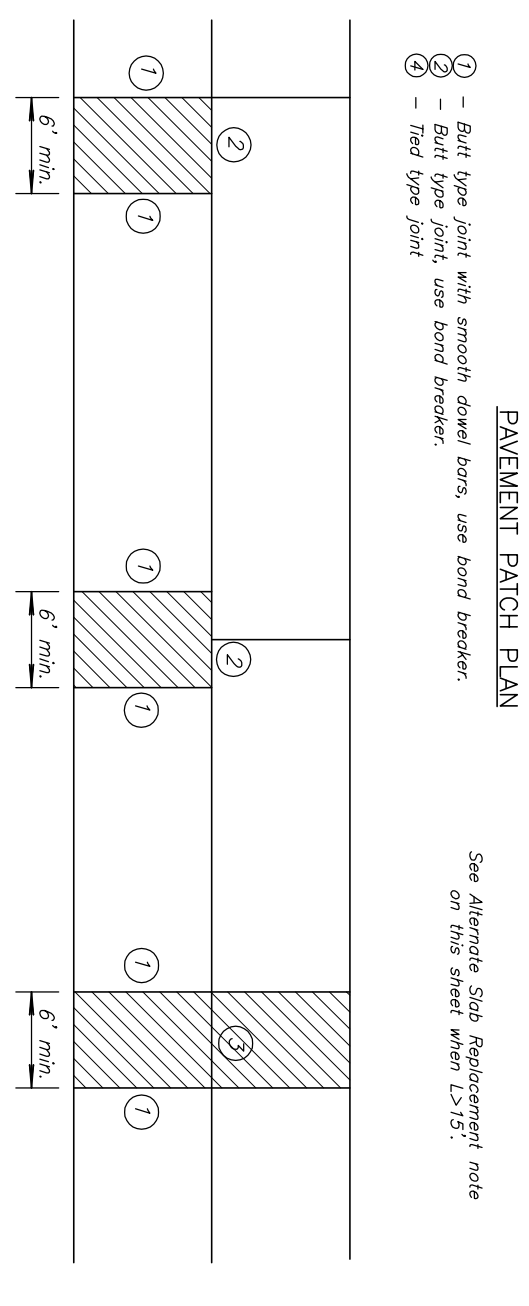
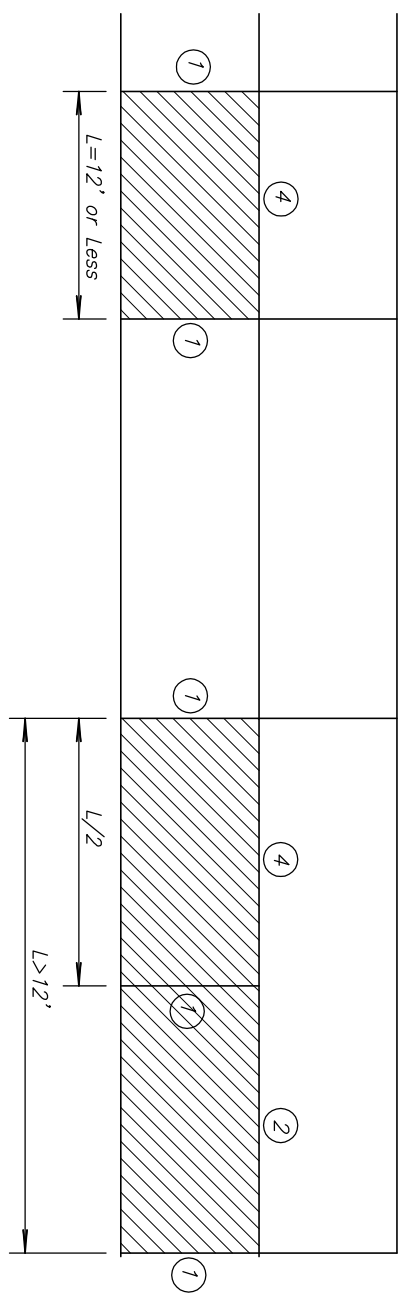


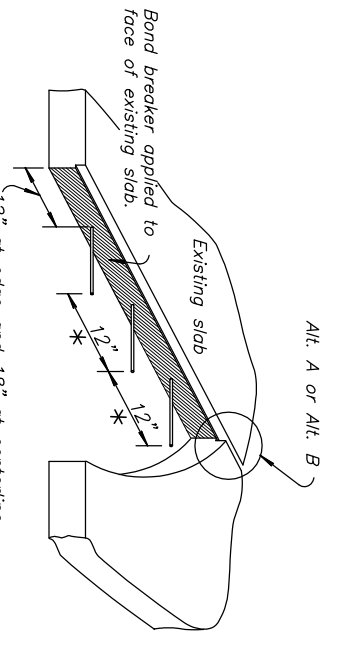
STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	87 N-0190-01	2005	20	96



- PAVEMENT PATCH PLAN**
- ① - Butt type joint with smooth dowel bars, use bond breaker.
 - ② - Butt type joint, use bond breaker.
 - ③ - Butt type joint (lanes poured separately)
 - ④ - Tied type joint

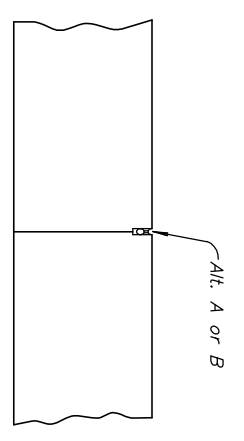
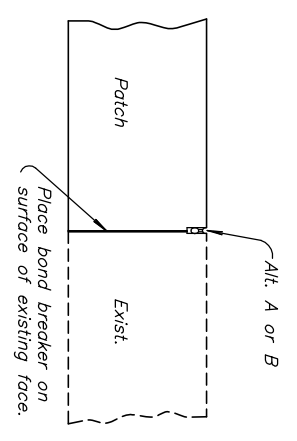
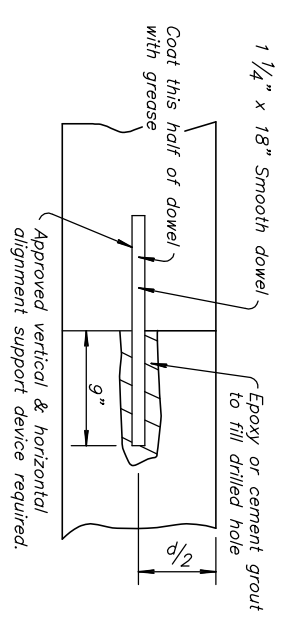
Note:
When practical it should be a general practice to construct slabs the same length as those being repaired. A patch must be at least 6' in length. It can be as long as 30'. However, when the patch exceeds 15' the patch must contain welded wire mesh reinforcement. This mesh shall consist of W4 longitudinal wire at 6" spacing and W4 transverse wire at 12" spacing. Mesh shall be placed at mid-depth of slab and shall extend to within 6" of the longitudinal edge and to within 9" of the transverse edge of the patch. If a continuous repair exceeds 30' then the repair should be divided into as many as 30' slabs as possible. If the remaining portion is between 1' and 5' combine it with the last 30' segment and divide it into equal parts. If the remaining portion is between 6' and 29' leave as is. The above does not prevent the contractor from using 15' slab lengths throughout.

See Sheet No. 19 for Sawed Joint Details.

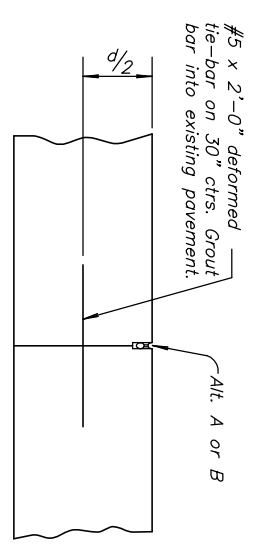


TYPE ① - CONTRACTION JOINT

* Install smooth dowels completely across section.
1 1/4" x 18" See Type ① - Detail.

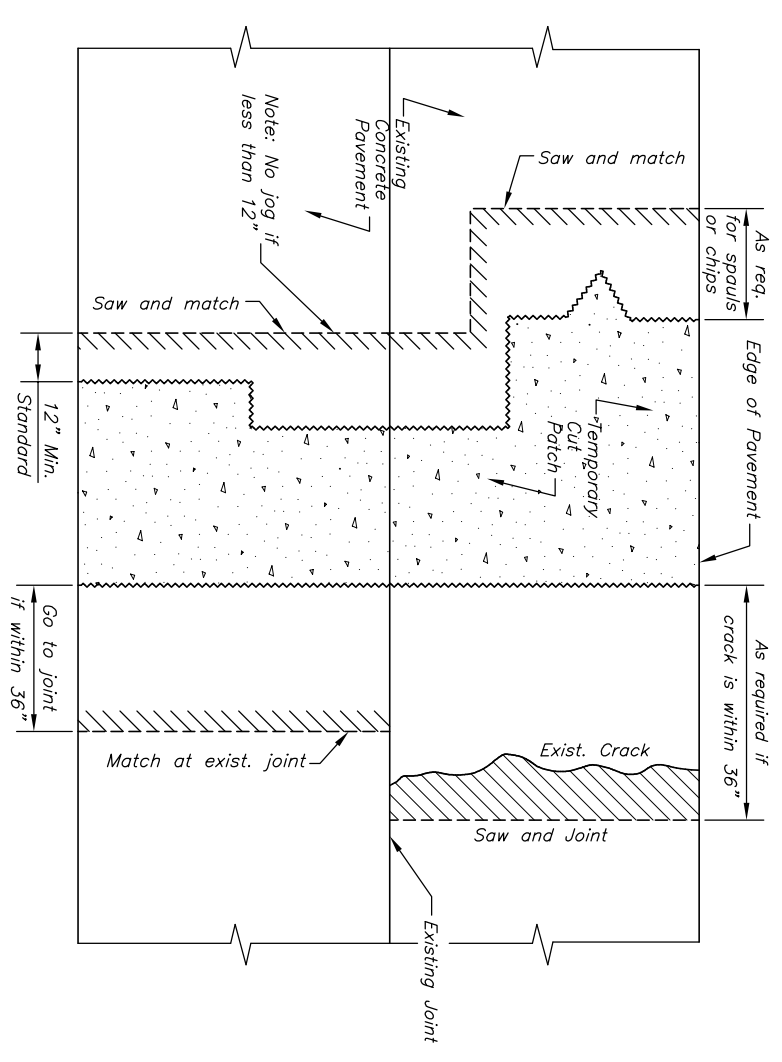


TYPE ③ - CENTERLINE JOINT
(FULL WIDTH PATCH)
(TWO LANES POURED SEPARATELY)



TYPE ④ - CENTERLINE JOINT
(FULL WIDTH PATCH)

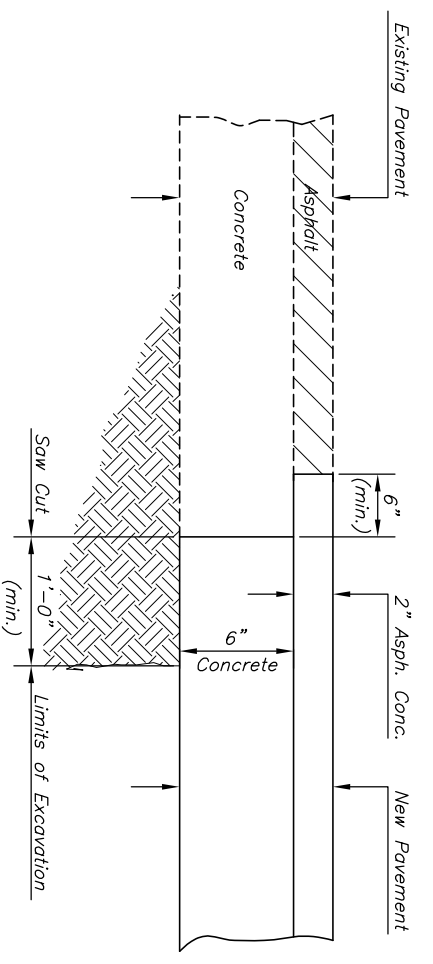
NOTE: SAW CUTTING REQUIRED FOR PANEL REPLACEMENT, JOINT REPAIR, PAVEMENT AND GUTTER REPLACEMENT IS SUBSIDIARY TO THESE WORK ITEMS.



UTILITY PATCH REPAIR

Lines to be perpendicular or parallel to centerline road

Not all conditions are shown, so some flexibility may be required. Since the Utility is ultimately responsible for the repair, whatever work required to do the patch to the satisfaction of the City shall be charged to the Utility.



UTILITY CUT REPAIR
(Asphalt Concrete Patch)

J:\CIVIL\04178\dwg\DETAILS\04178red2.dwg, 5/17/2005, 9:39:22 AM CST			
KANSAS DEPARTMENT OF TRANSPORTATION			
21ST STREET NORTH IMPROVEMENTS			
CONCRETE PAVEMENT PATCHING			
PROJECT NO.	87 N-0190-01	SEDCWICK CO	
M K E C ENGINEERING CONSULTANTS, INC.		WICHITA, KANSAS	
DESIGNED BY:	LAG	CHECKED BY:	JRA
DRAWN BY:	PRC	DATE:	3/18/05
		SHEET 20 OF 96	