

FHWA REGION NO.	STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
7	KANSAS	54-87 K-6657-01	2002	111	1122

CONCRETE PAVEMENT JOINT TYPES:

- A) CONTRACTION JOINT with #16 x 600mm DEFORMED TIE BARS & SAWN.
- B) CONSTRUCTION JOINT with #16 x 600mm DEFORMED TIE BARS ONLY.
- C) CONSTRUCTION JOINT: KEYWAY ONLY.
- D) CONSTRUCTION JOINT with KEYWAY & #16 x 600mm DEFORMED TIE BARS.

CONCRETE PAVEMENT JOINT TYPES:

- H) CONSTRUCTION JOINT with SMOOTH DOWEL BARS:
 - 1) 200mm PCCP = 25mm x 450mm.
 - 2) 260mm PCCP = 32mm x 450mm.
- I) CONSTRUCTION JOINT (mainline outside shoulder only) with 2-#16 continuous epoxy deformed bars left uncut & SAWN.

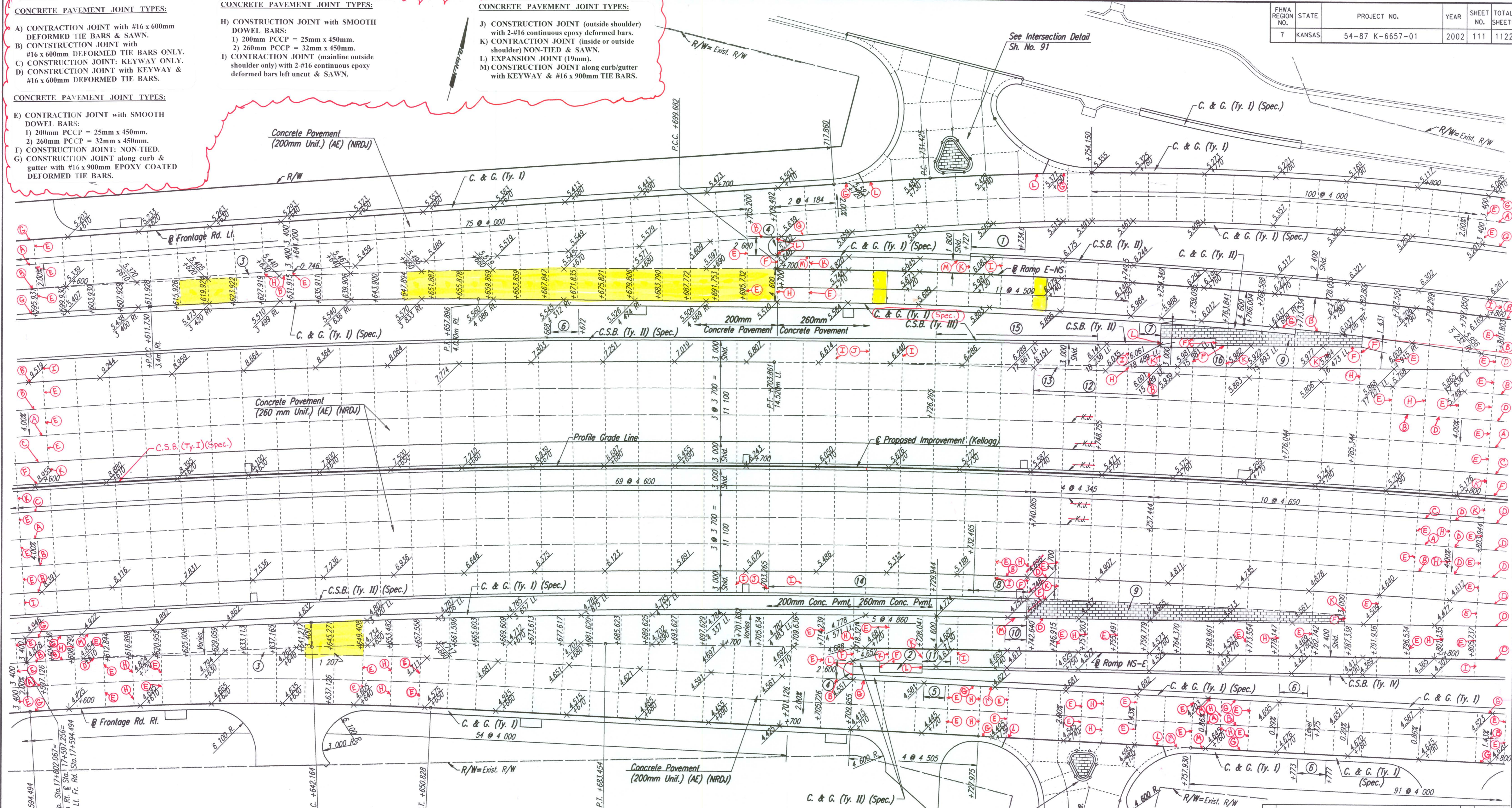
CONCRETE PAVEMENT JOINT TYPES:

- J) CONSTRUCTION JOINT (outside shoulder) with 2-#16 continuous epoxy deformed bars.
- K) CONTRACTION JOINT (inside or outside shoulder) NON-TIED & SAWN.
- L) EXPANSION JOINT (19mm).
- M) CONSTRUCTION JOINT along curb/gutter with KEYWAY & #16 x 900mm TIE BARS.

CONCRETE PAVEMENT JOINT TYPES:

- E) CONTRACTION JOINT with SMOOTH DOWEL BARS:
 - 1) 200mm PCCP = 25mm x 450mm.
 - 2) 260mm PCCP = 32mm x 450mm.
- F) CONSTRUCTION JOINT: NON-TIED.
- G) CONSTRUCTION JOINT along curb & gutter with #16 x 900mm EPOXY COATED DEFORMED TIE BARS.

DATE	
BY	
REFERENCES NOTED	
REFERENCES CHECKED	



- ① 9.0m 7.6m Transition from Combined C. & G. (Ty. I) (Spec.) to Concrete Safety Barrier. See Sheet No. 232A
- ② Impact Attenuator. See Sh. No. 234 for Concrete Pad & Backup Wall Details.
- ③ Place monolithic with ramp pavement.
- ④ See Sh. No.130 for Median Nose Detail.
- ⑤ 3.0 m transition from C. & G. (Ty. II) (Spec.) to C. & G. (Ty. I) (Spec.).
- ⑥ 4.0 m transition from C. & G. (Ty. I) to C. & G. (Ty. I) (Spec.).
- ⑦ 3.0 m transition from C. & G. (Ty. I) to C. & G. (Ty. II).

- ⑧ 9.0m 7.6m Transition from C.S.B. (Ty. III) to no barrier. (150mm high @ end) Paid for as C.S.B. (Ty. III). See Sheet No. 232A
- ⑨ Integrally Colored Cement Concrete Pavement (D=260mm) See Landscape Plans.
- ⑩ 3.0m transition from 150mm curb to no curb.
- ⑪ 4.0 m transition from 815mm tall barrier to 1300mm tall barrier. Paid for as C.S.B. (Ty. IV).
- ⑫ Transition outside shoulder x-slope from +4.00% Lt. @ Sta. 17+740 to +2.00% Lt. @ Sta. 17+757.444.
- ⑬ Place monolithic with shoulder pavement. Paid for as "Concrete Pavement (260mm Unit.) (AE) (NRDJ)".
- ⑭ C.S.B. (Ty. III) Sta. 17+703.265 to Sta. 17+732.465.
- ⑮ C.S.B. (Ty. III) Sta. 17+726.265 to Sta. 17+748.755. (22.490m)
- ⑯ Impact Attenuator. See Sh. No. 236 for Concrete Pad & Backup Wall Details.

NOTE: TIE BARS WERE OMITTED ACROSS ALL SIDE ROADS ALONG FRONTAGE ROAD PAVEMENT.

DSWR: WDH OPER: JGP SCALE: 1:250
 i:1997/9/2362/PAVING/PAV176.DGN 2-21-2001 11:03:39 LAST REV: 11-27-01 BY: JGP

No.	Revisions	By	Date

CITY OF WICHITA

PAVING PLAN
STA. 17+600 TO STA. 17+800

SEDGWICK COUNTY

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Designed by WDH Checked by WDH
 Drawn by JGP Date Apr 11, 2002 Job No. 97362