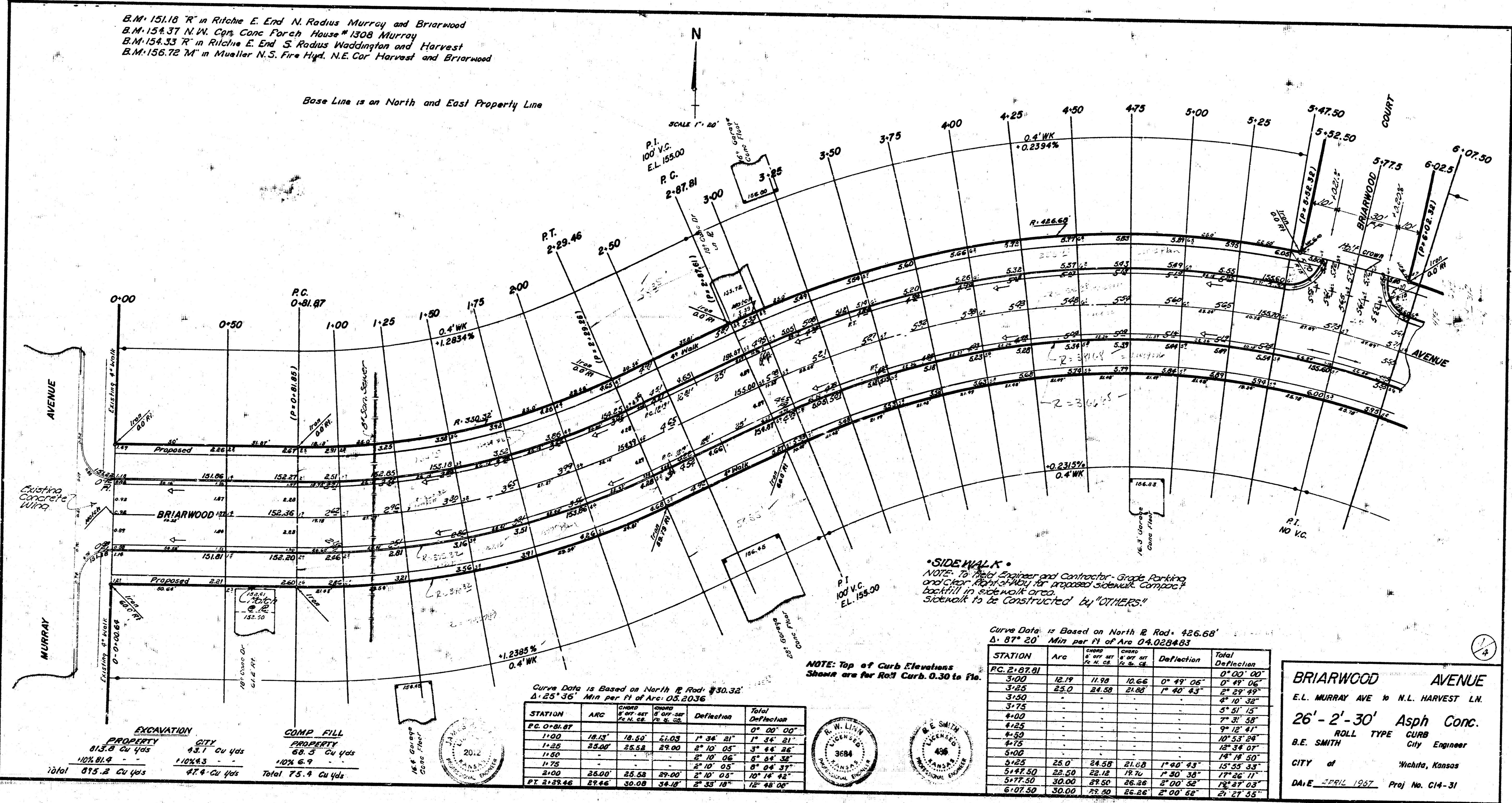


B.M. 151.18 'R' in Ritchie E. End N. Radius Murray and Briarwood
 B.M. 154.37 N.W. Cor. Conc. Foreh. House # 1308 Murray
 B.M. 154.33 'R' in Ritchie E. End S. Radius Waddington and Harvest
 B.M. 156.72 'M' in Mueller N.S. Fire Hyd. N.E. Cor. Harvest and Briarwood

Base Line is on North and East Property Line



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 B. E. SMITH
 City Engineer

EXCAVATION		CITY		COMP. FILL	
PROPERTY	CITY	PROPERTY	CITY	PROPERTY	CITY
813.8 Cu Yds	43.1 Cu Yds	68.3 Cu Yds	47.4 Cu Yds	75.9 Cu Yds	
102.814	110.943				
Total 815.8 Cu Yds					

Curve Data is Based on North & Rod # 30.32
 $\Delta = 25^{\circ} 36'$ Min per ft of Arc = 0.52036

STATION	ARC	CHORD	CHORD	DEFLECTION	TOTAL DEFLECTION
P.C. 0+81.87		10' W of 10'	10' W of 10'		
1+00	18.18	18.50	21.03	1° 36' 21"	0° 00' 00"
1+25	33.00	35.58	29.00	2° 10' 05"	1° 36' 21"
1+50	-	-	-	2° 10' 05"	2° 56' 26"
1+75	-	-	-	2° 10' 05"	4° 06' 31"
2+00	35.00	35.58	29.00	2° 10' 05"	6° 16' 36"
P.T. 2+29.46	27.46	30.08	34.17	2° 33' 18"	8° 49' 54"

NOTE: Top of Curb Elevations Shown are for Road Curb 0.30 to ft.



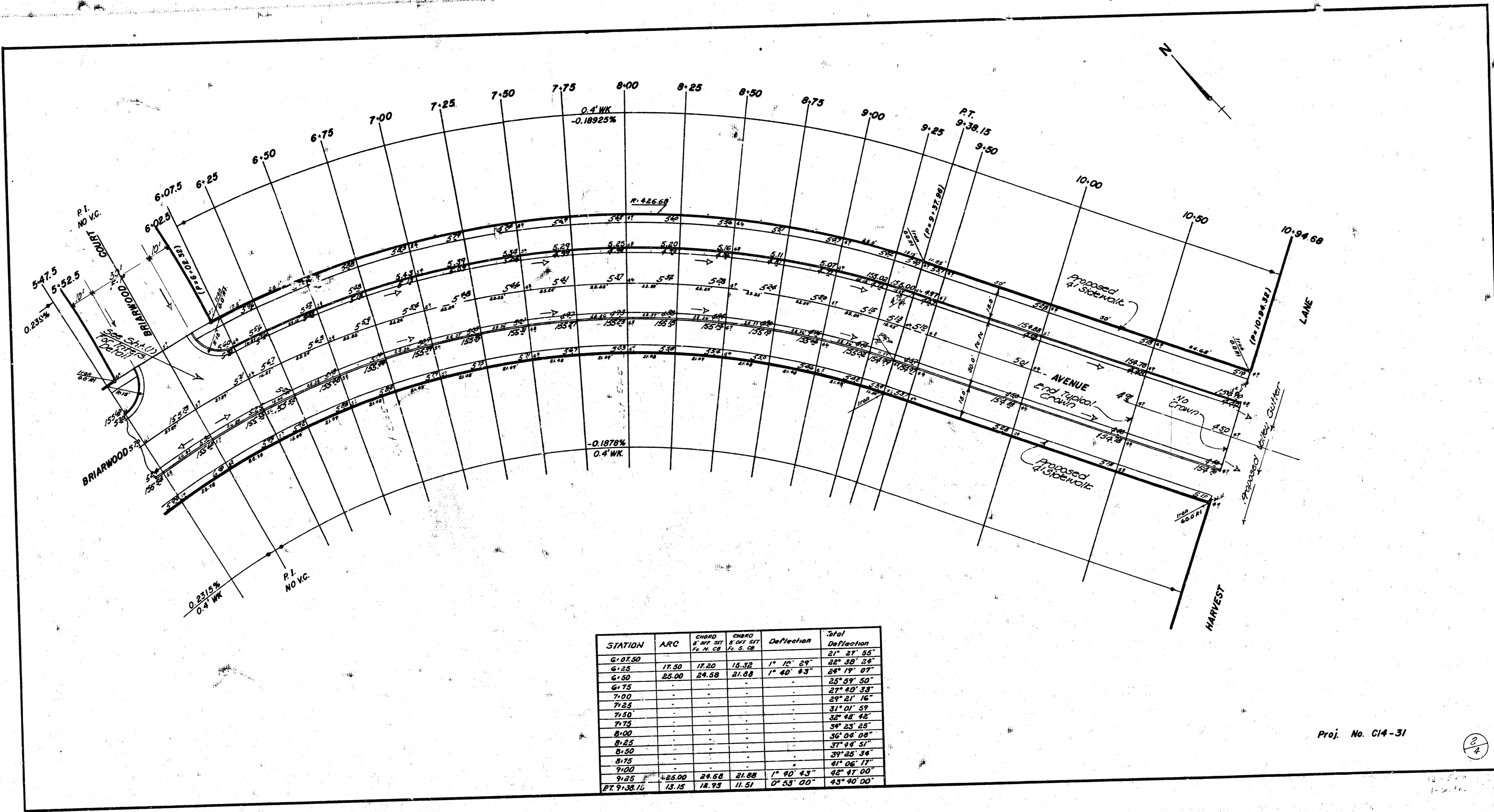
SIDEWALK
 NOTE: To Field Engineer and Contractor - Grade Parking and Curb - Right of Way for proposed sidewalk. Compact backfill in sidewalk area. Sidewalk to be constructed by "OTHERS".

Curve Data is Based on North & Rod # 426.68
 $\Delta = 87^{\circ} 20'$ Min per ft of Arc = 0.4028483

STATION	ARC	CHORD	CHORD	DEFLECTION	TOTAL DEFLECTION
P.C. 2+87.81		10' W of 10'	10' W of 10'		
3+00	12.19	11.90	10.66	0° 49' 06"	0° 00' 00"
3+25	25.0	24.58	21.08	1° 40' 43"	0° 49' 06"
3+50	-	-	-	-	1° 40' 43"
3+75	-	-	-	-	2° 51' 26"
4+00	-	-	-	-	4° 02' 09"
4+25	-	-	-	-	5° 12' 52"
4+50	25.0	24.58	21.08	1° 40' 43"	6° 23' 35"
4+75	30.00	28.50	26.26	1° 50' 30"	7° 34' 05"
5+00	30.00	28.50	26.26	1° 50' 30"	8° 44' 35"
5+25	30.00	28.50	26.26	1° 50' 30"	9° 55' 05"
5+50	30.00	28.50	26.26	1° 50' 30"	11° 05' 35"
5+75	30.00	28.50	26.26	1° 50' 30"	12° 16' 05"
6+00	30.00	28.50	26.26	1° 50' 30"	13° 26' 35"
6+25	30.00	28.50	26.26	1° 50' 30"	14° 37' 05"
6+50	30.00	28.50	26.26	1° 50' 30"	15° 47' 35"
6+75	30.00	28.50	26.26	1° 50' 30"	16° 58' 05"
7+00	30.00	28.50	26.26	1° 50' 30"	18° 08' 35"
7+25	30.00	28.50	26.26	1° 50' 30"	19° 19' 05"
7+50	30.00	28.50	26.26	1° 50' 30"	20° 29' 35"
7+75	30.00	28.50	26.26	1° 50' 30"	21° 40' 05"
8+00	30.00	28.50	26.26	1° 50' 30"	22° 50' 35"
8+25	30.00	28.50	26.26	1° 50' 30"	24° 01' 05"
8+50	30.00	28.50	26.26	1° 50' 30"	25° 11' 35"
8+75	30.00	28.50	26.26	1° 50' 30"	26° 22' 05"
9+00	30.00	28.50	26.26	1° 50' 30"	27° 32' 35"
9+25	30.00	28.50	26.26	1° 50' 30"	28° 43' 05"
9+50	30.00	28.50	26.26	1° 50' 30"	29° 53' 35"
9+75	30.00	28.50	26.26	1° 50' 30"	31° 04' 05"
10+00	30.00	28.50	26.26	1° 50' 30"	32° 14' 35"

BRIARWOOD AVENUE
 E.L. MURRAY AVE to N.L. HARVEST L.N.
 26'-2'-30' Asph Conc.
 ROLL TYPE CURB
 B.E. SMITH City Engineer
 CITY of Wichita, Kansas
 DATE: APRIL 1927 Proj No. C14-31

Surveyed by
 Checked by
 Date



STATION	ARC	CHORD S. OF ST T. H. C.	CHORD S. OF ST T. S. C.	Deflection	Total Deflection
6+00.00					21° 27' 55"
6+25	17.50	17.20	15.32	1° 10' 29"	22° 38' 24"
6+50	35.00	34.58	31.08	1° 40' 43"	24° 19' 07"
6+75					25° 59' 50"
7+00					27° 40' 33"
7+25					29° 21' 16"
7+50					31° 01' 59"
7+75					32° 42' 42"
8+00					34° 23' 25"
8+25					36° 04' 08"
8+50					37° 44' 51"
8+75					39° 25' 34"
9+00					41° 06' 17"
9+25	17.50	17.20	15.32	1° 40' 43"	42° 47' 00"
9+50	35.00	34.58	31.08	1° 40' 43"	44° 27' 43"
9+75					46° 08' 26"
10+00					47° 49' 09"
10+25					49° 29' 52"
10+50					51° 10' 35"
10+75					52° 51' 18"
10+94.68					54° 32' 01"

Proj. No. C14-31

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