

# LONGFORD

S. E. LINE BLUESTEM TO E. COR. LT. 4, BLK. 6  
& S. COR. LT. 24, BLK. I, CEDAR RIDGE 2ND

# LONGFORD COURT

N. E. LINE LONGFORD TO & INCL. CUL-DE-SAC

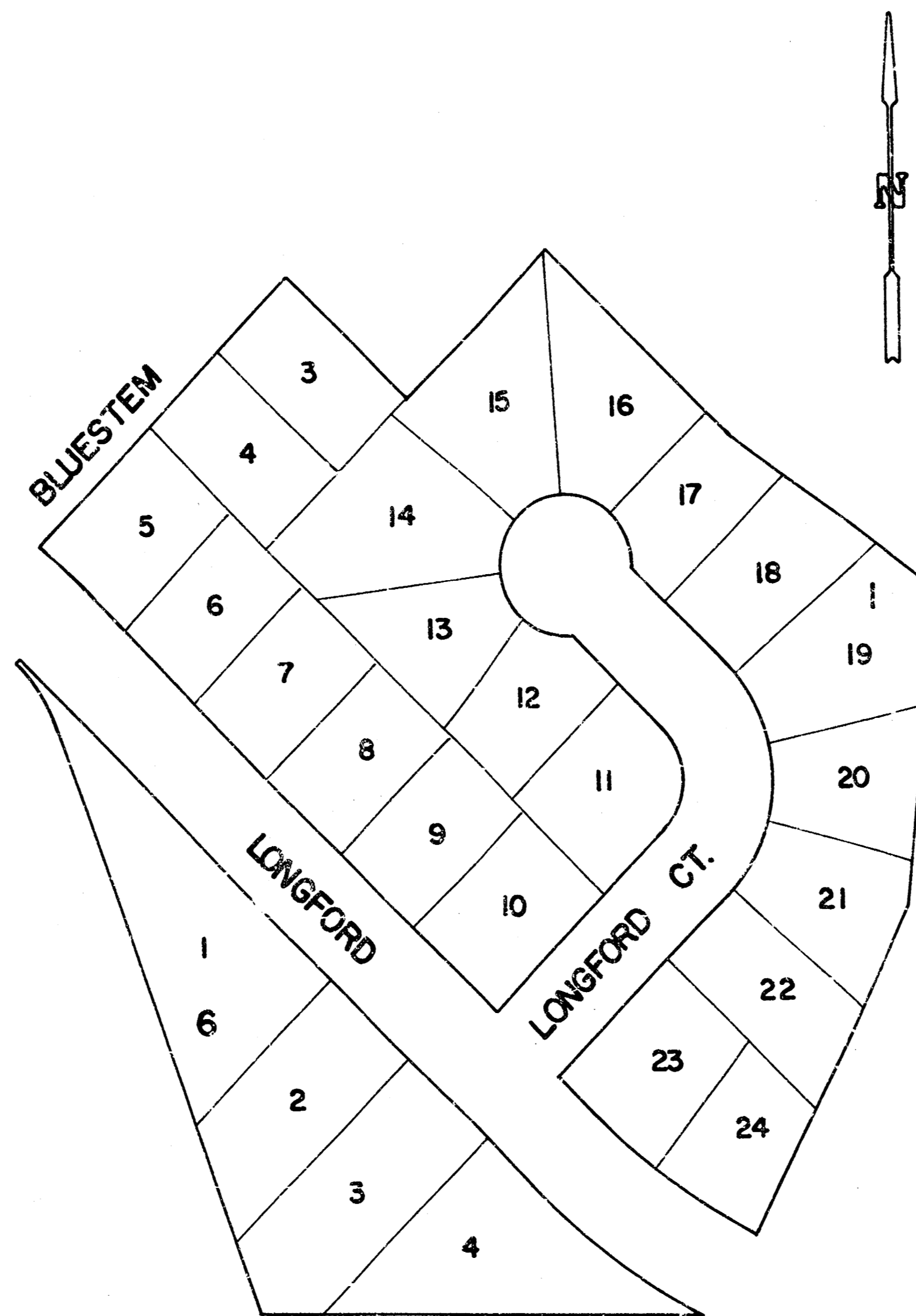
CITY OF WICHITA, KANSAS

DEPARTMENT OF ENGINEERING

R. W. BRUGGEMAN

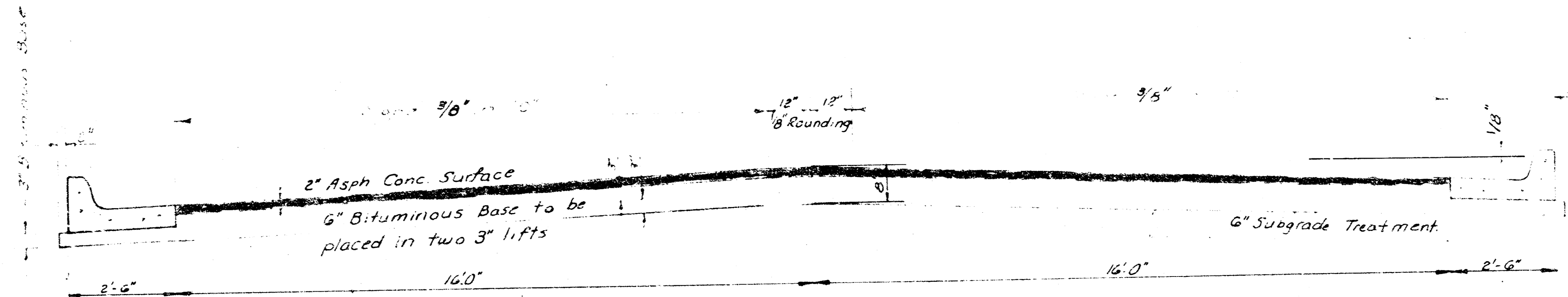
DIRECTOR OF ENGINEERING/CITY ENGINEER

DATE: PROJ. NO. 472 76 245 81085 000 000 001



Manipulation = 5,305 S.Y.

LONGFORD  
 S. E. LINE BLUESTEM TO E. COR. LT. 4, BLK. 6  
 & S. COR. LT. 24, BLK. 1, CEDAR RIDGE 2 ND.



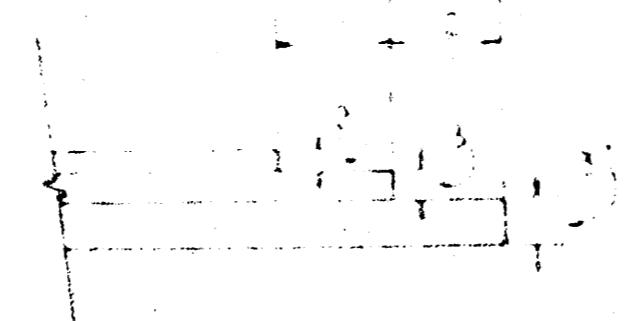
TYPICAL SECTION

37' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

A TACK COAT OF EMULSIFIED ASPHALT (SS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE 0.05 GALLONS PER SQ YD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.

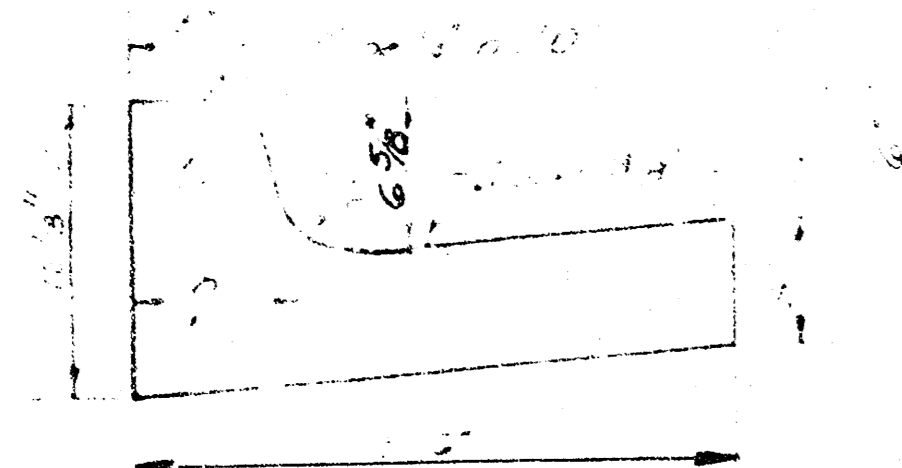
BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND PLACED SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE IN THE TOP LIFT.

DETAIL OF TRANSVERSE CONSTRUCTION JOINTS

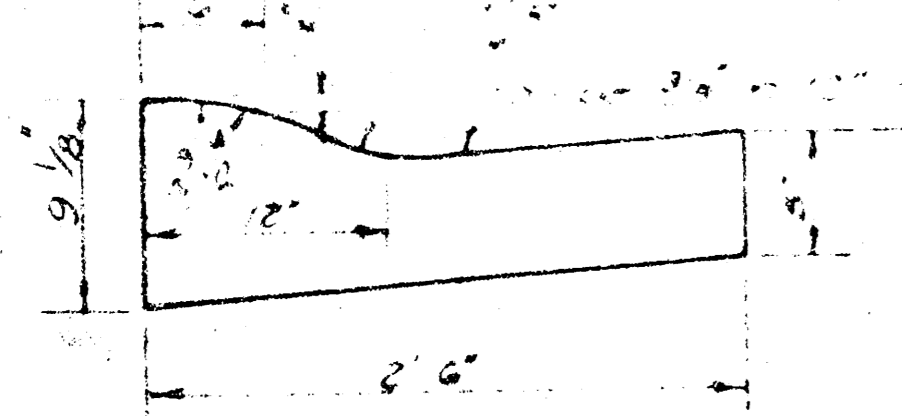


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF ASPHALTIC CONCRETE PAVEMENT.

CONTROLLED CURB & GUTTER



ROLL TYPE CURB & GUTTER



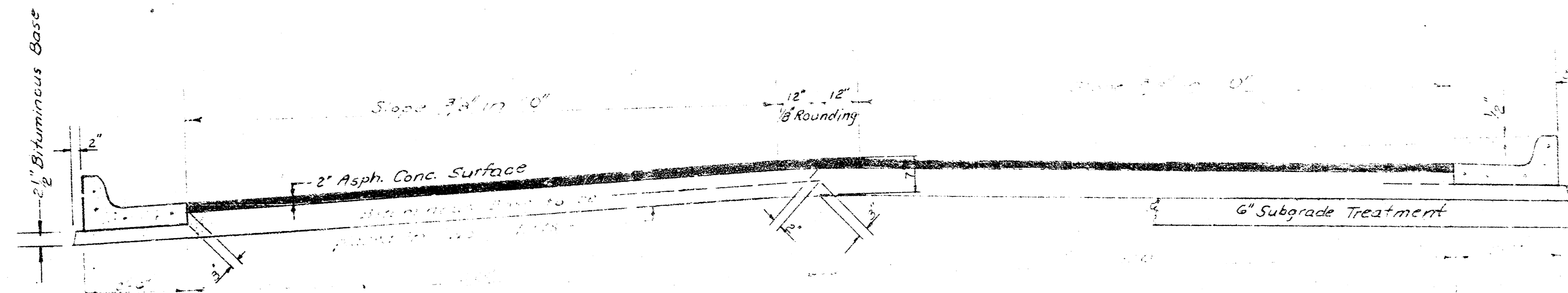
CITY OF WICHITA KANSAS  
 DEPARTMENT OF ENGINEERING

R. W. BRUGGEMAN  
 DIRECTOR OF ENGINEERING/CITY ENGINEER

DATE \_\_\_\_\_ Proj. No. 472 76 245 81085 000 000 001

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LONGFORD CT.  
N.E. LINE LONGFORD TO & INCL. CUL-DE-SAC



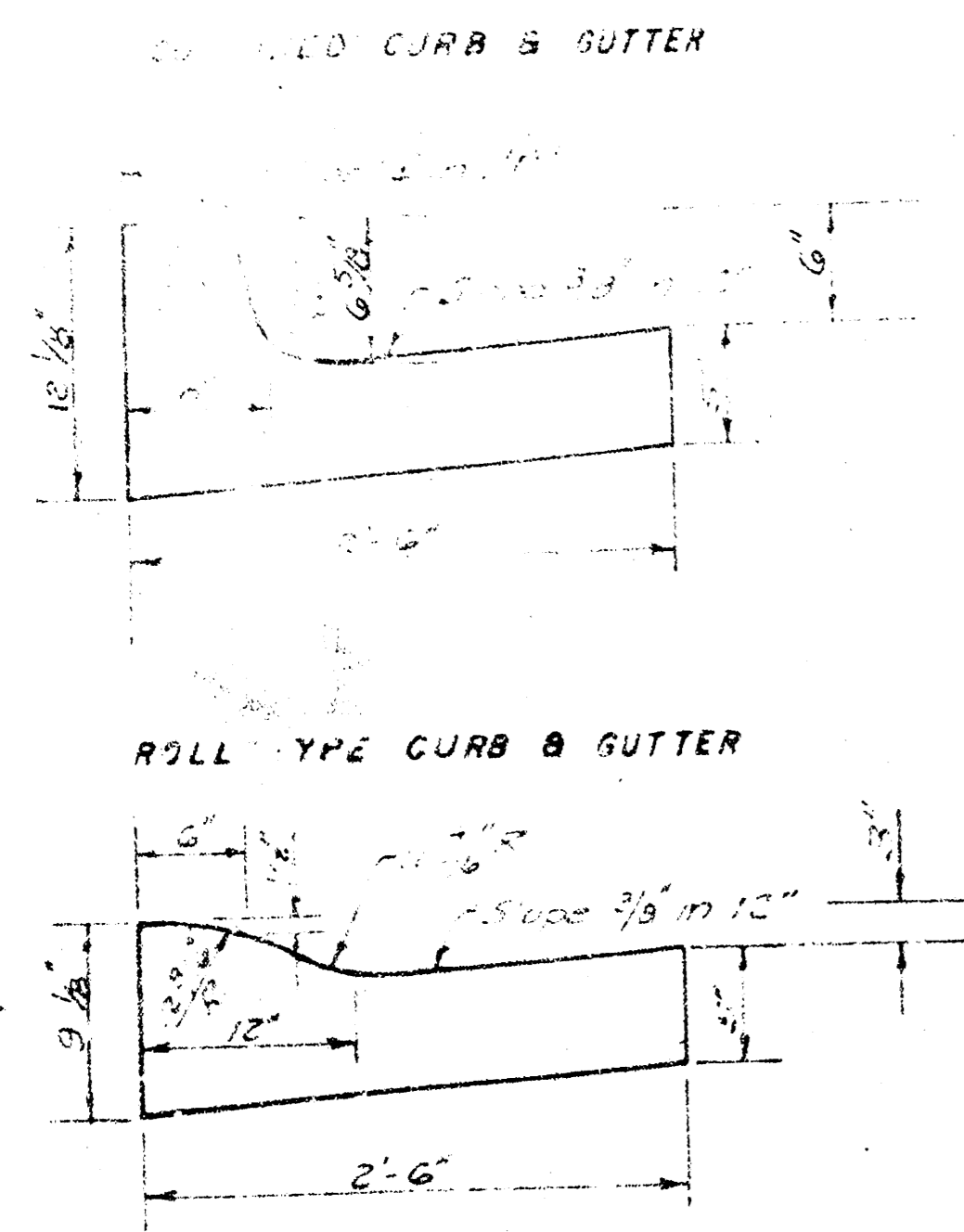
TYPICAL SECTION

35' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

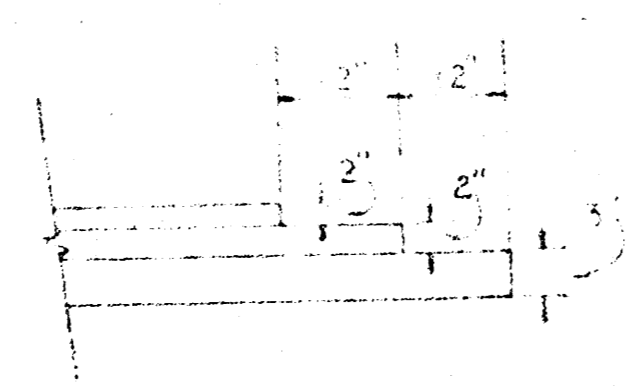
A TACK COAT OF EMULSIFIED ASPHALT (SS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE 0.05 GALLONS PER SQ YD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND PLACED SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE IN THE TOP LIFT.

*The 35' width shown in this section shall be used for all 35' wide sections. The 35' width shown in this section shall be used for all 35' wide sections.*



DETAIL OF TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF ASPHALTIC CONCRETE PAVEMENT.

CITY OF WICHITA KANSAS  
DEPARTMENT OF ENGINEERING  
R. W. BRUGGEMAN  
DIRECTOR OF ENGINEERING/CITY ENGINEER  
DATE: PROJ. No. 472 76 245 81085 000 000 001

3/10

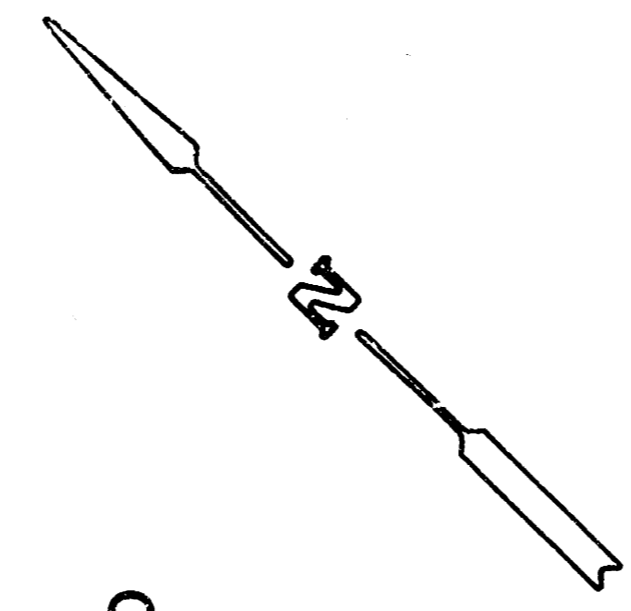
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BM = 160.76 ~ 10" cut top S. Cb. Longford, 100' E. W. of W. L. Bluestem.  
 BM = 169.52 ~ R.R. Spk. S. W. P. HLP N. E. Cor. 1st. Vernon & Cypress.

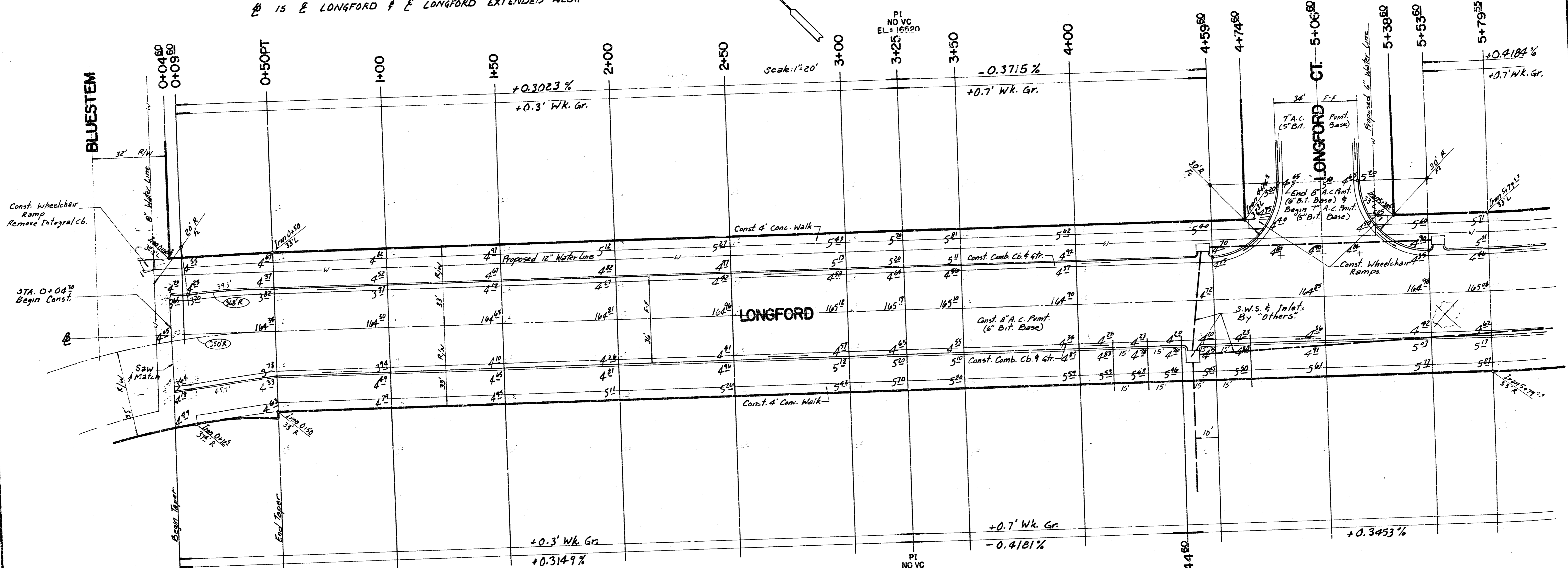
# 15 E LONGFORD & E LONGFORD EXTENDED WEST.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR SHALL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

SUB-GRADE  
 TYPE OF SUB-GRADE TREATMENT SHALL BE DETERMINED BY THE FIELD ENGINEER. SUB-GRADE TREATMENT MAY CONSIST OF LIME TREATMENT, SUB-GRADE MODIFICATION, OR ANY COMBINATION OF THESE.



Scale: 1" = 20'



**EARTHWORK**

	EXCAVATION	COMP. FILL 30%
X-SECTION	5,669.1 C.Y.	58.9 C.Y.
10%	567.0 C.Y.	5.9 C.Y.
TOTAL	6,236.1 C.Y.	64.8 C.Y.

Note: Contractor to coordinate work with sewer contractors.

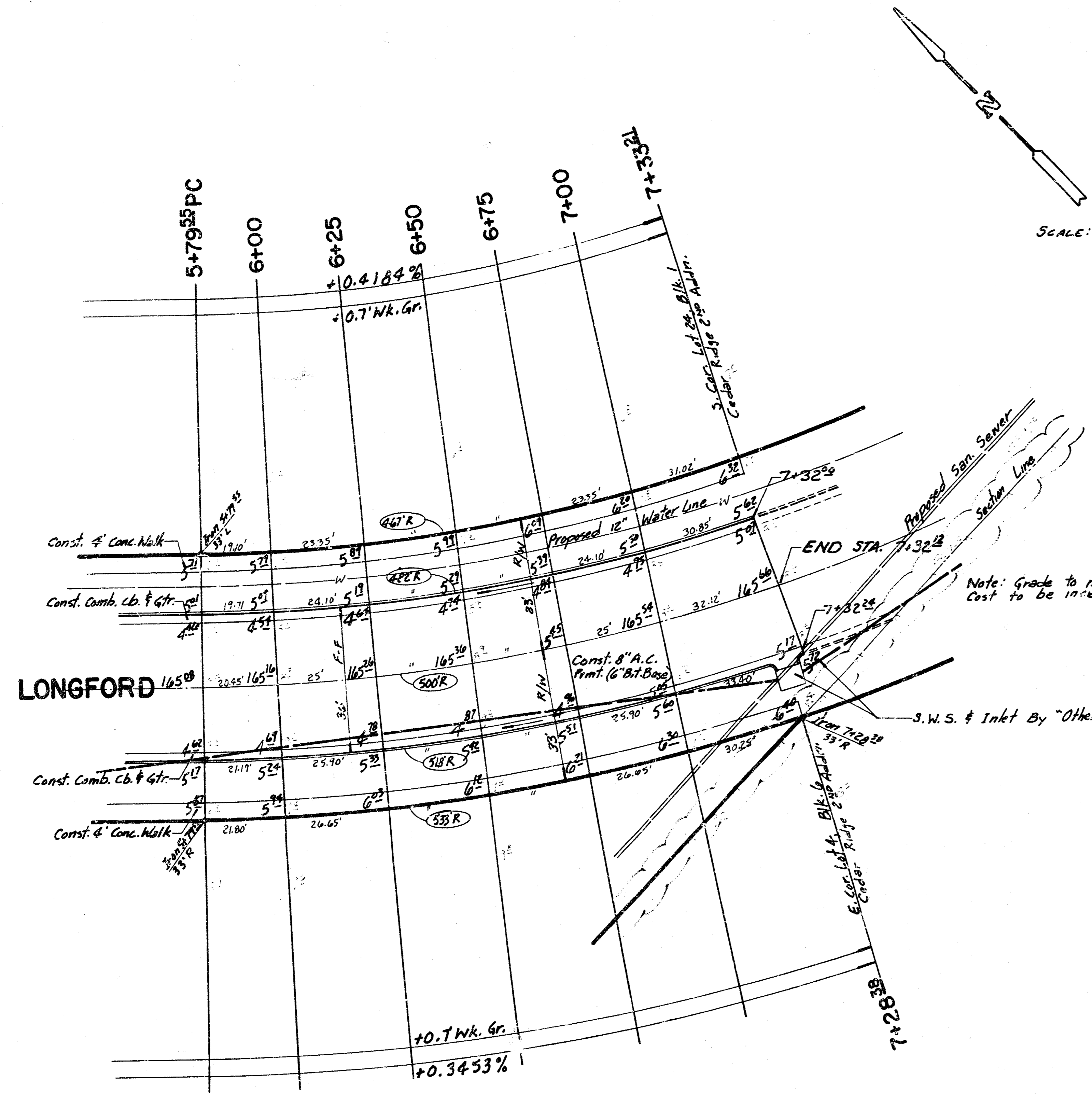
Note: Trees to be removed are marked thus except that any tree marked for removal which in the opinion of the Engineer can be saved shall be spared.

Note: Const. not more than 23-20' Drives.

Note: Lime application rate = 25#/s.y.  
 Preheater Fines application rate = 35#/s.y.

LONGFORD  
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SCALE: 1"=20'

$\Delta = 17^{\circ}28'10''$  R. = 500.00 T. = 76.82 L. = 152.45' I.C. = 151.86'

CURVE DATA BASED ON  $E$  RAD  $\Delta = 8^{\circ}46'05''$

STA.	ARC.	CHORD LENGTH		DEFLECTION	
		OFF_CUR	OFF_CB	DEFLECTION	TOTAL DEFLECTION
5+79.55 PC				0°00'00"	0°00'00"
6+00	20.45'	19.39'		1°10'18"	1°10'18"
6+25	25'	23.70'		1°25'57"	2°36'15"
6+50	"	"		1°25'56"	4°02'11"
6+75	"	"		1°25'57"	5°28'08"
7+00	25'	23.70'		1°25'57"	6°54'05"
7+32.35	32'	30.33'		1°49'40"	8°44'05"

Defl./Ft. = 3.43774 Min.

INTERSECTION QUANTITIES

S.Y.	Concrete Pavement
155	S.Y. Asphaltic Conc. Pavement (6" Bituminous Base)
20	S.Y. Bituminous Base
96	L.F. Combined Curb & Gutter
	L.F. Integral Curb
	S.F. 4" Rectangular Ramp
	S.F. 4" Walk
	C.Y. Fill
	C.Y. Gravel Fill
	Lbs. Reinforcing Steel
170	S.Y. Manipulation
3	Tons Lime or Cement
S.Y. V.G.	Concrete & Asphaltic Concrete Base

$\Delta = 17^{\circ}29'47''$  R. = 500.00 T. = 76.94' L. = 152.49' I.C. = 152.10'

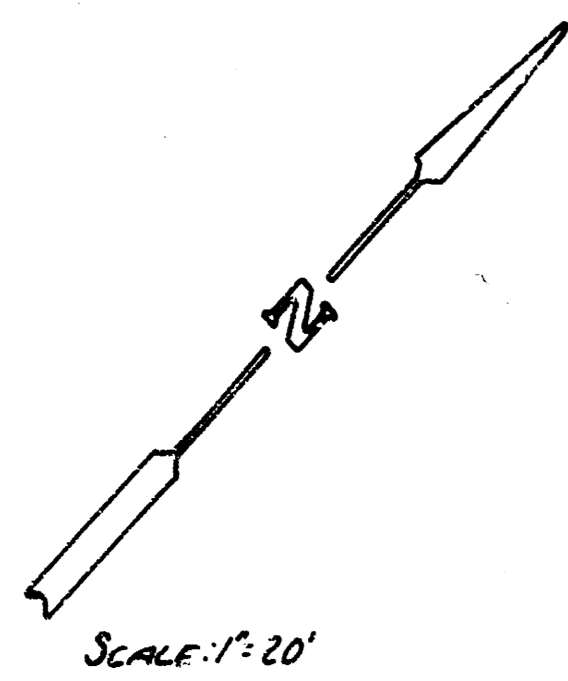
CURVE DATA BASED ON  $E$  RAD  $\Delta = 8^{\circ}46'05''$

STA.	ARC.	CHORD LENGTH		DEFLECTION	
		OFF_CUR	OFF_CB	DEFLECTION	TOTAL DEFLECTION
5+79.55 PC				0°00'00"	0°00'00"
6+00	20.45'	21.51'		1°10'18"	1°10'18"
6+25	25'	26.90'		1°25'57"	2°36'15"
6+50	"	"		1°25'56"	4°02'11"
6+75	"	"		1°25'57"	5°28'08"
7+00	25'	26.90'		1°25'57"	6°54'05"
7+32.35	32.24'	33.91'		1°49'40"	8°44'05"

Defl./Ft. = 3.43774 Min.

LONGFORD  
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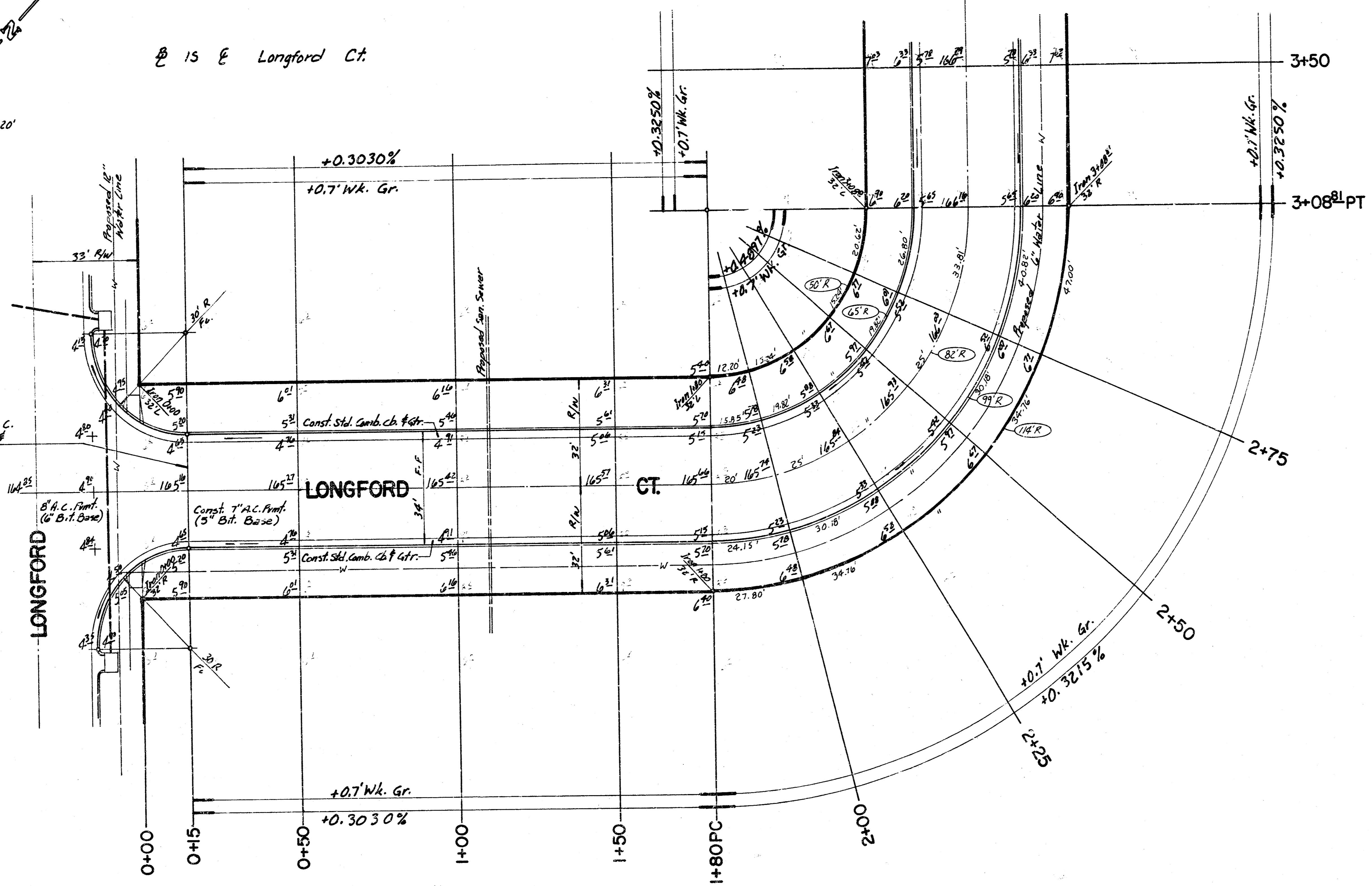
15 E Longford Ct.

Sta. 0+15 End B.A.C. Pmnt. (6" Bit. Base) & Begin 7" A.C. Pmnt. (5" Bit. Base)

LONGFORD

LONGFORD

CT.



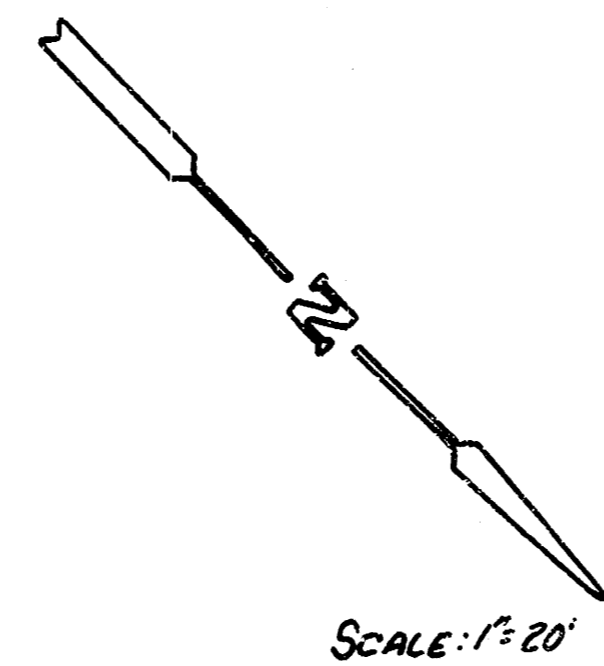
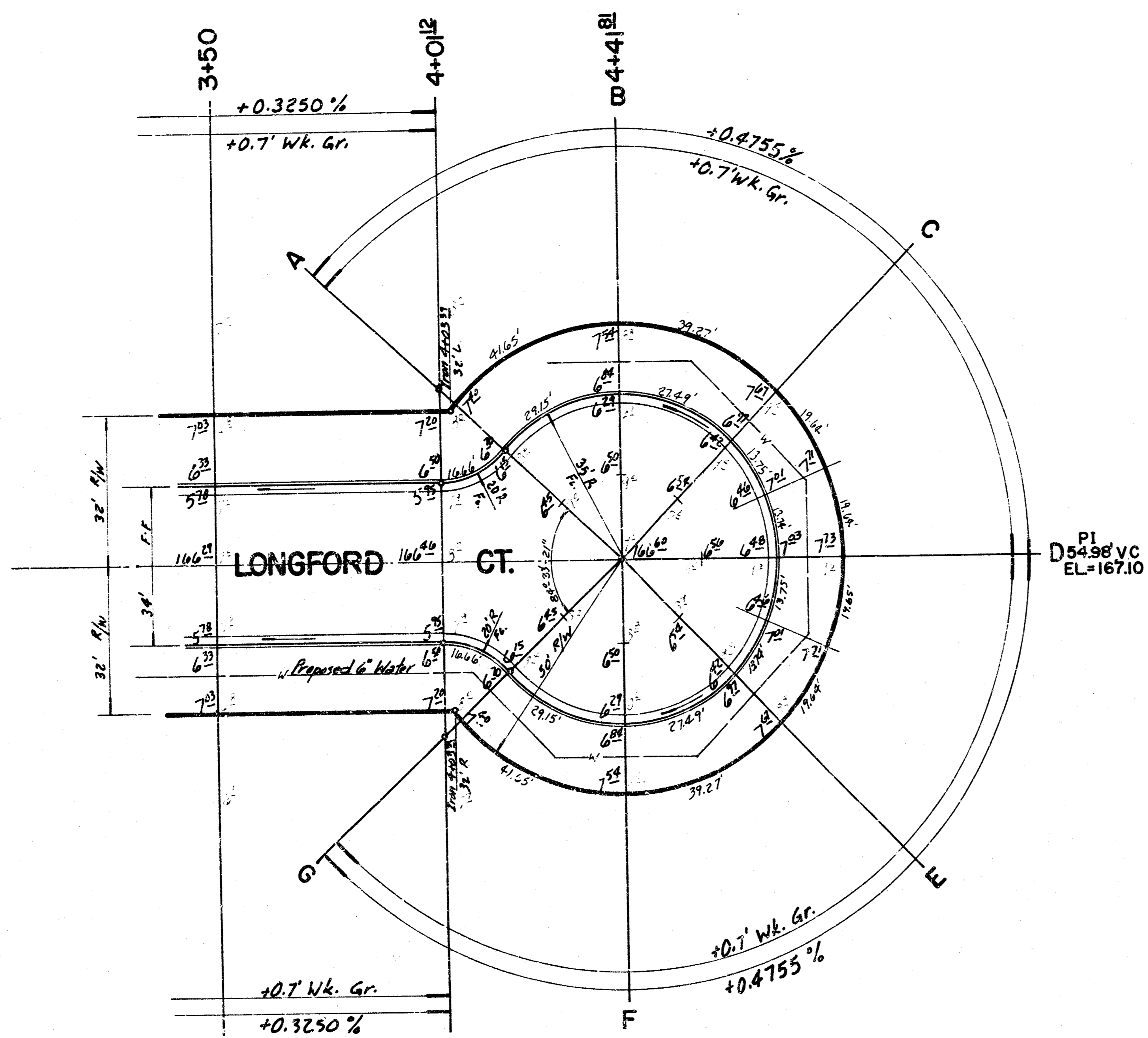
$\Delta = 10^{\circ}00'00''$   $R = 82.00'$   $T = 82.00'$   $L = 128.81'$   $LC = 115.97'$

CURVE DATA GAS				
STA	CH	Δ	N	S
1+80 PC	—	—	—	0°00'00"
2+00	20'	15.87'	26.03'	65°59'16"
2+25	25'	17.51'	32.50'	8°44'05"
2+50	"	"	"	8°44'05"
2+75	25'	17.51'	32.50'	8°44'05"
3+08 <sup>BL</sup> PT	33.81'	25.36'	43.81'	11°48'37"

Defl./ft. = 20.96111 Min.

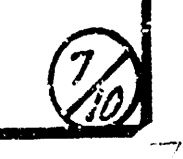
LONGFORD CT.  
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SCALE: 1"=20'

LONGFORD CT.  
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 21000

DATE		STATEMENT OF COST				PROJ. NO. 41276 245 81065 000 000 001		P.E.
DEC 82		FOR CONSTRUCTION OF: LONGFORD LANE & LONGFORD CT. IN CEDAR RIDGE 2ND						171,692.73
WORKING DAYS		QUANTITIES		UNIT	CONSTRUCTION ITEM	COSTS		
PROG.	CITY	TOTAL	PRICE			PROG.	CITY	TOTAL
		2,649'	13.34'	SQ. YDS. 8" RC. PVMT. (6" BIT)				35,337.66
		1,755'	12.71'	SQ. YDS. 7" RC. PVMT. (5" BIT)				22,306.05
		438'	4.75'	SQ. YDS. 3" BIT. BASE				2,080.50
		298'	4.65'	SQ. YDS. 2 1/2" BIT. BASE				1,385.70
		2,481'	6.19'	LIN. FT. COMB. CURB & GUTTER				15,357.39
		5,669'	1.25'	CU. YDS. EXCAVATION				7,086.25
5.271*	220*	5,521'	1.75'	SQ. FT. 4" WALK	9399.25	385.00		9,784.25
	157*	157'	3.00'	SQ. FT. W.C.R.		471.00		471.00
		59'	0.90'	CU. YDS. COMB. FILL (90%)				53.10
		5,280'	1.35'	SQ. YDS. MANIPULATION				7,128.00
		4'	50.00'	INLET HOOK-UPS				200.00
		0'	60.00'	SMALL TREE REMOVED				0.00
		37'	1.00'	SAWED JOINT (LIN. FT.)				37.00
		23'	3.50'	SQ. YDS. PAVMT. REMOVED				80.50
		20'	3.00'	LIN. FT. COMB. C&G. REMOVED				60.00
		0'	2.50'	LIN. FT. INTEGRAL CURB REM.				0.00
		74'	42.00'	TONS. PRE-HEATER FINES				3,108.00
		1'	L.S.	CHANGE ORDER #1				2,200.00
		1'	L.S.	CHANGE ORDER #2				100.00
				SUBTOTALS	98,294.31	8481.09		106,775.40
				ENGINEERING + INSP.	8,842.20	758.45		9,600.65
				ADMINISTRATION	1,443.90	123.85		1,567.75
				PUBLICATION	125.76	-		125.76
				ABSTRACTING	6.50	-		6.50
				CONSTRUCTION COST	108,712.67	9363.39		118,076.06
		6/83		TEMP. NOTE JNT.	6,535.21	560.57		7,095.78
				TOTAL COST	#115,247.88	#9923.96		#125,171.84

FORM 152-202 MARCH 81

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