcing in the deck.

Detour for N. Hydraulic over Middle Fork Chisholm Creek



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Structure Inventory & Appraisal (English)

(8) STRUCTURE NO **530400870MFCC20** (1) STATE **20 Kansas** (4) CITY **Wichita** (3) COUNTY **Sedgwick**
 LPA Bridge ID **255** LPA Bridge Name _ (5A) ROUTE ON/UNDER **Route On Structure**

HBP FUNDING ELIGIBILITY **Not Eligible**

IDENTIFICATION	
(5B) ROUTE TYPE	5 City Street
(5C) SERVICE TYPE	1 Mainline
(5D) ROUTE NUMBER	00000
(5E) SUFFIX	0 N/A (NBI)
(6A) FEATURE INTERSECTED	MIDDLE FORK CHISHOLM CR
(9) LOCATION	HILLSIDE N OF 45TH ST. N
(16) LATITUDE	37 ° 46 ' 25.00 "
(17) LONGITUDE	97 ° 17 ' 57.00 "
(2) HIGHWAY AGENCY DISTRICT	District 05 - Hutchinson
(98A) BORDER BRIDGE STATE	Not Applicable (P)
(98B) BORDER BRIDGE RESPONSIBILITY	%
(99) BORDER BRIDGE STRUCTURE NO.	-
(7) (ROUTE NAME) FACILITY CARRIED	HILLSIDE

FUNCTIONAL DESCRIPTION	
(26) FUNCTIONAL CLASSIFICATION	17 Urban Collector
(104) NHS DESIGNATION	0 Not on NHS
(100) STRAHNET DESIGNATION	0 Not a STRAHNET hwy
(110) NATIONAL TRUCK NET	0 Not part of natl netwo
(12) BASE HIGHWAY NET	Not on Base Network
(13A) LRS INVENTORY ROUTE	(13B) LRS SUBRTE #
(11) LRS MILE POINT	0.000 MI
(105) FEDERAL LANDS HIGHWAY	0 N/A (NBI)
(20) TOLL	3 On free road
(21) MAINTAINANCE RESPONSIBILITY	04 Cty/Muncpl Hwy Agency
(22) OWNER	04 Cty/Muncpl Hwy Agency
(37) HISTORICAL SIGNIFICANCE	5 Not eligible for NRHP
(101) PARALLEL STRUCTURE	No bridge exists
(103) TEMPORARY STRUCTURE	

AGE AND SERVICE	
(29) AVERAGE DAILY TRAFFIC	3,500
(109) AVERAGE DAILY TRUCK TRAFFIC	2 %
(30) YEAR OF ADT	2006
(27) YEAR BUILT	1982
(106) YEAR REHABILITATED	0
(102) ONE WAY OR TWO WAY TRAFFIC	2 2-way traffic
(42A) SERVICE ON THE BRIDGE	1 Highway
(42B) SERVICE UNDER THE BRIDGE	5 Waterway
(28A) LANES ON ROUTE	2
(28B) LANES UNDER ROUTE	0
(19) BYPASS DETOUR LENGTH	1.2 MI

POSTING	
(41) POSTING STATUS	A Open, no restriction
JUSTIFICATION	N/A
TYPE	N/A
STATUS	N/A
R12-1	Grs Wt: N/A
R12-5	Sgl Unit: N/A 2-Unit: N/A 3-Unit: N/A
EMERGENCY VEH	Sgl Axle: N/A Tdm Axle: N/A EV Gross Wt: N/A

LOAD RATING	
(66) INVENTORY LOAD (tons) or RATING FACTOR (RF)	42.2 tons
(64) MAXIMUM LOAD (tons) or RATING FACTOR (RF)	70.5 tons
(31) DESIGN LOAD	5 MS 18 (HS 20)
(65) INVENTORY LOAD RATING METHOD	1 LF Load Factor (tons)
(63) OPERATING (MAX) LOAD RATING METHOD	1 LF Load Factor (tons)
(70) POSTING REQUIREMENTS	5 At/Above Legal Loads

SCHEDULE	
(90) ROUTINE INSPECTION DATE	09/14/2023
(91) ROUTINE INSPECTION FREQUENCY	24 MO
(92) CRITICAL FEATURE INSPECTION:	(93) INSP DATE
A) FRACTURE CRITICAL	N MO A)
B) UNDERWATER INSP	N MO B)
C) SPECIAL INSP	N MO C)

GEOMETRIC DATA	
(112) NBIS BRIDGE DEFINITION	Long Enough
(49) STRUCTURE LENGTH	132.9 FT
(48) MAXIMUM SPAN LENGTH	29.9 FT
(32) ROUTE WIDTH	26.0 FT
(51) BRIDGE ROADWAY WIDTH, CURB TO CURB	52.0 FT
(52) DECK WIDTH OUT TO OUT	54.0 FT
(50A) LEFT CURB OR SIDEWAY WIDTH	0.0 FT
(50B) RIGHT CURB OR SIDEWAY WIDTH	0.0 FT
(34) SKEW	45.0 °
(47) ROUTE HORIZONTAL CLEARANCE	52.0 FT
(10) MIN VERT CLEARANCE OVER ROUTE	99.99 FT
(53) MIN VERT CLEARANCE OVER BRIDGE	99.99 FT
(33) MEDIAN	0 No median
(35) STRUCTURE FLARED	0 No flare
(54A) MIN VERT UNDERCLEARANCE REF	N Feature not hwy or RR
(54B) MIN VERT UNDERCLEARANCE	0.00 FT
(55A) MIN LATERAL UNDERCLEAR REF RT	N Feature not hwy or RR
(55B) MIN LATERAL UNDERCLEAR RT	0.0 FT
(56) MIN LATERAL UNDERCLEARANCE LEFT	0.0 FT

STRUCTURE AND MATERIALS	
(45) NUMBER OF MAIN SPANS	5
(43B) MAIN SPAN DESIGN TYPE	01-Slab
(43A) MAIN SPAN MATERIAL TYPE	2 Concrete Continuous
KDOT MAIN SPAN MATERIAL TYPE:	2 - 01 : RCSC
(107) DECK TYPE	1 Concrete-Cast-in-Place
(108A) DECK SURFACE	6 Bituminous
(108B) MEMBRANE	0 None
(108C) DECK PROTECTION	None
(46) NUMBER OF APPROACH SPANS	0
(44B) APPROACH SPAN DESIGN TYPE	Unknown (P)
(44A) APPROACH SPAN MATERIAL TYPE	Unknown (NBI)
KDOT APPROACH SPAN MATERIAL TYPE:	

CONDITION	
(58) DECK CONDITION RATING	6
(59) SUPERSTRUCTURE CONDITION	6
(60) SUBSTRUCTURE CONDITION	7
(62) CULVERT CONDITION	N
(61) STREAM STABILITY / CHANNEL	6
NBI BRIDGE CONDITION (G/F/P)	Fair

APPRAISAL	
DEFICIENCY STATUS	Not Deficient
(72) BRIDGE ROUTE ALIGNMENT	8
(71) WATERWAY ADEQUACY	7
(113) SCOUR VULNERABILITY	4
(67) STRUCTURAL EVALUATION	6
(68) DECK WIDTH APPRAISAL	9
(69) HORIZ. UNDERCLEARANCE APPRAISAL	N
SUFFICIENCY RATING	98.70
(36A) BRIDGE RAILS	1
(36B) RAIL TRANSITIONS	0
(36C) APPROACH GUARDRAILS	1
(36D) APPROACH GUARDRAIL ENDS	0

NAVIGATION DATA	
(38) NAVIGABLE WATERWAY	Permit Not Required
(39) NAVIGATION VERTICAL CLEARANCE	FT
(40) NAVIGATION HORIZONTAL CLEARANCE	FT
(111) SUBSTRUCTURE NAV PROTECTION	Unknown (NBI)
(116) MIN NAV VERT CLEAR VERT LIFT BRIDGE	FT

PROPOSED IMPROVEMENTS	
(75A) TYPE OF WORK	Not Applicable (P)
(75B) WORK BY	Unknown (NBI)
(76) IMPROVEMENT LENGTH	FT
(94) BRIDGE COST	
(95) ROADWAY COST	
(96) TOTAL COST	
(97) COST ESTIMATE YEAR	
(114) FUTURE ADT	6,000
(115) FUTURE ADT YEAR	2039

Structure Inventory & Appraisal (English)

(8) STRUCTURE NO **530400870MFCC20** (1) STATE **20 Kansas** (4) CITY **Wichita** (3) COUNTY **Sedgwick**
LPA Bridge ID **255** LPA Bridge Name **-** (5A) ROUTE ON/UNDER **Route On Structure**
HBP FUNDING ELIGIBILITY **Not Eligible**

NOTES

STRUCTURE NOTES A Special Inspection and Load Rating was performed as a part of the Multi -year Kansas Local Bridge Rating Program (KDOT Project No. 106 C-4505-20). As a part of the program NBI condition ratings were verified and updated if necessary, load rating data was updated accordingly, and basic NBI data was reviewed and updated where needed. See the Load Rating Report for load rating calculations and special inspection notes.

INSPECTION NOTES (09-14-2023)

Deck: Spalling with exposed rusting rebar along deck soffit edges in all spans .

Superstructure: Spalling with exposed rusting rebar and delaminating 4ft wide in all spans at centerline. Worst condition is in span 4 with large hanging piece of concrete.

Channel: Large debris caught at bents.

ROADWAY NOTES -

POSTING NOTES



Kansas Local Bridge Inspection Form

(8) STRUCTURE NO 530400870MFCC20

(1) STATE 20 Kansas

(4) CITY Wichita

(3) COUNTY Sedgwick

LPA Bridge ID 255

(5A) ROUTE ON/UNDER

Route On Structure

HBP FUNDING ELIGIBILITY

Not Eligible

IDENTIFICATION

(5B) ROUTE TYPE 5 City Street
 (5C) SERVICE TYPE 1 Mainline
 (5D) ROUTE NUMBER 00000 (5E) SUFFIX 0 N/A
 (6A) FEATURE INTERSECTED MIDDLE FORK CHISHOLM CR
 (9) LOCATION HILLSIDE N OF 45TH ST. N
 (16) LATITUDE 37 ° 46 ' 25.00 "
 (17) LONGITUDE 97 ° 17 ' 57.00 "
 (2) HIGHWAY AGENCY DISTRICT Hutchinson
 (98A) BORDER BRIDGE STATE Not Applicable
 (98B) BORDER BRIDGE RESPONSIBILITY
 (99) BORDER BRIDGE STRUCTURE NO.
 (7) (ROUTE NAME) FACILITY CARRIED HILLSIDE

FUNCTIONAL

(26) FUNCTIONAL CLASSIFICATION 17 Urban Collector
 (104) NHS DESIGNATION 0 Not on NHS
 (100) STRAHNET DESIGNATION 0 Not a STRAHNET Hwy
 (110) NATIONAL TRUCK NET 0 Not part of natl network
 (12) BASE HIGHWAY NET Not on Base Network
 (13A) LRS INVENTORY ROUTE (13B) LRS SUBRTE #
 (11) LRS MILE POINT 0.000
 (105) FEDERAL LANDS HIGHWAY 0 N/A
 (20) TOLL 3 Rdwy Agreement
 (21) MAINTENANCE RESPONSIBILITY City/Municipal Hwy Agenc
 (22) OWNER City/Municipal Hwy AgencY
 (37) HISTORICAL SIGNIFICANCE Not eligible for NRHP
 (101) PARALLEL STRUCTURE No || bridge exists
 (103) TEMPORARY STRUCTURE No

AGE

(29) AVERAGE DAILY TRAFFIC 3500
 (109) AVERAGE DAILY TRUCK TRAFFIC 02 %
 (30) YEAR OF ADT 2006
 (27) YEAR BUILT 1982
 (106) YEAR REHABILITATED 0000
 (102) ONE WAY OR TWO WAY TRAFFIC 2-way traffic
 (42A) SERVICE ON THE BRIDGE 1 Highway
 (42B) SERVICE UNDER THE BRIDGE 5 Waterway
 (28A) LANES ON ROUTE 2
 (28B) LANES UNDER ROUTE 0
 (19) BYPASS DETOUR LENGTH 1.2 MI

POSTINGS

(41) POSTING STATUS A Open, no restriction
 JUSTIFICATION N/A
 POSTING TYPE N/A
 POSTING STATUS N/A
 R12-1 Grs Wt N/A
 R12-5 Sgl Unit N/A 2-unit N/A 3-unit N/A
 EMERGENCY VEHICLES Sgl Axle N/A Tdm Axle N/A Grs Wt N/A

RATING

(66) INVENTORY LOAD (tons) or RATING FACTOR (RF) 42.2 tons
 (64) MAXIMUM LOAD (tons) or RATING FACTOR (RF) 70.5 tons
 (31) DESIGN LOAD 5 MS 18 (HS 20)
 (65) INVENTORY LOAD RATING METHOD 1 LF Load Factor
 (63) OPERATING (MAX) LOAD RATING METHOD 1 LF Load Factor
 (70) POSTING REQUIREMENTS 5 At/Above Legal Loads
 (41) POSTING STATUS A Open, no restriction

SCHEDULE

September 14, 2023

(90) ROUTINE INSPECTION DATE 09/22/2021
 (91) ROUTINE INSPECTION FREQUENCY 24 MO
 (92) CRITICAL FEATURE INSPECTION: (93) INSP DATE
 A) FRACTURE CRITICAL N MO A)
 B) UNDERWATER INSP N MO B)
 C) SPECIAL INSP N MO C)



Kansas Local Bridge Inspection Form

(8) STRUCTURE NO 530400870MFCC20

(1) STATE 20 Kansas

(4) CITY Wichita

(3) COUNTY Sedgwick

LPA Bridge ID 255

(5A) ROUTE ON/UNDER Route On Structure

HBP FUNDING ELIGIBILITY Not Eligible

PROPOSED IMPROVEMENTS

(75A) TYPE OF WORK Not Applicable _____
 (75B) WORK BY _____
 (76) IMPROVEMENT LENGTH _____
 (94) BRIDGE COST _____
 (95) ROADWAY COST _____
 (96) TOTAL COST _____
 (97) COST ESTIMATE YEAR _____
 (114) FUTURE ADT 6000 _____
 (115) FUTURE ADT YEAR 2039 _____

GEOMETRIC DATA

(112) NBIS BRIDGE DEFINITION Long Enough _____
 (49) STRUCTURE LENGTH 132.9 FT _____
 (48) MAXIMUM SPAN LENGTH 29.9 FT _____
 (32) ROUTE WIDTH 26.0 FT _____
 (51) BRIDGE ROADWAY WIDTH, CURB TO CURB 52.0 FT _____
 (52) DECK WIDTH OUT TO OUT 54.0 FT _____
 (50A) LEFT CURB OR SIDEWAY WIDTH 0.0 FT _____
 (50B) RIGHT CURB OR SIDEWAY WIDTH 0.0 FT _____
 (34) SKEW 45 ° _____
 (47) ROUTE HORIZONTAL CLEARANCE 52.00 FT _____
 (10) MIN VERT CLEARANCE OVER ROUTE 99.99 FT _____
 (53) MIN VERT CLEARANCE OVER BRIDGE 99.99 FT _____
 (33) MEDIAN No Median _____
 (35) STRUCTURE FLARED No flare _____
 (54A) MIN VERT UNDERCLEARANCE REF Feature not hwy or RR _____
 (54B) MIN VERT UNDERCLEARANCE 0.00 FT _____
 (55A) MIN LATERAL UNDERCLEAR REF RT Feature not hwy or RR _____
 (55B) MIN LATERAL UNDERCLEAR RT 0.0 FT _____
 (56) MIN LATERAL UNDERCLEARANCE LEFT 0.0 FT _____

STRUCTURE TYPE

(45) NUMBER OF MAIN SPANS 5 _____
 (43B) MAIN SPAN DESIGN TYPE Slab _____
 (43A) MAIN SPAN MATERIAL TYPE Concrete Continuous _____
 KDOT Type Code - Main: 1022 RCSC _____
 (46) NUMBER OF APPROACH SPANS 0 _____
 (44B) APPROACH SPAN DESIGN TYPE _____
 (44A) APPROACH SPAN MATERIAL TYPE _____
 KDOT Type Code - Appr: -1 None/UNK/NULL _____
 (107) DECK TYPE Concrete-Cast-in-Place _____
 (108A) DECK SURFACE 6 Bituminous _____
 (108B) MEMBRANE 0 None _____
 (108C) DECK PROTECTION 0 None _____

CONDITION

(58) DECK CONDITION RATING 6 _____
 (59) SUPERSTRUCTURE CONDITION 6 _____
 (60) SUBSTRUCTURE CONDITION 7 _____
 (62) CULVERT CONDITION N _____
 (61) STREAM STABILITY / CHANNEL 6 _____

APPRAISAL

(72) BRIDGE ROUTE ALIGNMENT 8 _____
 (71) WATERWAY ADEQUACY 7 _____
 (36A) BRIDGE RAILS 1 _____
 (36B) RAIL TRANSITIONS 0 _____
 (36C) APPROACH GUARDRAILS 1 _____
 (36D) APPROACH GUARDRAIL ENDS 0 _____
 (113) SCOUR VULNERABILITY 4 _____
 (67) STRUCTURAL EVALUATION 6 _____
 (68) DECK WIDTH APPRAISAL 9 _____
 (69) HORIZ. UNDERCLEARANCE APPRAISAL N _____
 SUFFICIENCY RATING 98.70
 DEFICIENCY STATUS Not Deficient



Kansas Local Bridge Inspection Form

(8) STRUCTURE NO 530400870MFCC20

(1) STATE 20 Kansas

(4) CITY Wichita

(3) COUNTY Sedgwick

LPA Bridge ID 255

(5A) ROUTE ON/UNDER

Route On Structure

HBP FUNDING ELIGIBILITY

Not Eligible

NOTES

Deck:

Spalling with exposed rusting rebar along deck soffit edges in all spans.

Superstructure:

Spalling with exposed rusting rebar and delaminating 4ft wide in all spans at centerline. Worst condition is in span 4 with large hanging piece of concrete.

Substructure:

Waterway:

Large debris caught at bents.

Culvert:

Misc:

PAUL EARNEST

September 14, 2023

Print Name

Inspuskey: 5841

Team Leader's Signature

Date

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023

City/Co: Wichita

Inspector(s): PCE



1 - Bridge from north



2 - Bridge from south



3 - Deck



4 - Upstream east

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023 City/Co:

Wichita

Inspector(s):

PCE



5 - Downstream west



6 - ID plate



7 - East elevation



8 - Abutment 1

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023

City/Co: Wichita

Inspector(s): PCE



9 - Bent 1



10 - Span 2



11 - Typical edge



12 - Debris in span 3

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023

City/Co: Wichita

Inspector(s): PCE



13 - Span 1



14 - Span 2



15 - Span 3



16 - Debris in span 3

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023

City/Co: Wichita

Inspector(s): PCE



17 - Span 4



18 - Typical edge



19 - Debris in span 4



20 - Span 5

Bridge: 530400870MFCC20
LPA #: 255

Inspection Date: 9/14/2023

City/Co: Wichita

Inspector(s): PCE



21 - Abutment 2



22 - West elevation