

CITY OF WICHITA, KANSAS  
 MICHAEL E. LINDEBAK, P.E., CITY ENGINEER  
**STREET IMPROVEMENTS**

CLUBHOUSE DRIVE - E.L. WEBB ROAD TO 424.8' S.E. OF E.L. STONEYBROOK

IN  
**TALLGRASS EAST ADDITION**

CITY OF WICHITA PROJECT NO. 472-76-245-81626-000-000-001

INDEX OF SHEETS

1. TITLE SHEET
- 2.-3. PLAT
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5. PAVING DETAILS
- 6.-8. PAVING PLANS
9. VALLEY CUTTER AND WHEELCHAIR RAMP DETAILS
10. STANDARD DRIVE ENTRANCES
11. SIDEWALK PLAN
- 12.-14. CROSS SECTIONS

PROJECT SURVEY CONTROL

- VERTICAL DATUM: CITY OF WICHITA DATUM
- DATUM BENCH MARK: C.O.W. DISC 44" SOUTH AND 48" EAST OF  
 E WEBB RD. AND 21ST STREET NORTH. ELEV. +205.24
- BENCH MARK NO. 1: CHISELED "0" N.E. CORNER HEADWALL R.C.B. APPROX.  
 1050' EAST OF WEBB ROAD ON 21ST STREET NORTH. ELEV. +202.55
- BENCH MARK NO. 2: CHISELED "0" TOP OF CURB AT EAST END OF 6 MEDIUM  
 ISLAND AT PLUMTHICKET CIRCLE AND WEBB ROAD. ELEV. +228.75
- BENCH MARK NO. 3: C.O.W. DISC 32" NORTH AND 28" EAST OF W. 1/4 COR.  
 SEC. 4, T27S, R2E AT WEBB ROAD AND 25TH STREET  
 NORTH. ELEV. +225.05
- BENCH MARK NO. 4: R.R. SPIKE IN N.E. FACE 12" OSAGE ORANGE APPROX.  
 150' LT. CLUBHOUSE DRIVE STA. 4+00. ELEV. +229.82
- BENCH MARK NO. 5: R.R. SPIKE IN EAST FACE 10" ASH APPROX. 90' RT.  
 CLUBHOUSE DRIVE STA. 13+00. ELEV. +214.54
- BENCH MARK NO. 6: STEP-NAIL IN SOUTH FACE LAMINATED H.L.P. APPROX.  
 275' EAST AND 30' NORTH OF N.E. CORNER OF  
 LOT 36, BLOCK 1. ELEV. +207.69

EARTHWORK

EXCAVATION	
X-SECTIONS	2907 CU. YDS.
TOTAL	291 CU. YDS.
TOTAL	3198 CU. YDS.

SUBGRADE MANIPULATION - 62297 S. Y.

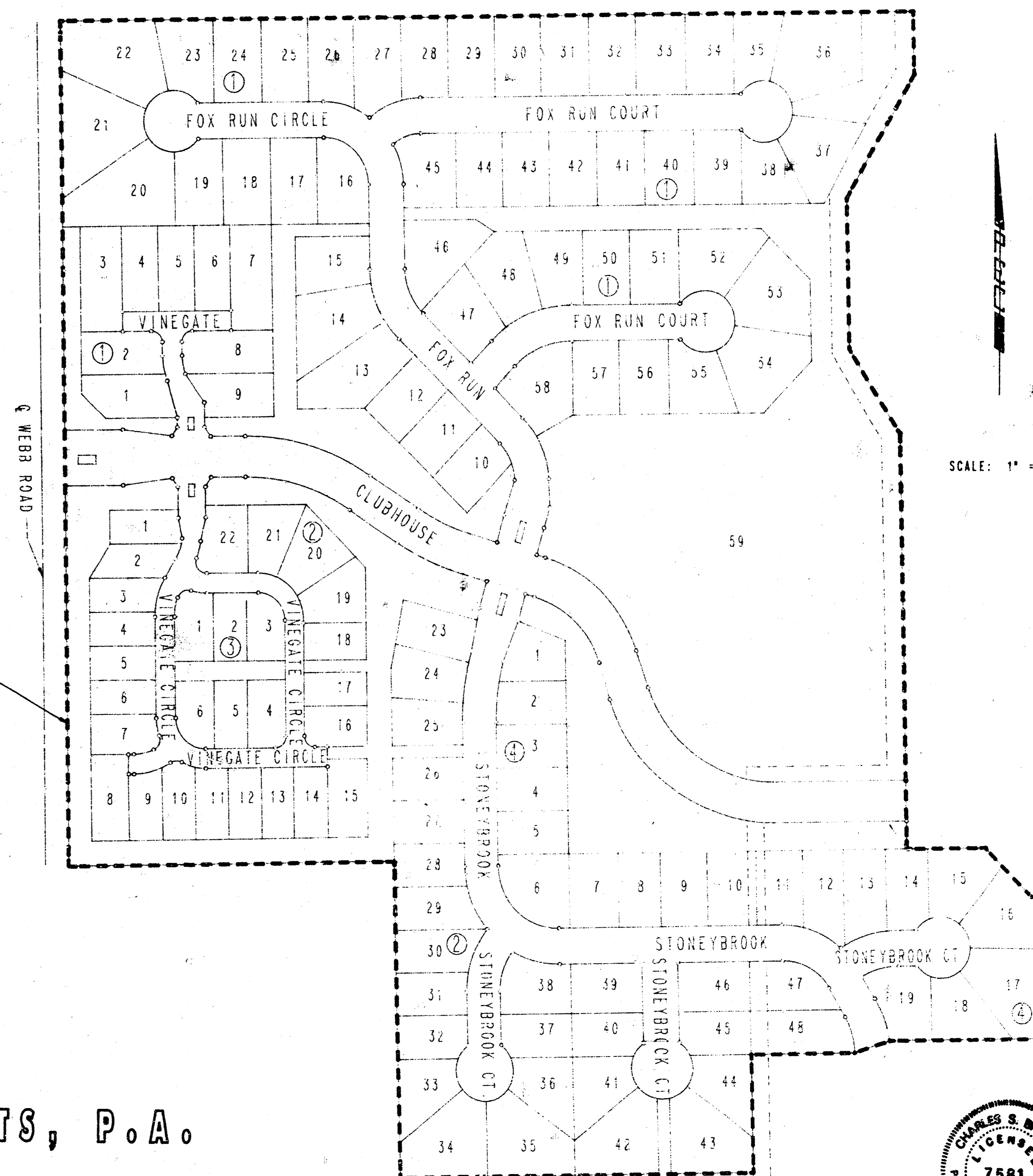
GENERAL NOTES

UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, AND ETCETERA ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH THE EXISTING GROUND SURFACE.

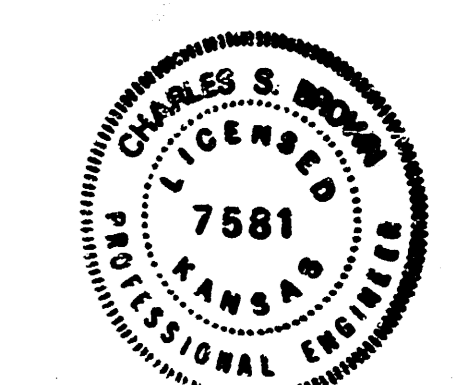
ALL EXCESS EXCAVATED MATERIAL FROM THIS PROJECT SHALL BE WASTED AS "LOOSE FILL" ON SITE. NO EXCESS MATERIAL SHALL BE PLACED WITHIN STREET RIGHTS-OF-WAY. WASTE MATERIAL SHALL BE BLADED SMOOTH AND SLOPED TO DRAIN. THIS WORK SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

IMPROVEMENT DISTRICT  
 BOUNDARY



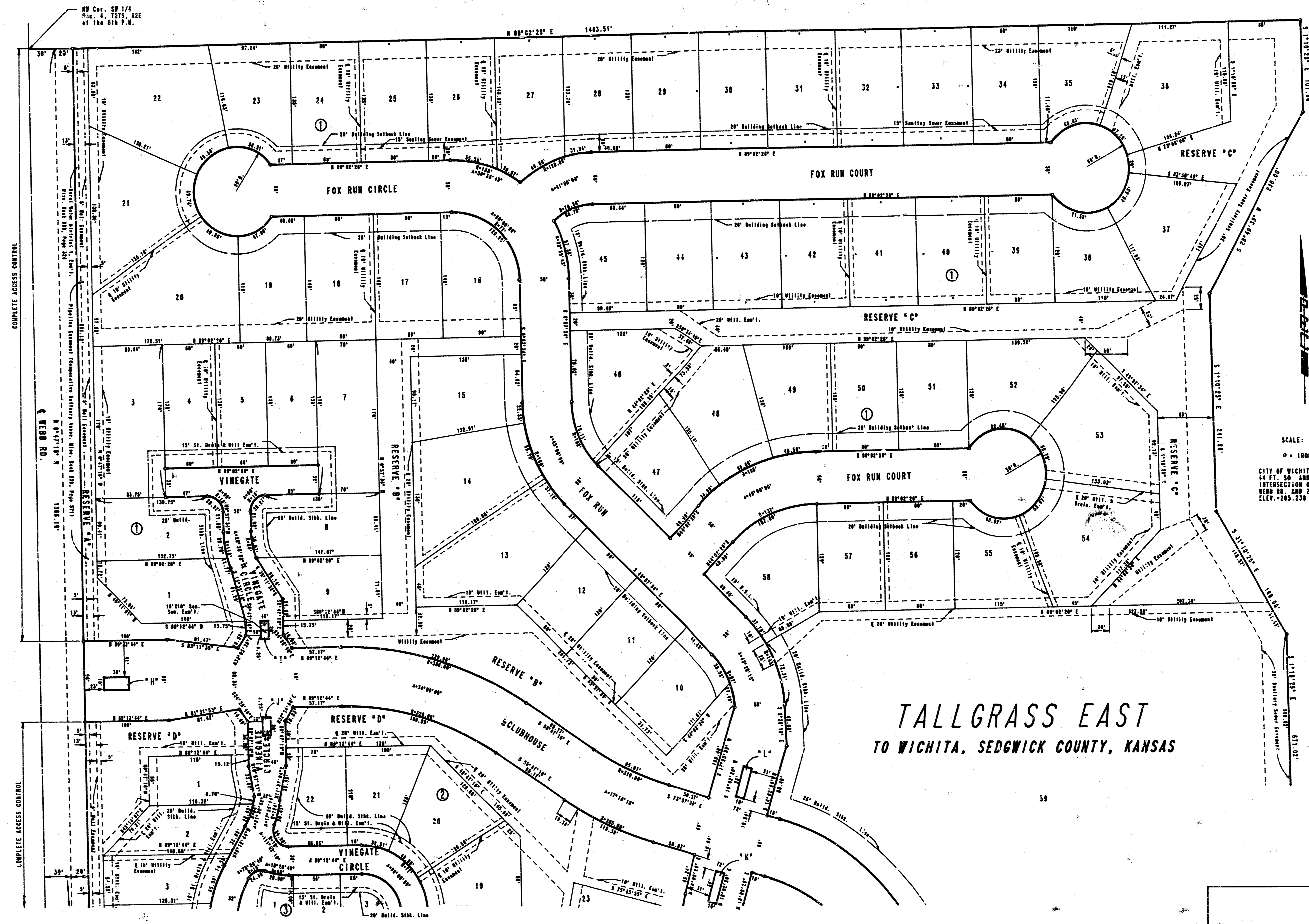
**JUNE, 1987**

PLANS PREPARED BY  
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
 ENGINEERS  
 WICHITA, KANSAS



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PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81628-000-001	2	14



SCALE: 1"=60'  
 ○ = IRON SET  
 CITY OF WICHITA B.M. DISC  
 84 FT. 00" AND 40 FT. EAST OF  
 INTERSECTION OF CENTERLINE OF  
 WEBB RD. AND 21ST ST.  
 ELEV. +205.230 CITY DATUM

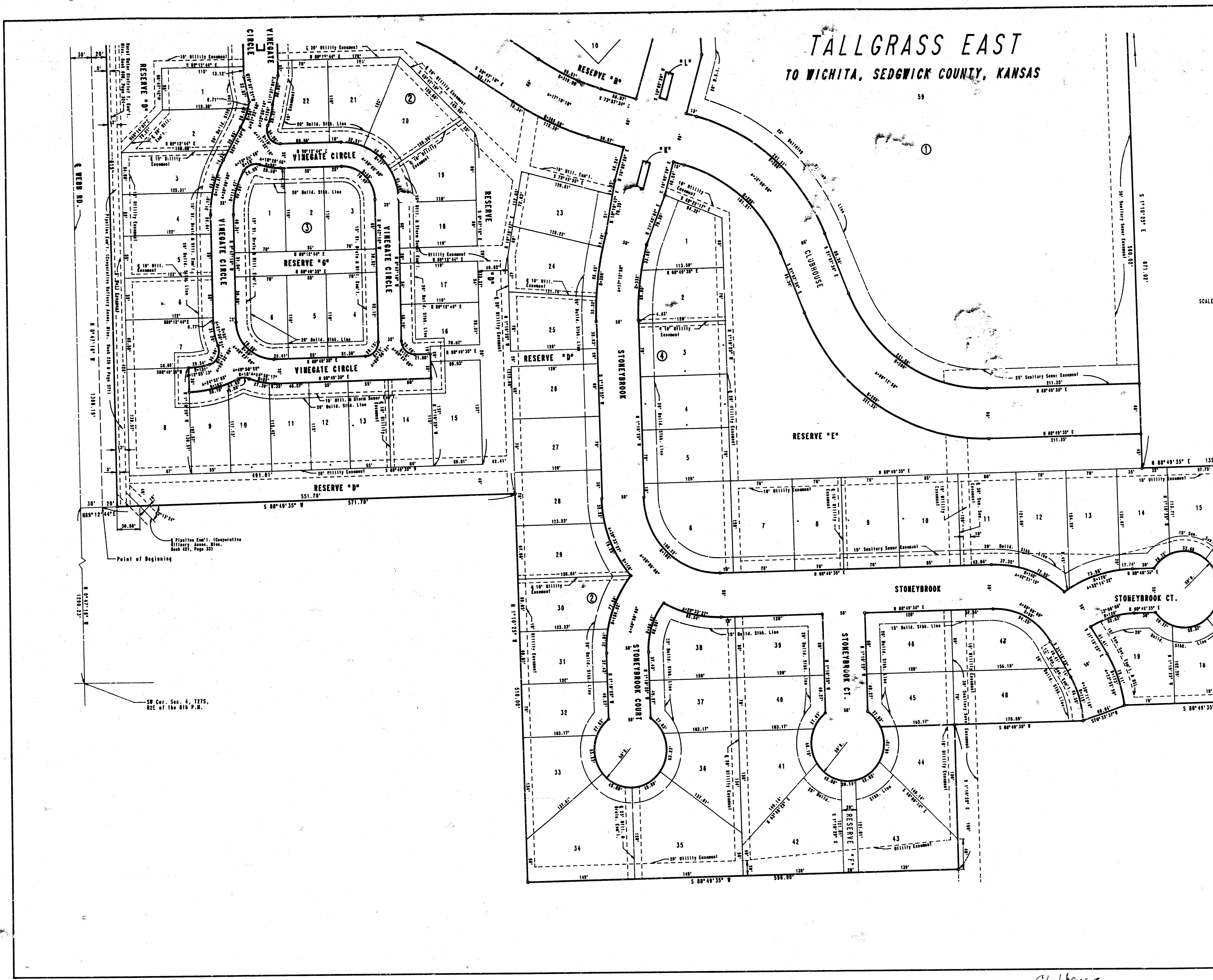
**TALLGRASS EAST**  
 TO WICHITA, SEDGWICK COUNTY, KANSAS

PLAT	
PROFESSIONAL ENGINEERING CONSULTANTS, P.A. ENGINEERS WICHITA, KANSAS	
Designed by	Checked by
Drawn by DEP	Date MAR., 1987 Job No. 86476-3

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# TALLGRASS EAST TO WICHITA, SEDGWICK COUNTY, KANSAS

PROJECT NO. 472-76-245-01828-000-001	SHEET NO. 3	TOTAL SHEETS 14
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**PLAT**

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
ENGINEERS  
WICHITA, KANSAS

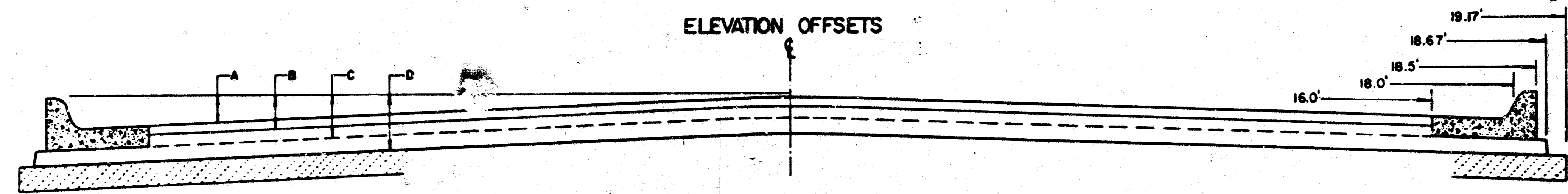
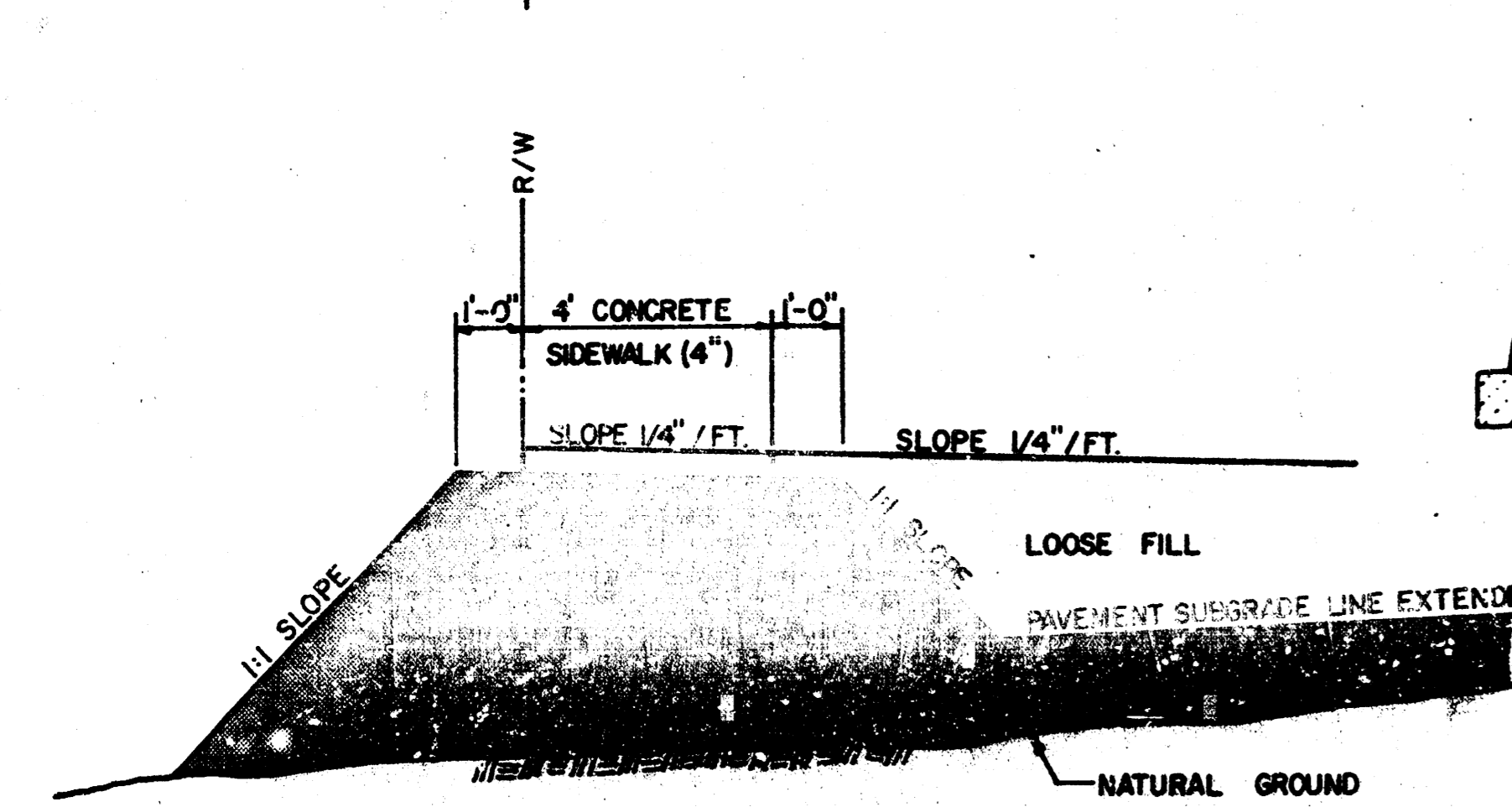
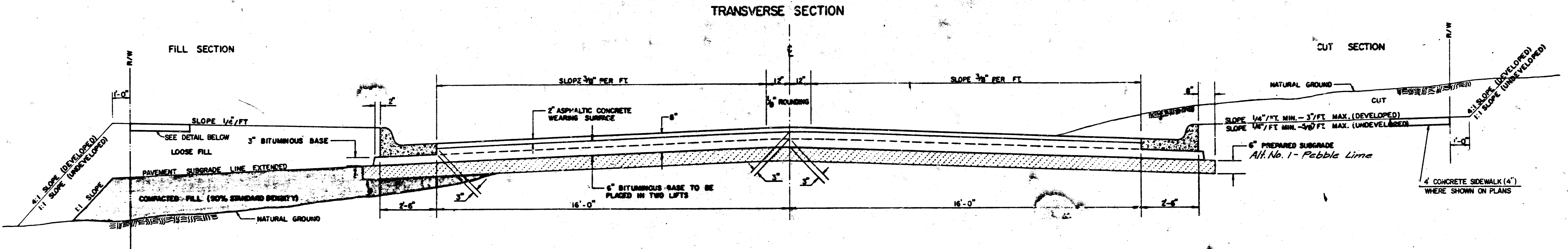
Designed by	Checked by
Drawn by DEP	Date MAR., 1987 Job No. 86476-3

*Cl. house*

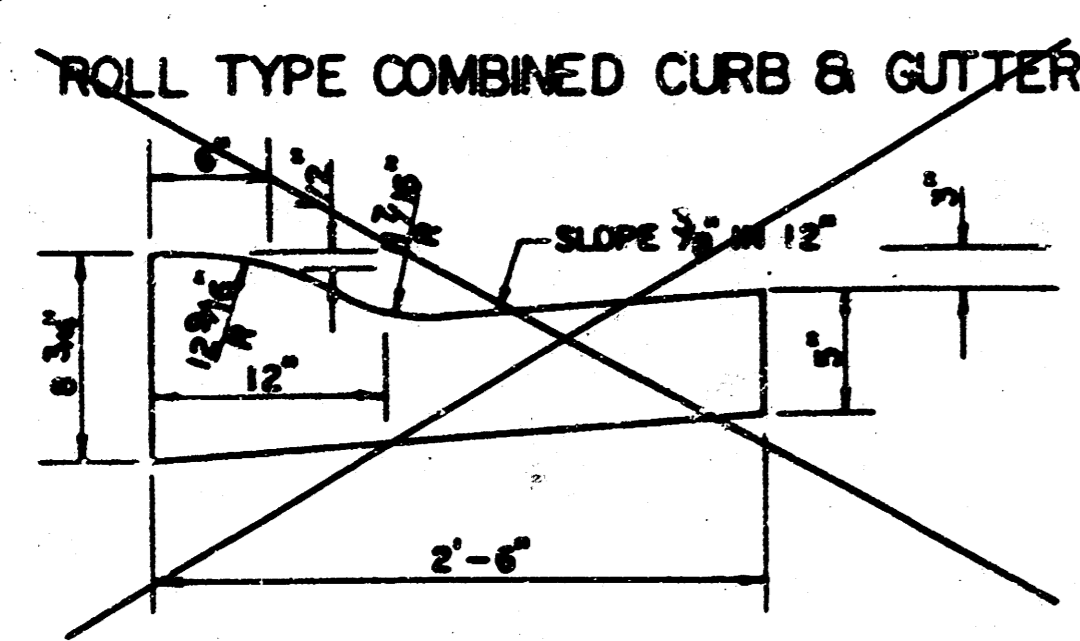
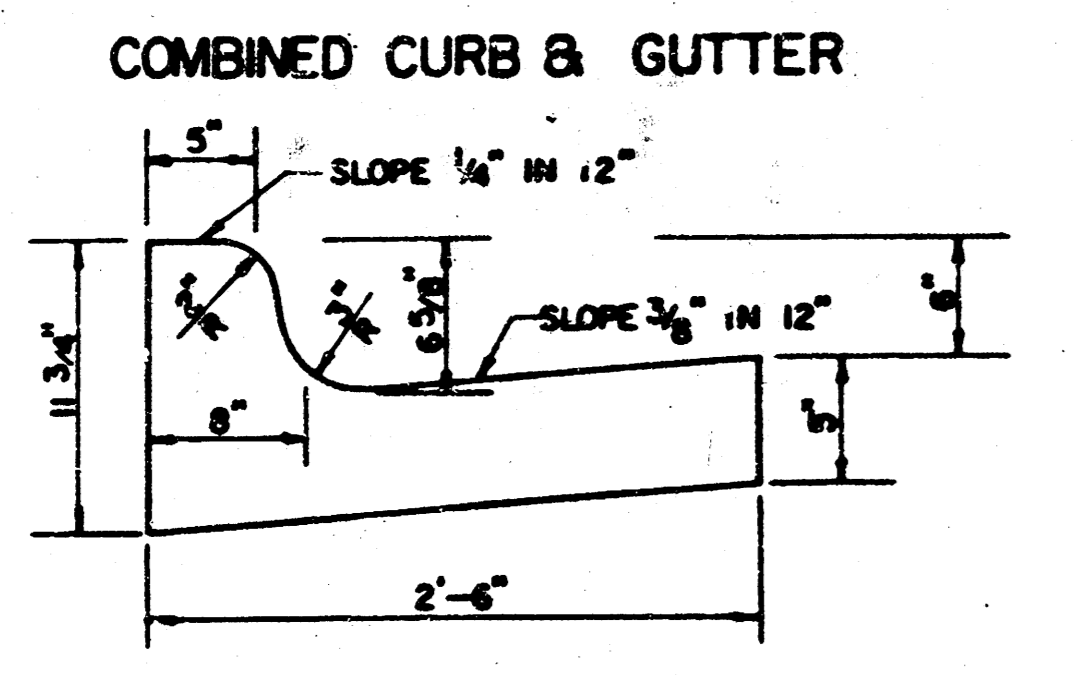
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AVAILABLE COPY**

# TYPICAL 37' PAVEMENT DETAILS

4 14



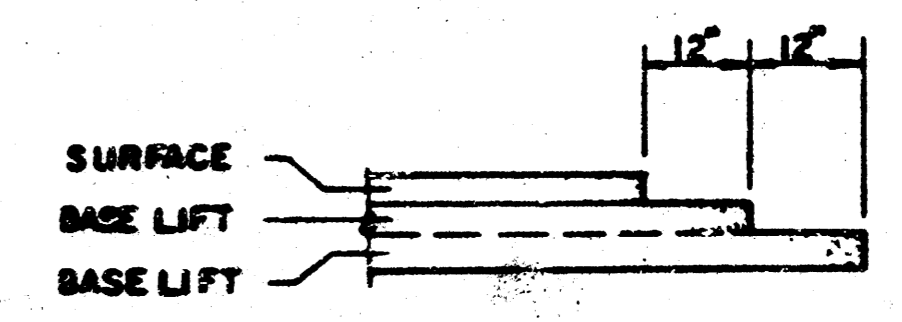
	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0	2	4	6	8	10	12	14	16	18	18.5	18.67	19.17
A: TOP OF CURBS TO TOP OF SURFACE LIFT	.01	.05	.11	.18	.24	.30	.36	.43	.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	.17	.22	.28	.34	.40	.47	.53	.59	.65	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	.42	.47	.53	.59	.65	.72	.78	.84	.90	.97	.98	.99	—
D: TOP OF CURBS TO TOP OF SUBGRADE	.67	.72	.78	.84	.90	.97	1.03	1.09	1.15	1.22	1.23	1.24	1.25



### GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PROVIDED BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 8" ASPHALTIC CONCRETE (6" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 3" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAVERON MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

### TRANSVERSE CONSTRUCTION JOINTS

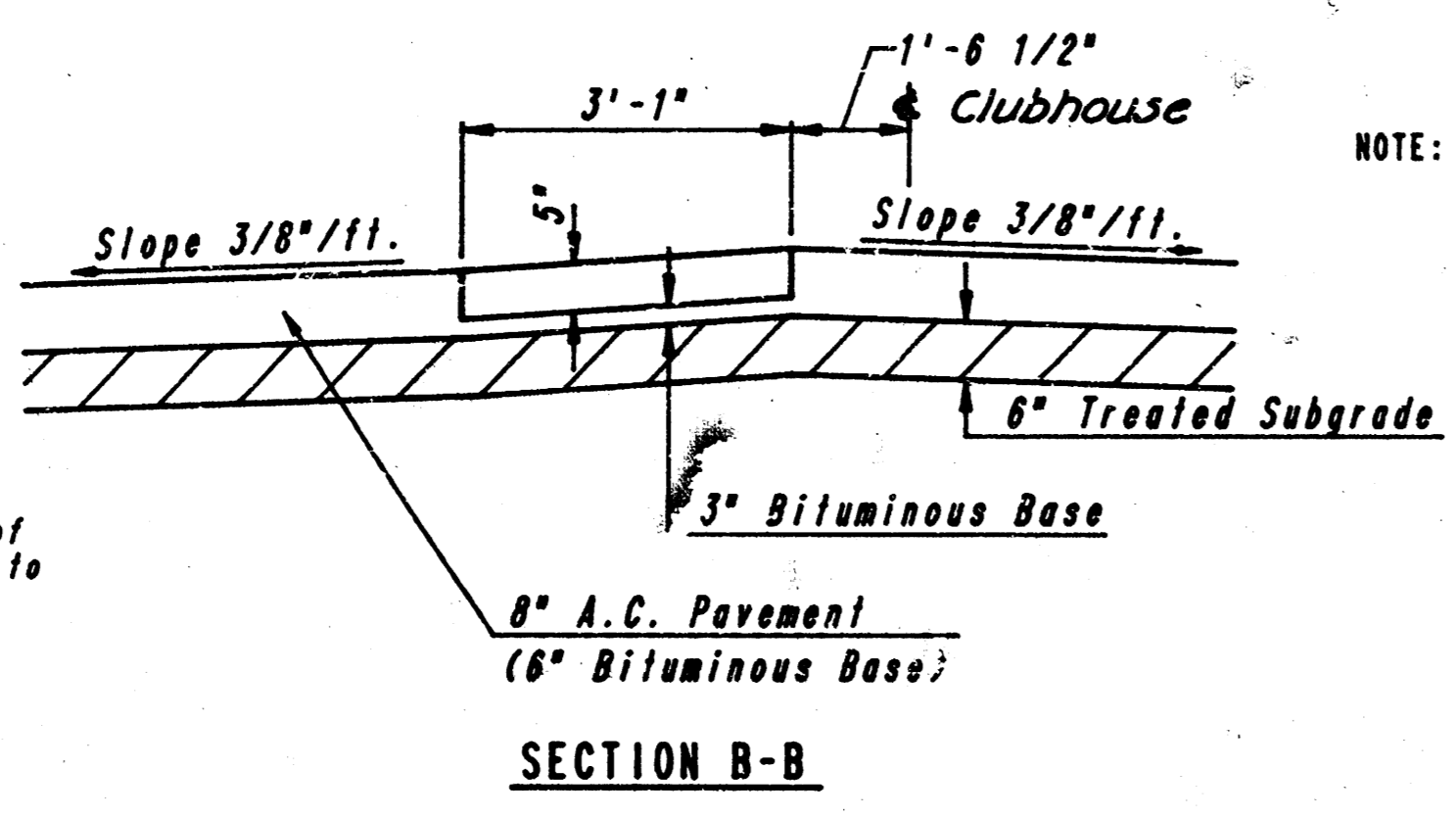
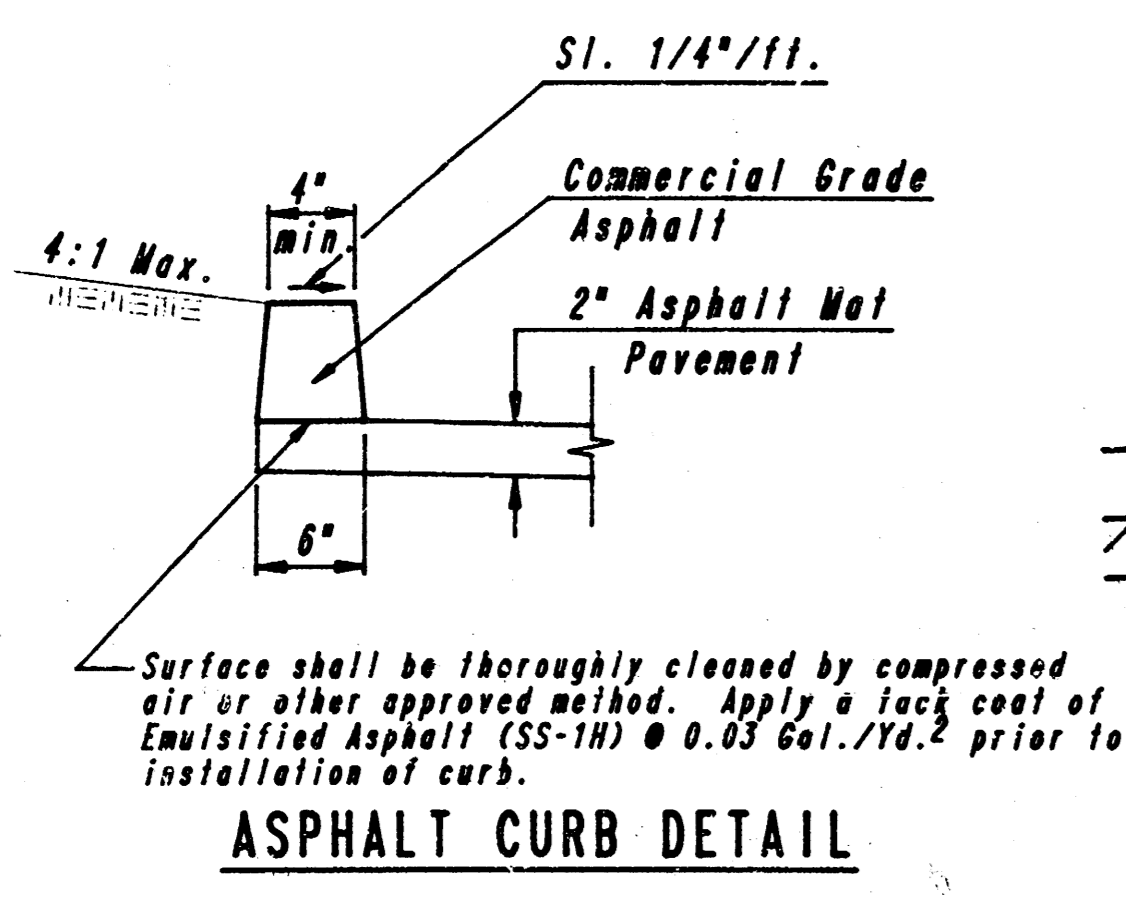
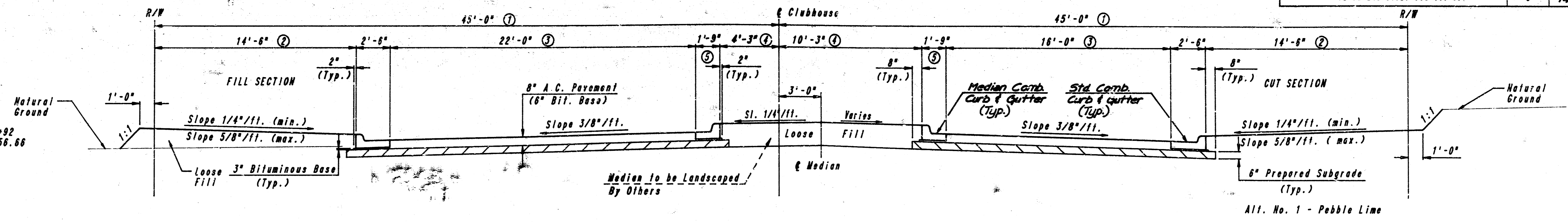


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 8" ASPHALTIC CONCRETE (6" BITUMINOUS BASE).

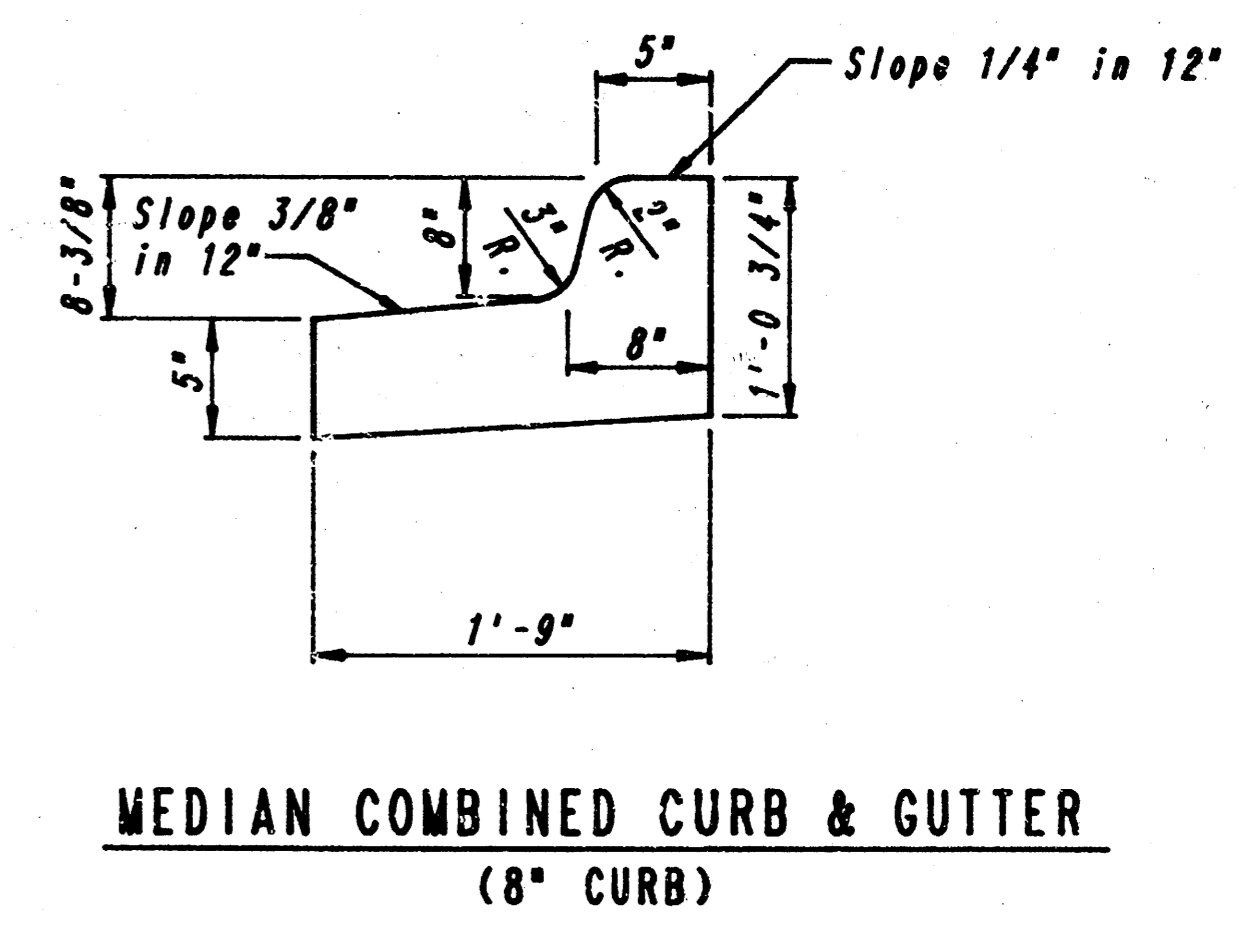
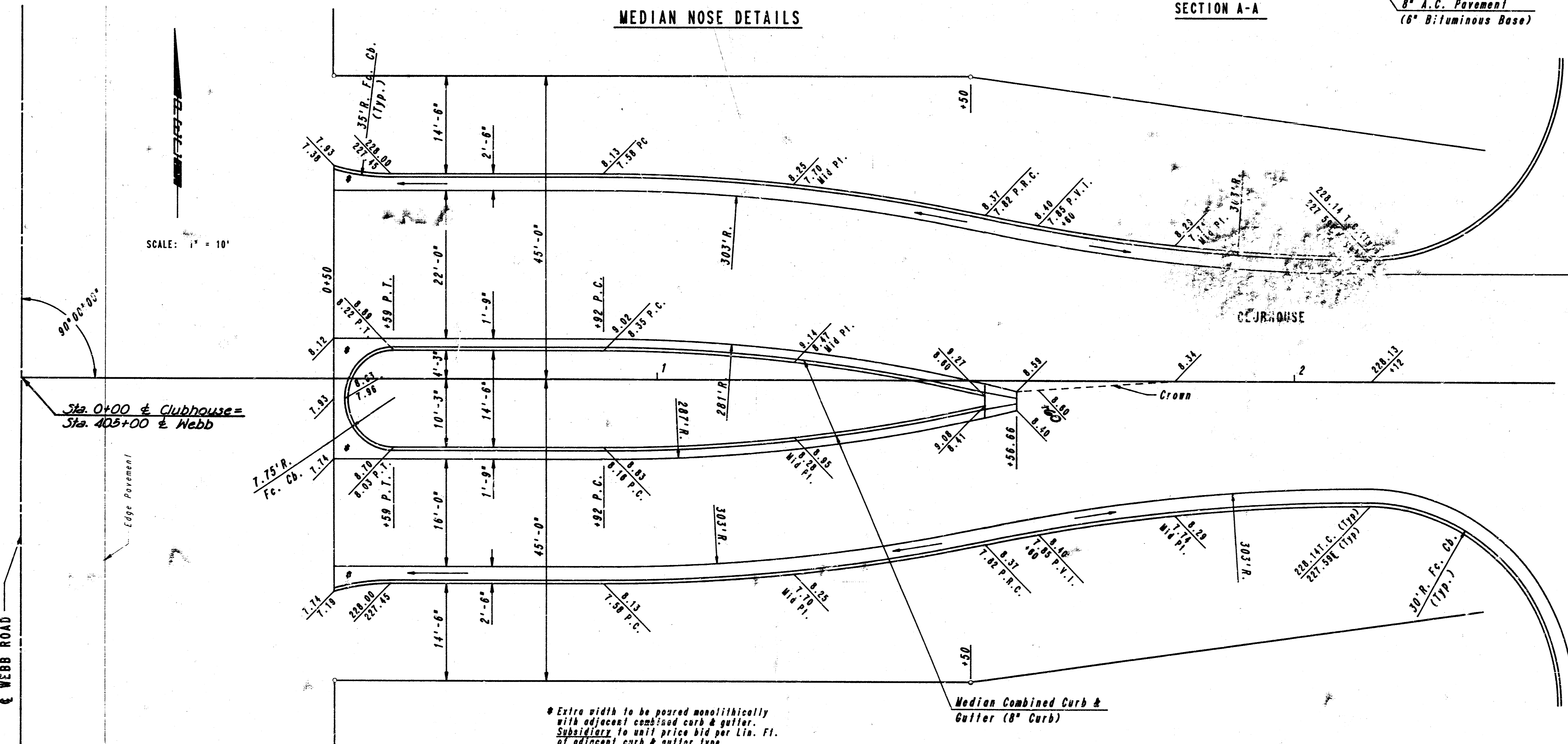
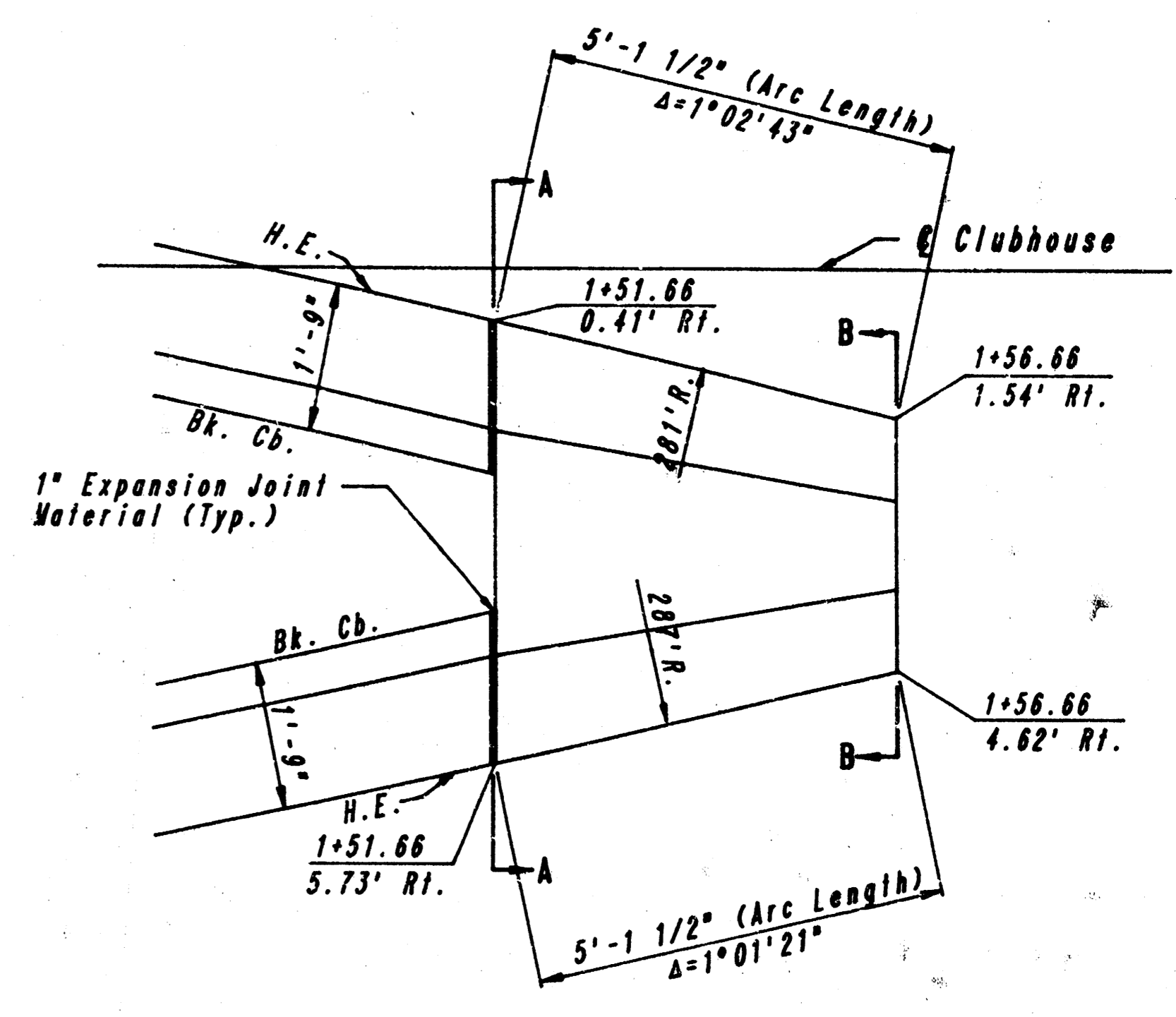
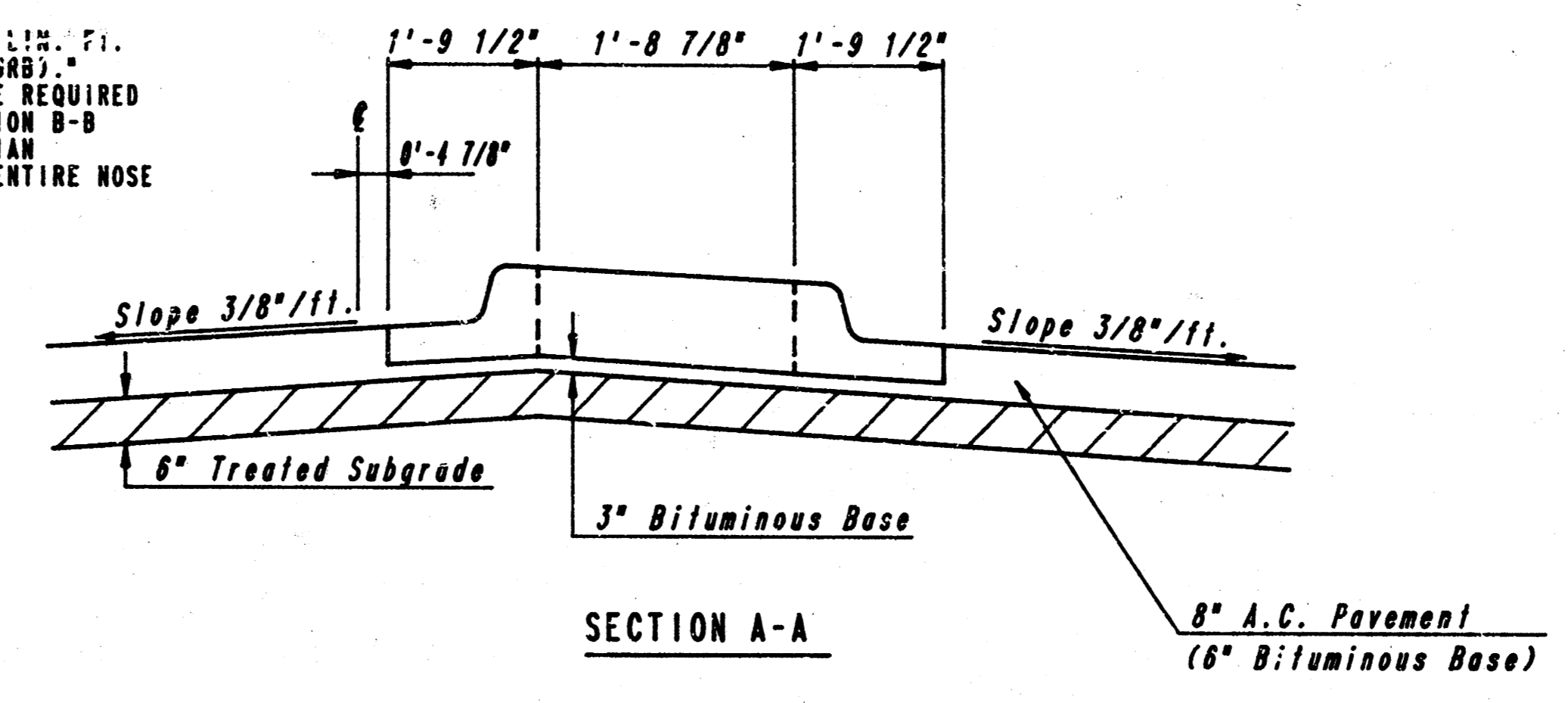
8 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 6 INCH BITUMINOUS BASE  
**CITY OF WICHITA, KANSAS**  
 PROJECT NUMBER  
 472 76 245 81626 000 000 001

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- ① Varies from Sta. 1+50 to Sta. 2+12. See Plan
- ② Varies from Sta. 0+92 to Sta. 2+12. See Plan
- ③ Varies from Sta. 1+58.66 to Sta. 2+12. See Plan
- ④ See Plan & Median Nose Details for Sta. 0+92 through Sta. 1+51.66. 0'-0" from Sta. 1+58.66 to Sta. 2+12.
- ⑤ 0'-0" from Sta. 1+51.66 to Sta. 2+12.



NOTE: MEDIAN NOSE SHALL BE PAID FOR AS 10.25 C.Y. P.I. MEDIAN COMBINED CURB AND GUTTER (8\"/>



**PAVING DETAILS**

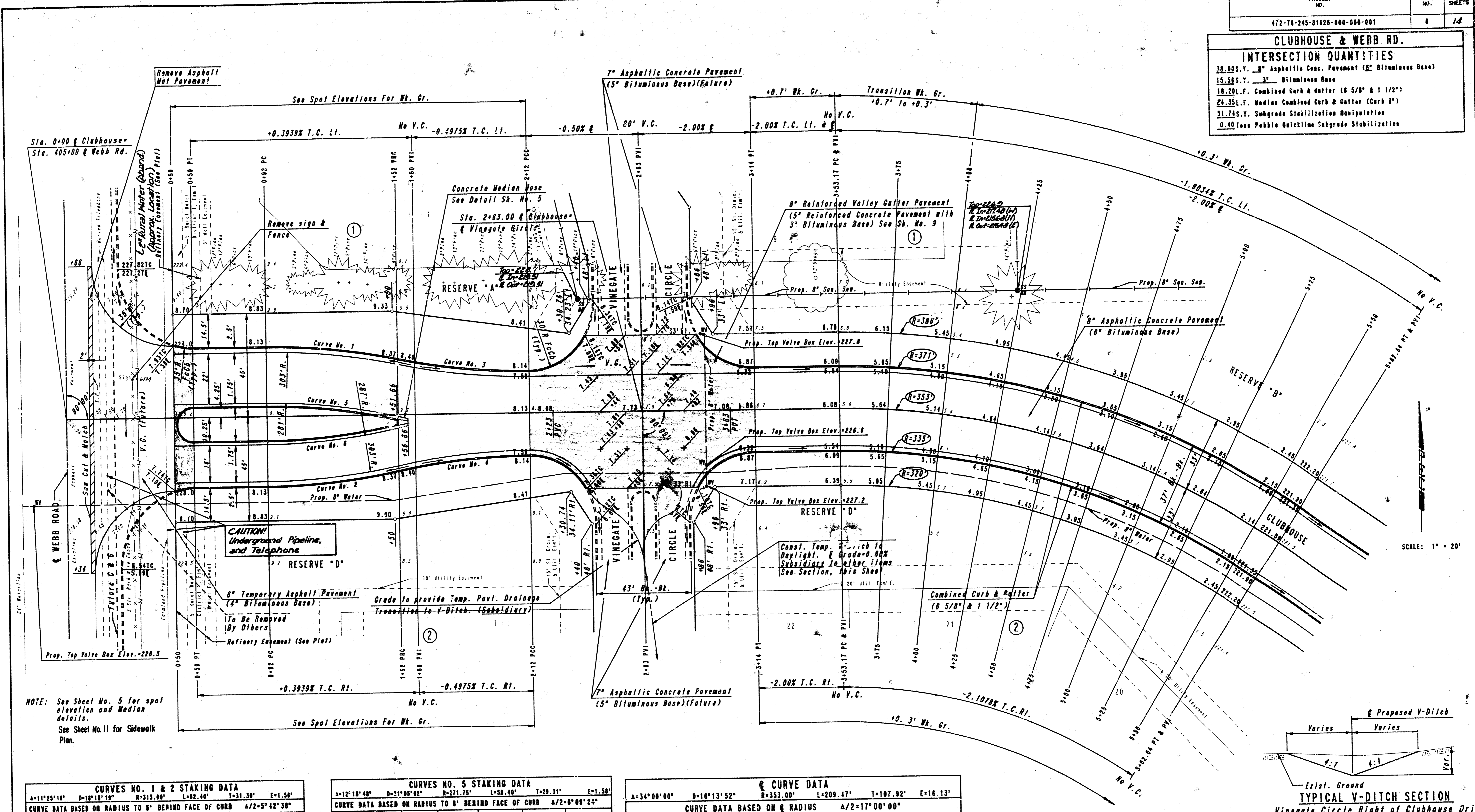
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
ENGINEERS  
WICHITA, KANSAS

Designed by CSB, GDD	Checked by
Drawn by DEP	Date MAR., 1987

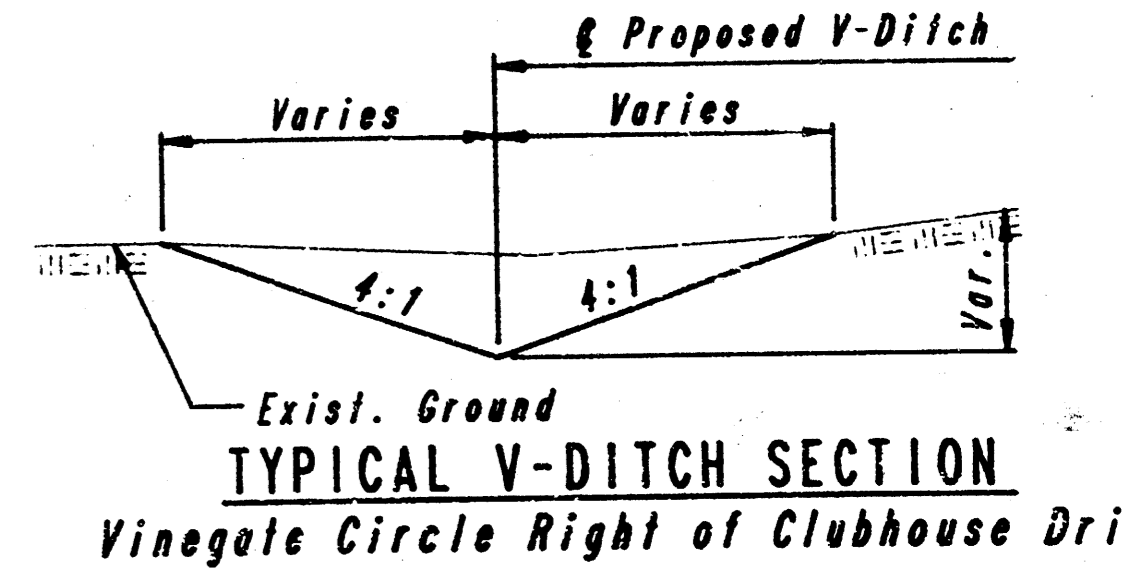
Job No. 86476-3

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CLUBHOUSE & WEBB RD.	
INTERSECTION QUANTITIES	
38,025 Y.	8" Asphaltic Conc. Pavement (2" Bituminous Base)
15,585 Y.	3" Bituminous Base
18,201 F.	Combined Curb & Gutter (6 5/8" & 1 1/2")
24,351 F.	Median Combined Curb & Gutter (Curb 6")
31,745 Y.	Subgrade Stabilization Manipulation
0.40 Tons	Pebble Quicklime Subgrade Stabilization



NOTE: See Sheet No. 5 for spot elevation and Median details.  
See Sheet No. 11 for Sidewalk Plan.

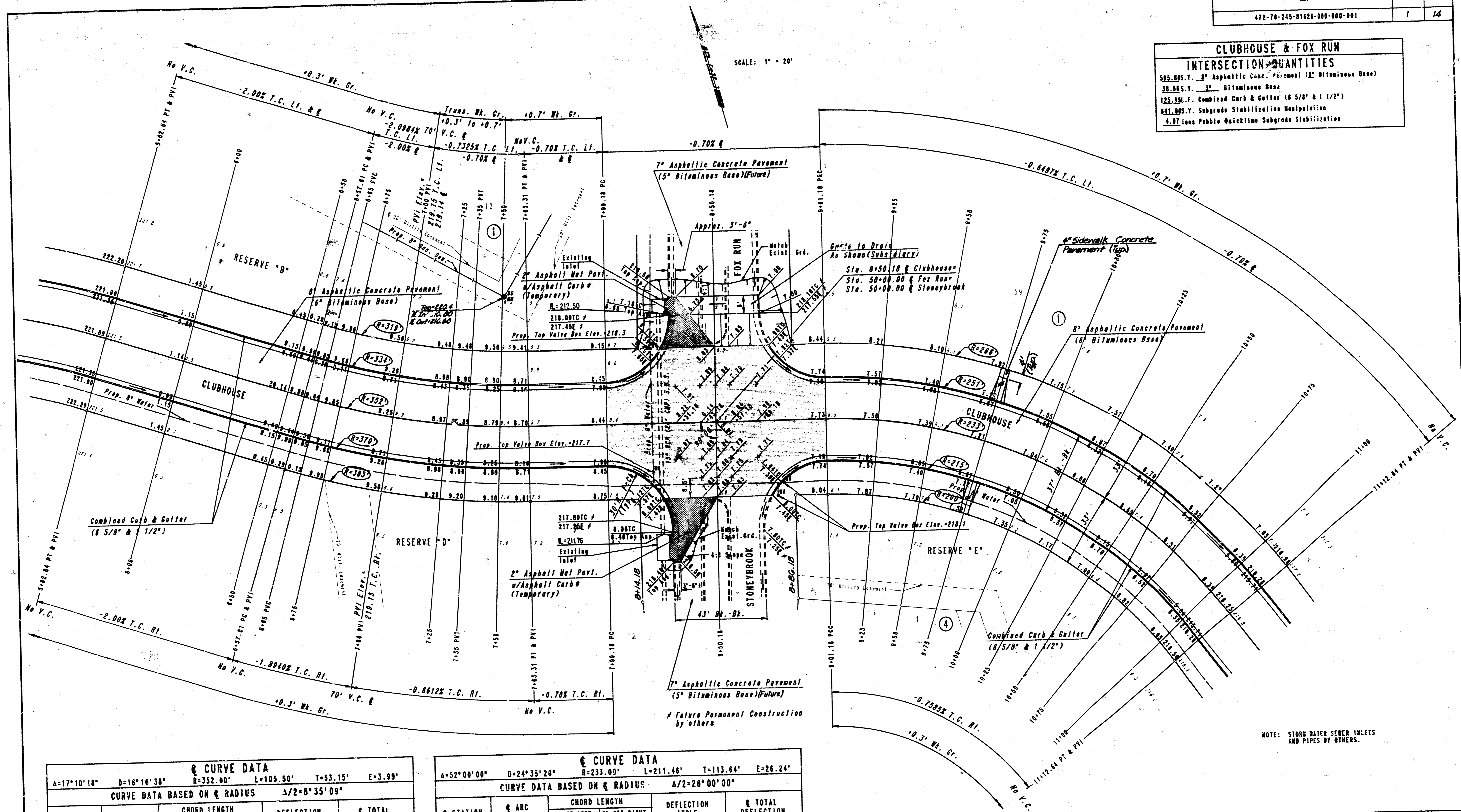


CURVES NO. 1 & 2 STAKING DATA						
CURVE DATA BASED ON RADIUS TO 8' BEHIND FACE OF CURB A/2=5°42'38"						
STATION	DISTANCE TO CURB	ARC LENGTH	CHORD LENGTH	DEFLECTION ANGLE	TOTAL DEFLECTION	REMARKS
0+00.00	31.50'			0°00'00"	0°00'00"	P.C.
1+23.14	31.50'	31.25'	31.10'	2°51'18"	2°51'18"	BIPOINTE
1+54.39	31.50'	31.25'	31.10'	2°51'18"	5°42'36"	P.T.

CURVES NO. 5 STAKING DATA						
CURVE DATA BASED ON RADIUS TO 8' BEHIND FACE OF CURB A/2=6°00'24"						
STATION	DISTANCE TO CURB	ARC LENGTH	CHORD LENGTH	DEFLECTION ANGLE	TOTAL DEFLECTION	REMARKS
0+00.00	3.32'			0°00'00"	0°00'00"	P.C.
1+21.14	4.02'	29.20'	29.10'	3°04'42"	3°04'42"	BIPOINTE
1+50.35	4.02'	29.20'	29.10'	3°04'42"	6°09'24"	P.T.

CURVE DATA						
CURVE DATA BASED ON RADIUS TO 8' BEHIND FACE OF CURB A/2=17°00'00"						
STATION	ARC LENGTH	CHORD LENGTH	DEFLECTION ANGLE	TOTAL DEFLECTION	REMARKS	
3+53.17			0°00'00"	0°00'00"		
3+75	21.83'	23.43'	20.22'	1°40'18.5"	1°40'18.5"	
4+00	25.00'	26.84'	23.15'	2°01'44"	3°42'02.5"	
4+25	25.00'	26.84'	23.15'	2°01'44"	5°43'46.5"	
4+50	25.00'	26.84'	23.15'	2°01'44"	7°45'30.5"	
4+75	25.00'	26.84'	23.15'	2°01'44"	9°47'14.5"	
5+00	25.00'	26.84'	23.15'	2°01'44"	11°48'58.5"	
5+25	25.00'	26.84'	23.15'	2°01'44"	13°50'42.5"	
5+50	25.00'	26.84'	23.15'	2°01'44"	15°52'26.5"	
5+75	25.00'	26.84'	23.15'	2°01'44"	17°54'10.5"	
6+00	25.00'	26.84'	23.15'	2°01'44"	19°55'54.5"	
6+25	25.00'	26.84'	23.15'	2°01'44"	21°57'38.5"	
6+50	25.00'	26.84'	23.15'	2°01'44"	23°59'22.5"	
6+75	25.00'	26.84'	23.15'	2°01'44"	26°01'06.5"	
7+00	25.00'	26.84'	23.15'	2°01'44"	28°02'50.5"	
7+25	25.00'	26.84'	23.15'	2°01'44"	30°04'34.5"	
7+50	25.00'	26.84'	23.15'	2°01'44"	32°06'18.5"	
7+75	25.00'	26.84'	23.15'	2°01'44"	34°08'02.5"	
8+00	25.00'	26.84'	23.15'	2°01'44"	36°09'46.5"	
8+25	25.00'	26.84'	23.15'	2°01'44"	38°11'30.5"	
8+50	25.00'	26.84'	23.15'	2°01'44"	40°13'14.5"	
8+75	25.00'	26.84'	23.15'	2°01'44"	42°14'58.5"	
9+00	25.00'	26.84'	23.15'	2°01'44"	44°16'42.5"	
9+25	25.00'	26.84'	23.15'	2°01'44"	46°18'26.5"	
9+50	25.00'	26.84'	23.15'	2°01'44"	48°20'10.5"	
9+75	25.00'	26.84'	23.15'	2°01'44"	50°21'54.5"	
10+00	25.00'	26.84'	23.15'	2°01'44"	52°23'38.5"	
10+25	25.00'	26.84'	23.15'	2°01'44"	54°25'22.5"	
10+50	25.00'	26.84'	23.15'	2°01'44"	56°27'06.5"	
10+75	25.00'	26.84'	23.15'	2°01'44"	58°28'50.5"	
11+00	25.00'	26.84'	23.15'	2°01'44"	60°30'34.5"	
11+25	25.00'	26.84'	23.15'	2°01'44"	62°32'18.5"	
11+50	25.00'	26.84'	23.15'	2°01'44"	64°34'02.5"	
11+75	25.00'	26.84'	23.15'	2°01'44"	66°35'46.5"	
12+00	25.00'	26.84'	23.15'	2°01'44"	68°37'30.5"	
12+25	25.00'	26.84'	23.15'	2°01'44"	70°39'14.5"	
12+50	25.00'	26.84'	23.15'	2°01'44"	72°40'58.5"	
12+75	25.00'	26.84'	23.15'	2°01'44"	74°42'42.5"	
13+00	25.00'	26.84'	23.15'	2°01'44"	76°44'26.5"	
13+25	25.00'	26.84'	23.15'	2°01'44"	78°46'10.5"	
13+50	25.00'	26.84'	23.15'	2°01'44"	80°47'54.5"	
13+75	25.00'	26.84'	23.15'	2°01'44"	82°49'38.5"	
14+00	25.00'	26.84'	23.15'	2°01'44"	84°51'22.5"	
14+25	25.00'	26.84'	23.15'	2°01'44"	86°53'06.5"	
14+50	25.00'	26.84'	23.15'	2°01'44"	88°54'50.5"	
14+75	25.00'	26.84'	23.15'	2°01'44"	90°56'34.5"	
15+00	25.00'	26.84'	23.15'	2°01'44"	92°58'18.5"	
15+25	25.00'	26.84'	23.15'	2°01'44"	94°59'02.5"	
15+50	25.00'	26.84'	23.15'	2°01'44"	97°00'46.5"	
15+75	25.00'	26.84'	23.15'	2°01'44"	99°02'30.5"	
16+00	25.00'	26.84'	23.15'	2°01'44"	101°04'14.5"	
16+25	25.00'	26.84'	23.15'	2°01'44"	103°05'58.5"	
16+50	25.00'	26.84'	23.15'	2°01'44"	105°07'42.5"	
16+75	25.00'	26.84'	23.15'	2°01'44"	107°09'26.5"	
17+00	25.00'	26.84'	23.15'	2°01'44"	109°11'10.5"	
17+25	25.00'	26.84'	23.15'	2°01'44"	111°12'54.5"	
17+50	25.00'	26.84'	23.15'	2°01'44"	113°14'38.5"	
17+75	25.00'	26.84'	23.15'	2°01'44"	115°16'22.5"	
18+00	25.00'	26.84'	23.15'	2°01'44"	117°18'06.5"	
18+25	25.00'	26.84'	23.15'	2°01'44"	119°19'50.5"	
18+50	25.00'	26.84'	23.15'	2°01'44"	121°21'34.5"	
18+75	25.00'	26.84'	23.15'	2°01'44"	123°23'18.5"	
19+00	25.00'	26.84'	23.15'	2°01'44"	125°25'02.5"	
19+25	25.00'	26.84'	23.15'	2°01'44"	127°26'46.5"	
19+50	25.00'	26.84'	23.15'	2°01'44"	129°28'30.5"	
19+75	25.00'	26.84'	23.15'	2°01'44"	131°30'14.5"	
20+00	25.00'	26.84'	23.15'	2°01'44"	133°31'58.5"	
20+25	25.00'	26.84'	23.15'	2°01'44"	135°33'42.5"	
20+50	25.00'	26.84'	23.15'	2°01'44"	137°35'26.5"	
20+75	25.00'	26.84'	23.15'	2°01'44"	139°37'10.5"	
21+00	25.00'	26.84'	23.15'	2°01'44"	141°38'54.5"	
21+25	25.00'	26.84'	23.15'	2°01'44"	143°40'38.5"	
21+50	25.00'	26.84'	23.15'	2°01'44"	145°42'22.5"	
21+75	25.00'	26.84'	23.15'	2°01'44"	147°44'06.5"	
22+00	25.00'	26.84'	23.15'	2°01'44"	149°45'50.5"	
22+25	25.00'	26.84'	23.15'	2°01'44"	151°47'34.5"	
22+50	25.00'	26.84'	23.15'	2°01'44"	153°49'18.5"	
22+75	25.00'	26.84'	23.15'	2°01'44"	155°51'02.5"	
23+00	25.00'	26.84'	23.15'	2°01'44"	157°52'46.5"	
23+25	25.00'	26.84'	23.15'	2°01'44"	159°54'30.5"	
23+50	25.00'	26.84'	23.15'	2°01'44"	161°56'14.5"	
23+75	25.00'	26.84'	23.15'	2°01'44"	163°57'58.5"	
24+00	25.00'	26.84'	23.15'	2°01'44"	165°59'42.5"	
24+25	25.00'	26.84'	23.15'	2°01'44"	168°01'26.5"	
24+50	25.00'	26.84'	23.15'	2°01'44"	170°03'10.5"	
24+75	25.00'	26.84'	23.15'	2°01'44"	172°04'54.5"	
25+00	25.00'	26.84'	23.15'	2°01'44"	174°06'38.5"	
25+25	25.00'	26.84'	23.15'	2°01'44"	176°08'22.5"	
25+50	25.00'	26.84'	23.15'	2°01'44"	178°10'06.5"	
25+75	25.00'	26.84'	23.15'	2°01'44"	180°11'50.5"	
26+00	25.00'	26.84'	23.15'	2°01'44"	182°13'34.5"	
26+25	25.00'	26.84'	23.15'	2°01'44"	184°15'18.5"	
26+50	25.00'	26.84'	23.15'	2°01'44"	186°17'02.5"	
26+75	25.00'	26.84'	23.15'	2°01'44"	188°18'46.5"	
27+00	25.00'	26.84'	23.15'	2°01'44"	190°20'30.5"	
27+25	25.00'	26.84'	23.15'	2°01'44"	192°22'14.5"	
27+50	25.00'	26.84'	23.15'	2°01'44"	194°23'58.5"	
27+75	25.00'	26.84'	23.15'	2°01'44"	196°25'42.5"	
28+00	25.00'	26.84'	23.15'	2°01'44"	198°27'26.5"	
28+25	25.00'	26.84'	23.15'	2°01'44"	200°29'10.5"	
28+50	25.00'	26.84'	23.15'	2°01'44"	202°30'54.5"	
28+75	25.00'	26.84'	23.15'	2°01'44"	204°32'38.5"	
29+00	25.00'	26.84'	23.15'	2°01'44"	206°34'22.5"	
29+25	25.00'	26.84'	23.15'	2°01'44"	208°36'06.5"	
29+50	25.00'	26.84'	23.15'	2°01'44"	210°37'50.5"	
29+75	25.00'	26.84'	23.15'	2°01'44"	212°39'34.5"	
30+00	25.00'	26.84'	23.15'	2°01'44"	214°41'18.5"	
30+25	25.00'	26.84'	23.15'	2°01'44"	216°43'02.5"	
30+50	25.00'	26.84'	23.15'	2°01'44"	218°44'46.5"	
30+75	25.00'	26.84'	23.15'	2°01'44"	220°46'30.5"	
31+00	25.00'	26.84'	23.15'	2°01'44"	222°48'14.5"	
31+25	25.00'	26.84'	23.15'	2°01'44"	224°50'02.5"	
31+50	25.00'	26.84'	23.15'	2°01'44"	226°51'50.5"	
31+75	25.00'	26.84'	23.15'	2°01'44"	228°53'38.5"	
32+00	25.00'	26.84'	23.15'	2°01'44"	230°55'26.5"	
32+25	25.00'	26.84'	23.15'	2°01'44"	232°57'14.5"	
32+50	25.00'	26.84'	23.15'	2°01'44"	234°59'02.5"	
32+75	25.00'	26.84'	23.15'	2°01'44"	237°00'50.5"	
33+00	25.00'	26.84'	23.15'	2°01'44"	239°02'38.5"	
33+25	25.00'	26.84'	23.15'	2°01'44"	241°04'26.5"	
33+50	25.00'	26.84'	23.15'	2°01'44"	243°06'14.5"	
33+75	25.00'	26.84'	23.15'	2°01'44"	245°08'02.5"	
34+00	25.00'	26.84'	23.15'	2°01'44"	247°09'50.5"	
34+25	25.00'	26.84'	23.15'	2°01'44"	249°11'38.5"	
34+50	25.00'	26.84'	23.15'	2°01'44"	251°13'	

CLUBHOUSE & FOX RUN INTERSECTION QUANTITIES	
585,895.Y.	8" Asphaltic Concrete Pavement (5" Bituminous Base)
38,585.Y.	3" Bituminous Base
123,591.F.	Combined Curb & Gutter (6 5/8" & 1 1/2")
641,895.Y.	Subgrade Stabilization Manipulation
4.87	Lean Pabble Quicklime Subgrade Stabilization



CURVE DATA					
Δ=17°10'18"		D=16°16'38"		E=3.99'	
R=352.00'		L=105.50'		T=53.15'	
CURVE DATA BASED ON Δ/2=8°35'09"					
STATION	ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	TOTAL DEFLECTION
		1' OFF LEFT FACE CURB	1' OFF RIGHT FACE CURB		
6+57.81	0.00'	0.00'	0.00'	0°00'00"	0°00'00"
6+75	1.19'	15.82'	18.46'	1°23'57"	1°23'57"
7+00	25.00'	23.15'	28.84'	2°02'04"	3°28'01"
7+25	25.00'	23.15'	28.84'	2°02'04"	5°28'05"
7+50	25.00'	23.15'	28.84'	2°02'04"	7°30'09"
7+83.31	13.31'	12.33'	14.29'	1°05'00"	8°35'09"

CURVE DATA					
Δ=52°00'00"		D=24°35'26"		E=26.24'	
R=233.00'		L=211.46'		T=113.64'	
CURVE DATA BASED ON Δ/2=26°00'00"					
STATION	ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	TOTAL DEFLECTION
		1' OFF LEFT FACE CURB	1' OFF RIGHT FACE CURB		
9+01.18	0.00'	0.00'	0.00'	0°00'00"	0°00'00"
9+25	25.02'	26.47'	21.15'	2°55'43"	2°55'43"
9+50	25.00'	27.78'	22.20'	3°04'26"	6°00'09"
9+75	25.00'	27.78'	22.20'	3°04'26"	9°04'35"
10+00	25.00'	27.78'	22.20'	3°04'26"	12°09'01"
10+25	25.00'	27.78'	22.20'	3°04'26"	15°13'27"
10+50	25.00'	27.78'	22.20'	3°04'26"	18°17'53"
10+75	25.00'	27.78'	22.20'	3°04'26"	21°22'19"
11+00	25.00'	27.78'	22.20'	3°04'26"	24°26'45"
11+12.64	12.64'	14.05'	11.23'	1°33'15"	26°00'00"

NOTES:  
 ASPHALT CURB, INCLUDING SURFACE PREPARATION, TACK COAT & FINISHING, SHALL BE SUBSIDIARY TO 2" ASPHALT MAT PAVEMENT. SEE DETAIL, SHEET NO. 5.  
 2" ASPHALT MAT PAVEMENT W/ ASPHALT CURB IS FOR TEMPORARY DRAINAGE OF CLUBHOUSE DRIVE PAVEMENT. IN THE EVENT THAT FOX RUN AND/OR STONEYBROOK ARE CONSTRUCTED CONCURRENTLY WITH THIS PROJECT, THE TEMPORARY DRAINAGE FACILITIES WILL NOT BE CONSTRUCTED, AS DIRECTED BY THE ENGINEER. BID ITEMS INVOLVED AT THAT LOCATION SHALL BE SUBJECT TO UNDERCUT UP TO 100%.

Construct Standard Wheelchair Ramp

**CLUBHOUSE**  
 STA. 5+62.64 TO STA. 11+12.64

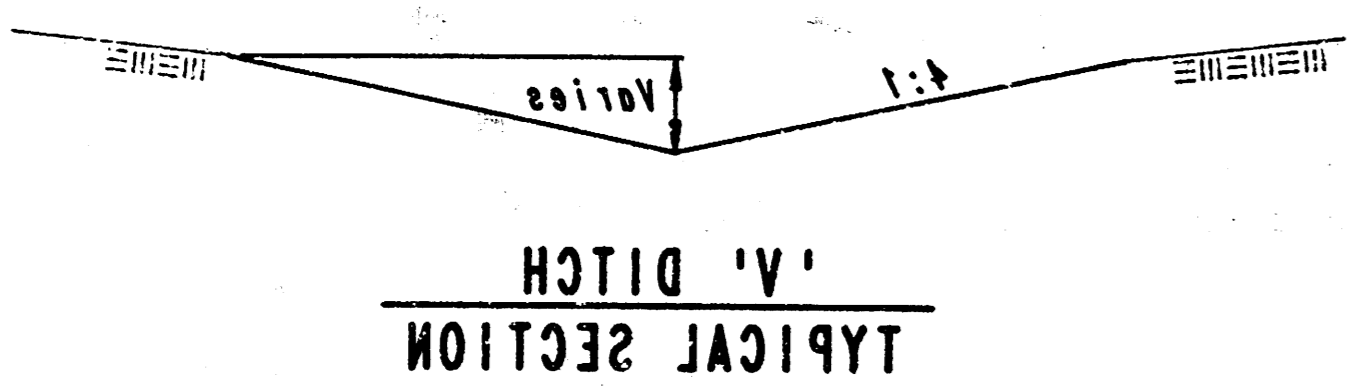
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 ENGINEERS  
 WICHITA, KANSAS

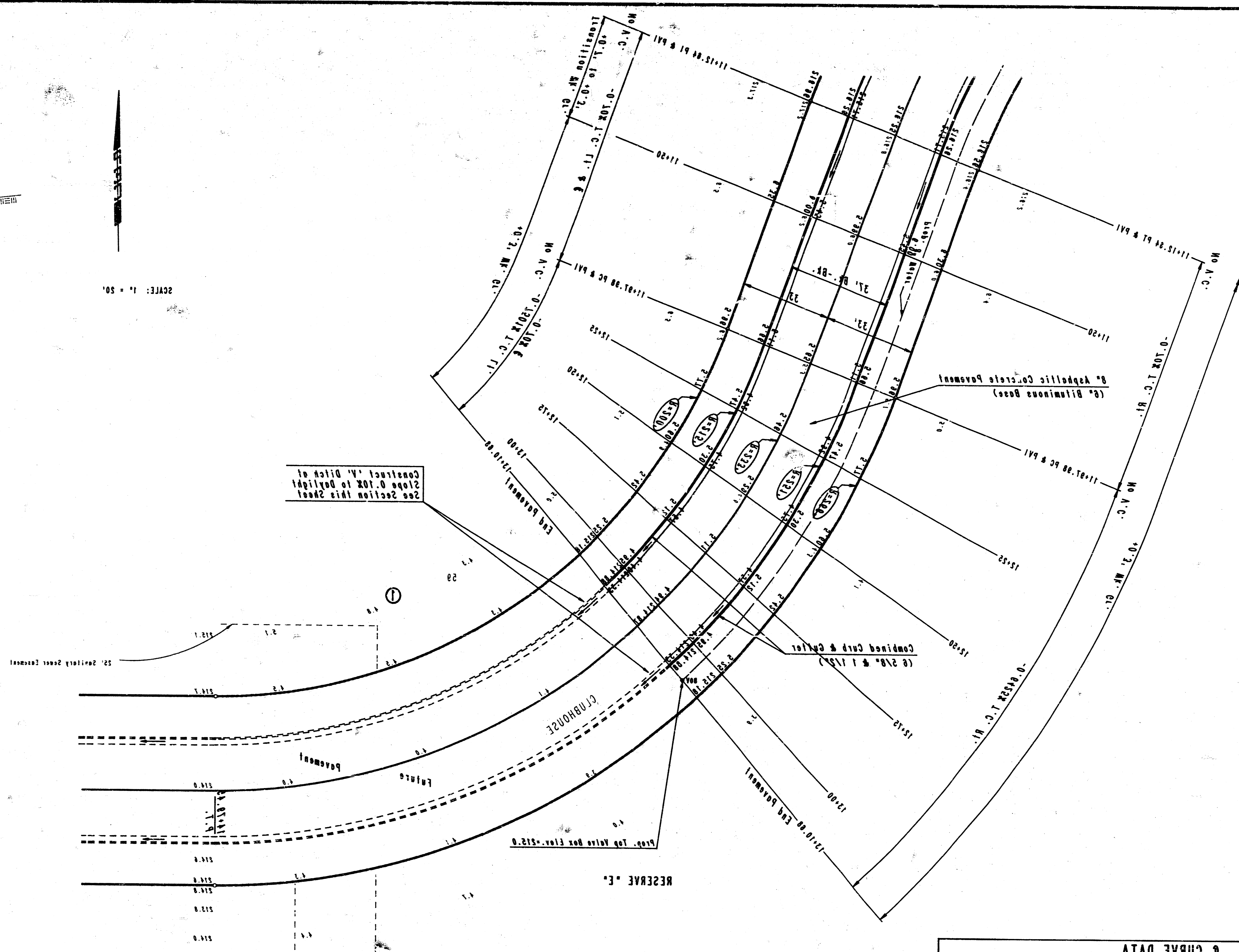
Designed by	CSB, GDD, BBB	Checked by	
Drawn by	DEP	Date	MAR., 1987
		Job No.	85474-3

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TOTAL SHEETS	8	PROJECT NO.	415-18-542-81858-000-000-001
SHEET NO.	8		



SCALE: 1" = 50'



Construct 'V' Ditch of Slope 0.10X to Right See Section in Sheet

CURVE DATA BASED ON RADIUS				
STATION	ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE
		FACE CURB	BACK CURB	
11+01.00	0.00	0.00	0.00	0.00
11+05.00	5.00	1.78	5.52	0.44
11+10.00	10.00	3.56	11.04	0.88
11+15.00	15.00	5.34	16.56	1.32
11+20.00	20.00	7.12	22.08	1.76
11+25.00	25.00	8.90	27.60	2.20
11+30.00	30.00	10.68	33.12	2.64
11+35.00	35.00	12.46	38.64	3.08
11+40.00	40.00	14.24	44.16	3.52
11+45.00	45.00	16.02	49.68	3.96
11+50.00	50.00	17.80	55.20	4.40

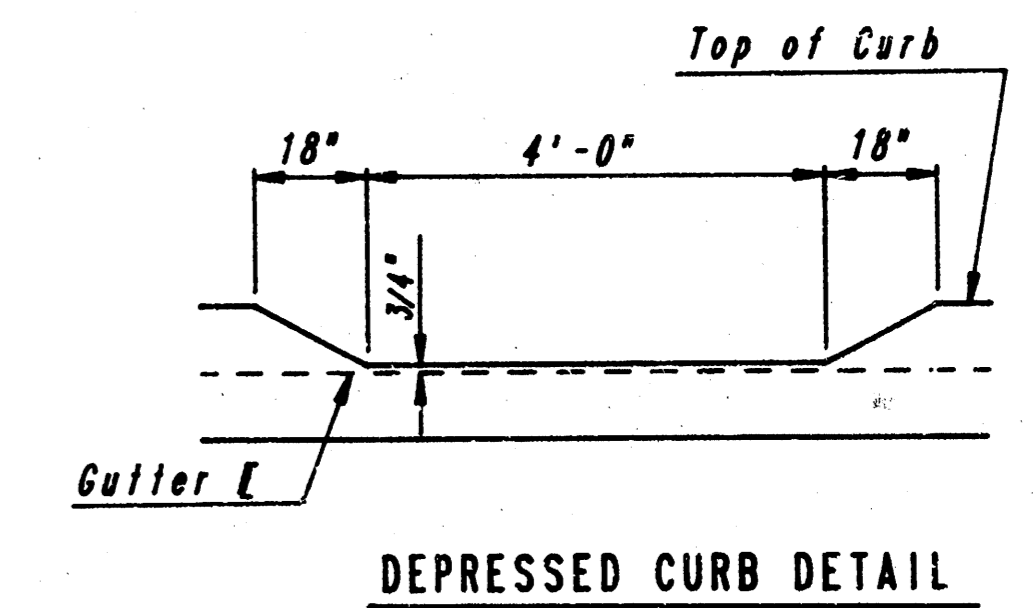
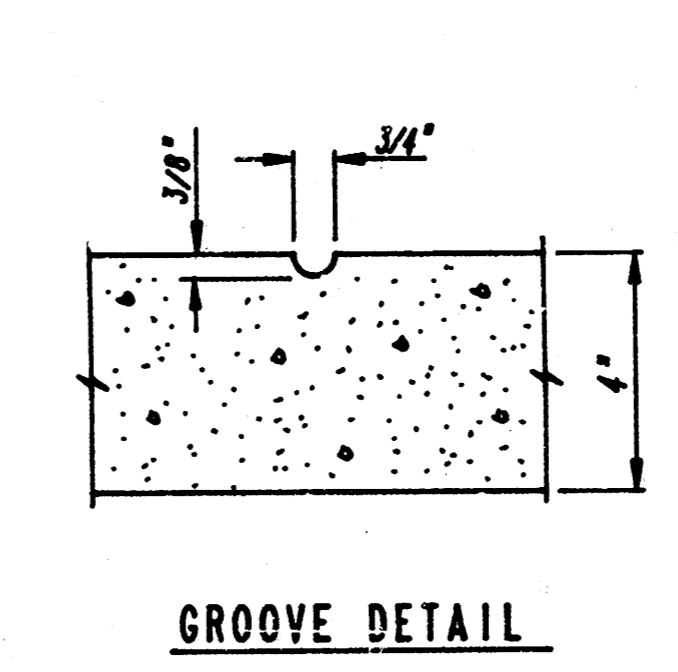
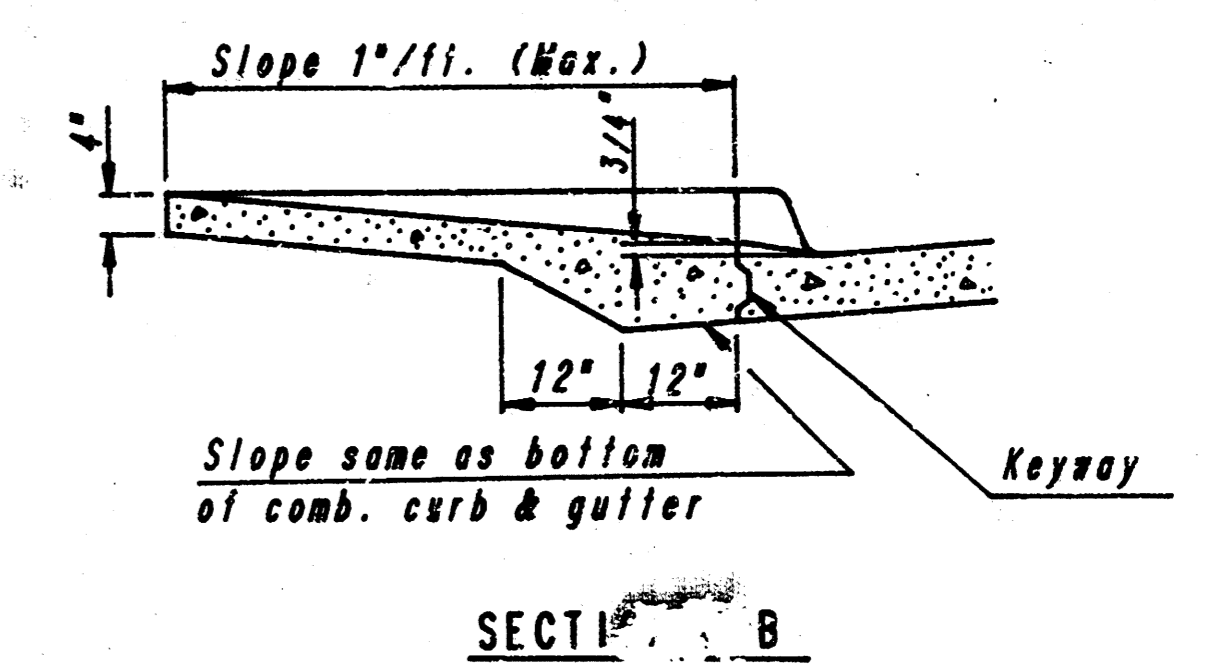
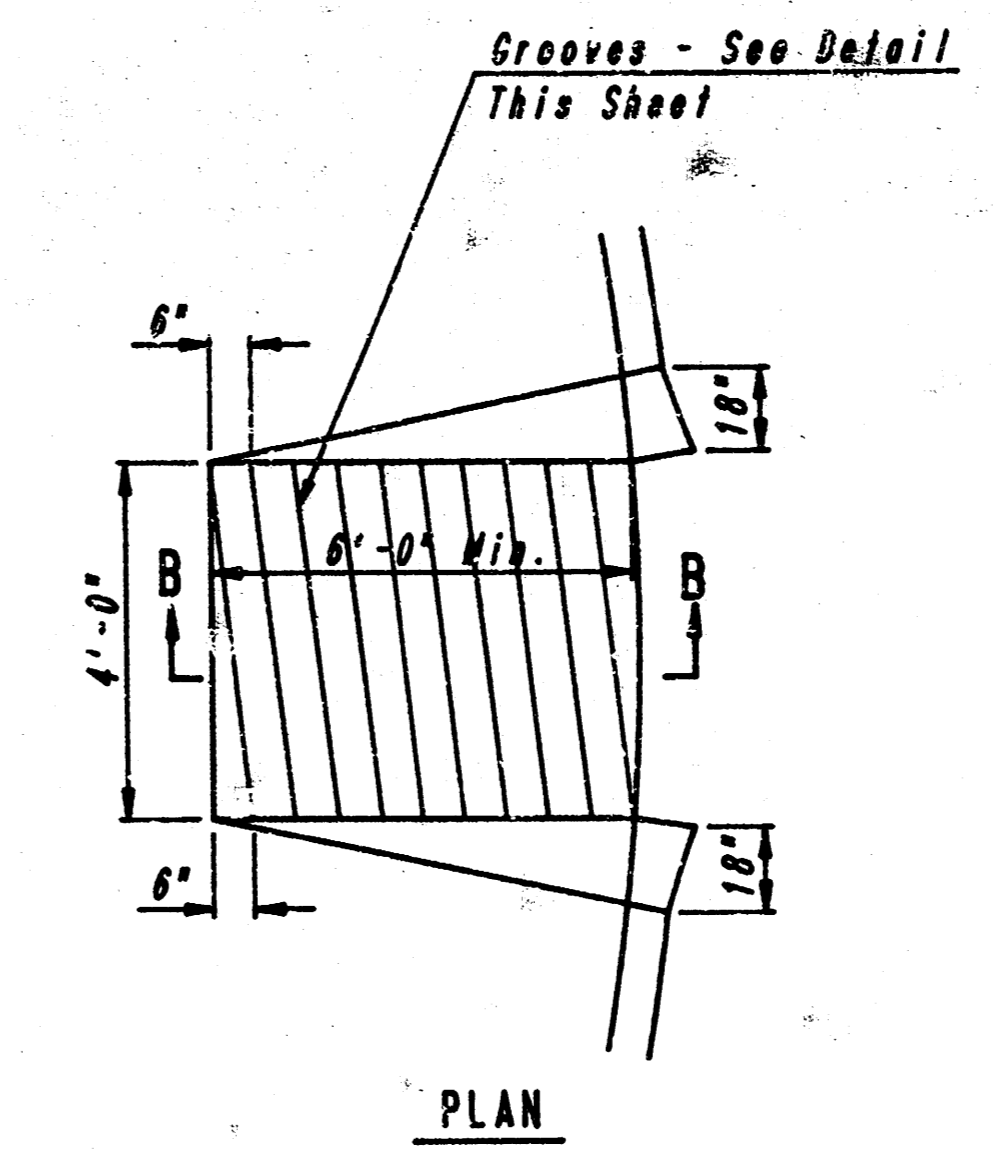
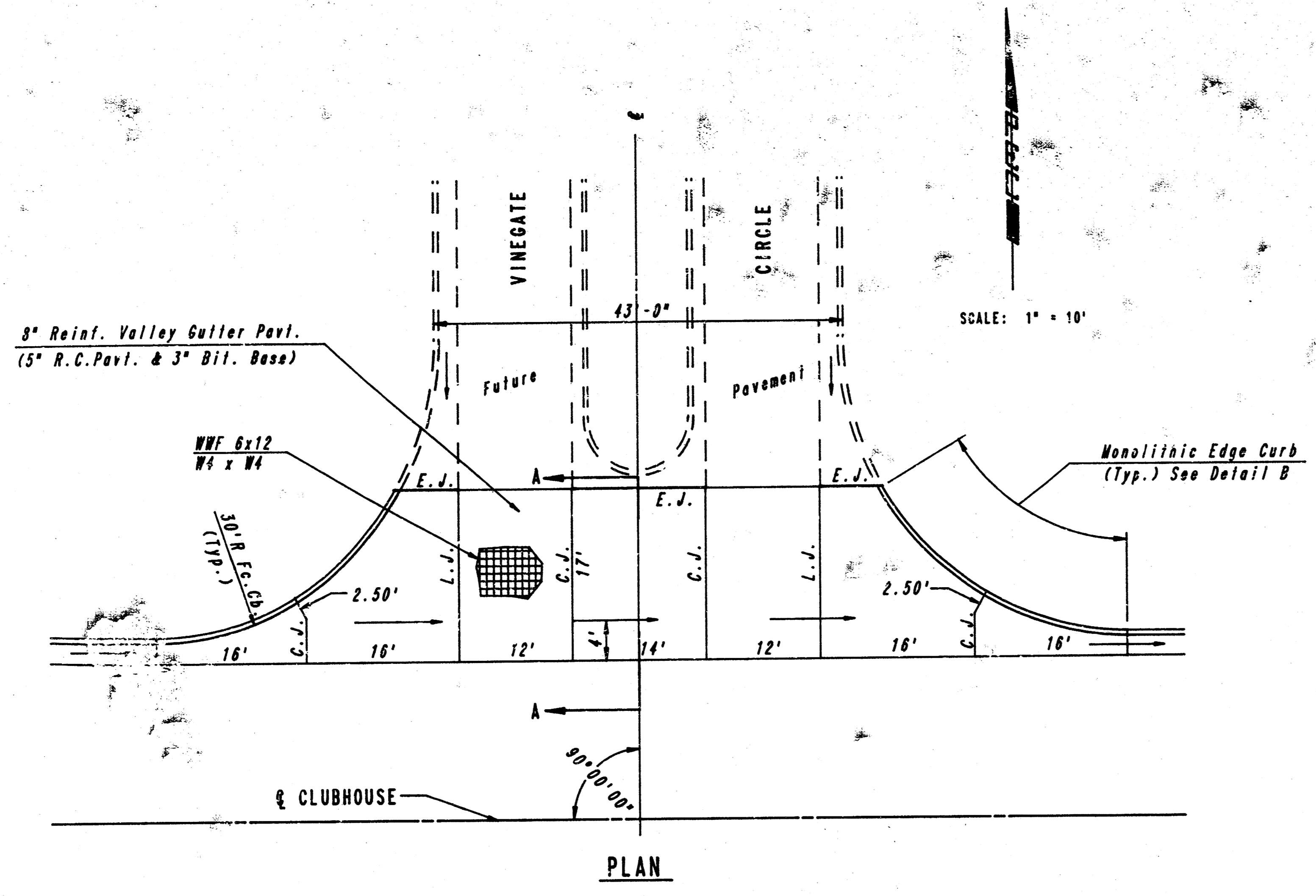
**CLUBHOUSE**  
STA. 11+15.84 TO STA. 13+10.88

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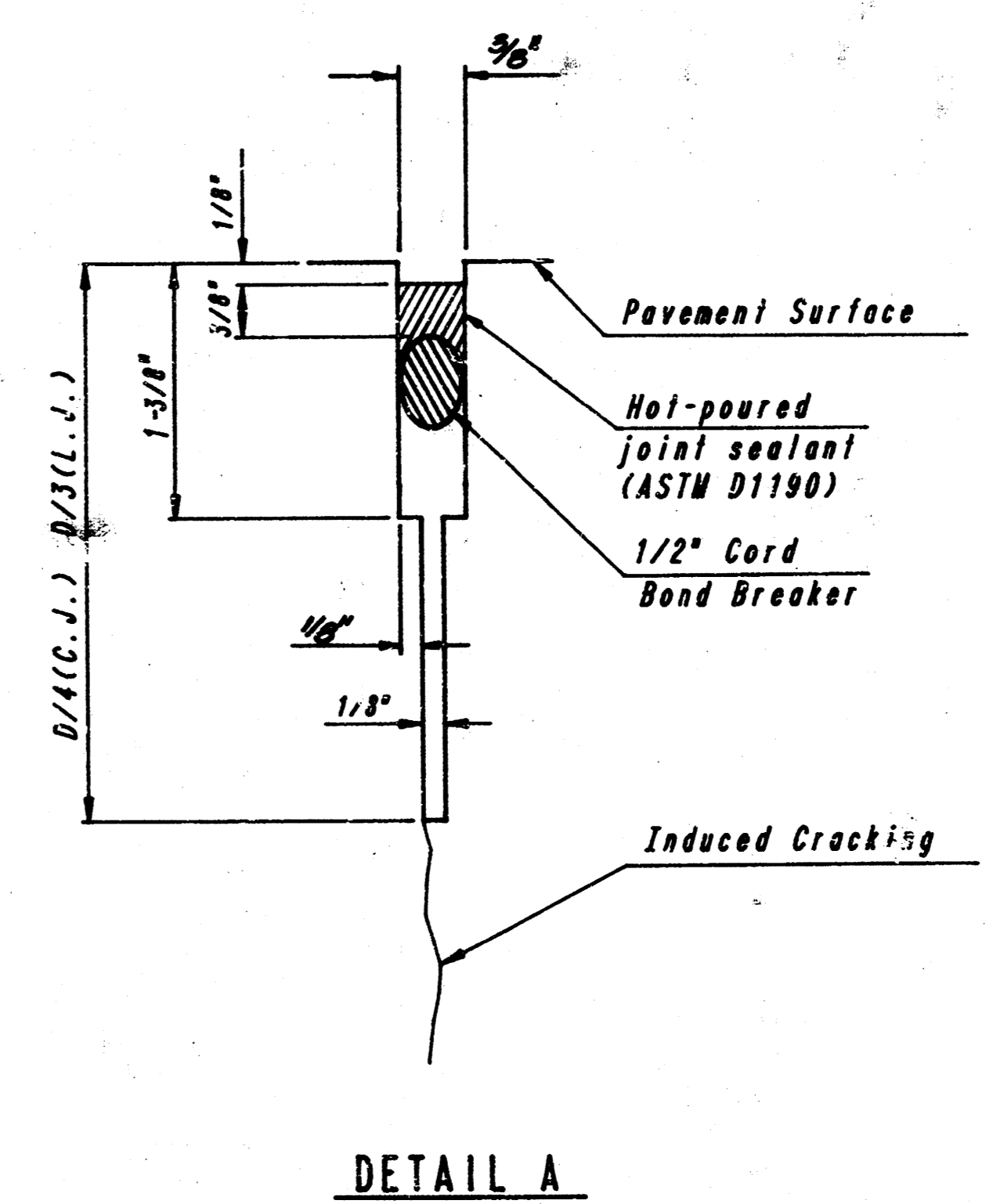
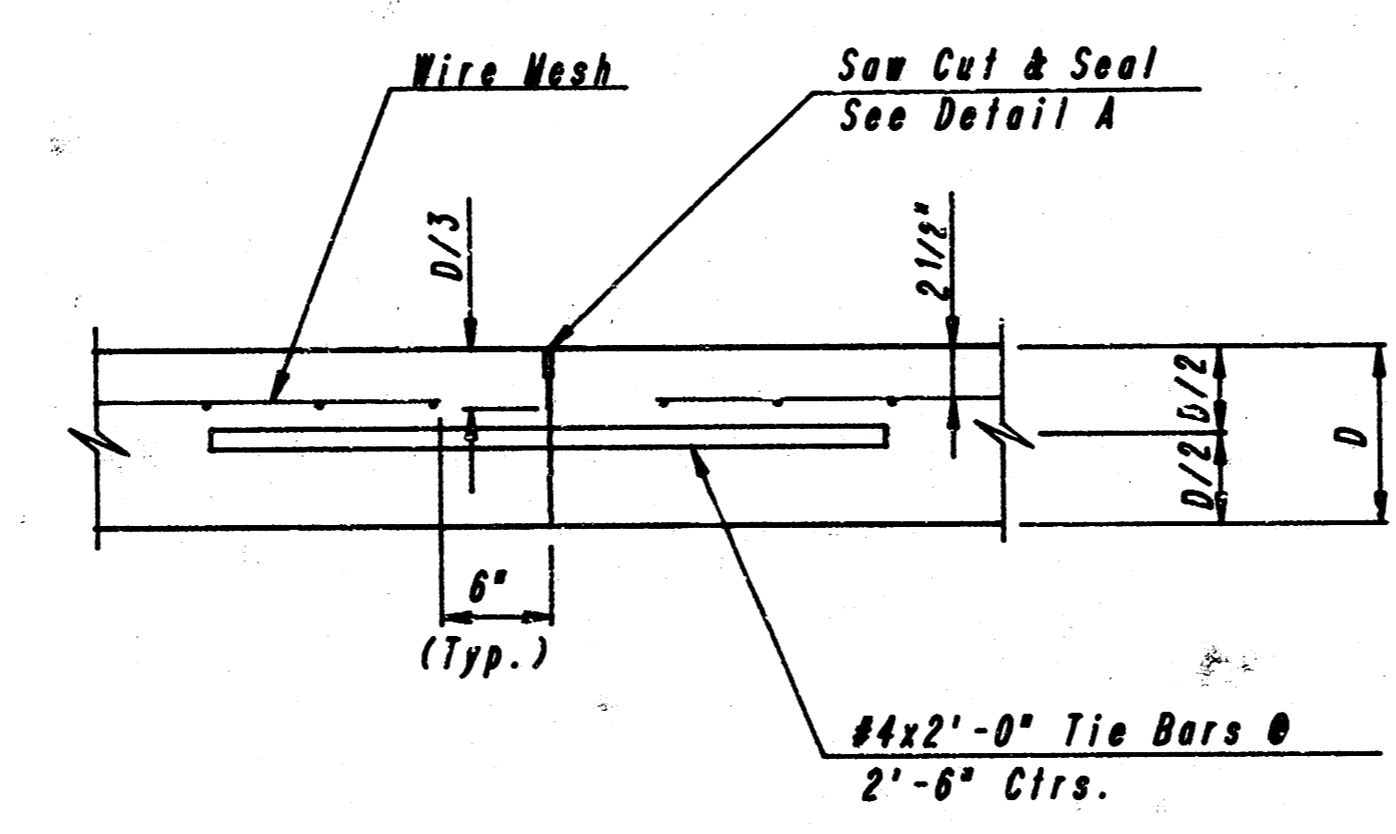
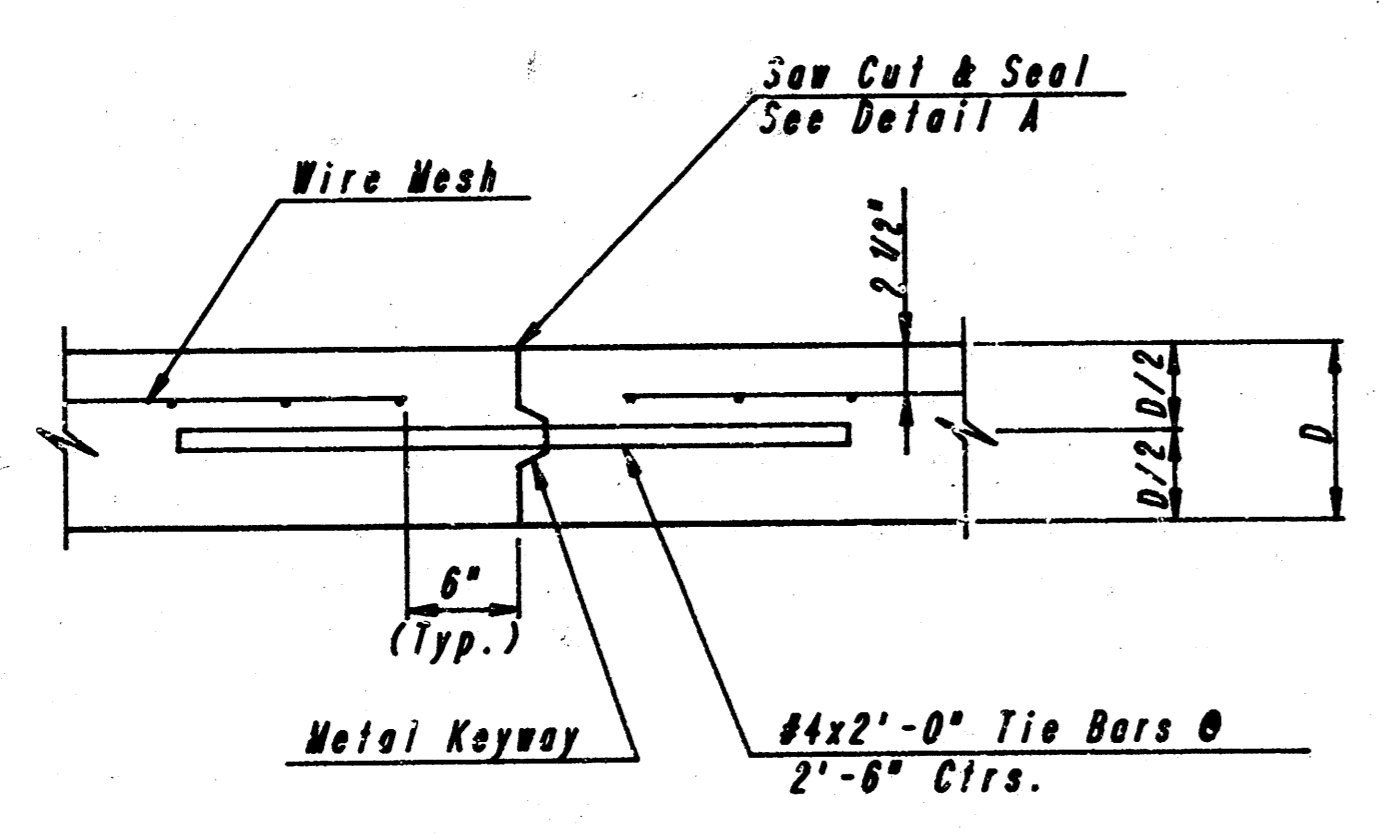
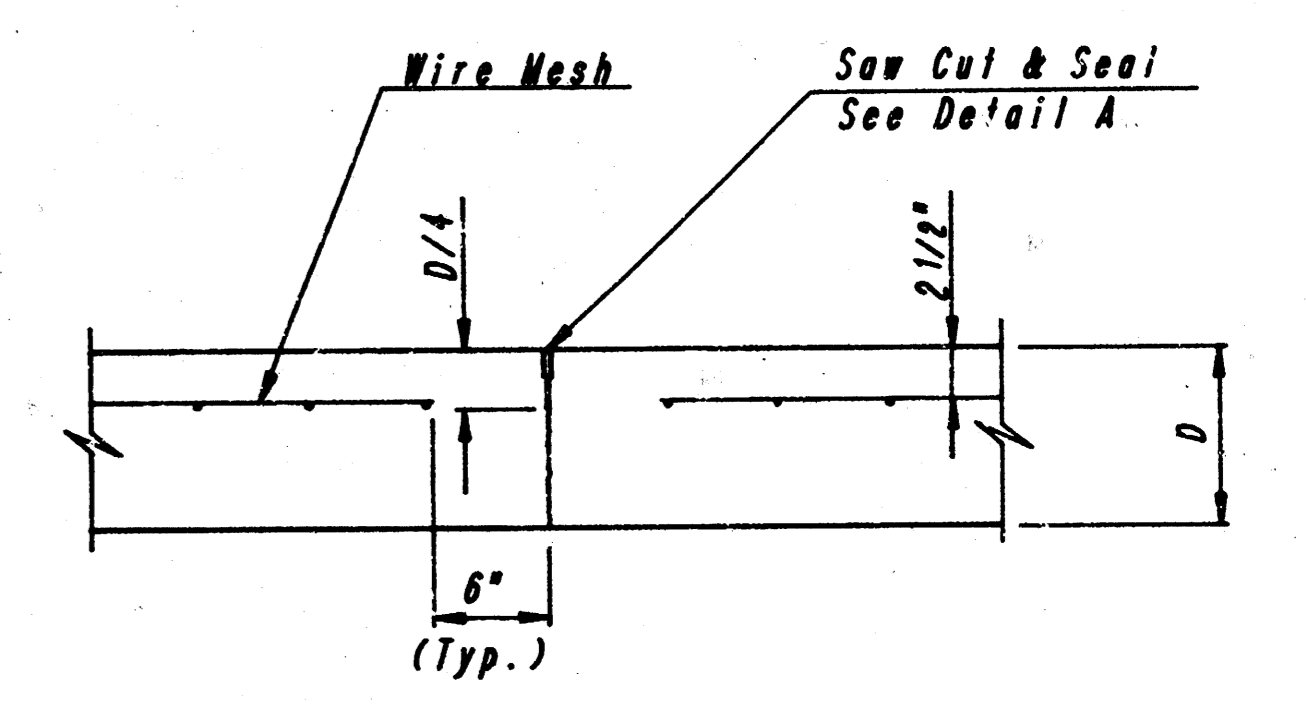
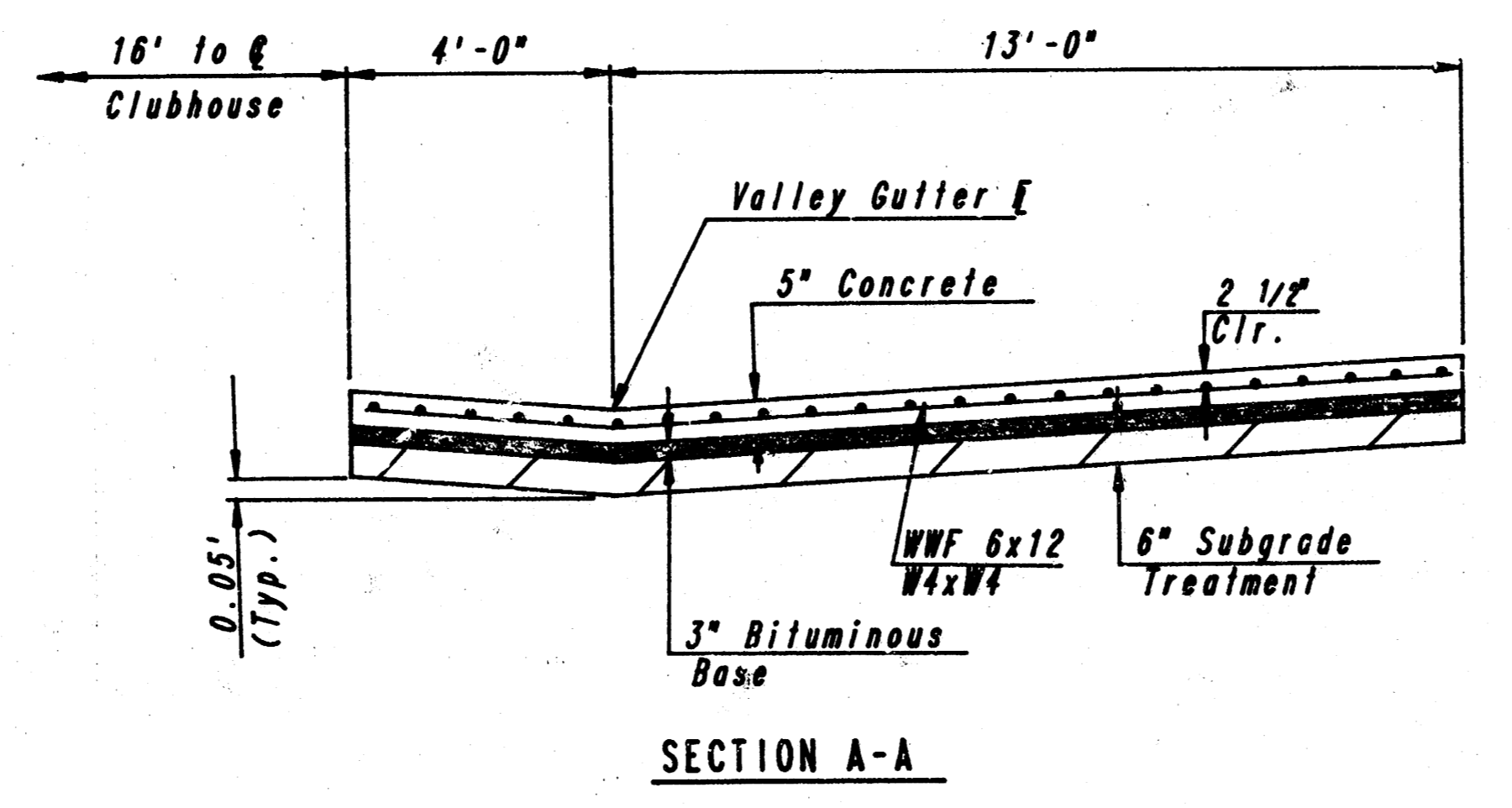
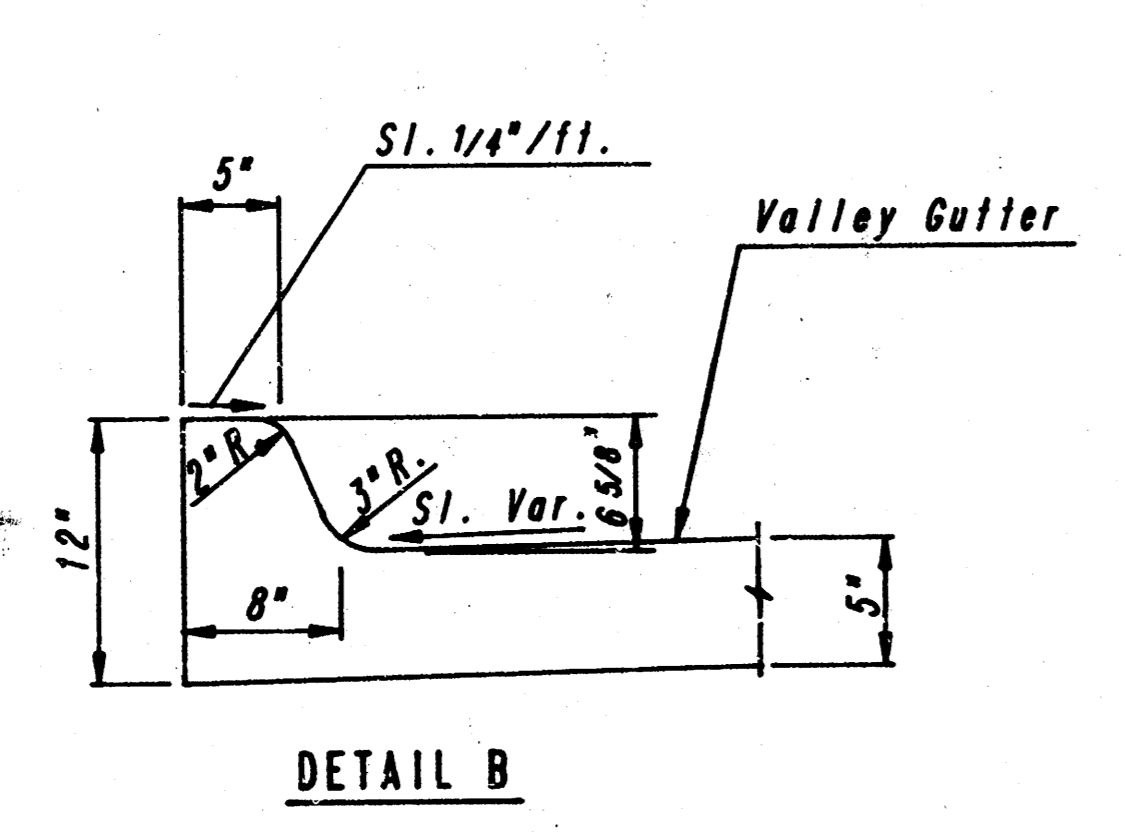
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Checked by: CDR, GDB, BDB  
Wichita, Kansas

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PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81826-000-001	9	14



TYPICAL WHEELCHAIR RAMP DETAILS



CONTRACTION JOINT DETAIL REINFORCED PAVEMENT (C.J.)

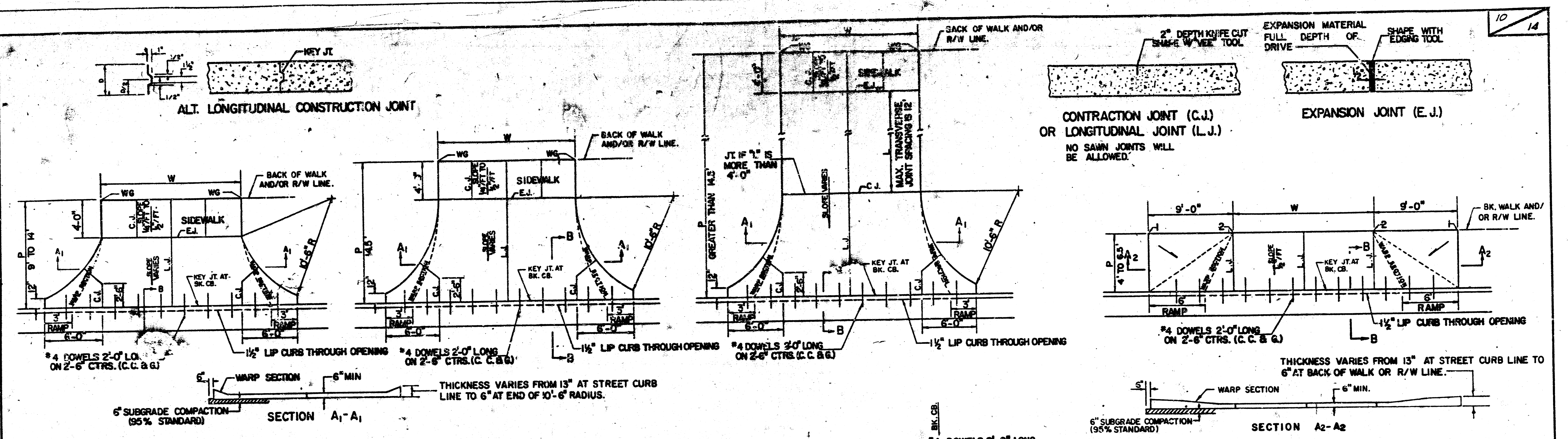
VALLEY GUTTER DETAILS & WHEELCHAIR RAMP DETAILS

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ENGINEERS  
WICHITA, KANSAS

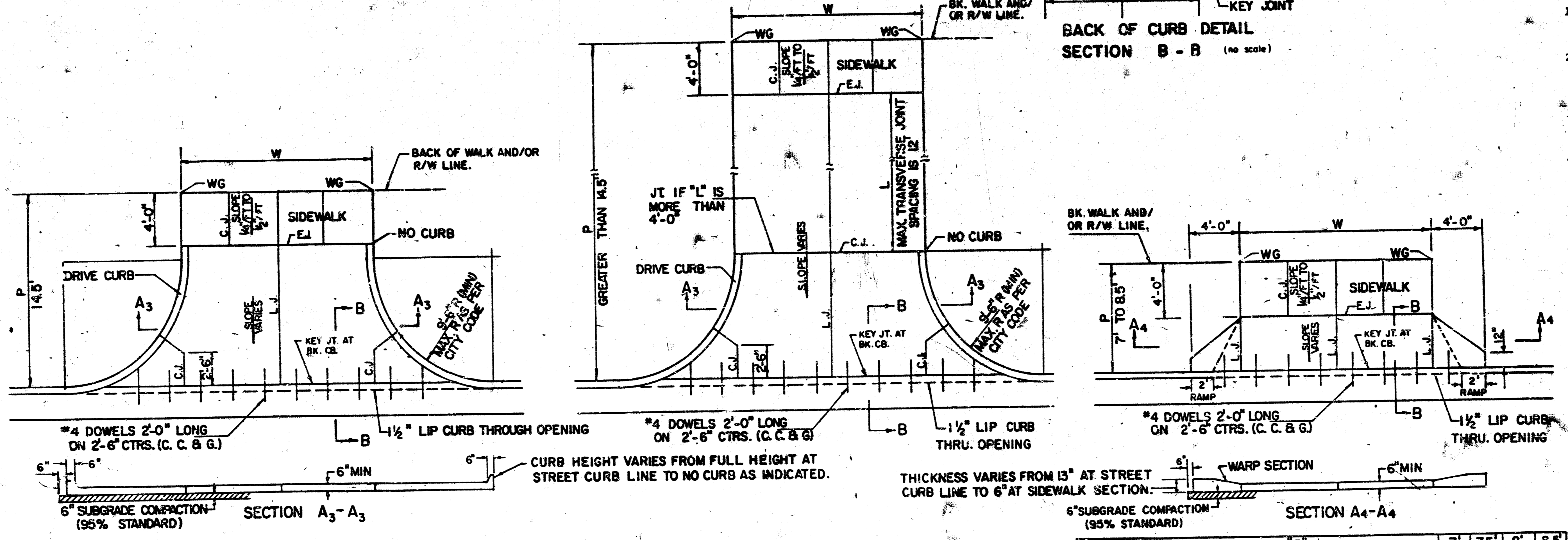
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Drawn by	DEP	Date	MAR., 1987

Job No. 86476-3

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**RADIUS RAMP DRIVES (P=9.0' & GREATER)**



**FULL RADIUS DRIVES (P=14.5' & GREATER)**

- GENERAL NOTES**
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH ROLL CURB AND/OR FULL WALK.
  - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "P" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACING NOT TO EXCEED 12' FOR DRIVES WITH A "P" DIMENSION GREATER THAN 24'.
  - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
  - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
  - DOSEL BARS SHALL BE OMITTED FROM THE KEVED CONSTRUCTION JOINT ALONG THE BACK OF THE 5'-6" CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
  - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
  - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
  - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" #4-#4 WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONFORMANCE.
  - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE GUTTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.

REVISED OCTOBER 1985  
SCALE: 1"=5'

PARKING WIDTH "P"	7'	7.5'	8'	8.5'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00	0.10	0.20	0.30
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00	0.10	0.20	0.30
OPTIMUM MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-15"	-16"	-17"	-17"
ABSOLUTE MAX. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-25"	-20"	-20"	-20"

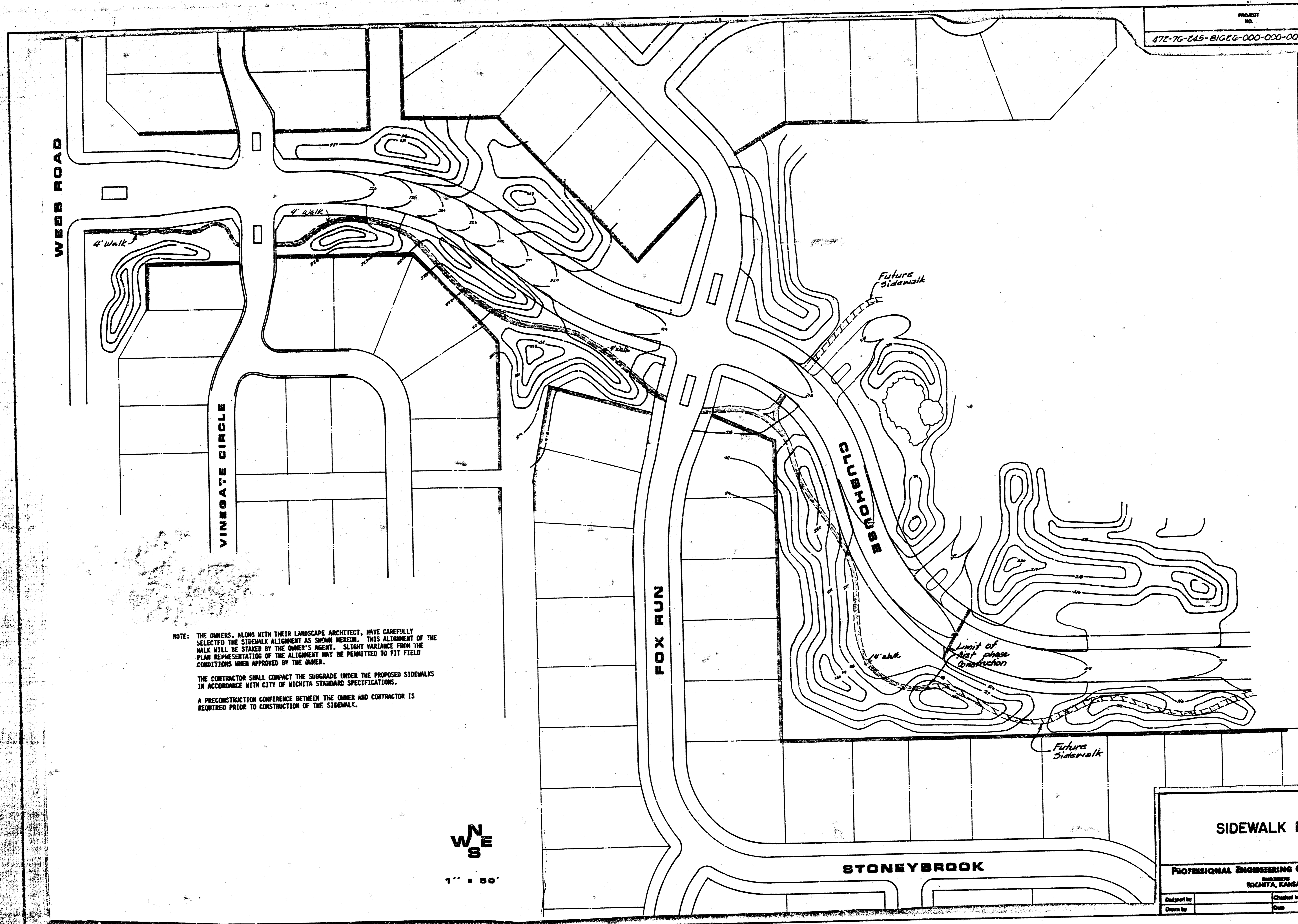
**FULL RAMP DRIVE (P=7.0' TO 8.5')**

**STANDARD DRIVE ENTRANCES**  
**FULL HEIGHT CURB**  
CITY OF WICHITA, KANSAS

PROJECT NUMBER  
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47E-76-245-BIGEG-000-000-001	11	14



NOTE: THE OWNERS, ALONG WITH THEIR LANDSCAPE ARCHITECT, HAVE CAREFULLY SELECTED THE SIDEWALK ALIGNMENT AS SHOWN HEREON. THIS ALIGNMENT OF THE WALK WILL BE STAKED BY THE OWNER'S AGENT. SLIGHT VARIANCE FROM THE PLAN REPRESENTATION OF THE ALIGNMENT MAY BE PERMITTED TO FIT FIELD CONDITIONS WHEN APPROVED BY THE OWNER.

THE CONTRACTOR SHALL COMPACT THE SUBGRADE UNDER THE PROPOSED SIDEWALKS IN ACCORDANCE WITH CITY OF WICHITA STANDARD SPECIFICATIONS.

A PRECONSTRUCTION CONFERENCE BETWEEN THE OWNER AND CONTRACTOR IS REQUIRED PRIOR TO CONSTRUCTION OF THE SIDEWALK.

WN  
SE  
1" = 50'

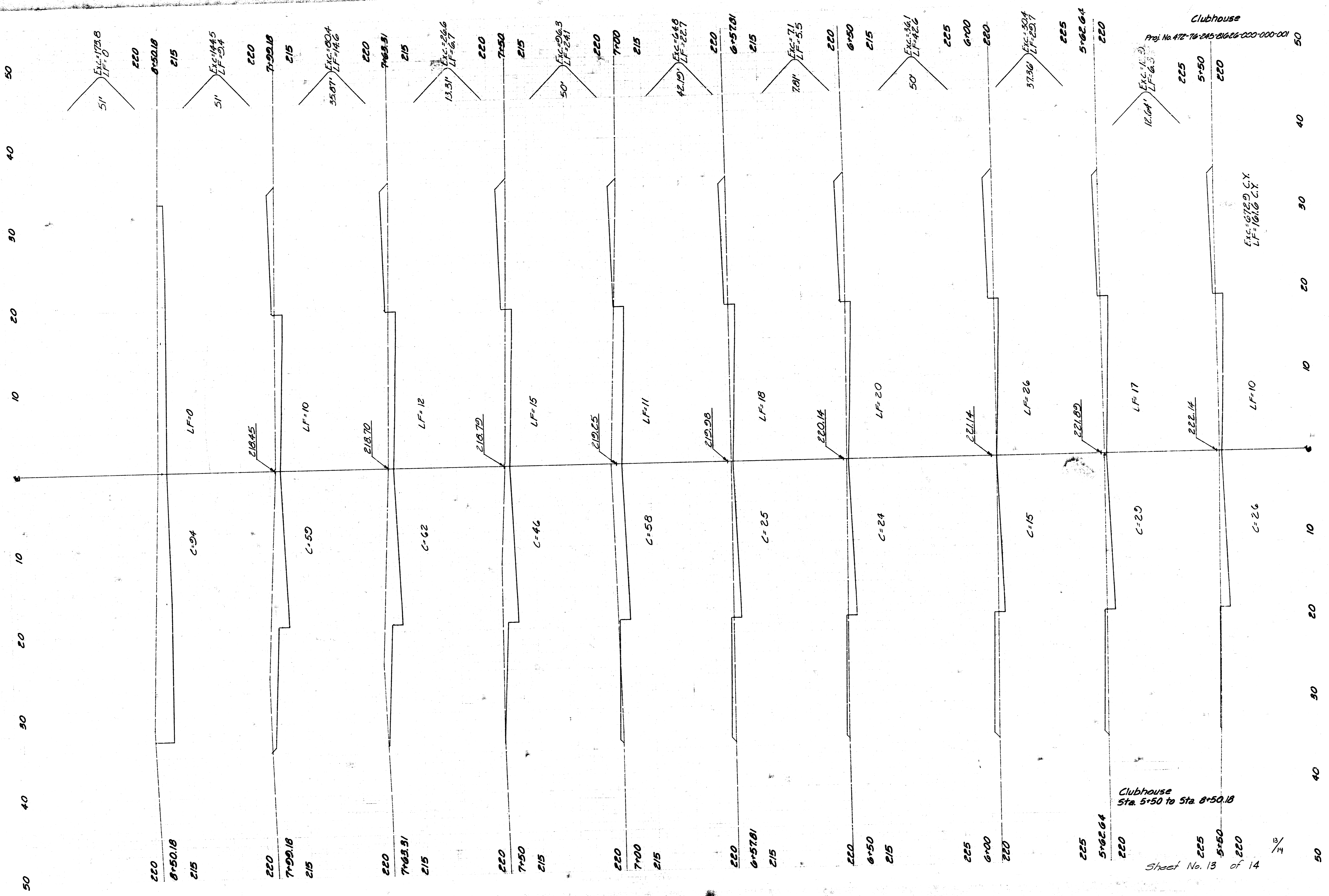
**SIDEWALK PLAN**

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
WICHITA, KANSAS

Designed by	Checked by	Job No. 66476-3
Drawn by	Date	

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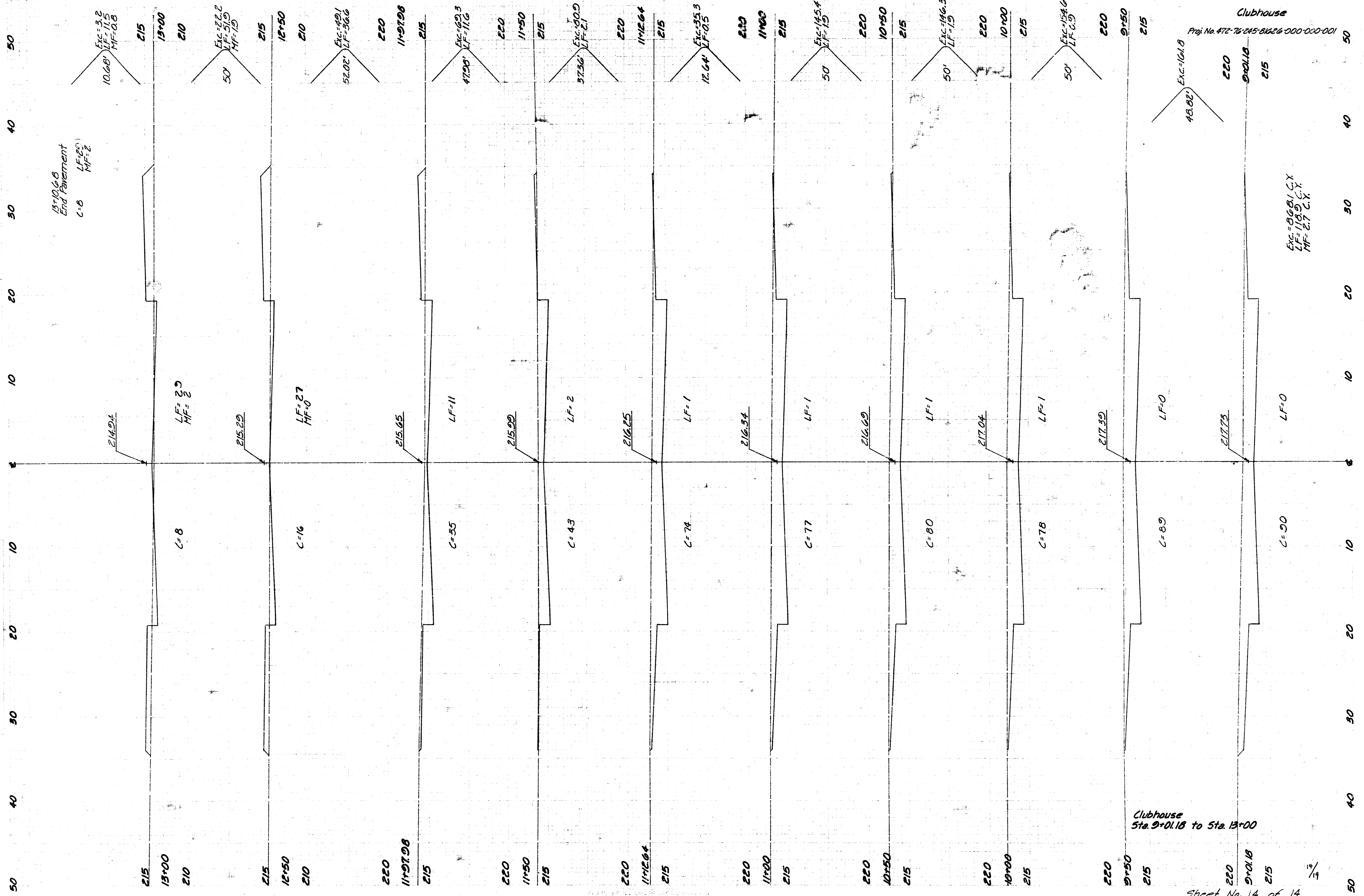
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Sheet No. 13 of 14

Clubhouse Sta. 5+50 to Sta. 8+50.18

Clubhouse

Proj. No. 472-76-245-01626-000-001



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