

PHASE II PRIVATE STREET IMPROVEMENTS FOR TOWNE PARC 2ND ADDITION

LINDEN FROM THE S.L. TOWNE PARC 2ND ADDITION TO THE N.L. LOT 10, BLOCK 1
 SCOTT FROM THE W.L. LOT 2, BLOCK 2 TO THE E.L. TOWNE PARC 2ND ADDITION
 LINDEN CT. FROM THE W.L. LINDEN TO AND INCLUDING CUL-DE-SAC
 SCOTT CT. FROM THE S.L. SCOTT TO AND INCLUDING CUL-DE-SAC

INDEX NO. 607507

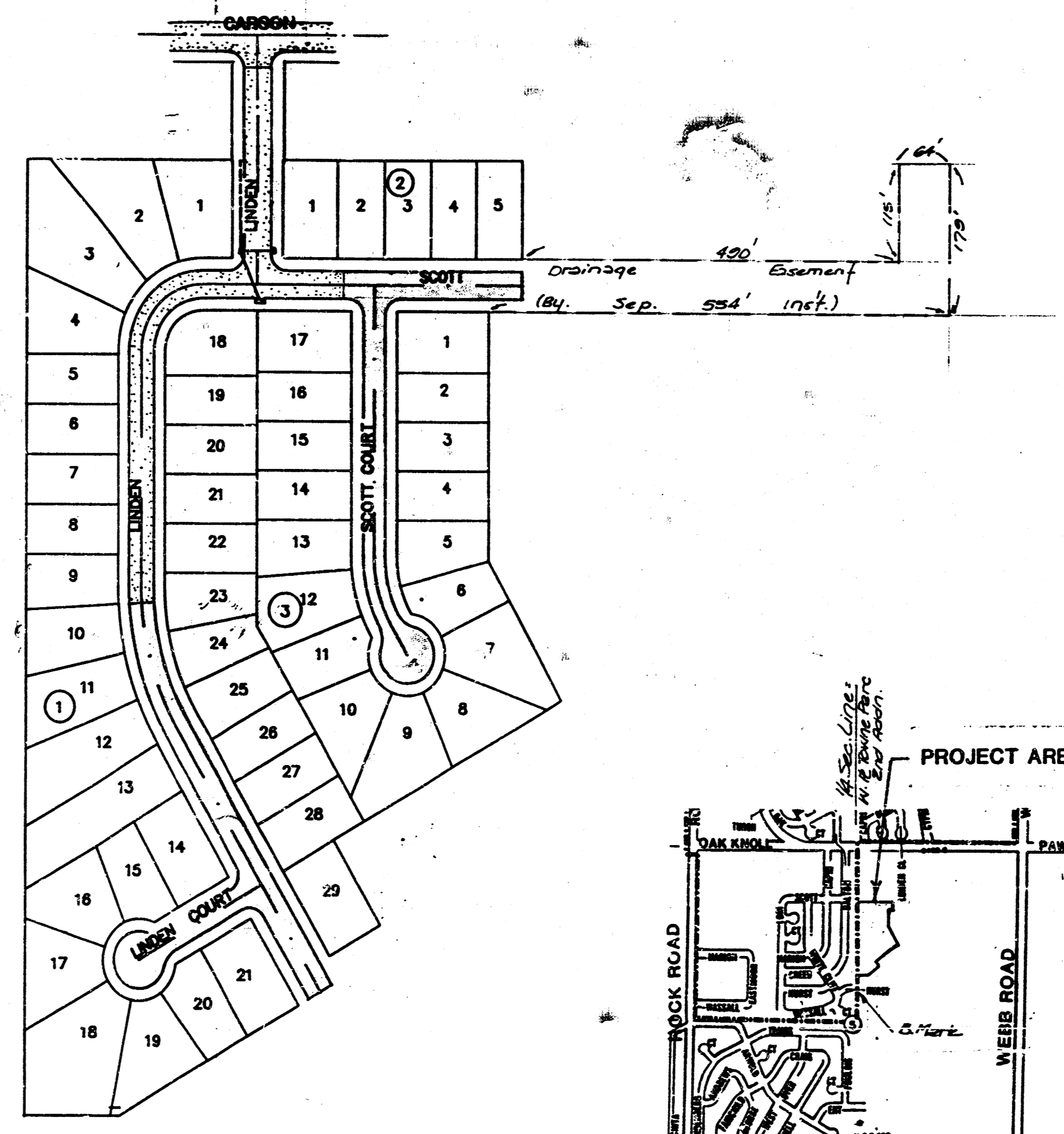
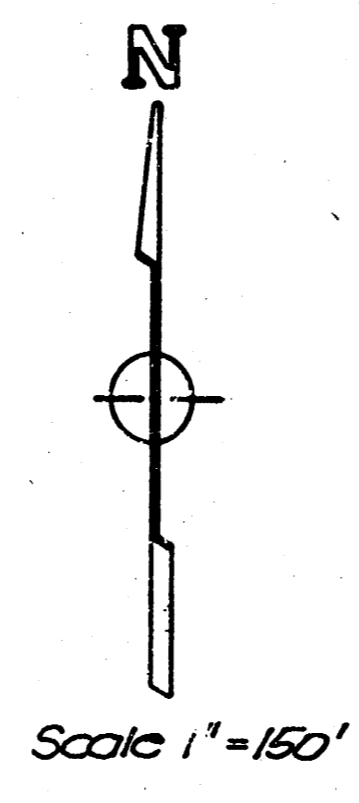
Project No.

472-76-245-80001-000-000-051

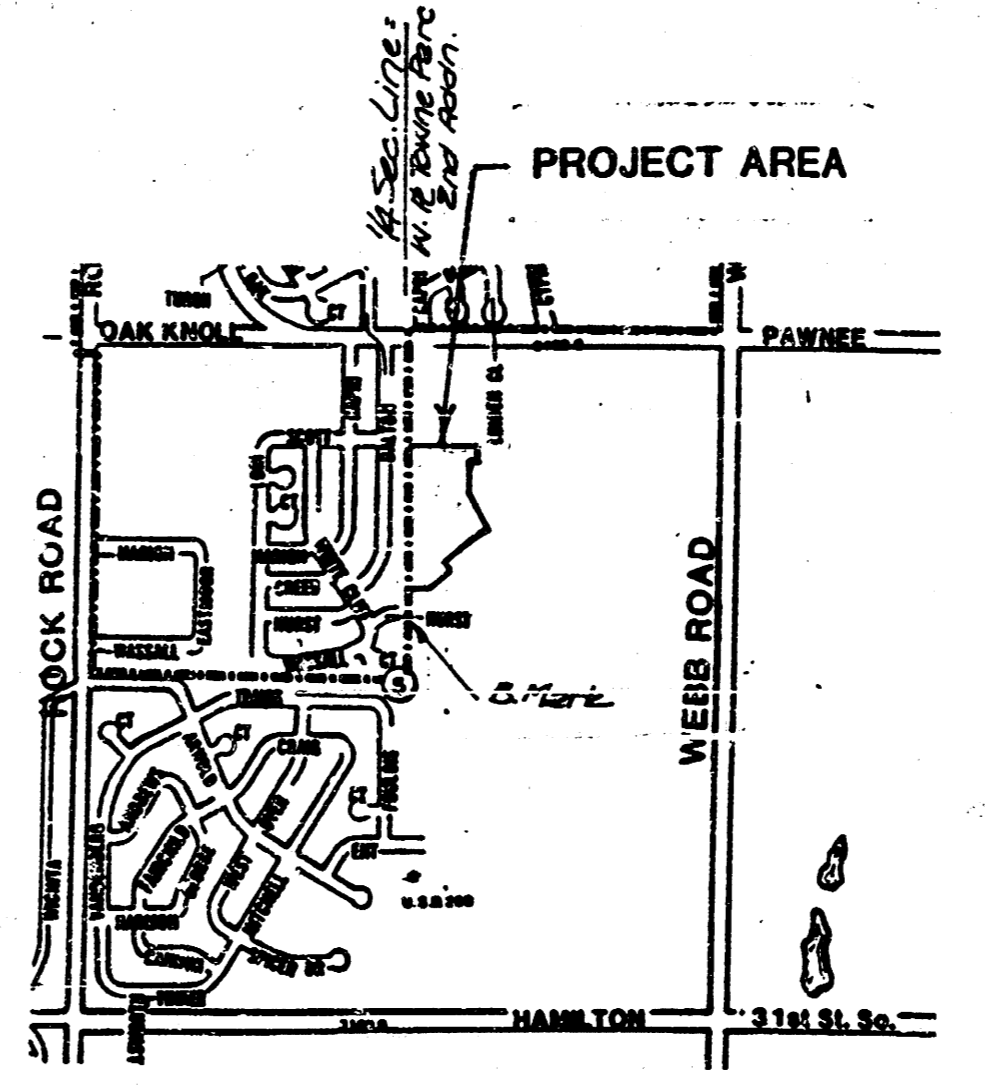
City of Wichita, Kansas - Michael E. Lindebak, City Engineer

Index to the Plans

Title Sheet	1
Typical 35' (7" A.C.) Pavement Section	2
Typical 20' (7" A.C.) Pavement Section	3
Plan Sheet; Linden Sta. of 00-5132.48	4
Plan Sheet; Linden & Scott Sta. 5153.58-10714.30	5
Plan Sheet; Scott Sta. 10714.30-13719.30	6
Plan Sheet; Linden Court Sta. of 00-1104.58	7
Plan Sheet; Scott Court Sta. of 00-4123.33	8
Plat of Towne Parc 2nd Addition	9



Location Map



VICINITY MAP

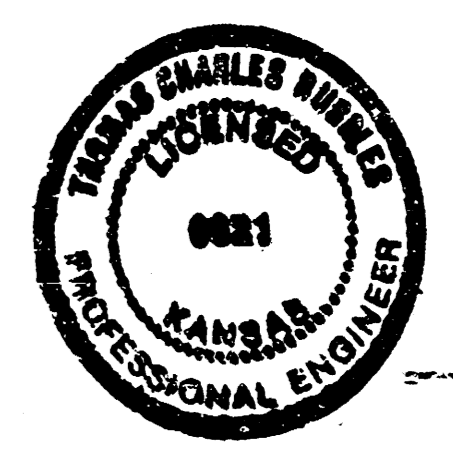
General Notes

- Utility service lines, poles, valve boxes, meters, and etcetera are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the Contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction.
- The Contractor shall be responsible for preserving property irons. The Contractor will be required to re-establish any property irons which are damaged or destroyed by his construction operations. Such irons shall be re-established by a licensed land surveyor in accordance with state laws.

NOTE TO CONTRACTOR
 INSPECTION AND TESTING FOR THIS PROJECT IS TO BE PROVIDED BY THE OWNER/DEVELOPER. SAID INSPECTION TO BE IN ACCORDANCE WITH THE CITY OF WICHITA STANDARDS AND PROFESSIONAL ENGINEERING PRACTICES AND CERTIFIED BY A LICENSED PROFESSIONAL ENGINEER. NO WORK SHALL BE PERFORMED IN DEDICATED EASEMENTS OR THE PUBLIC RIGHT-OF-WAY BY THE CONTRACTOR WITHOUT SUCH INSPECTION NOR SHALL ANY WORK BE COMMENCED IN DEDICATED EASEMENTS OR PUBLIC RIGHT-OF-WAY WITHOUT WRITTEN AUTHORIZATION BY THE CITY ENGINEER.

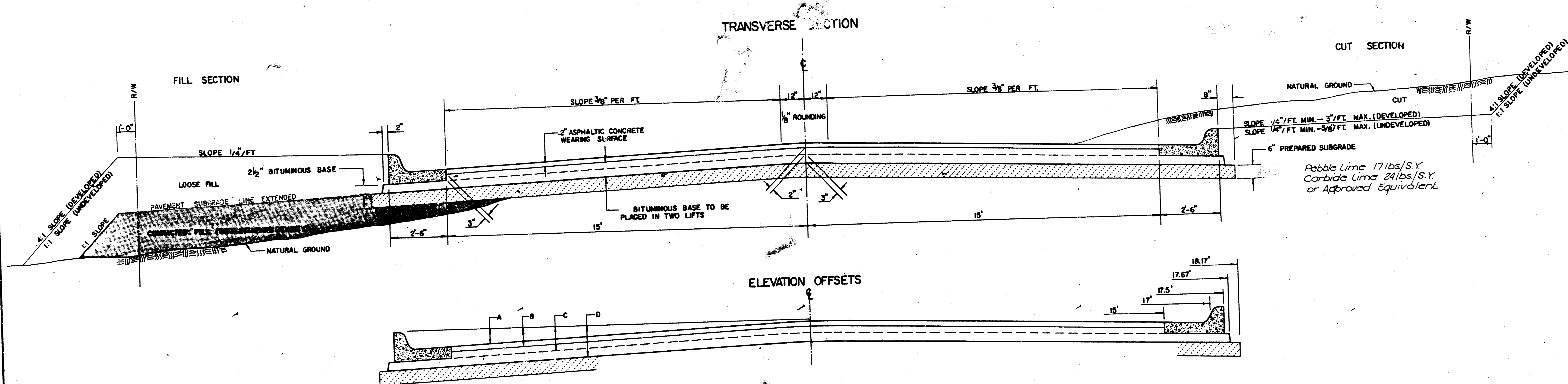
APPROVED AS ENGINEER
 By CITY ENGINEER OF WICHITA

Sanitary Sewers _____
 Storm Sewers _____
 Driveway Approaches _____
 Water Mains _____
 Paving VRH 2/21/87



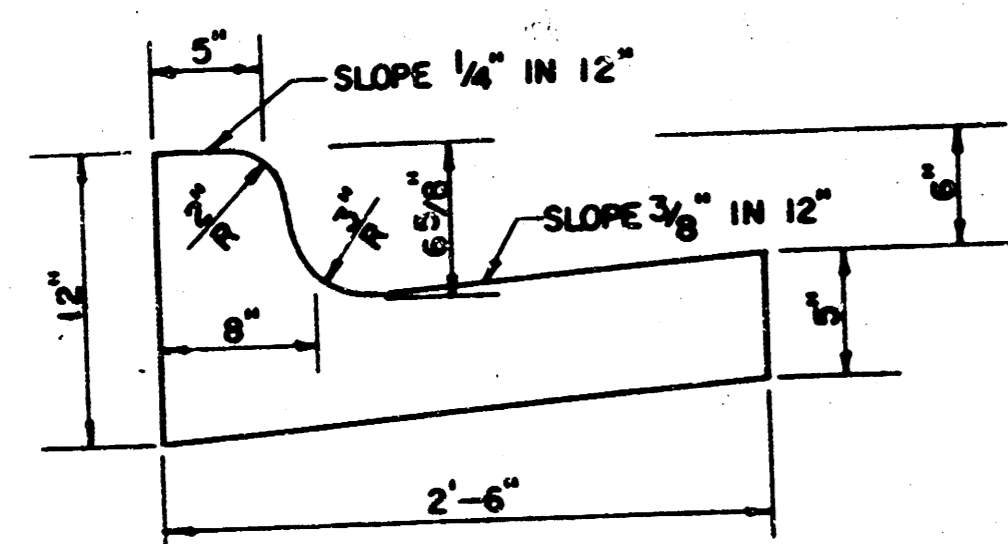
Revised as built 5-4-87
 Revised for Phase II Construction 12-8-88
BAUGHMAN COMPANY P. A.
 SURVEYING & ENGINEERING
 316/282-7271 • 315 ELLIS • WICHITA, KANSAS 67211

TYPICAL 35' PAVEMENT DETAILS

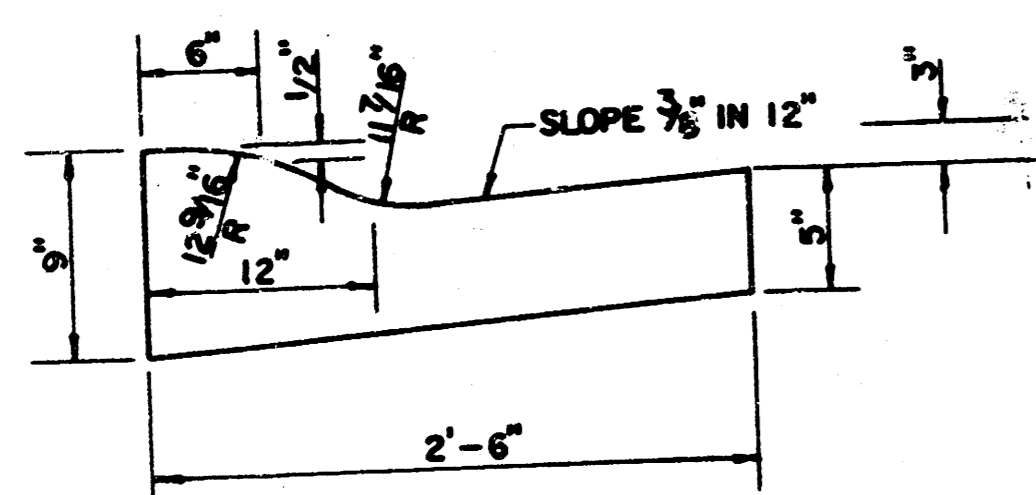


	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0'	2'	4'	6"	8.5'	10'	12'	14'	15'	17'	17.5'	17.67'	18.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.04	0.08	0.14	0.21	0.29	0.33	0.39	0.46	0.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.21	0.25	0.31	0.37	0.45	0.50	0.56	0.62	0.65	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.37	0.43	0.50	0.57	0.67	0.72	0.79	0.87	0.90	0.98	1.00	1.00	—
D: TOP OF CURBS TO TOP OF SUBGRADE	0.62	0.67	0.74	0.81	0.90	0.95	1.02	1.08	1.12	1.19	1.21	1.21	1.23

COMBINED CURB & GUTTER



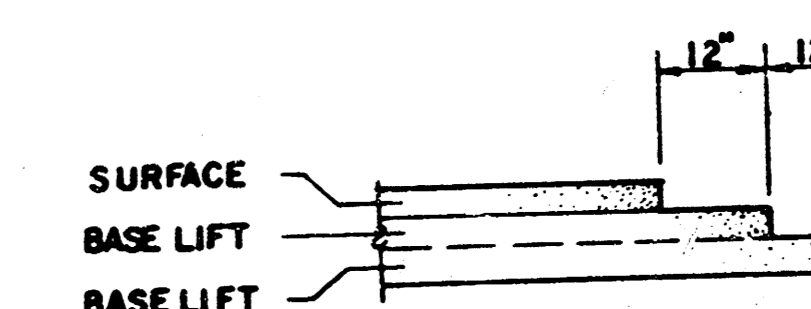
ROLL TYPE COMBINED CURB & GUTTER



GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

TRANSVERSE CONSTRUCTION JOINTS

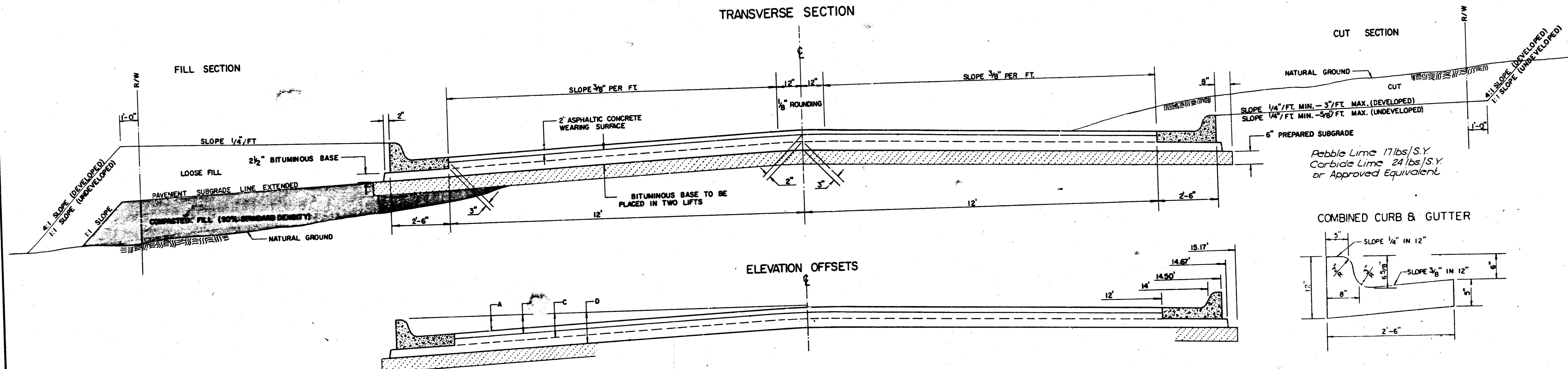


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

7 INCH RESIDENTIAL ASPHALTIC CONCRETE
PAVEMENT WITH 5 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS 2
PROJECT NUMBER
472 - 76-245-80001-000-000-051 9

TYPICAL 29' PAVEMENT DETAILS

TRANSVERSE SECTION



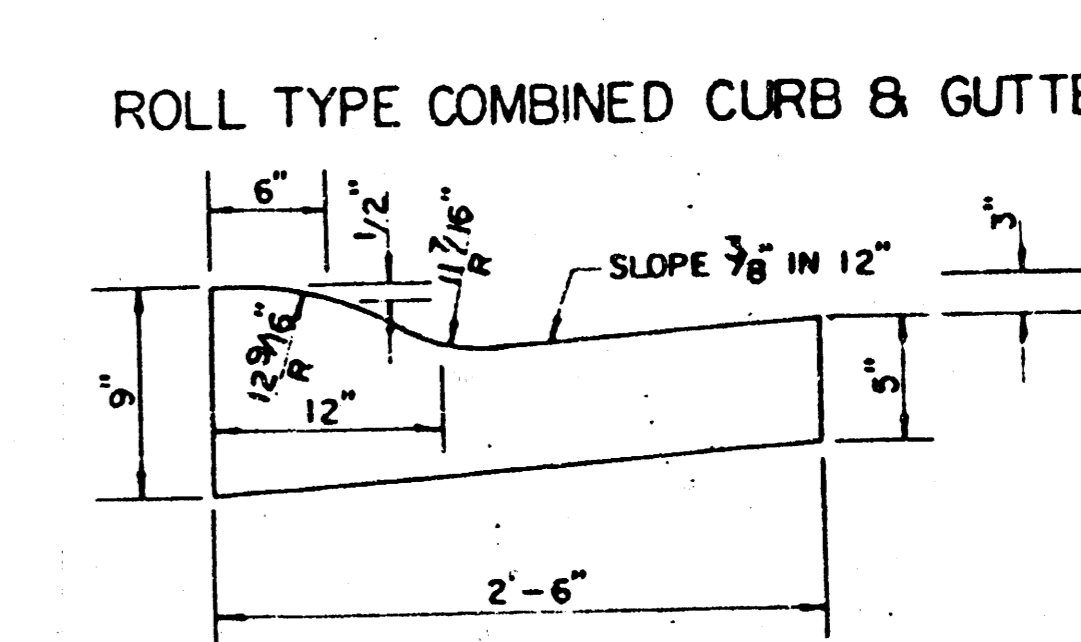
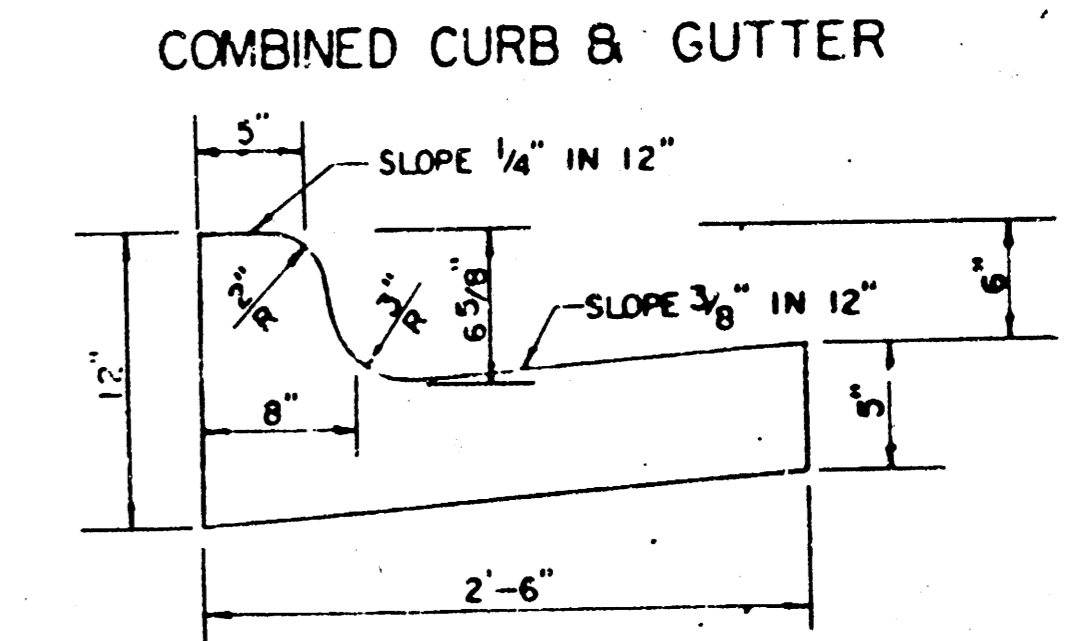
CUT SECTION

NATURAL GROUND
CUT

SLOPE 1/4" / FT. MIN. - 3" / FT. MAX. (DEVELOPED)
SLOPE 1/4" / FT. MIN. - 5" / FT. MAX. (UNDEVELOPED)

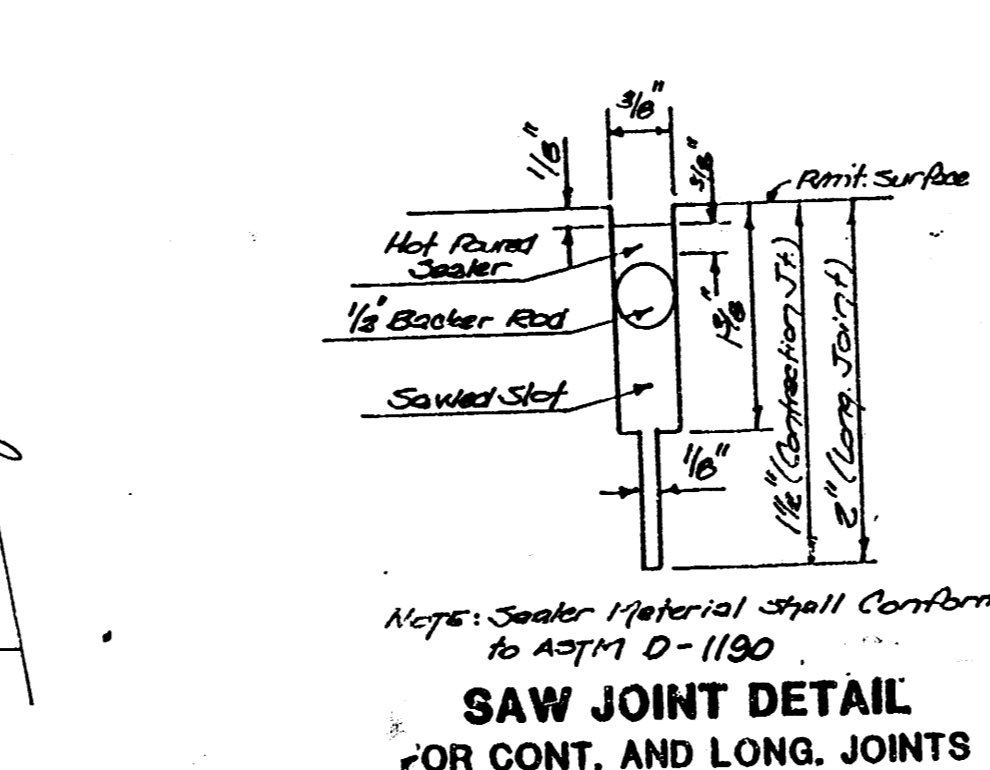
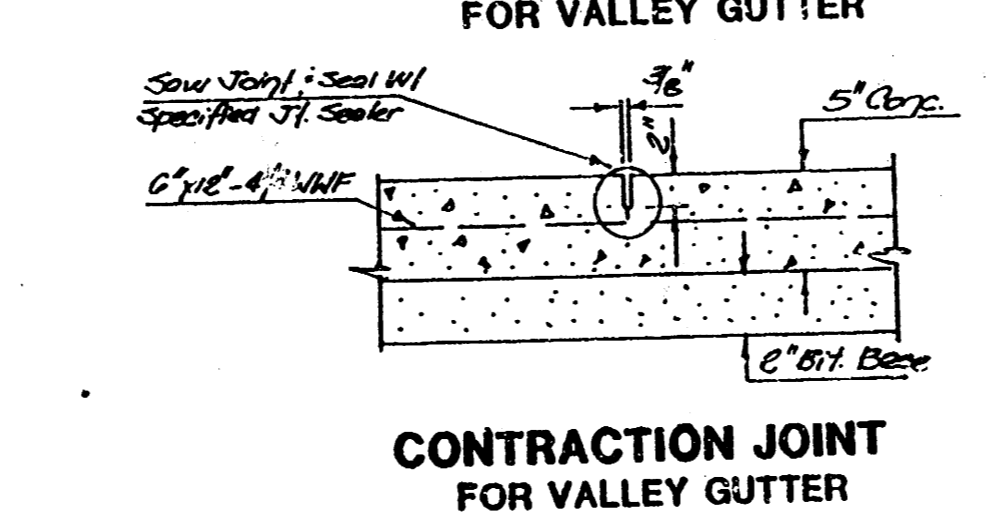
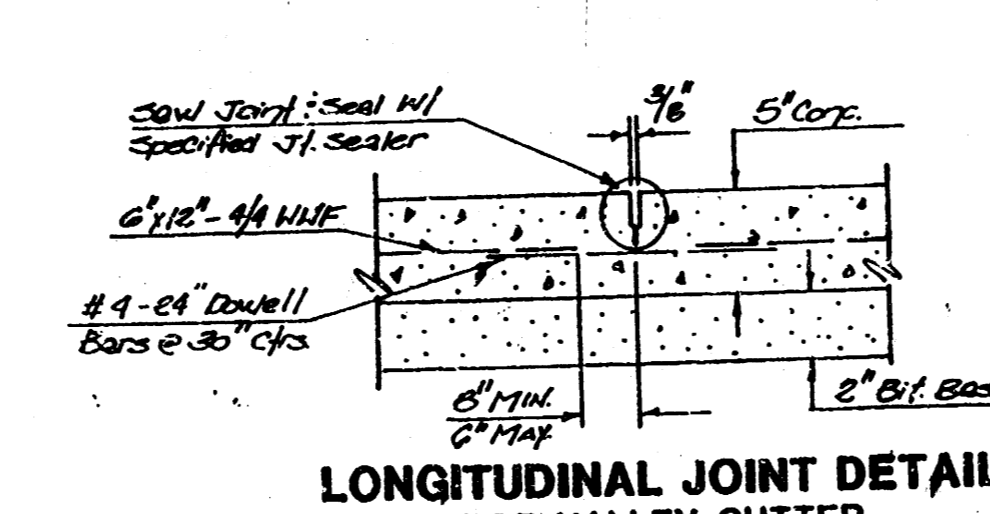
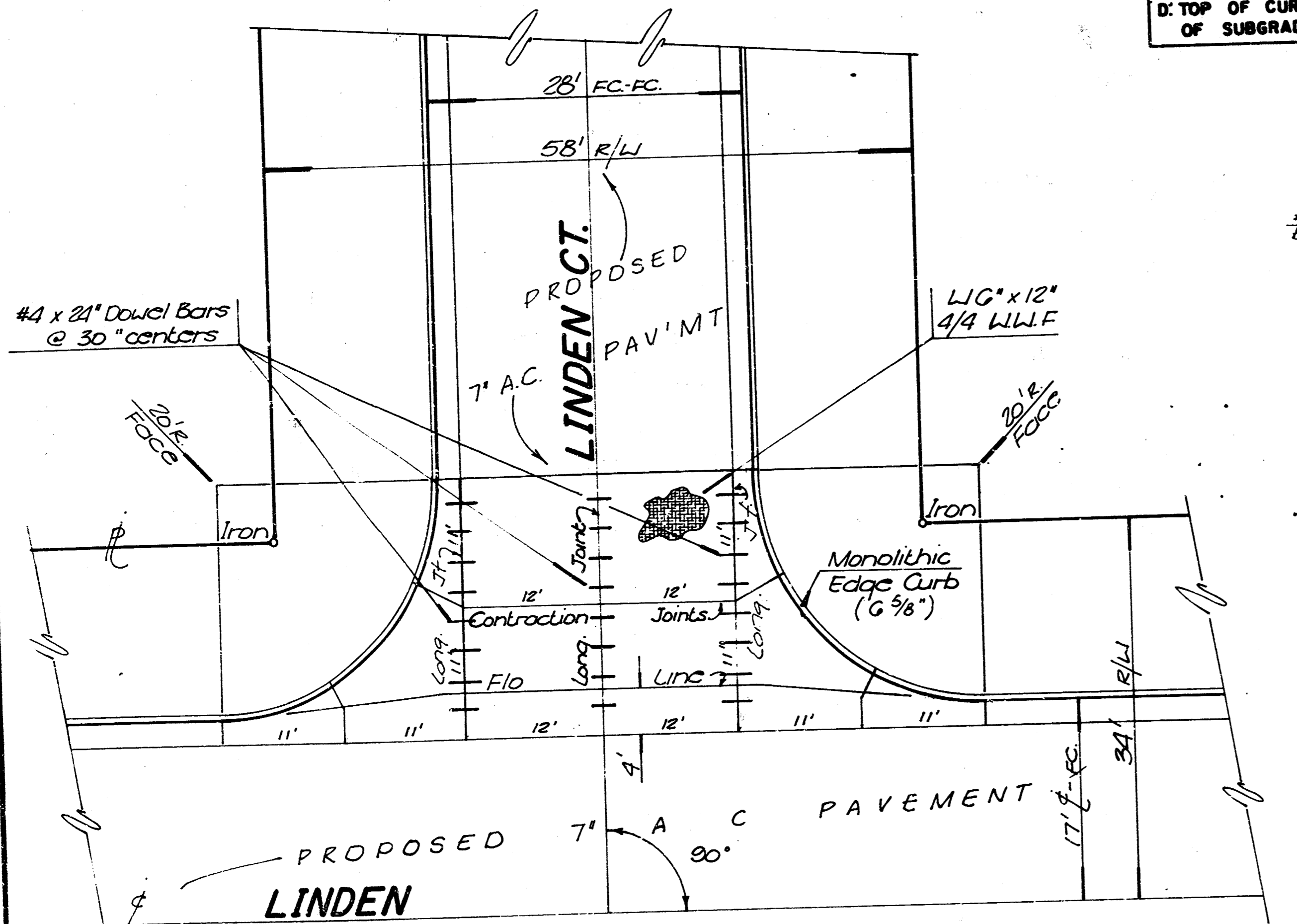
6" PREPARED SUBGRADE

*Pebble Lime 17 lbs / S.Y.
Carbide Lime 24 lbs / S.Y.
or Approved Equivalent*



	DISTANCE FROM CENTERLINE (LT. & RT.)											
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	14.67'	15.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.47	0.52	0.60	0.68	0.71	0.75	0.83	0.90	0.98	1.00	1.01	—
D: TOP OF CURBS TO TOP OF SUBGRADE	0.72	0.77	0.84	0.91	0.94	0.98	1.05	1.12	1.19	1.21	1.21	1.23

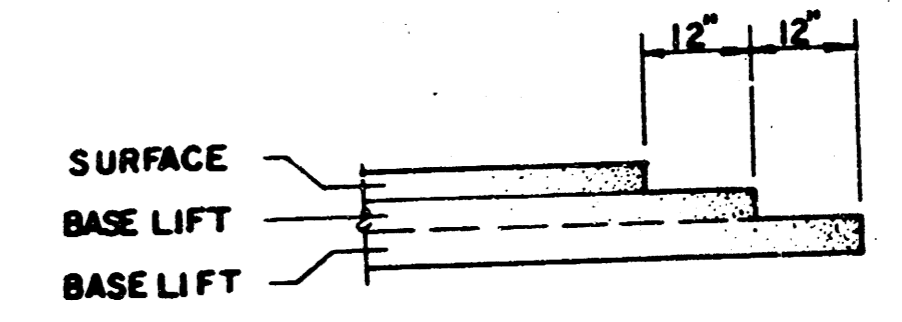
VALLEY GUTTER DETAIL



GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

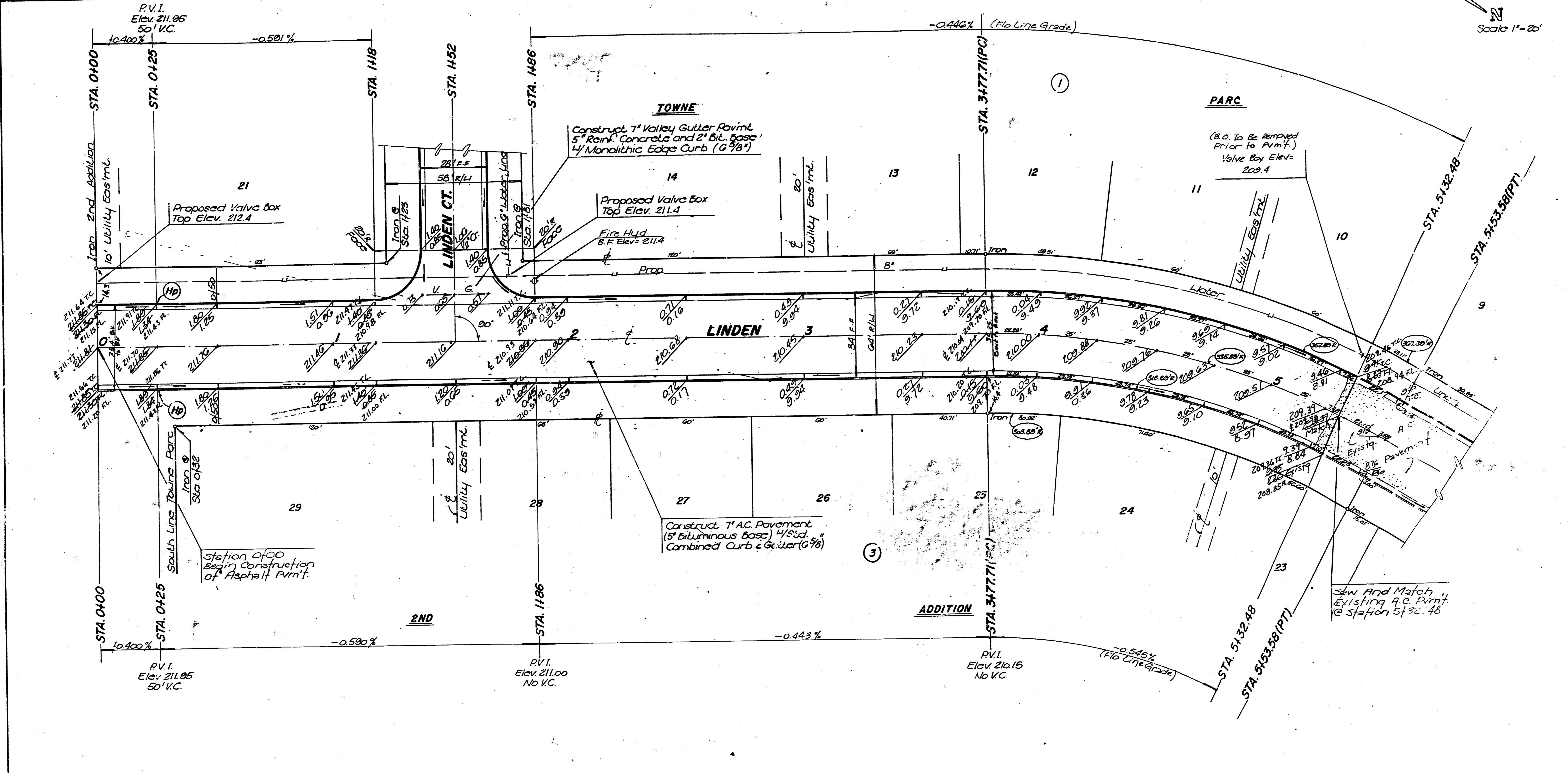
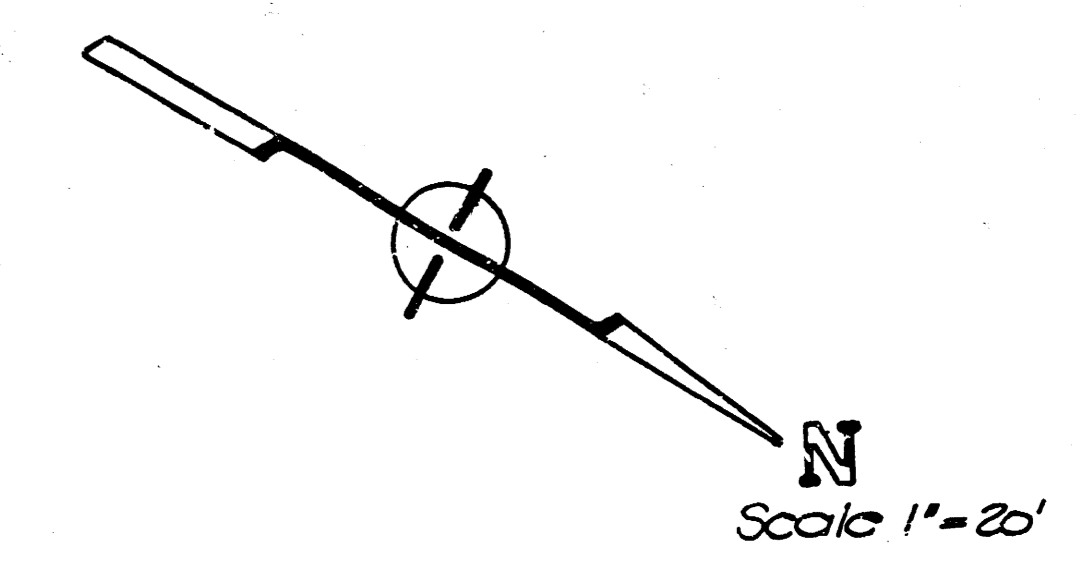
TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS
PROJECT NUMBER
472-76-245-80001-000-000-051

BENCHMARKS
 Bench Mark 1: 1' dia Cut. Top of Curb 68' ± W. of 1/4 Section Line S. side of Hurst. Elevation 211.22 City Datum
 Bench Mark 2: 1' dia Cut. Top of Curb on the S. side of Carson and W. R./W. of Linden. Elevation 201.71 City Datum



CURVE DATA BASED ON CENTERLINE DATA

$R = 335.89'$, $\Delta = 30^\circ 00'$, $L = 175.81'$, $Chd. = 173.81'$, $Tan. = 90'$, $Del/ft. = 5.1174'$ min.

Station	Arc Length	CHORD LENGTHS		Deflection Angle	Total Deflection Angle
		8' Left/Face	8' Right/Face		
3+77.71 (PC)	—	—	—	0°00'	0°00'
4+00	22.29	23.95'	20.03'	1°54'04"	1°54'04"
4+25	25	26.85'	23.13'	2°07'52"	4°02'00"
4+50	25	26.85'	23.13'	2°07'52"	6°09'52"
4+75	25	26.85'	23.13'	2°07'52"	8°17'44"
5+00	25	26.85'	23.13'	2°07'52"	10°25'36"
5+25	25	26.85'	23.13'	2°07'52"	12°33'28"
5+32.48	7.48	8.03'	6.82'	0°38'10"	13°12'01"
5+33.58 (PT)	2.10	22.07'	19.83'	1°47'59"	15°00'00"

Revised as built 5-4-89

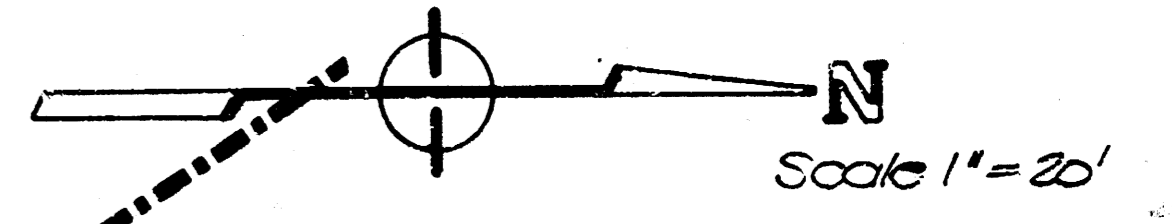
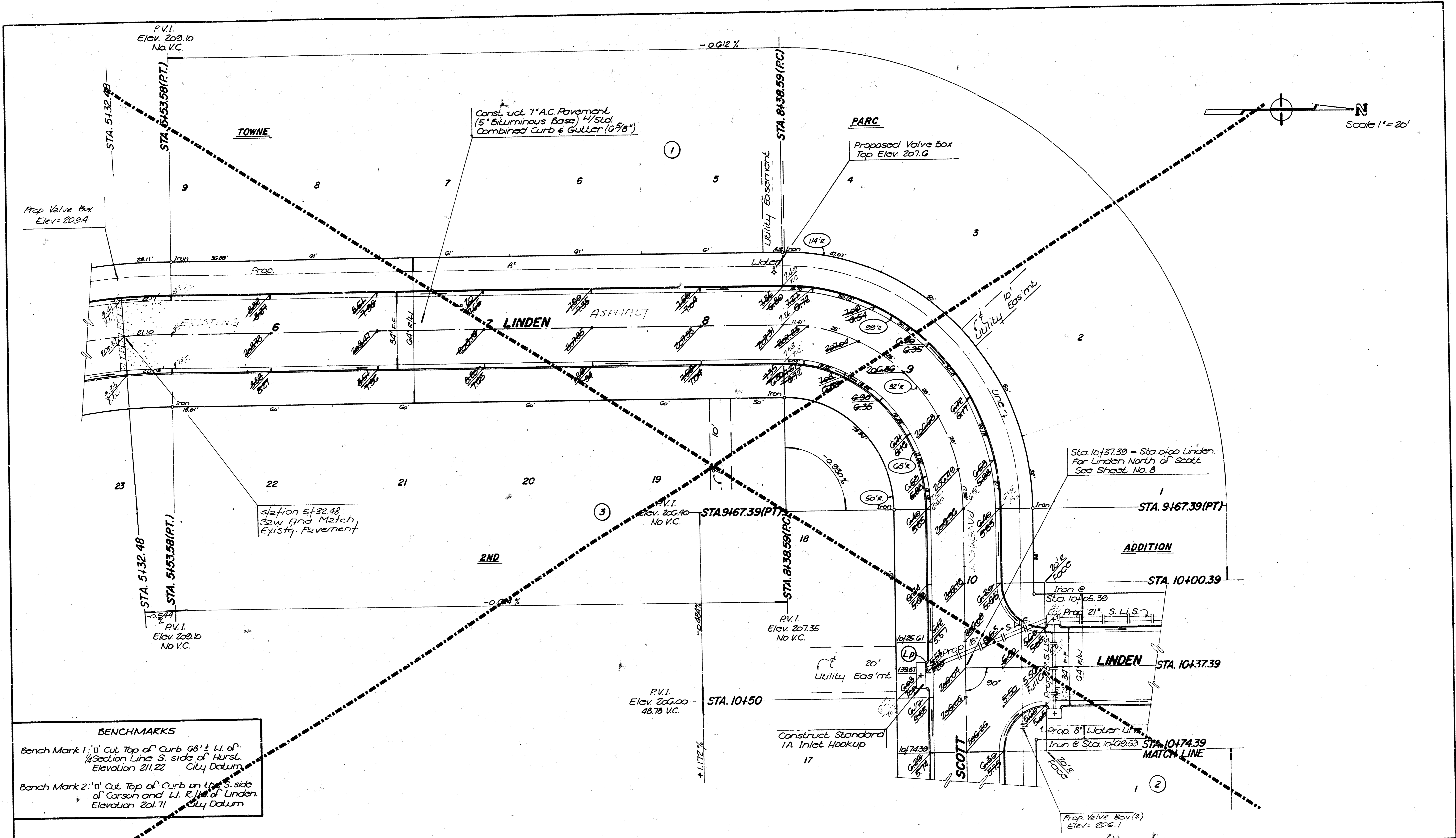
LINDEN
 STA. 0+00 TO STA. 5+32.48

BAUGHMAN COMPANY P. A.
 SURVEYING & ENGINEERING
 316/282-7271 • 315 ELLIS • WICHITA, KANSAS 67211

PROJECT NUMBER: 472-76-245-80001-000-000-051

DESIGN: C. Bohm DRAWN: R.J. Plush APPROVED: T.C.R. DATE: SCALE: 1" = 20'

SHEET 4 OF 9



BENCHMARKS

Bench Mark 1: 1'0\"/>

Bench Mark 2: 1'0\"/>

CURVE DATA BASED ON CENTERLINE DATA

R = 82' Δ = 90°00' Chd. = 115.97' Tan. = 82' Del./ft. = 20.9627/min.

Station	Arc Length	CHORD LENGTHS		Deflection Angle	Total Deflection Angle
		8' Left/Face	8' Right/Face		
8132.50 PC	—	—	—	0'00"	0'00"
8150	11.41'	14.88'	7.95'	3'50'11"	3'59'11"
8175	25'	32.60'	17.31'	8'44'04"	12'43'16"
8200	25'	32.60'	17.31'	8'44'04"	21'27'19"
8225	25'	32.60'	17.31'	8'44'04"	30'11'23"
8250	25'	32.60'	17.31'	8'44'04"	38'55'27"
8275	25'	32.60'	17.31'	8'44'04"	47'39'31"
8300 PT	17.39'	22.25'	12.07'	0'01'33"	48'00'00"

Note: Pavement on this sheet has been constructed. Provided for information only.

LINDEN & SCOTT
STA. 5153.58 TO STA. 10474.39

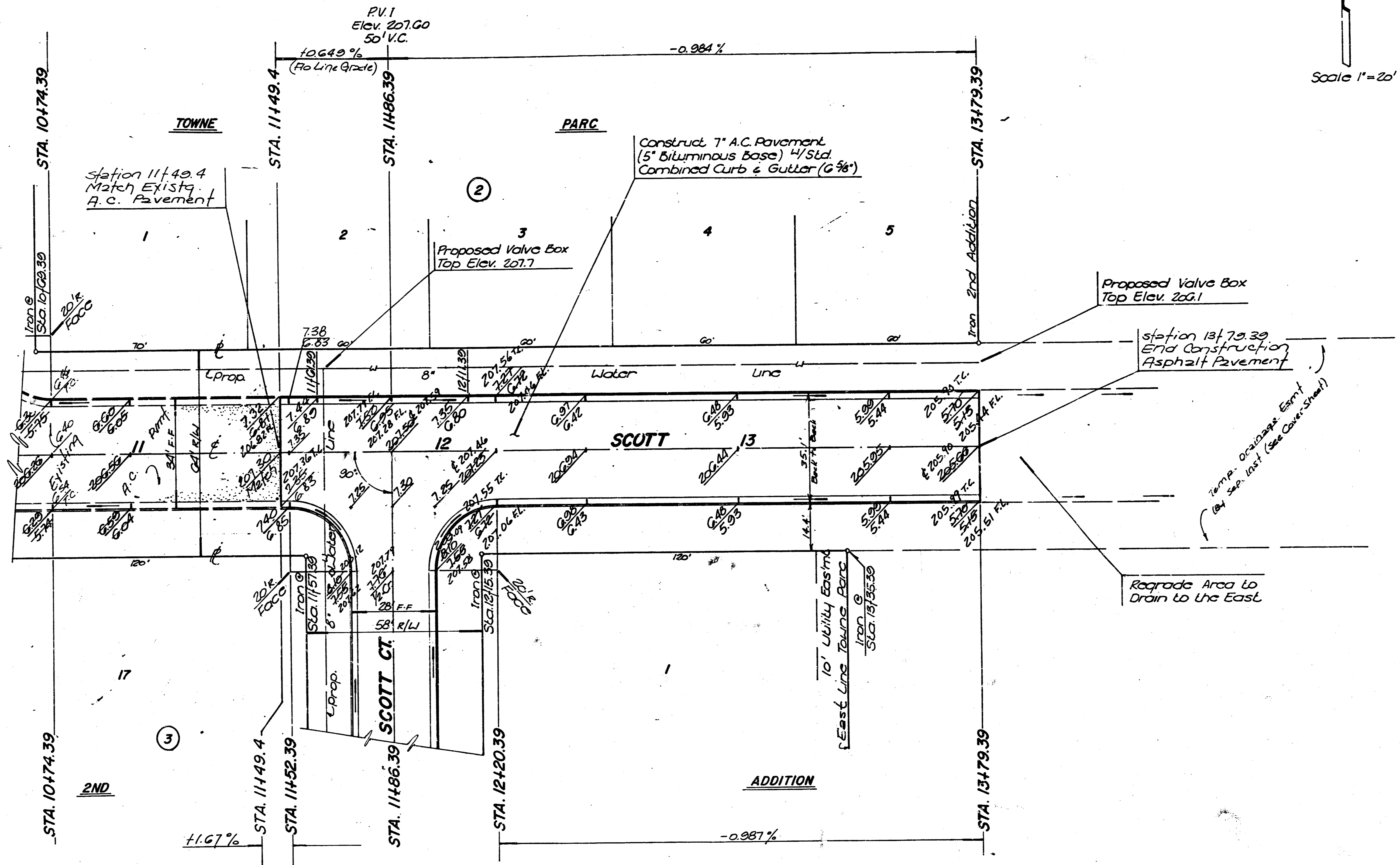
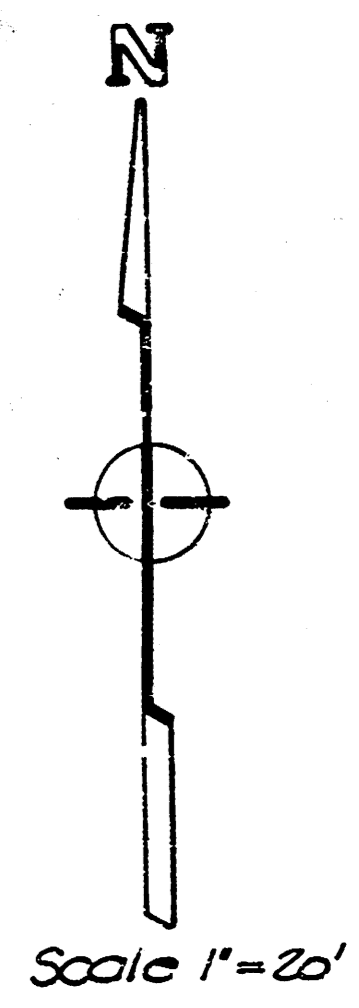
BAUGHMAN COMPANY P. A.
SURVEYING & ENGINEERING
318/282-7271 • 315 ELLIS • WICHITA, KANSAS 67211

PROJECT NUMBER: 472-76-245-80001-000-000-051

DESIGN: C. Bohm DRAWN: R. J. Aush APPROVED: T. C. E. DATE: SCALE: 1" = 20'

REV. 5/9 SHEET 5 OF 9

BENCHMARKS
 Bench Mark 1: 'd' Cut Top of Curb 68'± W. of Section Line S. side of Hurst. Elevation 211.22 City Datum
 Bench Mark 2: 'd' Cut Top of Curb on the S. side of Carson and W. R. L. of Linden. Elevation 201.71 City Datum



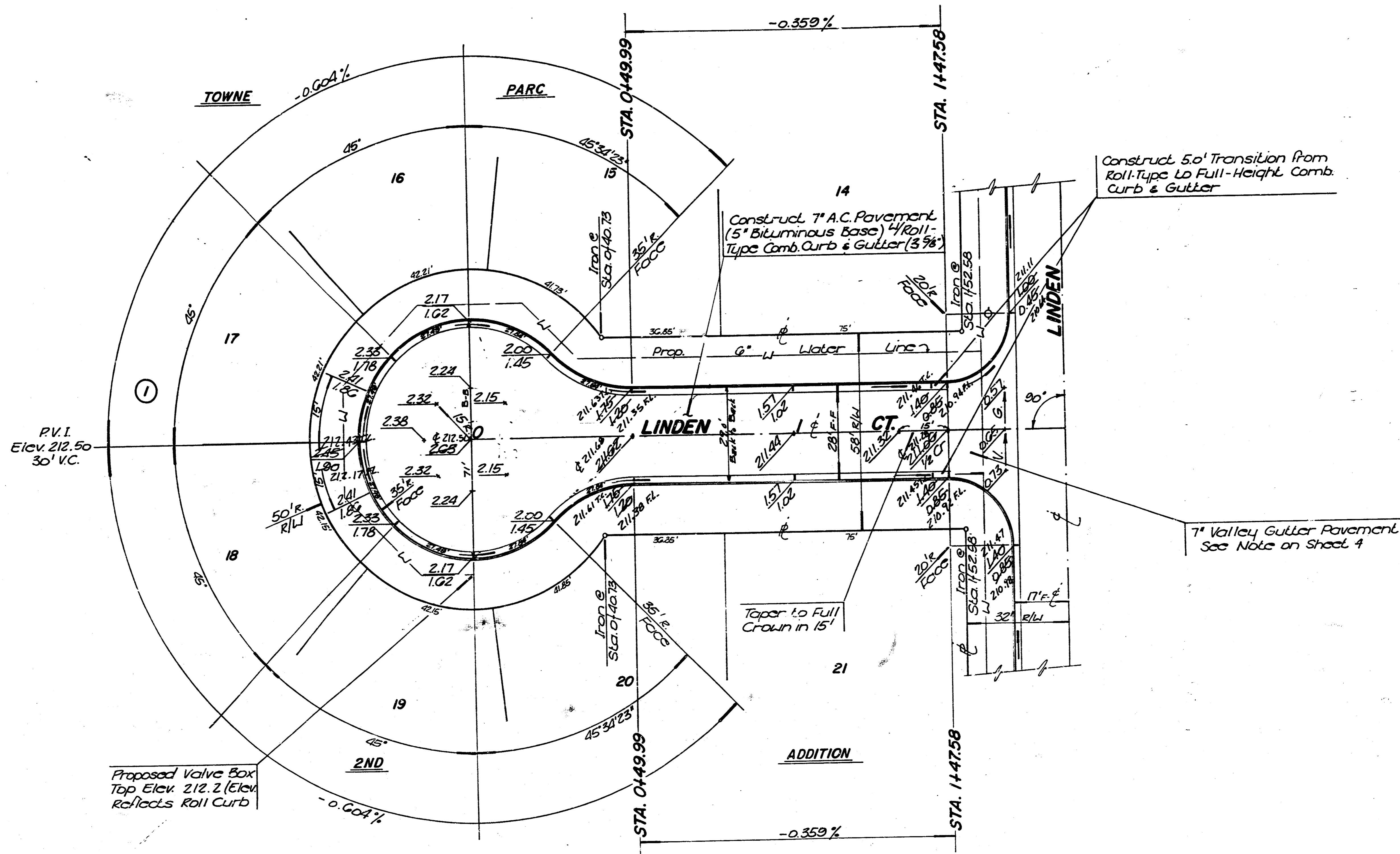
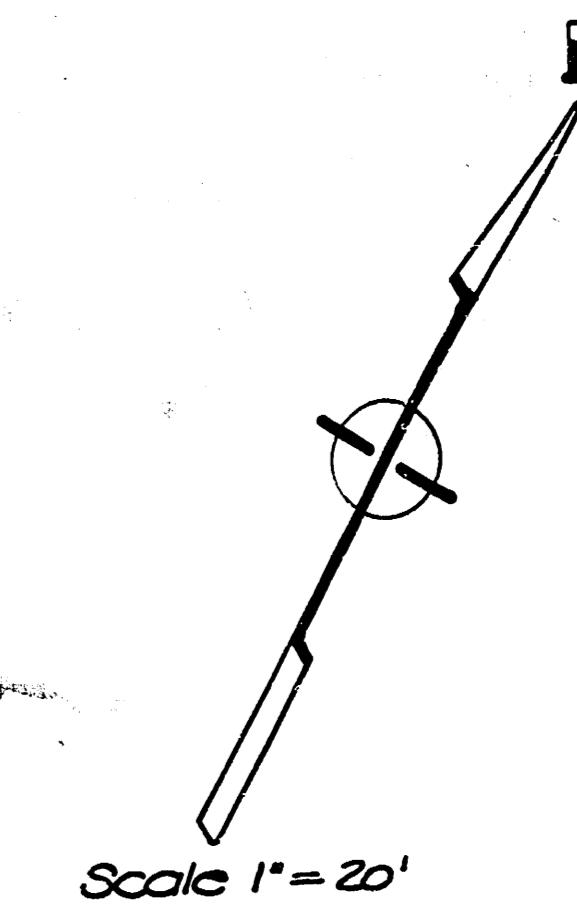
Revised as built 5-4-89

SCOTT			
STA. 11149.40 TO STA. 13479.39			
BAUGHMAN COMPANY P. A.		REV.	
SURVEYING & ENGINEERING		316/262-7271 • 315 ELLIS • WICHITA, KANSAS 67211	
PROJECT NUMBER			SHEET
472-76-245-80001-000-000-051			6
DESIGN	DRAWN	APPROVED	DATE
C. Bohm	R. J. Plush	T. C. R.	
SCALE			OF
1" = 20'			9

BENCHMARKS

Bench Mark 1: 'a' Cut Top of Curb 68' ± W. of
1/4 Section Line S. side of Hurst.
Elevation 211.22 City Datum

Bench Mark 2: 'a' Cut Top of Curb on the S. St.
of Carson and W. R. 1/4 of Linden.
Elevation 201.71 City Datum



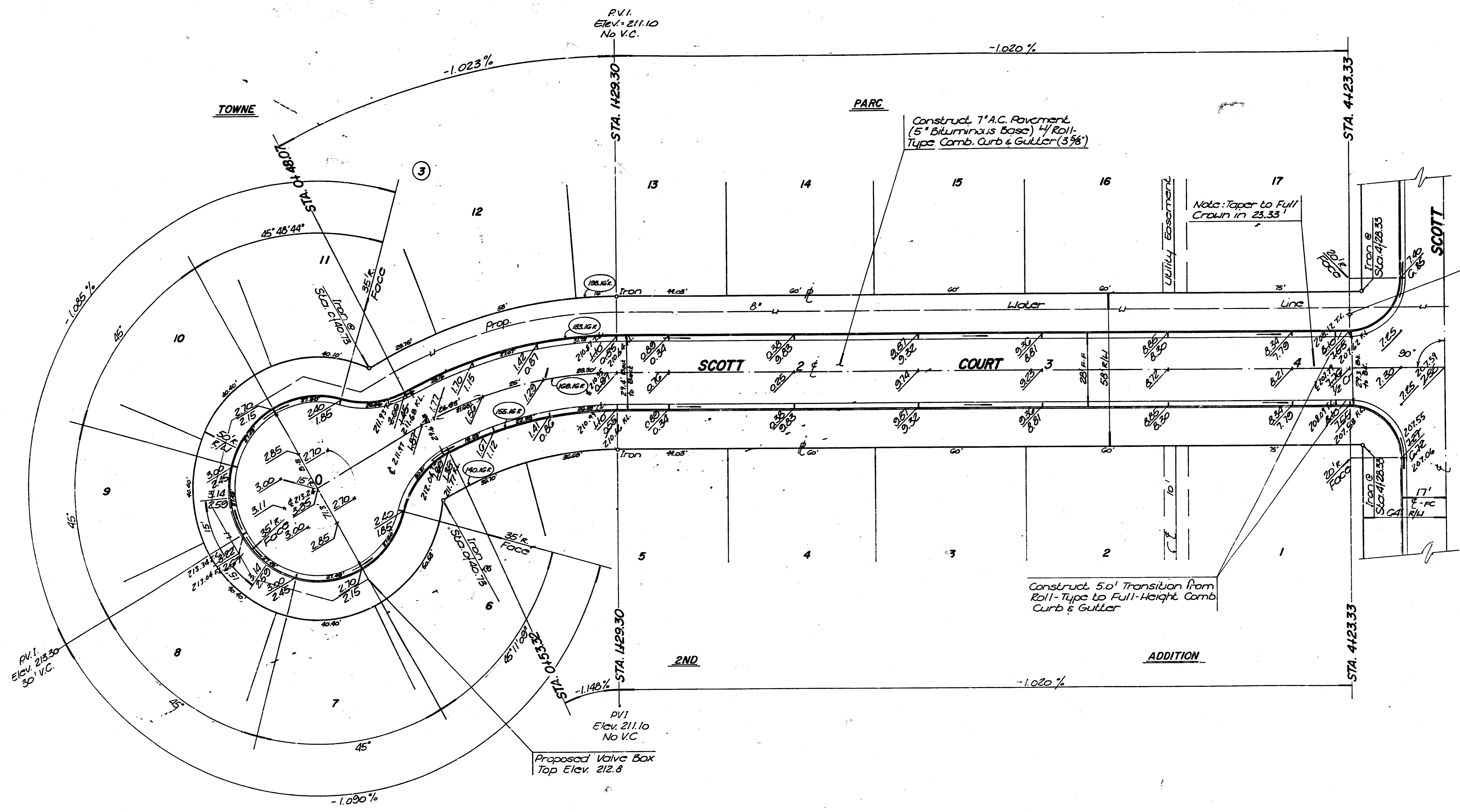
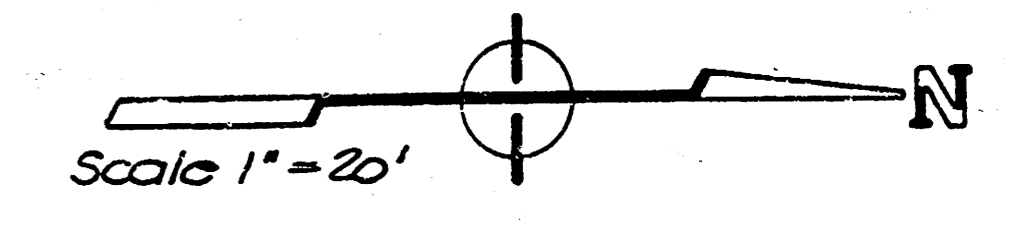
Proposed Valve Box
Top Elev. 212.2 (Elev.
Reflects Roll Curb)

NOTE
Roll-Type Combined Curb & Gutter (3 5/8") is to
be Constructed in Linden Court. Transitions
from Roll-Type to Full-Height Curb to be paid
for as LF of Full-Height Comb. Curb & Gutter (6 5/8")

Revised as built 5-4-89

LINDEN CT.			
STA. 0+00 TO STA. 1+84.58			
BAUGHMAN COMPANY P. A.		REV.	
SURVEYING & ENGINEERING		7	
316/262-7271 • 315 E. J. LIS • WICHITA, KANSAS 67211		SHEET	
PROJECT NUMBER		OF 9	
472-76-245-80001-000-000-051			
DESIGN	DRAWN	APPROVED	DATE
C. Bohm	R. J. Plush	T. C. R.	
			SCALE
			1" = 20'

BENCHMARKS
 Bench Mark 1: 1'0" Cut Top of Curb 68' ± W. of 1/4 Section Line S. side of Hurst. Elevation 211.22 City Datum
 Bench Mark 2: 1'0" Cut Top of Curb on the S. side of Carson and L.L. R. W. of Under. Elevation 201.71 City Datum



CURVE DATA BASED ON CENTERLINE DATA

$R = 169.16$ $\Delta = 30^\circ$ $L = 88.57$ $Chd. = 87.56$ $Tan. = 45.33$ $Del./ft. = 10.1645$ $min.$

Station	Arc Length	CHORD LENGTHS		Deflection Angle	Total Deflection Angle
		B'Left/Face	B'Right/Face		
0+40.73 PC	—	—	—	0°00'	0°00'
0+48.0714	7.34	8.29	—	1°14'35"	1°14'35"
0+53.32 (CL)	5.25	10.95	—	0°53'21"	2°07'56"
0+75	21.68	30.40	18.86	3°40'18"	5°48'14"
1+00	25	28.23	21.73	4°14'02"	10°02'16"
1+29.30 PT	29.30	33.07	25.45	4°57'44"	15°00'00"

NOTE
 Roll-Type Combined Curb & Gutter (3 3/8") is to be constructed in Scott Court. Transitions from Roll-Type to Full-Height Curb to be paid for as LF of Full-Height Comb. Curb & Gutter (2 5/8")

Revised as built 5-4-89

SCOTT COURT
 STA. 0+00 TO STA. 4+60.33

B BAUGHMAN COMPANY P. A.
 SURVEYING & ENGINEERING
 316-282-7271 • 315 ELLIS • WICHITA, KANSAS 67211

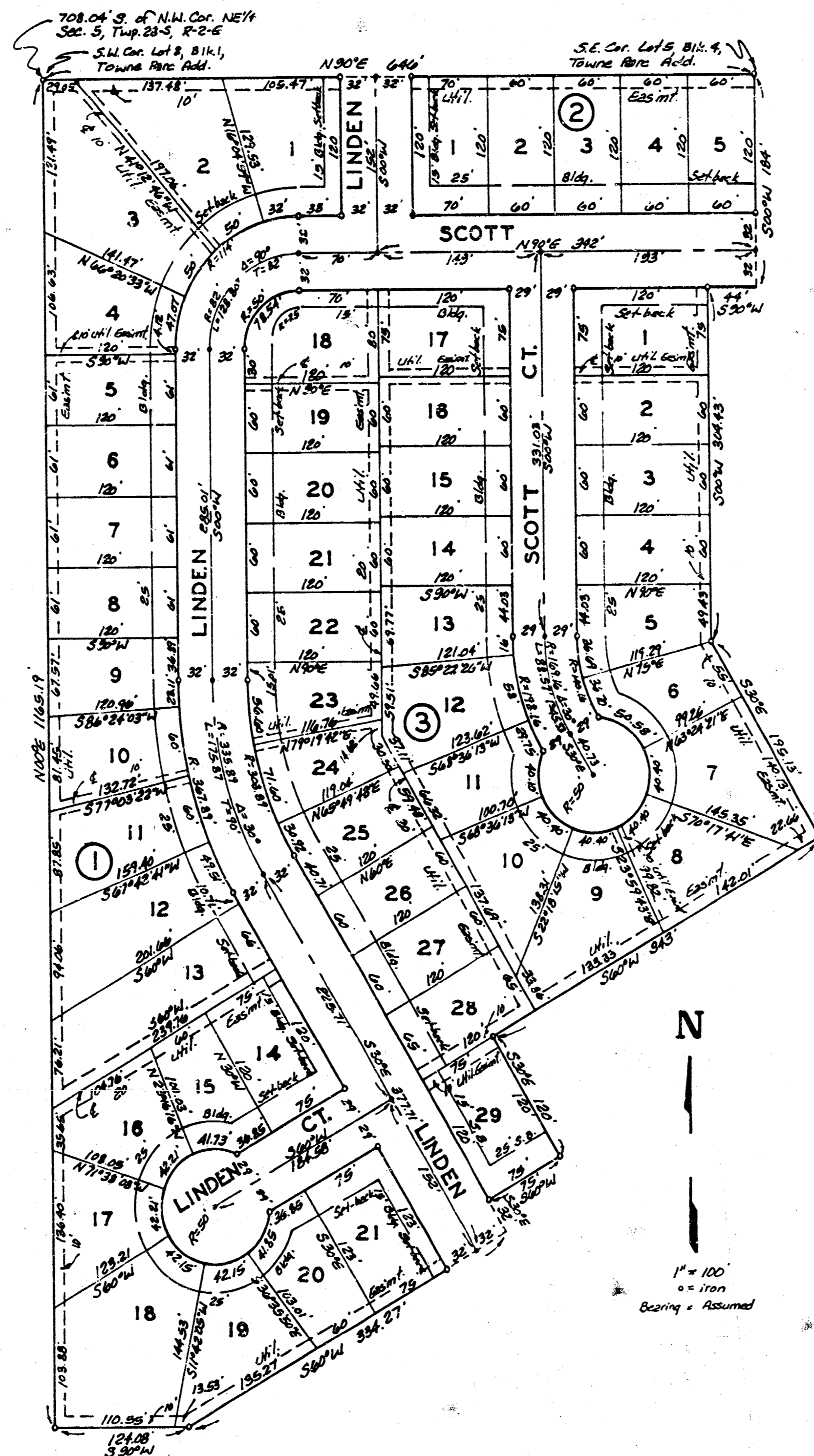
PROJECT NUMBER
472-76-245-80001-000-000-051

DESIGN: C. Bohm DRAWN: R. J. Plush APPROVED: T. C. E. DATE: SCALE: 1" = 20'

SHEET 8 OF 3

TOWNE PARC 2ND ADDITION

WICHITA, KANSAS



This plat of "TOWNE PARC 2ND ADDITION", Wichita, Kansas, has been submitted to and approved by the Wichita-Sedgewick County Metropolitan Area Planning Commission, Wichita, Kansas, dated this _____ day of _____, 1988.

Elton Parsons Chairman

Marvin S. Kneut Secretary

This plat approved and all dedications shown hereon accepted by the City Council of the City of Wichita, Kansas, this _____ day of _____, 1988.

Sheldon Kamen Mayor

Dale E. Resa Deputy City Clerk

Entered on transfer record this _____ day of _____, 1988.

Don Wright County Clerk

State of Kansas } ss. This is to certify that this plat has been filed for record in the office of the Register of Deeds, this _____ day of _____, 1988 at _____ o'clock _____ M.; and is duly recorded.

Pat Kettler Register of Deeds

Ed Resa Deputy

Know all men by these presents that we, the undersigned, have caused the land described in the surveyors certificate to be platted into Lots, Blocks and Streets to be known as "TOWNE PARC 2ND ADDITION", Wichita, Kansas. The utility easements are hereby granted as indicated for the construction and maintenance of all public utilities. The streets are hereby dedicated to and for the use of the public.

Bernard Novick Betty Novick
I. T. J. Investments, Inc.

Irma N. Jacoby President

State of Kansas } ss. The foregoing instrument was acknowledged before me, this _____ day of _____, 1988, by Bernard Novick and Betty Novick, his wife and by Irma N. Jacoby, President of I. T. J. Investments, Inc. on behalf of the corporation.

My Appt. Exp. _____

Larry K. Meeker Notary Public

State of Kansas } ss. We, Baughman Company, P.A., Surveyors in and Sedgewick County and state do hereby certify that we have surveyed and platted "TOWNE PARC 2ND ADDITION", Wichita, Kansas and that the accompanying plat is a true and correct exhibit of the property surveyed, described as that part of the W 1/2 of the NE 1/4 of Sec. 5, Twp. 28-S, R. 2-E of the 6th P.M., Sedgewick County, Kansas, commencing at the N.W. corner of the W 1/2 of said NE 1/4; thence S 00° W, along the west line of said NE 1/4, 708.04 feet to the S.W. corner of Block 1, Towne Parc Addition, Wichita, Kansas, for a place of beginning; thence N 80° E along the south line of said Towne Parc Addition, 446 feet to the S.E. corner of Lot 5, Block 4, in said Addition; thence S 00° W, 184 feet; thence S 30° W, 44 feet; thence S 00° W, 304.43 feet; thence S 30° E, 195.13 feet; thence S 60° W, 343 feet; thence S 30° E, 120 feet; thence S 60° W, 75 feet; thence S 30° E, 32 feet; thence S 60° W, 334.27 feet; thence S 30° W, 124.08 feet to the west line of said NE 1/4; thence N 00° E, along the west line of said NE 1/4, 1163.19 feet to the place of beginning.

Baughman Company, P.A.

Mark A. Savoy Surveyor