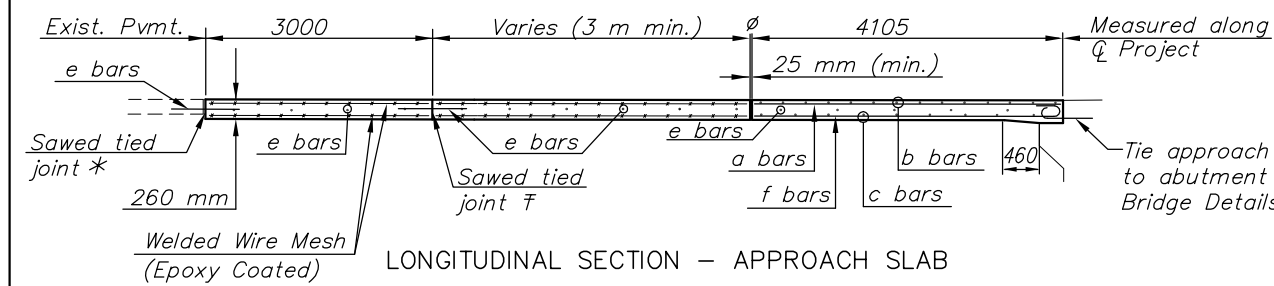
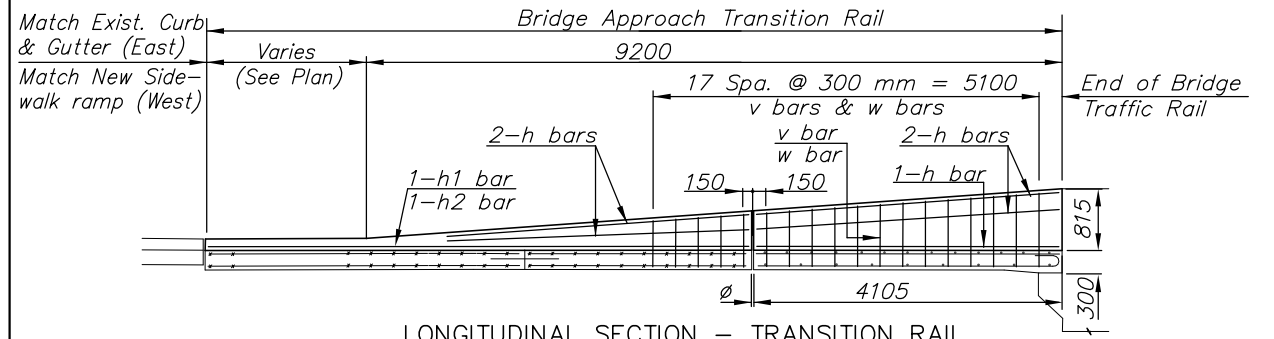
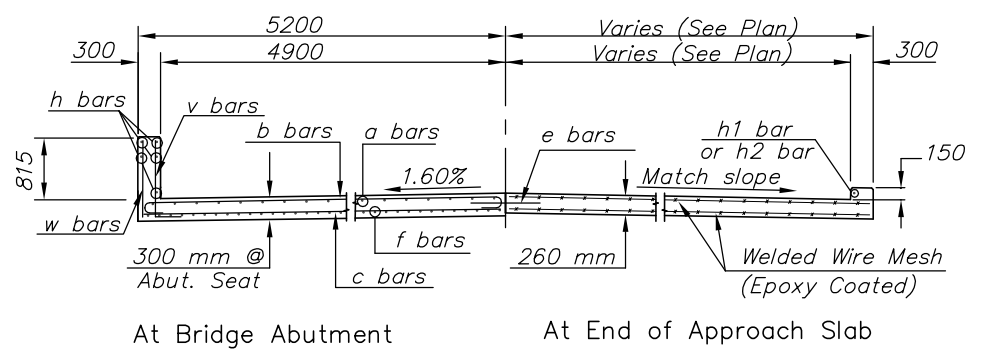


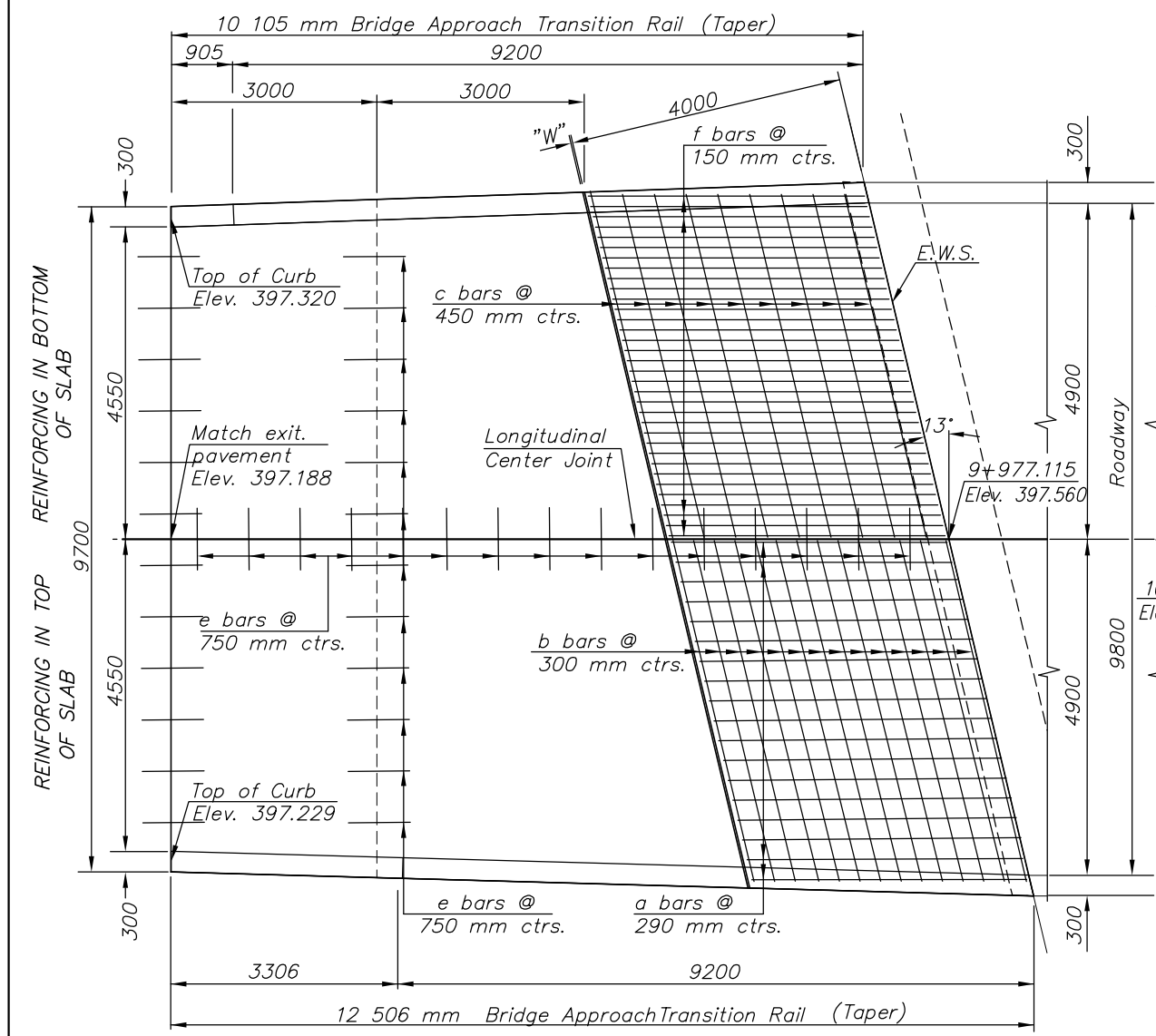
STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	87 N-0187-01	2006	5	47



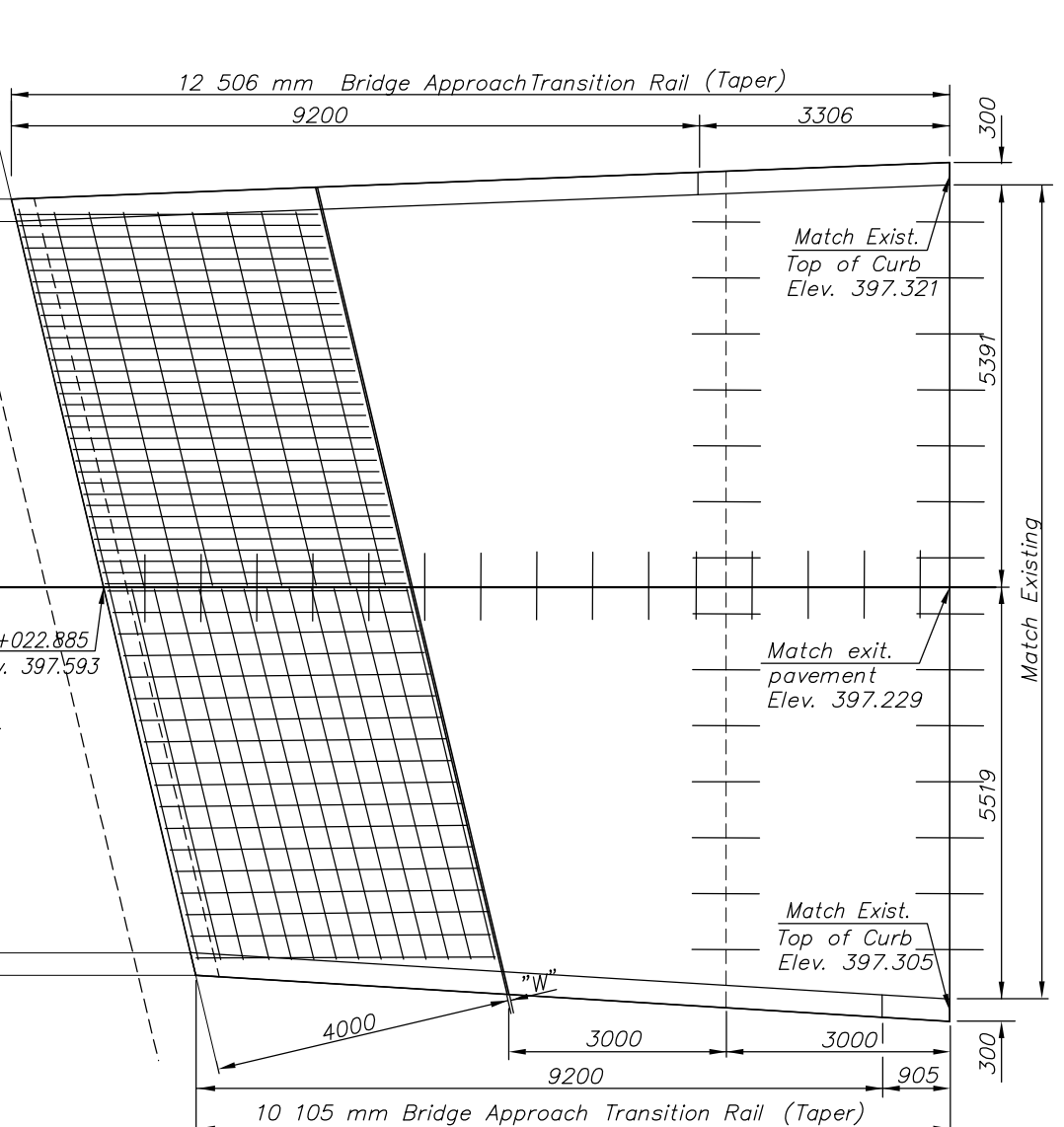
∅ Expansion Joint. See Std. RD662SI.
 † Contractor has the option of substituting a Tied Keyed Construction Joint.
 * Bars shall be drilled and grouted into existing pavement.



At Bridge Abutment At End of Approach Slab
 TRANSVERSE SECTION OF SLAB AND RAIL
 Transition cross slope from 1.60% at E.W.S. to match existing cross slope at end of approach slab



WEST PLAN OF APPROACH SLAB

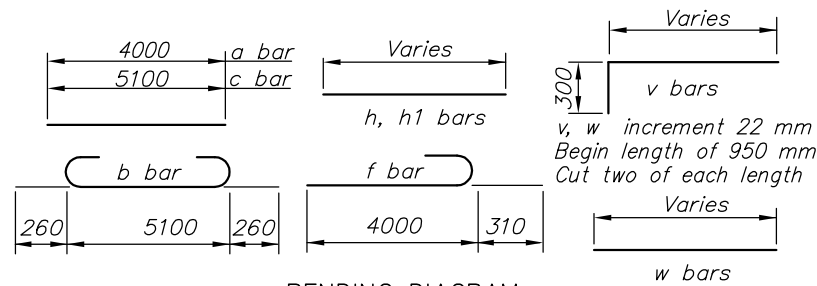


EAST PLAN OF APPROACH SLAB

Notes:
 1. Spacing of longitudinal reinforcing bars is normal to center line.
 Spacing of transverse reinforcing bars is parallel to center line.

GENERAL NOTES

1. Special Concrete Bridge Approach shall be paid for as sq. m. of "Concrete Pavement (260 mm Unif.)(AE)" and includes all work and materials required to construct the approach slab as shown on this sheet.
2. All work and materials required for installation of expansion joints and pressure relief joints shall be subsidiary to this bid item.
3. All preformed joint material shall be installed perpendicular to pavement surface. Formed joints shall be edged with 6 mm radius tool for length of joint.
4. At the Contractor's option #13 x 900 mm tie bars @ 380 mm centers may be substituted for the #19 e bars at 750 mm centers.
5. Clearance from the face of concrete for all reinforcing steel shall be 50 mm.
6. Standard reinforcing bar hooks in accordance with the latest ACI specifications shall be used throughout.
7. See Standard Drawing RD661 SI for details of joint and reinforcing mesh.
8. Transition Rail to be paid as m "Concrete Safety Barrier (Special)(AE) and includes all work and materials required for construction as shown.
9. All reinforcing steel shall be epoxy coated.



BENDING DIAGRAM

All dimensions are out to out unless otherwise noted.
 All bars are epoxy coated

BILL OF MATERIALS - 13° Skew						
Bar		Schedule		△ See Bending Diagram		
SLAB	Bar	a	b	c	e	f
	No.	72	56	36	82	136
	Size	16	16	16	19	19
Length		4000	5620	5100	900	4310
RAIL	Bar	h	h1	h2	v	w
	No.	36	2	2	72	72
	Size	16	16	16	13	13
Length		4000	5900	8240	△	△
Reinforcing Steel						2939 kg
Concrete Pavement (260 mm Unif.)(AE)						227.4 m ²
Expansion Joint						20 860 mm
Concrete Safety Barrier (Special)(AE)						45.2 m

Quantities listed for east and west approach slabs and rails.
 Reinforcing steel and joint lengths shown for information only.

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 KANSAS DEPARTMENT OF TRANSPORTATION
 APPROACH SLAB AND TRANSITION RAIL DETAILS
 PROJECT NO. 87 N-0187-01 SEDGWICK CO.
 M K E C ENGINEERING CONSULTANTS, INC.
 WICHITA, KANSAS

DESIGNED BY:	KJS	CHECKED BY:	KJS
DRAWN BY:	DPG/JSB	DATE:	DEC. 2005 SHEET 5 OF 47