

# CITY OF WICHITA, KANSAS

## MICHAEL E. LINDEBAK, P.E., CITY ENGINEER

# STREET IMPROVEMENTS

**25TH ST. NORTH - FROM THE E.L. OF GLENDALE TO A POINT 43 FT. WEST OF THE E.L. OF LOT 3, BLOCK 1, PRAIRIE HILLS ADDITION.**  
**TURNAROUNDS TO BE CONSTRUCTED IN 25TH ST. NORTH AT BLECKLEY AND BATTIN.**

## IN 25TH STREET NORTH

**CITY OF WICHITA PROJECT NO. 472-76-245-82429-000-000-001**  
**INDEX NO. 765420**

**INDEX OF SHEETS**

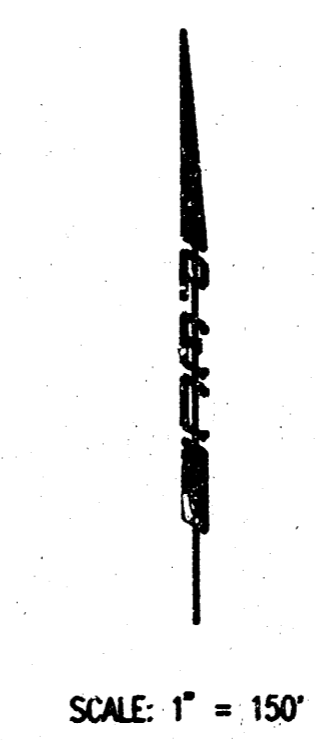
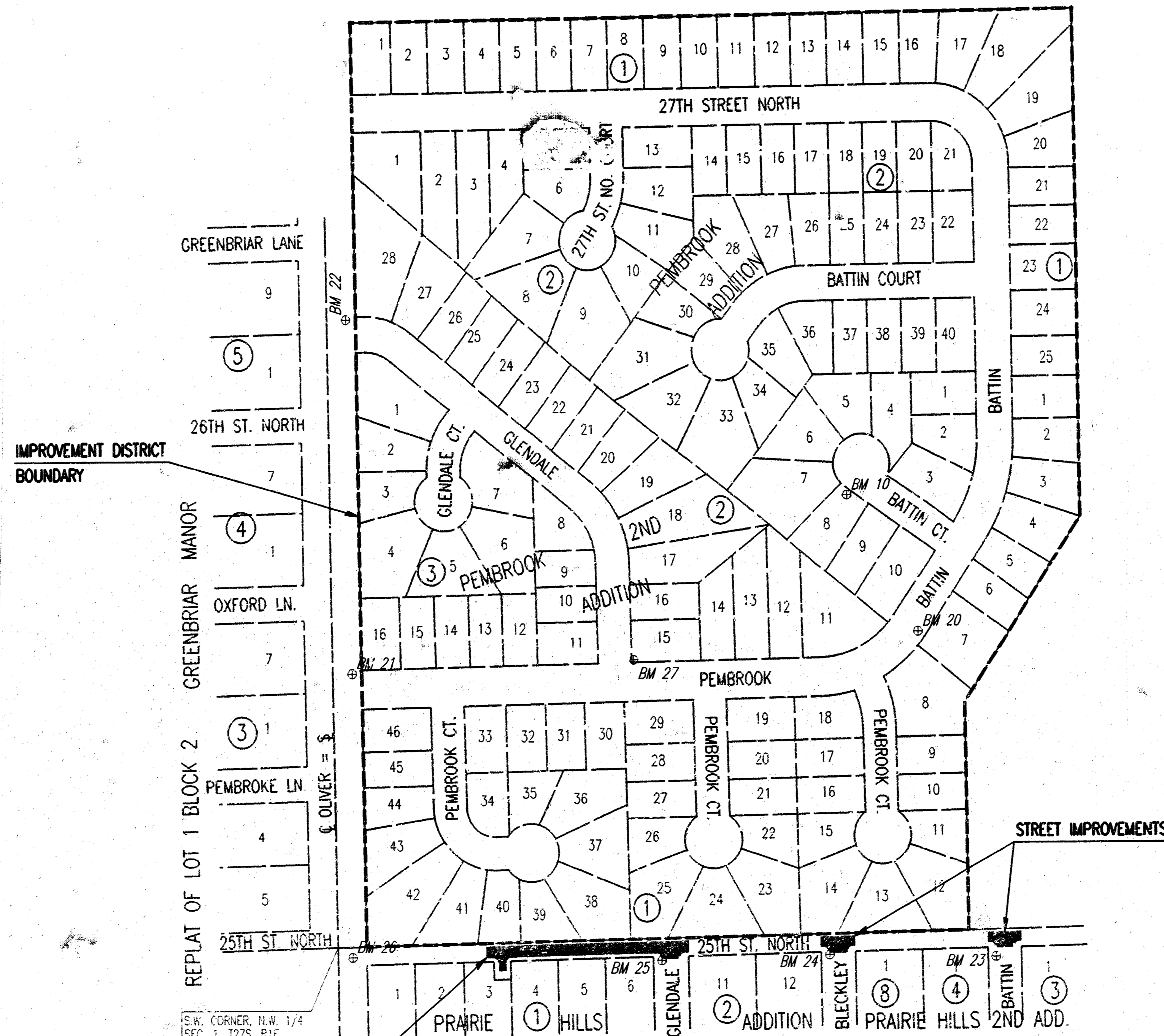
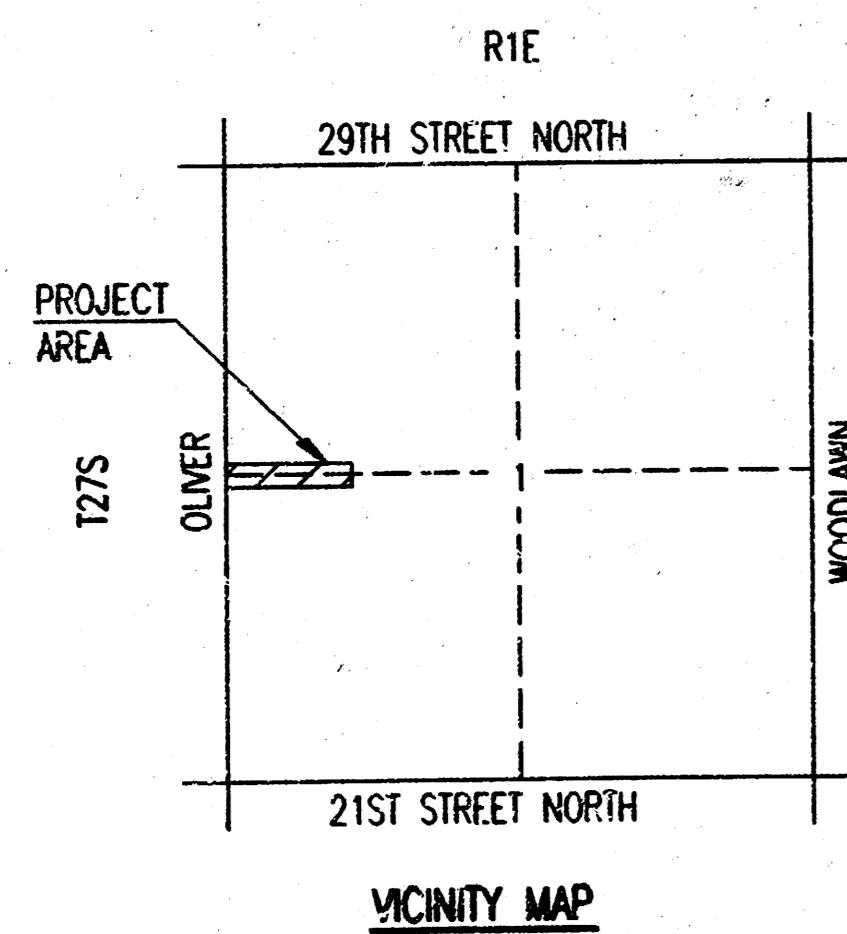
1. TITLE SHEET
2. PLAN
3. TYPICAL SECTIONS
4. PAVING PLAN-25TH ST. N.
5. PAVING PLAN-25TH ST. N./BLECKLEY
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**PROJECT SURVEY CONTROL**

- BM 10 - "T" POST APPROXIMATELY 5' SOUTH OF P.C., LOT 8, BLOCK 2, PEMBROOK 2ND ADDITION. ELEV. = 175.55
- BM 20 - CHISELED "d" EAST BACK OF CURB, NEAR PC OF CURB, NORTHEAST SIDE OF PEMBROOK STREET LOT 7, BLOCK 1, PEMBROOK 2ND ADDITION. ELEV. = 177.02
- BM 21 - CHISELED "d" ON MIDPOINT OF CURB @ THE NORTHEAST CORNER OF PEMBROOK STREET AND OLIVER, PEMBROOK 2ND ADDITION. ELEV. = 169.96
- BM 22 - CHISELED "d" ON MIDPOINT OF CURB @ THE NORTHEAST CORNER OF GLENDALE AND OLIVER, PEMBROOK 2ND ADDITION. ELEV. = 165.96
- BM 23 - CHISELED "d" NORTHEAST CORNER CONCRETE DRIVE OF HOUSE #2551 NORTH BATTIN SOUTH OF HEDGECROW @ NORTH END OF EXISTING STREET @ HOUSE #2525, SOUTH SIDE OF HEDGECROW, PRAIRIE HILLS. ELEV. = 176.44
- BM 24 - CHISELED "d" WEST TOP OF CURB BLECKLEY @ NORTH END OF EXISTING STREET @ HOUSE #2525, SOUTH SIDE OF HEDGECROW, PRAIRIE HILLS. ELEV. = 177.53
- BM 25 - CHISELED "d" WEST TOP OF CURB GLENDALE @ NORTH END OF EXISTING STREET @ HOUSE #4821 EAST 25TH STREET NORTH, SOUTH SIDE OF HEDGECROW, PRAIRIE HILLS. ELEV. = 181.71
- BM 26 - CHISELED "d" EAST TOP OF CURB OLIVER, @ E. OF 25TH STREET NORTH. ELEV. = 175.18
- BM 27 - "T" POST 5' NORTHEAST OF THE SOUTHWEST CORNER OF LOT 15, BLOCK 2, PEMBROOK 2ND ADDITION. ELEV. = 178.52

**EARTHWORK**

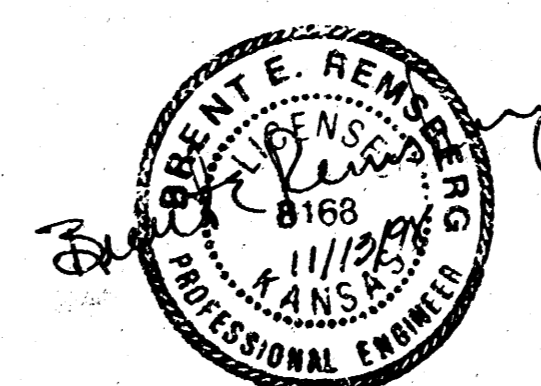
EXCAVATION 711 CU. YDS.  
 COMPACTED FILL 0 CU. YDS.  
 PROJECT LENGTH = 477.50 LIN. FT.



**NOTE: 2" GAS LINE SHOWN ON SHEET NO. 4 IS TO BE RELOCATED PRIOR TO OR DURING CONSTRUCTION BY KANSAS GAS SERVICE - DRCS.**

**GENERAL NOTES**

- UNDERGROUND UTILITY SERVICE LINES AND OVERHEAD UTILITY POLE LINES ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.
- TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED BY THE CONTRACTOR WITH THE ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.
- RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR AND APPROVED AS NOTED BELOW.
- ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WOULD REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS WOULD REQUIRE ADDITIONAL ARCHEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION. EXCESS EARTHWORK SHALL BE STOCKPILED ON SITE AS DIRECTED BY THE DEVELOPER (MR. LARRY CHAMBERS (316) 263-3201).
- CONTRACTOR SHALL SATISFY HIMSELF OF SURFACE AND SUBSURFACE CONDITIONS PRIOR TO BIDDING.
- TEMPORARY SURFACING MATERIAL (ROCK, ASPHALT, ETC.) MAY HAVE BEEN PLACED WITHIN STREET RIGHTS-OF-WAY FOR HAUL ROADS AND TEMPORARY ACCESS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS TO DETERMINE EXTENT, IF ANY, OF SUCH SURFACING. CONTRACTOR SHALL REMOVE SAID TEMPORARY SURFACING IN THE SAME MANNER AS NOTED ABOVE FOR RUBBLE. THIS REMOVAL SHALL BE SUBSIDIARY TO "EXCAVATION".
- CONTRACTOR SHALL PROVIDE A MINIMUM FORTY-EIGHT (48) HOUR ADVANCE NOTICE (EXCLUDING WEEKENDS AND HOLIDAYS) PRIOR TO BEGINNING ANY EXCAVATION TO KANSAS ONE-CALL SYSTEM, A UTILITY LOCATION SERVICE, AT (316) 687-2470 TO REQUEST THE FOLLOWING UTILITY COMPANIES TO LOCATE ALL EXISTING LINES WITHIN THE PROJECT AREA: K.S.S. GAS, K.C.E. ELECTRIC, SOUTHWESTERN BELL TELEPHONE, MULTIMEDIA CABLEVISION, CITY OF WICHITA SEWER MAINTENANCE AND CITY OF WICHITA WATER DEPARTMENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS.
- THE WATER DEPARTMENT SHALL FIELD LOCATE WATER VALVES ONE TIME DURING CONSTRUCTION WHEN REQUESTED BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PRESERVE SUCH FIELD LOCATIONS DURING THE CONSTRUCTION PROCESS. WATER VALVES, WATER VALVE BOXES OR FIRE HYDRANTS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL ADJUST WATER VALVE BOXES AS DIRECTED BY THE ENGINEER. THIS WORK TO BE SUBSIDIARY TO OTHER BID ITEMS.
- INLET HOOKUPS AND INLET PROTECTION SHALL BE BID PER EACH REGARDLESS OF INLET SIZE.
- THE CONTRACTOR SHALL ADJUST MAIL BOXES AS DIRECTED IN CITY OF WICHITA STANDARD SPECIFICATIONS.
- THIS PROJECT INCLUDES A CERTAIN AMOUNT OF ROLL TYPE CURB CONSTRUCTION. ROLL CURBS SHALL BE DERESSED THROUGH ALL DRIVEWAY OPENINGS WHEN SUCH DRIVES ARE CONSTRUCTED AS A PART OF THE PROJECT. NO MORE THAN 4 DRIVES 70 FEET IN WIDTH OR EQUIVALENT COMBINATIONS THEREOF, ARE TO BE CONSTRUCTED WITH THIS PROJECT. CONTRACTOR TO OBTAIN SIGNED DRIVEWAY REQUEST FORMS FROM LANDOWNERS (3) PRIOR TO CONSTRUCTION. DRIVES.
- THE CONTRACTOR SHALL SEED, FERTILIZE AND MULCH ALL UNPAVED AREAS DISTURBED BY HIS CONSTRUCTION (INCLUDING FRESH EARTHWORK STOCKPILES) ACCORDING TO THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE PAID FOR AT THE LUMP SUM PRICE BID FOR "SEEDING/FERTILIZING/MULCHING".



**NOVEMBER 1998**

**PLANS PREPARED BY**  
**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
**ENGINEERS**  
**WICHITA, KANSAS**

1:1998/082375/25-Title.dgn  
 date plotted: november 13, 1998  
 deliver to: dwaino dunn

# PEMBROOK 2ND ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82429-000-001	2	13



B.M. - DISC 29' NORTH AND 33' EAST OF CENTERLINE OLIVER AND 29TH STREET NORTH.  
ELEV.=159.68 CITY DATUM

B.M. - RAILROAD SPIKE IN S.W. FACE OF POWER POLE 26.5' NORTH OF 1/16 CORNER, 1/4 MILE EAST OF 29TH STREET NORTH AND OLIVER.  
ELEV.=161.18 CITY DATUM

MINIMUM PAD ELEVATION (LOWEST OPENING) AS FOLLOWS:

BLOCK 1  
LOT 1 ELEV. 167.00 CITY DATUM  
LOT 2 ELEV. 168.00 CITY DATUM  
LOTS 3 AND 4 ELEV. 169.00 CITY DATUM  
LOTS 5 THROUGH 7 ELEV. 170.00 CITY DATUM  
LOTS 8 AND 9 ELEV. 171.00 CITY DATUM  
LOTS 10 THROUGH 12 ELEV. 172.00 CITY DATUM

THIS ADDITION IS SUBJECT TO THE REQUIREMENTS OF THE BEACON HILL, C.U.P., DP-147 ON FILE WITH THE METROPOLITAN AREA PLANNING DEPARTMENT.

SCALE: 1"=100'

○ = IRON SET  
● = IRON FOUND  
CALC. MEAS. = CALCULATED FROM MEASUREMENT  
MEAS. = MEASURED

S.S. = SANITARY SEWER  
B.S.L. = BUILDING SETBACK LINE



25TH STREET NORTH

PLAT

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.

ENGINEERS  
WICHITA, KANSAS

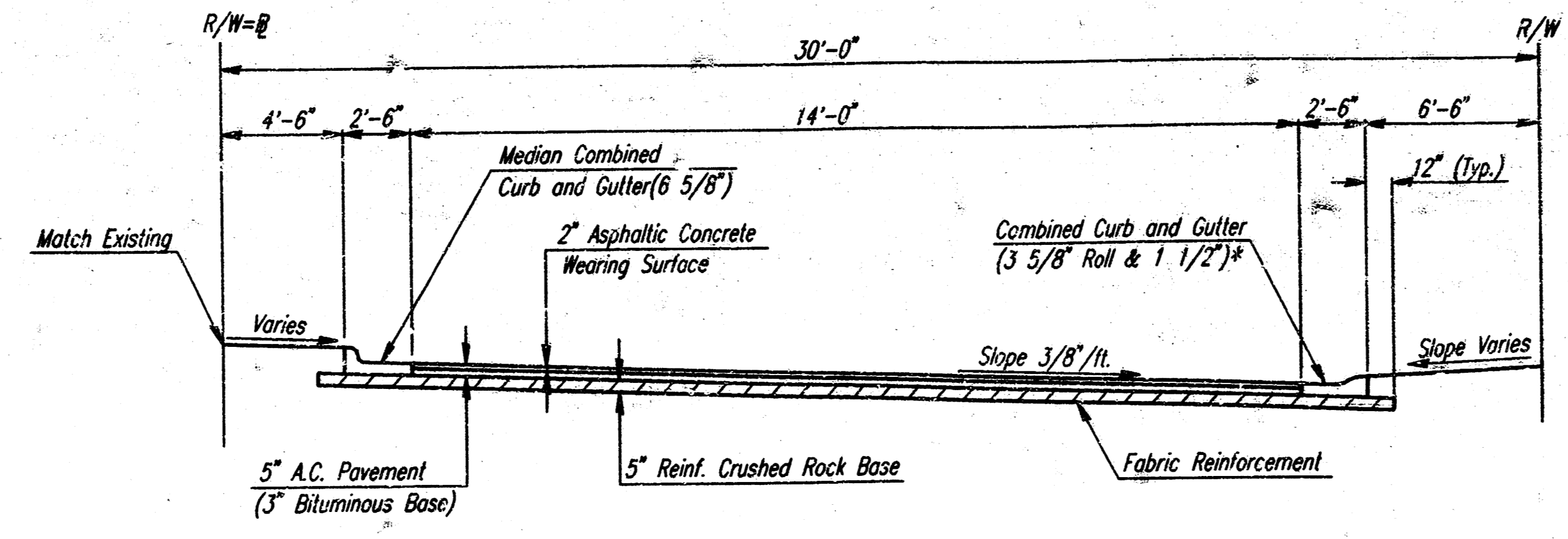
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DEP		OCT. 1998	98375

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 date plotted: october 26, 1998  
 deliver to: dwalton adm

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82429-000-001	3	13

**COORDINATE LIST**

POINT	N	E
12	4999.3552	4870.0053
20	5023.2446	6081.1458
42	5010.4718	5487.0607
43	5016.8773	5784.9919
52	5011.8690	5552.0457
79	5018.2745	5849.9769
105	5004.6215	5214.9514
107	5006.2766	5291.9336
109	5008.6410	5401.9082
434	5027.5436	6281.0996
435	4996.4140	5815.3350
437	4989.5010	5881.0860
438	5002.1990	6126.9530
439	4994.7780	6115.3670
441	4977.3540	5264.1240
442	5003.9860	5499.4330
444	4983.2230	5387.0960
448	5011.9718	5556.8291
450	5018.2952	5850.9388
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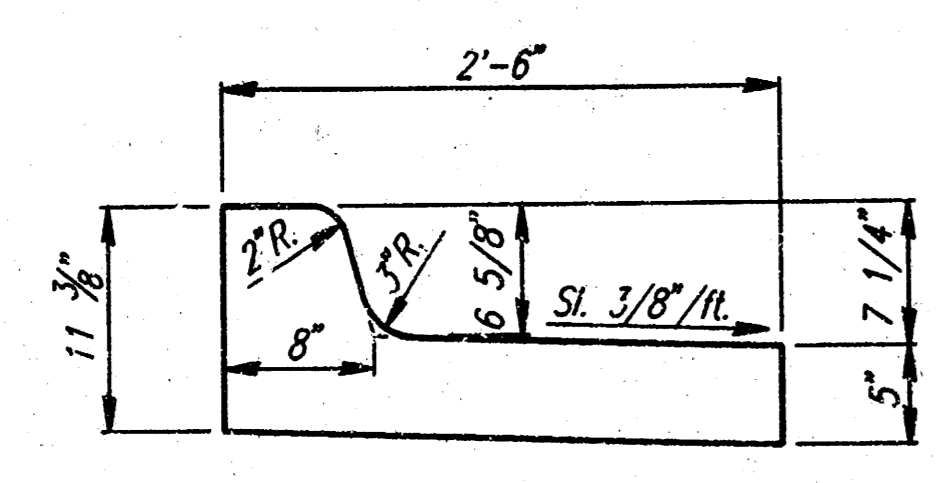
**TYPICAL SECTION**  
Sta. 2+59.6 to Sta. 6+17.1

\* Except Sta. 2+59.6 to Sta. 2+65.1  
Which Shall be "Combined Curb and Gutter (6 5/8" & 1 1/2")"

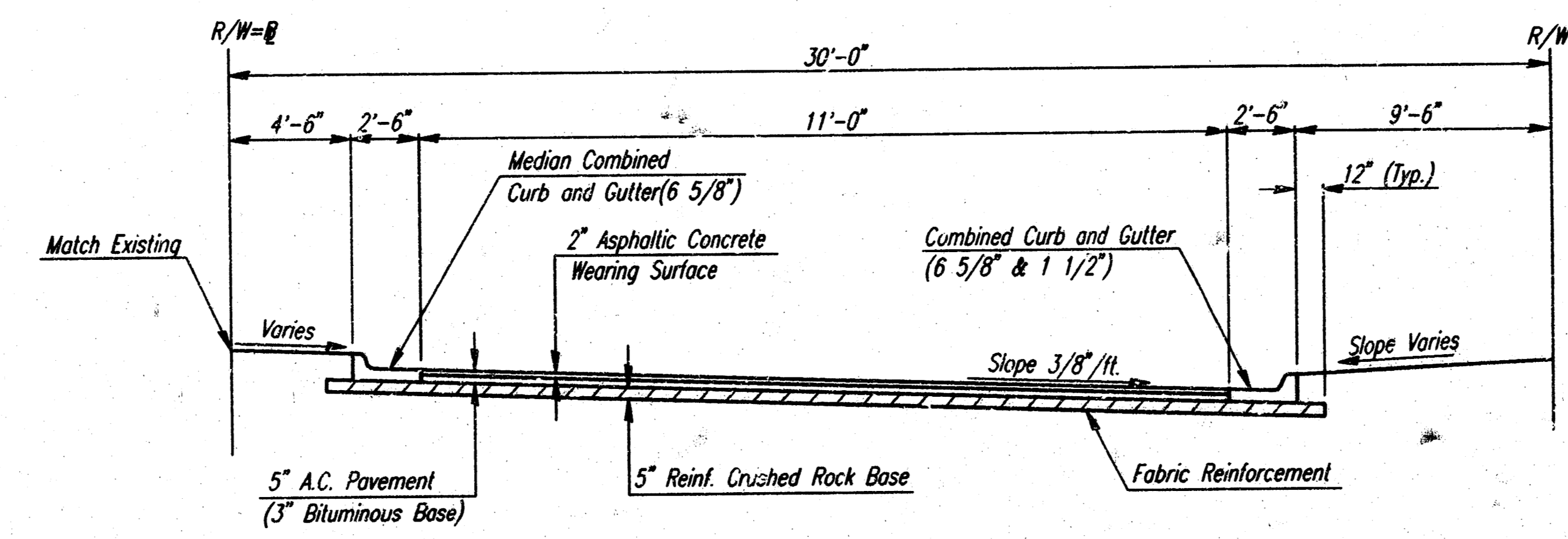
**CRUSHED ROCK GRADATION REQUIREMENTS PERCENT OF AGGREGATE RETAINED**

2-1/2"	0
3/4"	20-40
1/4"	50-80
#40	80-94
#200	90-98

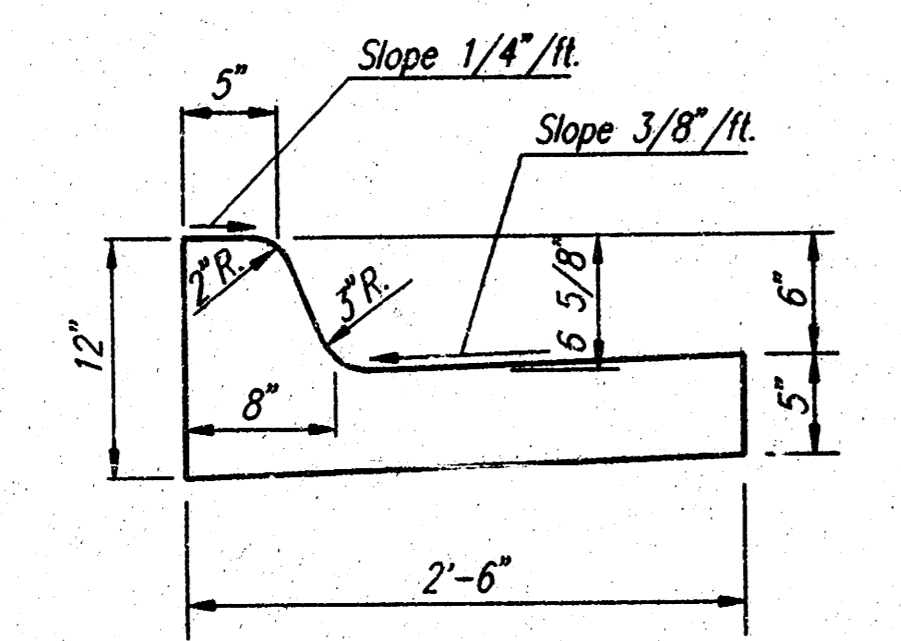
ROCK QUALITY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY THE KDOT 1990 EDITION STANDARD SPECIFICATION SUBSECTION 1102 FOR DURABILITY CLASS 1



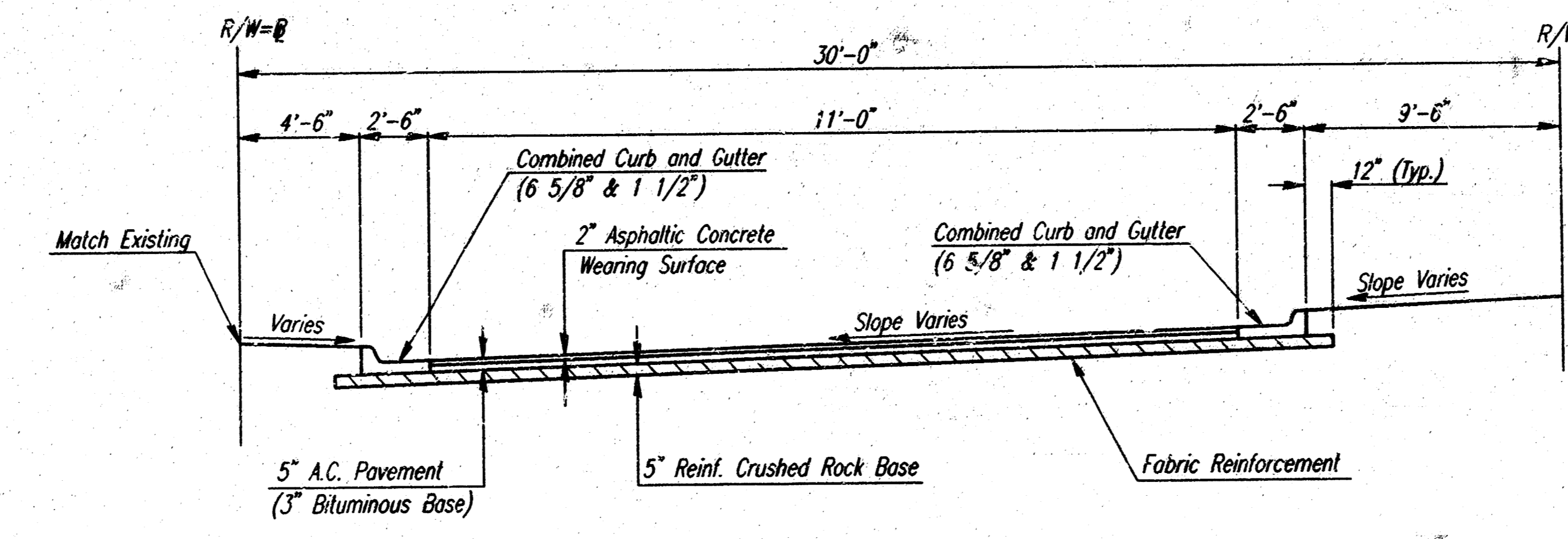
**MEDIAN COMBINED CURB & GUTTER (6 5/8")**



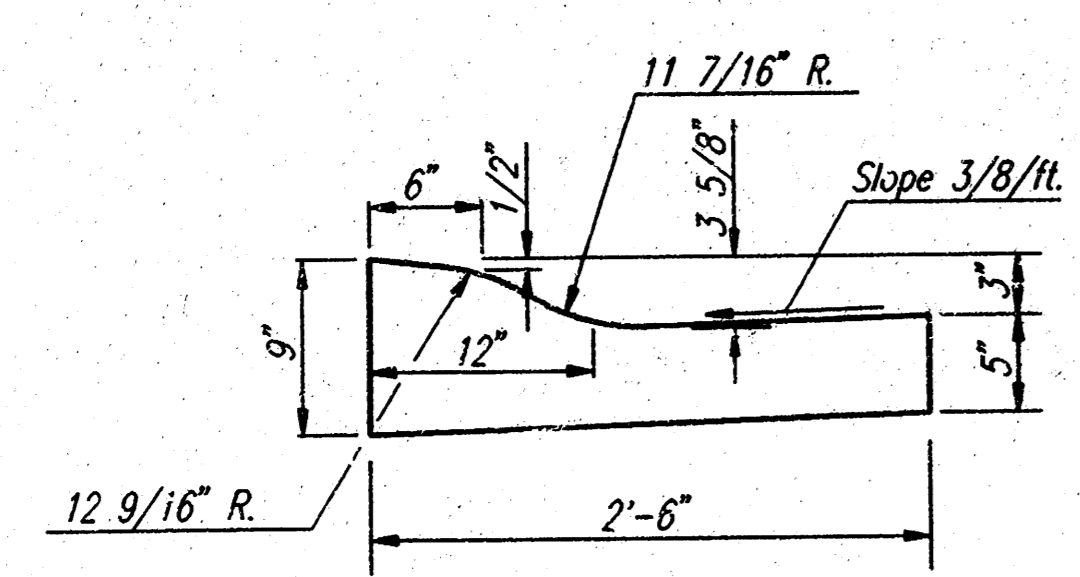
**TYPICAL SECTION**  
Sta. 8+50.06 to Sta. 9+11.06



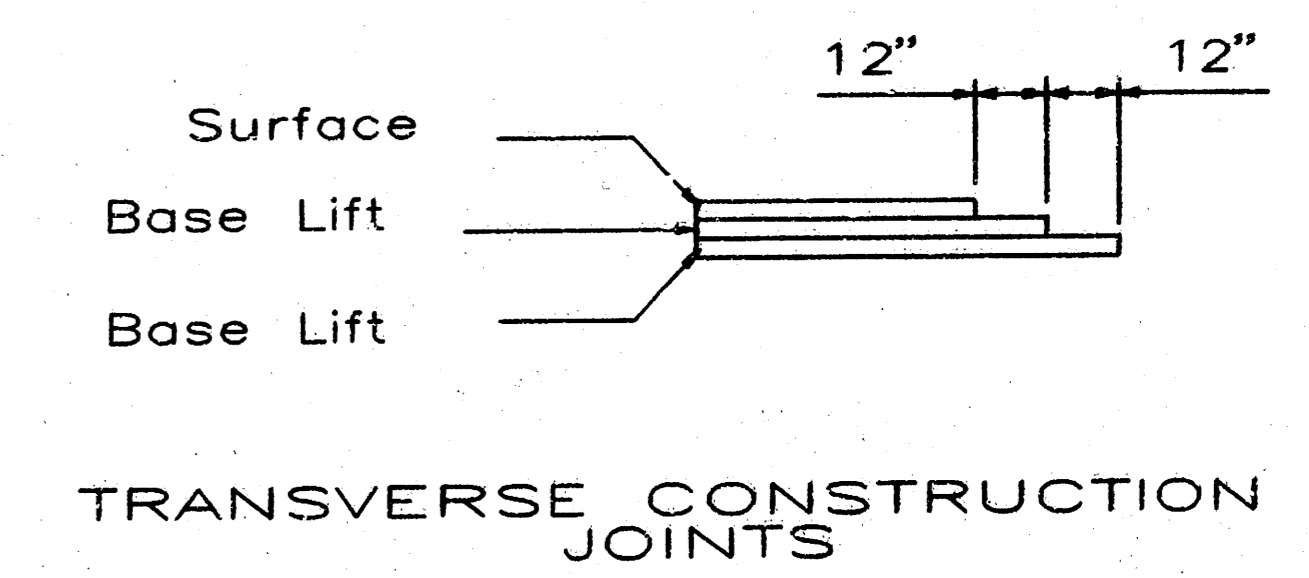
**COMBINED CURB & GUTTER**



**TYPICAL SECTION**  
Sta. 11+44.12 to Sta. 12+05.12



**COMBINED ROLL TYPE CURB & GUTTER**



Transverse construction joints shall be constructed in flexible base pavements at locations where pavement joints existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

**General Notes**

- FABRIC BASE REINFORCEMENT SHALL BE B X 1100 GEOGRID AS MANUFACTURED BY TENSAR CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.
- A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

25TH STREET NORTH

**TYPICAL SECTIONS**

**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
ENGINEERS  
WICHITA, KANSAS

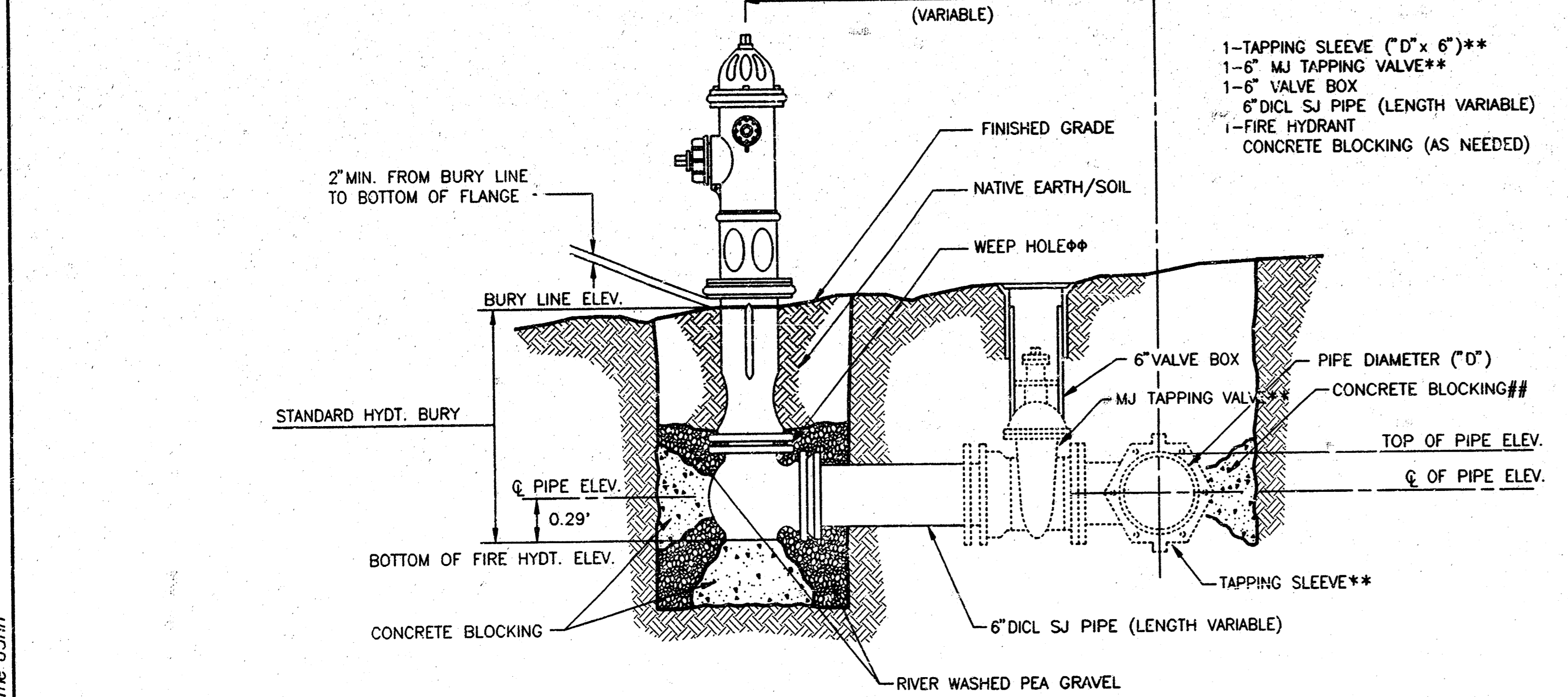
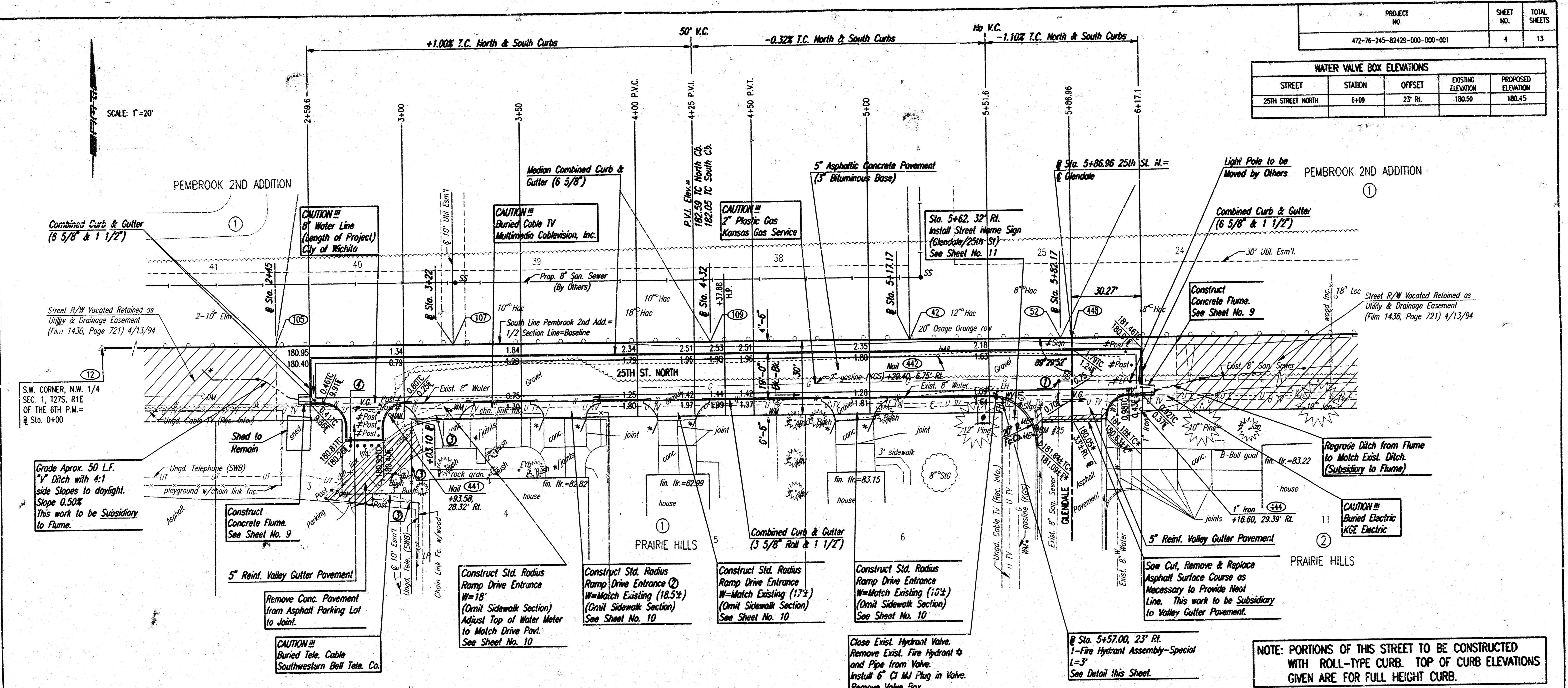
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Drawn by	BJS	Date	NOV. 1998

Sheet No. 98375

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 date plotted: november 13, 1998  
 deliver to: chrisne dunn

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82429-000-001	4	13

WATER VALVE BOX ELEVATIONS				
STREET	STATION	OFFSET	EXISTING ELEVATION	PROPOSED ELEVATION
25TH STREET NORTH	6+09	23' RL	180.50	180.45



**FIRE HYDRANT ASSEMBLY - SPECIAL**  
 TO BE INSTALLED IN ACCORDANCE WITH CITY OF WICHITA, KANSAS  
 STANDARD SPECIFICATIONS FOR WATERLINE CONSTRUCTION.

**NOTE: PORTIONS OF THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.**

ADJUST WATER METER TOPS TO MATCH FINISHED GRADE AS PART OF "SITE RESTORATION".

NOTE: SEE SHEET NO. 7 FOR ADDITIONAL REMOVAL NOTES AT GLENDALE INTERSECTION

"FIRE HYDRANT ASSEMBLY-SPECIAL" SHALL BE BID AS A LUMP SUM WHICH SHALL BE FULL COMPENSATION FOR ALL WORK TO CLOSE AND REMOVE EXIST. FIRE HYDRANT ASSEMBLY AS STATED ON THIS SHEET PLUS ALL MATERIALS, TOOLS, EQUIPMENT, LABOR, ETC. TO COMPLETE INSTALLATION OF NEW FIRE HYDRANT AS SHOWN.

1) ADJUST EXISTING SANITARY SEWER MANHOLE RING (APPROX. +0.06') AND SET TO MATCH PROPOSED GRADE AND CROSS SLOPE (3/8' FT.). THIS WORK TO BE SUBSIDIARY TO ASPHALT PAVEMENT.

2) CONSTRUCT DRIVE ENTRANCE INSIDE EXISTING CHAIN LINK FENCE POSTS.

3) REMOVE CHAIN LINK FENCE & GATE WEST OF PROPERTY LINE (+03.10) AND SALVAGE FOR OWNER. ALSO REMOVE CHAIN LINK FENCE TO EAST SIDE OF FIRST DRIVE. SET SALVAGED END POST AS DIRECTED BY THE ENGINEER. CONTRACTOR SHALL INSTALL (USING SALVAGED MATERIALS) CHAIN LINK FENCE AT PROPERTY LINE (+03.10) FROM 30' RT. TO 64' RT.± AND CONNECT TO EXISTING FENCE AT SOUTH END (64' RT.±). FENCE REMOVAL SHALL BE PART OF "CLEARING R/W AND SITE PREP.". INSTALLING FENCE ON PROPERTY L.P.± SHALL BE PART OF "SITE RESTORATION".

4) CONTRACTOR TO SET 5 GUARD POSTS APPROX. 3' APART AND 24" SOUTH OF CURB AS SHOWN. POSTS TO BE SALVAGED OR NEW 4" STEEL PIPE FILLED WITH CONC. 30" ABOVE GROUND. THIS WORK TO BE PART OF "SITE RESTORATION".

453) COORDINATE POINT NUMBER SEE SHEET NO. 3 FOR COORDINATE LIST.

★ FIRE HYDRANT TO BE RELOCATED.

# MATCH EXISTING

≠ REMOVE THESE ITEMS AS PART OF THE BID ITEM "CLEARING R/W AND SITE PREP.". SALVAGE SIGNS AND RESET (EXCEPT STREET NAME SIGN) AS DIRECTED BY THE ENGINEER. RESETTING SIGNS SHALL BE SUBSIDIARY TO "CLEARING R/W AND SITE PREP.".

♣ 5' TRANSITIONS FROM COMBINED CURB & GUTTER (3 5/8" ROLL & 1 1/2") TO MONOLITHIC EDGE CURB (6 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB & GUTTER (3 5/8" ROLL & 1 1/2)".

25TH STREET NORTH  
 STA. 2+59.6 TO STA. 6+17.1

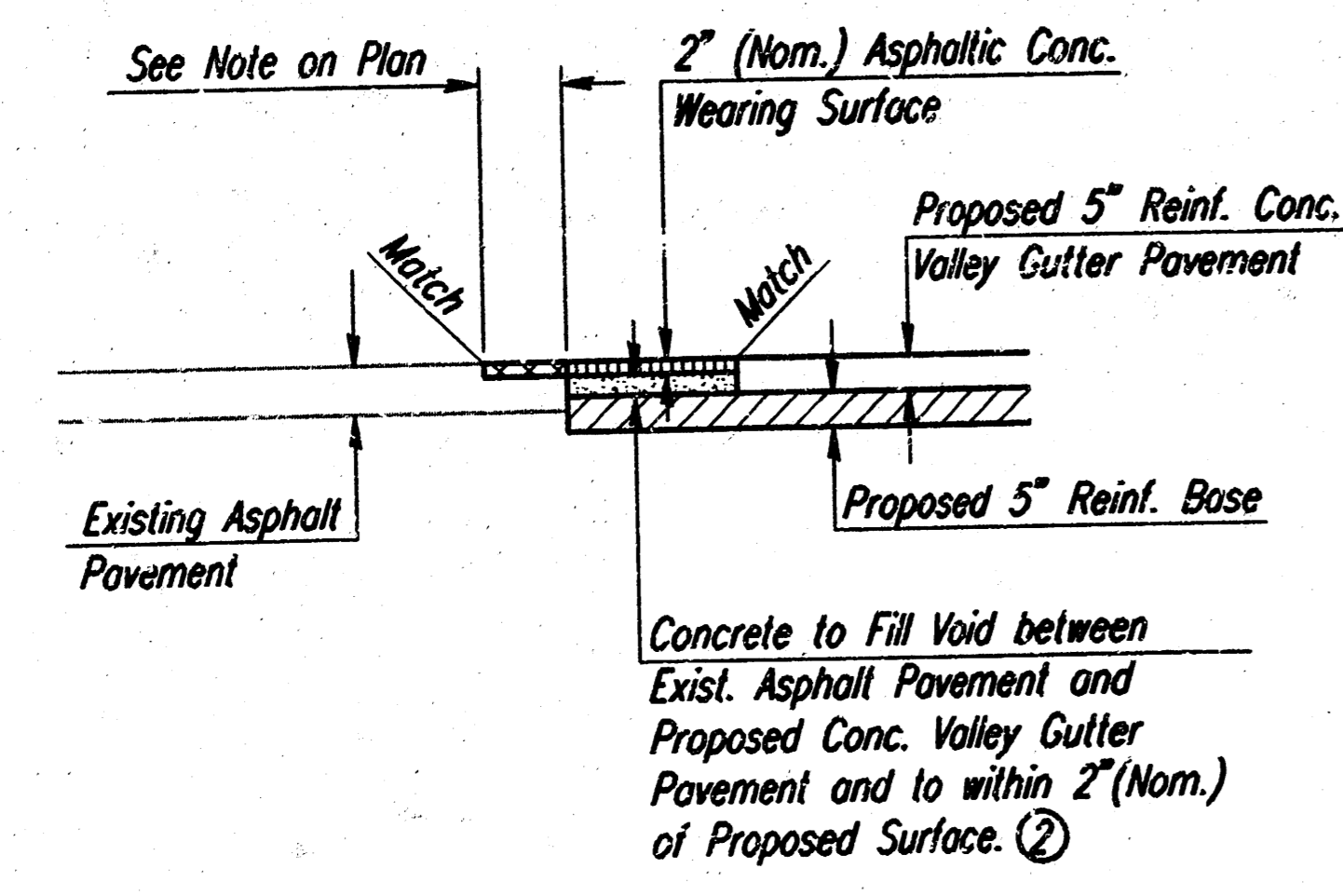
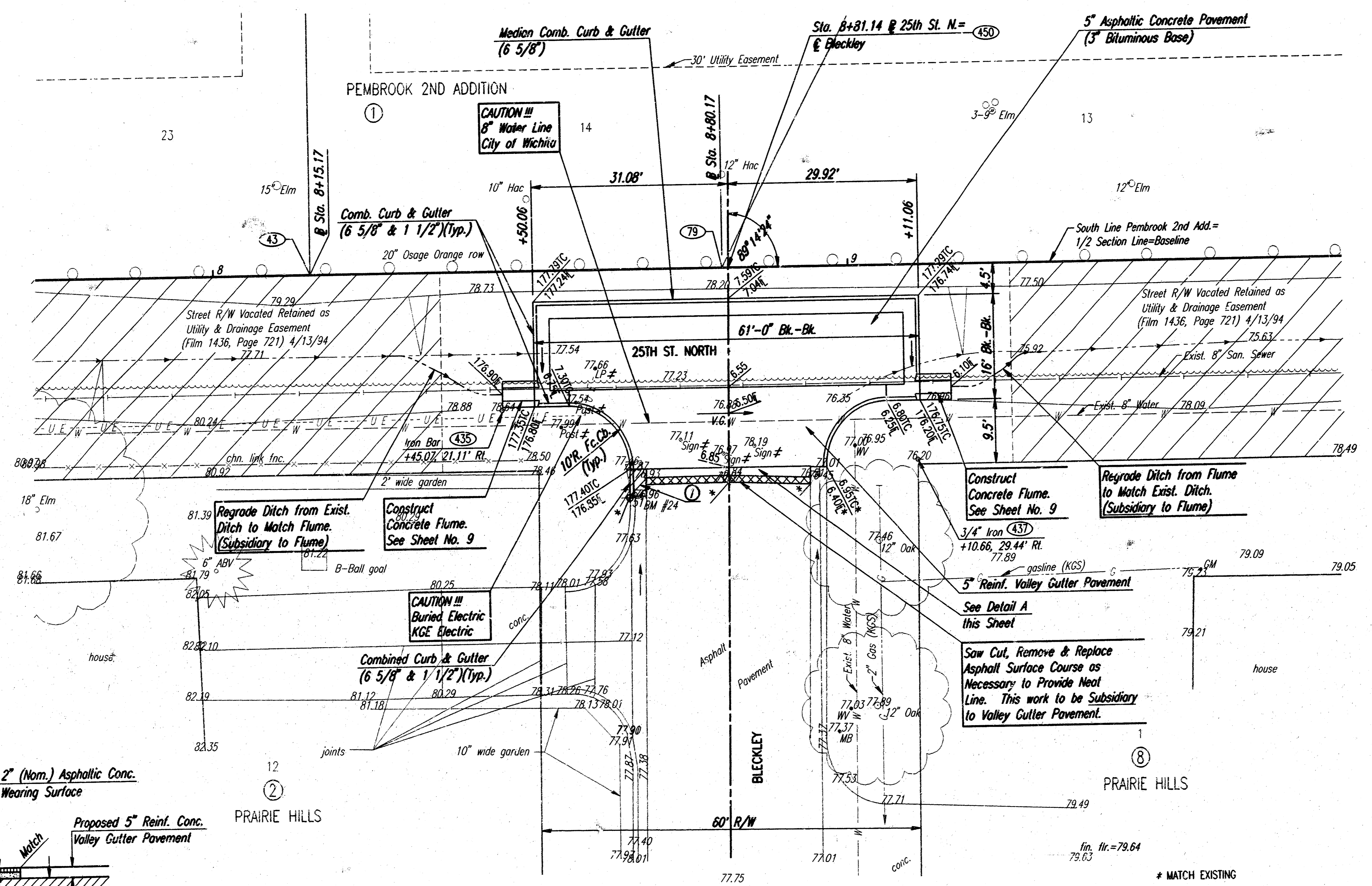
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 ENGINEERS  
 WICHITA, KANSAS

Designed by	BER, GDD	Checked by	
Drawn by	BJS	Date	NOV. 1998
		Job No.	98375

11/1998/98375/25thplan.dgn  
 date plotted: november 16, 1998  
 deliver to: dwaine dunn

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-62429-000-001	5	13

SCALE: 1"=10'



**DETAIL A**  
This work to be Subsidiary to 5" Reinf. Valley Gutter Pavement

- ① REMOVE CURVED SECTION OF COMBINED CURB & GUTTER TO JOINT. SAW CUT EXISTING PAVEMENT AT NEW HIGH EDGE ALIGNMENT. SAW CUT TO BE SUBSIDIARY TO "COMBINED CURB & GUTTER REMOVED".
- ② CONCRETE AND 2" (NOM.) ASPHALTIC CONCRETE WEARING SURFACE TO BE SUBSIDIARY TO "5" REINF. VALLEY GUTTER PAVEMENT".
- \* MATCH EXISTING
- ≠ REMOVE POSTS, REMOVE & SALVAGE SIGNS TO BE RESET BY CONTRACTOR FOLLOWING CONSTRUCTION OF IMPROVEMENTS AS DIRECTED BY THE ENGINEER. LIGHT POLE TO BE REMOVED BY OTHERS DURING OR PRIOR TO CONSTRUCTION. THESE ITEMS TO BE CONSIDERED PART OF "CLEARING R/W & SITE PREP."
- ④53 COORDINATE POINT NUMBER SEE SHEET NO. 3 FOR COORDINATE LIST.

25TH STREET NORTH

## 25TH ST. N./BLECKLEY

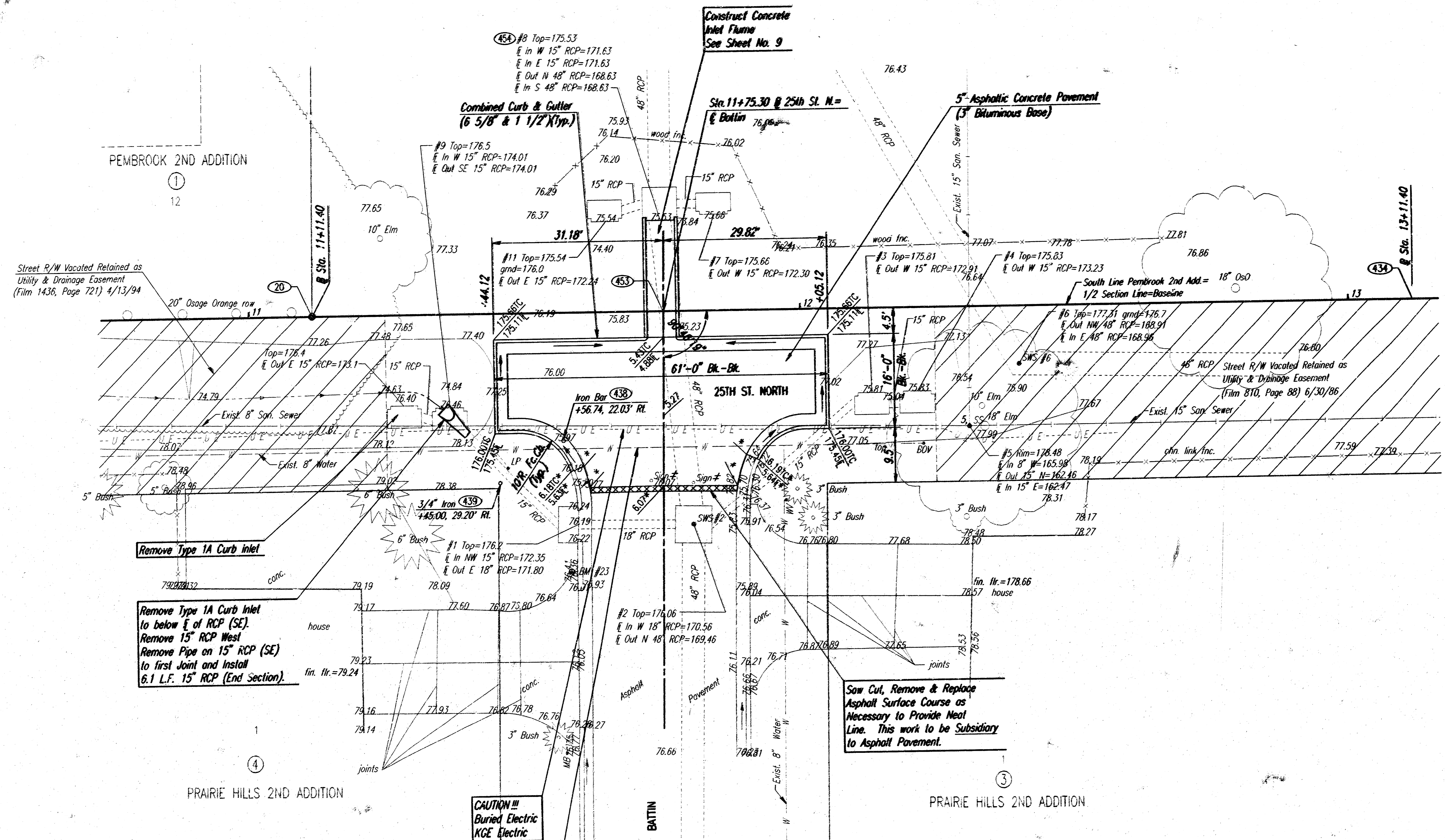
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ENGINEERS  
WICHITA, KANSAS

Designed by: BER, GDD	Checked by:
Drawn by: BJS	Date: NOV. 1998
	Job No. 98375

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Last printed November 16, 1998  
deliver to dwgline.dwg

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82425-000-001	8	13

SCALE: 1"=10'



PEMBROCK 2ND ADDITION

PRAIRIE HILLS 2ND ADDITION

PRAIRIE HILLS 2ND ADDITION

Remove Type 1A Curb Inlet to below 1/2 of RCP (SE). Remove 15\"/>

Remove Type 1A Curb Inlet to below 1/2 of RCP (SE). Remove 15\"/>

Saw Cut, Remove & Replace Asphalt Surface Course as Necessary to Provide Neat Line. This work to be Subsidiary to Asphalt Pavement.

CAUTION !!! Buried Electric KGE Electric

CAUTION !!! 8\"/>

# MATCH EXISTING

REMOVE & SALVAGE SIGNS TO BE RESET BY CONTRACTOR FOLLOWING CONSTRUCTION OF IMPROVEMENTS AS DIRECTED BY THE ENGINEER. THESE ITEMS TO BE CONSIDERED PART OF "CLEARING R/W & SITE PREP."

(453) COORDINATE POINT NUMBER SEE SHEET NO. 3 FOR COORDINATE LIST.

25TH STREET NORTH

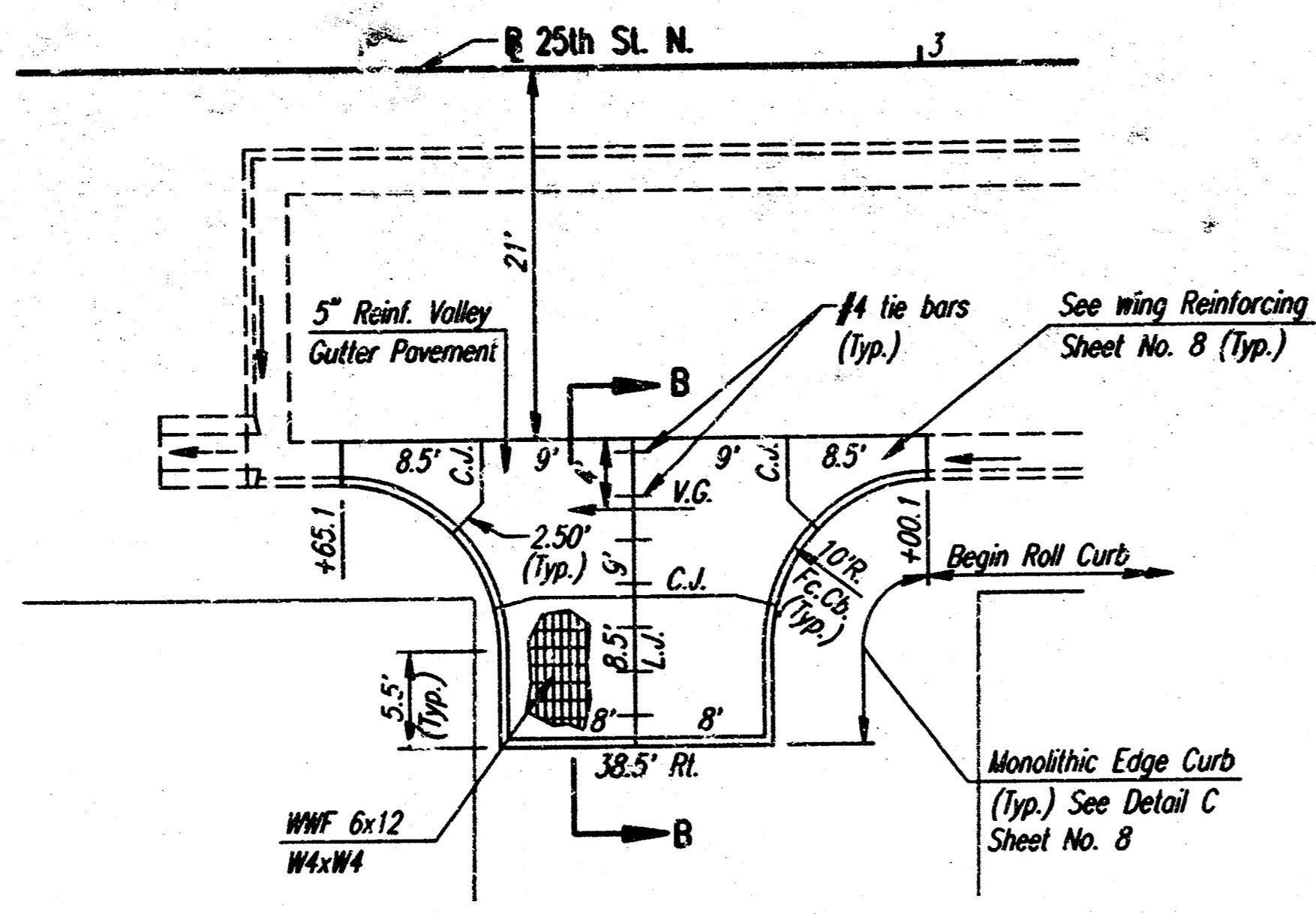
## 25TH ST. N./BATTIN

**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
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WICHITA, KANSAS

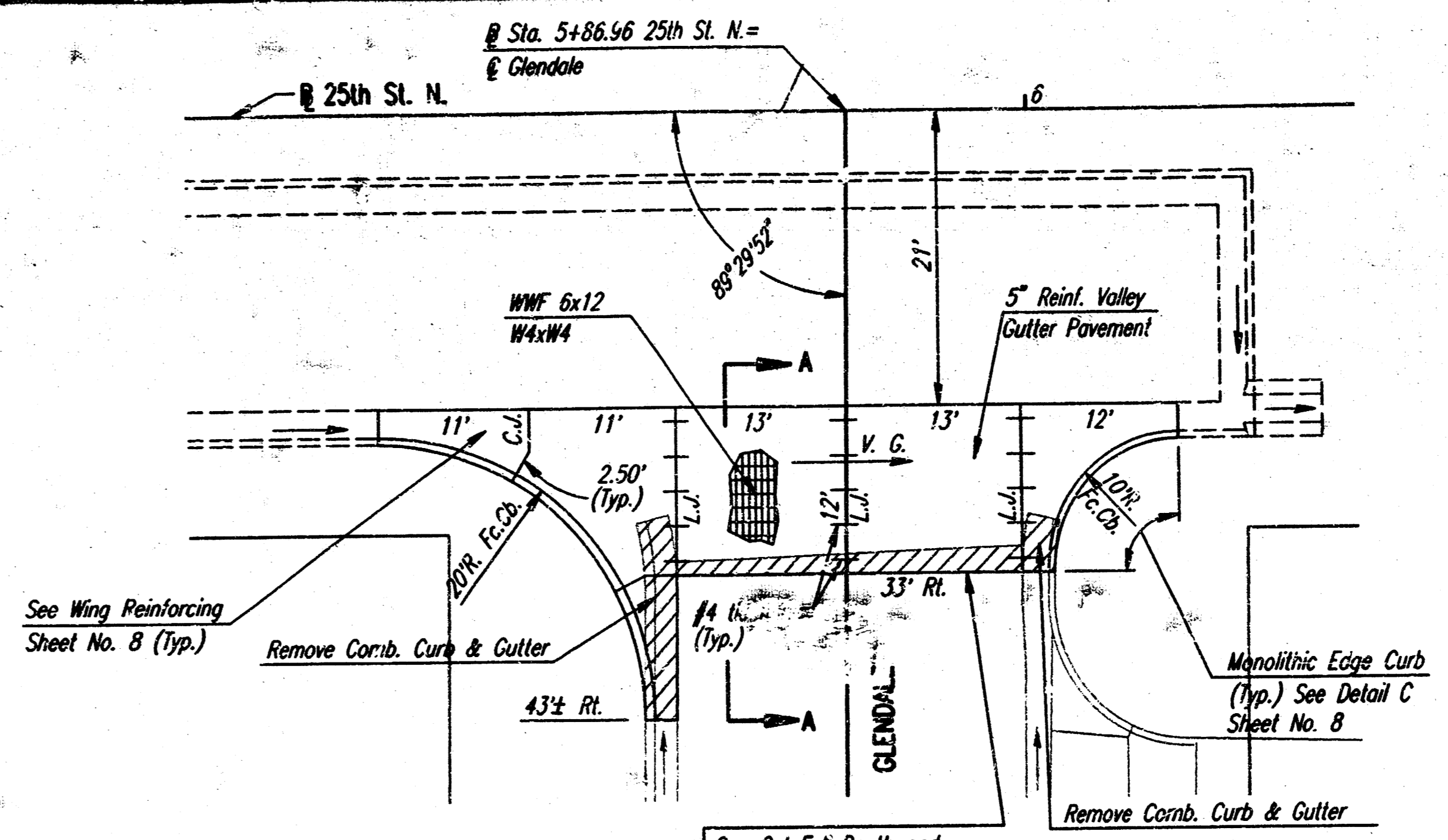
Designed by: BER, GGD	Checked by:
Drawn by: BJS	Date: NOV. 1998
Job No. 98375	

11/1998, 98375/25-battin.dwg  
date plotted: november 16, 1998  
delivered to: dwalsh@city

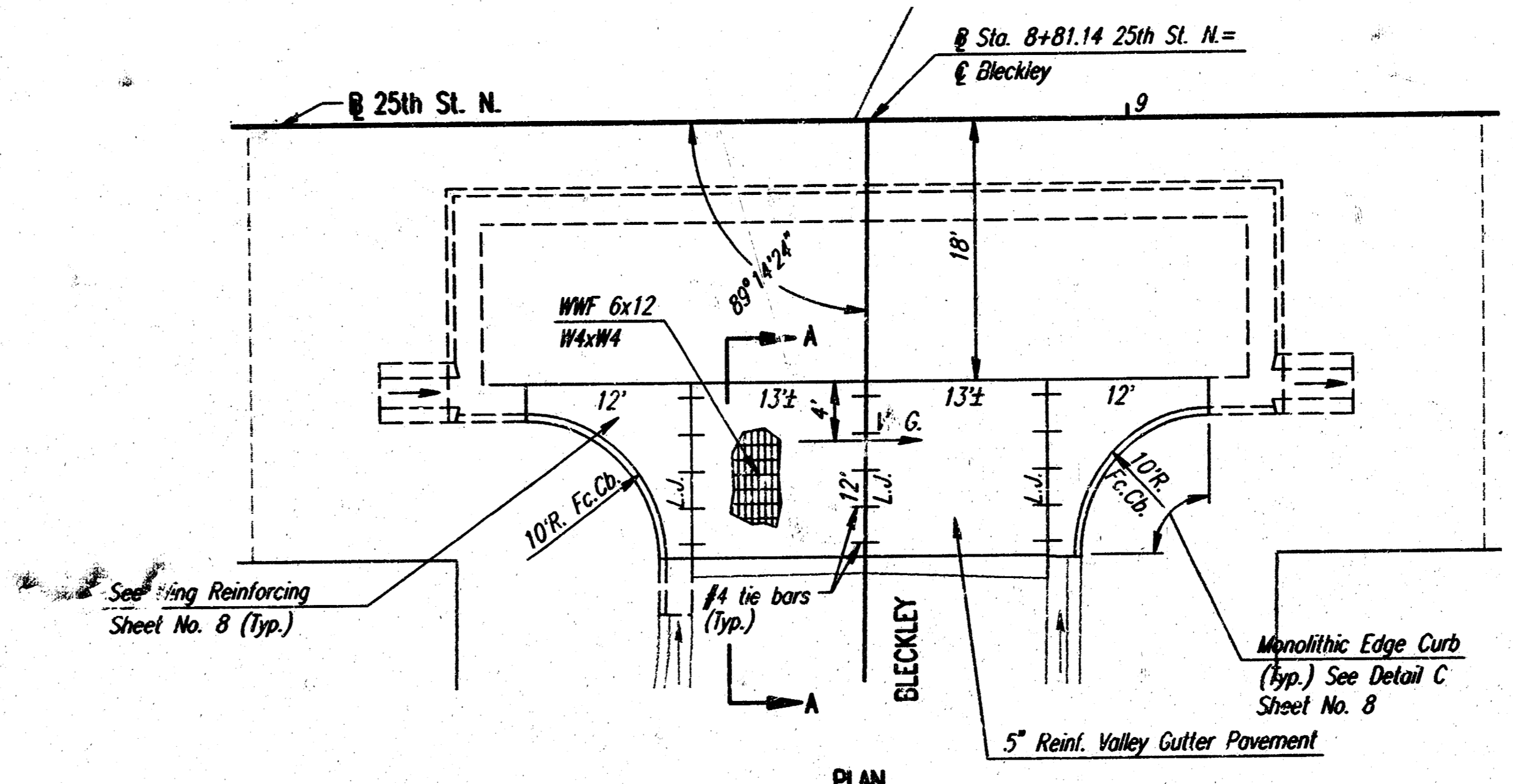
PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82429-000-001	7	13



PLAN  
REINFORCED VALLEY GUTTER



PLAN  
REINFORCED VALLEY GUTTER



PLAN  
REINFORCED VALLEY GUTTER

SCALE: 1" = 10'

6"x12" W4xW4 WIRE FABRIC REINFORCING SHALL BE PLACED SUCH THAT THE WIRES WITH THE 6" SPACING WILL RUN PARALLEL WITH THE LONGITUDINAL JOINT.

1:1998/11/15/valgutter.dwg  
date plotted: november 16, 1998  
deliber: tc-cw:trn: dunn

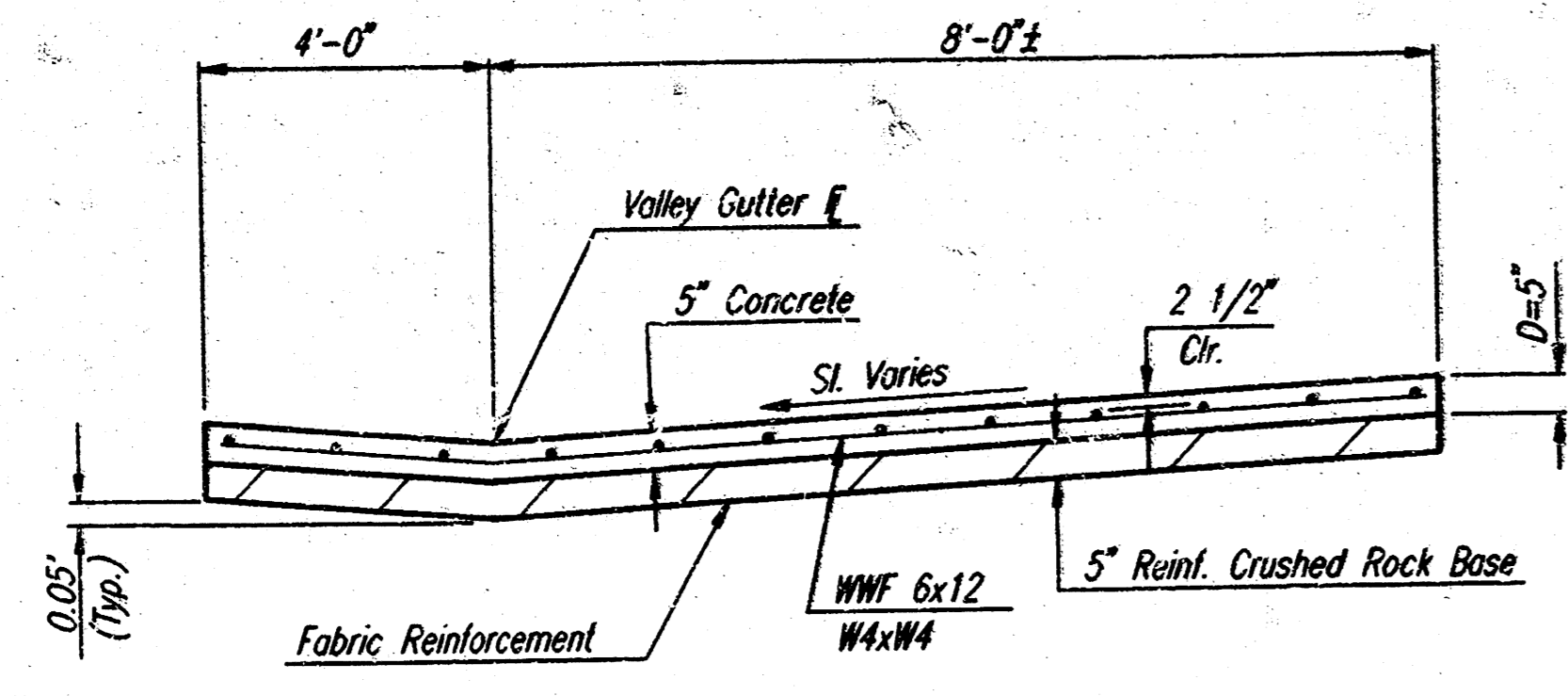
25TH STREET NORTH

## VALLEY GUTTER PLANS

**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
ENGINEERS  
WICHITA, KANSAS

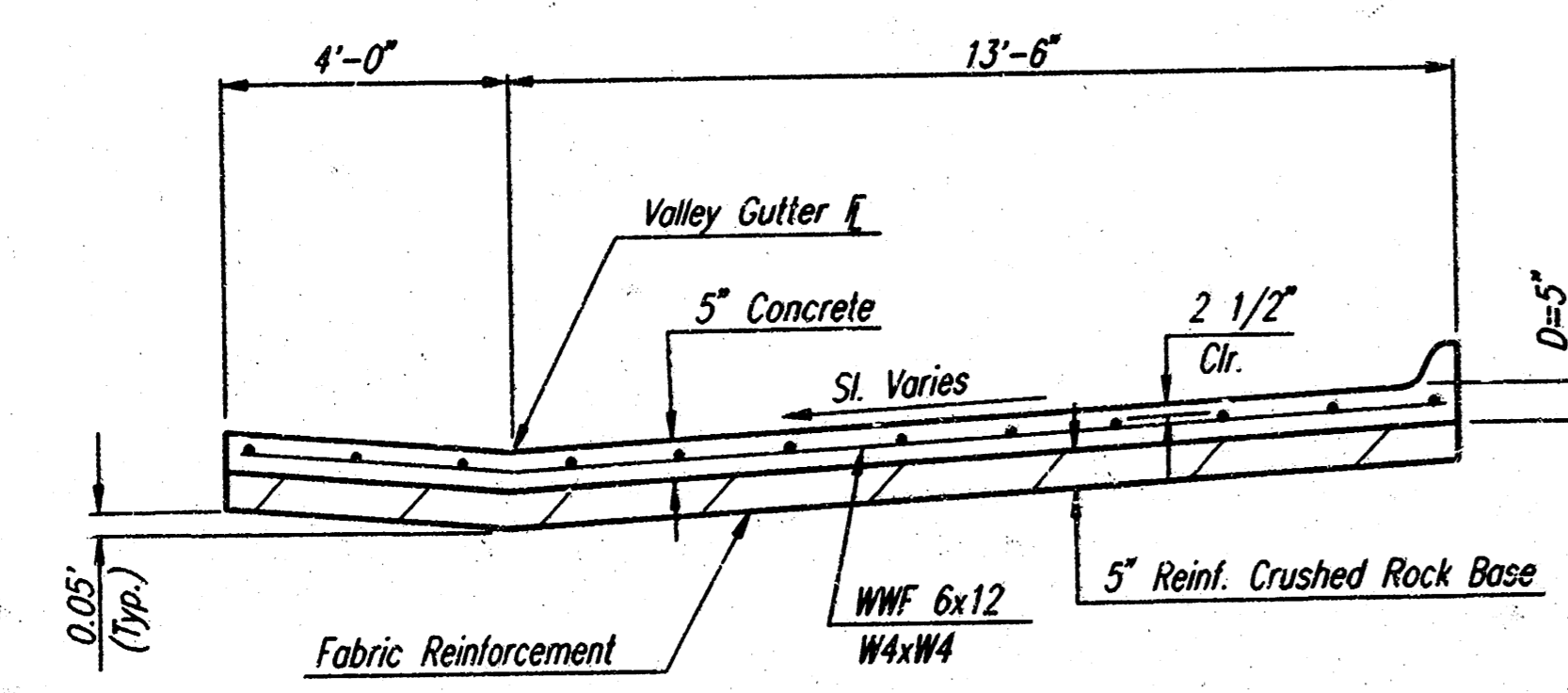
Designed by	BER, GDD	Checked by	
Drawn by	BJS	Date	NOV. 1998
		Job No.	98375

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-82429-000-000-001	8	13



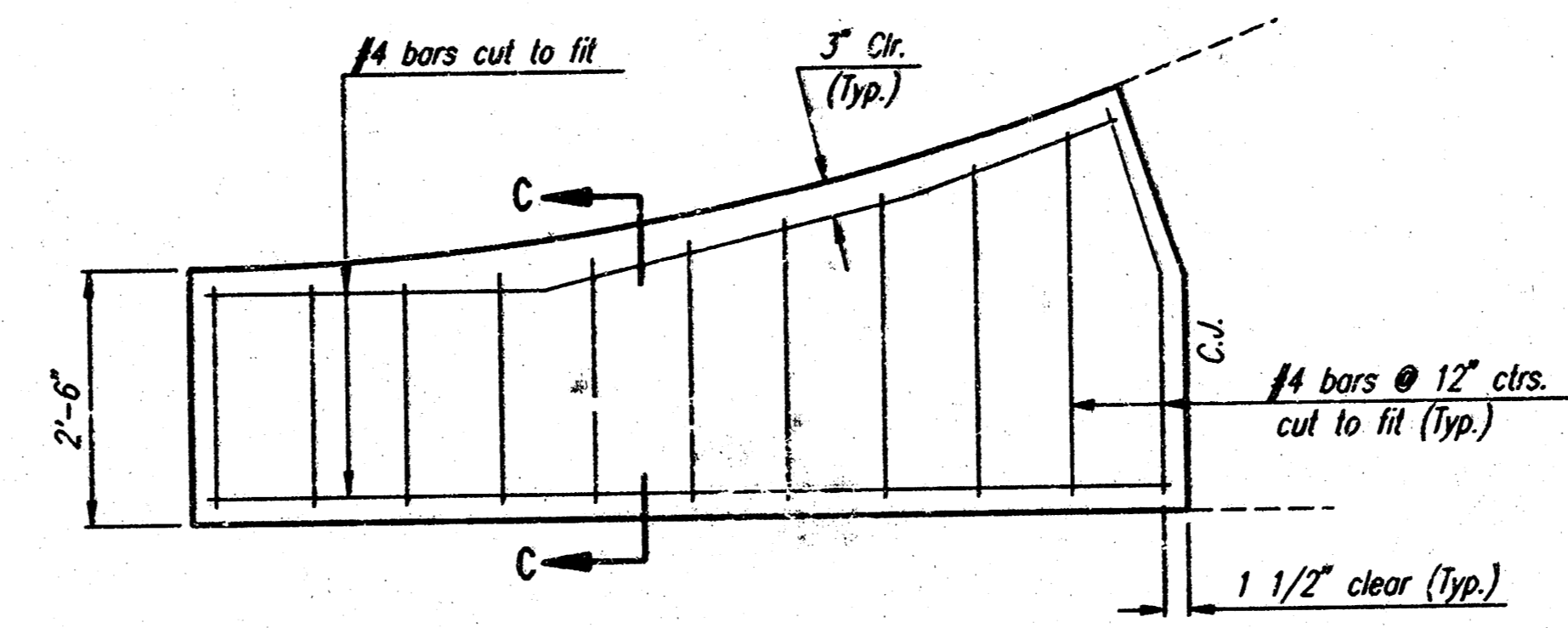
**SECTION A-A**

NOTE: OMIT REINFORCING MESH AT ALL JOINTS  
 NOTE: ALL CONCRETE VALLEY GUTTER REINFORCEMENT SHALL BE ADEQUATELY SUPPORTED BY BAR CHAIRS IN THE REQUIRED POSITION UNLESS APPROVED OTHERWISE BY THE ENGINEER.



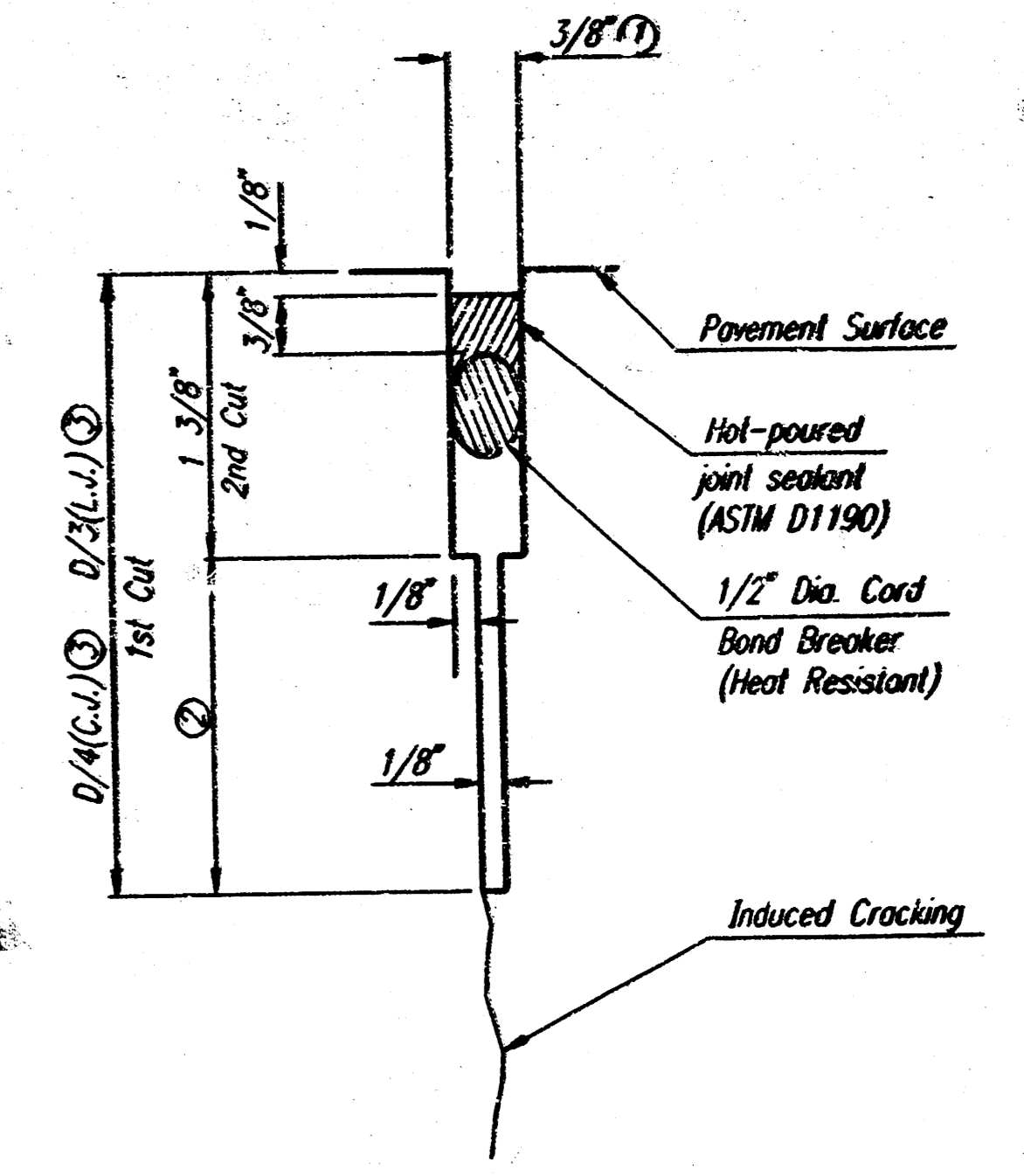
**SECTION B-B**

NOTE: OMIT REINFORCING MESH AT ALL JOINTS  
 NOTE: ALL CONCRETE VALLEY GUTTER REINFORCEMENT SHALL BE ADEQUATELY SUPPORTED BY BAR CHAIRS IN THE REQUIRED POSITION UNLESS APPROVED OTHERWISE BY THE ENGINEER.



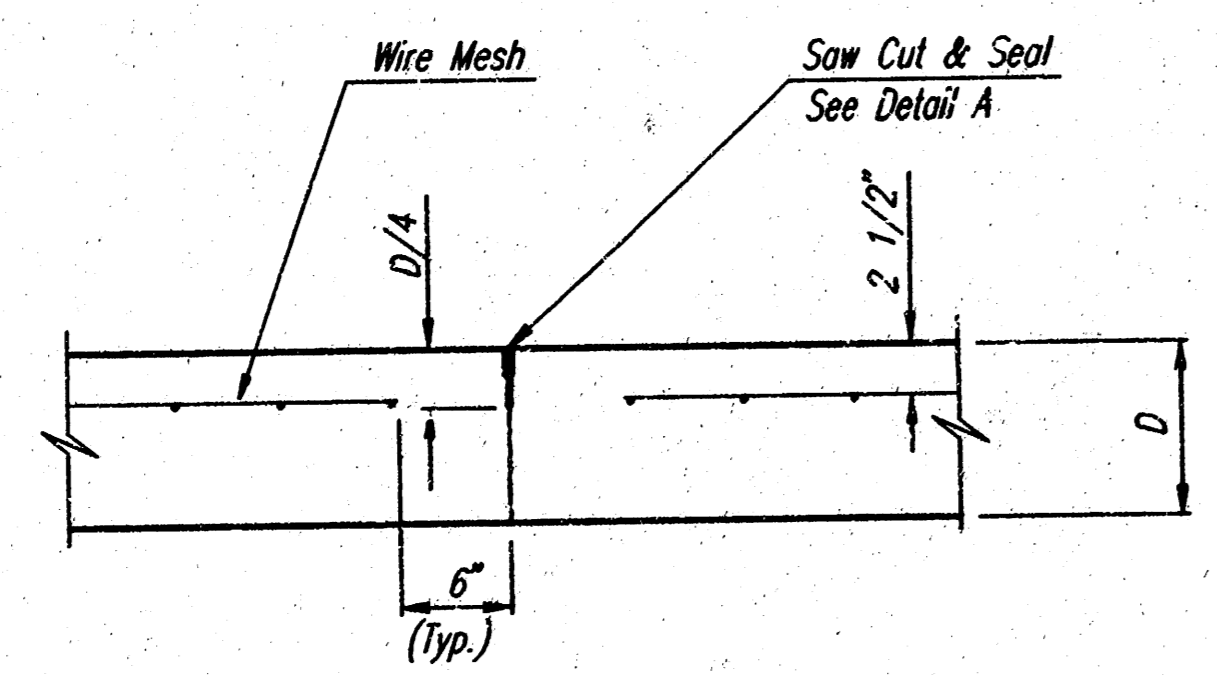
**WING REINFORCING DETAIL**

NOTE: OMIT WIRE FABRIC REINFORCING IN THIS SECTION.

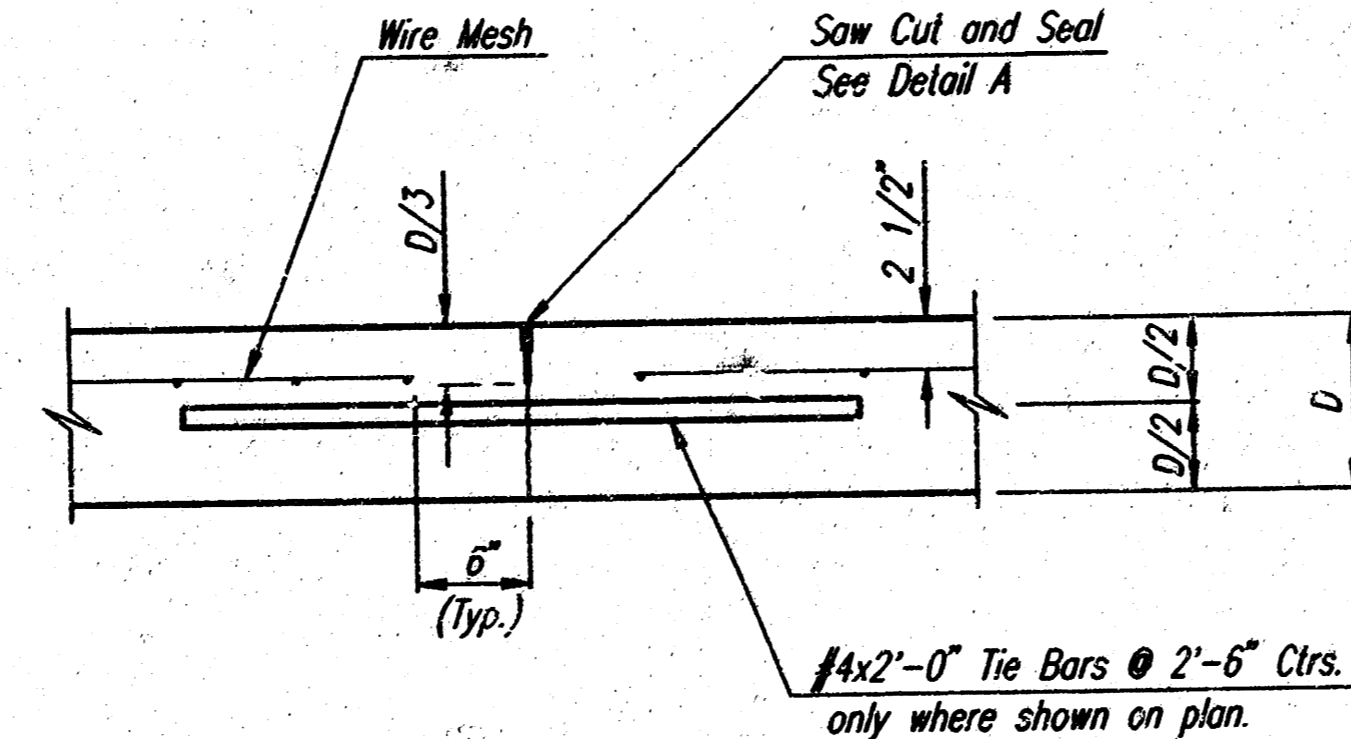


**DETAIL A**

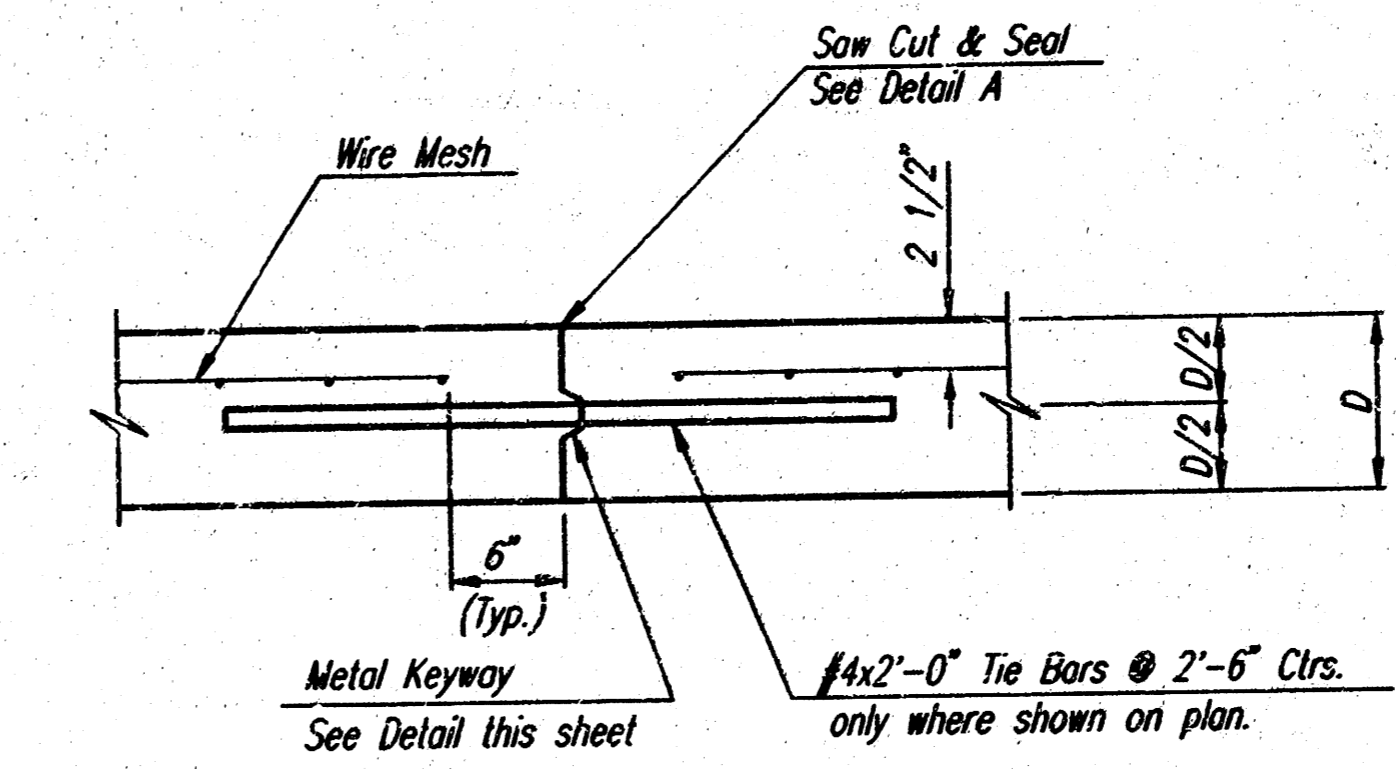
- ① To be accomplished in 2 cuts for Longitudinal Joints and Contraction Joints. Initial cut to be 1/8" wide.
- ② Eliminate bottom of cut when metal keyway is used as part of Longitudinal or Transverse Construction Joint and at Doweled Construction Joint Locations.
- ③ 1 1/2" Minimum



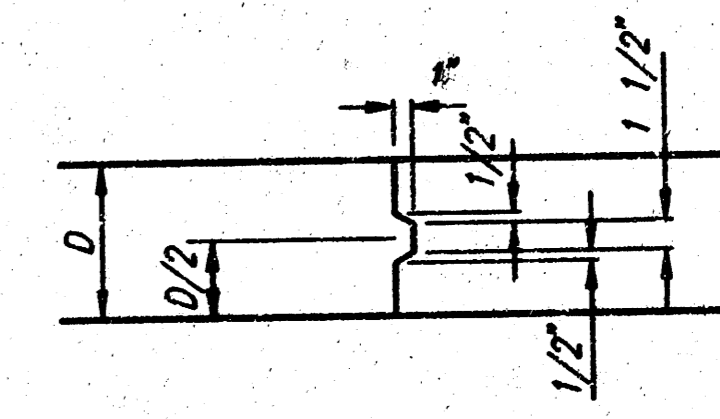
**CONTRACTION JOINT DETAIL REINFORCED PAVEMENT (C.J.)**



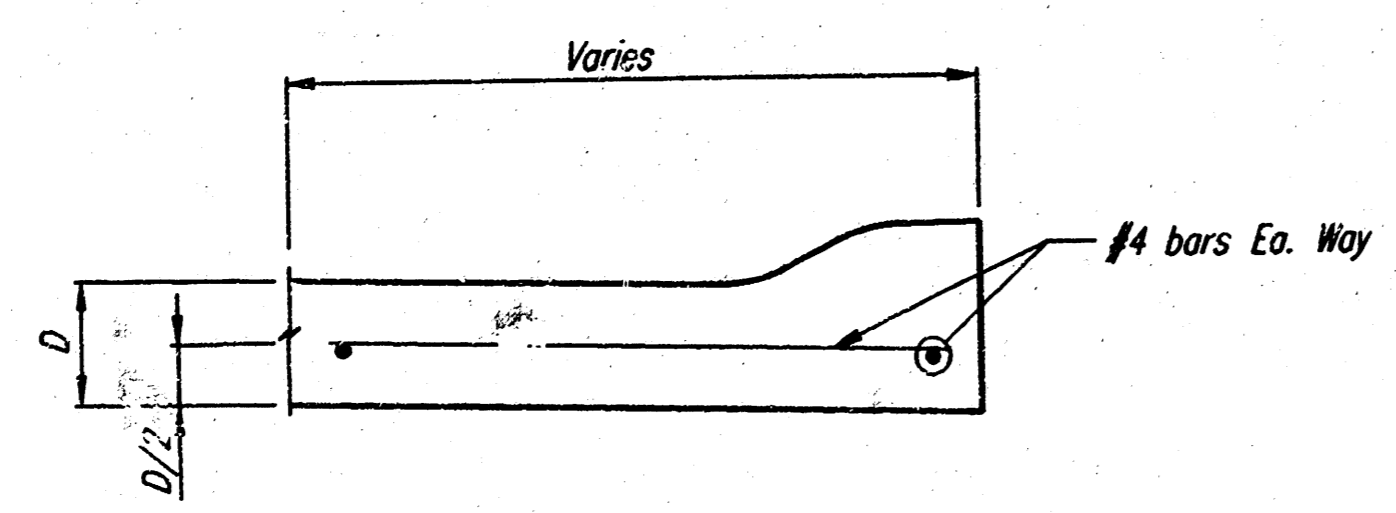
**LONGITUDINAL JOINT DETAIL REINFORCED PAVEMENT (L.J.)**



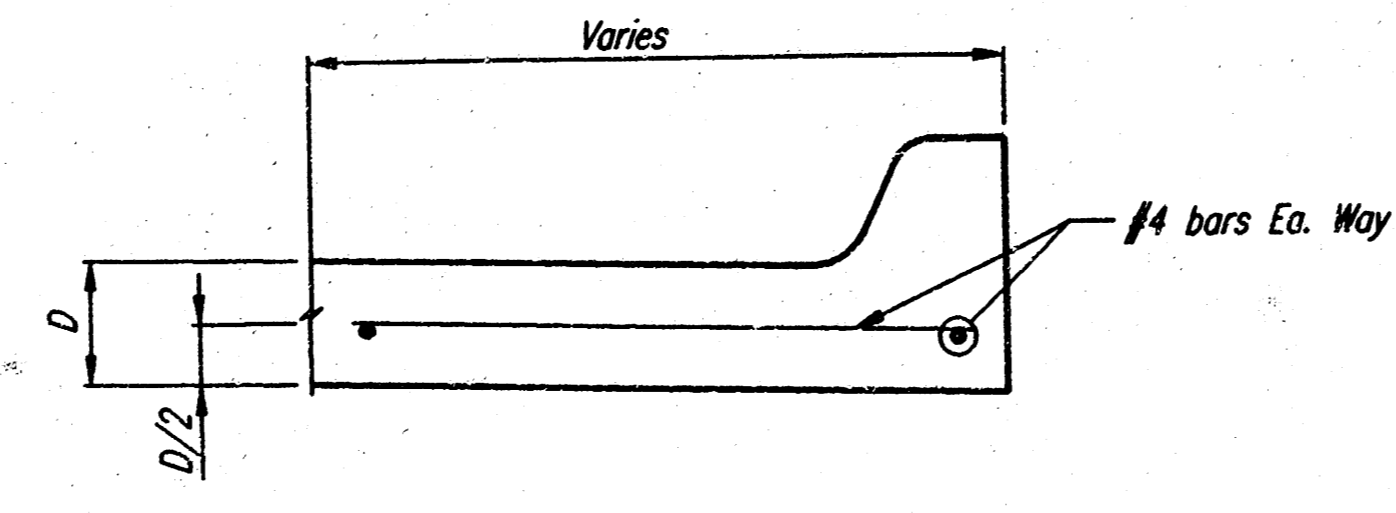
**LONGITUDINAL CONSTRUCTION JOINT DETAIL REINFORCED PAVEMENT (TRANSVERSE SECTION) (ALTERNATE L.J.)**



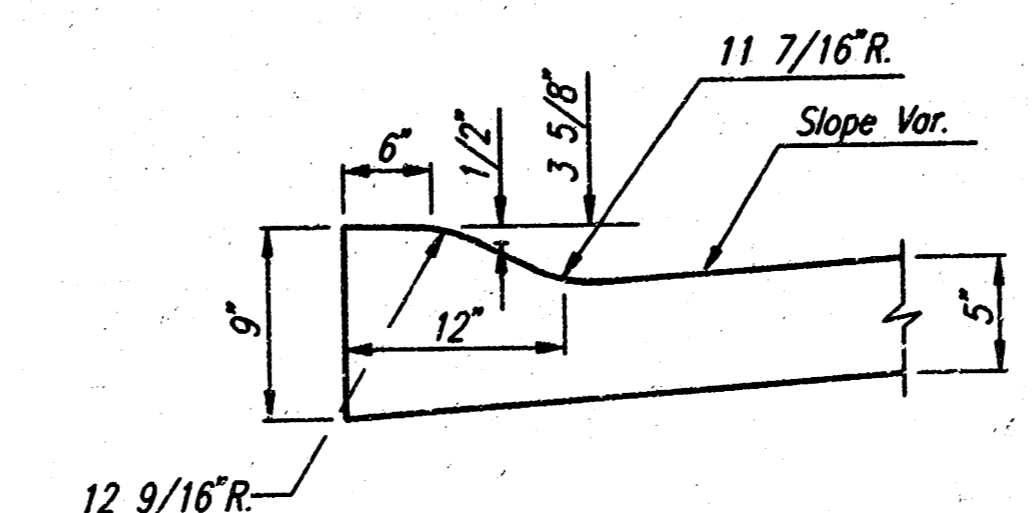
**KEYWAY DETAIL**



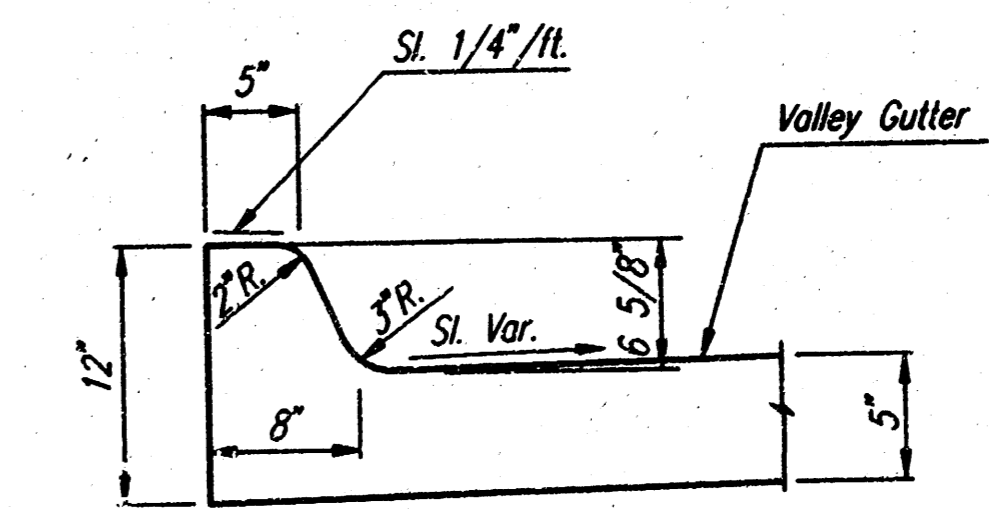
**SECTION C-C (ROLL-TYPE CURB)**



**SECTION C-C (STD. CURB)**



**DETAIL B (ROLL-TYPE CURB)**



**DETAIL C (STD. CURB)**

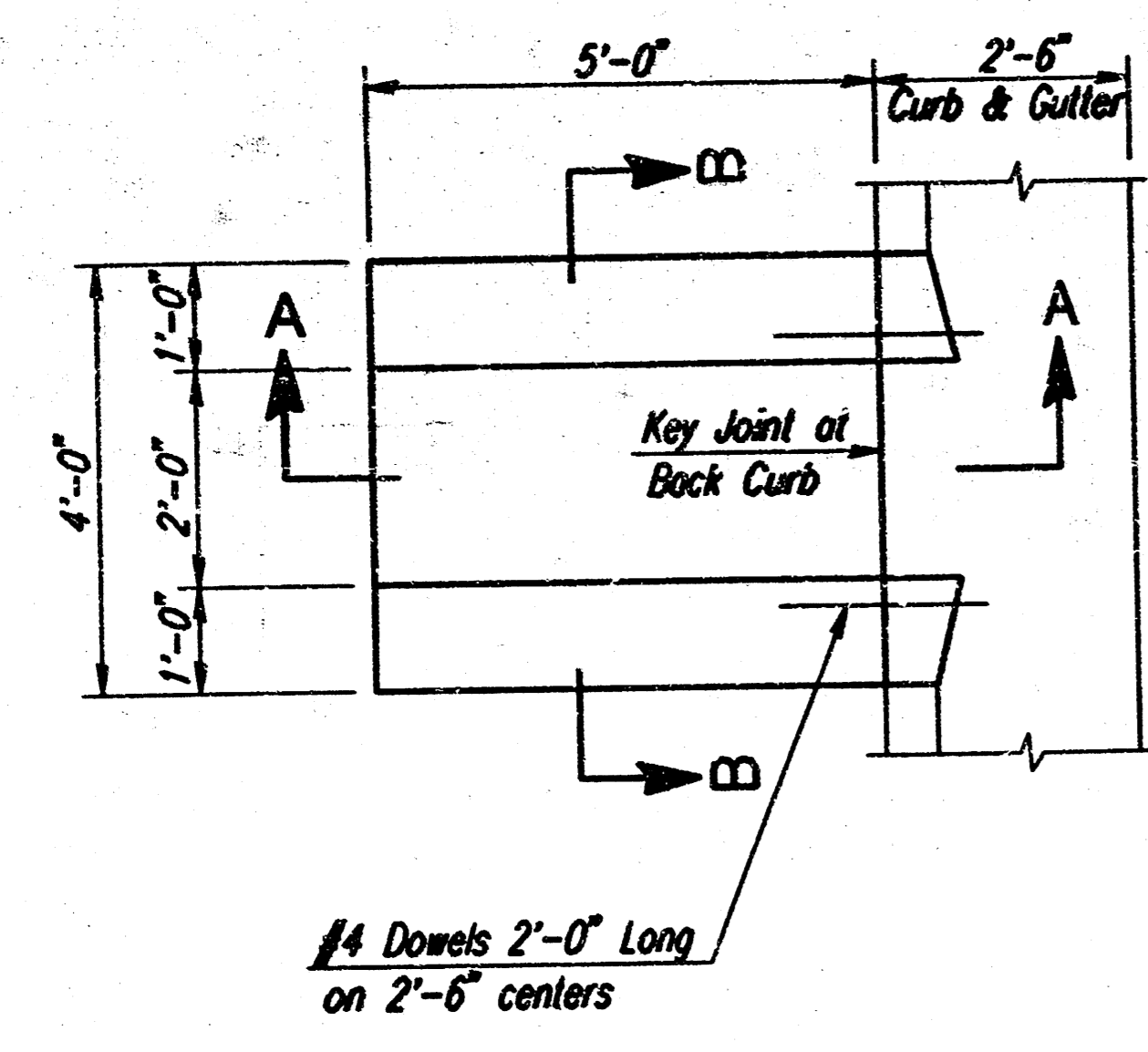
25TH STREET NORTH

## VALLEY GUTTER DETAILS

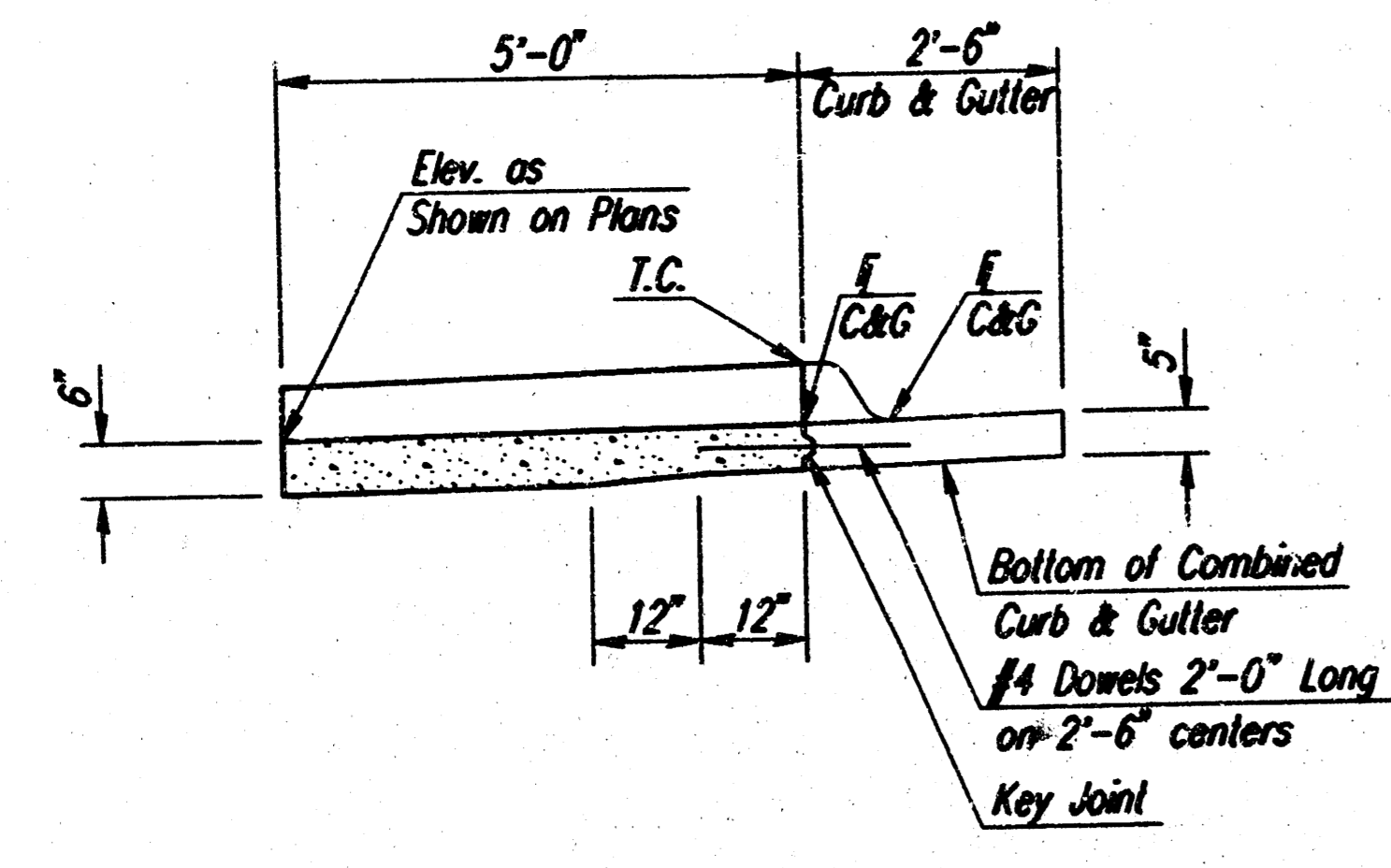
**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
ENGINEERS  
 WICHITA, KANSAS

Designed by: BER, GDD	Checked by:
Drawn by: BJS	Date: NOV. 1998
	Job No. 98375

1: 1998/08/31/valgutter.dgn  
 date plotted: november 16, 1998  
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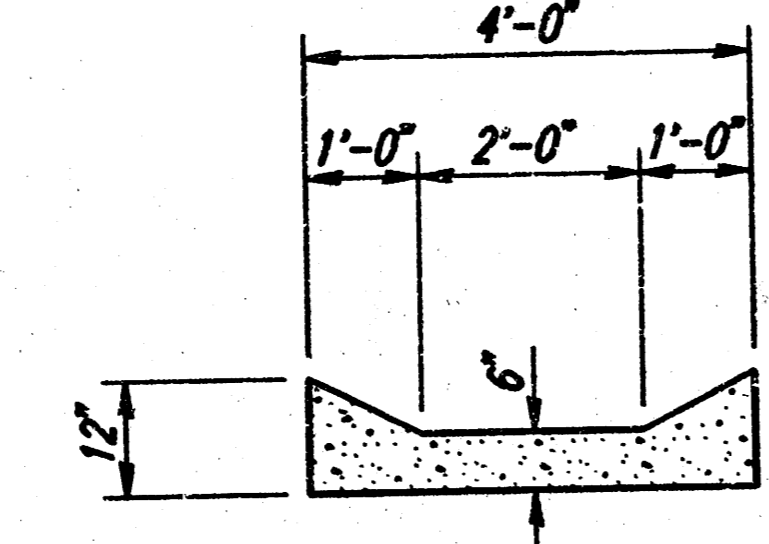


PLAN

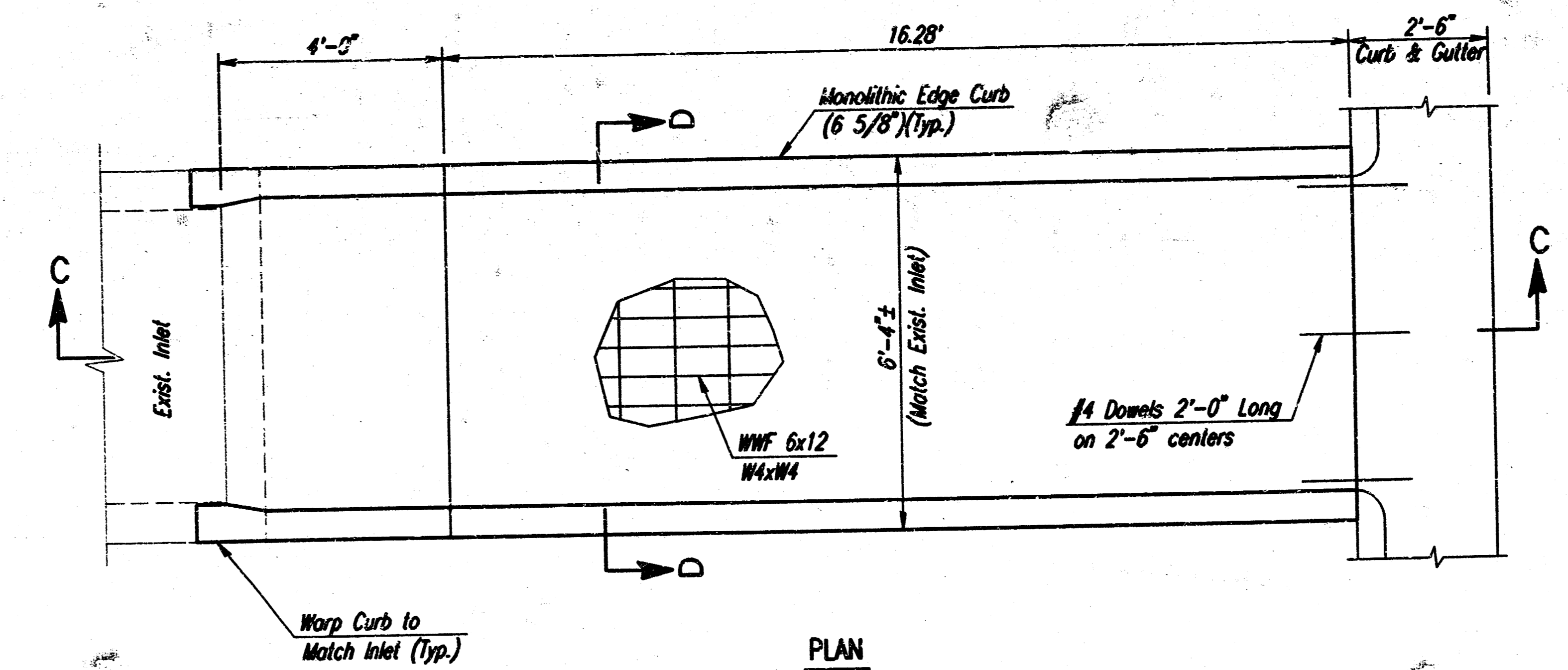


SECTION A-A

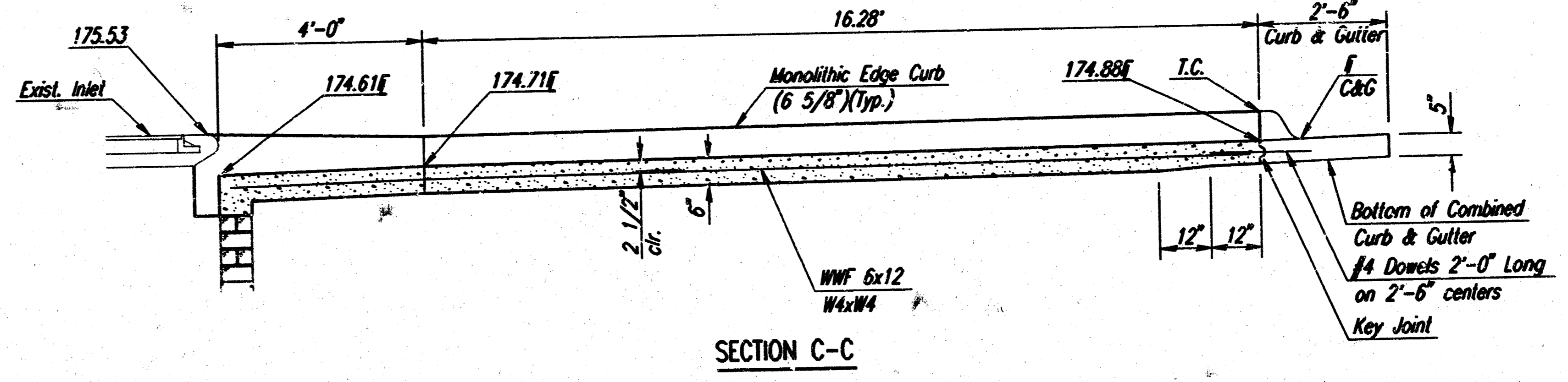
CONCRETE FLUME DETAILS



SECTION B-B

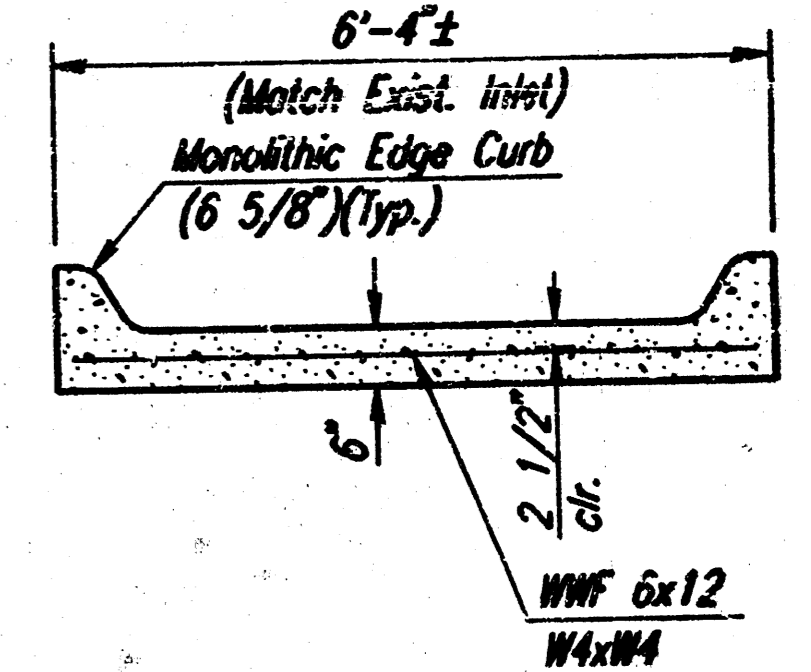


PLAN



SECTION C-C

CONCRETE INLET FLUME DETAILS



SECTION D-D

GENERAL NOTES

CONCRETE FOR FLUMES SHALL BE THE SAME AS THAT USED FOR COMBINED CURB AND GUTTER.

THE ENTIRE AREA OF THE FLUME SHALL BE POURED MONOLITHICALLY, AND ALL EXPOSED EDGES SHALL BE FINISHED WITH AN EDGING TOOL.

MONOLITHIC EDGE CURB (6 5/8") SHALL NOT BE BID SEPARATELY BUT SHALL BE CONSIDERED PART OF THE CONCRETE INLET FLUME.

FLUMES SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE BID PER EACH COMPLETE AND IN PLACE OF THE VARIOUS TYPES AS SHOWN ON THE PLANS. THIS SHALL BE CONSIDERED FULL COMPENSATION FOR ALL MATERIALS, LABOR, TOOLS, EXCAVATION, BACKFILLING, CONNECTION TO EXISTING STRUCTURES (IF REQUIRED), OR INCIDENTALS NECESSARY TO COMPLETE THE WORK.

25TH STREET NORTH

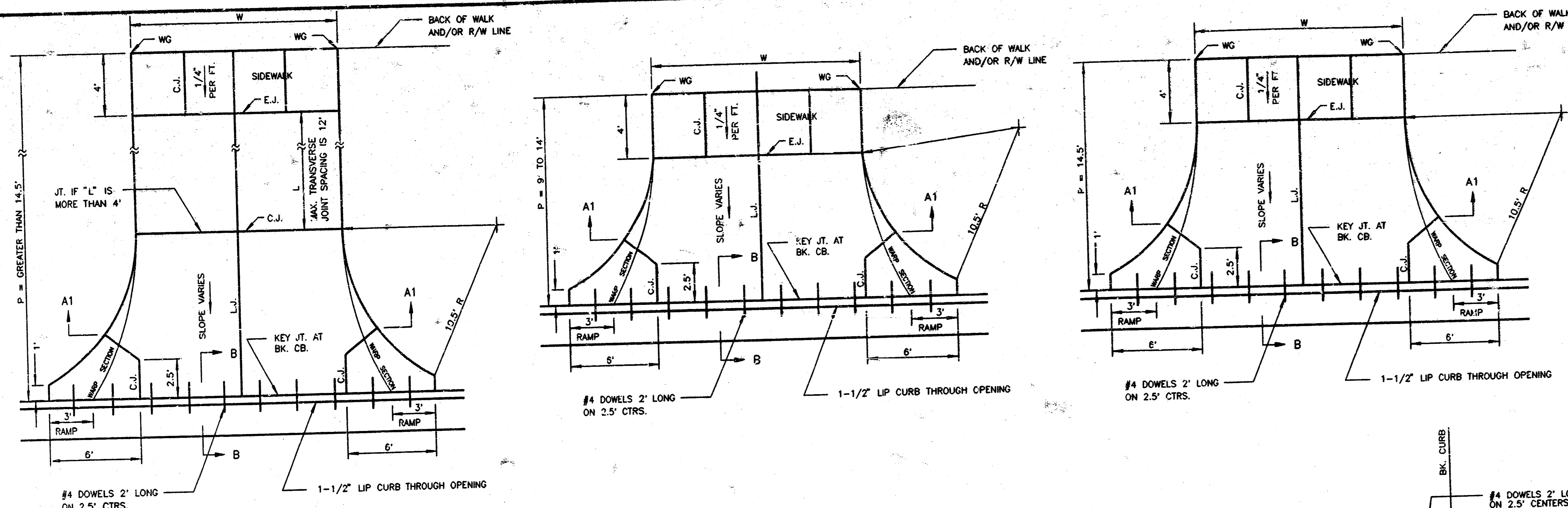
**CONCRETE FLUME AND  
CONCRETE INLET FLUME DETAILS**

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
ENGINEERS  
WICHITA, KANSAS

Designed by BER, GDD	Checked by
Drawn by BJS	Date NOV, 1998
Job No. 98375	

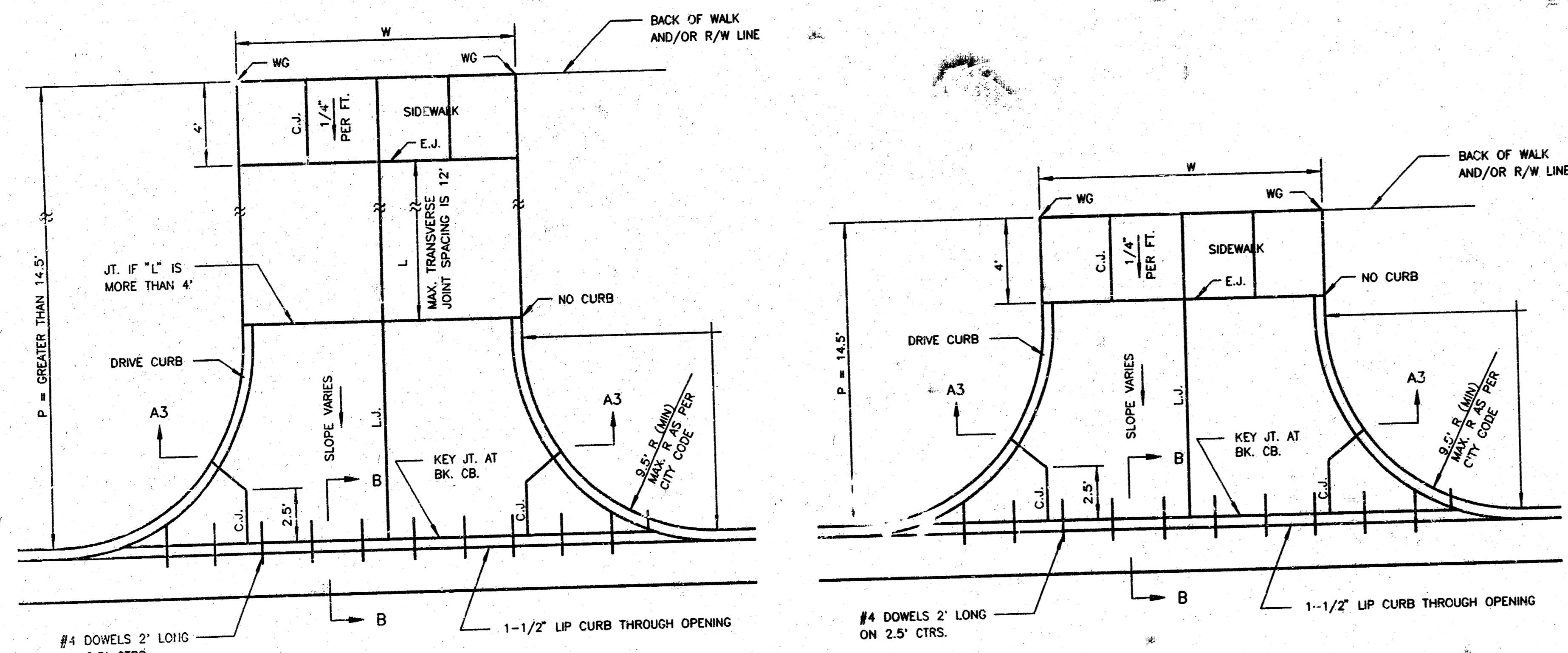
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I.L. 7-99375.dfw  
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 deliver to: drawings room



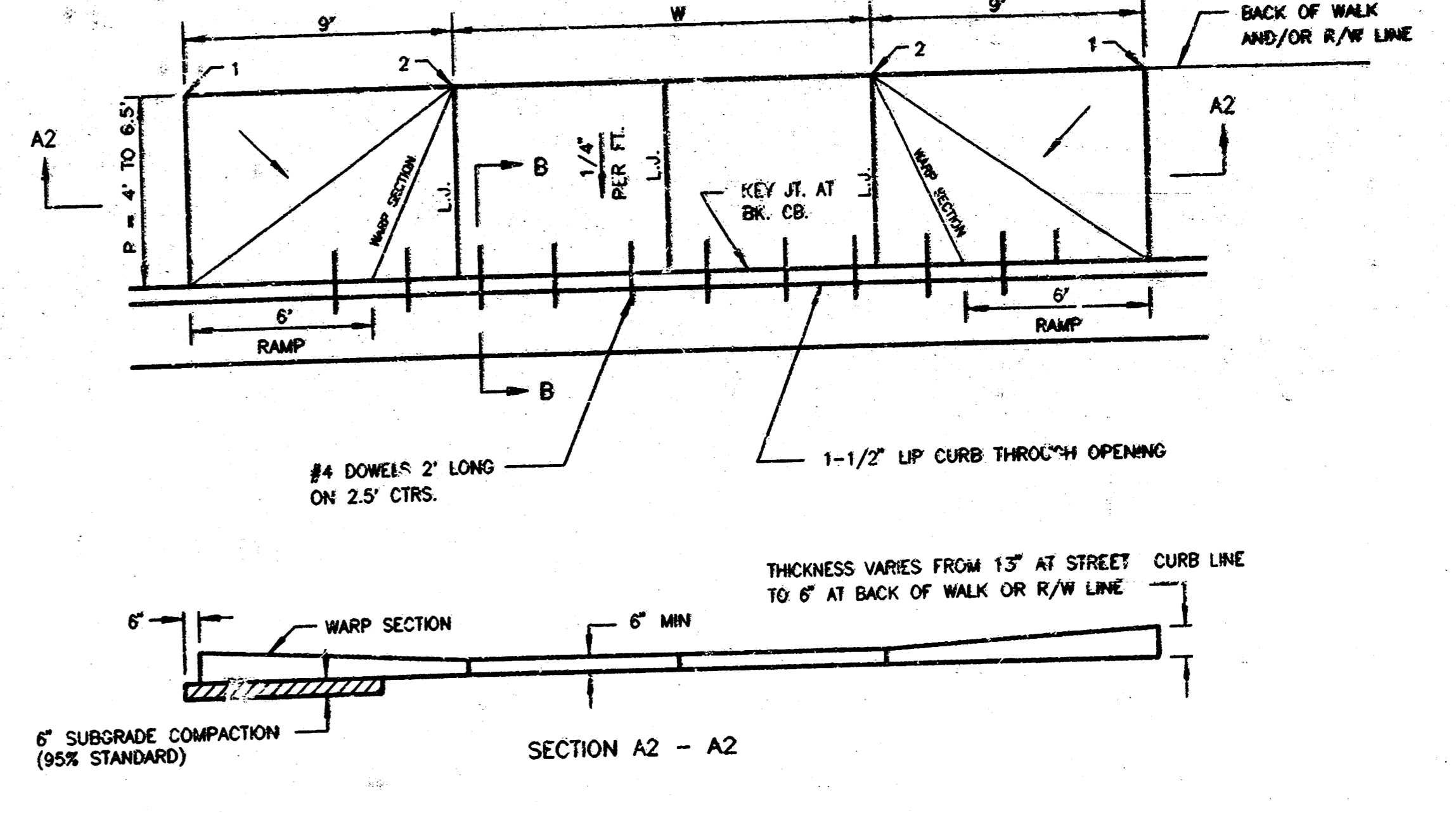
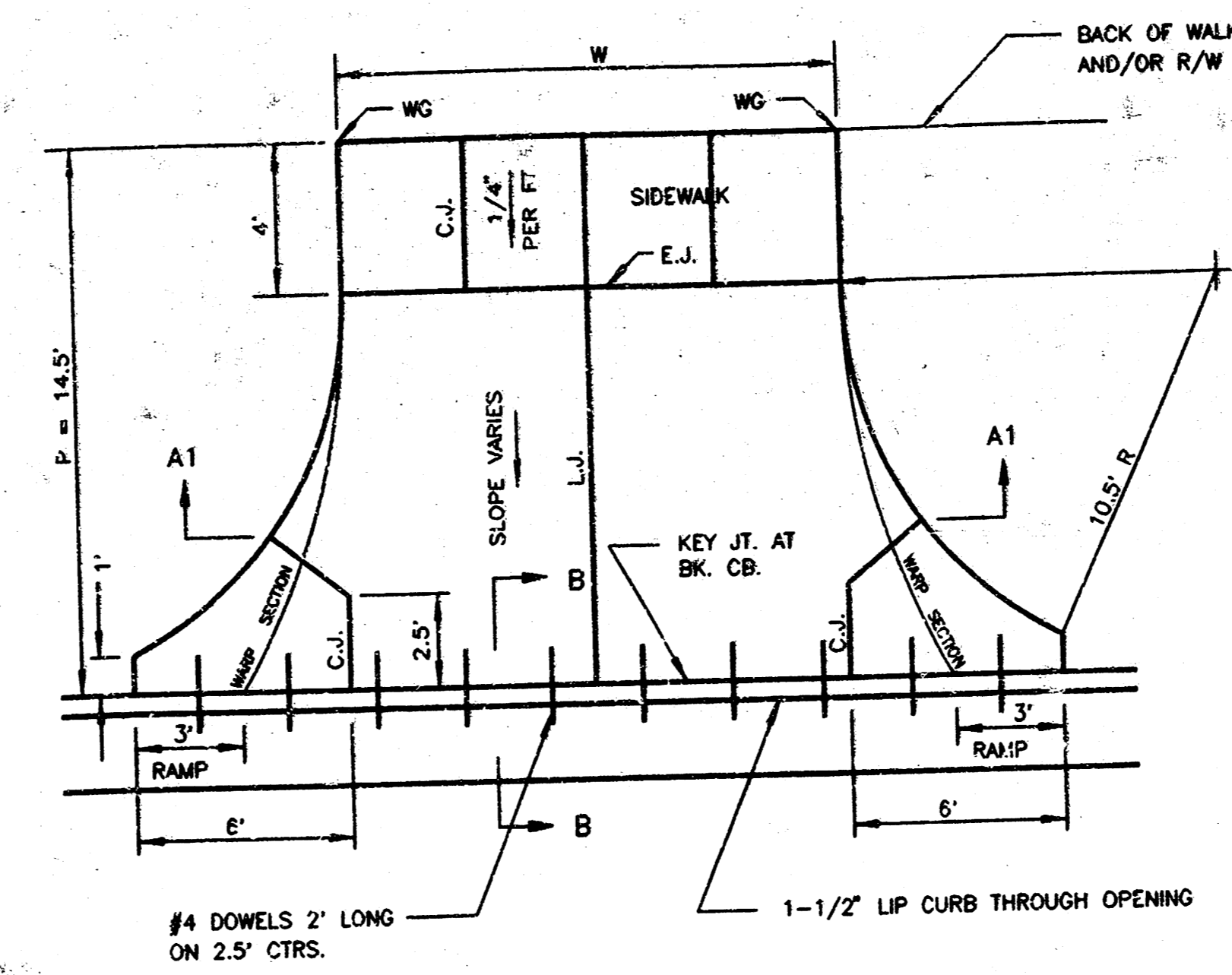
PARKING WIDTH "P"	9'	10'	11'	12'	13'	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.27'	0.27'	0.32'	0.37'	0.52'	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.27'	0.27'	0.32'	0.37'	0.52'	0.62'	0.96'	1.22'	1.48'	1.74'	2.00'	2.26'	2.52'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.19'	0.21'	0.23'	0.25'	0.30'	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-0.19'	-0.16'	-0.13'	-0.10'	-0.06'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'	

**RADIUS RAMP DRIVES (P = 9.0' & GREATER)**



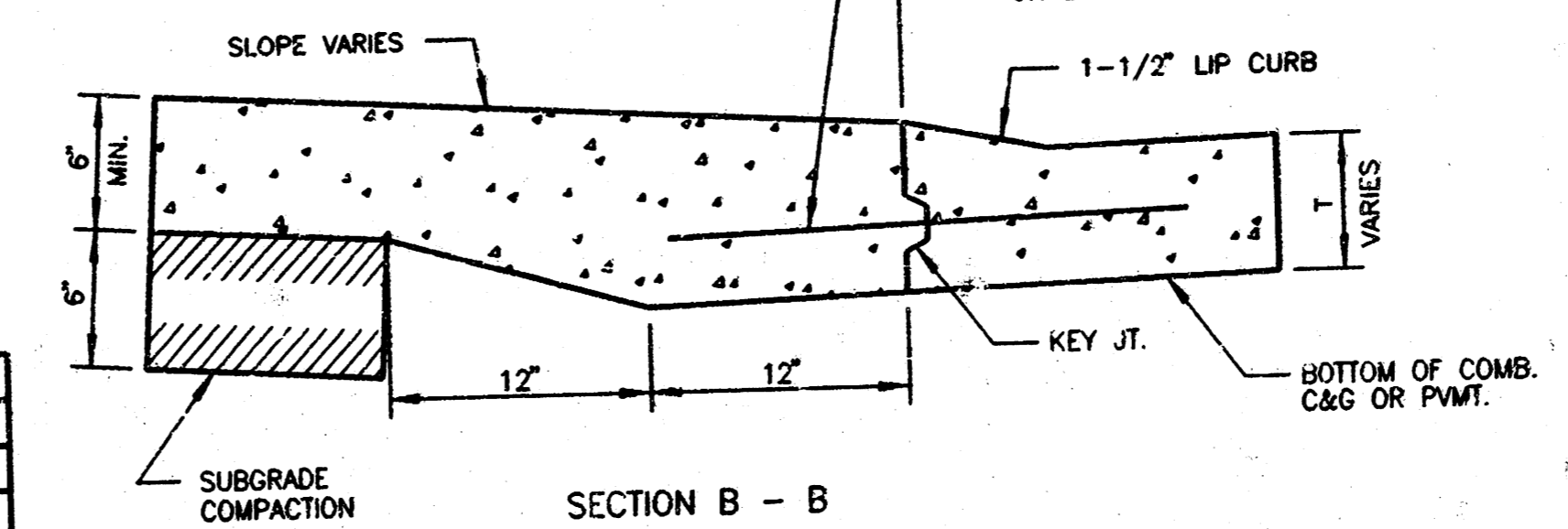
PARKING WIDTH "P"	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.72'	1.27'	1.77'	2.27'	2.77'	3.27'	3.77'	4.27'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.70'	1.04'	1.30'	1.50'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

**FULL RADIUS DRIVES (P = 14.5' & GREATER)**

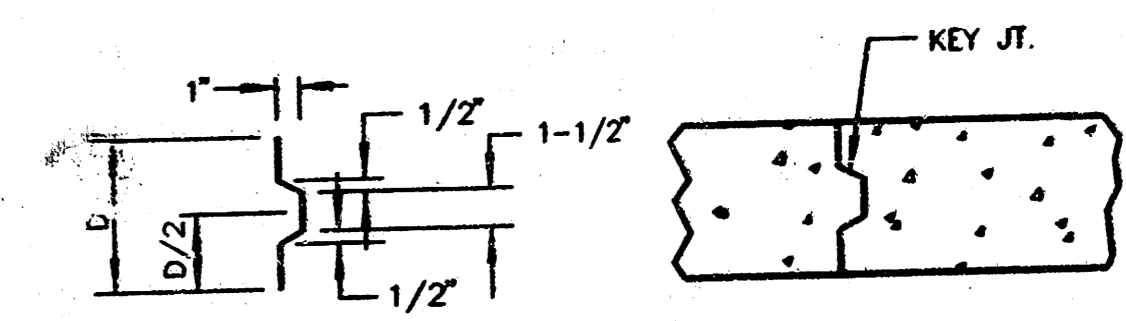


PARKING WIDTH "P"	4'	4.5'	5'	5.5'	6'	6.5'
DIST. OF PT. "1" ABOVE TOP OF FULL CURB	-0.8'	0.02'	0.18'	0.22'	0.18'	0.22'
DIST. OF PT. "2" BELOW TOP OF FULL CURB	-0.8'	0.02'	0.18'	0.22'	0.18'	0.22'

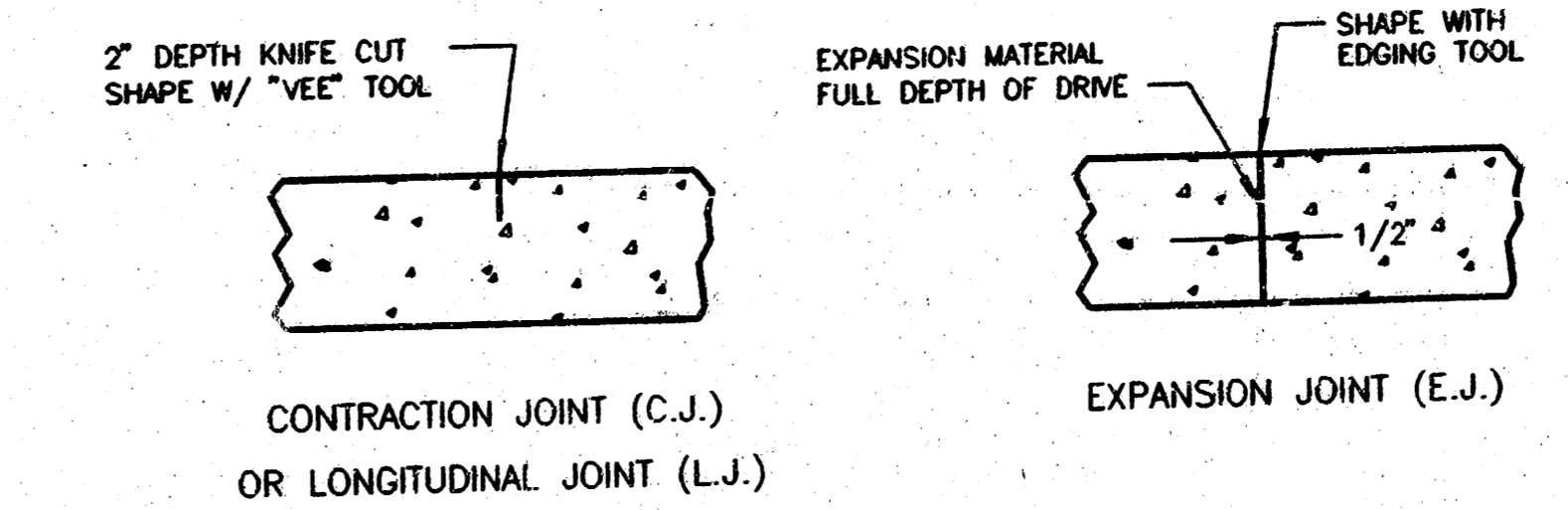
**FULL RAMP DRIVE (P = 4.0' TO 6.5')**



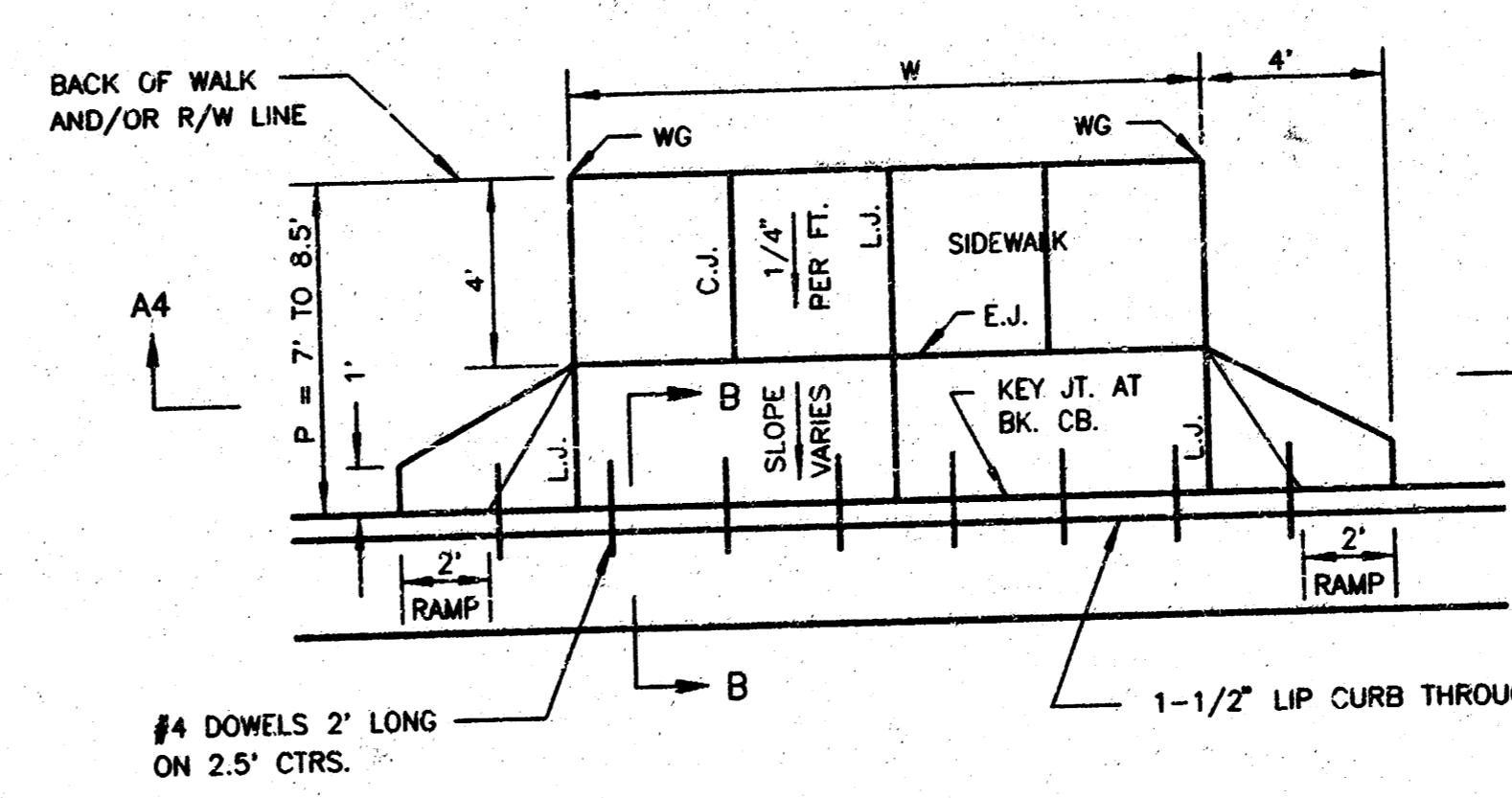
**BACK OF CURB DETAIL**



**ALT. LONGITUDINAL CONSTRUCTION JOINT**



**CONTRACTION JOINT (C.J.) OR LONGITUDINAL JOINT (L.J.)**



PARKING WIDTH "P"	7'	7.5'	8'	8.5'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-0.8'	0.02'	0.18'	0.22'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-0.8'	0.02'	0.18'	0.22'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-1.5'	-1.6'	-1.7'	-1.7'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-2.5'	-2.0'	-2.0'	-2.0'

**FULL RAMP DRIVE (P = 7.0' TO 8.5')**

- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT SIDEWALK CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH FULL WALK.
- ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "W" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "W" DIMENSION GREATER THAN 24'.
- DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
- CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
- DOWEL BARS SHALL BE OMITTED FROM THE KEYS CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
- ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
- ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
- ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" W4-W4 WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONCURRENCE.
- OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE GUTTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.

THE CITY OF WICHITA

CITY ENGINEER'S OFFICE  
CITY HALL - SEVENTH FLOOR  
105 NORTH MAIN STREET  
WICHITA, KANSAS 67202  
(316) 266-1400  
(316) 266-4114 FAX

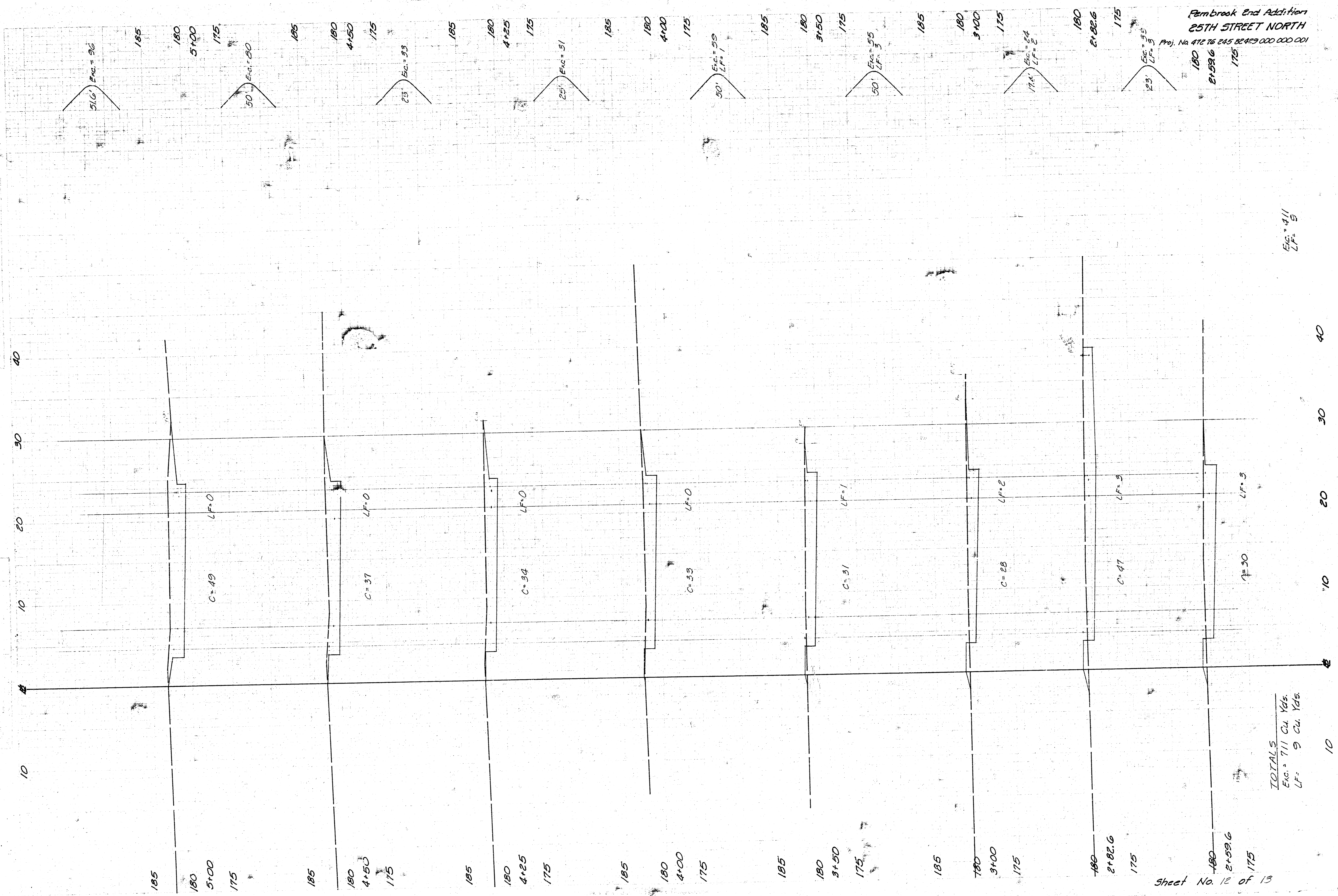
**STANDARD DRIVE ENTRANCES FULL HEIGHT CURB**

M. E. LINDEBAK P.E. - CITY ENGINEER

PROJECT NUMBER 472-82429	INDEX CODE 765420
DATE MAR 96	SHEET 10 OF 13



Pembroke End Addition  
 25TH STREET NORTH  
 Proj. No. 412 76 245 82459 000 000 001



TOTALS  
 Exc. = 711 Cu Yds.  
 LF = 9 Cu Yds.

Sheet No. 12 of 13

Exc. = 411  
 LF = 5

Pembroke End Addition  
 25TH STREET NORTH  
 Proj. No. 472 76 245 82429 000000001

Exc. = 300 C.Y.

