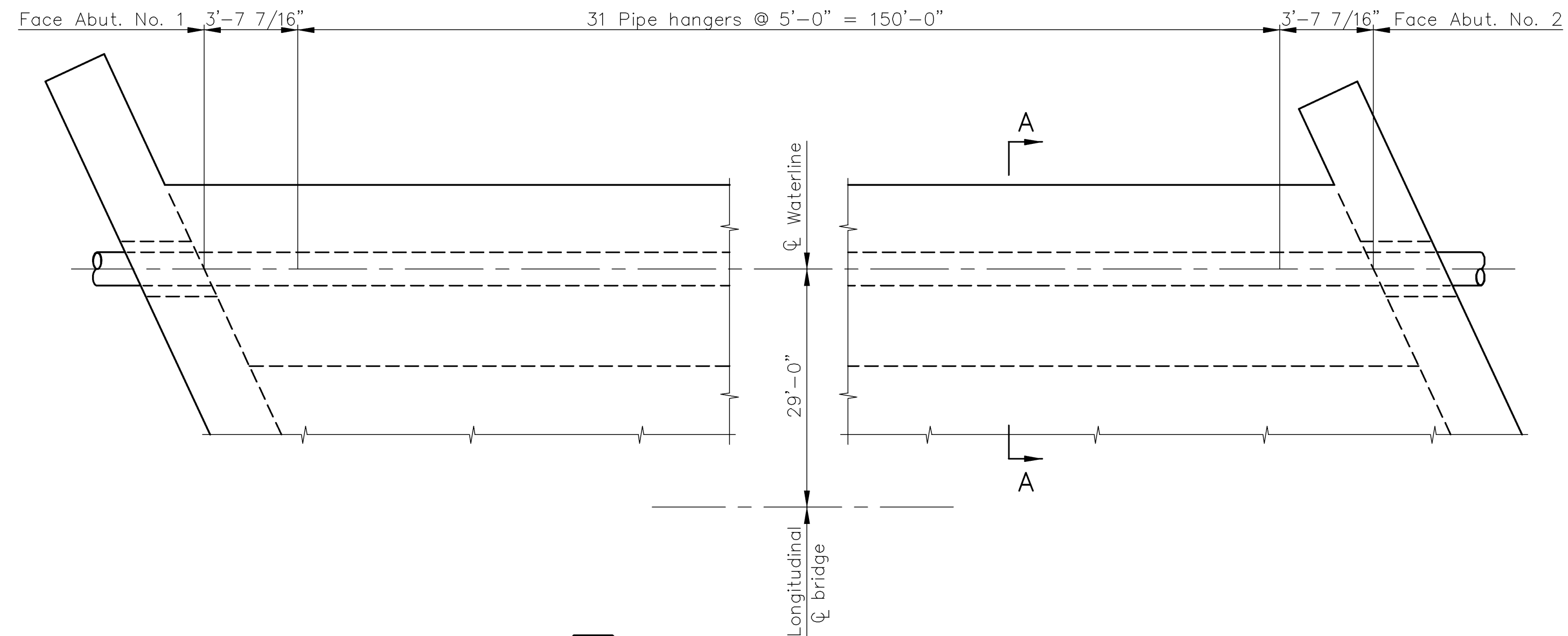
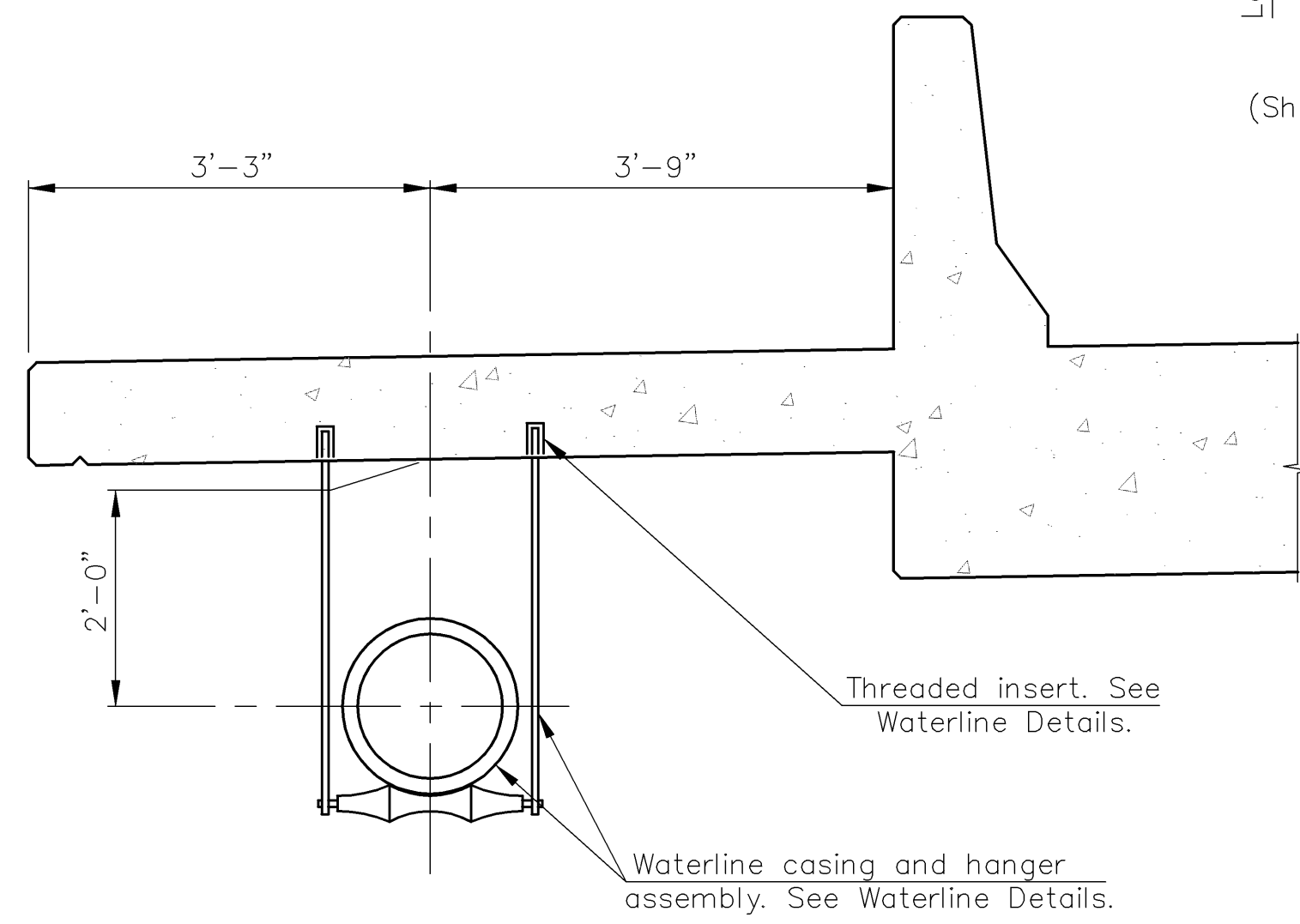


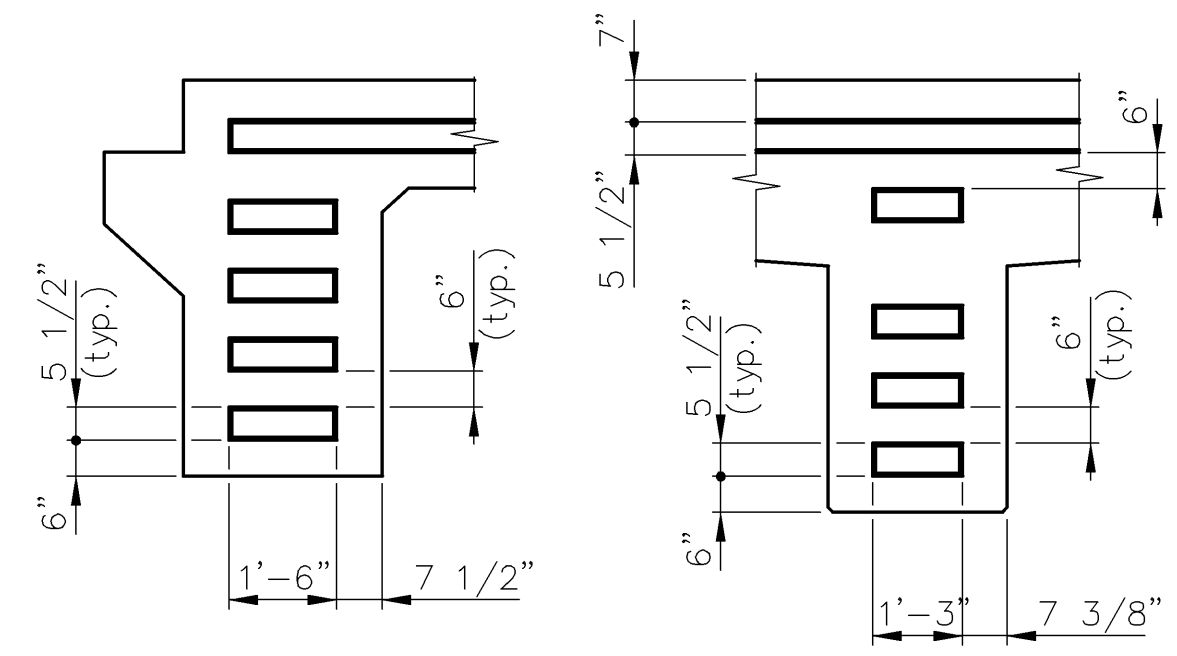
STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	87 N-0359-01	2006	24	61



PLAN
(Showing waterline left)

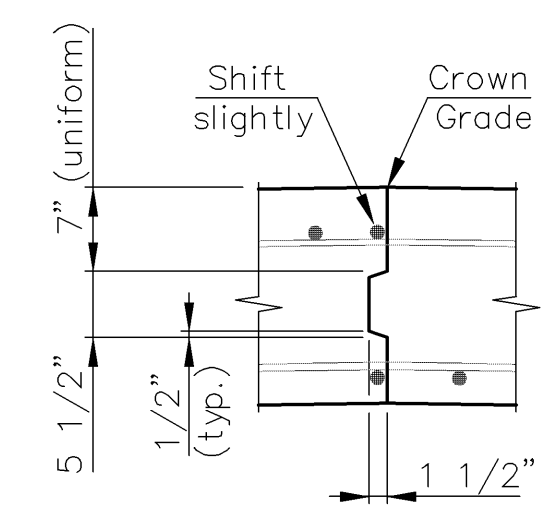


SECTION A-A



AT ABUTMENT

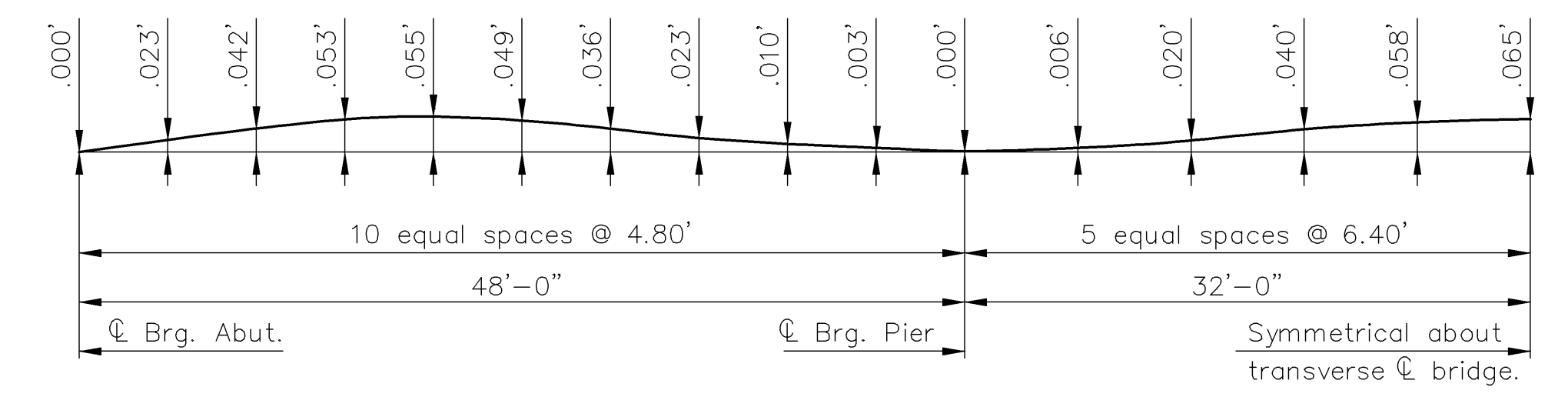
AT PIER



SECTION THRU SLAB

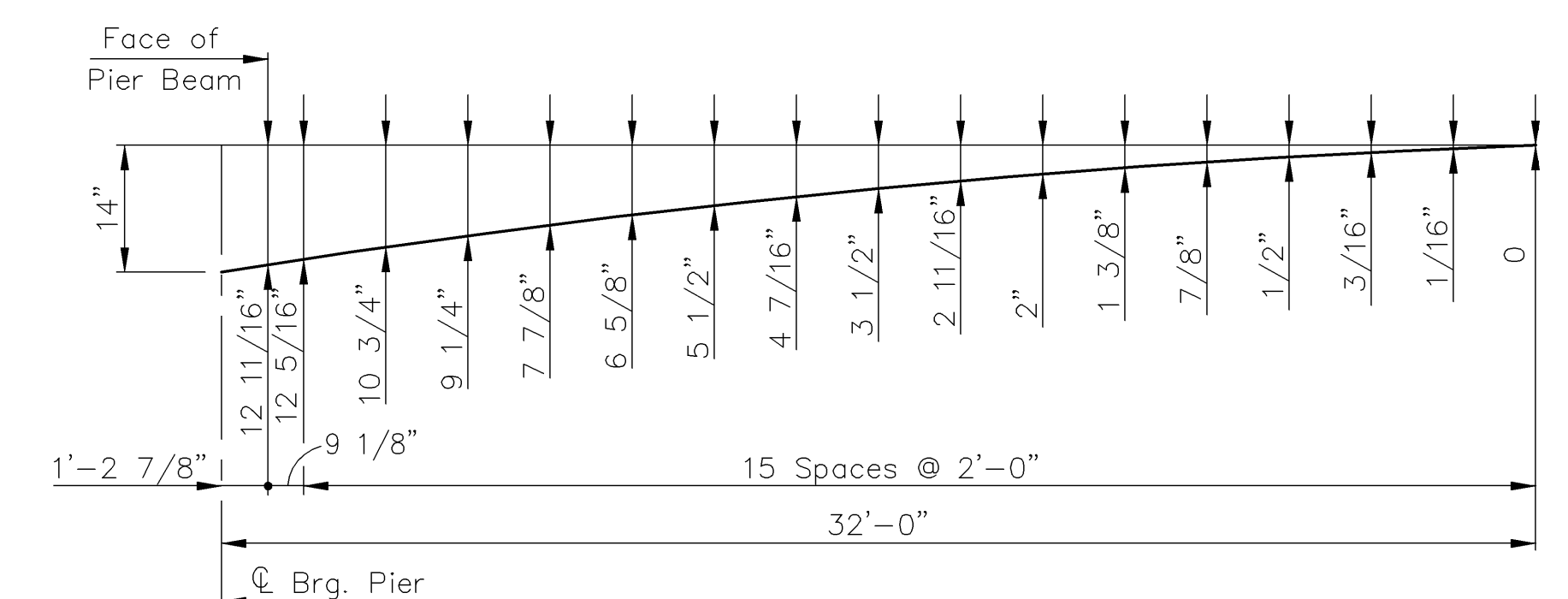
LONGITUDINAL CONSTRUCTION JOINT

Note: With the approval of the Engineer, alternate keys may be used.

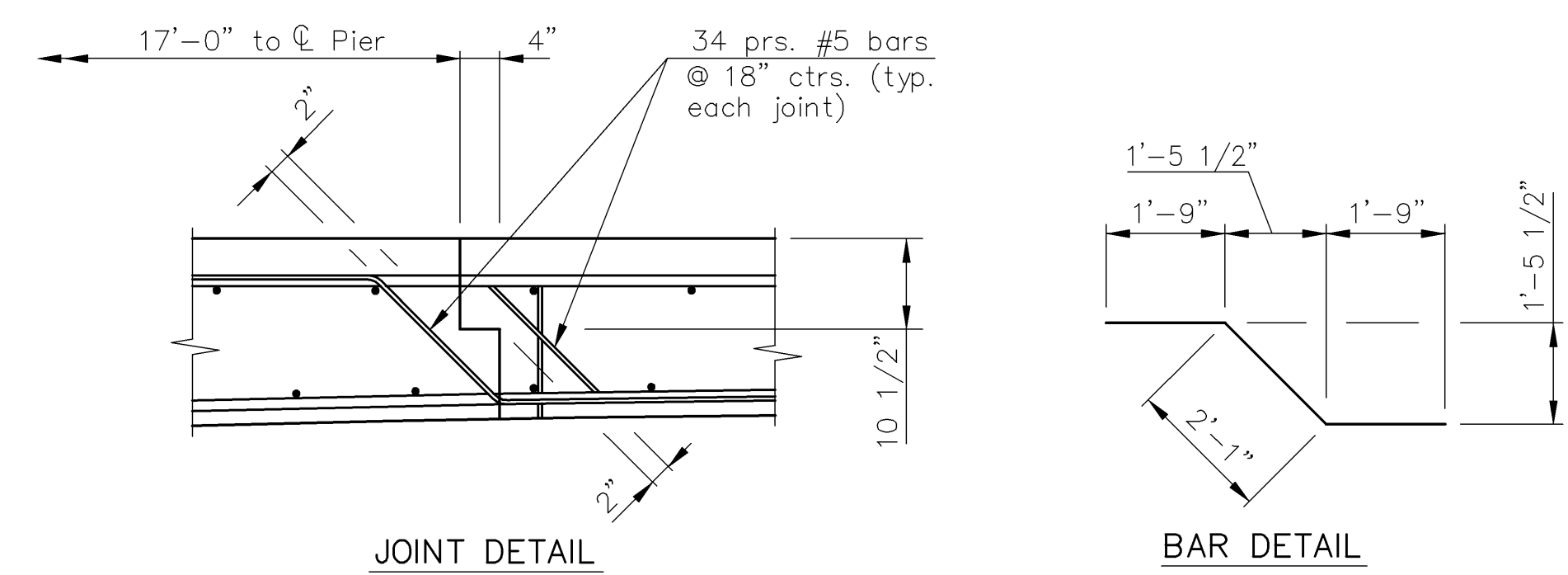


DEAD LOAD CAMBER DIAGRAM

NOTE: Long Term Deflection = Initial Deflection X 4.0
Initial Deflection based on $E_c = 3.644 \times 10^6$ psi.



PARABOLIC HAUNCH ORDINATES

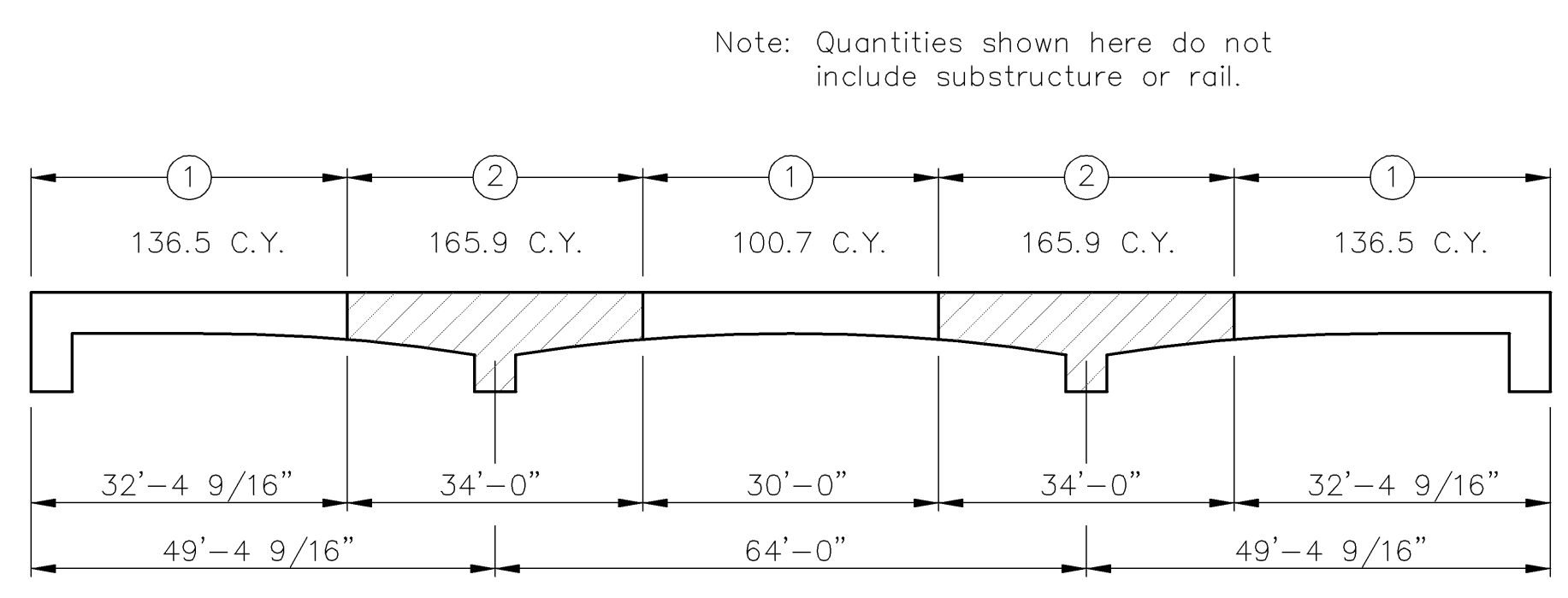


JOINT DETAIL

BAR DETAIL

OPTIONAL TRANSVERSE CONSTRUCTION JOINT

Note: If transverse construction joints are to be used they shall be constructed as shown. The reinforcing steel shown shall be provided by the Contractor at his own expense and is not included in the bill of reinforcing.



CONCRETE PLACING SEQUENCE DIAGRAM

When long span steel beams having a concrete dead load deflection greater than $1/4$ " are used or when timber falsework with greater than 12'-0" clear span is used, the placing sequence shown shall be followed. Segmental, combined or continuous pours are allowed, but any discontinuous pour must stop short of a construction joint short of an interior bent.

When timber falsework with 12'-0" or less clear span is used, the Contractor, subject to the approval of the Engineer, may use a continuous pour or may discontinue the pour at any construction joint shown.

The safety barrier rail may be placed continuously from one end of the bridge to the other.

PROJECT NO. 87 N-0359-01		CFS Cook, Flatt & Strobel ENGINEERS, P.A.
MISCELLANEOUS SUPERSTRUCTURE DETAILS		
HILLSIDE ST. BRIDGE OVER GYPSUM CREEK		DESIGNED RSC SCALE
STA. 35+05.75 CITY OF WICHITA		DETAILED DEG DATE
		QUANTITIES SHEET OF

J:\2004PROJ\04559\DWGS\SLAB 1"=4'