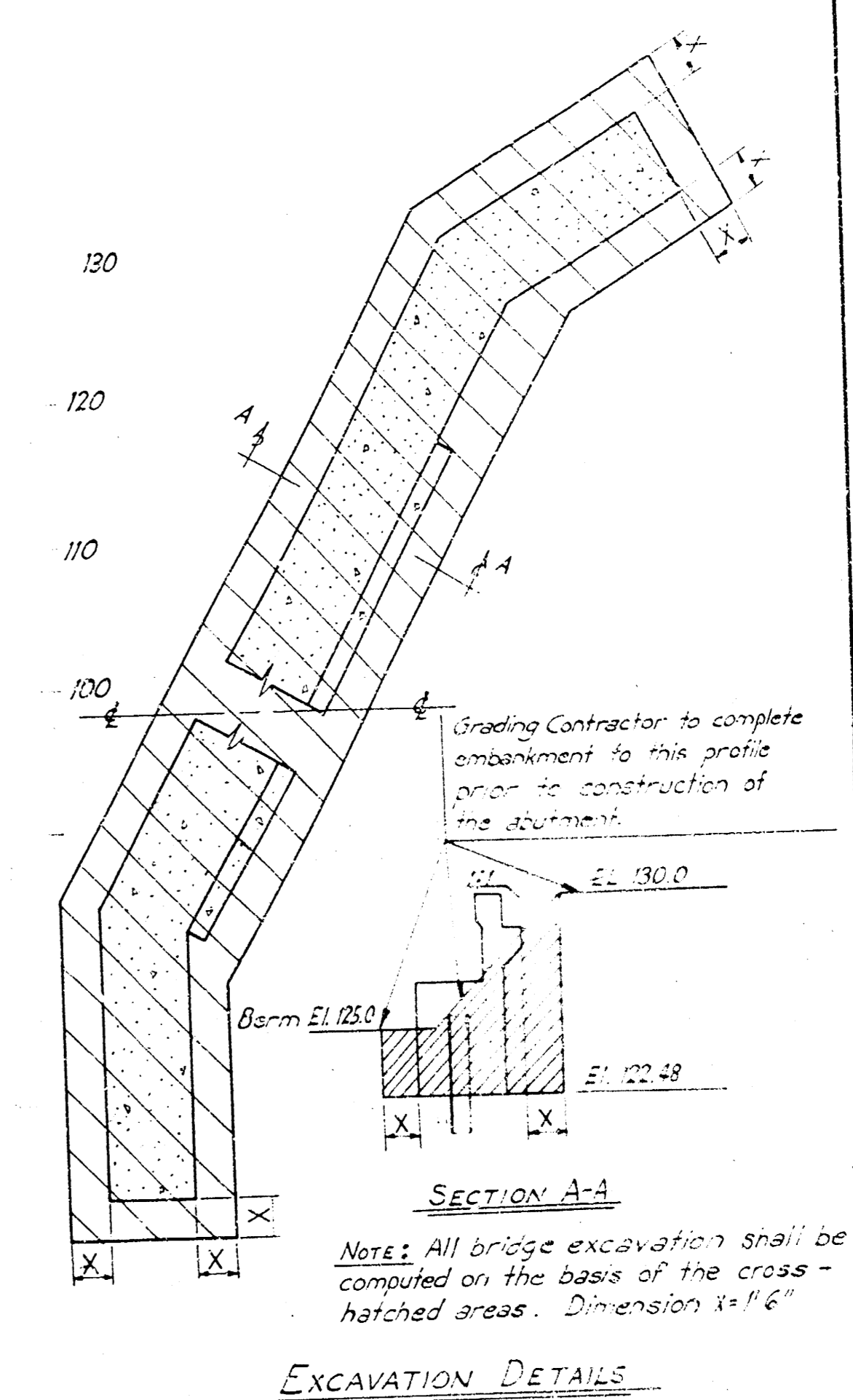


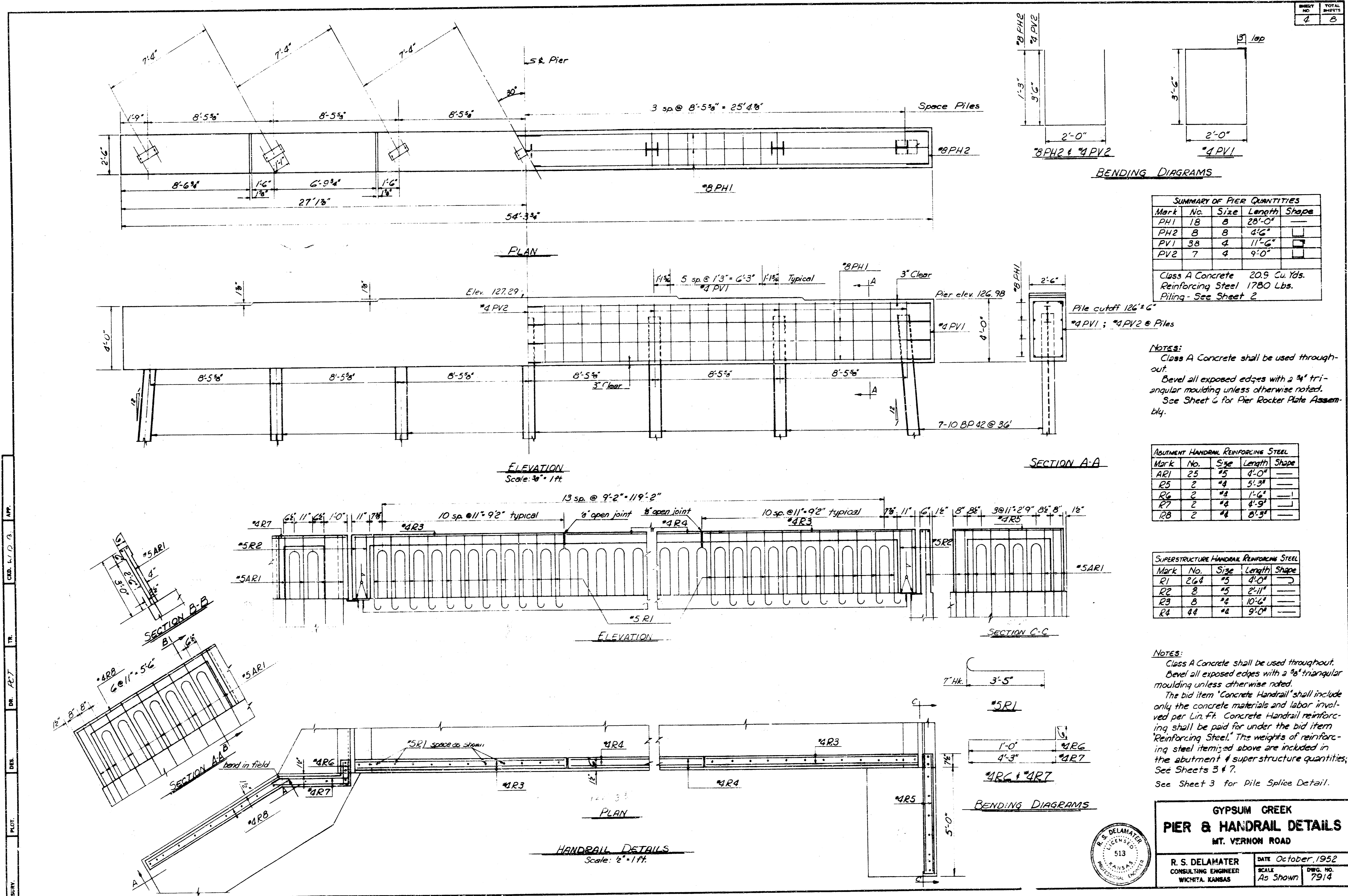
ELEVATION
Sta 7+50 Const 37'-46" - 37' Cont Steel Beam Spans Steel Pile Piers & Abutments 44' Roadway 30° Skew Rt.



GENERAL NOTES:
CHANNEL CHANGES & GRADINGS: The Grading Contractor shall excavate the channel change at the bridge site and complete the embankment at the abutments prior to construction of the bridge.
SOUNDINGS: Soundings are as taken by borings in the field and represent the best information available to the city.
PILES: Piles shall be driven to a minimum computed bearing of 33 tons per pile and to the penetration shown unless in the judgement of the Engineer such penetration cannot be obtained without injury to the pile.
TEST PILE: A steel test pile shall be driven at the location shown or as directed by the Engineer. Test pile shall remain in place as a permanent pile.
CONCRETE: All concrete shall be Class A.
REINFORCING STEEL: All reinforcing steel shall be billet steel and shall conform to A.S.T.M. Specification A 305-49.
DESIGN: Design Loading: $f_c = 20,000$ psi - Reinforcing Steel
 $f_s = 18,000$ psi - Structural Steel
 $f_c = 1,000$ psi - Class A Concrete
 Maximum Pile Loading 33 Tons per Pile.
GENERAL REQUIREMENTS: It is the intention of these plans & specifications that construction of the bridge shall be in accordance with applicable standard specifications and requirements of the Kansas Highway Commission and that materials shall conform to those specifications, unless otherwise expressly indicated.

LOCATION	Class III Exc. (Cu.)	Class A Conc. (Cu.)	Steel		Cast Iron Drains Lbs.	Steel Piles Lin. Ft.	Steel Test Piles Lin. Ft.	Concrete Handrail Lin. Ft.
			Reinforcing Lbs.	Structural Lbs.				
Abut. #1	88	56.4	4470	---	---	788	40	17.5
Pier #1	---	20.9	1780	---	---	---	---	---
Pier #2	---	20.9	1780	---	---	---	---	---
Abut. #2	88	56.4	4470	---	---	324	---	17.5
Superstructure	---	140.4	26270	90,050	2895	284	---	242.5
Totals	176	295.0	38770	90,050	2895	1116	40	279.5

GYPSUM CREEK CONSTRUCTION LAYOUT
 MT. VERNON ROAD
 R. S. DELAMATER
 CONSULTING ENGINEER
 WICHITA, KANSAS
 DATE October 1952
 SCALE 1" = 10'
 DWG. NO. 7912



SHEET NO.	TOTAL SHEETS
2	6

SUMMARY OF PIER QUANTITIES

Mark	No.	Size	Length	Shape
PH1	18	8	28'-0"	—
PH2	8	8	4'-6"	—
PV1	38	4	11'-6"	—
PV2	7	4	9'-0"	—

Class A Concrete 20.5 Cu Yds.
Reinforcing Steel 1780 Lbs.
Piling - See Sheet 2

ABUTMENT HANDRAIL REINFORCING STEEL

Mark	No.	Size	Length	Shape
AR1	25	#5	4'-0"	—
AR2	2	#4	5'-3"	—
AR3	2	#4	1'-4"	—
AR4	2	#4	4'-9"	—
AR5	2	#4	8'-5"	—

SUPERSTRUCTURE HANDRAIL REINFORCING STEEL

Mark	No.	Size	Length	Shape
R1	264	#5	4'-0"	—
R2	8	#5	2'-11"	—
R3	8	#4	10'-4"	—
R4	44	#4	9'-0"	—

NOTES:
 Class A Concrete shall be used throughout.
 Bevel all exposed edges with a 3" triangular moulding unless otherwise noted.
 The bid item "Concrete Handrail" shall include only the concrete materials and labor involved per Lin. Ft. Concrete Handrail reinforcing shall be paid for under the bid item "Reinforcing Steel". The weights of reinforcing steel items listed above are included in the abutment & superstructure quantities. See Sheets 3 & 7.
 See Sheet 3 for Pile Splice Detail.

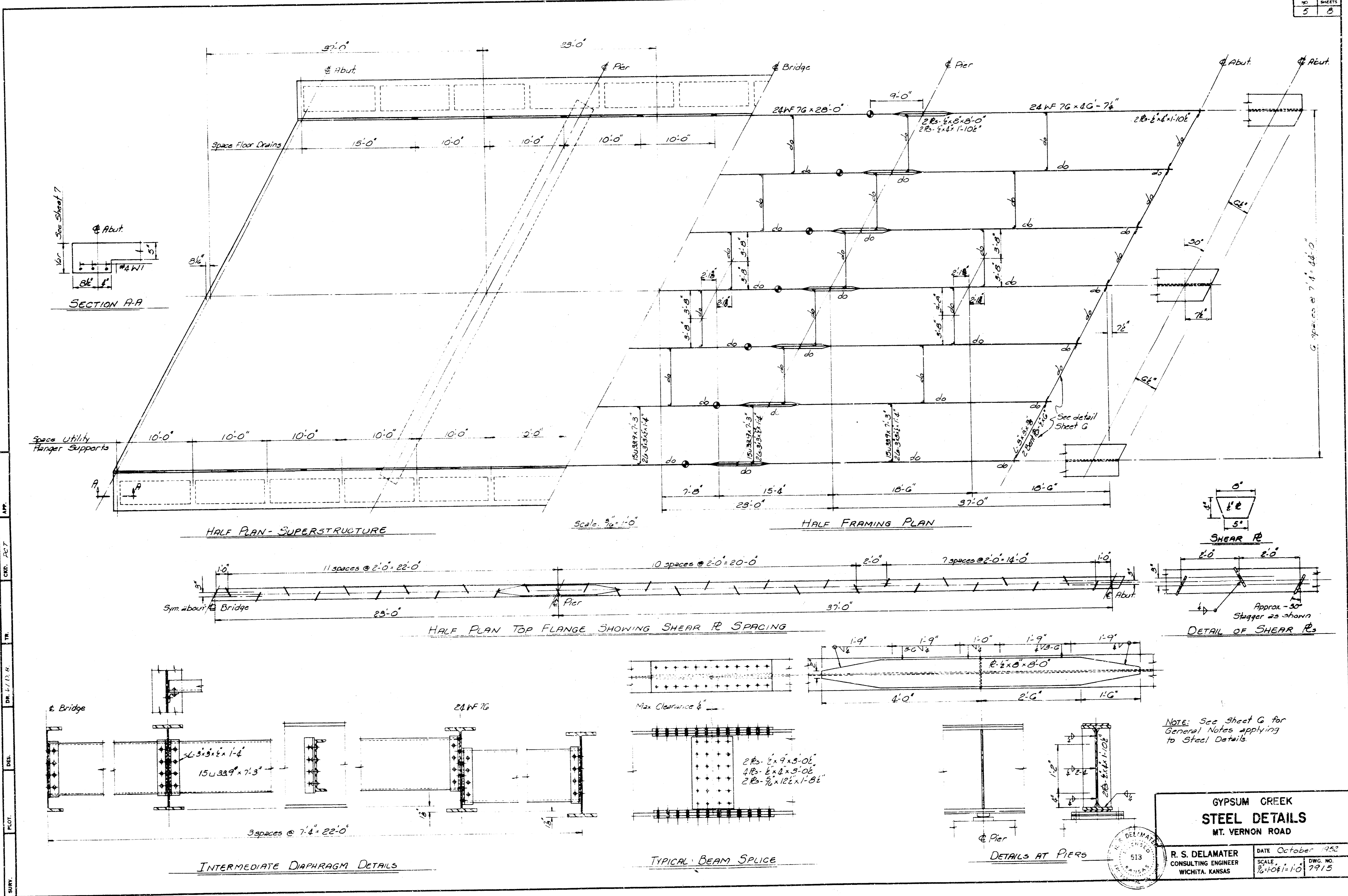
**GYPSUM CREEK
PIER & HANDRAIL DETAILS
MT. VERNON ROAD**

R. S. DELAMATER
CONSULTING ENGINEER
WICHITA, KANSAS

DATE October, 1952
SCALE As Shown
DWG. NO. 7914

SURV. PLOT. DED. DR. 12-7. TR. CO. 1-1-13. MP.

SHEET NO.	TOTAL SHEETS
5	5



NOTE: See Sheet G for General Notes applying to Steel Details

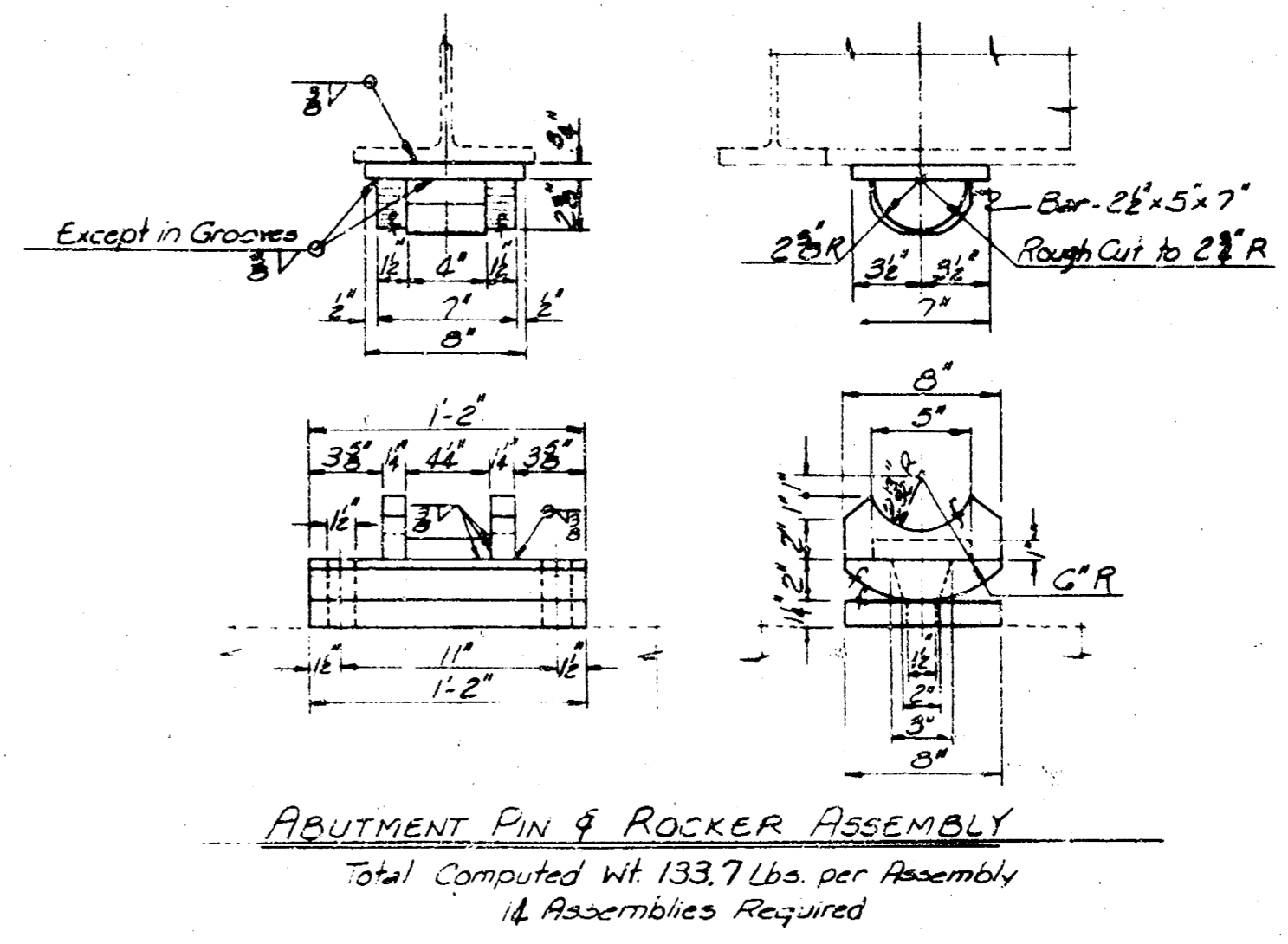
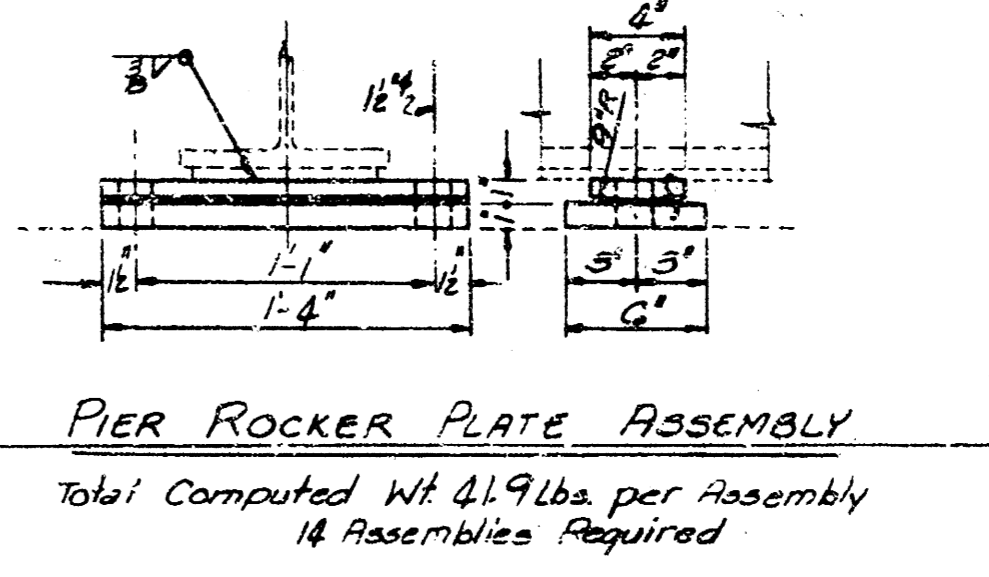
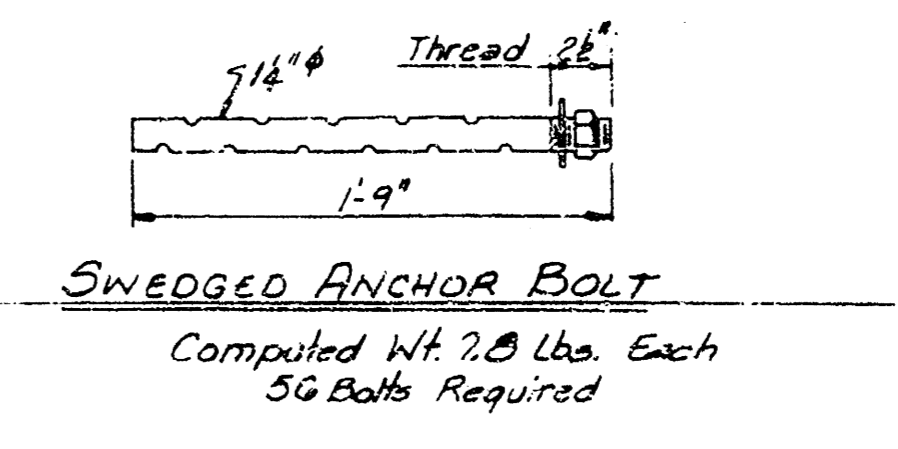
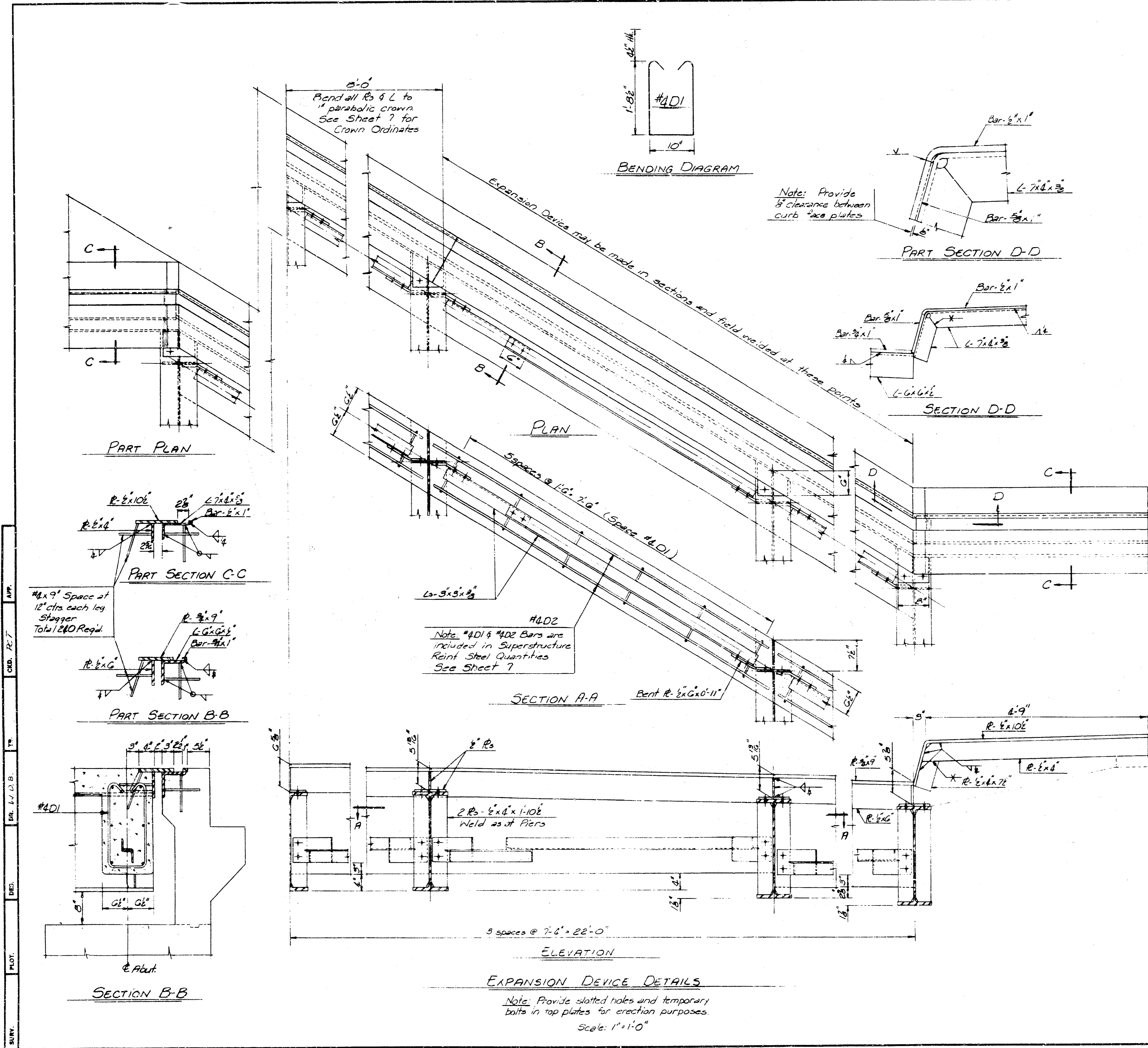
**GYPSUM CREEK
STEEL DETAILS
MT. VERNON ROAD**

R. S. DELAMATER
CONSULTING ENGINEER
WICHITA, KANSAS

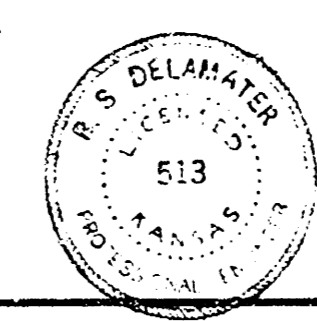
DATE: October 1952
SCALE: 3/8" = 1'-0"
DWG. NO. 7915

SURV. PLOT. DES. DIR. L. I. D. G. TR. C.E.D. 10-7-52

SHEET NO.	TOTAL SHEETS
6	8



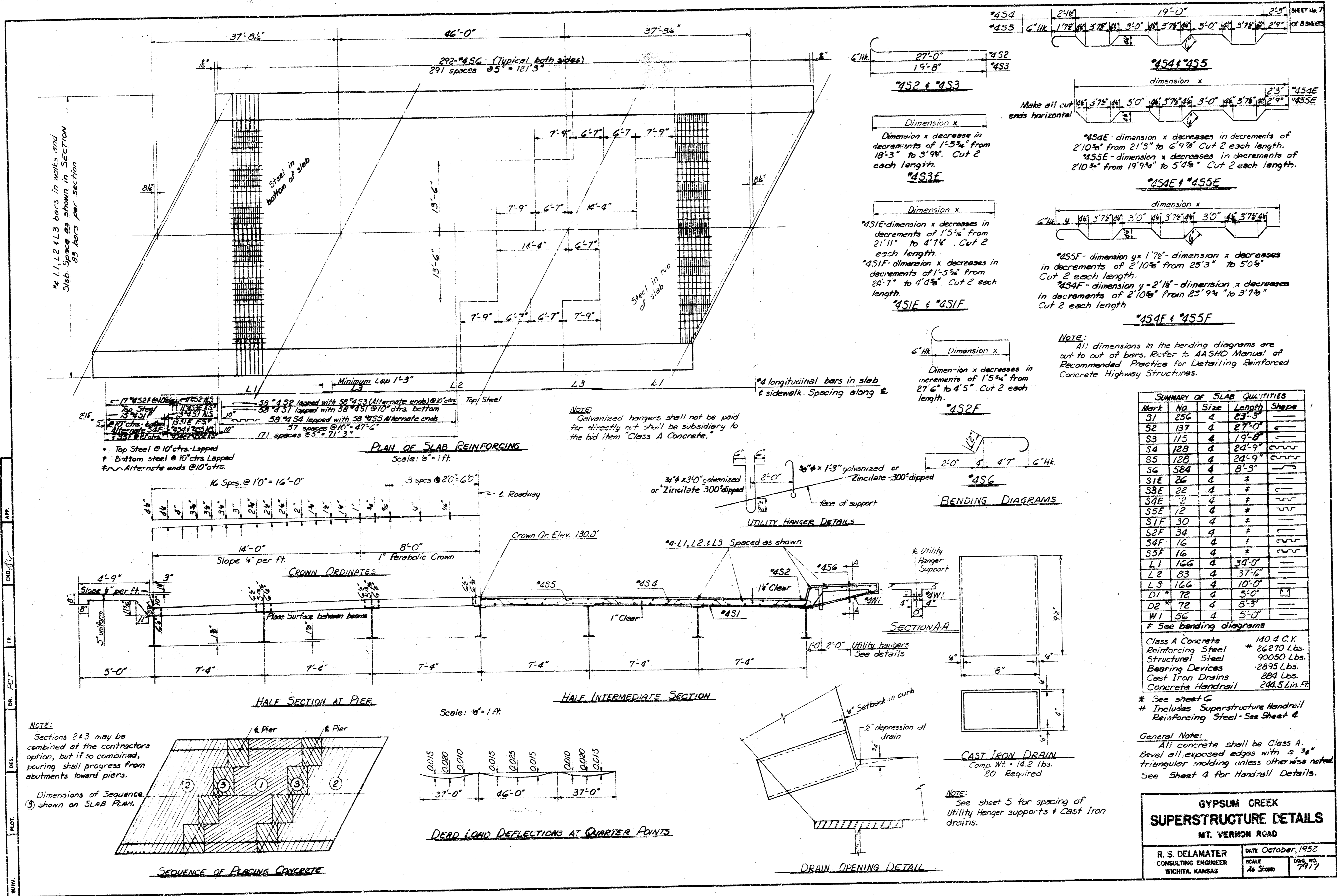
GENERAL NOTES:
RIVETS & BOLTS: All rivets shall be 3/4" in 1 1/2" open holes. All bolts required for field erection shall be 3/4" structural rib bolts in 1 1/2" open holes.
BEAM SPICES: Use 3/4" structural rib bolts in beam splices. All open holes shall be sub-punched or sub-drilled 1/4" and reamed to 1/2" with component parts assembled to correct line and matched marked. The number of bolts shall not be less than shown.
WELDING: All welds shall be 1/2" continuous fillet welds unless otherwise noted.
PAINTING: All parts accessible after erection shall receive one coat of tinted aluminum followed by one coat of aluminum paint.
ERECTION: Each line of girders shall be erected to correct line and grade before field splices are bolted. The structural steel in the two end spans shall be completely erected and all bolting of end diaphragms and expansion devices completed prior to construction of the back-walls above bridge seat elevation.
CONSTRUCTION: Dead load deflections shall be provided for in the amount shown on Sheet 7 by raising the slab as indicated.
BEARING DEVICES: All Bearing Plates, Rocker Pins and Plates, and Swedged Anchor Bolts shall be included in "Bearing Devices" bid item. All parts of Bearing Devices shall be structural steel.



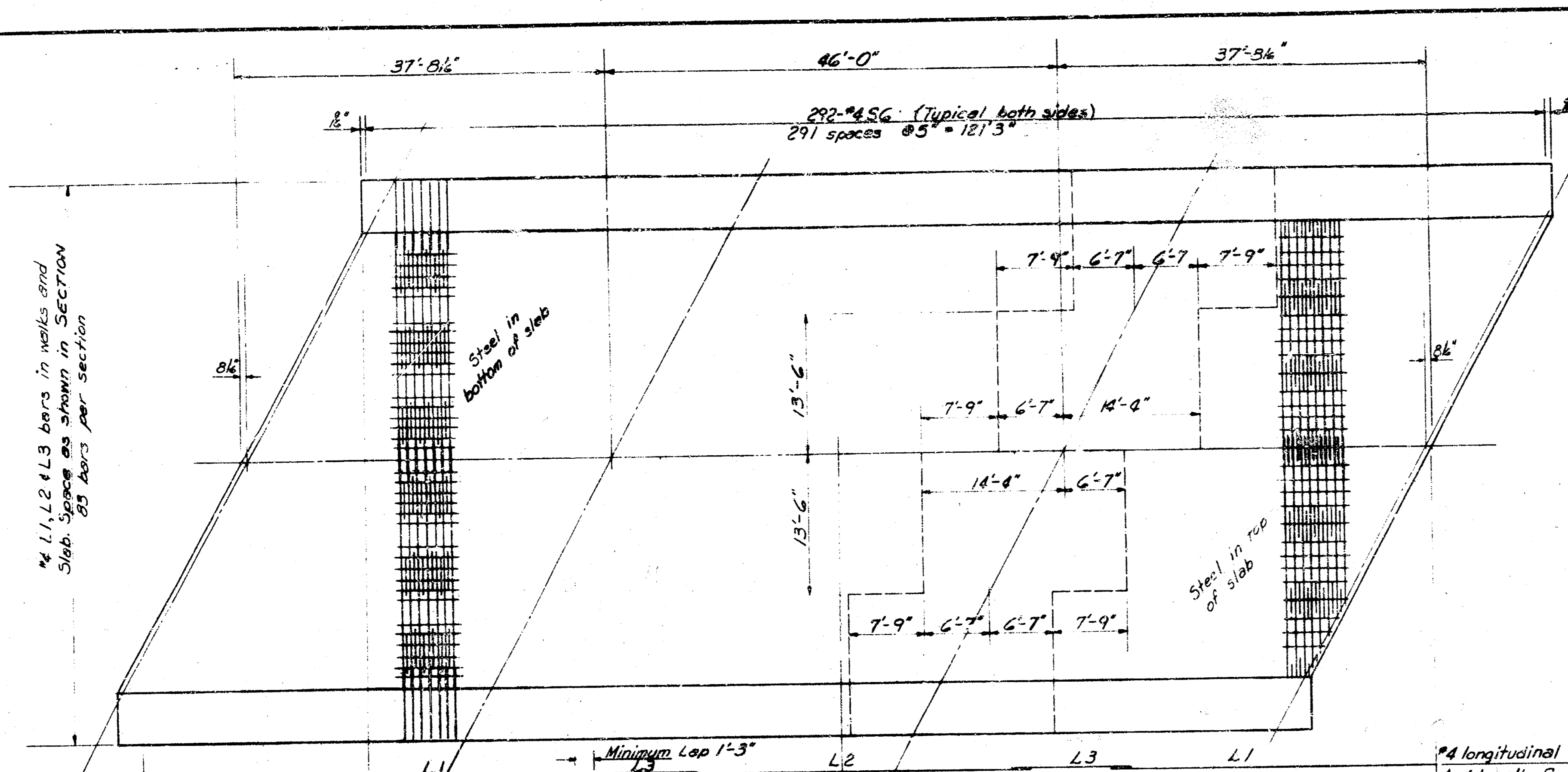
**GYPSUM CREEK
 EXPANSION & BEARING
 DEVICE DETAILS**
 MT. VERNON ROAD

R. S. DELAMATER
 CONSULTING ENGINEER
 WICHITA, KANSAS

DATE October 1952
 SCALE 1/4"=1'-0"
 DWG. NO. 7910

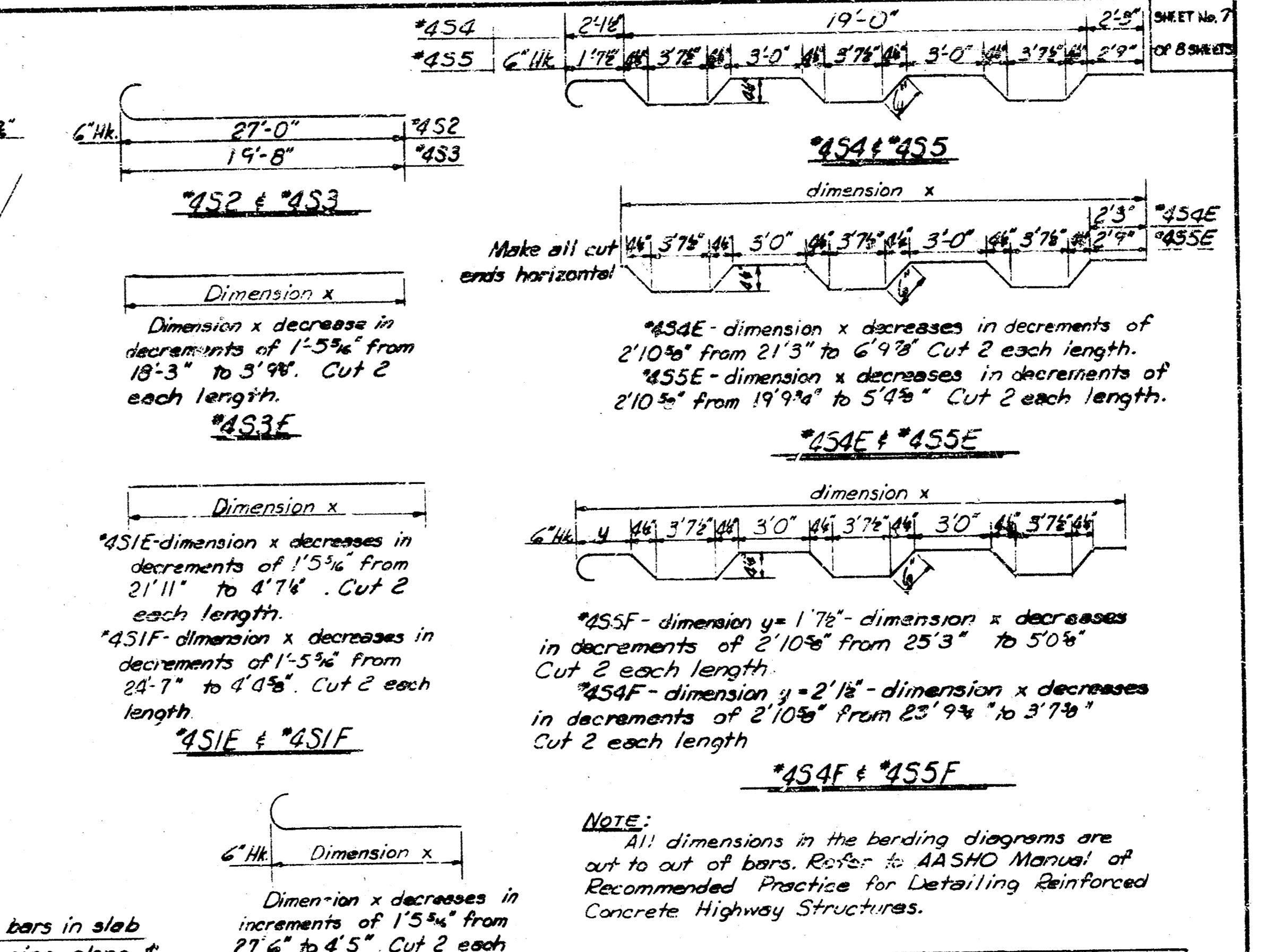
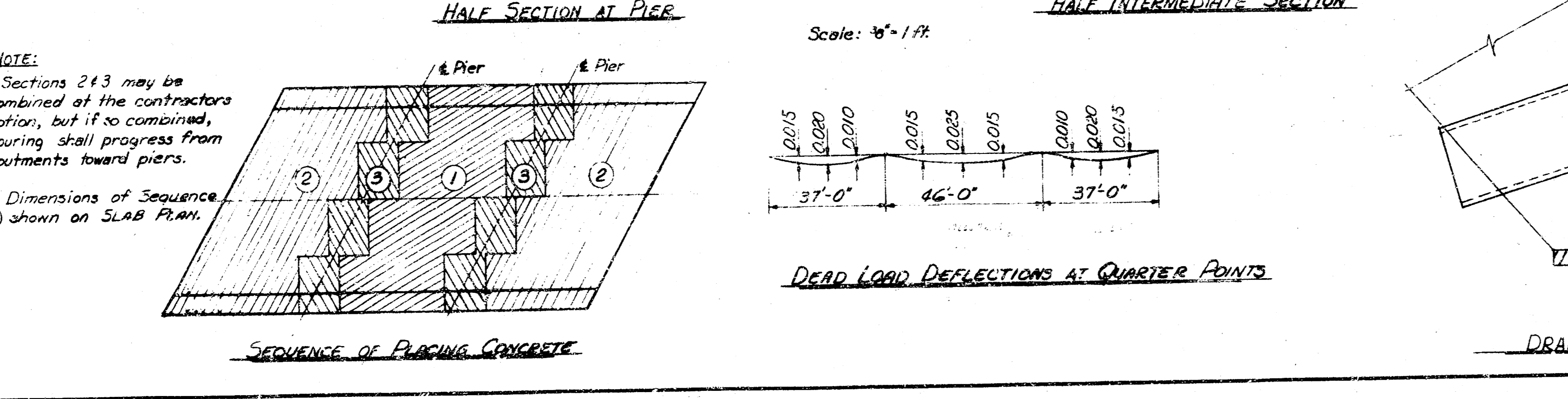
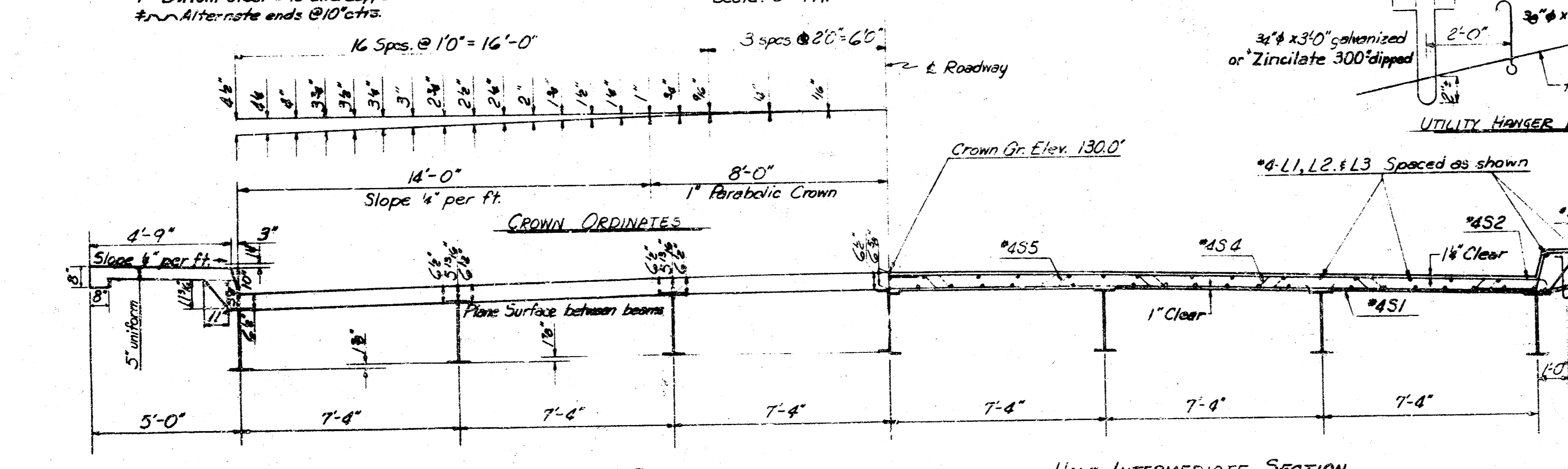


APP. _____
 CHG. _____
 TR. _____
 DR. ACT. _____
 DES. _____
 PLOT. _____
 REV. _____



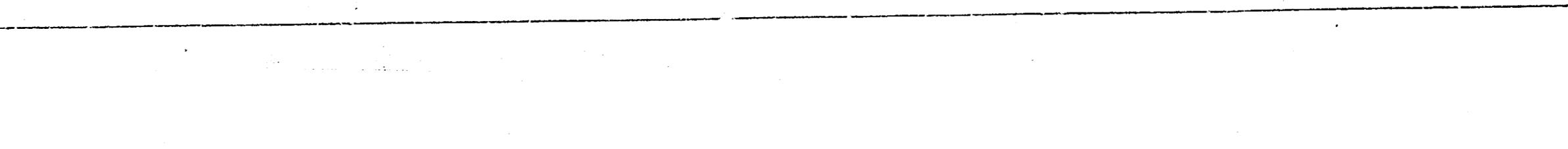
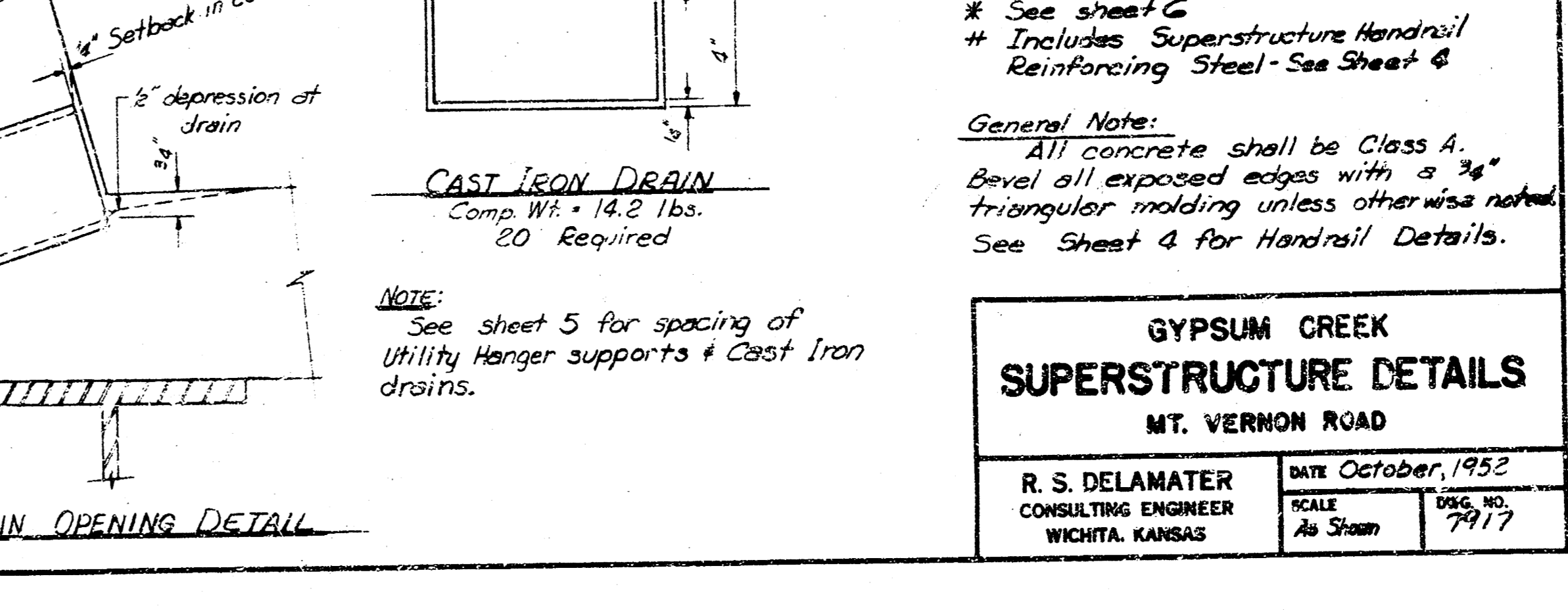
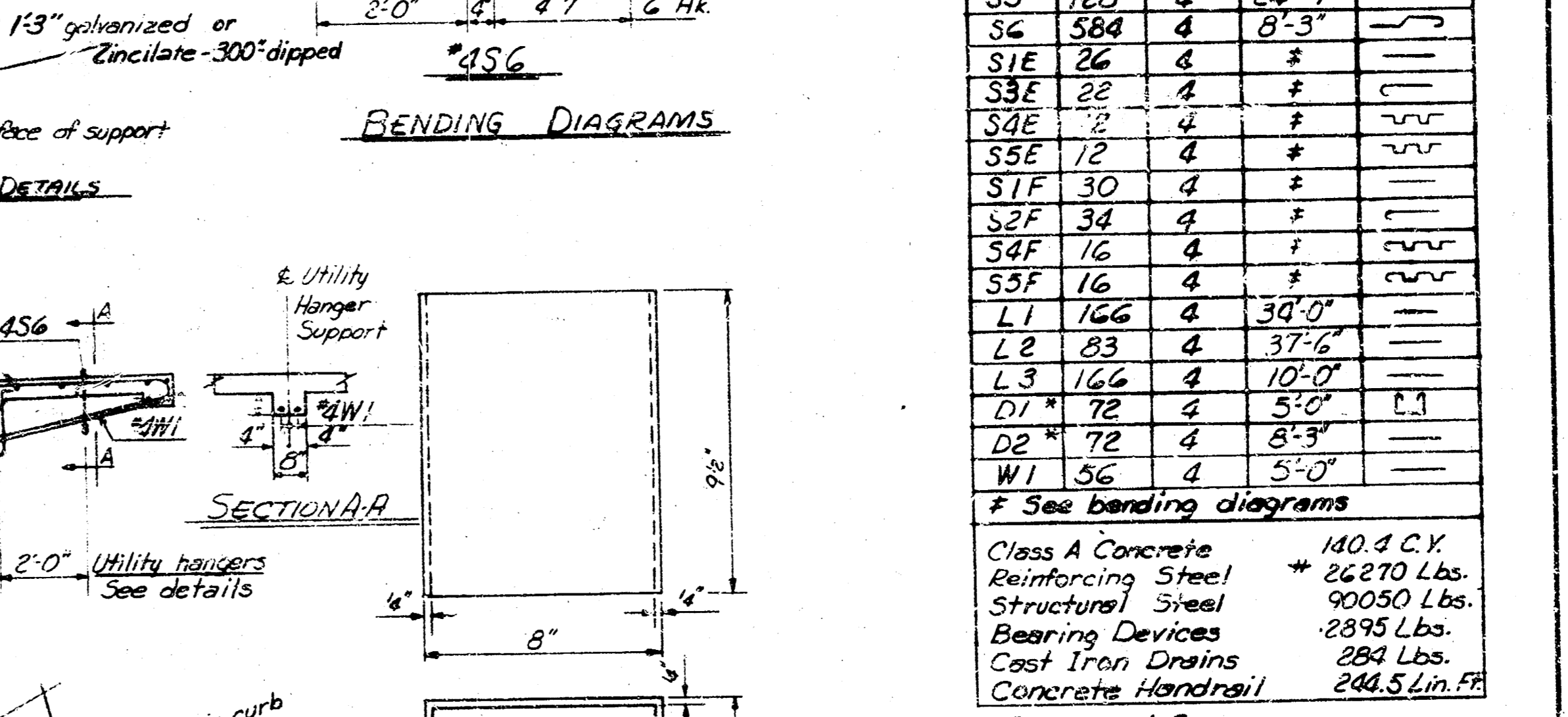
PLAN OF SLAB REINFORCING
 Scale: 1/8" = 1'-0"

NOTE: Galvanized hangers shall not be paid for directly but shall be subsidiary to the bid item "Class A Concrete."



BENDING DIAGRAMS

NOTE: All dimensions in the bending diagrams are cut to cut of bars. Refer to AASHTO Manual of Recommended Practice for Detailing Reinforced Concrete Highway Structures.



Mark	No.	Size	Length	Shape
S1	25C	4	23'-3"	—
S2	137	4	27'-0"	—
S3	115	4	19'-8"	—
S4	128	4	24'-9"	—
S5	128	4	24'-9"	—
S6	584	4	8'-3"	—
S7E	26	4	#	—
S8E	26	4	#	—
S9E	2	4	#	—
S10E	12	4	#	—
S11F	30	4	#	—
S12F	34	4	#	—
S13F	16	4	#	—
S14F	16	4	#	—
S15F	16	4	#	—
L1	166	4	39'-0"	—
L2	83	4	37'-6"	—
L3	166	4	10'-0"	—
D1	72	4	5'-0"	—
D2	72	4	8'-3"	—
W1	56	4	5'-0"	—

See bending diagrams

Class A Concrete 140.4 C.Y.
 Reinforcing Steel 26270 Lbs.
 Structural Steel 90050 Lbs.
 Bearing Devices 2895 Lbs.
 Cast Iron Drains 284 Lbs.
 Concrete Handrail 244.5 Lin. Ft.

* See sheet G
 # Includes Superstructure Handrail Reinforcing Steel - See Sheet G

General Note:
 All concrete shall be Class A. Bevel all exposed edges with a 3/4" triangular mauling unless otherwise noted. See Sheet G for Handrail Details.

**GYPSUM CREEK
 SUPERSTRUCTURE DETAILS
 MT. VERNON ROAD**

R. S. DELAMATER
 CONSULTING ENGINEER
 WICHITA, KANSAS

DATE October, 1952
 SCALE 1/8" = 1'-0"
 DRAWING No. 7977

