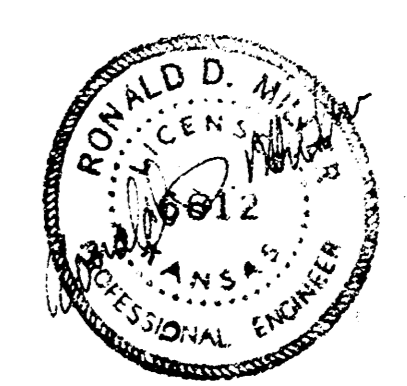
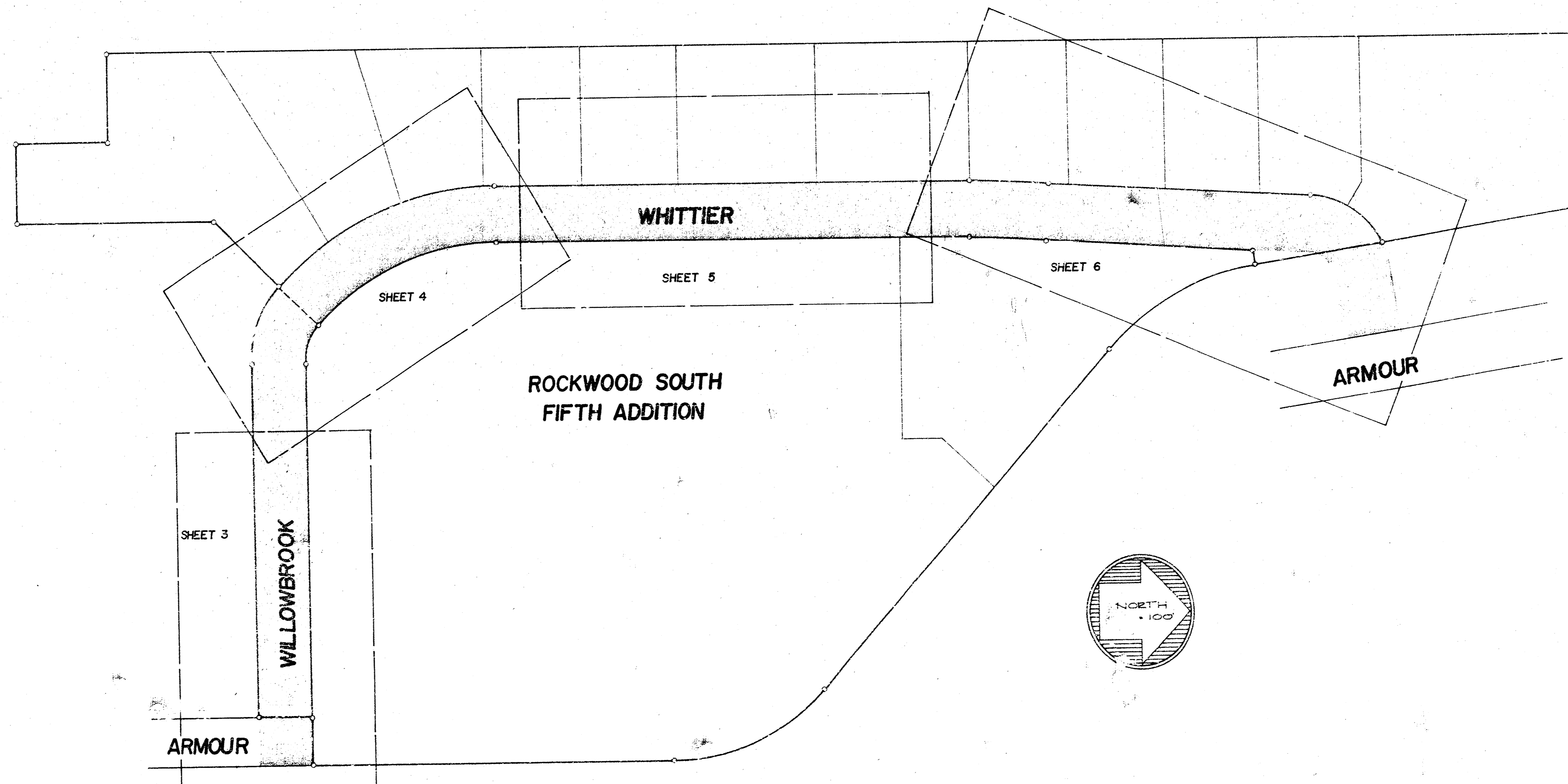
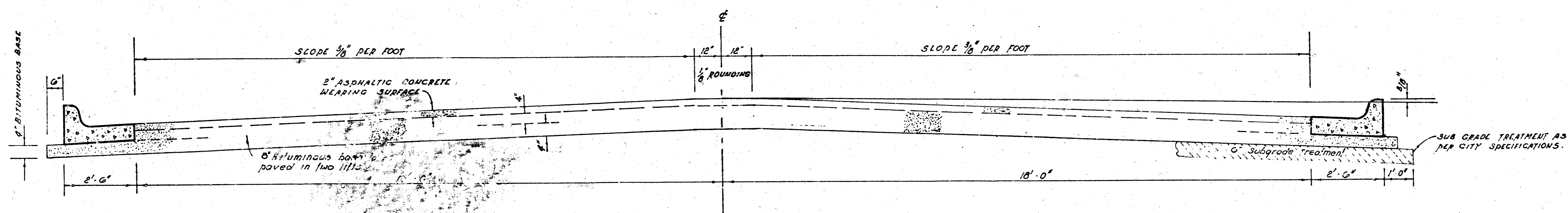


WILLOWBROOK St.-E.L.: Armour - S.L.: Whittier  
WHITTIER St.- N.L. Willowbrook - W.L.: Armour

PROJECT NO. 472-76-245-80858-000-000-001  
CITY OF WICHITA, KANSAS  
DEPT. OF PUBLIC WORKS-ENG. DIV.  
DEAN SELLERS, ACTING CITY ENGINEER  
BY LAW/KINGDON, P.A., WICHITA, KANSAS  
DATE \_\_\_\_\_



WILLOWBROOK ST. E.L. ARMOUR TO S.L. WHITTIER  
 WHITTIER ST N.L. WILLOWBROOK TO W.L. ARMOUR  
 ROCKWOOD SOUTH 5<sup>TH</sup> ADDATION  
 PROJECT NO. 472-76-245-80858-000-000-001



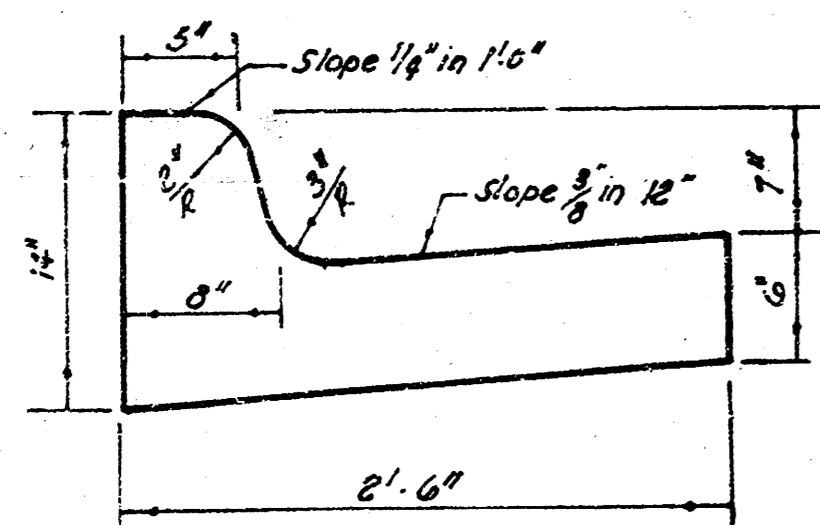
TYPICAL SECTION

The A.C. pavement between the curb & gutter shall be paid as 39 yds 10' A.C. pavement (8' Bituminous base). The Bituminous base under the curb & gutter shall be paid as 39 yds 9' Bituminous base.

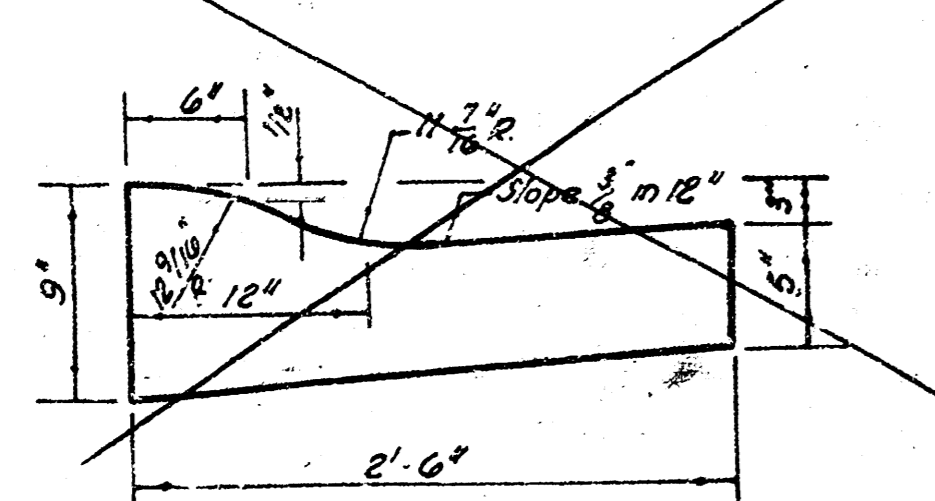
4' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

A TACK COAT OF EMULSIFIED ASPHALT (SS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE 0.05 GALLONS PER SQ YD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT. BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.

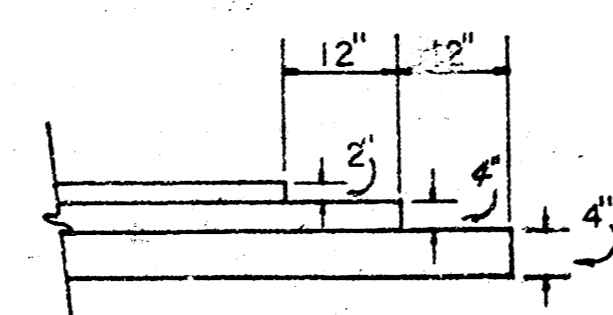
COMBINED CURB & GUTTER



ROLL TYPE CURB & GUTTER



DETAIL OF TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF ASPHALTIC CONCRETE PAVEMENT.

CITY OF WICHITA, KANSAS

DEPARTMENT of PUBLIC WORKS - Engineering Division.

DEAN SELLERS ACTING CITY ENGINEER

DATE \_\_\_\_\_

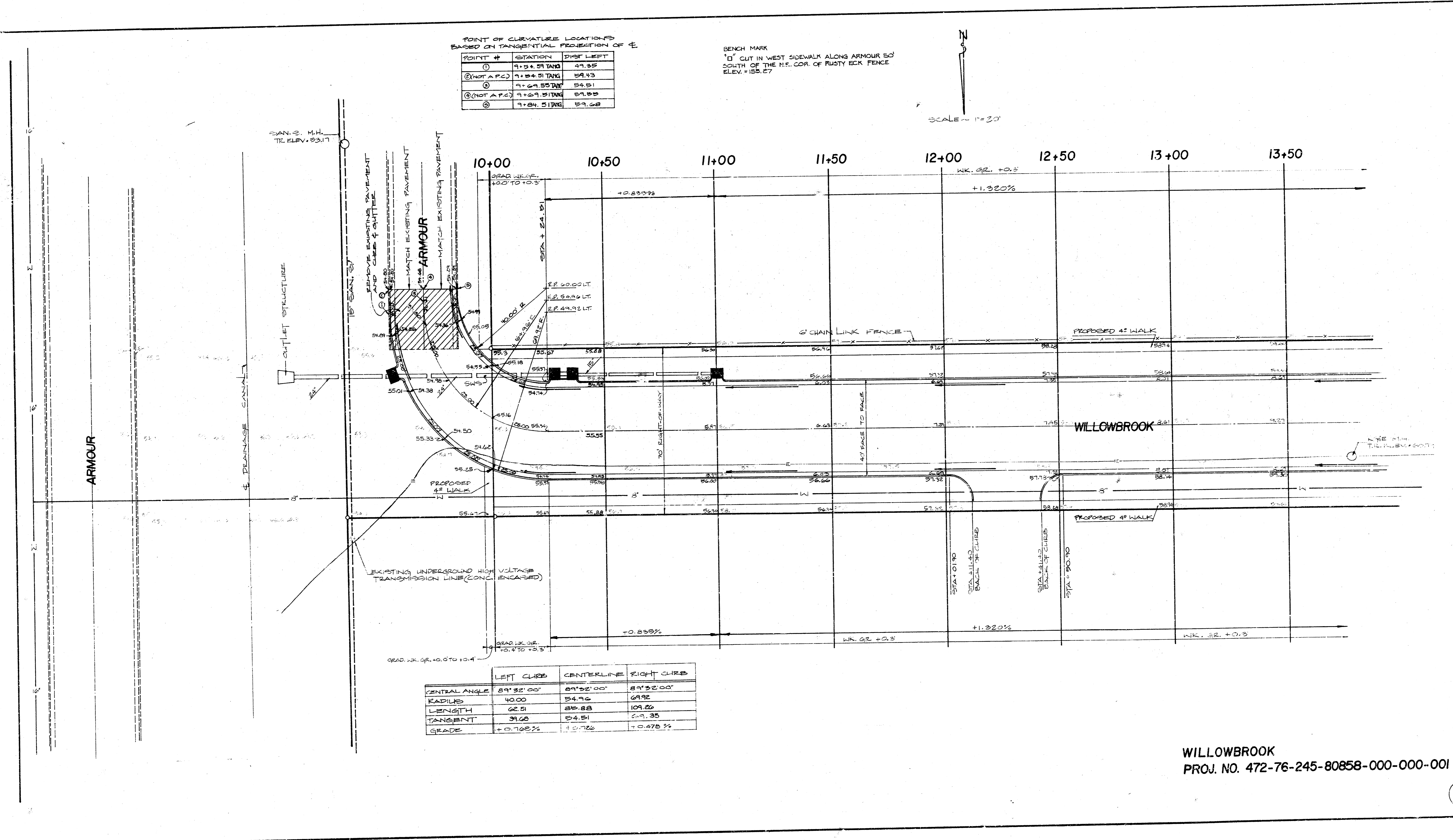
PROJ. NO. 472-76-245-80858-000-000-001

POINT OF CURVATURE LOCATIONS  
BASED ON TANGENTIAL PROJECTION OF E

POINT #	STATION	DIST LEFT
①	9+34.59 TANG	47.35
② (NOT A P.C.)	9+34.59 TANG	54.43
③	9+29.55 TANG	54.51
④ (NOT A P.C.)	9+29.55 TANG	54.55
⑤	9+24.51 TANG	54.62

BENCH MARK  
"D" CUT IN WEST SIDEWALK ALONG ARMOUR 50'  
SOUTH OF THE H.F. COR. OF RUSTY ECK FENCE  
ELEV. +155.27

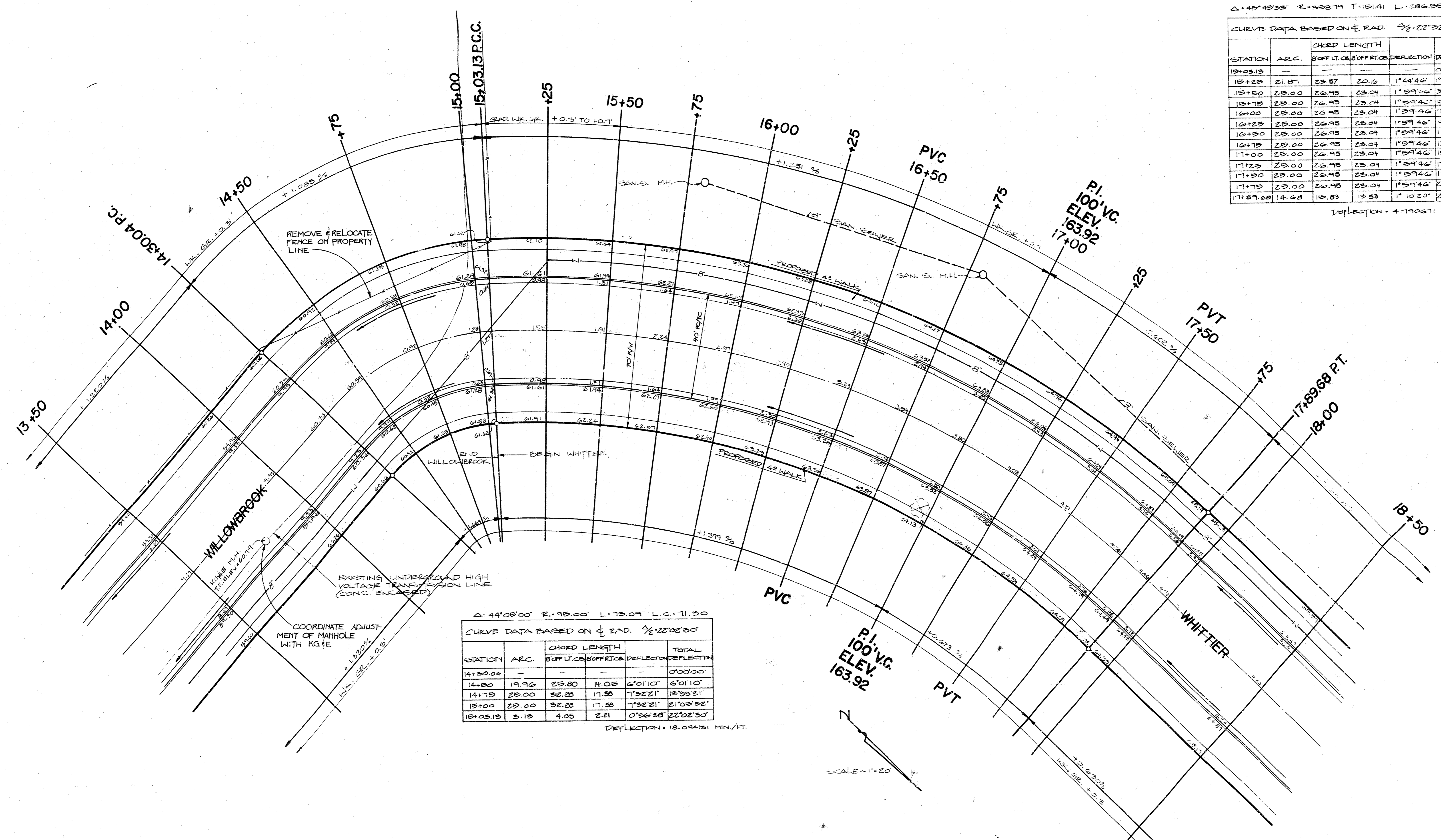
SCALE = 1" = 20'



	LEFT CURB	CENTERLINE	RIGHT CURB
CENTRAL ANGLE	89°32'00"	89°32'00"	89°32'00"
RADIUS	40.00	54.76	69.92
LENGTH	62.51	85.88	109.26
TANGENT	31.60	54.51	54.35
GRADE	+0.705%	+0.712%	+0.475%

WILLOWBROOK  
PROJ. NO. 472-76-245-80858-000-000-001

1/6



△: 49°45'33" R: 95.874 T: 101.41 L: 286.55 L.C.: 278.77

CURVE DATA BASED ON R. RAD. 95.22'02"46"

STATION	ARC	CHORD LENGTH	OFF SET	OFF SET	DEFLECTION	TOTAL DEFLECTION
13+50.13	---	---	---	---	---	0'00'00"
13+50	21.87	23.57	20.16	1'04'42"	1'04'42"	1'04'42"
13+50	23.00	24.95	23.04	1'09'42"	2'14'24"	2'14'24"
13+75	23.00	24.95	23.04	1'09'42"	3'23'66"	3'23'66"
13+75	23.00	24.95	23.04	1'09'42"	4'33'08"	4'33'08"
14+00	23.00	24.95	23.04	1'09'42"	5'42'50"	5'42'50"
14+25	23.00	24.95	23.04	1'09'42"	6'52'32"	6'52'32"
14+50	23.00	24.95	23.04	1'09'42"	8'02'14"	8'02'14"
14+75	23.00	24.95	23.04	1'09'42"	9'11'56"	9'11'56"
15+00	23.00	24.95	23.04	1'09'42"	10'21'38"	10'21'38"
15+25	23.00	24.95	23.04	1'09'42"	11'31'20"	11'31'20"
15+50	23.00	24.95	23.04	1'09'42"	12'41'02"	12'41'02"
15+75	23.00	24.95	23.04	1'09'42"	13'50'44"	13'50'44"
16+00	23.00	24.95	23.04	1'09'42"	15'00'26"	15'00'26"
16+25	23.00	24.95	23.04	1'09'42"	16'10'08"	16'10'08"
16+50	23.00	24.95	23.04	1'09'42"	17'19'50"	17'19'50"
16+75	23.00	24.95	23.04	1'09'42"	18'29'32"	18'29'32"
17+00	23.00	24.95	23.04	1'09'42"	19'39'14"	19'39'14"
17+25	23.00	24.95	23.04	1'09'42"	20'48'56"	20'48'56"
17+50	23.00	24.95	23.04	1'09'42"	21'58'38"	21'58'38"
17+75	23.00	24.95	23.04	1'09'42"	23'08'20"	23'08'20"
18+00	14.42	16.83	13.93	1'10'00"	24'18'02"	24'18'02"

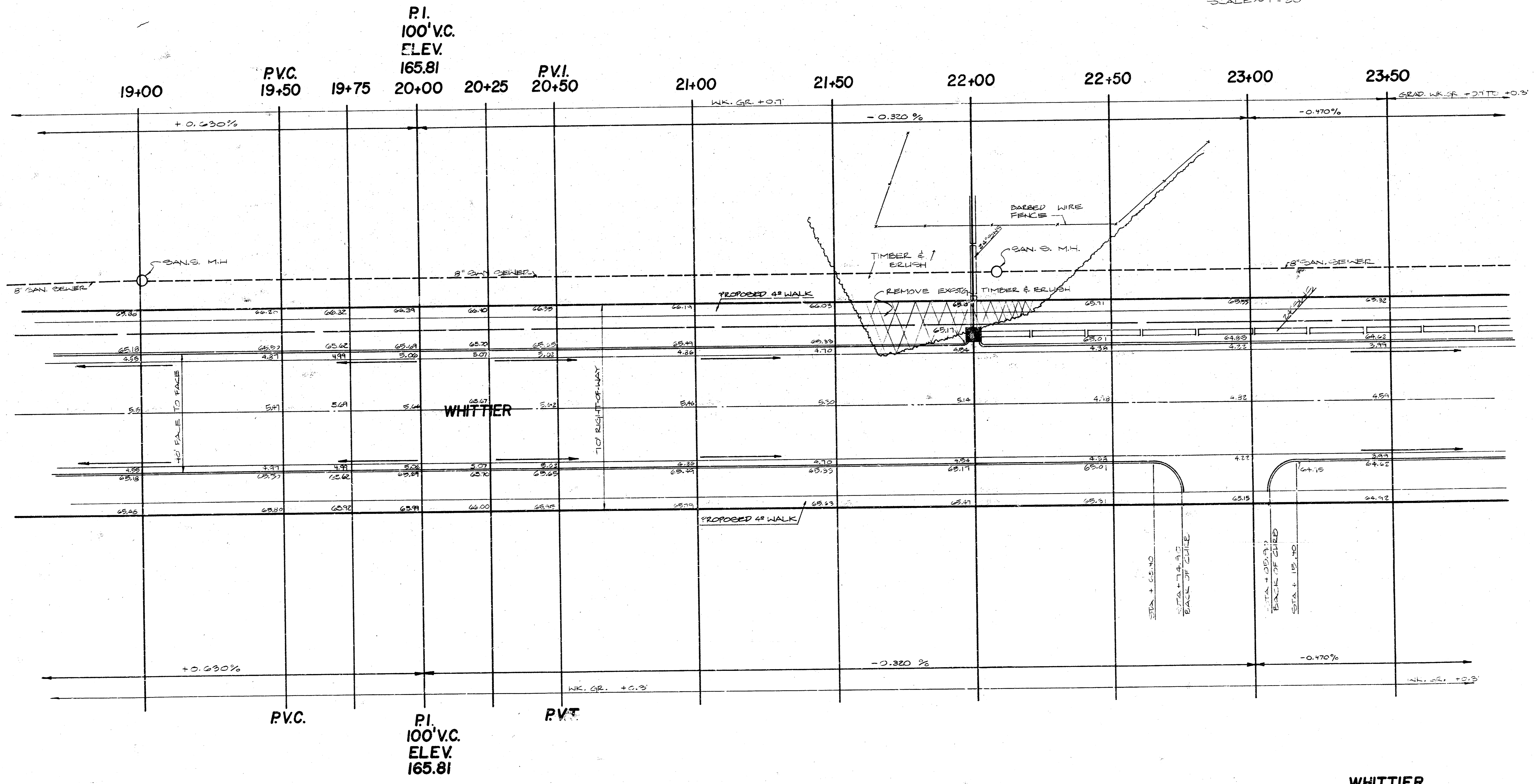
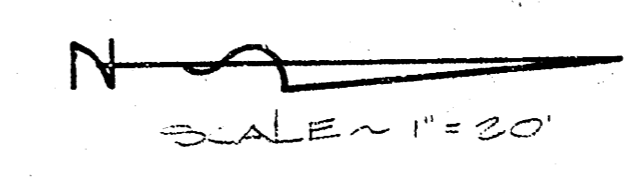
DEFLECTION = 1'10'00" MIN./FT.

△: 44°08'00" R: 95.00 L: 73.09 L.C.: 71.30

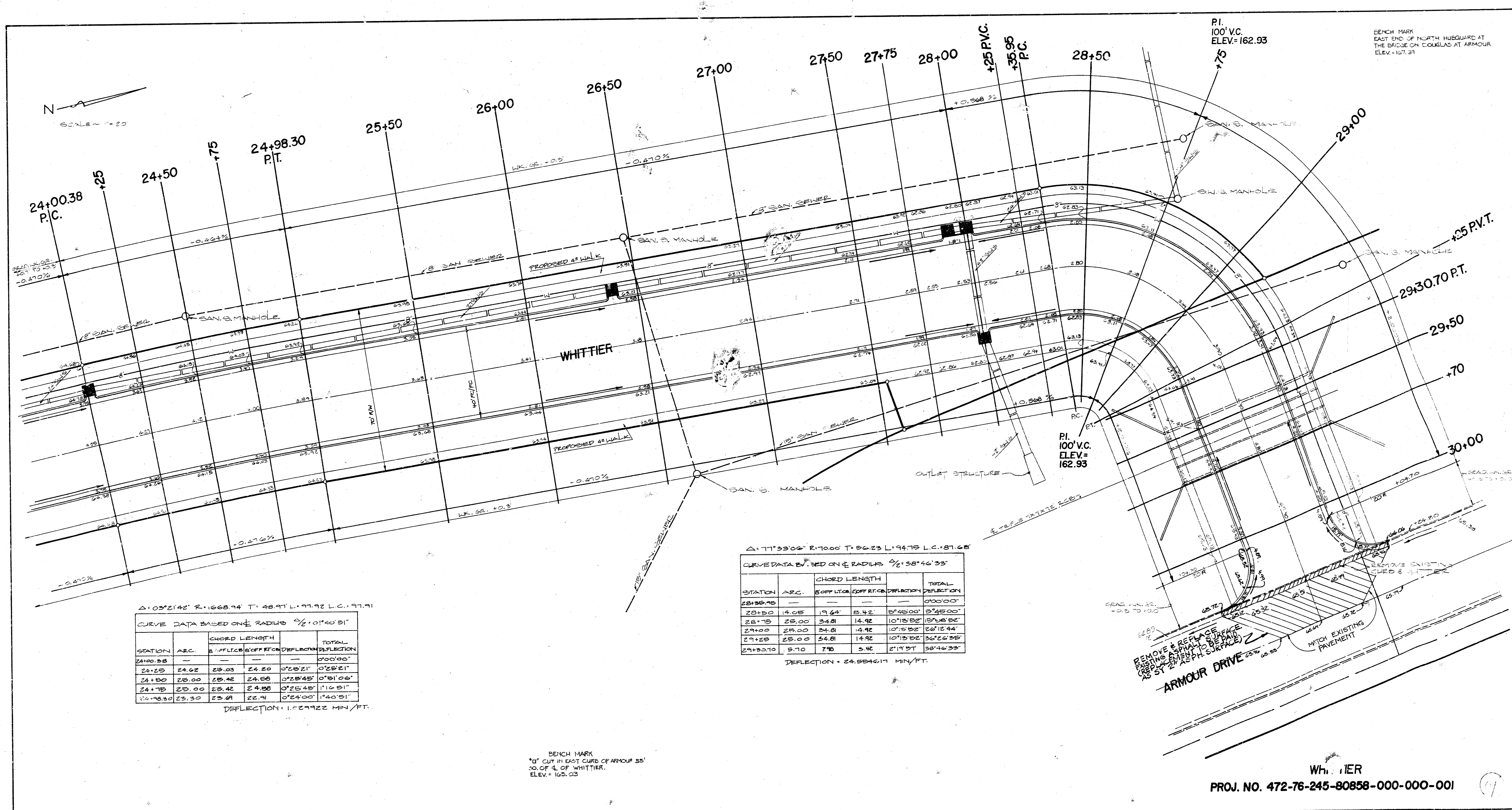
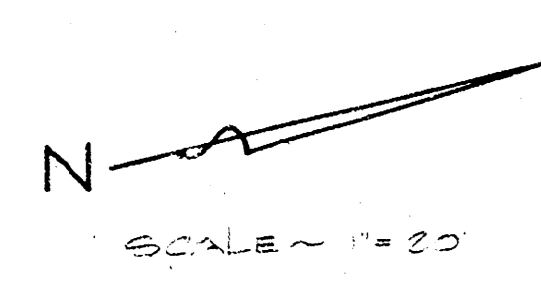
CURVE DATA BASED ON R. RAD. 95.22'02"30"

STATION	ARC	CHORD LENGTH	OFF SET	OFF SET	DEFLECTION	TOTAL DEFLECTION
14+00.04	---	---	---	---	---	0'00'00"
14+00	19.96	23.80	14.08	6'01'10"	6'01'10"	6'01'10"
14+25	23.00	24.95	17.50	7'02'21"	13'03'31"	13'03'31"
14+50	23.00	24.95	17.50	7'02'21"	20'05'52"	20'05'52"
14+50.13	3.13	4.05	3.21	0'56'38"	21'02'30"	21'02'30"

DEFLECTION = 18.09'41" MIN./FT.



WHITTIER  
PROJ. NO. 472-76-245-80858-000-000-001



△: 103°21'42" R: 1668.74 T: 48.97 L: 97.92 L.C.: 197.91

CURVE DATA BASED ON RADIUS 1/2" 01'40" 81"

STATION	ARC	CHORD LENGTH	DEFLECTION	TOTAL DEFLECTION
24+00.58				0°00'00"
24+25	24.02	23.03	0°25'21"	0°25'21"
24+50	23.00	23.46	0°25'45"	0°51'06"
24+75	23.00	23.46	0°25'45"	1°16'51"
24+98.30	23.30	23.61	22.7'	0°54'00"

DEFLECTION = 1.62722 MIN/FT.

△: 77°33'06" R: 7000 T: 84.23 L: 94.75 L.C.: 87.68

CURVE DATA BASED ON RADIUS 1/2" 158'46" 35"

STATION	ARC	CHORD LENGTH	DEFLECTION	TOTAL DEFLECTION
28+50.90				0°00'00"
28+50	14.08	13.64	8.42	8°42'00"
28+75	23.00	24.81	14.92	10°13'52"
29+00	23.00	24.81	14.92	10°13'52"
29+25	23.00	24.81	14.92	10°13'52"
29+50.70	5.70	7.75	3.92	21°19'21"

DEFLECTION = 2.884611 MIN/FT.

BENCH MARK  
EAST END OF NORTH HUBGUARD AT  
THE BRIDGE ON DOUGLAS AT ARMOUR  
ELEV. = 167.31

BENCH MARK  
"0" CUT IN EAST CURB OF ARMOUR ST  
NO. OF 4 OF WHITTIER.  
ELEV. = 165.03

Whittier  
PROJ. NO. 472-76-245-80858-000-000-001