

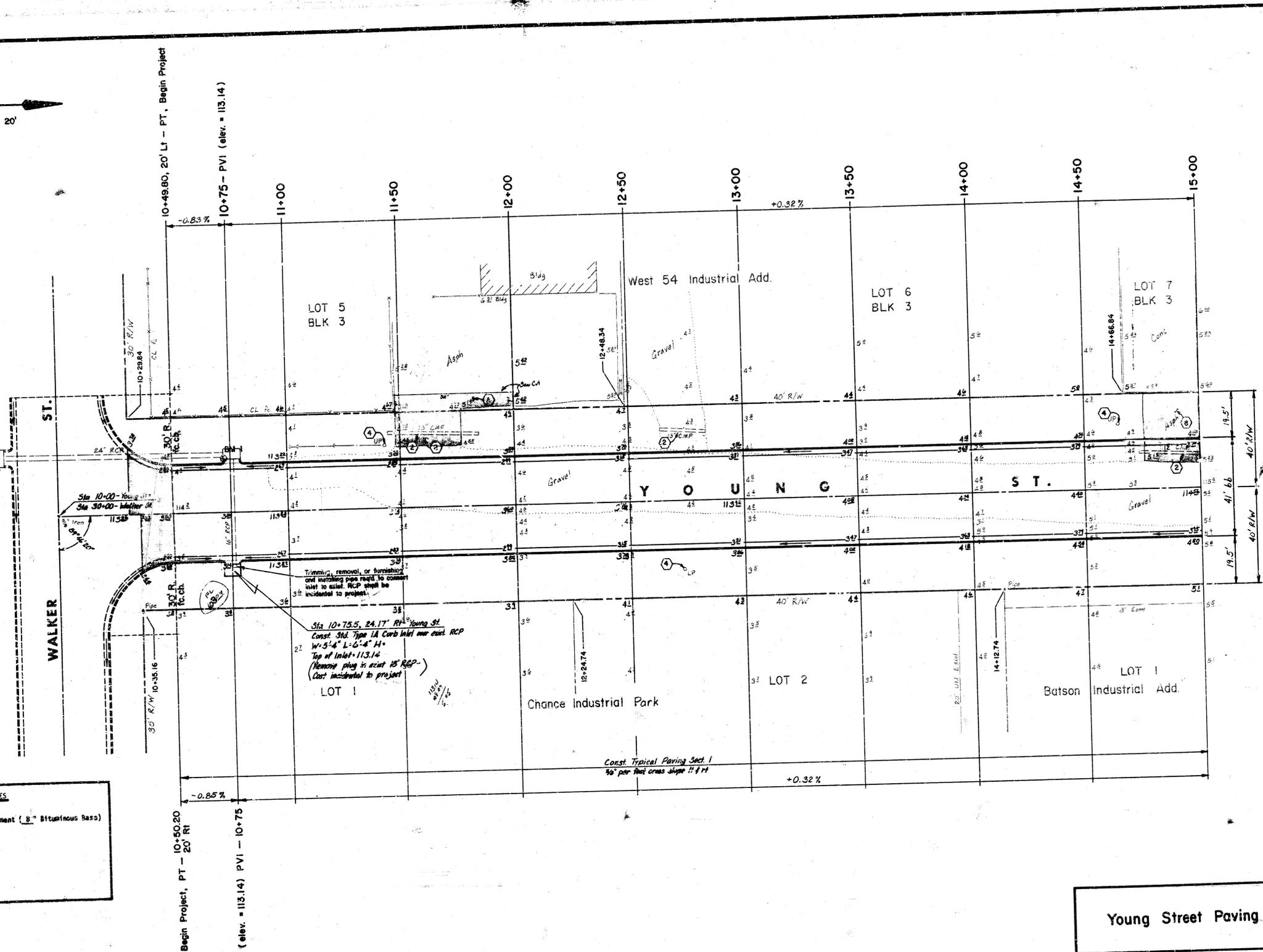
- NOTES**
- 1 Relocate (by others)
 - 2 Remove
 - 3 Remove and replace
 - 4 Protect, adjust or relocate (by others)
 - 5 Adjust as required (by others)
 - 6 Leave in place, Do Not Disturb
 - 7 Remove mat or pavement as required and replace with temporary asphalt
 - 8 Remove entrance
 - 9 Construct concrete entrance
 - A Remove & Replace asphalt

- D.M. 81 - Chiseled "C" on SE corner storm inlet Young St
 10+72, 23' Lt; Walker St
 25+76.7, 71.9' Lt
 Elevation = 113.14
- D.M. 82 - 60d spike step in E face of PP
 Young Sta 18+77, 26' Lt.
 Elevation = 116.65
- D.M. 83 - Chiseled "C" on E edge MI ring
 Walker Sta 41+41.5, 29.5' Lt.
 Elevation = 111.87
- D.M. 84 - 60d spike step in E face of PP at MI corner of East St. and Walker St.
 Elevation = 113.57
- NOTE: Elevations shown on plans are City Data unless otherwise noted.

INTERSECTION QUANTITIES

LOCATION: YOUNG & WALKER

77.0 S.Y.	10" Asphaltic Concrete Pavement (B" Bituminous Base)
11.1 S.Y.	4" Bituminous Base
37.4 L.F.	Combined Curb and Gutter
	C.V. Excavation
	C.V. Compacted F111
200 S.Y.	Manipulation
	20' Tons Lime or Cement



EXCAVATION
 2411.3 CU YDS
 10% 241.1
 TOTAL 2652.4 CU YDS

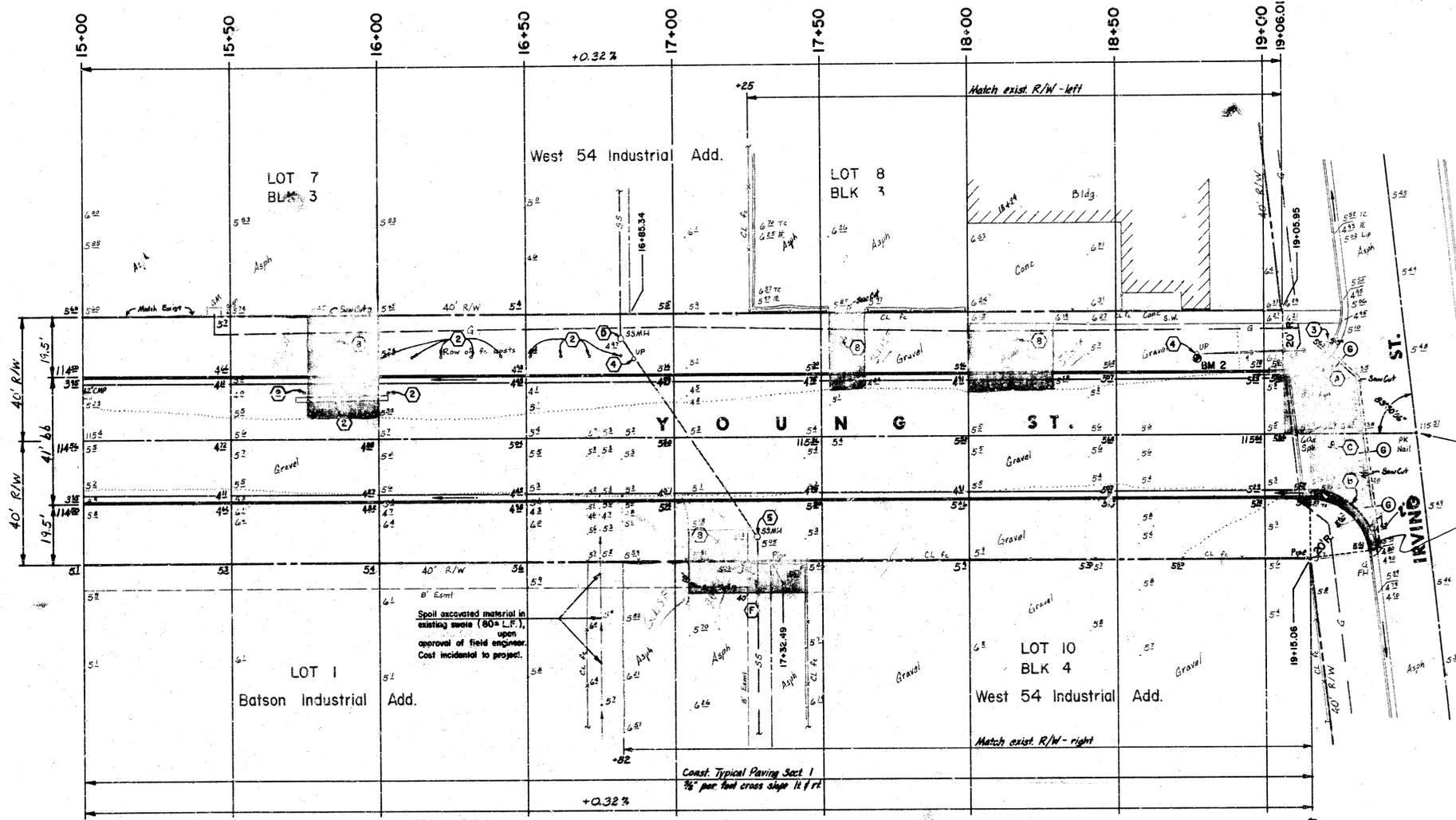
4260 SQ YDS MANIPULATION

Young Street Paving

PRC. NO. 472-73-245-B1347-000-000-001

Van Doren Hazard Stallings
 Architects - Engineers - Planners
 Topeka - Wichita - Minneapolis - Kansas City

Design	SEA
Drawn by	JUL
Checked by	SEA
Date	DEC 1984
Job No.	84-237-80
Sheet	2
	9



NOTES

- ① Relocate (by others)
 - ② Remove
 - ③ Remove and replace
 - ④ Protect, adjust or relocate (by others)
 - ⑤ Adjust as required (by others)
 - ⑥ Leave in place, Do Not Disturb
 - ⑦ Remove mat or pavement as required and replace with temporary asphalt
 - ⑧ Remove entrance
 - ⑨ Construct concrete entrance
- (A) Remove 17" LF conc. curb & gutter and replace
 - (B) Remove 29" LF conc. curb & gutter and replace
 - (C) Remove 104± SY pavement (6" conc. base with 2" asph overlay) and replace w/ full depth asph. pvt.
 - (D) Adjust SSMH. Exist elev = 114.97 Prop. elev = 115.32
 - (E) Adjust SSMH. Exist elev = 115.05 Prop. elev = 115.45
 - (F) Remove and Replace asph. mat

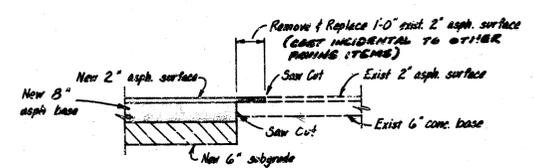
- B.M.#1 - Chiseled "C" on SE corner storm inlet Young St. 10+72, 23'±; Walker Sta 29+76.7, 71.9'±. Elevation = 113.14
 - B.M.#2 - 60d spike step in E face of PP Young Sta 16+77, 26'±. Elevation = 116.45
 - B.M.#3 - Chiseled "C" on E edge of Fire Walker Sta 41+41.5, 29.5'±. Elevation = 111.82
 - B.M.#4 - 60d spike step in W face of PP at NE corner of West St. and Walker St. Elevation = 113.57
- NOTE: Elevations shown on plans are City Maps unless otherwise noted.

EXTRA WIDTH OF GUTTER SUBSIDIARY TO CURB & GUTTER

INTERSECTION QUANTITIES

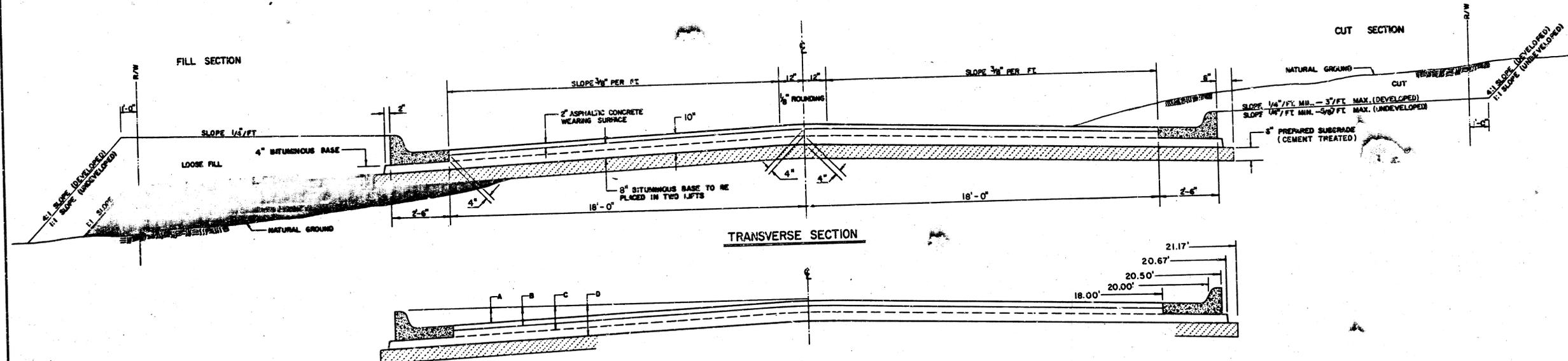
LOCATION: YOUNG & IRVING

105.0 S.Y.	10" Asphaltic Concrete Pavement (8" Bituminous Base)
14.4 S.Y.	4" Bituminous Base
47.7 L.F.	Combined Curb and Gutter
	C.V. Excavation
	C.V. Compacted Fill
121.7 S.Y.	Manipulation
3.1	Tons Lime or Cement



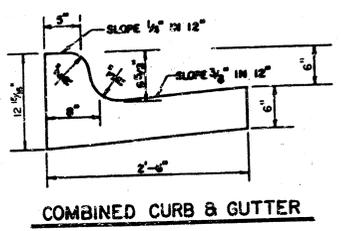
Young Street Paving		Design SEA Drawn by D.J.L. Checked by SEA Date DEC 1964 Job no. 84-207-80
PROJ. NO. 472-76-245-81347-000-000-001		Sheet 3 of 9 Van Doren Hazard Stallings Architects - Engineers - Planners Topeka - Wichita - Minneapolis - Kansas City

TYPICAL 41' PAVEMENT DETAILS



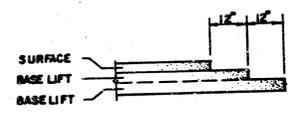
	DISTANCE FROM CENTERLINE (LT. & RT.)													
	0	2	4	6	8	10	12	14	16	18	20	20.5	20.67	21.17
A: TOP OF CURBS TO TOP OF SURFACE LIFT	-.05	.00	.07	.13	.19	.25	.32	.38	.44	.50	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	.17	.24	.30	.36	.42	.49	.55	.61	.67	—	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	.44	.50	.57	.63	.69	.75	.82	.88	.94	1.00	1.07	1.08	1.09	—
D: TOP OF CURBS TO TOP OF SUBGRADE	.77	.83	.90	.96	1.02	1.08	1.15	1.21	1.27	1.33	1.40	1.41	1.42	1.43

ELEVATION OFFSETS



GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 10" ASPHALTIC CONCRETE (8" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 4" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF ASPHALTIC CONCRETE, 8" BITUMINOUS BASE.

TRANSVERSE CONSTRUCTION JOINTS

10 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 8 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 472-76-245-81347 - 000-000-001

DATE
BY

115 STA 14+00
110
C = 1250 CY

C = 75 SF

115 STA 13+60
110
C = 1083.3 CY

C = 60 SF

115 STA 13+00
110
C = 1400 CY

C = 57 SF

115 STA 12+48
110
C = 149.1 CY

C = 83 SF

115 STA 12+00
110
C = 178.4 CY

C = 92 SF

115 STA 11+47
110
C = 154.1 CY

C = 90 SF

115 STA 11+00
110
C = 90.2 CY

C = 87 SF

↓ C = 47 SF ↑ C = 40 SF

C = 75.4 CY

115 STA 10+49.60 Lt
110
C = 150 SF

C = 45 SF

↑ C = 53 SF ↑ C = 53 SF

115 STA 10+28.84 Lt
110

YOUNG STREET
CROSS SECTIONS
PROJ. NO. 472-76-245-81347-000-000-001

115 STA 10+50.20 Rt
110

115 STA 10+35.16 Rt
110



60 50 40 30 20 10 0 10 20 30 40 50 60 70

MANHOLE FRAME AND COVER DETAIL

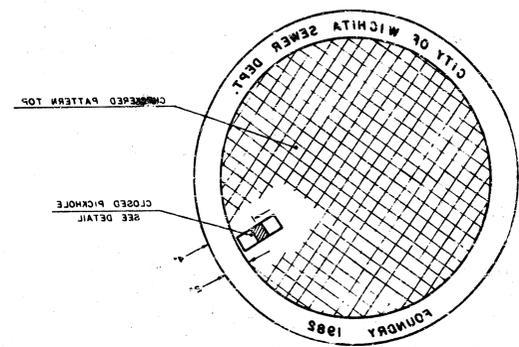
ADOPTED AS STANDARD DESIGN

BY

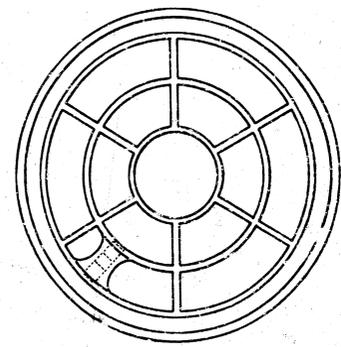
City of Wichita, Kansas

MANHOLE COVER

Weight: 180 Lbs.

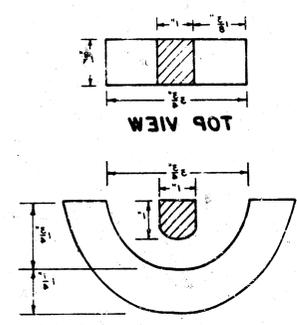


TOP VIEW



BOTTOM VIEW

PICKHOLE DETAIL

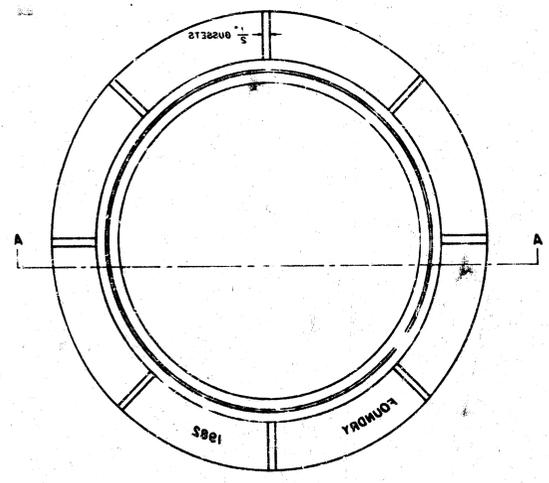


TOP VIEW

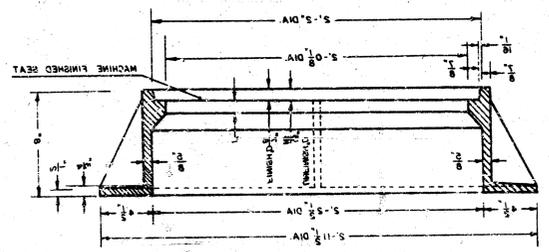
SECTION VIEW

MANHOLE FRAME

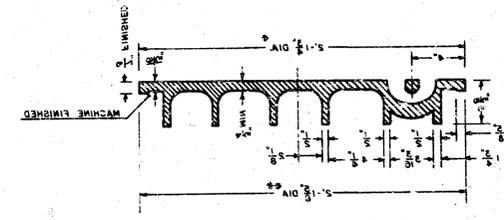
Weight: 540 Lbs.



TOP VIEW



SECTION A-A



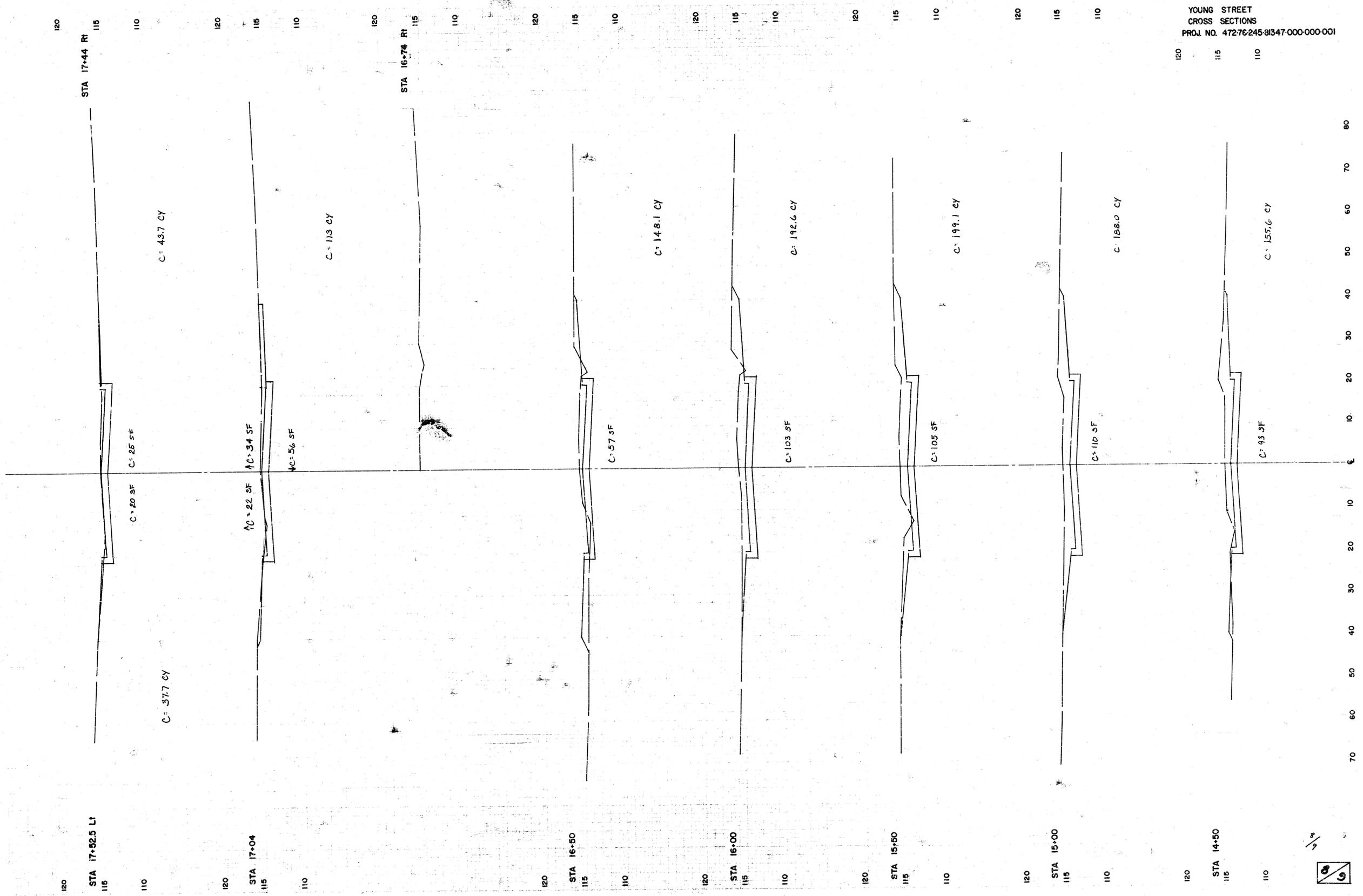
SECTION VIEW

* OUTSIDE DIA. TOP OF COVER
** OUTSIDE DIA. BOTTOM OF COVER

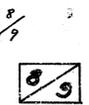
- GENERAL NOTES**
1. MANHOLE CASTINGS SHALL BE MANUFACTURED USING GOOD QUALITY GRAY IRON CONFORMING TO CLASS 30 OF A.S.T.M. SPECIFICATION A-48. DIMENSIONS AND WEIGHTS SHOWN ON THE DETAIL DRAWINGS SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS AND ALL DIMENSIONS FROM THE DIMENSIONS SHOWN MUST BE SPECIFICALLY APPROVED. THE FINISHED CASTINGS SHALL BE OF UNIFORM QUALITY, FREE FROM BLOWHOLES, POROSITY, HARD SPOTS, SHRINKAGE DISTORTIONS OR OTHER DEFECTS.
 2. MANHOLE CASTINGS SHALL BE COATED WITH AN ASPHALT PAINT RESULTING IN A SMOOTH, TIGHT AND TENACIOUS COATING WHICH IS NOT BRITTLE OR FLAKY.
 3. MANHOLE CASTINGS SHALL BE MANUFACTURED SUCH THAT A COVER MANUFACTURED BY ANY ONE FOUNDRY WILL FIT INTERCHANGEABLY INTO A FRAME MANUFACTURED BY ANOTHER FOUNDRY AND MANUFACTURING OF THE MATCHING PARTS OF THE COVER AND THE FRAME TO CLOSE TOLERANCES STILL PERMISSIBLE WITHINABLE CLEARANCES AND NO-ROCKING REQUIREMENTS. THIS WILL REQUIRE THE OUTSIDE CIRCUMFERENCE OF THE COVER AND THE INSIDE CIRCUMFERENCE OF THE FRAME TO BE MANUFACTURED TO TOLERANCES SUCH THAT THE CLEARANCE BETWEEN THE COVER AND FRAME WILL NOT EXCEED 1/8" AT ANY POINT AROUND THE CIRCUMFERENCE OF THE SETTING SURFACE BETWEEN THE COVER AND FRAME SHALL BE MACHINED SUCH THAT THESE SURFACES SHALL MAKE FULL CONTACT FOR THEIR FULL CIRCUMFERENCE TO PRECLUDE THE COVER FROM ROCKING IN THE FRAME.
 4. THE MANHOLE FRAME AND COVER SHALL BE MARKED WITH LETTERING INDICATING THE NAME OF THE MANUFACTURER AND THE YEAR WHEN THE COVER OR FRAME WAS CAST. THE COVER SHALL BE FURTHER IDENTIFIED WITH REGARD TO OWNER'S USING LETTERS AT LEAST 1/2" IN HEIGHT. THIS IDENTIFICATION SHALL BE "CITY OF WICHITA SEWER DEPARTMENT", THE WORD "MANUFACTURED" MAY BE ABBREVIATED. THE TEXTURE OF THE TOP SURFACE OF THE COVER SHALL BE MANUFACTURED IN A CHECKERED PATTERN DESIGN AS INDICATED ON THE DRAWINGS. SMOOTH BLOCKOUTS SHALL BE UTILIZED TO HIGHLIGHT THE LETTERING ON THE COVER SURFACE. THE TOTAL AREA OF SMOOTH SURFACE BLOCKOUT SHALL NOT EXCEED THE AREA AS INDICATED ON THE DRAWING. POSITIONING OF SMOOTH BLOCKOUTS AND LETTERING MAY VARY FROM THAT SHOWN ON THE DETAIL DRAWING.

D.L.
D.L.
D.L.

1/24
11-24



YOUNG STREET
CROSS SECTIONS
PROJ. NO. 472-76-245-81347-000-000-001



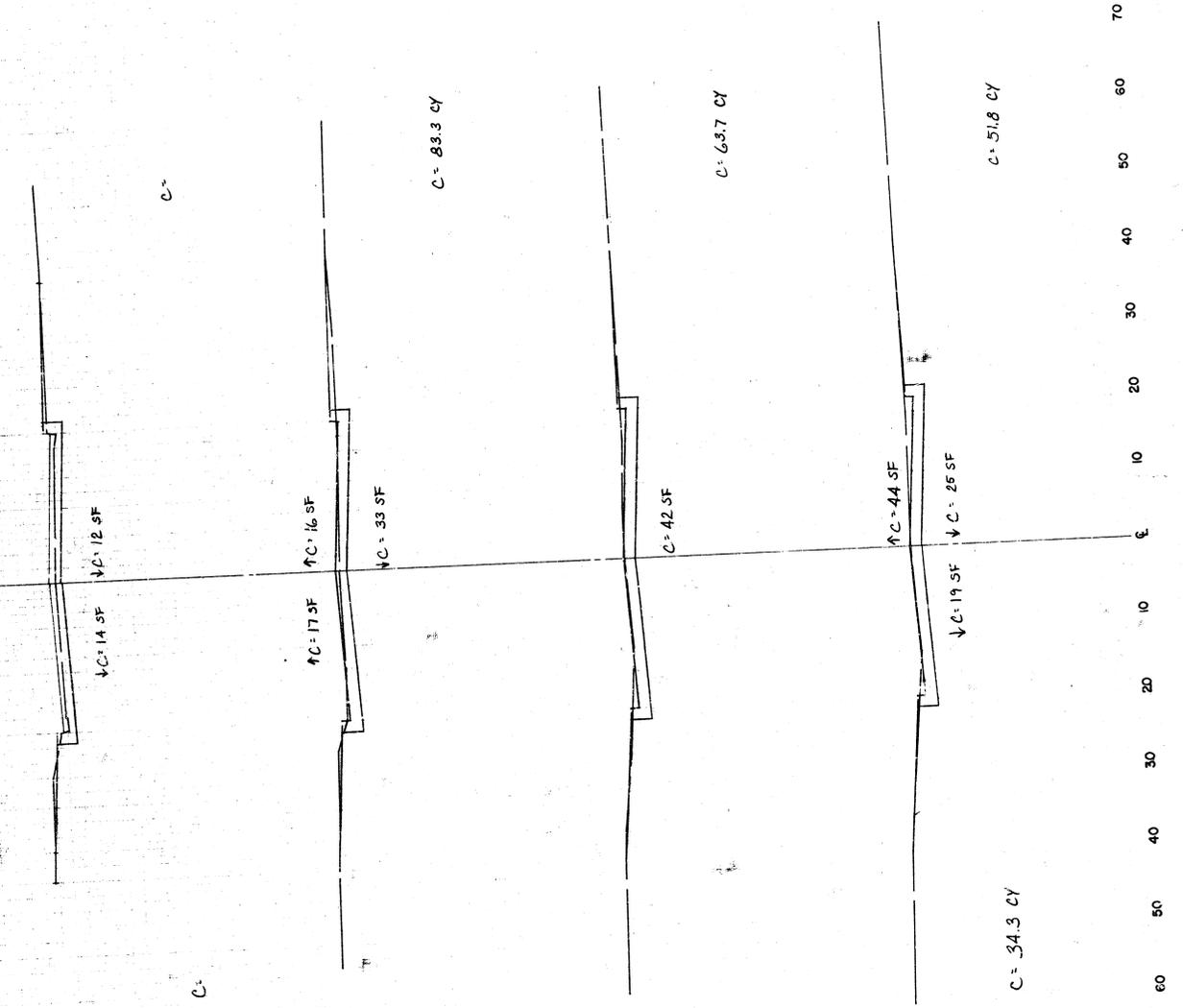
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D-1 1/84

YOUNG STREET
CROSS SECTIONS
PROJ. NO. 472-76-245-81347-000-000-001

120
115

120
115
STA 15+15.13 RI



120
115
STA 19+00.01 LI

120
115
STA 19+00

120
115
STA 18+40

120
115
STA 18+00

1/9

FILED FROM THE BEST