

**GENERAL NOTES**

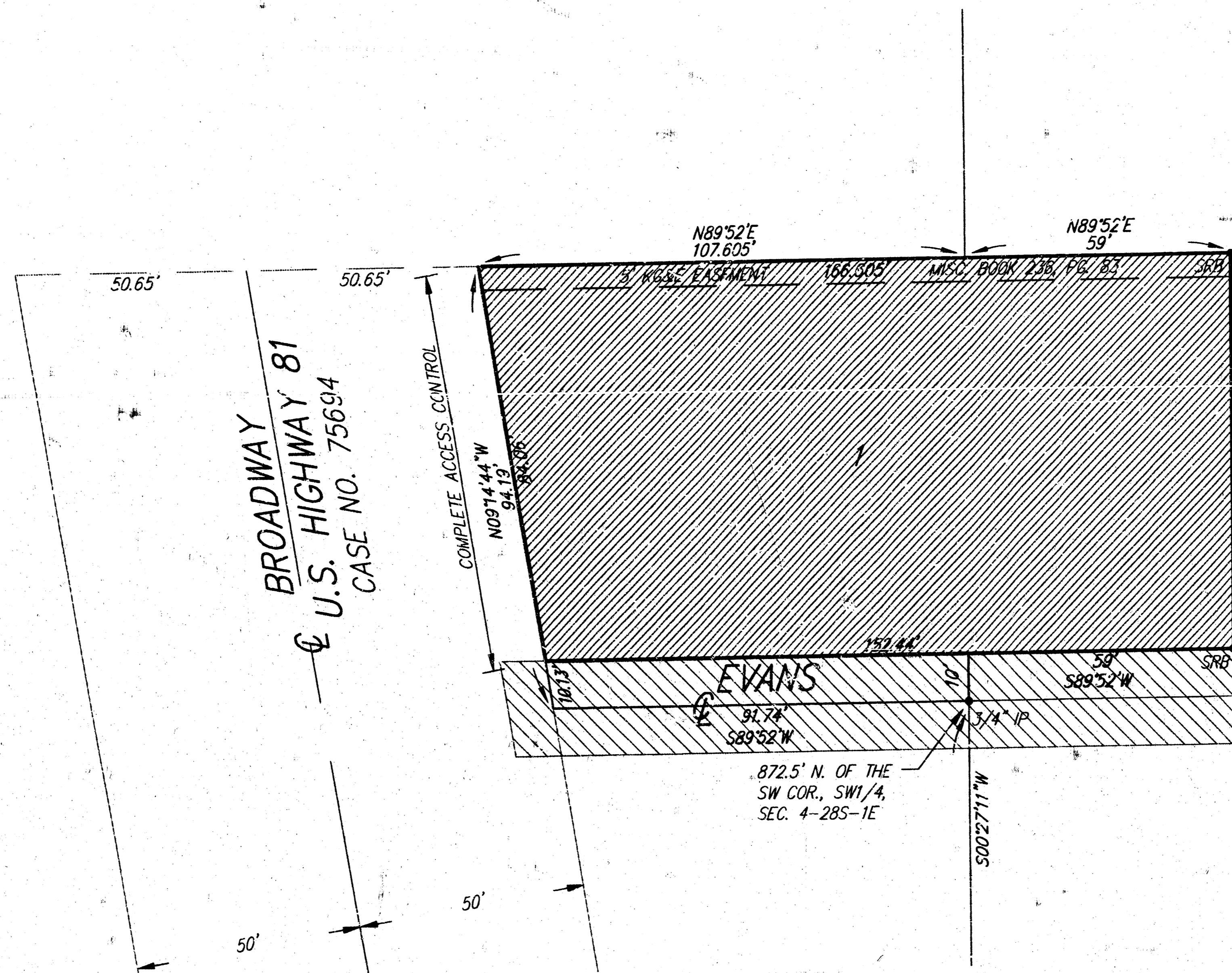
1. Interurban traffic generated outside the project area is not to be carried through construction. Local business or apartment traffic generated within the project area is to be carried through construction as further promulgated by project special provisions.
2. Utility service lines, poles, valve boxes, meters, and all others are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the Contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The Contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction.
3. Rubble from the removal of miscellaneous structures and excess excavation which is to be wasted shall be disposed of on sites to be provided by the Contractor. These sites shall be approved by the Engineer as to suitability, appearance and site location. Locations that, in the opinion of the Engineer, will leave an unsightly appearance will not be approved.  
  
All disposal sites must be approved by the Kansas Department of Health and Environment. Material either stockpiled or disposed of in a flood plain would require a Kansas State Board of Agriculture permit. Any material dumped in waters of the United States or wetlands is subject to U.S. Corps of Engineers permitting regulations. Any material buried or stockpiled beyond approved construction limits would require additional archeological investigations unless buried in a previously approved borrow location.
4. This project does not include any provisions for construction of driveways.
5. Trees and shrubs in public right-of-way which are in direct conflict with proposed new construction shall be removed by the Contractor with the Engineer's approval. Trees and shrubs which are not in direct conflict with proposed new construction shall be saved and protected from damage.
6. Limits of earthwork shall match existing ground elevations at the right-of-way line unless otherwise noted on the plans with a new finished grade elevation. When a new finished grade elevation is shown, the earthwork shall extend one foot beyond the right-of-way line and then sloped up or down using permissible slopes to match the existing ground surface.
7. The Contractor shall be responsible for preserving property lines. The Contractor will be required to re-establish any property lines which are damaged or destroyed by his construction operations. Such lines shall be re-established by a licensed land surveyor in accordance with state laws.
8. Removal of existing concrete parking lot pavements, asphalt parking lot pavements, asphalt driveway pavement and/or asphalt sidewalk pavements will be paid for as square feet of walk and drive removed.
9. Construction of asphalt parking lot pavement will be paid for at the unit price bid per square yard of 6" asphalt parking lot construction (4" bituminous base).
10. Construction of concrete parking lot pavement will be paid as square feet of drive for the thickness indicated.
11. The Contractor shall give all property owners and/or tenants of developed property abutting the project limits a minimum of ten (10) days advance notice prior to start of construction.

**STREET IMPROVEMENTS  
PURSLEY ADDITION - EVANS STREET**

**PROJECT NO. 472-82955  
INDEX CODE 765073**

**BENCH MARK:**

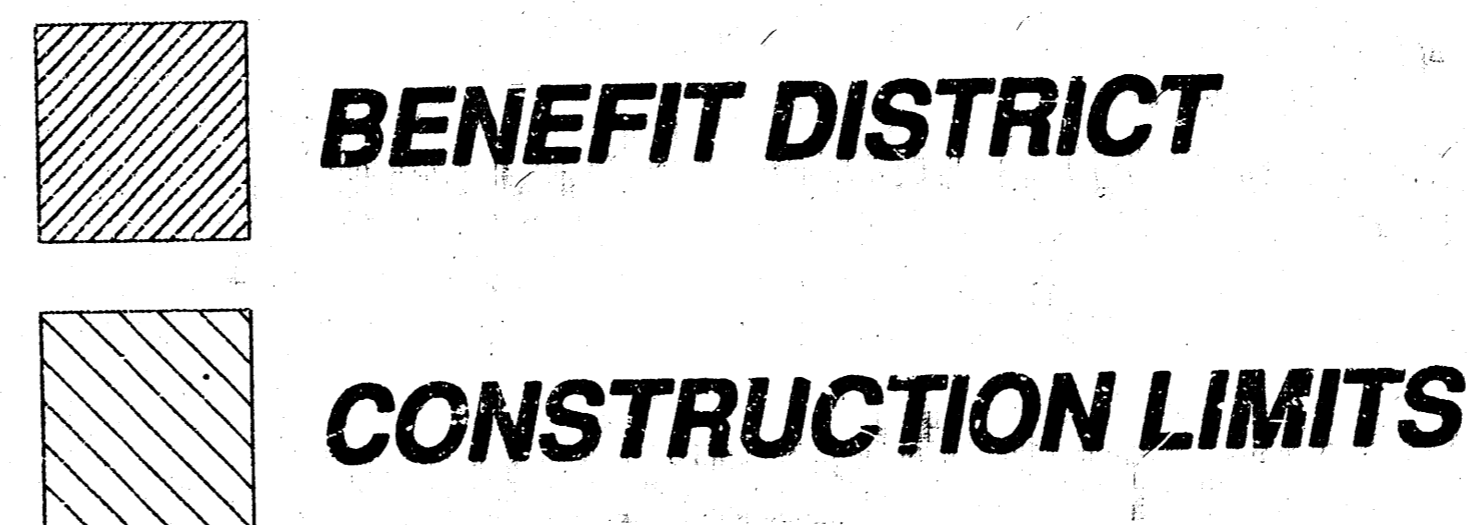
Top of PK Nail, 6' 8" of  
the SW Cor. of Lot 1,  
Pursley Addition  
Elev. = 1284.57 MSL



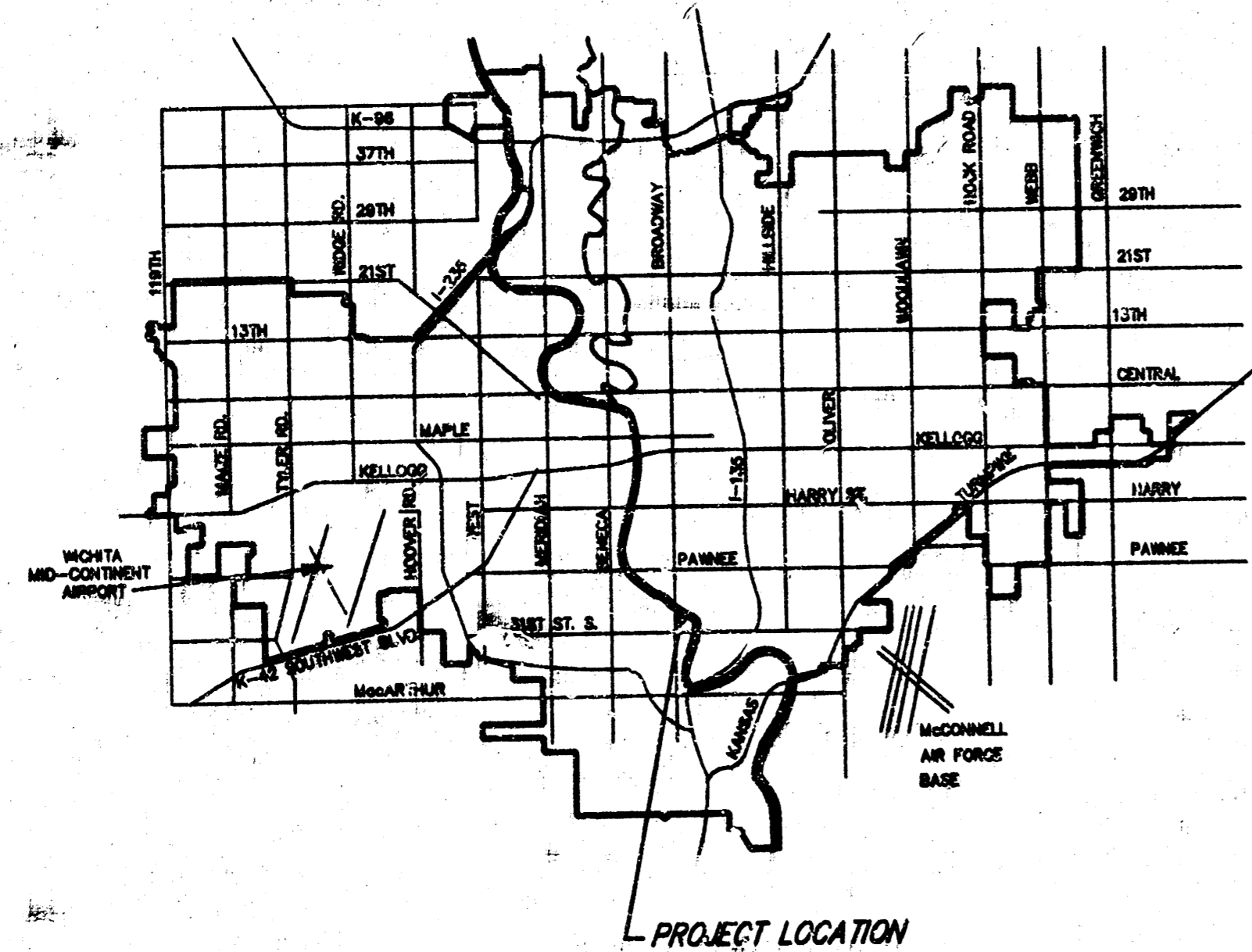
SCALE:  
1"=20'

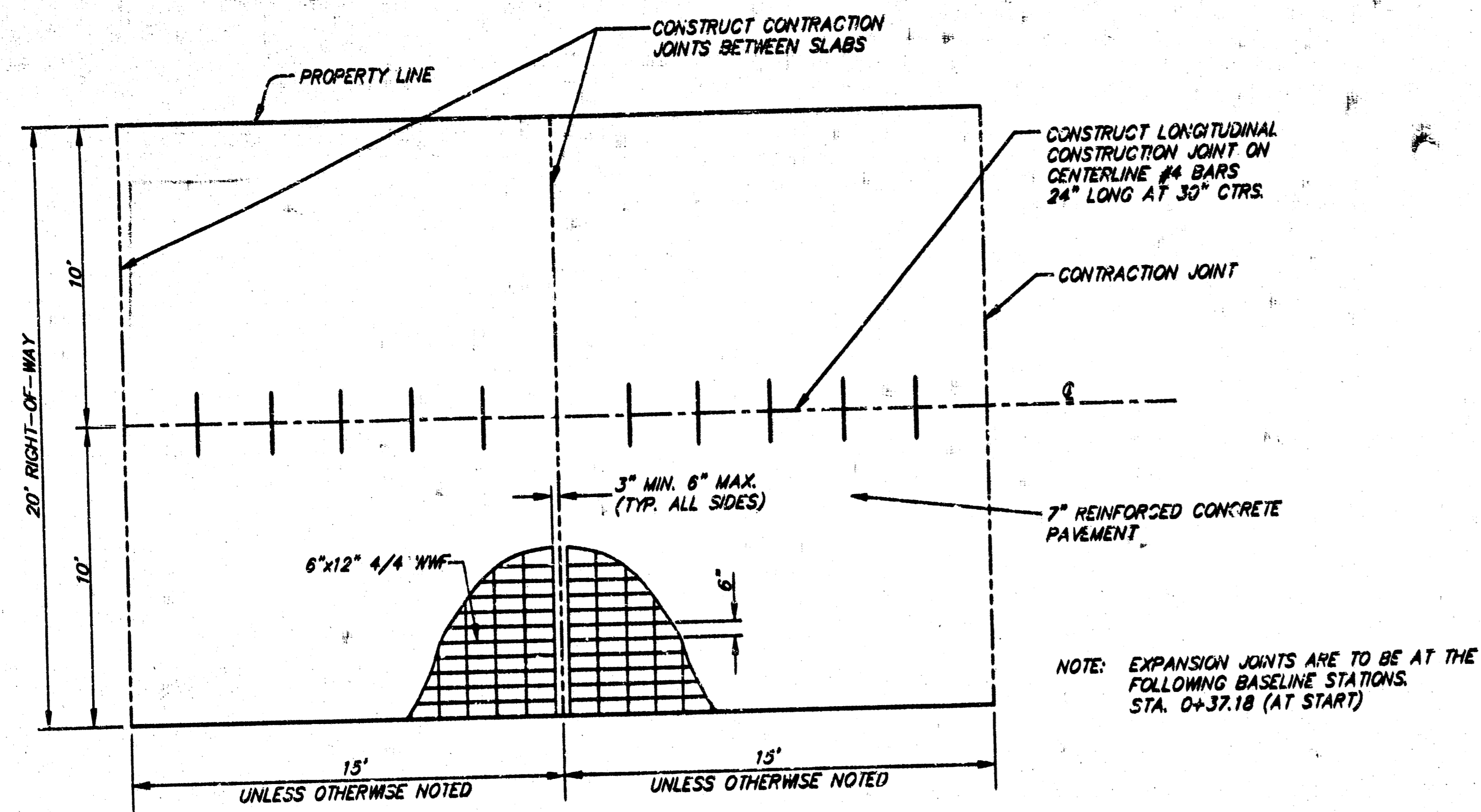
**SHEET INDEX**

1. COVER SHEET
2. PLAN SHEET
3. PLAT COPY

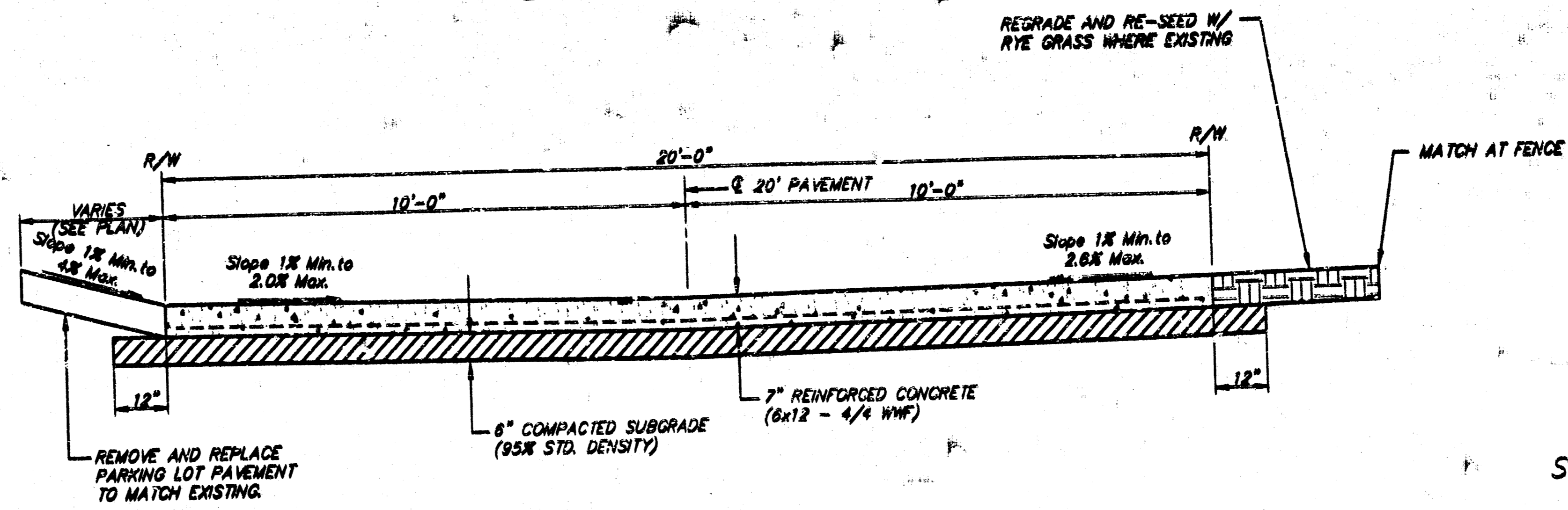


**CITY OF WICHITA, KANSAS  
MICHAEL E. LINDEBAK - CITY ENGINEER**





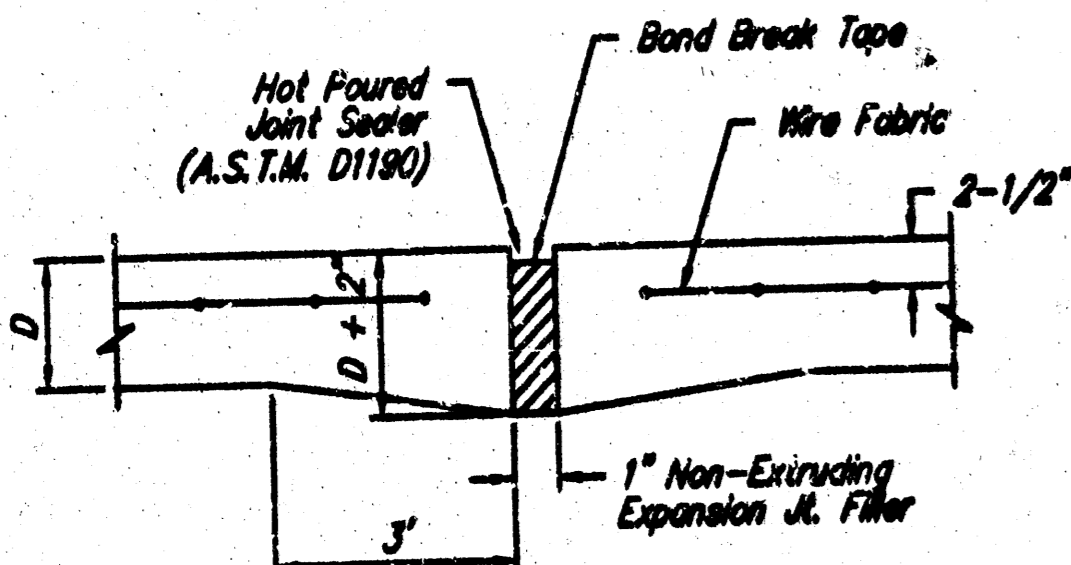
SLAB REINFORCEMENT DETAIL



TYPICAL PAVEMENT SECTION

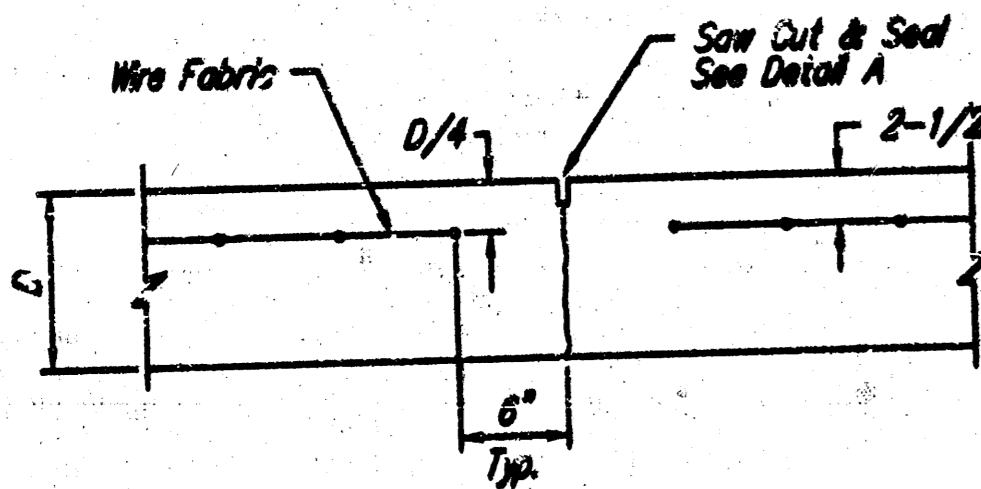
SCALE: 1"=20'

BENCHMARK:  
Top of Post Nail, 6.0' W. of  
the SW Cor. of Lot 1,  
Pursley Addition  
Elev. = 1284.57 MSL

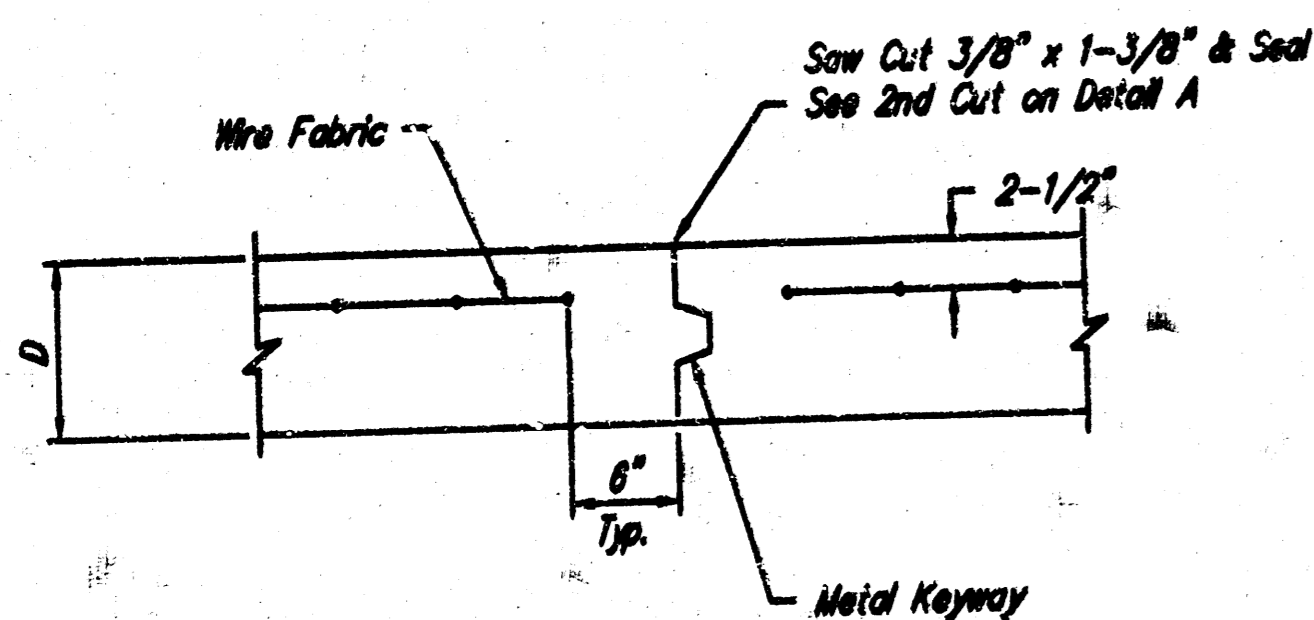


EXPANSION JOINT

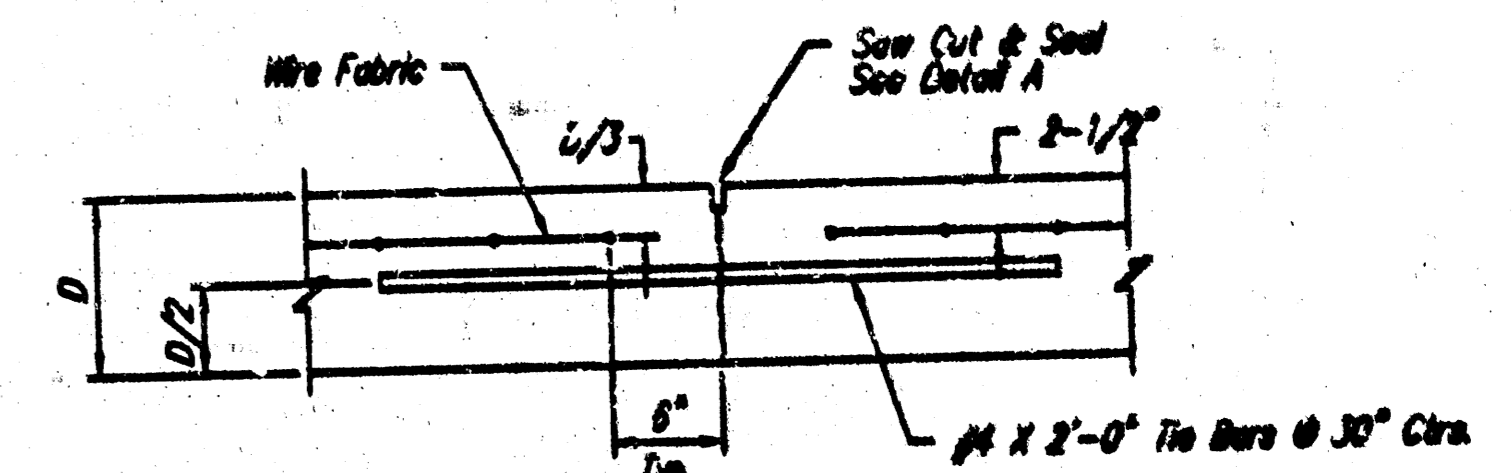
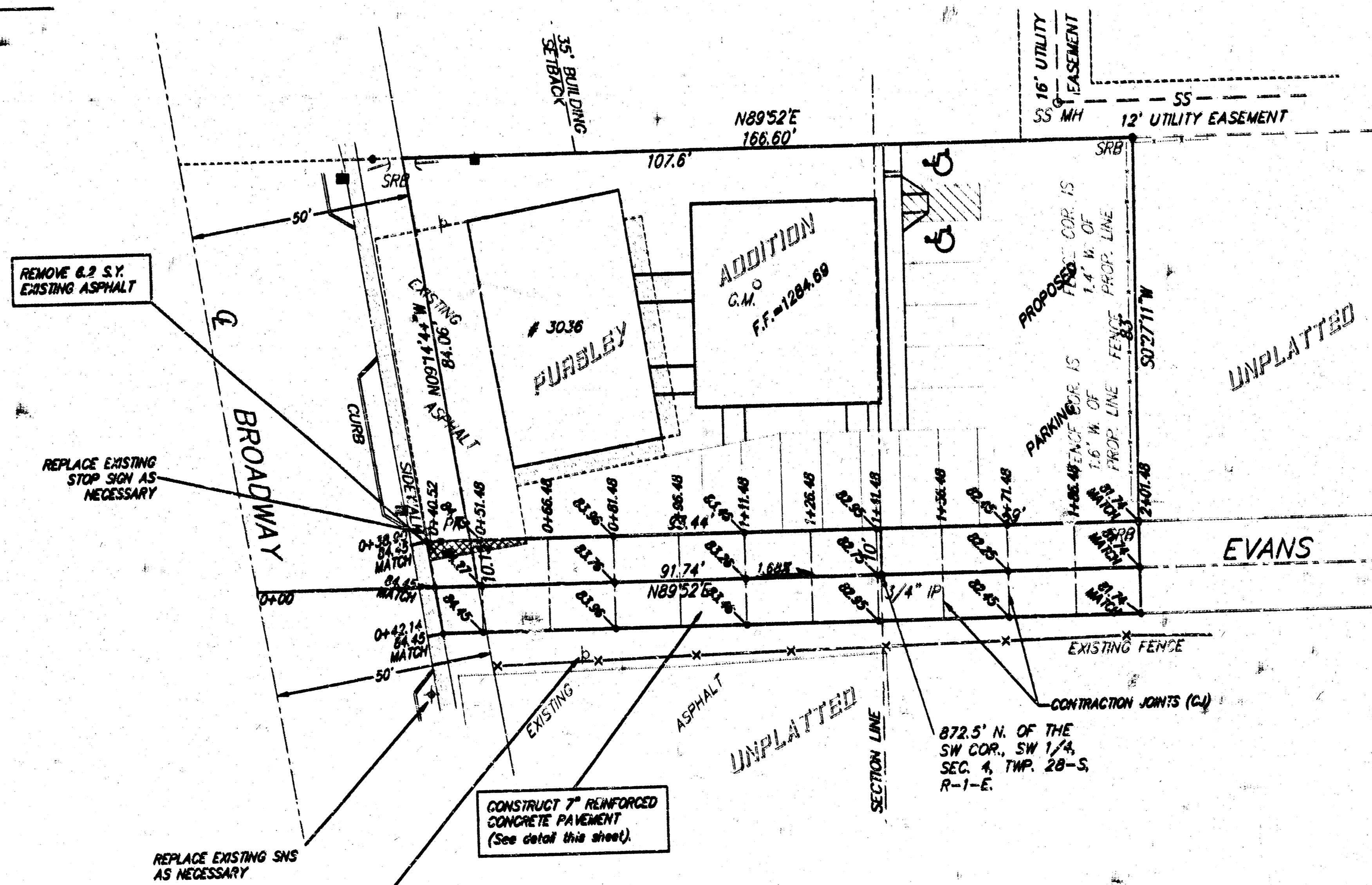
NOTE: Extra Thickness to be Subsidiary to Price of Square Yards Pavement



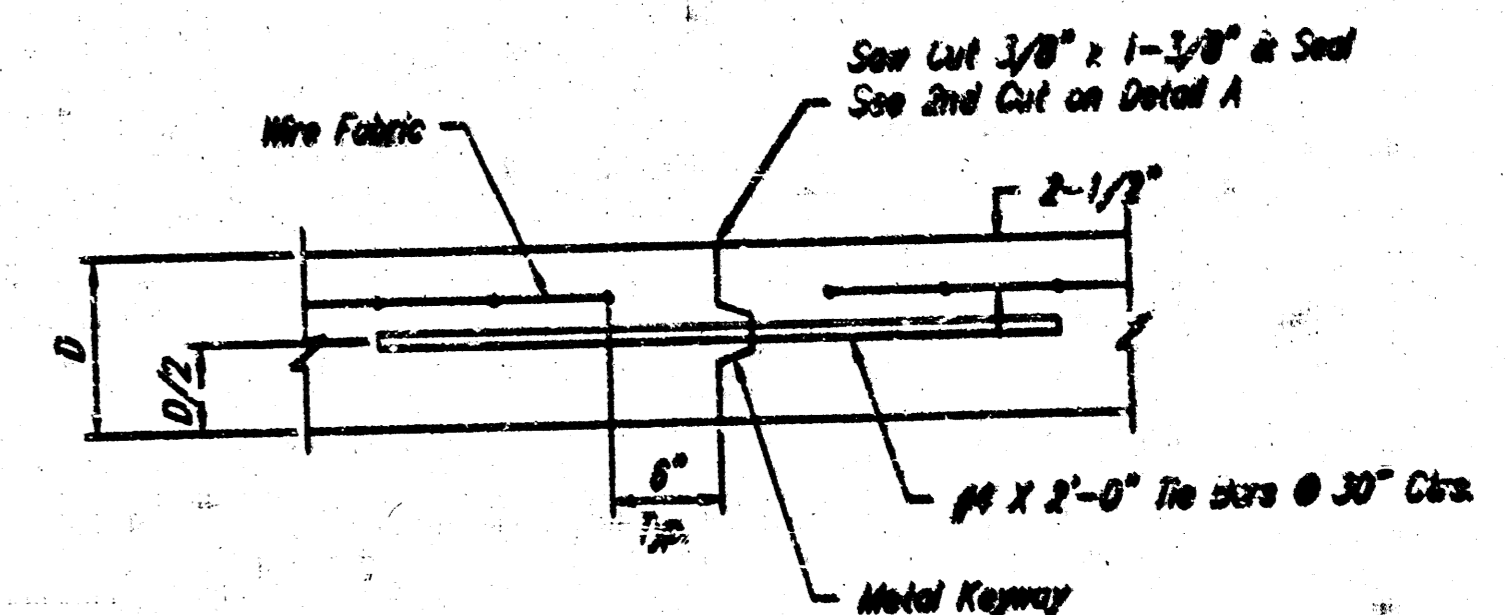
CONTRACTION JOINT DETAIL (C.J.)



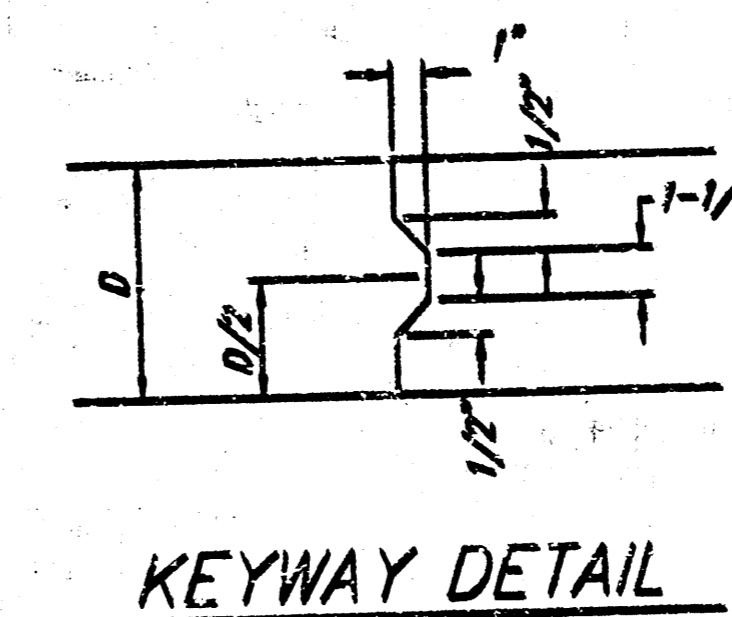
OPTIONAL CONTRACTION JOINT



LONGITUDINAL JOINT DETAIL (L.J.)



OPTIONAL LONGITUDINAL JOINT DETAIL (L.J.)



KEYWAY DETAIL

<b>PURBSLEY ADDITION PAVEMENT PLAN - EVANS ST. WICHITA, KANSAS</b>			
<b>SRB</b>	924 NORTH MAIN WICHITA, KANSAS 67203 http://www.srb.com/~sr	316-264-8008 FAX 264-4421 E-mail: srbsrb@sr.com	PROJECT NUMBER 472-82955 ( )
DESIGN JDS	DRAWN JDS	UTILITY JDS	REVIEW C. BOHM
		DATE June 23, 1998	REVISION
			SHEET 2 OF 3

# PURSLEY ADDITION

WICHITA, SEDGWICK COUNTY, KANSAS

State of Kansas)  
Sedgwick County) SS

We, Savoy, Ruggles & Bohm, P.A., Surveyors in aforesaid county and state do hereby certify that, under the supervision of the undersigned, we have surveyed and platted "PURSLEY ADDITION", Wichita, Sedgwick County, Kansas and that the accompanying plat is a true and correct exhibit of the property surveyed, described as follows:

A tract beginning at a point on the Section Line between Sec. 4 and Sec. 5, Twp. 28-S, R-1-E of the 6th P.M., Sedgwick County, Kansas, 872.5 feet north of the S.W. Corner of the SW1/4 of Sec. 4; thence east, parallel with the South Line of said SW1/4, 59 feet; thence north, parallel with the Section Line between said Sec. 4 and Sec. 5, 93 feet; thence west, parallel with the South Line of said SW1/4 of Sec. 4, and the SE1/4 of said Sec. 5, 166.6 feet more or less to the Easterly right-of-way of U.S. 81 Highway Case No. 75694; thence southeasterly, along said right-of-way to a point 872.5 feet north of the South Line of said SE1/4 of Sec. 5; thence east, 91.4 feet more or less to beginning, except the south 10 feet of the east 59 feet thereof, dedicated for roadway purposes.

All Public easements and dedications being vacated by virtue of K.S.A. 12-512(b).

Savoy, Ruggles & Bohm, P.A.

Date \_\_\_\_\_

Surveyor

Mark A. Savoy RLS #788

This plat of "PURSLEY ADDITION", Wichita, Sedgwick County, Kansas, has been submitted to and approved by the Wichita-Sedgwick County Metropolitan Area Planning Commission, Wichita, Kansas.  
Dated this \_\_\_\_\_ day of \_\_\_\_\_, 1998.

Wichita-Sedgwick County Metropolitan Area Planning Commission

Chairman

Richard Lopez

Secretary

Marvin S. Krout

This plat approved and all dedications shown hereon, accepted by the City Council of the City of Wichita, Kansas, this \_\_\_\_\_ day of \_\_\_\_\_, 1998.

Mayor

Bob Knight

City Clerk

Pat Burnett

Entered on transfer record this \_\_\_\_\_ day of \_\_\_\_\_, 1998

County Clerk

James Alford

State of Kansas)  
Sedgwick County) SS

This is to certify that this plat has been filed for record in the office of the Register of Deeds, this \_\_\_\_\_ day of \_\_\_\_\_, 1998, at \_\_\_\_\_ o'clock \_\_\_\_\_ M. and is duly recorded.

Register of Deeds

Bill Meek

Deputy

Linda Kizzire

Richard E. Pursley

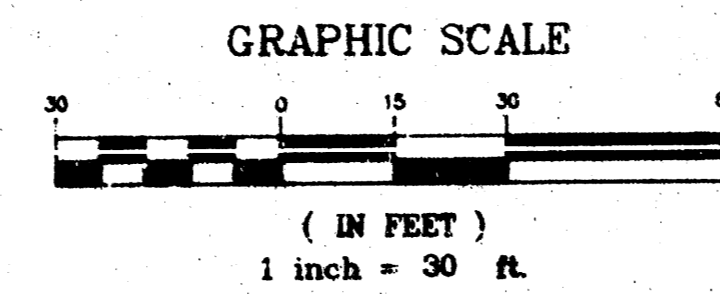
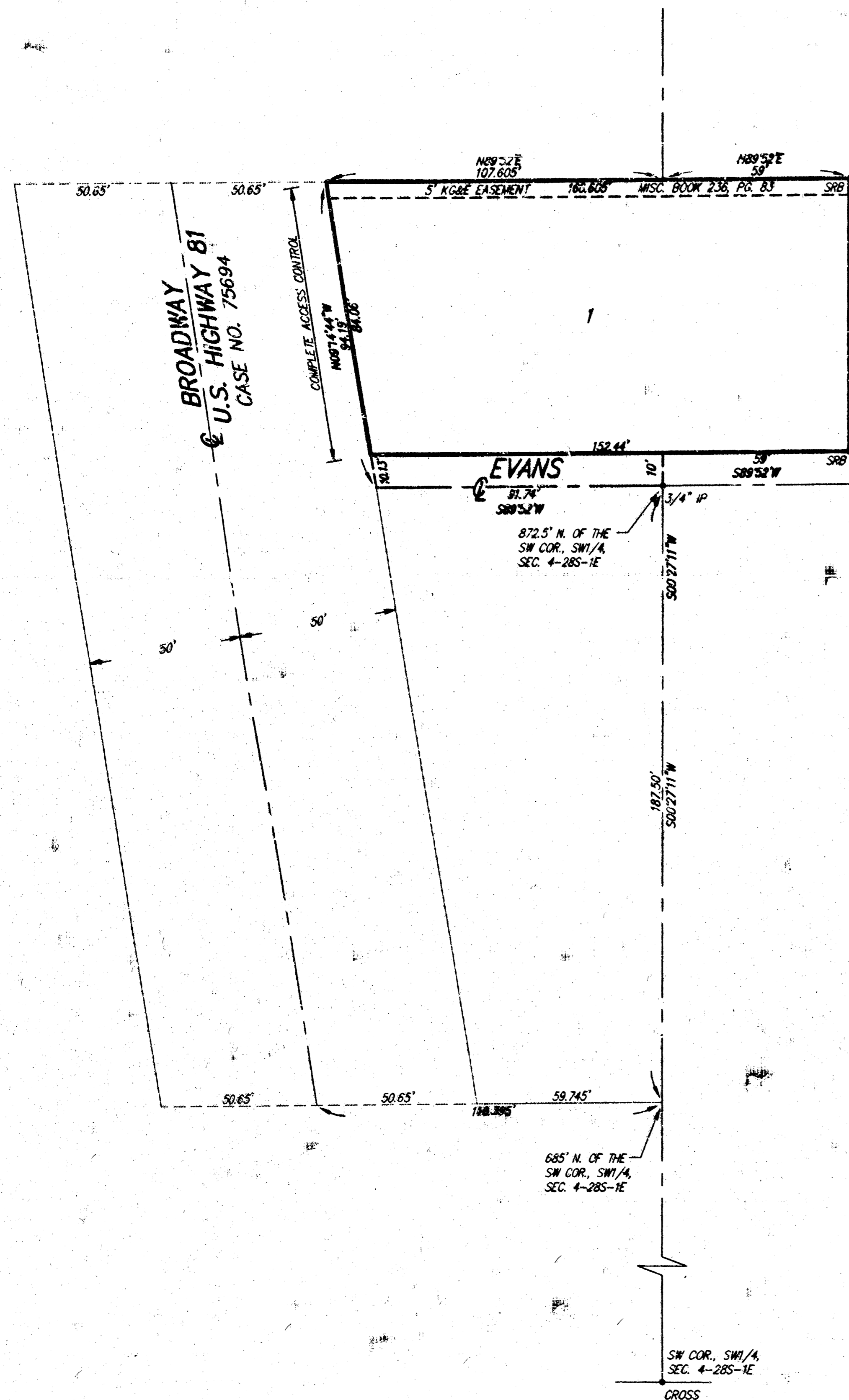
Georgia A. Pursley

State of Kansas)  
Sedgwick County) SS

The foregoing instrument acknowledged before me, this \_\_\_\_\_ day of \_\_\_\_\_, 1998, by Richard E. Pursley and Georgia A. Pursley, his wife.

Notary Public

My App't. Exp. \_\_\_\_\_



### LEGEND

- SRB ..... 1/2" REBAR W/SRB CAP
- CROSS ..... ENCASED CROSS
- IP ..... IRON PIPE

SRB 924 NORTH MAIN 316-264-8008  
WICHITA, KANSAS 67203 FAX 264-4621  
http://www.just.com/~srb E-mail: sr@slat.com

DWG FILE: 309929-CX  
PROJECT NO: 9800992P

SAVOY, RUGGLES & BOHM, P.A.  
ENGINEERING & SURVEYING 01-05-01