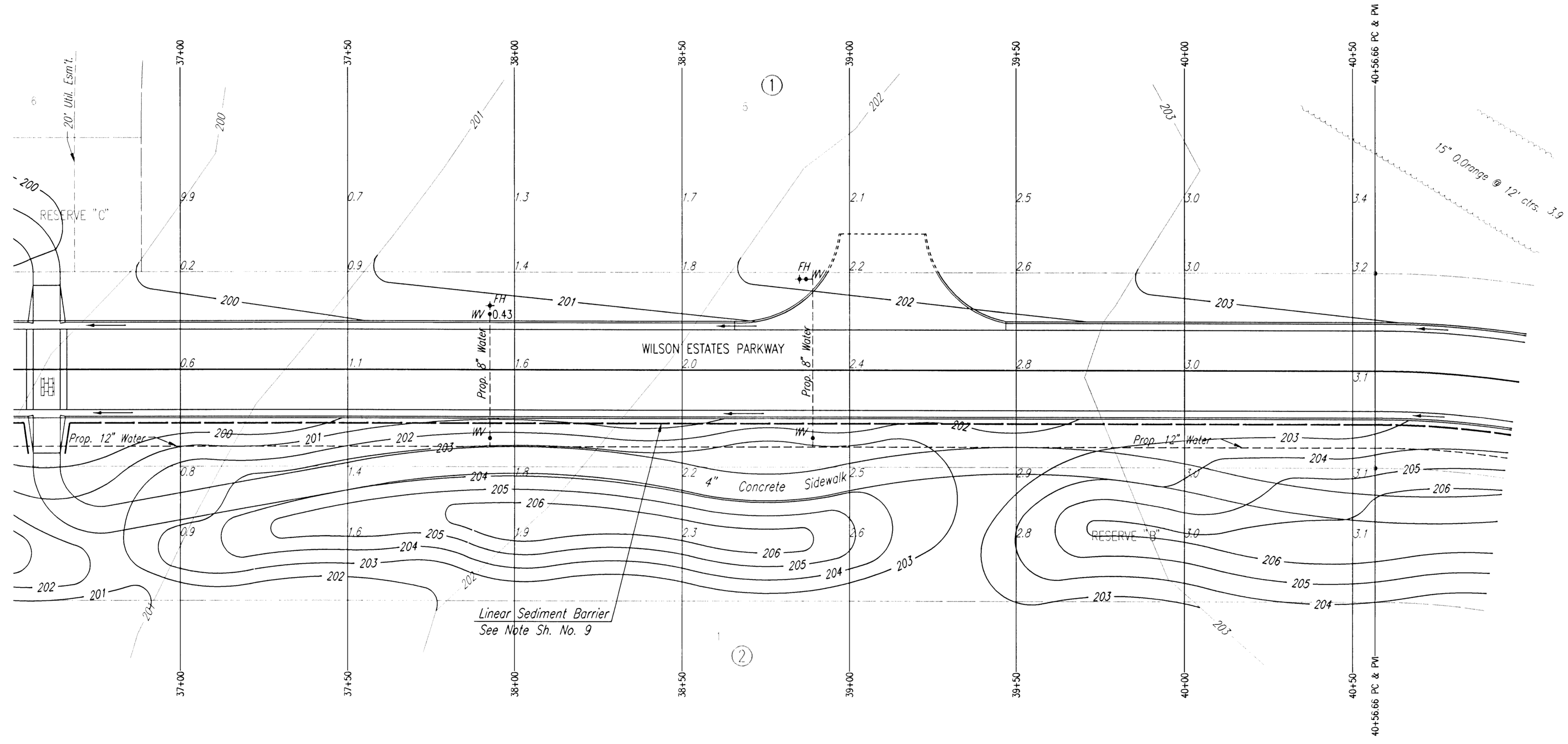



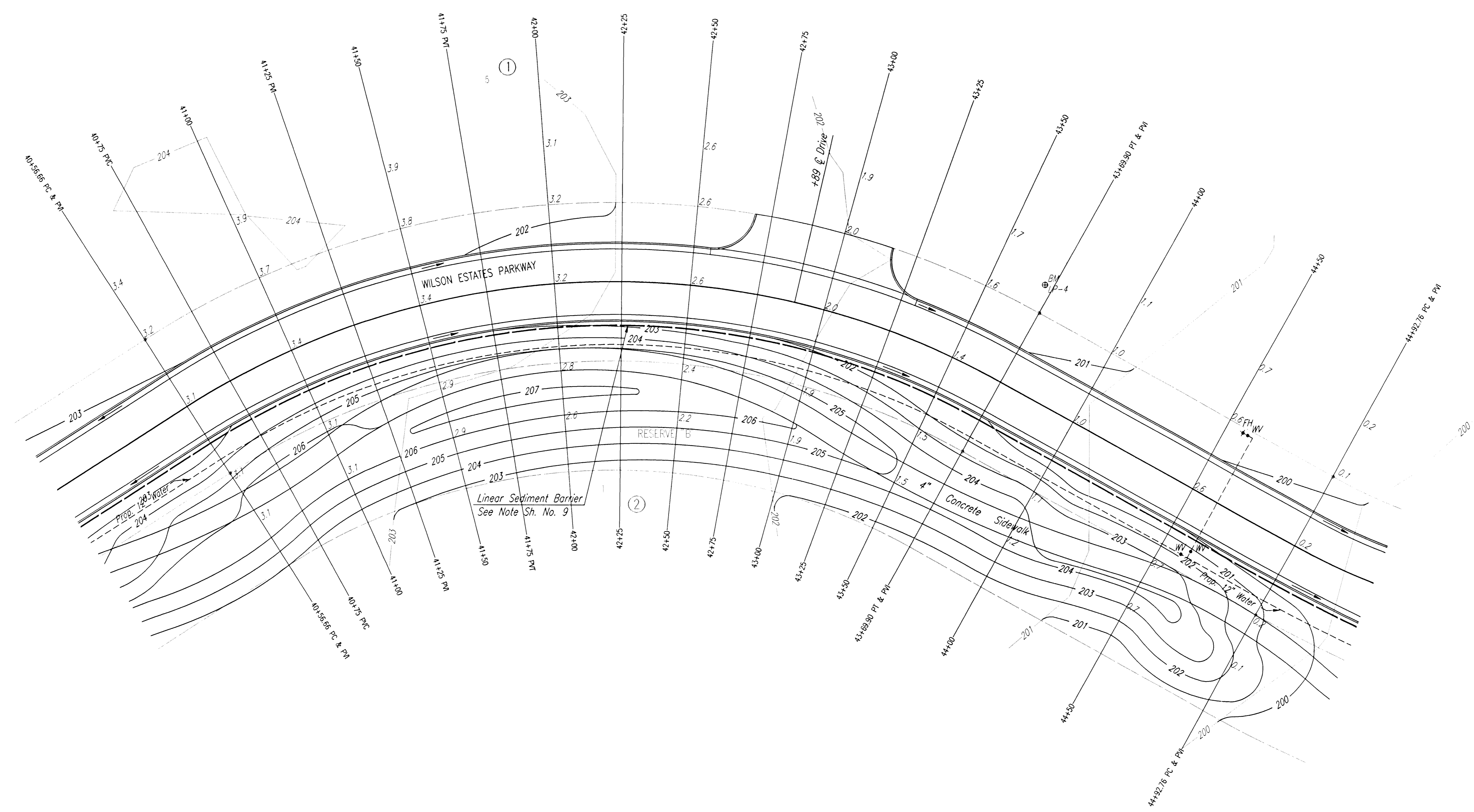
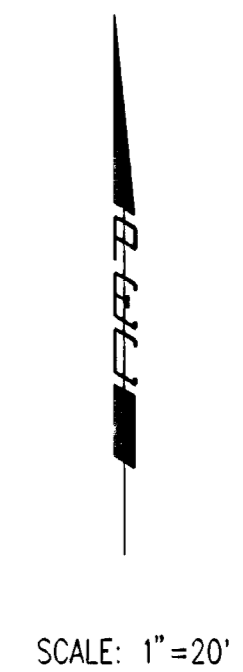
SCALE: 1"=20'



EXAM. AGO. UPPER DEP. SCALE: 1"=20.00
 01/20/00 00:00:00 SWEWALK 05-31-2000 11:51:15 am

WILSON ESTATES PARKWAY			
WILSON ESTATES PARKWAY			
STA. 37+00.00 TO STA. 40+56.66			
 Professional Engineering Consultants, P.A. <small>303 S. TOPEKA • WICHITA, KANSAS 67202 316-262-2691 • FAX 316-262-3003</small>			
Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

PROJECT NO. 472-83175	SHEET NO. 11	TOTAL SHEETS 24
--------------------------	-----------------	--------------------



USHP, GDD, GEP, DEP, SCALE: 1"=20.00
 2/12/2000 11:53:30 AM

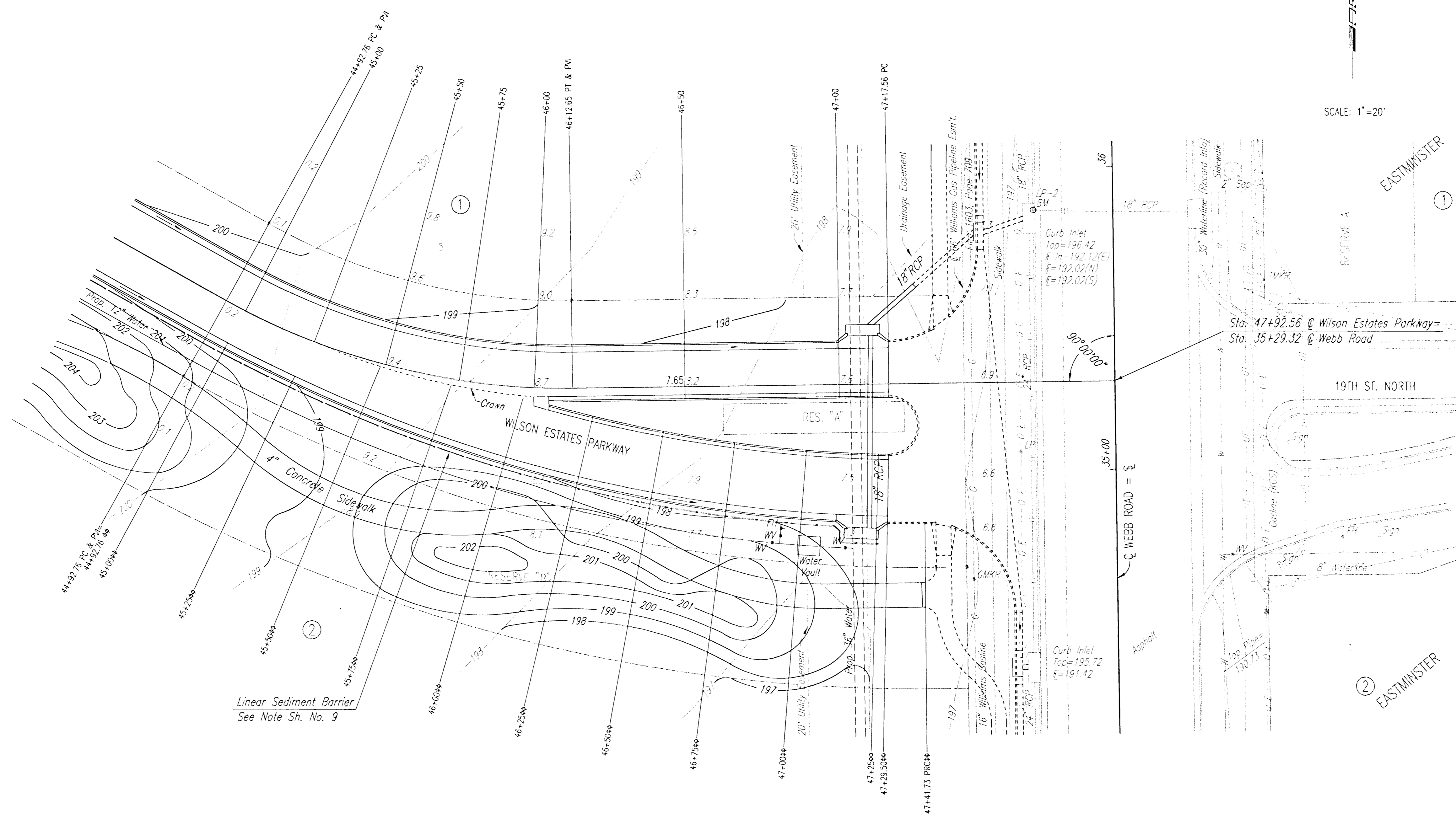
WILSON ESTATES PARKWAY

WILSON ESTATES PARKWAY
STA. 40+56.66 TO STA. 44+92.76

Professional Engineering Consultants, P.A.
 303 S. TOPEKA • WICHITA, KANSAS 67202
 316-262-2691 • FAX 316-262-3003

Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-83175	12	24



SCALE: 1"=20'

Sta. 47+92.56 @ Wilson Estates Parkway =
Sta. 35+29.32 @ Webb Road

Linear Sediment Barrier
See Note Sh. No. 9

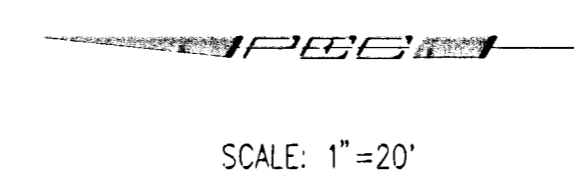
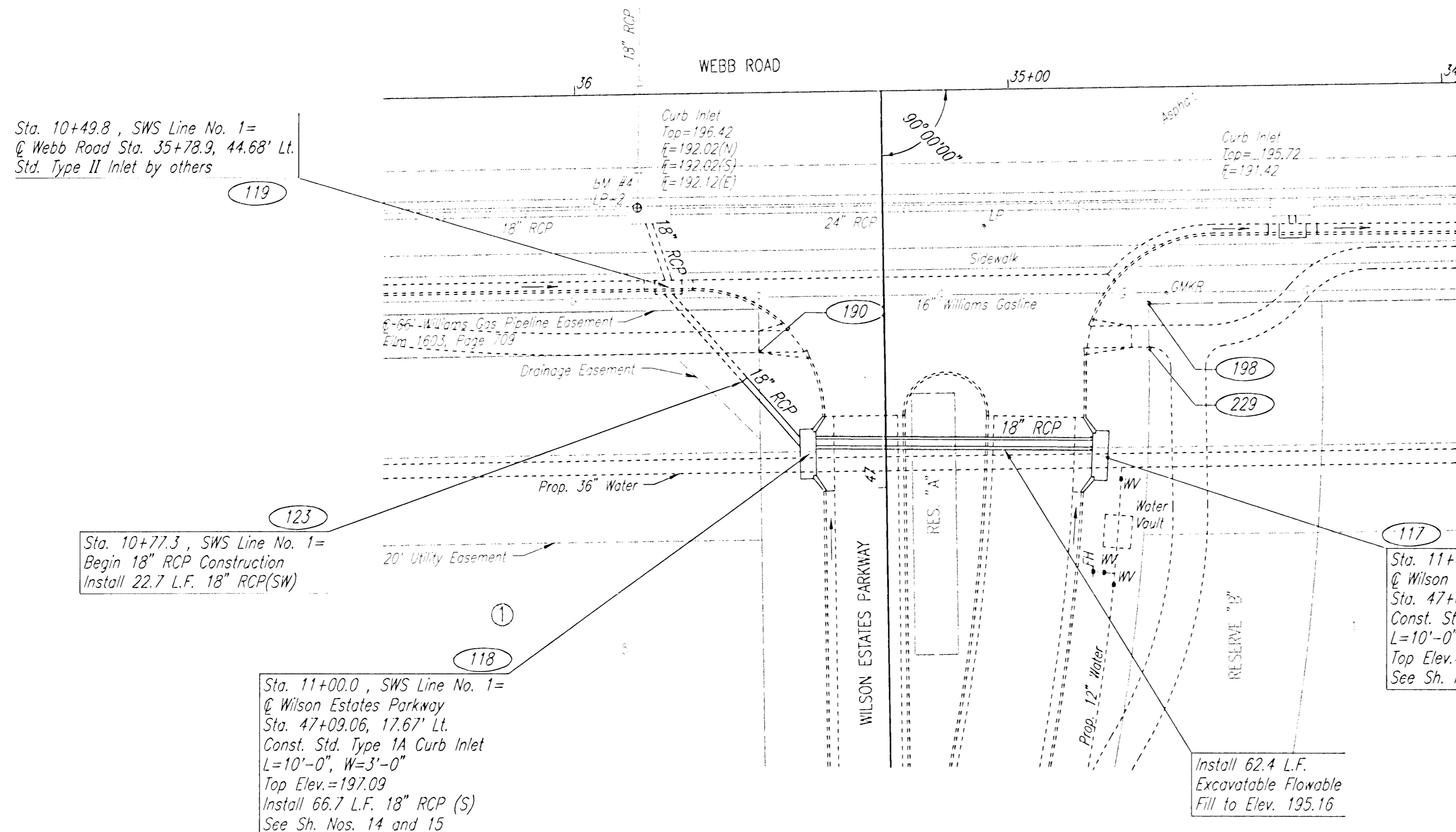
◆◆ RIGHT FACE OF CURB STATION

WILSON ESTATES PARKWAY
STA. 44+92.76 TO STA. 47+92.76

Professional Engineering Consultants, P.A.
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Designed by	BER, GOD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

CONR. W/EN. C/TKR. CAD. SCALE. 1"=20'
 03/20/00 08:54:04 134 1816 1614 4 105 24 2002 08 01 10 11 PM



COORDINATE LIST		
POINT	N	E
117	15659.5475	18625.9134
118	15726.2906	18626.1466
119	15758.8381	18664.4498
123	15741.0900	18643.4226
190	15737.9929	18649.4617
198	15648.1633	18660.8918
229	15647.9966	18650.4121

100 COORDINATE POINT NUMBER

Sta. 10+49.8, SWS Line No. 1=
@ Webb Road Sta. 35+78.9, 44.68' Lt.
Std. Type II Inlet by others

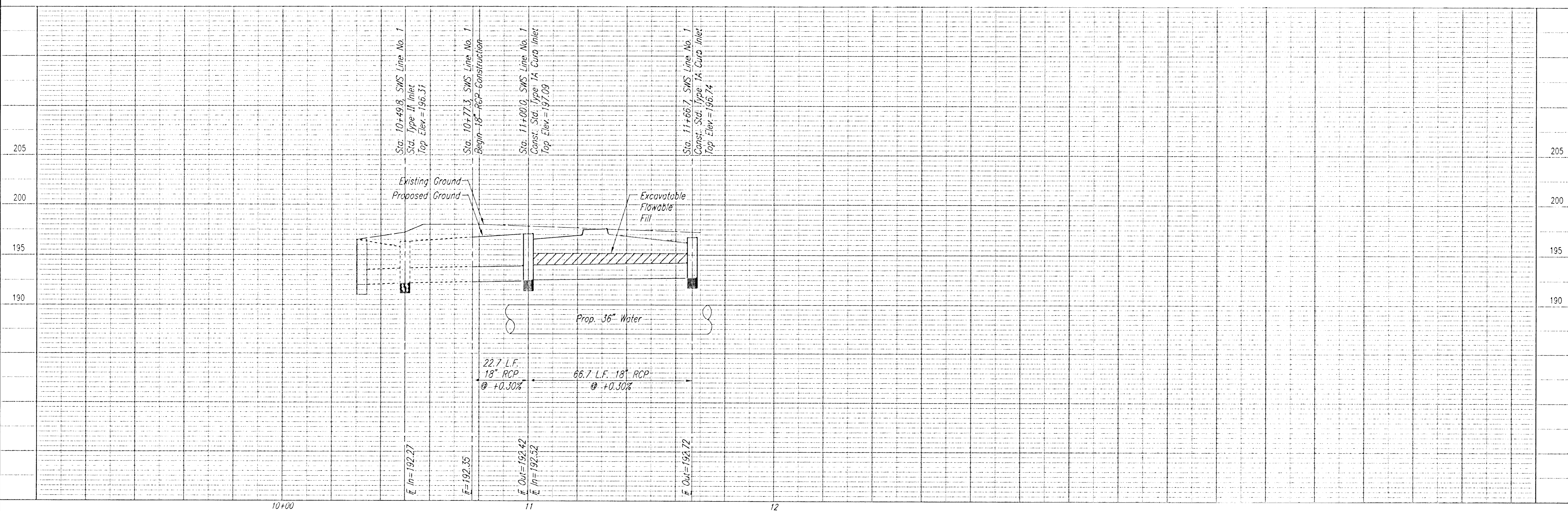
Sta. 10+77.3, SWS Line No. 1=
Begin 18" RCP Construction
Install 22.7 L.F. 18" RCP(SW)

Sta. 11+00.0, SWS Line No. 1=
@ Wilson Estates Parkway
Sta. 47+09.06, 17.67' Lt.
Const. Std. Type 1A Curb Inlet
L=10'-0", W=3'-0"
Top Elev.=197.09
Install 66.7 L.F. 18" RCP (S)
See Sh. Nos. 14 and 15

Sta. 11+66.7, SWS Line No. 1=
@ Wilson Estates Parkway
Sta. 47+07.77, 49.06' Rt.
Const. Std. Type 1A Curb Inlet
L=10'-0", W=3'-0"
Top Elev.=196.74
See Sh. Nos. 14 and 15

Install 62.4 L.F.
Excavatable Flowable
Fill to Elev. 195.16

NOTE: CONTRACTOR SHALL NOT BEGIN CONSTRUCTION ON
SWS LINE NO. 1 UNTIL PROPOSED 36" WATER LINE
IS IN PLACE. C.O.W. PROJ. NO. 448-89465.



WILSON ESTATES PARKWAY
LINE NO. 1
STA. 10+77.3 TO STA. 11+66.7

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Designed by BER, GDD
Drawn by BPP

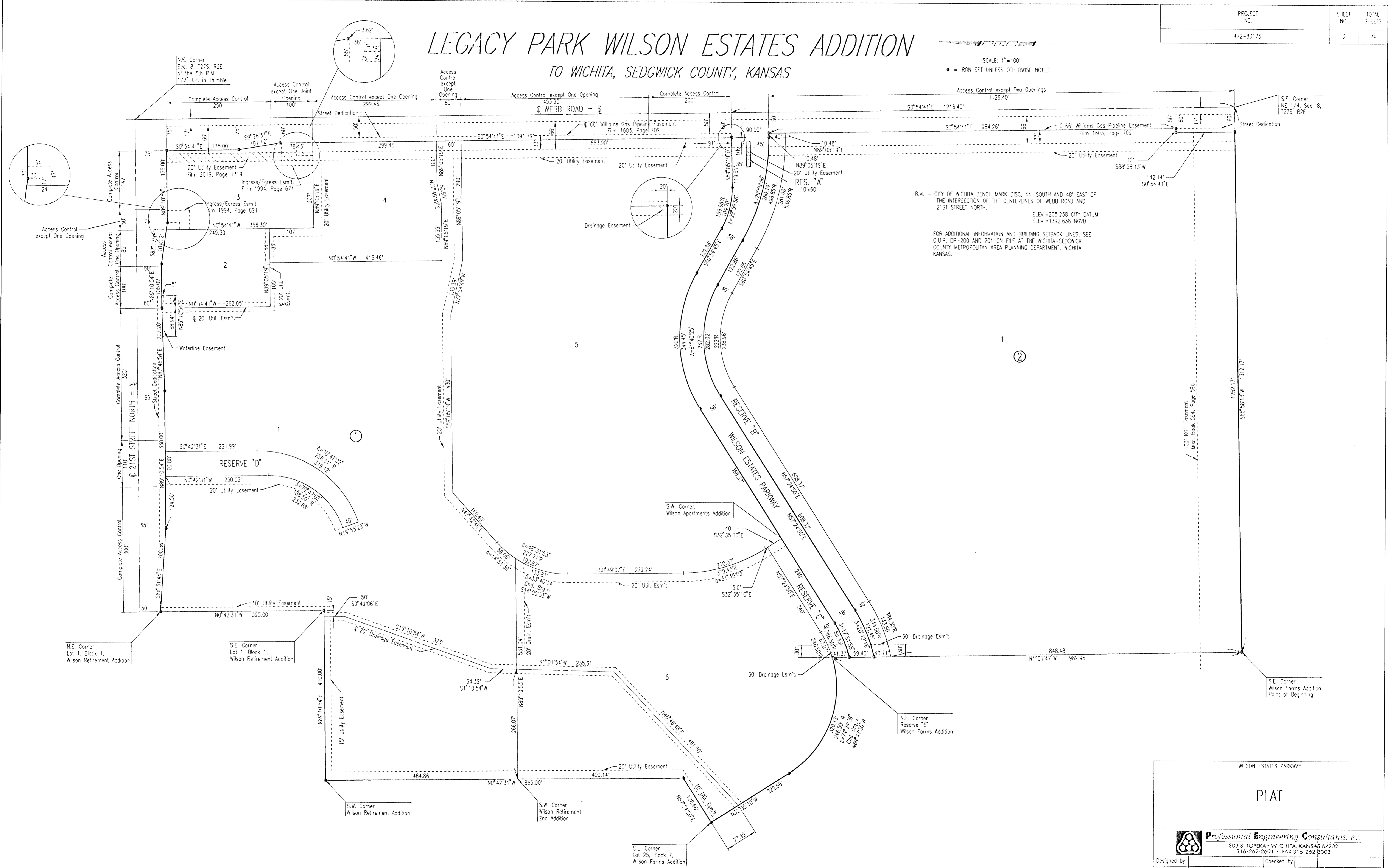
Job No. 00034
Date: MARCH, 2003

C:\MSR\GDD\DWG\SAU\SCALE 1-20.dwg
C:\ZONED\PROJ\448\DWG\SAU 05-20-2003 10:43:24 am

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-83175	2	24

LEGACY PARK WILSON ESTATES ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

SCALE: 1"=100'
● = IRON SET UNLESS OTHERWISE NOTED



B.M. - CITY OF WICHITA BENCH MARK DISC, 44' SOUTH AND 48' EAST OF THE INTERSECTION OF THE CENTERLINES OF WEBB ROAD AND 21ST STREET NORTH.
ELEV.=205.238 CITY DATUM
ELEV.=1392.638 NGVD

FOR ADDITIONAL INFORMATION AND BUILDING SETBACK LINES, SEE C.U.P. DP-200 AND 201 ON FILE AT THE WICHITA-SEDGWICK COUNTY METROPOLITAN AREA PLANNING DEPARTMENT, WICHITA, KANSAS.

WILSON ESTATES PARKWAY

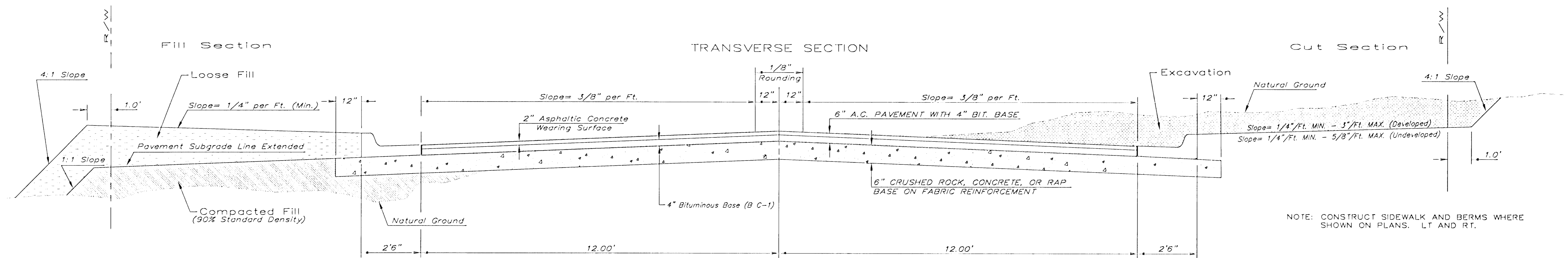
PLAT

Professional Engineering Consultants, P.A.
303 S. TOPEKA • WICHITA, KANSAS 67202
316-262-2691 • FAX 316-262-8003

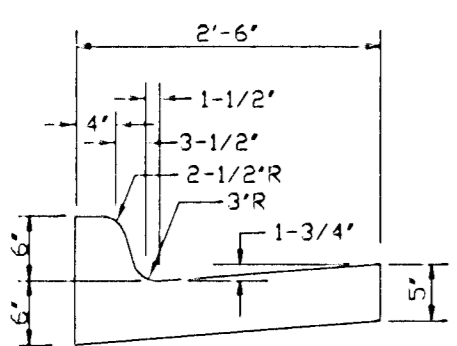
Designed by	Checked by	
Drawn by	Date MARCH, 2000	Job No. 00034

DSNR, BER, OPER, S&D SCALE: 1"=100.00
 Q:\2000\00034\PLAT 05-26-2000 04:41:30 pm

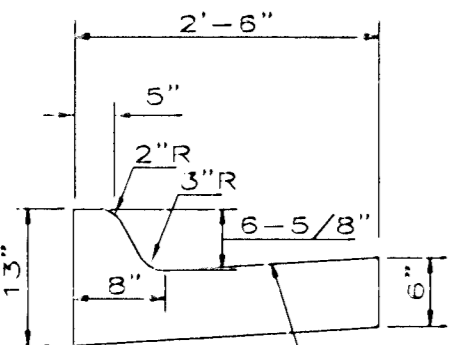
TYPICAL 29' B-B PAVEMENT DETAILS



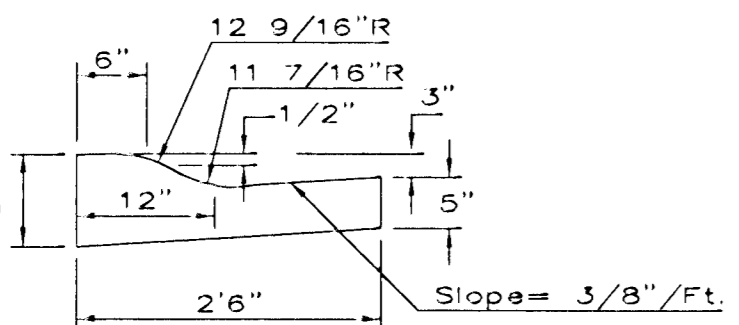
NOTE: CONSTRUCT SIDEWALK AND BERMS WHERE SHOWN ON PLANS. LT AND RT.



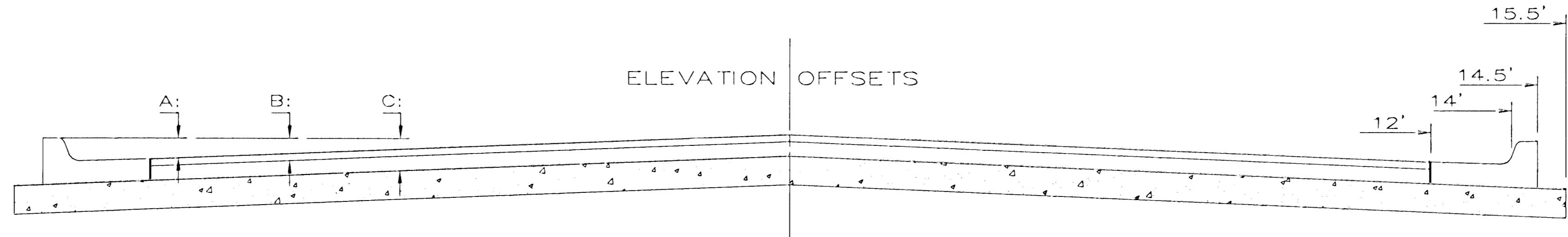
STATE CURB MODIFIED TYPE I COMBINED CURB & GUTTER



COMBINED CURB & GUTTER



COMBINED ROLL TYPE CURB & GUTTER

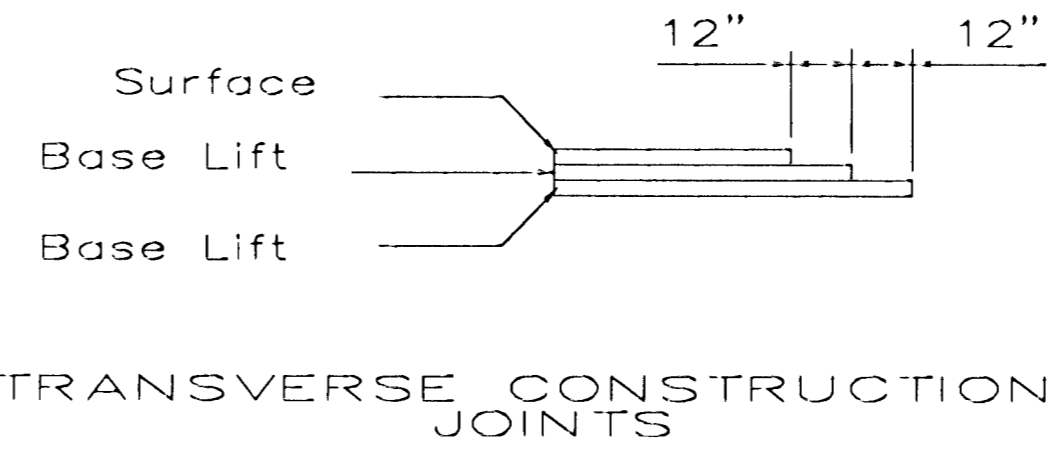


	DISTANCE FROM CENTERLINE (LT. & RT.)										
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	15.5'
A: Top of Curbs to Top of Surface Lift	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	-	-	-
B: Top of Curbs to Top of Upper Base Lift	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	-	-	-
C: Top of Curbs to Top of C.R. Subgrade	0.63	0.68	0.74	0.80	0.83	0.86	0.93	0.99	1.05	1.06	1.09

CRUSHED ROCK GRADATION REQUIREMENTS

PERCENT OF AGGREGATE RETAINED	
2-1/2"	0
3/4"	20-60
#4	50-80
#40	80-94
#200	90-98

ROCK QUALITY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY THE KOOT 1930 EDITION STANDARD SPECIFICATION SUBSECTION 1102 FOR DURABILITY CLASS I



Transverse construction joints shall be constructed in flexible base pavements at locations where pavement joints existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards 6" ASPHALTIC CONCRETE (4" BITUMINOUS BASE).

General Notes

FABRIC BASE REINFORCEMENT SHALL BE B X 1100 GEOGRID AS MANUFACTURED BY TENSAR CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

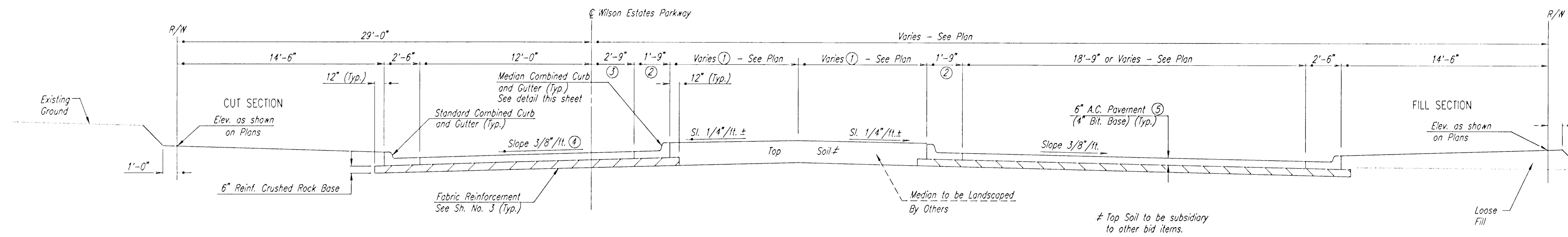
BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 6" ASPHALTIC CONCRETE (4" BITUMINOUS BASE).

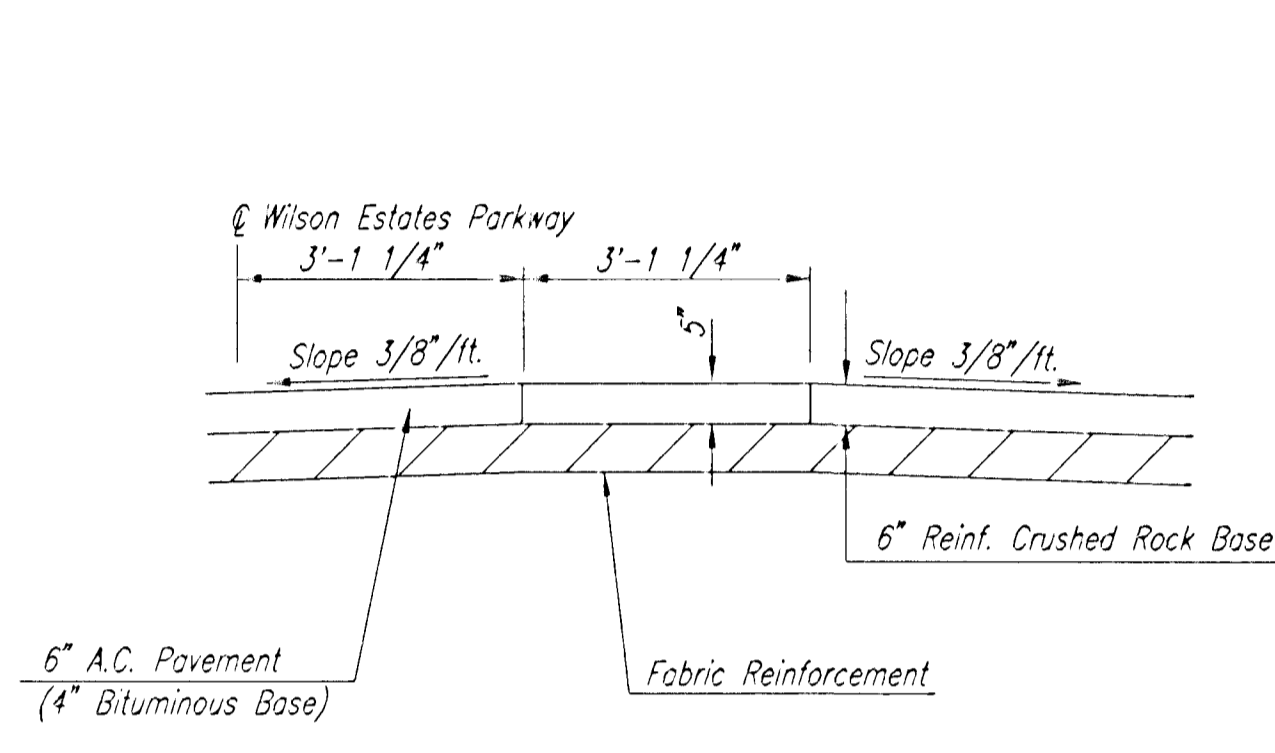
DSNR, BER, OPER, S&D, SCALE, 1=1.00, 01/20/00, 000, 04/29-517P, 05-26-2000, 04:42:11, pm

<p>CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202 (316) 268-4501 (316) 268-4114 FAX</p>	<p>29' PAVEMENT 6" ASPHALTIC CONCRETE W/ CRUSHED ROCK BASE</p>	
	<p>M. E. LINDEBAK P.E. - CITY ENGINEER</p>	
	<p>PROJECT NUMBER 472-83175</p>	<p>OCA NO. 765634</p>
	<p>DATE MAR 96</p>	<p>SHEET 3 OF 24</p>

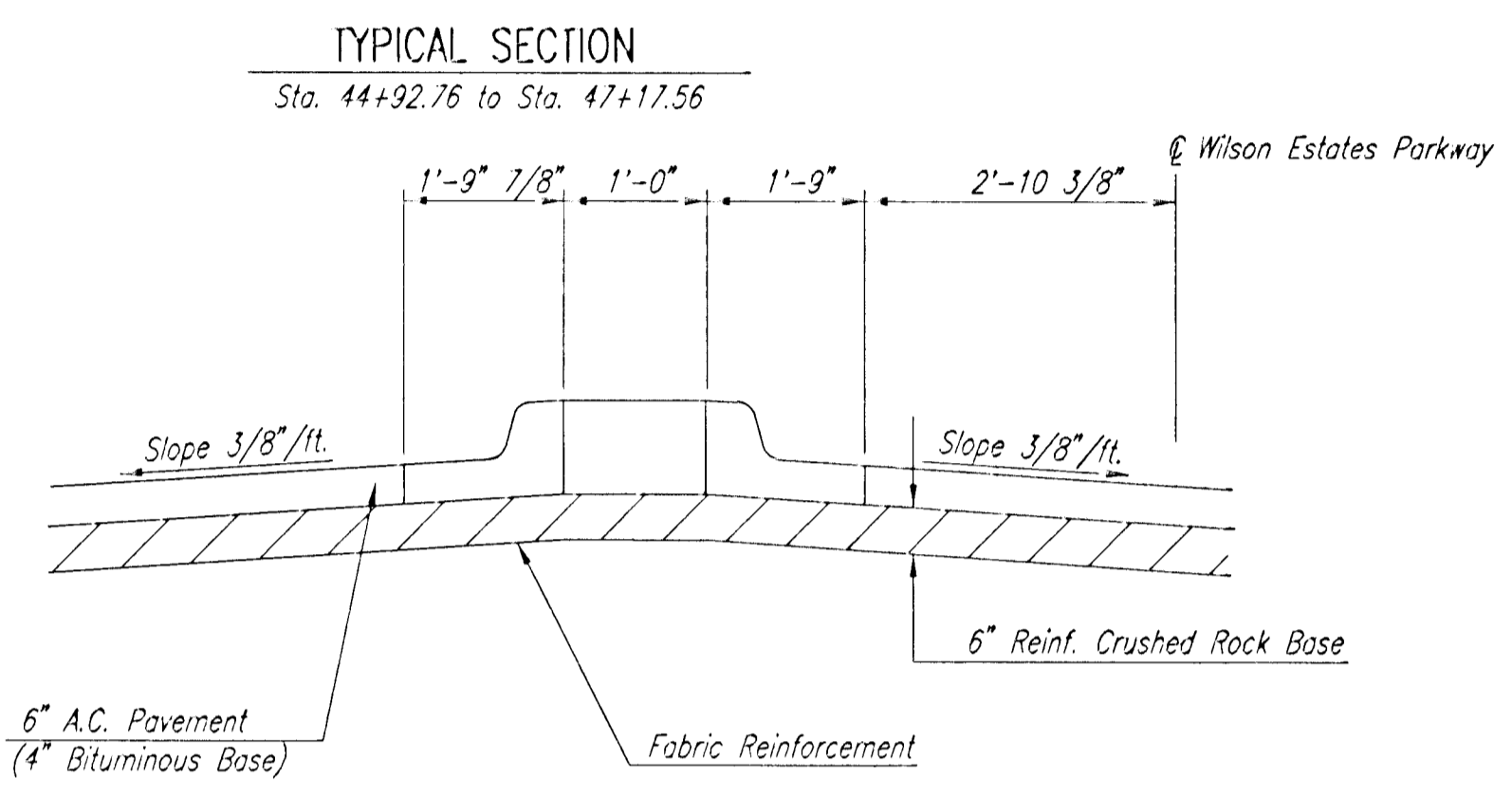


- ① Median Width Varies Sta. 46+04.85 to Sta. 47+17.56
Median Nose Sta. 45+99.91 to Sta. 46+04.85
No Median Sta. 44+92.76 to Sta. 45+99.91
See Plan
- ② 0'-0" from Sta. 44+92.76 to Sta. 45+99.91
See Plan
- ③ Varies Sta. 45+00 to Sta. 46+27.82
See Plan
- ④ Varies from Sta. 44+92.76 to Sta. 45+99.91
See Plan
- ⑤ "Brick Pavement" Sta. 46+73.56 to Sta. 47+17.56 Lt.
and Sta. 46+84.47 to Sta. 47+29.50 to Rt.
See Plan

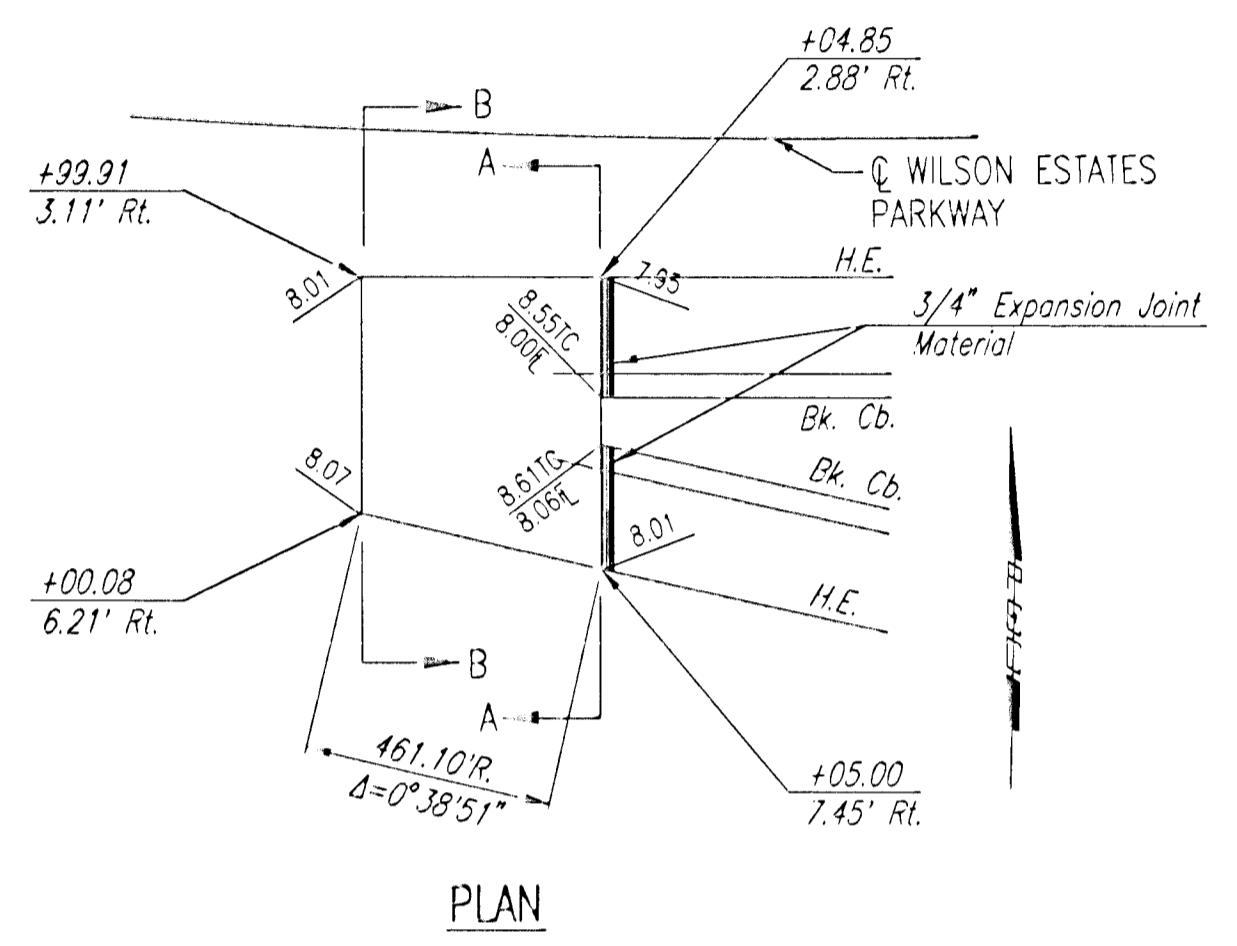
♦♦ RIGHT FACE OF CURB STATION
NOTE: SEE SH. NO. 3 FOR ADDITIONAL PAVING NOTES.



SECTION B-B



TYPICAL SECTION
Sta. 44+92.76 to Sta. 47+17.56

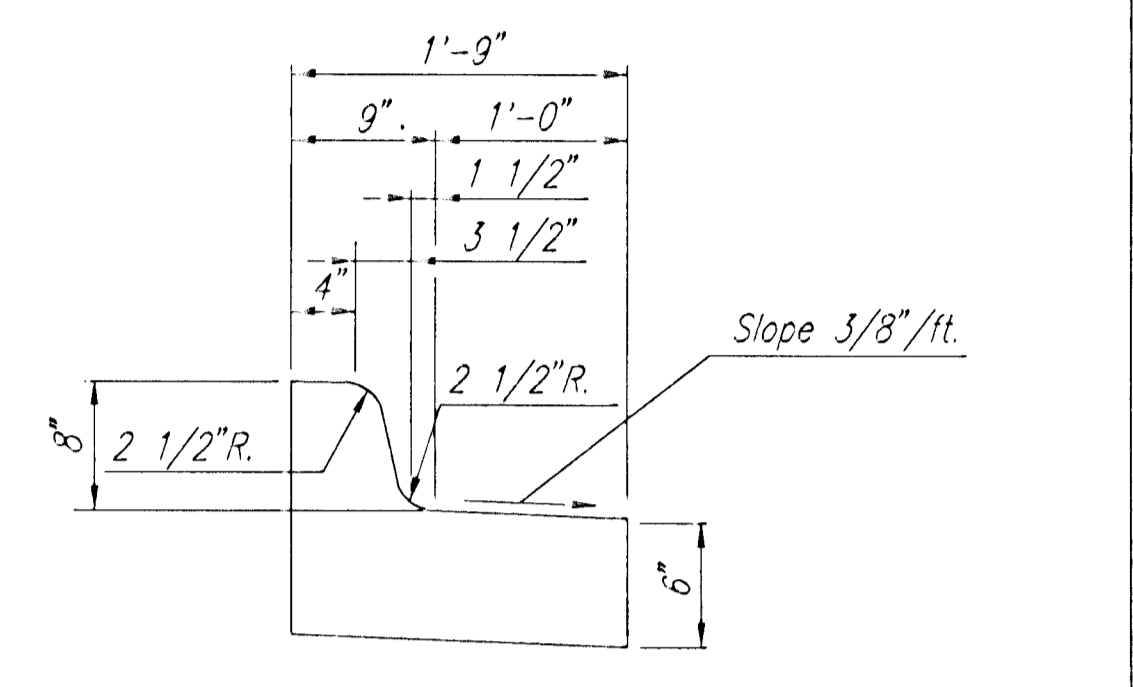


PLAN

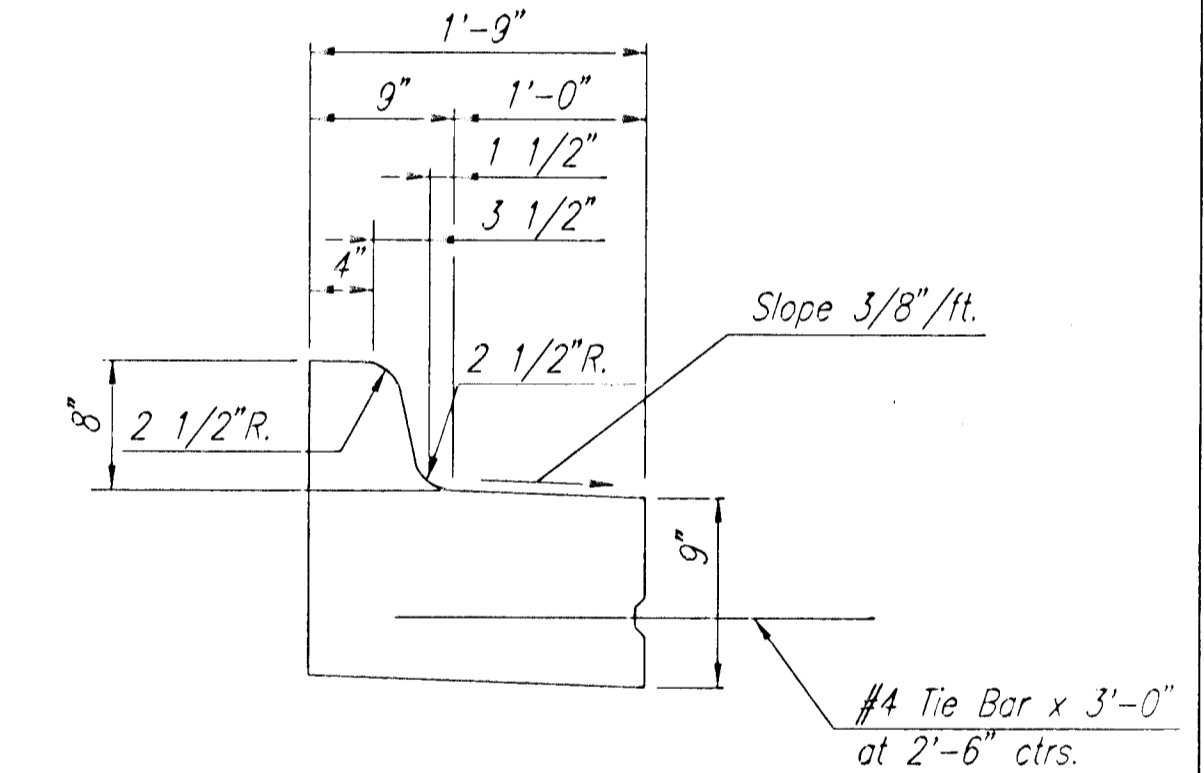
GENERAL NOTES
BRICK PAVEMENT

- CONCRETE PAVING BRICK SHALL BE 2 3/8" MIN. THICKNESS AND MEET OR EXCEED ASTM C-936-82. PAYMENT SHALL BE AT THE CONTRACT BID PRICE PER SQUARE YARD FOR "BRICK PAVEMENT". PAVING BRICK SHALL BE "PLAZA-STONE" PAVERS AS MANUFACTURED BY "PAVESTONE".
- THE BID ITEM "BRICK PAVEMENT" SHALL INCLUDE BRICK PAVERS, FINE SAND, CONCRETE PAVEMENT PLUS ALL TOOLS, LABOR, EQUIPMENT NECESSARY TO CONSTRUCT BRICK PAVEMENT COMPLETE AND IN PLACE.
- SAND BEDDING SHALL MEET THE CITY'S REQUIREMENTS FOR FINE AGGREGATE. AN UNCOMPACTED SAND LAYING COURSE SHALL BE SPREAD EVENLY OVER THE AREA TO BE PAVED AND THEN SCREED TO A LEVEL OF APPROXIMATELY 1" THICKNESS. ONCE SCREED AND LEVELED TO THE DESIRED ELEVATION, THE SAND LAYING COURSE SHALL NOT BE DISTURBED IN ANY WAY.
- THE PAVING BRICK SHALL BE INSTALLED PERPENDICULAR AND PARALLEL TO THE MAJOR AXIS OF THE CROSSWALK OR AREA BEING PAVED. STONES SHALL BE PLACED WITH THE CHAMFERED SIDE UP, AND JOINT SPACES KEPT UNIFORM APPROXIMATELY 1/8" THICK. THE GAPS AT THE EDGE OF THE PAVED SURFACE SHALL BE FILLED WITH STONES CUT TO FIT. CUTTING SHALL BE ACCOMPLISHED TO LEAVE A CLEAN EDGE TOWARD THE TRAFFIC SURFACE, USING A MASONRY SAW. WHENEVER POSSIBLE, NO CUTS SHOULD RESULT WITH A PAVER LESS THAN ONE-THIRD OF ITS ORIGINAL DIMENSION.
- PAVING BRICK SHALL BE VIBRATED TO THEIR FINAL LEVEL IN THE SAND LAYING COURSE BY TWO OR THREE PASSES OF A VIBRATING COMPACTOR CAPABLE OF 3000 TO 5000 POUNDS COMPACTION FORCE WITH THE SURFACE CLEAN AND JOINTS OPEN.
- AFTER VIBRATION, CLEAN CONCRETE SAND SHALL BE SPREAD OVER THE PAVING STONE SURFACE, ALLOWED TO DRY, AND VIBRATED INTO JOINTS WITH ADDITIONAL PASSES OF THE PLATE VIBRATOR SO AS TO COMPLETELY FILL THE JOINTS. A LIGHT COATING OF SAND SHALL BE SWEEPED OVER THE COMPLETED SURFACE AND LEFT TO WEATHER IN.

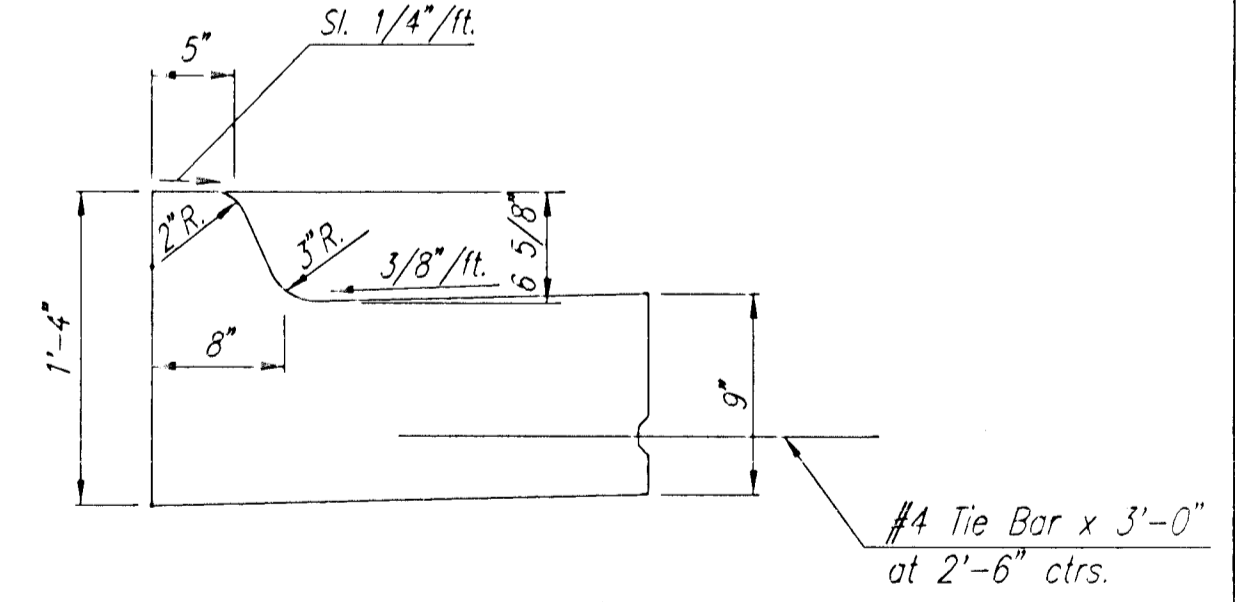
NOTE: "COMBINED CURB AND GUTTER (SPECIAL)(6 5/8") AND "MEDIAN COMBINED CURB AND GUTTER (SPECIAL)(8") SHALL BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER (6 5/8" & 1 1/2") AND "MEDIAN COMBINED CURB AND GUTTER (8")" RESPECTIVELY.



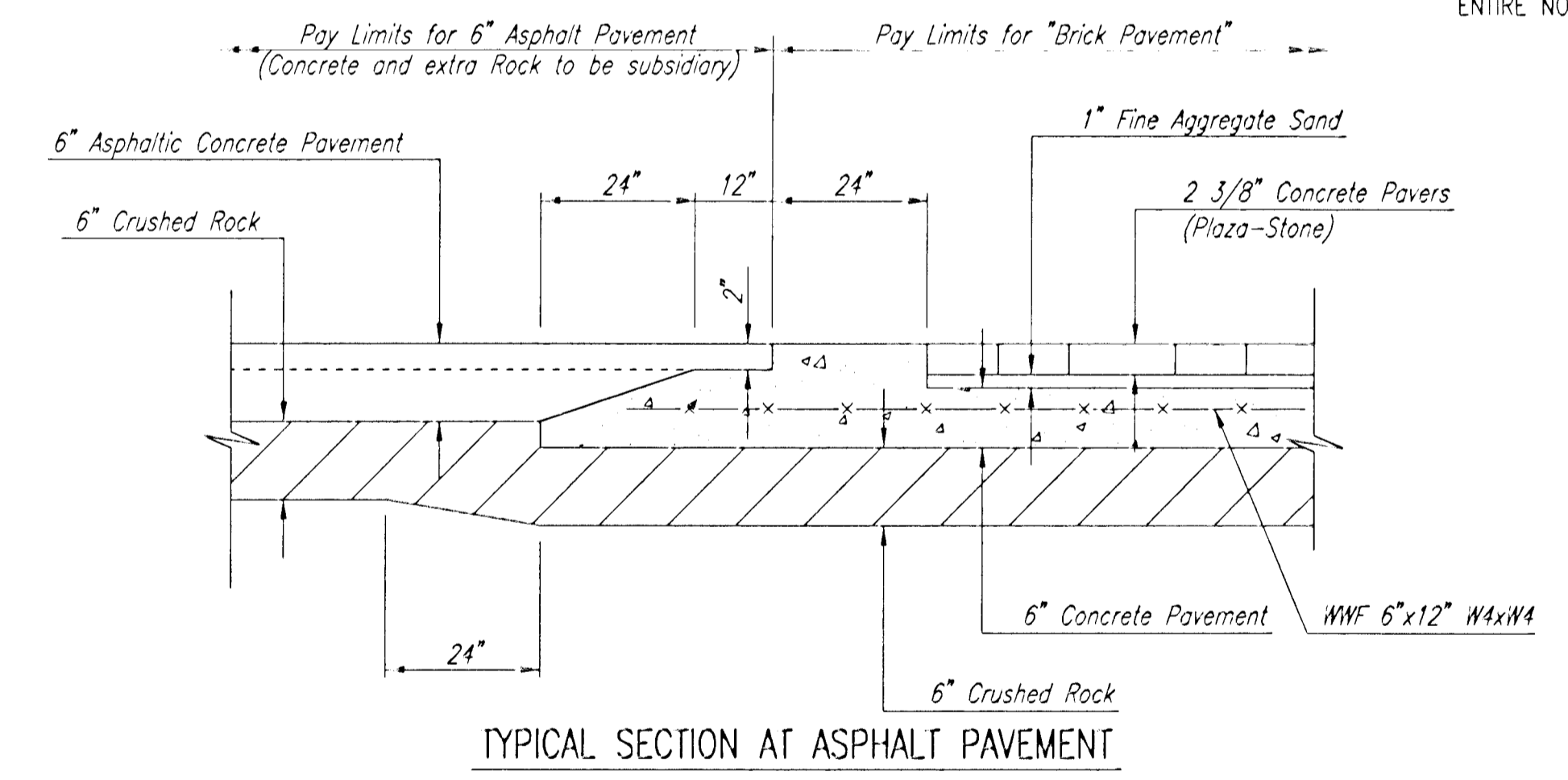
MEDIAN COMBINED CURB AND GUTTER (8")



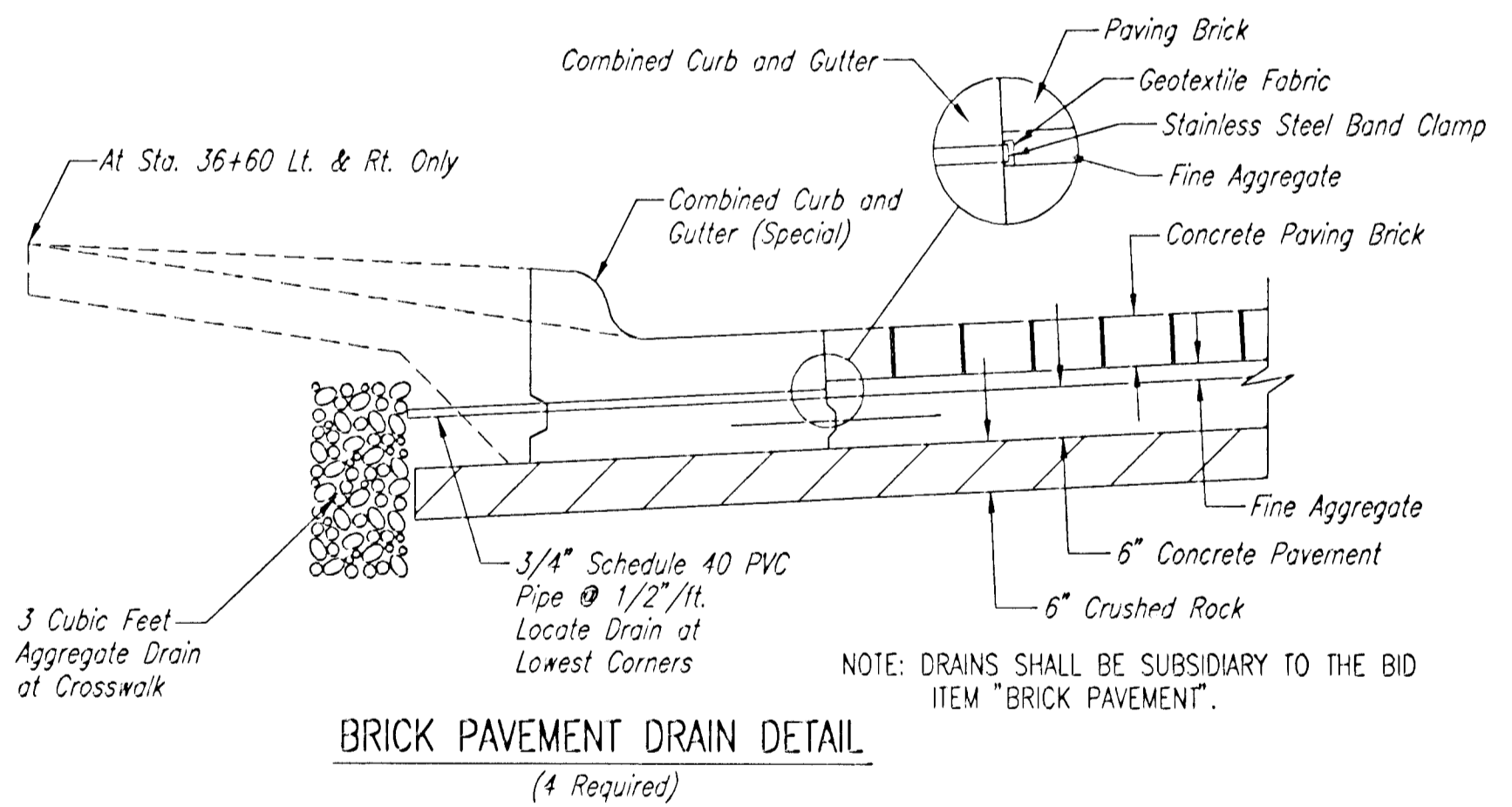
MEDIAN COMBINED CURB & GUTTER (SPECIAL) (8")



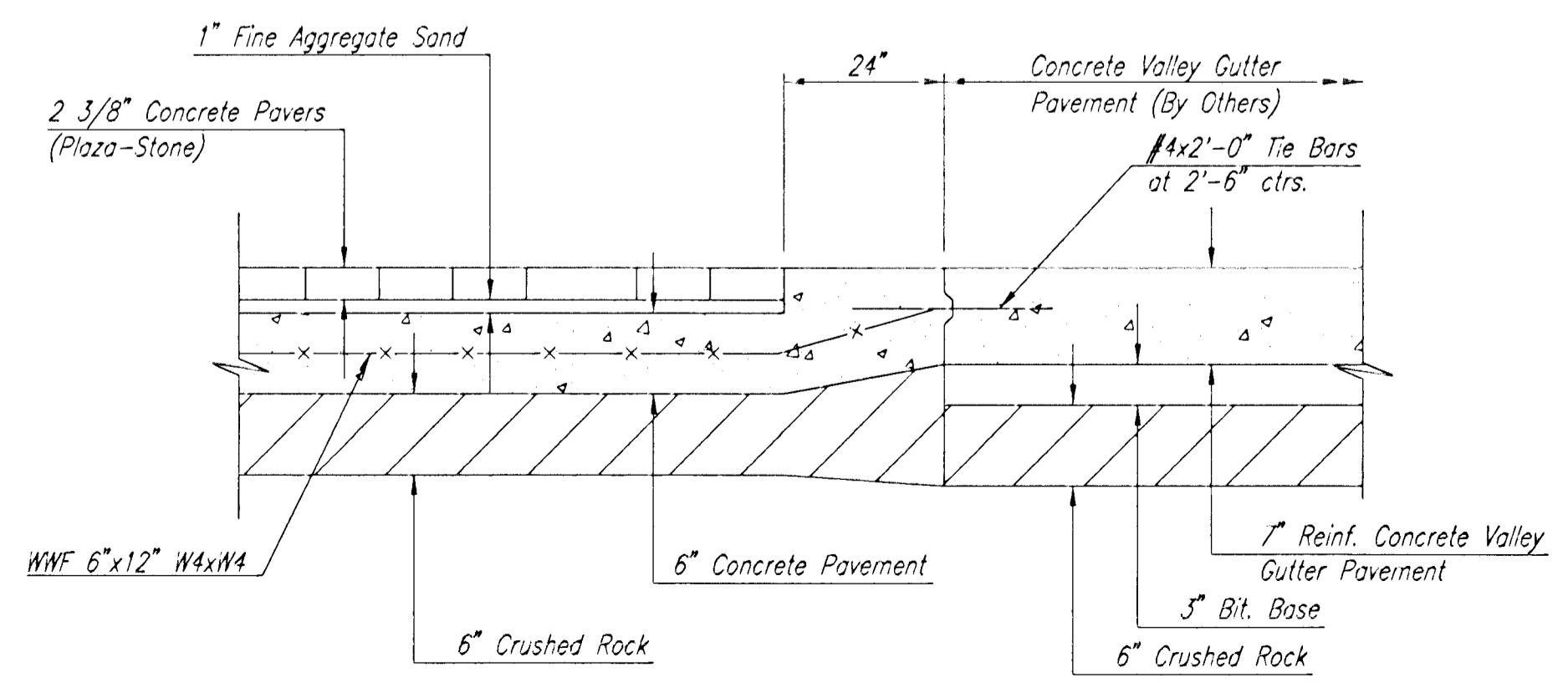
COMBINED CURB & GUTTER (SPECIAL) (6 5/8")



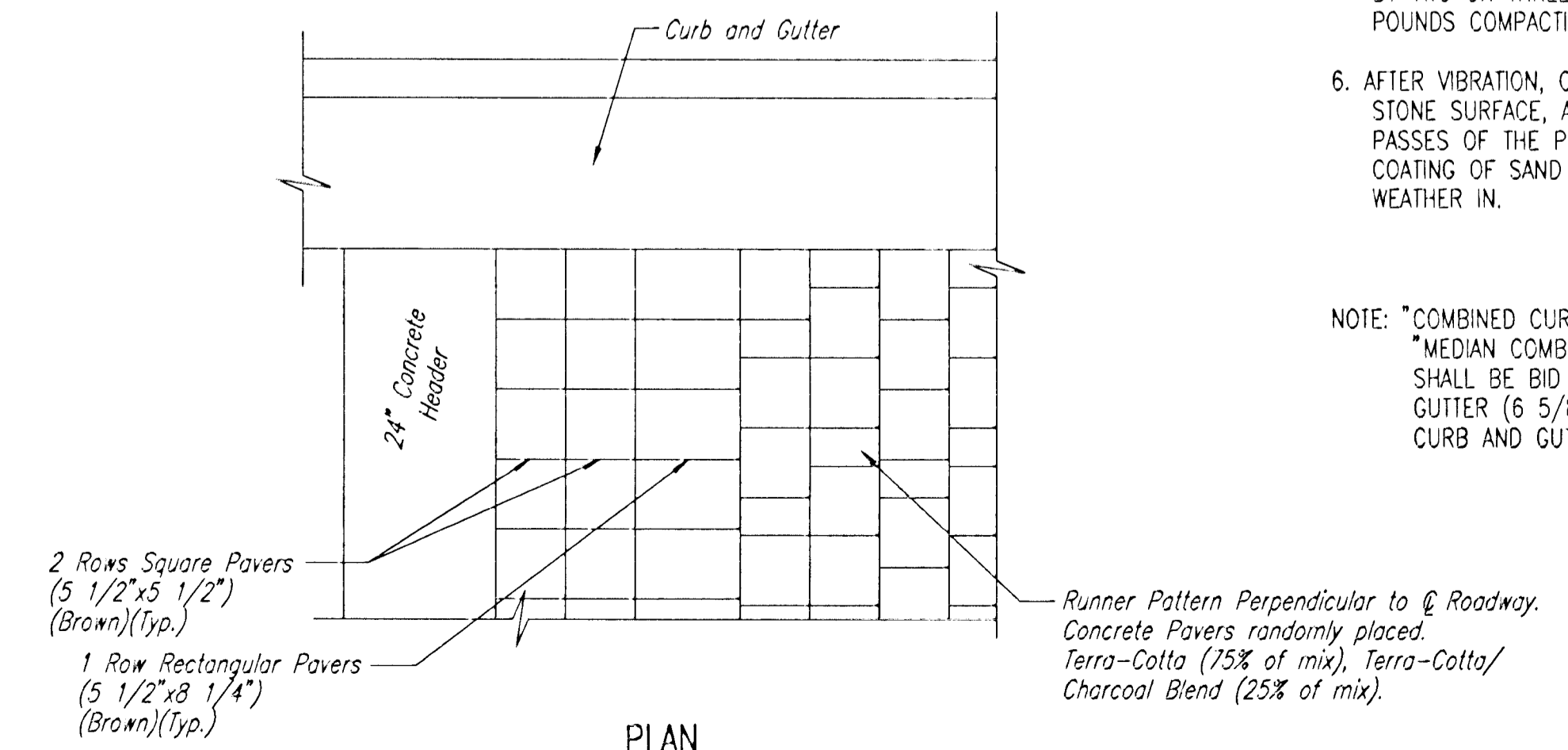
TYPICAL SECTION AT ASPHALT PAVEMENT



BRICK PAVEMENT DRAIN DETAIL (4 Required)



TYPICAL SECTION AT VALLEY GUTTER



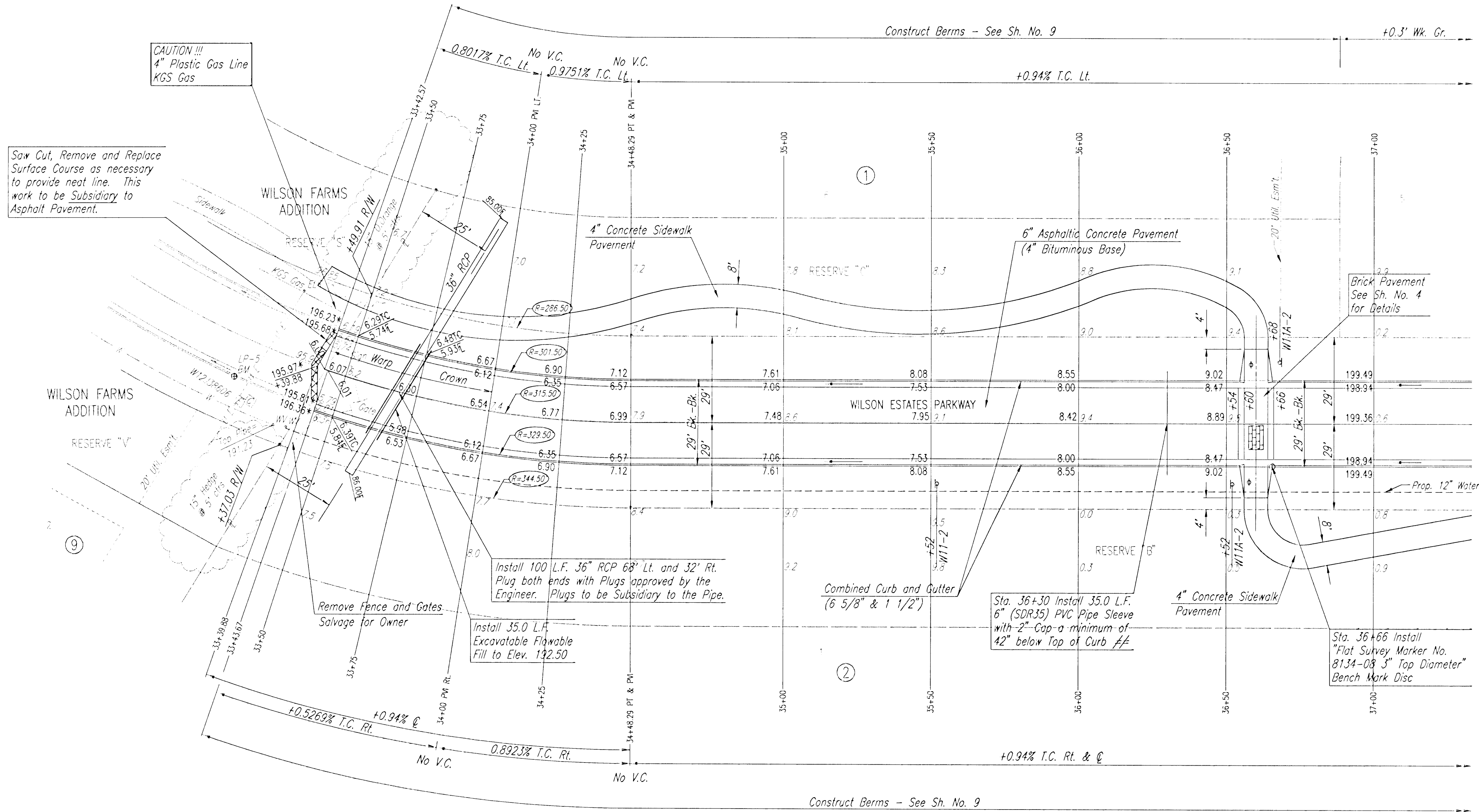
PLAN
Plaza-Stone Pavers

WILSON ESTATES PARKWAY
ENTRANCE DETAILS

Professional Engineering Consultants, P.A.
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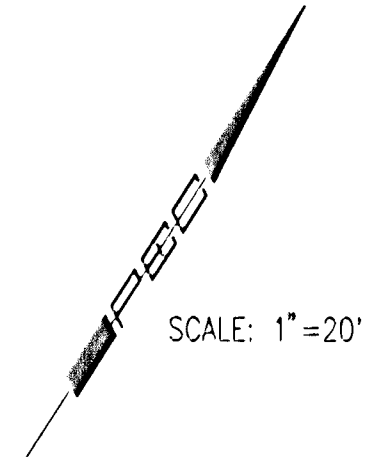
Designed by	BER, ODD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

DESIGN: GED, ODF, SHD, SCHE, 1-24-00
 DRAWING: GED, ODF, SHD, SCHE, 1-24-00



Saw Cut, Remove and Replace Surface Course as necessary to provide neat line. This work to be Subsidiary to Asphalt Pavement.

CAUTION!!!
4" Plastic Gas Line
KGS Gas



SCALE: 1"=20'

Δ=90°00'00" D=18°09'37" R=315.50' L=495.59' T=315.50' E=130.68'

CURVE DATA BASED ON C RADIUS Δ/2=45°00'00"

C STATION	C ARC LENGTH	FACE CURB LENGTH		CHORD LENGTH		DEFLECTION ANGLE	C TOTAL DEFLECTION
		LEFT CURB	RIGHT CURB	B/OFF LEFT FACE CURB	B/OFF RIGHT FACE CURB		
29+52.70						0°00'00"	0°00'00"
33+39.88	387.18'					38°09'23"	38°09'23"
33+42.57	2.69'					0°14'34"	38°24'02"
33+43.67	1.10'	1.05'		1.02'		0°06'00"	38°30'01"
33+50	6.33'	6.05'	6.61'	5.89'	6.77'	0°34'29"	38°04'31"
33+95	25.00'	23.89'	26.11'	23.25'	26.74'	2°16'12"	38°20'43"
34+00	25.00'	23.89'	26.11'	23.25'	26.74'	2°16'12"	40°36'56"
34+25	25.00'	23.89'	26.11'	23.25'	26.74'	2°16'12"	42°53'07"
34+48.29	23.29'	22.26'	24.32'	21.66'	24.91'	2°06'53"	45°00'00"
TOTAL	495.59'	101.03'	109.26'				

Defl./ft. = 5.448052 min.

◆ CONST. STD. WHEELCHAIR RAMP
* MATCH EXISTING

Each End of 6" PVC Pipe Sleeve shall be marked with Steel "T" Posts or 2" Steel Pipe a minimum of 12" above Grade. Pipe, Caps, Pipe Installation and Markers shall be Subsidiary to "Combined Curb and Gutter (8)".

WILSON ESTATES PARKWAY
STA. 33+39.88 TO STA. 37+00.00

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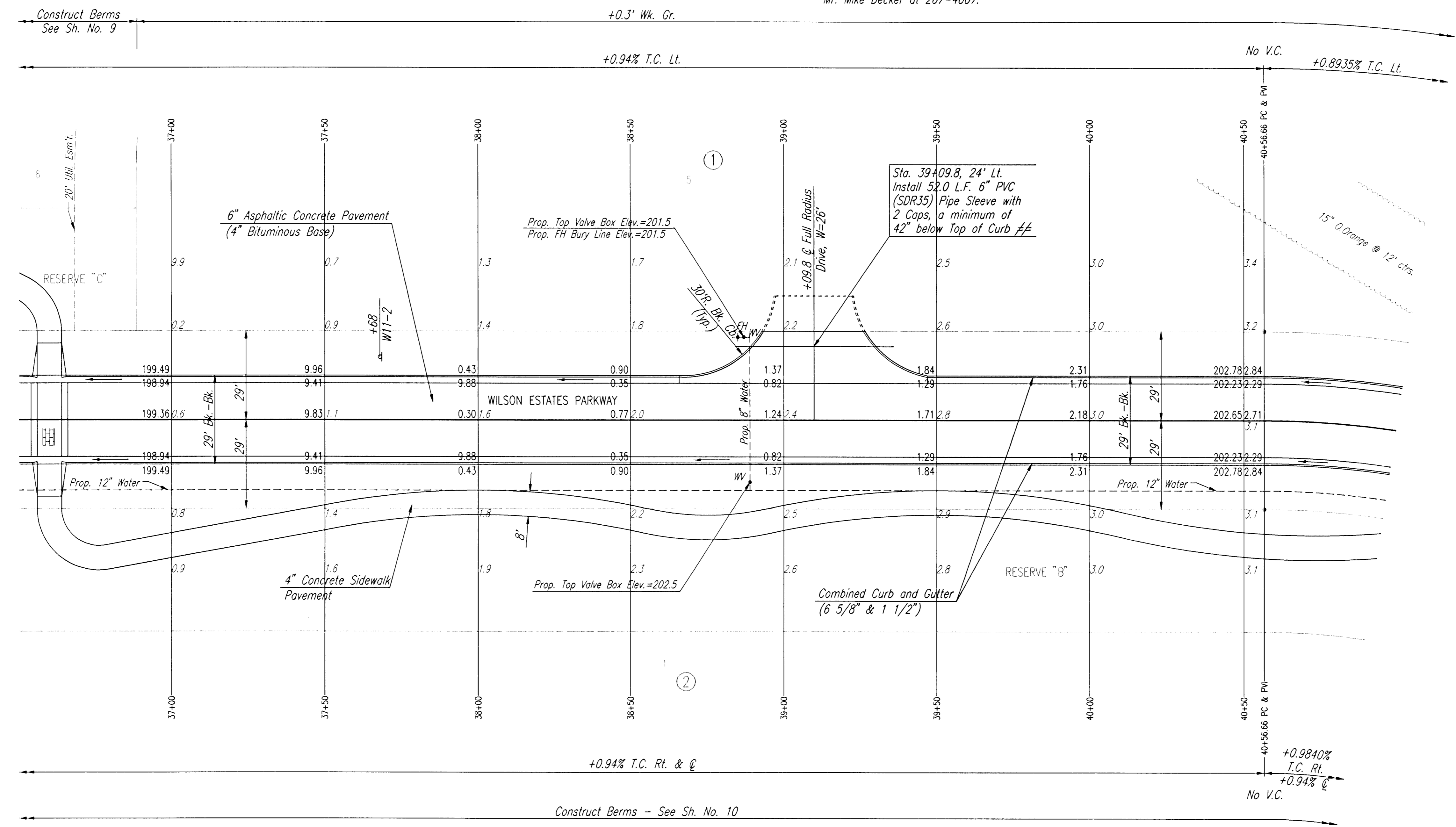
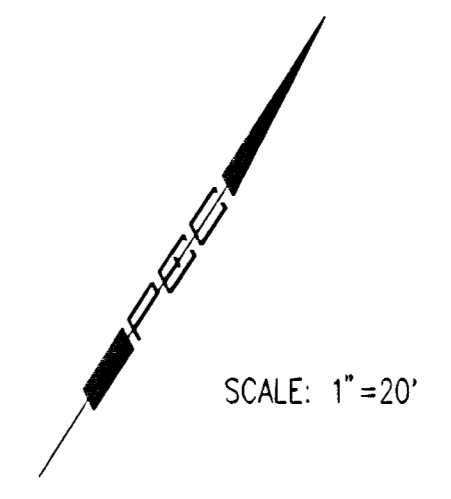
Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

DESIGN: BER, GDD; DATE: 1-20-00
 DRAWN: DEP; DATE: 3-1-00

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-83175	6	24

WATER VALVE BOX ELEVATIONS			
STREET	STATION	OFFSET	PROPOSED ELEVATION
WILSON ESTATES PARKWAY	38+86.68	27.0'LT.	201.5
WILSON ESTATES PARKWAY	38+88.68	20.5'RT.	202.5

Note:
Verify proposed entrance location and size with property owners representative, Mr. Mike Decker at 267-4007.



Each End of 6" PVC Pipe Sleeve shall be marked with Steel "T" Posts or 2" φ Steel Pipe a minimum of 12" above Grade. Pipe Caps, Pipe Installation and Markers shall be Subsidiary to "Combined Curb and Gutter (8)".

WILSON ESTATES PARKWAY

WILSON ESTATES PARKWAY

STA. 37+00.00 TO STA. 40+56.66

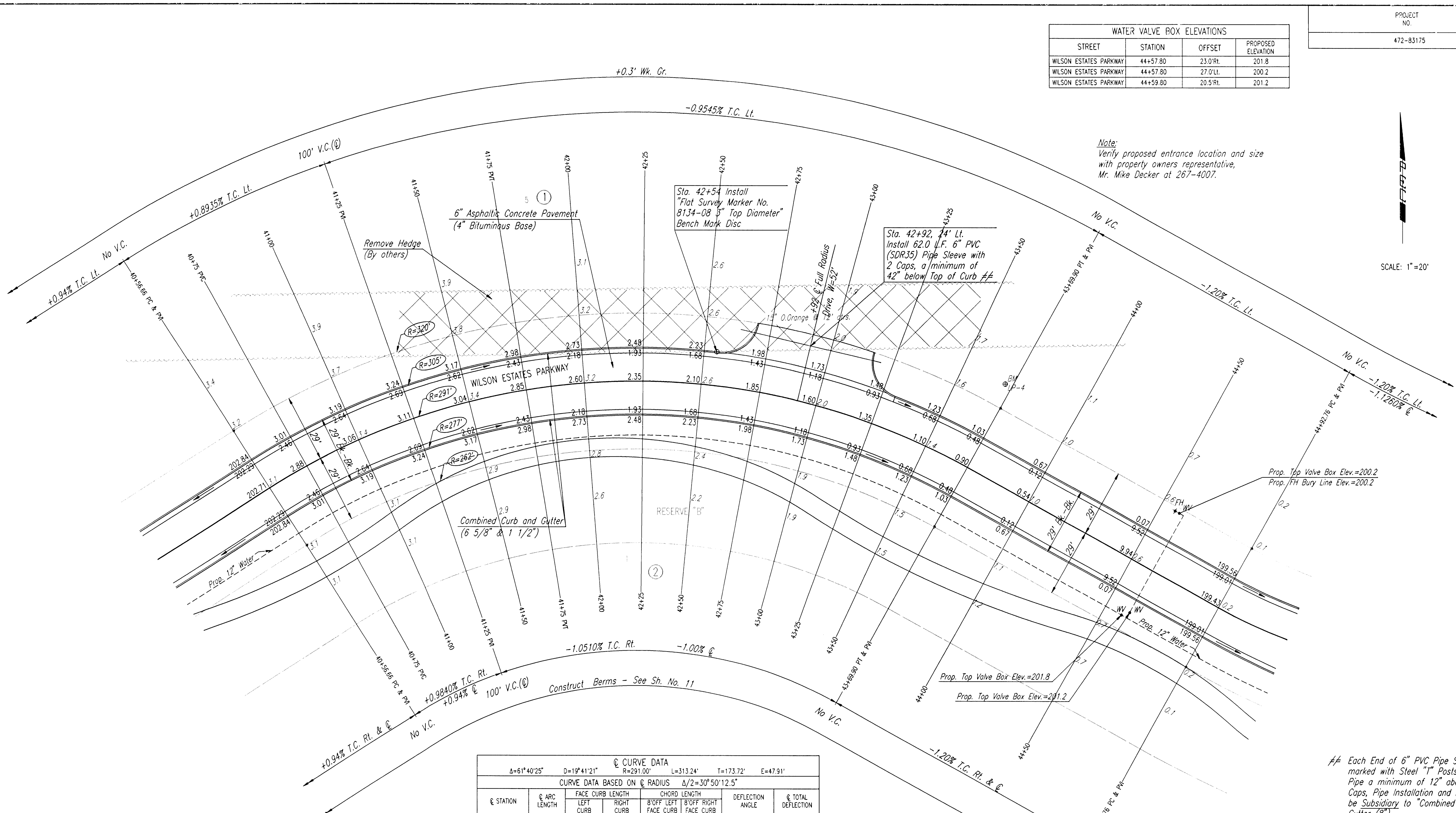
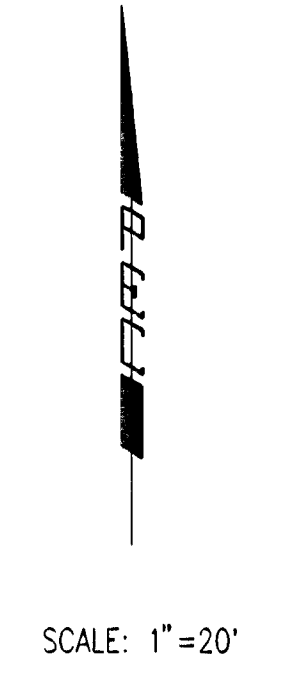
Professional Engineering Consultants, P.A.
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316-262-2691 • FAX 316-262-3003

Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

DSNP 000 OPER DEP SCALE: 1"=20'00
 SA: 03/03/00 08:45:00 05/31/2000 11:45:03 am

STREET	STATION	OFFSET	PROPOSED ELEVATION
WILSON ESTATES PARKWAY	44+57.80	23.0' RL	201.8
WILSON ESTATES PARKWAY	44+57.80	27.0' LL	200.2
WILSON ESTATES PARKWAY	44+59.80	20.5' RL	201.2

Note:
Verify proposed entrance location and size with property owners representative, Mr. Mike Decker at 267-4007.



$\Delta=61^{\circ}40'25''$	$D=19^{\circ}41'21''$	$R=291.00'$	$L=313.24'$	$T=173.72'$	$E=47.91'$			
CURVE DATA BASED ON ② RADIUS $\Delta/2=30^{\circ}50'12.5''$								
② STATION	② ARC LENGTH	FACE CURB LENGTH LEFT CURB	FACE CURB LENGTH RIGHT CURB	CHORD LENGTH	8" OFF LEFT FACE CURB	8" OFF RIGHT FACE CURB	DEFLECTION ANGLE	② TOTAL DEFLECTION
40+56.66							0°00'00"	0°00'00"
40+75	18.34'	19.23'	17.44'	19.72'	16.95'		1°48'20"	1°48'20"
41+00	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	4°16'00"
41+25	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	6°43'40"
41+50	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	9°11'20"
41+75	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	11°39'00"
42+00	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	14°06'40"
42+25	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	16°34'20"
42+50	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	19°02'00"
42+75	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	21°29'40"
43+00	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	23°57'20"
43+25	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	26°25'00"
43+50	25.00'	26.20'	23.80'	26.88'	23.10'		2°27'40"	28°52'40"
43+69.90	19.90'	20.87'	18.92'	21.40'	18.39'		1°57'33"	30°50'12.5"
TOTAL	313.24'	328.30'	298.16'					

Defl./ft. = 5.906680 min.

Each End of 6" PVC Pipe Sleeve shall be marked with Steel "T" Posts or 2" Steel Pipe a minimum of 12" above Grade. Pipe, Caps, Pipe Installation and Markers shall be Subsidiary to "Combined Curb and Gutter (B)".

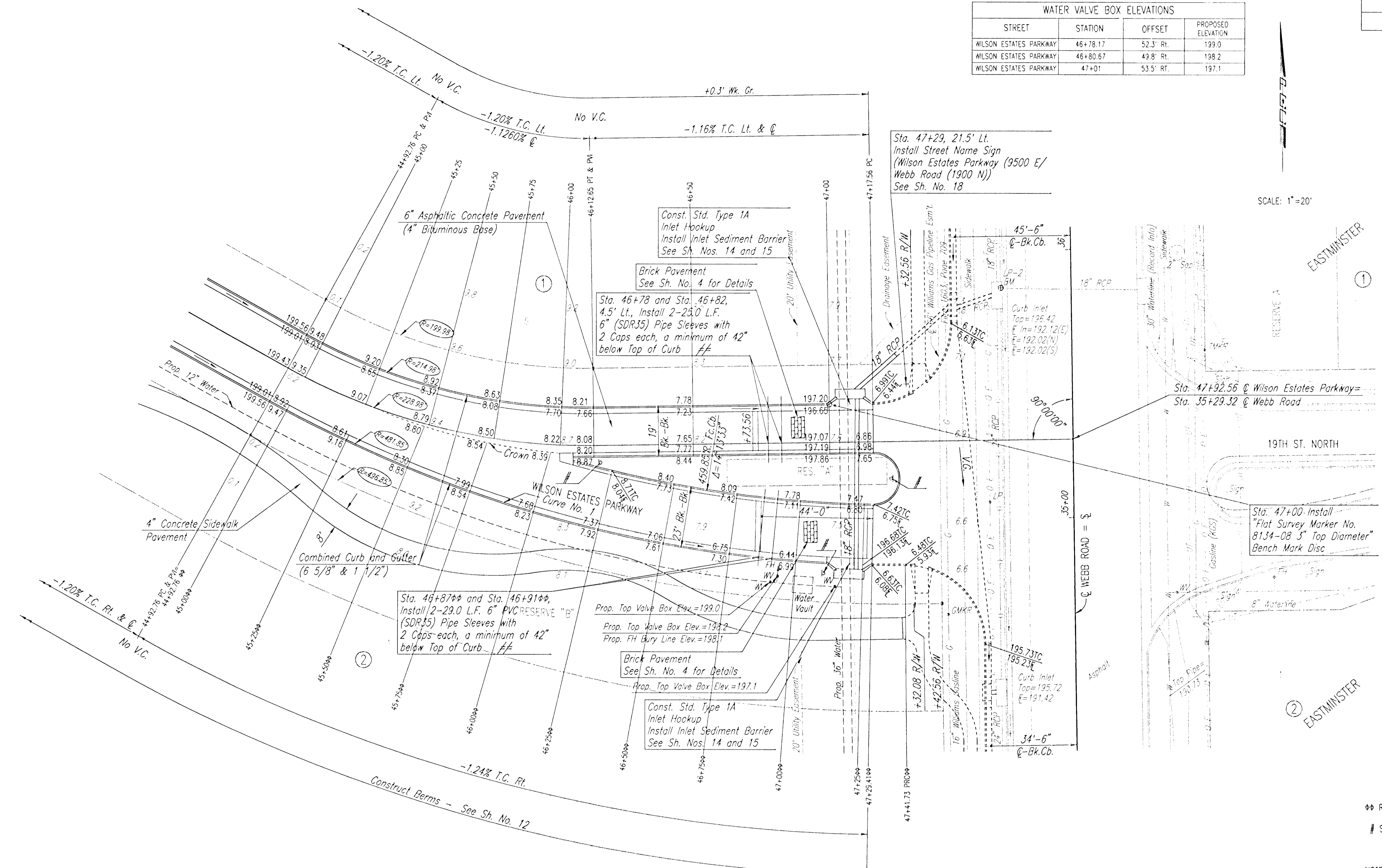
WILSON ESTATES PARKWAY
STA. 40+56.66 TO STA. 44+92.76

Professional Engineering Consultants, P.A.
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Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

D:\SHR\GEO\OPER\DEP\SCALE: 1"=20'00'
 2-3-2000 09:00:24 WILSON ESTATES PARKWAY - 09-31-2000 11:45:06 am

STREET	STATION	OFFSET	PROPOSED ELEVATION
WILSON ESTATES PARKWAY	46+78.17	52.3' RT.	199.0
WILSON ESTATES PARKWAY	46+80.67	49.8' RT.	198.2
WILSON ESTATES PARKWAY	47+01	53.5' RT.	197.1



SCALE: 1"=20'

◆◆ RIGHT FACE OF CURB STATION
 # SEE SH. NO. 18 FOR SIGNING NOTES AND DETAILS

NOTE: CONTRACTOR SHALL NOT BEGIN CONSTRUCTION OF THESE IMPROVEMENTS IN AREA ADJACENT TO 36" WATER LINE UNTIL THE PROPOSED 36" WATER LINE IS IN PLACE. C.O.W. PROJ. NO. 448-89465.

$\Delta=29^{\circ}59'56"$	$D=25^{\circ}01'20"$	$R=228.93'$	$L=119.89'$	$T=61.35'$	$E=8.08'$	
CURVE DATA BASED ON Q RADIUS $\Delta/2=14^{\circ}59'58"$						
Q STATION	Q ARC LENGTH	FACE CURB LENGTH		CHORD LENGTH		Q TOTAL DEFLECTION
		LEFT CURB	RIGHT CURB	8' OFF LEFT FACE CURB	8' OFF RIGHT FACE CURB	
44+92.76	---	---	---	---	---	0°00'00"
45+00	7.24'	6.80'	---	6.54'	---	0°54'21"
45+25	25.00'	23.47'	---	22.59'	---	5°07'40"
45+50	25.00'	23.47'	---	22.59'	---	9°09'41"
45+75	25.00'	23.47'	---	22.59'	---	12°17'21"
46+00	25.00'	23.47'	---	22.59'	---	15°25'00"
46+12.65	12.65'	11.88'	---	11.43'	---	1°34'58"
TOTAL	119.89'	112.56'	---	Defl./ft. = 7.506603 min.		14°59'58"

$\Delta=29^{\circ}59'56"$	$D=6^{\circ}02'16"$	$R=489.85'$	$L=256.48'$	$T=131.25'$	$E=17.28'$	
CURVE DATA BASED ON RADIUS TO 8' BEHIND FACE OF CURB $\Delta/2=14^{\circ}59'58"$						
FACE CURB STATION	ARC LENGTH ALONG FACE CURB	ARC LENGTH ALONG 8' OFFSET	CHD LENGTH 8' OFF FC. OF CB.	DEFLECTION ANGLE	TOTAL DEFLECTION	REMARKS
44+92.76	---	---	---	0°00'00"	0°00'00"	P.C.
45+00	7.24'	7.35'	7.36'	0°25'50"	0°25'50"	
45+25	25.00'	25.42'	25.41'	1°29'11"	1°55'00"	
45+50	25.00'	25.42'	25.41'	1°29'11"	3°24'11"	
45+75	25.00'	25.42'	25.41'	1°29'11"	4°53'22"	
46+00	25.00'	25.42'	25.41'	1°29'11"	6°22'33"	
46+12.65	12.65'	12.42'	12.43'	1°29'11"	7°51'44"	
46+25	25.00'	25.42'	25.41'	1°29'11"	9°20'54"	
46+50	25.00'	25.42'	25.41'	1°29'11"	10°50'05"	
46+75	25.00'	25.42'	25.41'	1°29'11"	12°19'16"	
47+00	25.00'	25.42'	25.41'	1°29'11"	13°48'27"	
47+25.00	4.50'	4.56'	4.57'	0°16'03"	14°04'30"	Begin V.G.
47+41.73	---	12.42'	12.43'	0°43'38"	14°48'07"	P.R.C.
47+45.05	---	---	3.38'	0°11'51"	14°59'58"	P.T. R/W
TOTAL	236.65'	253.11'	Defl./ft. = 3.567191 min.			

Curve Data
 Curve No. 1
 (Face of Curb)
 $\Delta=29^{\circ}59'56"$
 $D=11^{\circ}53'22"$
 $R=481.85'$
 $L=252.29'$
 $T=129.11'$
 $E=17.00'$

Each End of 6" PVC Pipe Sleeves shall be marked with Steel "I" Posts or 2" Steel Pipe a minimum of 12" above Grade. Pipe, Caps, Pipe Installation and Markers shall be Subsidiary to "Median Combined Curb and Gutter (8").

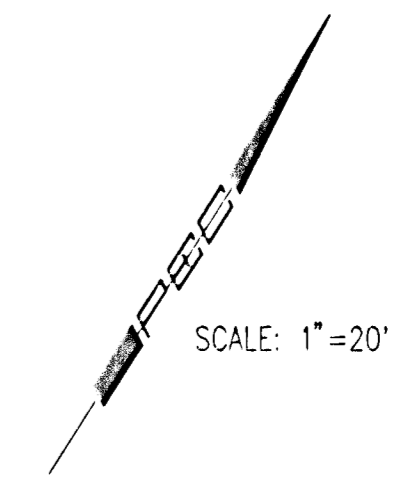
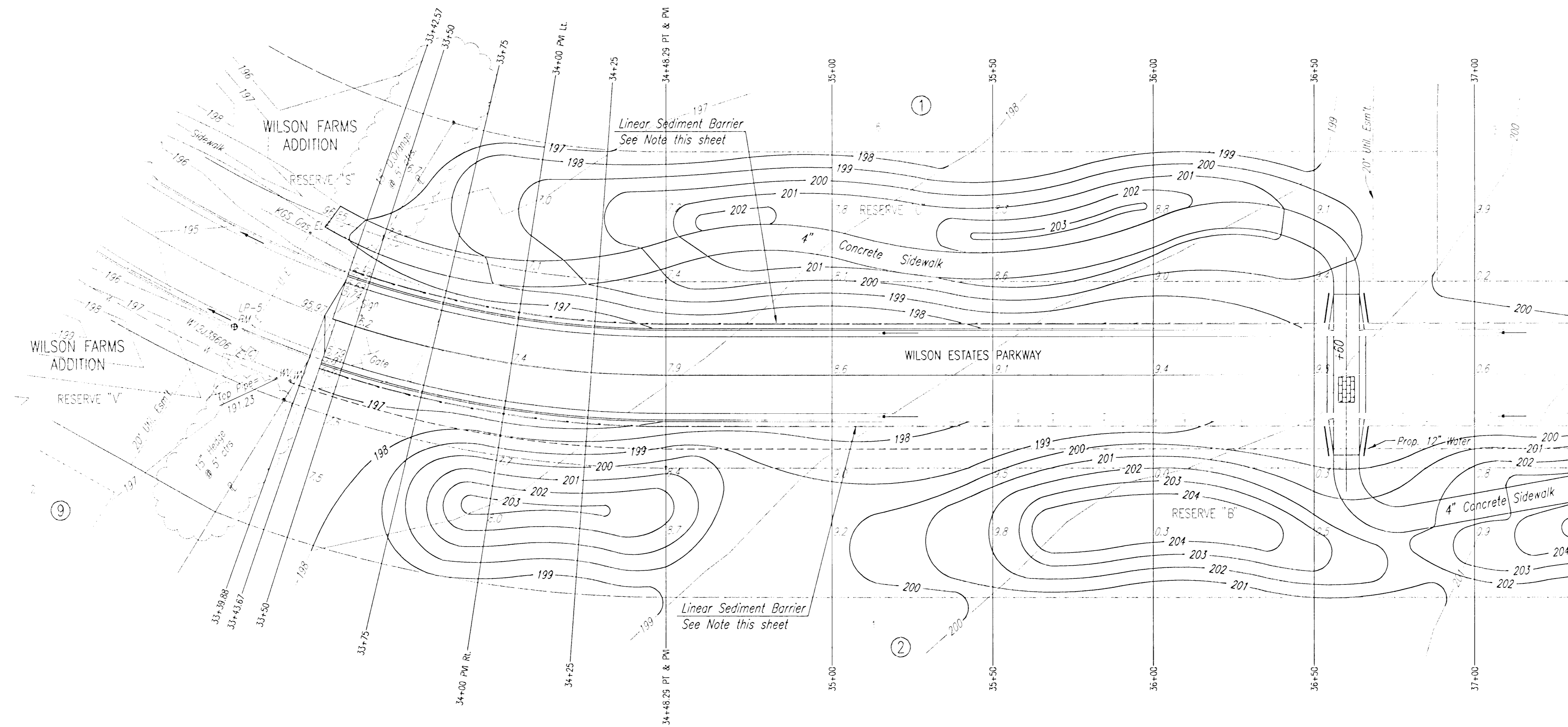
WILSON ESTATES PARKWAY
 STA. 44+92.76 TO STA. 47+92.76

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Designed by	BER, GDD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

D. BARKER, P.E., CHIEF ENGINEER
 2000 WILSON ESTATES PARKWAY, WICHITA, KS 67202

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-83175	9	24

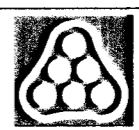


NOTE: CONSTRUCT TEMPORARY SILT FENCE SEDIMENT BARRIER WHERE SHOWN ON PLANS. BARRIER SHALL BE A MINIMUM OF 12" HIGH OF A TYPE AND CONSTRUCTION APPROVED BY THE ENGINEER. BARRIER SHALL BE IN PLACE IMMEDIATELY AFTER BERMS ARE IN PLACE AND KEPT IN SERVICEABLE CONDITION AT ALL TIMES. REMOVAL SHALL BE THE RESPONSIBILITY OF THE OWNER/DEVELOPER. THIS WORK SHALL BE BID AS LINEAL FEET OF "LINEAR SEDIMENT BARRIER" WHICH SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR AND INCIDENTALS TO CONSTRUCT BARRIER AND FOR MAINTENANCE DURING LIFE OF THE PROJECT.

WILSON ESTATES PARKWAY

WILSON ESTATES PARKWAY

STA. 33+39.88 TO STA. 37+00.00

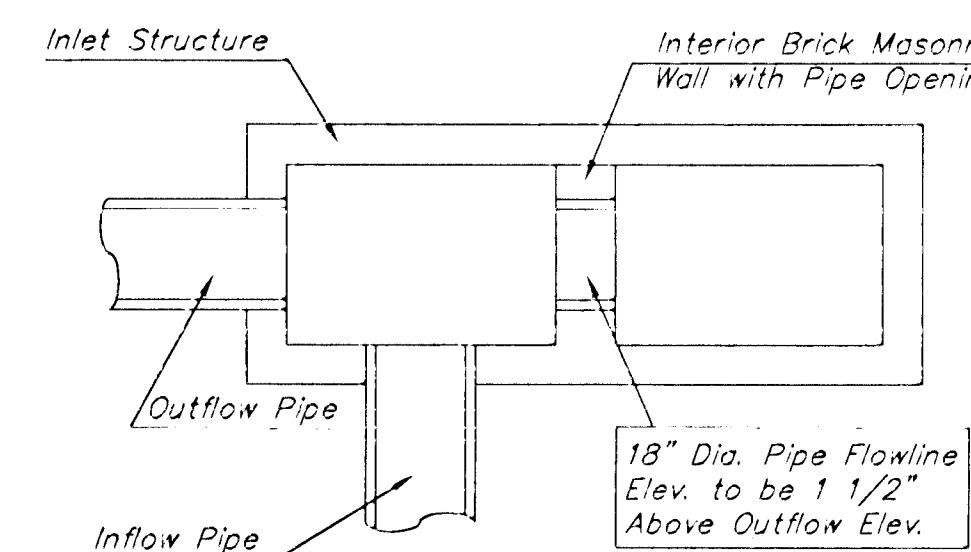
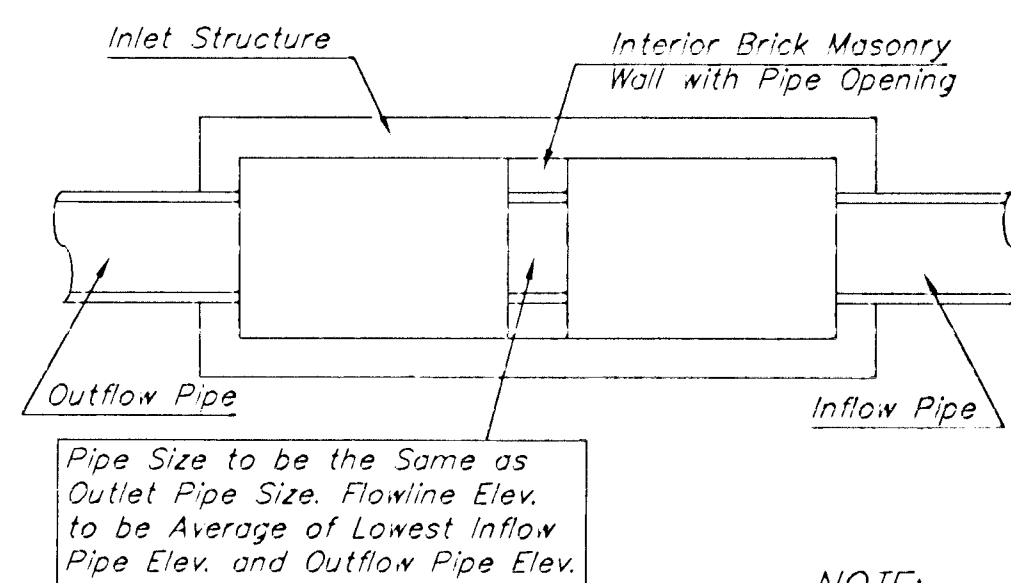
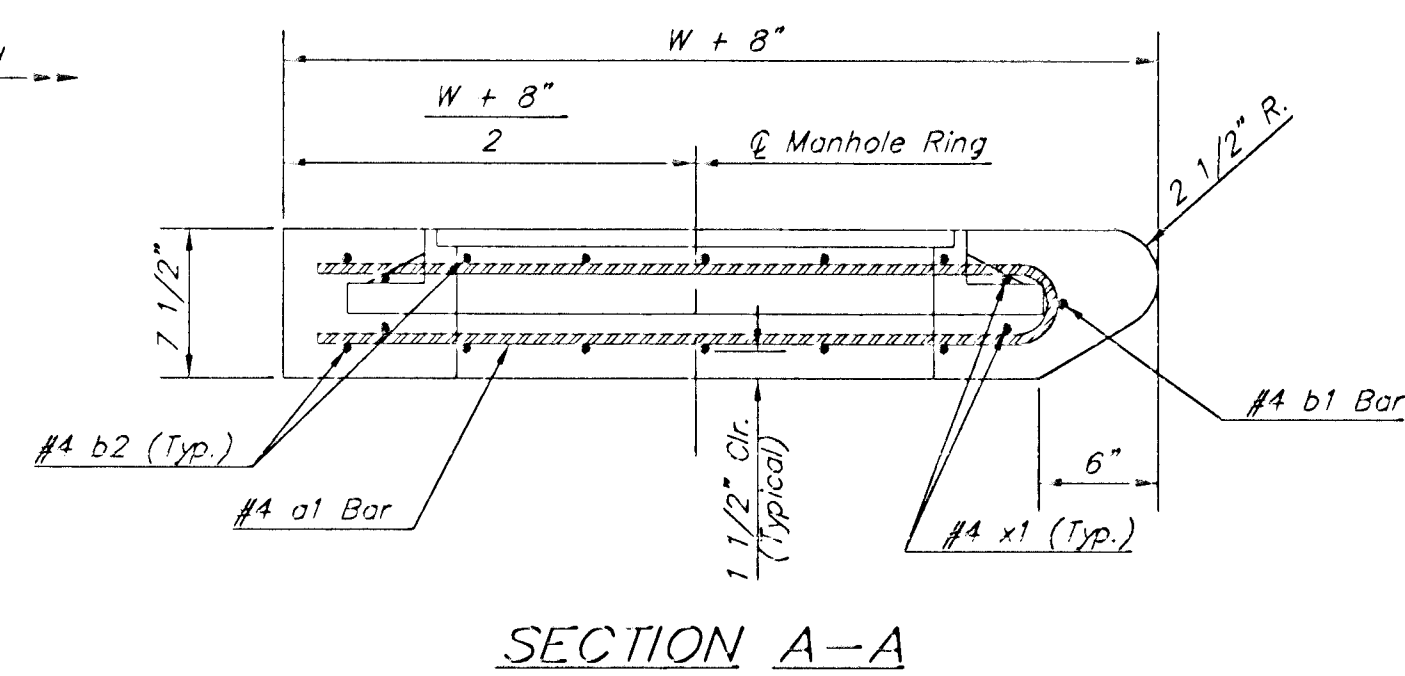
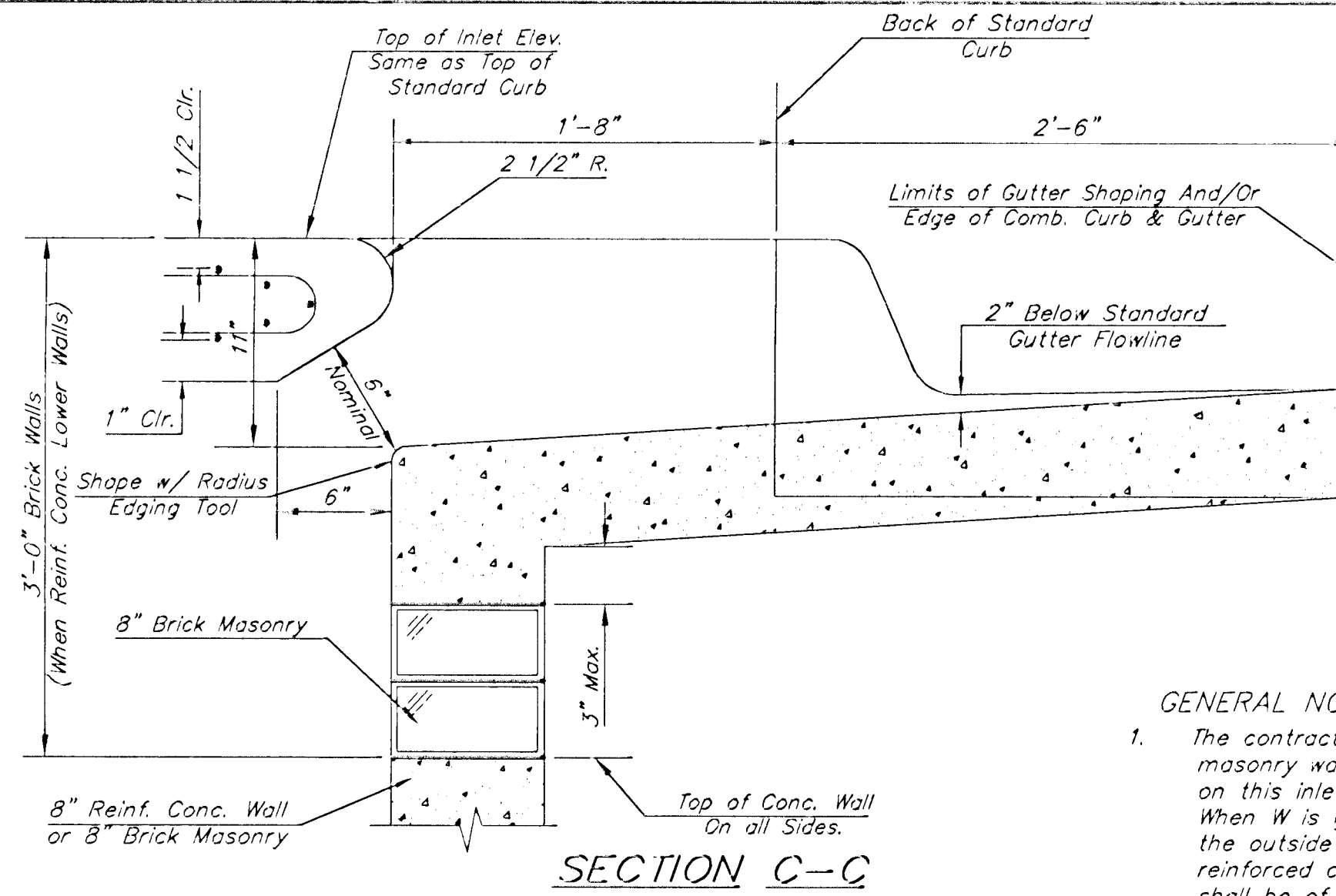
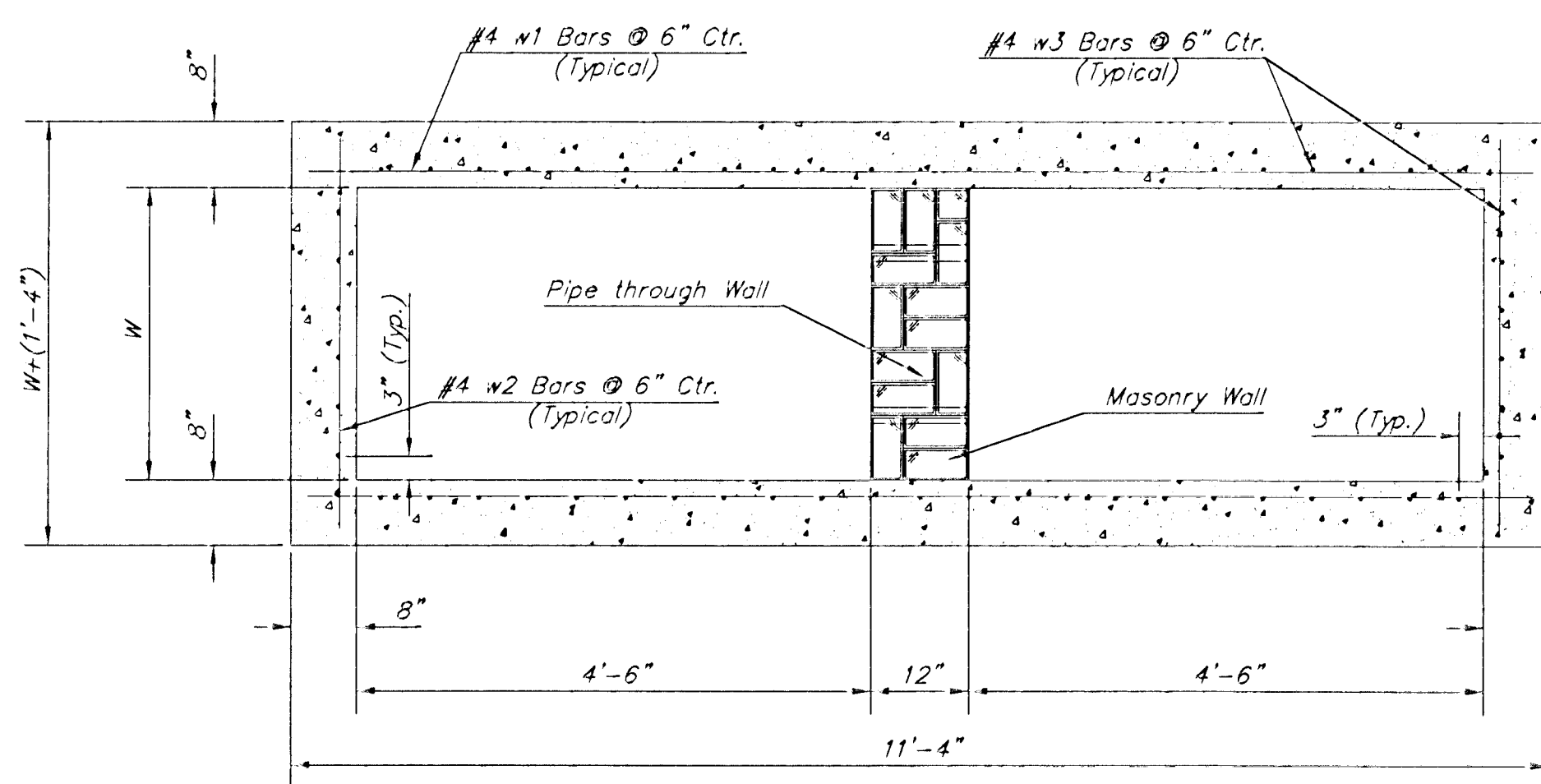
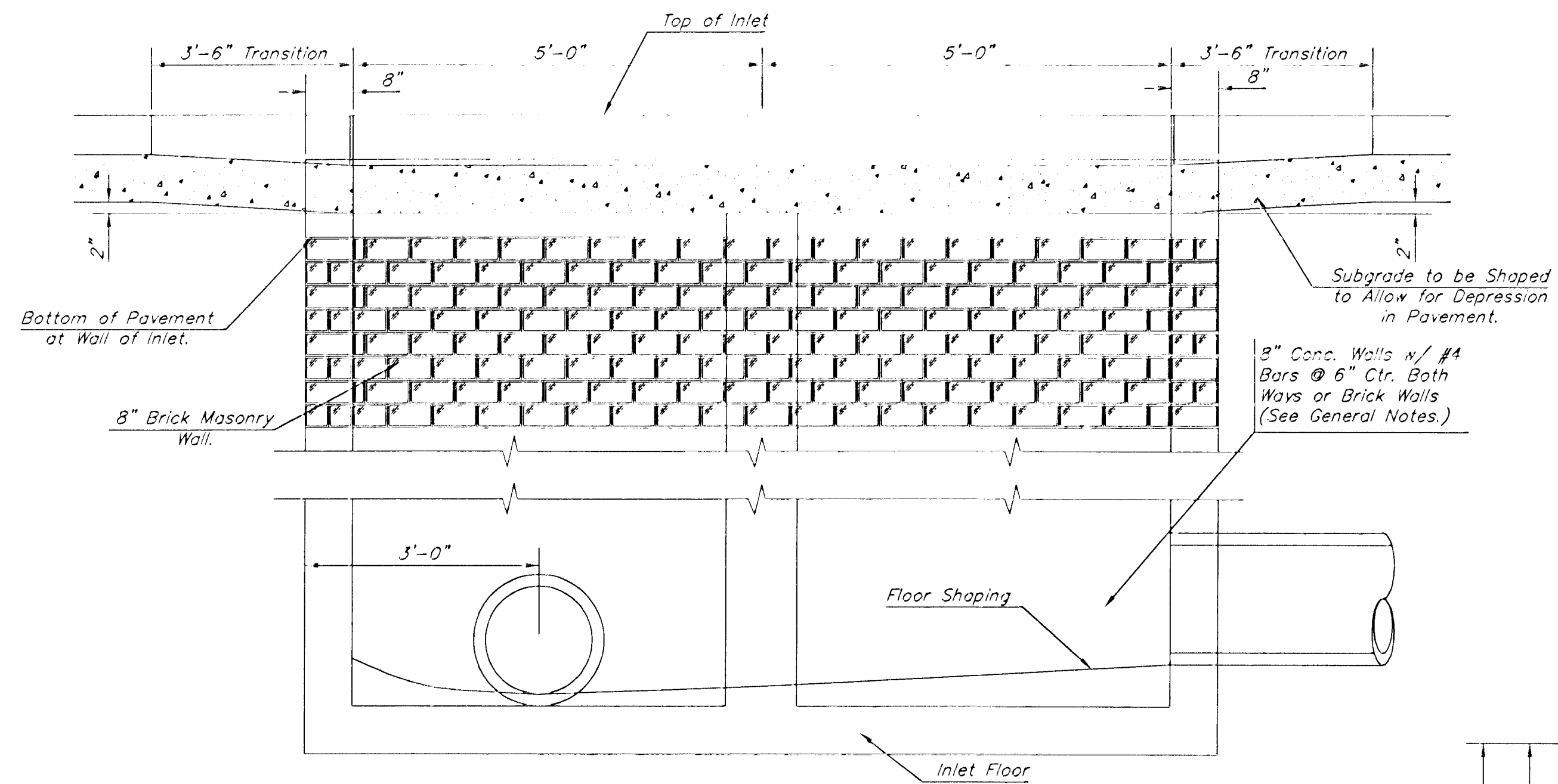
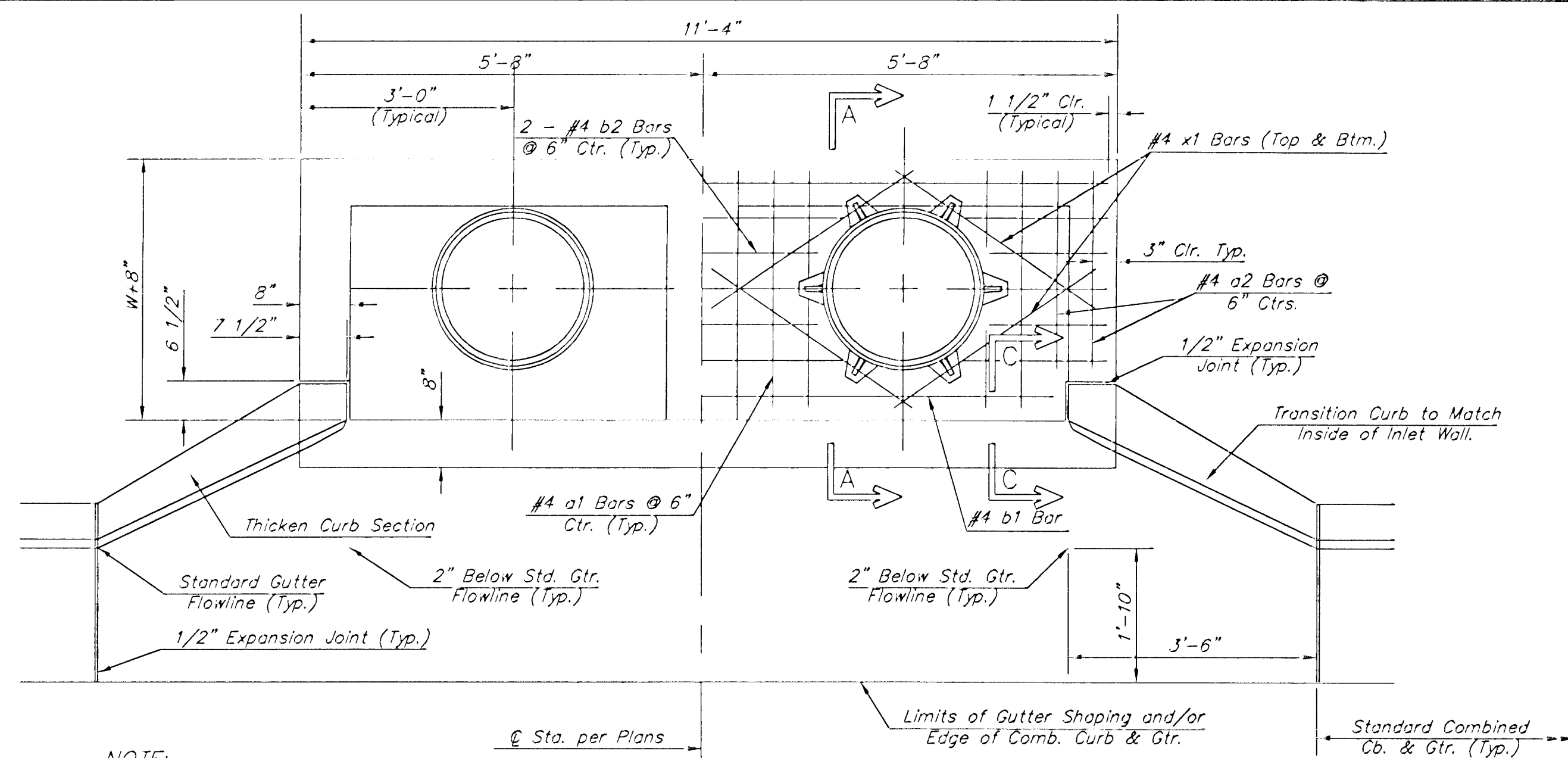


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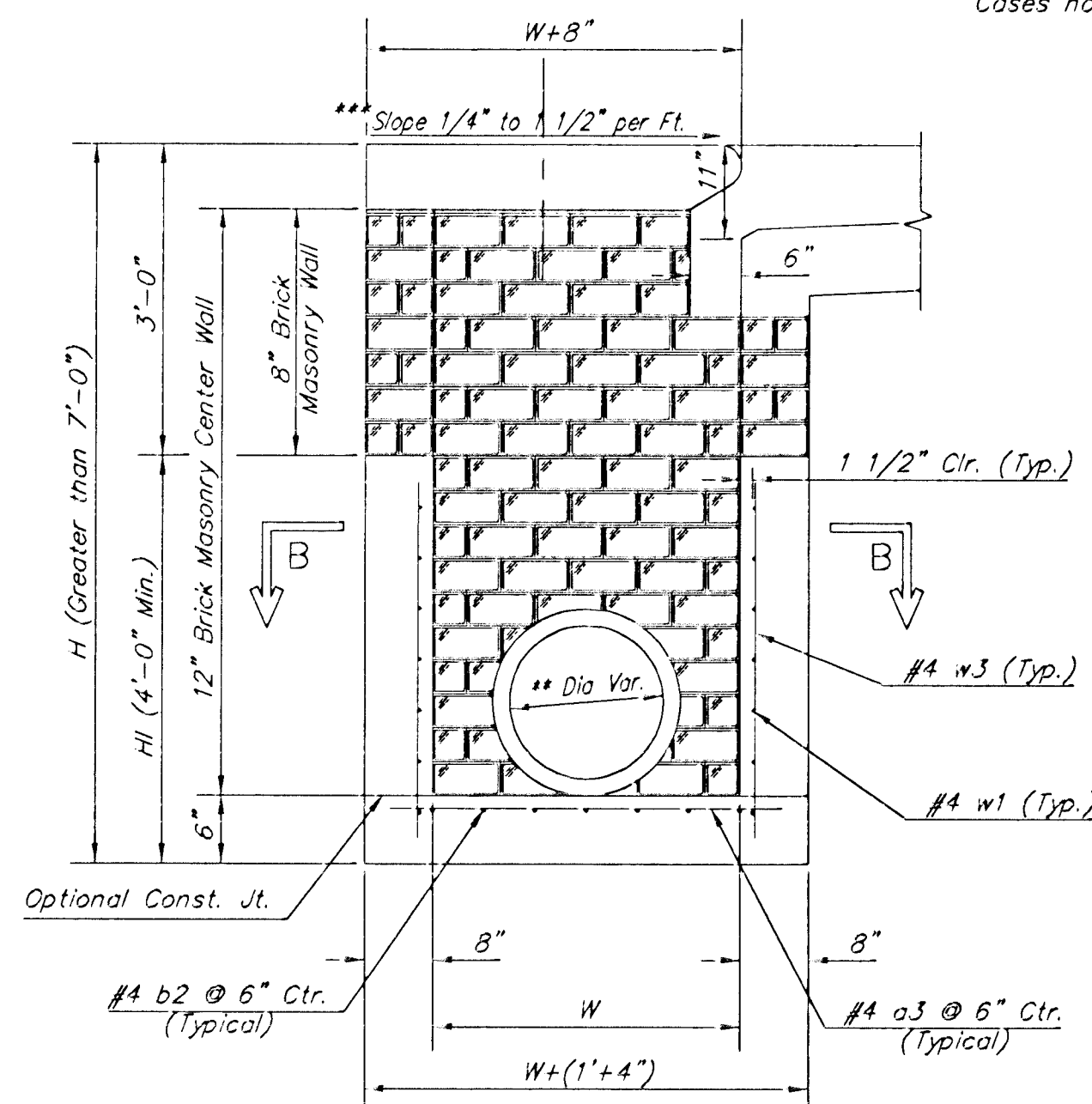
Designed by BER, CDD
Checked by
Drawn by DEP
Date MARCH, 2000
Job No. 00034

DRAWN BY: BER, CDD
 CHECKED BY: DEP
 DATE: MARCH, 2000
 JOB NO.: 00034

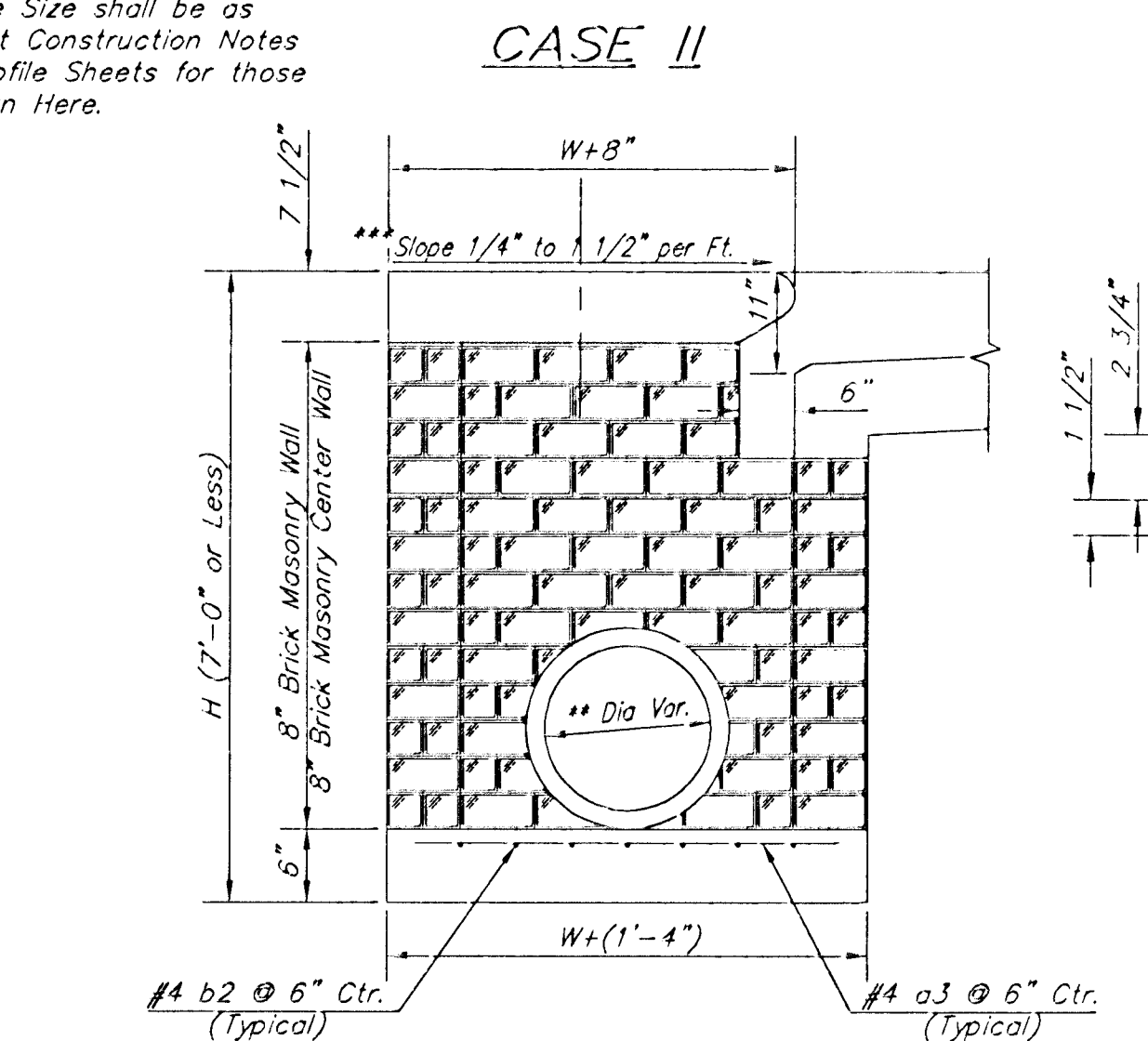
DSNR: BCR OPER. SAD SCALE: 1=1.00
 Q:\2000\00034\TYP1A-ZC 05-26-2000 05:00:37 am



NOTE:
 Center Wall Pipe Size shall be as Specified in Inlet Construction Notes on the Plan/Profile Sheets for those Cases not Shown Here.



TYPICAL INLET SECTION AT CENTER WALL
 (Reinforced Concrete Walls)

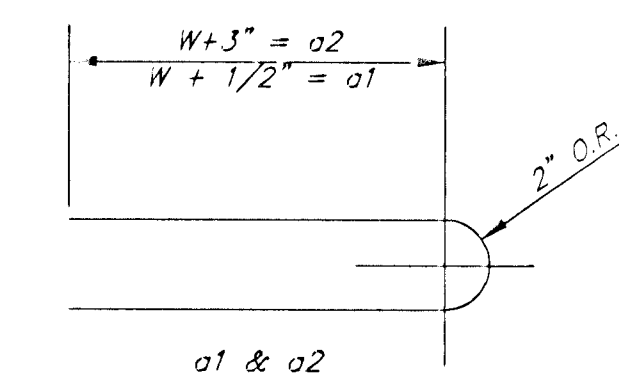


NOTES:
 ** A center wall opening shall be provided by means of a section of reinforced concrete pipe. See Case I and Case II above.
 *** Slope of inlet tops to match sidewalk of parking slopes within limits indicated

PRECAST SLAB AND FLOOR REINFORCING											
MARK	SIZE	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
		NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH
a1	#4	13	6'-7"	13	8'-7"	13	10'-7"	13	12'-7"	13	14'-7"
a2	#4	4	6'-0"	4	8'-0"	4	10'-0"	4	12'-0"	4	14'-0"
a3	#4	23	4'-1"	23	5'-1"	23	6'-1"	23	7'-1"	23	8'-1"
b1	#4	1	9'-9"	1	9'-9"	1	9'-9"	1	9'-9"	1	9'-9"
* b2	#4	23	11'-1"	29	11'-1"	35	11'-1"	41	11'-1"	47	11'-1"
x1	#4	16	3'-10"	16	4'-2"	16	4'-6"	16	4'-10"	16	5'-2"

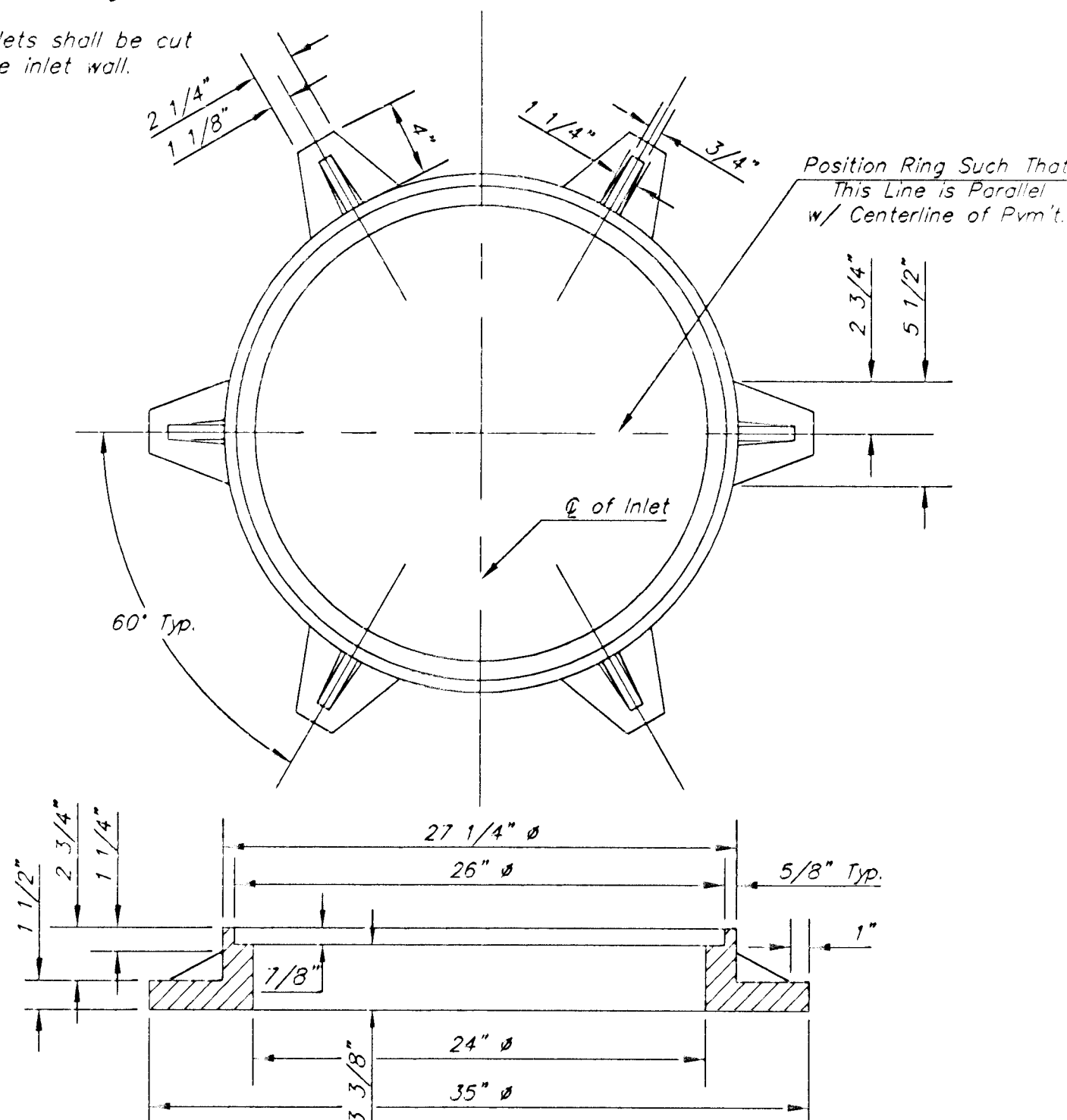
WALL REINFORCING											
MARK	SIZE	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
		NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH
w1	#4	1	11'-1"	1	11'-1"	1	11'-1"	1	11'-1"	1	11'-1"
w2	#4	1	4'-1"	1	5'-1"	1	6'-1"	1	7'-1"	1	8'-1"
w3	#4	52	2	56	2	60	2	64	2	68	2

* Field Bend or Cut Reinforcing as Required for Clearance.
 ① 4 (H1 - 12") (H1 - 21") Rounded down to nearest 0.5"
 ② H1 - 3"



BENDING DIAGRAM

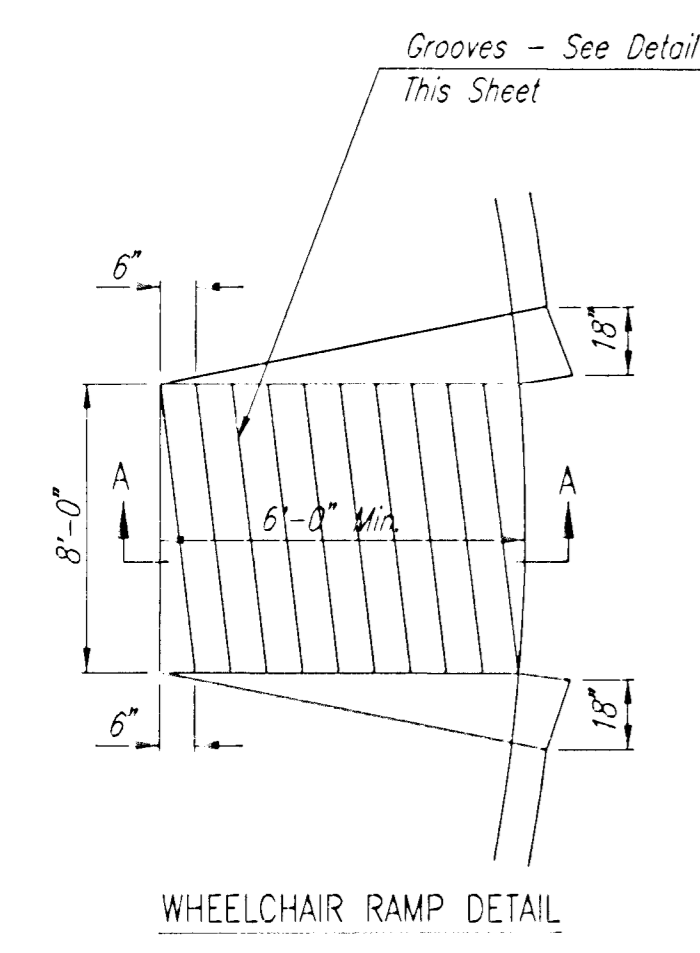
STANDARD CURB INLET PRECAST TOPS			
W	PRE-CAST TOP SIZE	PIPE SIZE	CU. YD. CONC.
3'-0"	3'-8" 11'-4" 7 1/2"	21" & SMALLER	0.83±
4'-0"	4'-8" 11'-4" 7 1/2"	24" & 30"	1.09±
5'-0"	5'-8" 11'-4" 7 1/2"	36" & 42"	1.35±
6'-0"	6'-8" 11'-4" 7 1/2"	48" & 54"	1.61±
7'-0"	7'-8" 11'-4" 7 1/2"	60" & 66"	1.87±



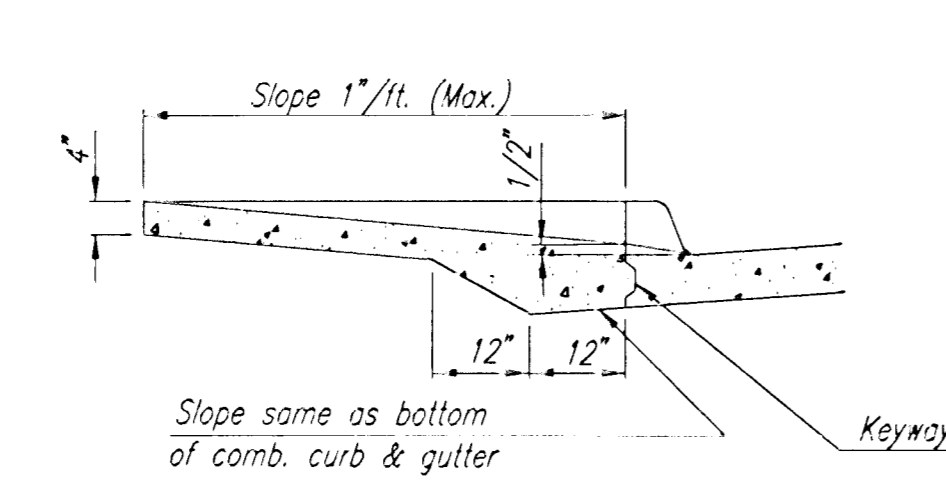
MANHOLE RING AND COVER
 Weight = 180 lbs.

*See City of Wichita Standard Manhole Ring and Cover Detail Sheet for Cover Details to Be Used With Inlet Frame.

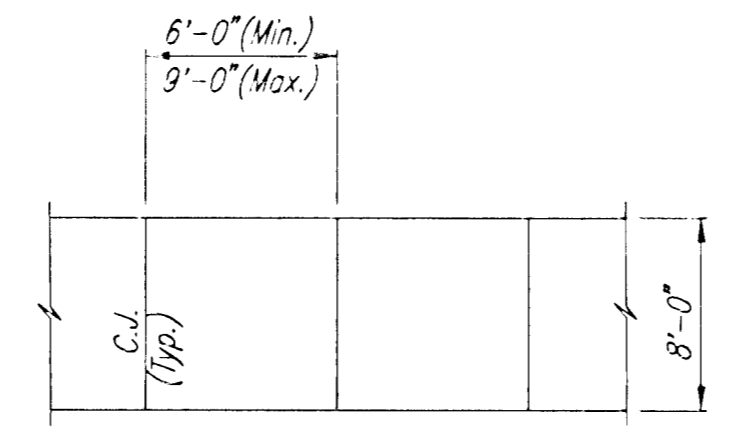
	STANDARD TYPE 1-A CURB INLET OPENING = 6" x 10'-0"	
	M. E. LINDEBAK P.E. - CITY ENGINEER	
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202 (316) 268-4501 (316) 268-4114 FAX	PROJECT NUMBER 472-83175	OCA NO. 765634
	DATE MAR 96	SHEET 14 OF 24



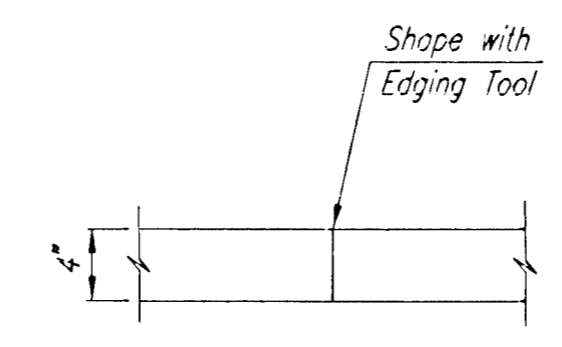
WHEELCHAIR RAMP DETAIL



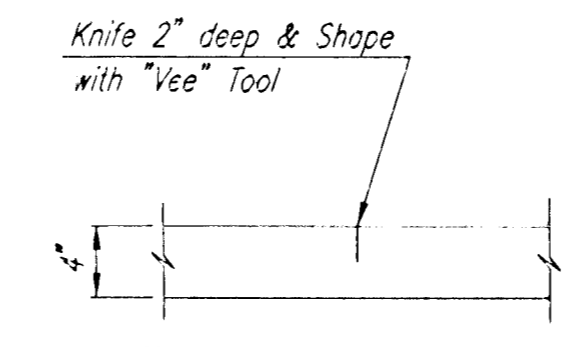
SECTION A-A



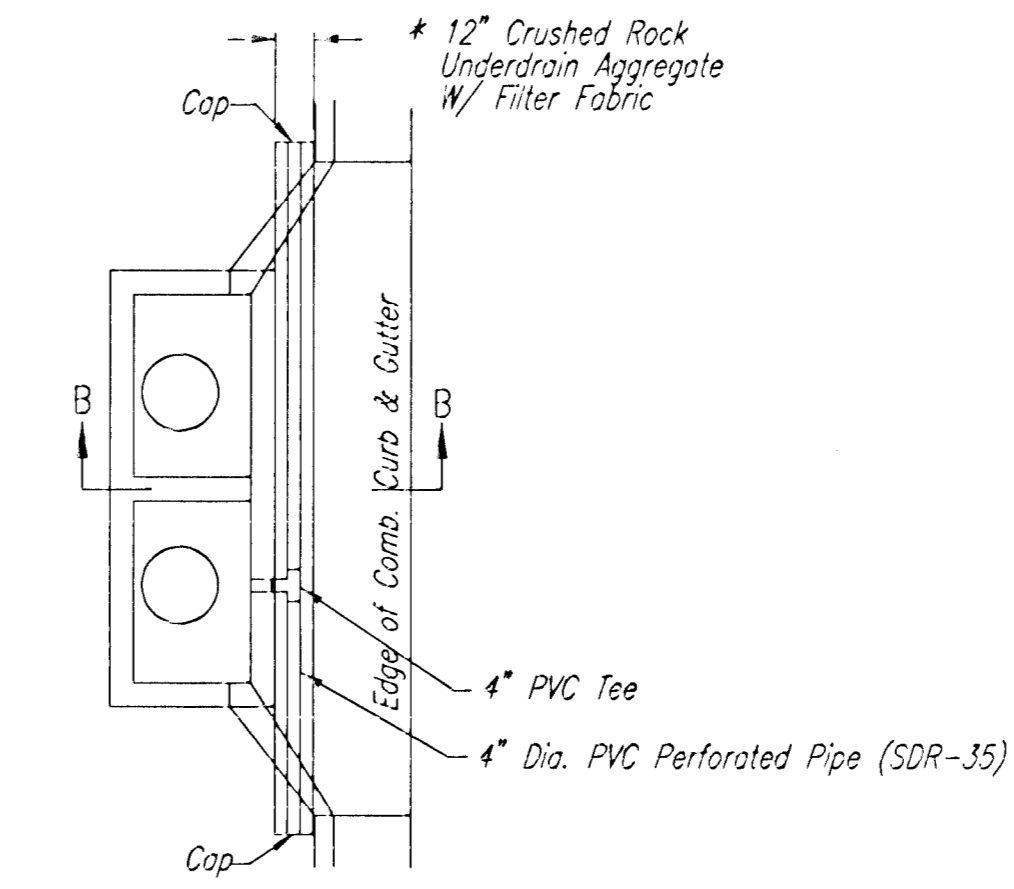
PLAN



CONSTRUCTION JOINT



CONTRACTION JOINT



PLAN

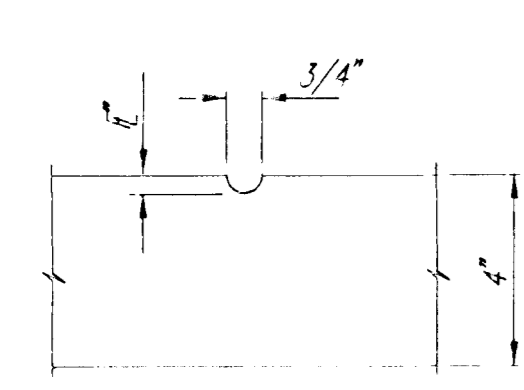
* UNDERDRAIN AGGREGATE PERCENT OF AGGREGATE RETAINED

1"	0
3/4"	0-10
3/8"	45-80
#4	90-100
#8	95-100

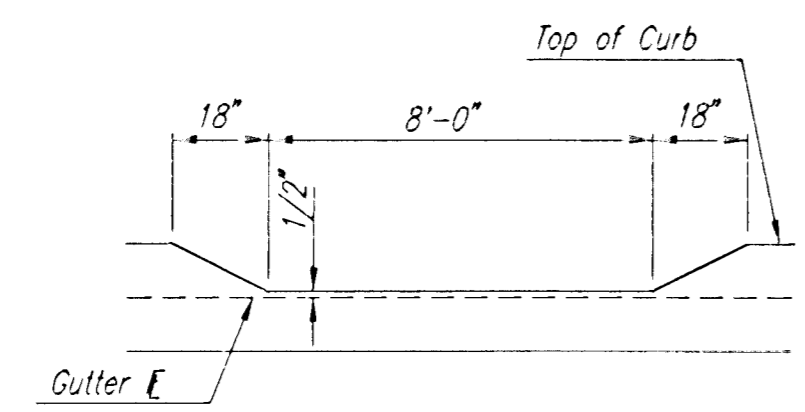
ROCK QUALITY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY THE KDOT 1990 EDITION STANDARD SPECIFICATION SUBSECTION 1102 FOR DURABILITY CLASS I.

PAVEMENT UNDERDRAIN LOCATIONS

STREET	STATION	SIDE
WILSON ESTATES PARKWAY	47+07.77	RT.
WILSON ESTATES PARKWAY	47+09.06	LT.



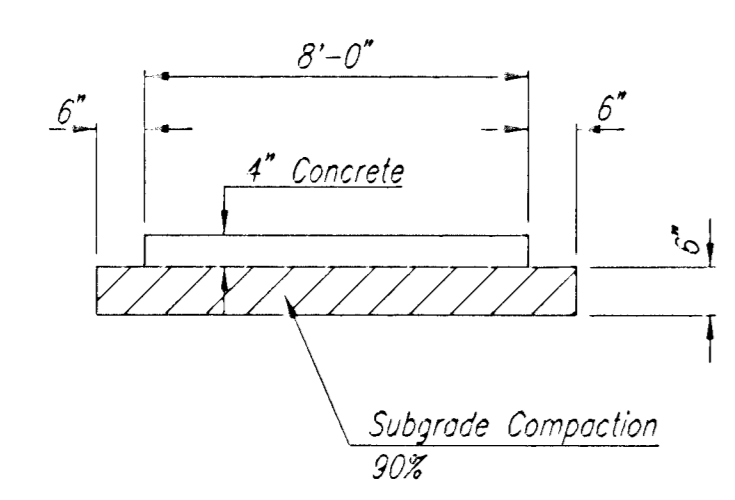
GROOVE DETAIL



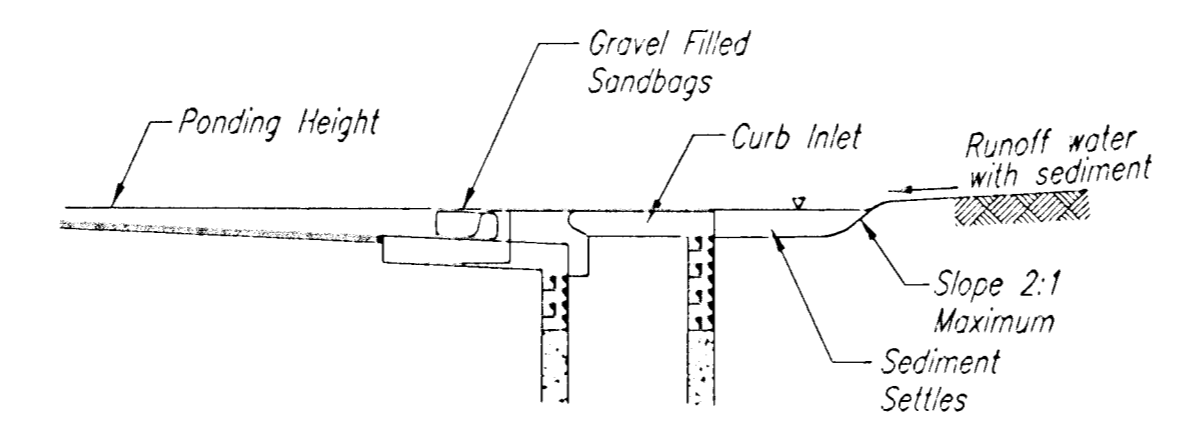
DEPRESSED CURB DETAIL

TYPICAL WHEELCHAIR RAMP DETAILS

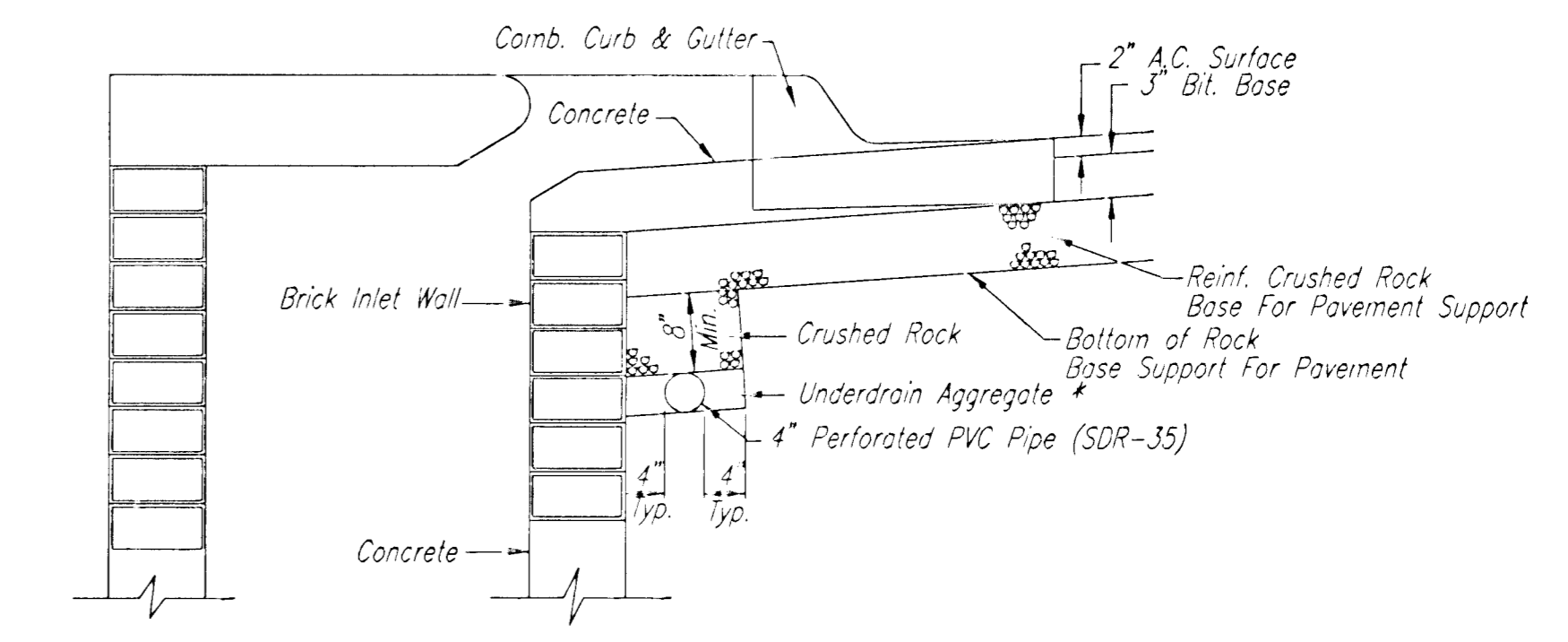
4" WALK DETAILS



TYPICAL SIDEWALK SECTION



SECTION C - C



SECTION B-B

PAVEMENT UNDERDRAIN DETAILS

NOTE: PLACE 4" PVC PERFORATED PIPE AT ALL DRAINAGE SUMP LOCATIONS OR WHERE SPECIFIED ON PLANS.

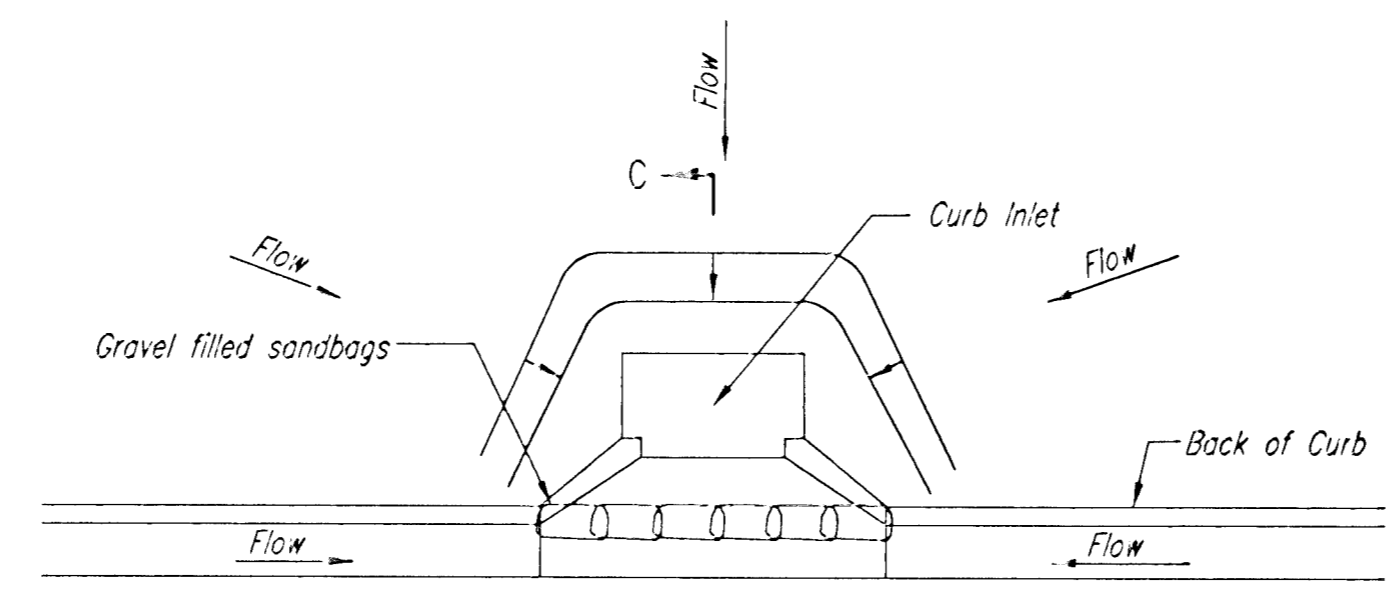
COST OF UNDERDRAIN SYSTEM TO BE INCIDENTAL TO THE REINFORCED CRUSHED ROCK BASE.

INLET TYPE MAY VARY FROM THAT SHOWN.

BRICK PAVEMENT DRAIN TO DISCHARGE INTO CRUSHED ROCK. (SEE SH. NO. 4)

SIDEWALK NOTES

1. THE SIDEWALK ALIGNMENT AS SHOWN IS APPROX. ONLY. THE FINAL ALIGNMENT SHALL BE APPROVED BY THE OWNER. (LAHAM DEVELOPMENT CO. LLC)
2. THE CONTRACTOR SHALL NOT START SIDEWALK CONSTRUCTION UNTIL ALL OF THE PROPOSED EARTH BERMS HAVE BEEN FINISH GRADED AND APPROVED BY THE ENGINEER.
3. THE CONTRACTOR SHALL COMPACT 6" SUBGRADE UNDER ALL SIDEWALKS AS DETAILED ON THIS SHEET. ALL SIDEWALK COMPACTION SHALL BE CONSIDERED SUBSIDIARY TO "SIDEWALK CONCRETE (4)".



INLET SEDIMENT BARRIER (CURB INLET)

NOTE: INLET TYPE AND SIZE MAY VARY FROM THAT SHOWN.

WILSON ESTATES PARKWAY

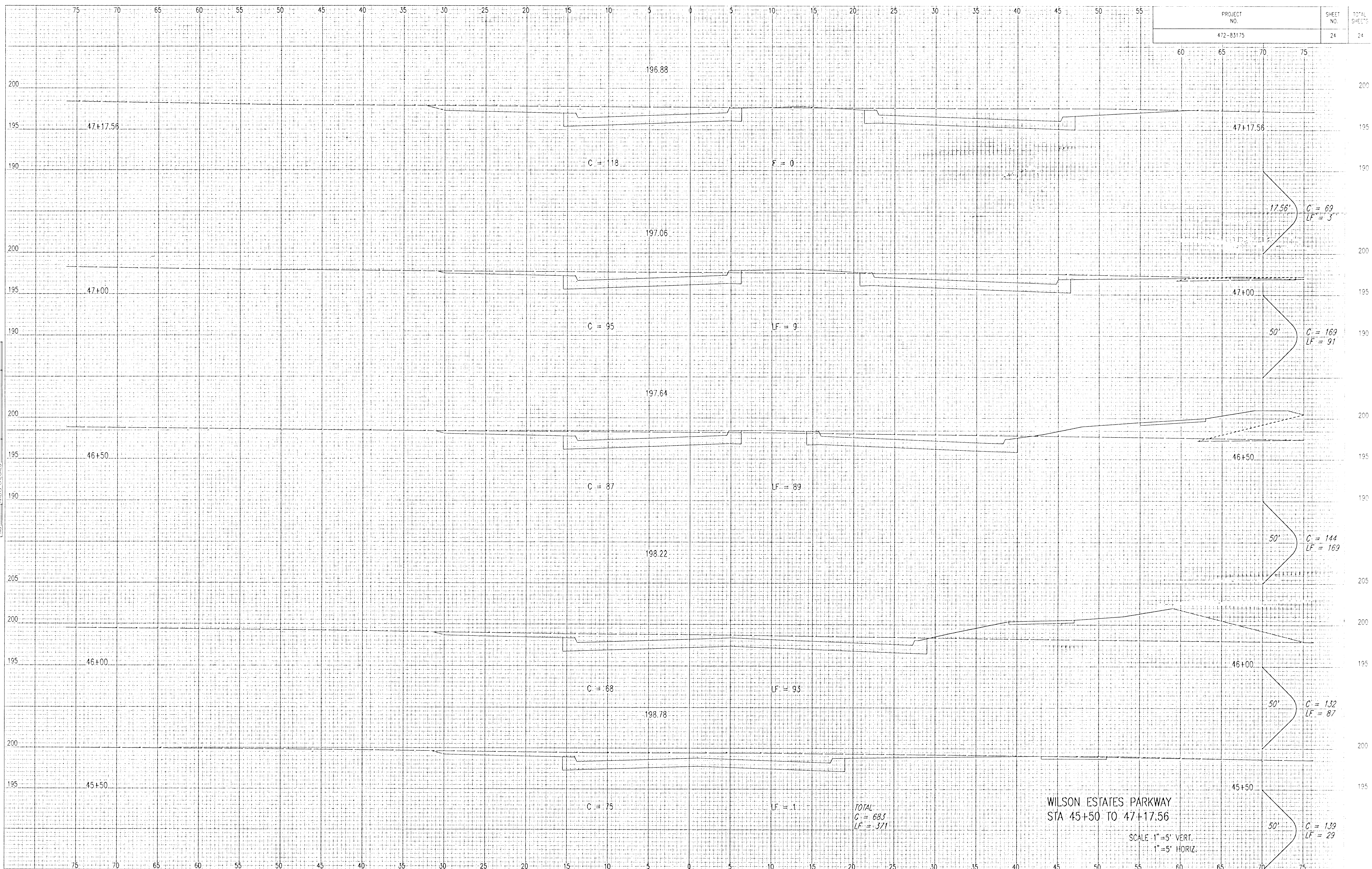
MISCELLANEOUS DETAILS

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 303 S. TOPEKA • WICHITA, KANSAS 67202
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Designed by	BER, ODD	Checked by	
Drawn by	DEP	Date	MARCH, 2000
		Job No.	00034

DSNR: BER OPER: SMO SCALE: 1=4.00
 0:1,2000\00034\MISC\CDTL 05-26-2000 04:59:47 pm

NOTE BOOK	TEMPLATE	AREAS	CHECKED
NO.			



TOTAL
C = 683
LF = 311

WILSON ESTATES PARKWAY
STA 45+50 TO 47+17.56

SCALE: 1" = 5' VERT.
1" = 5' HORIZ.

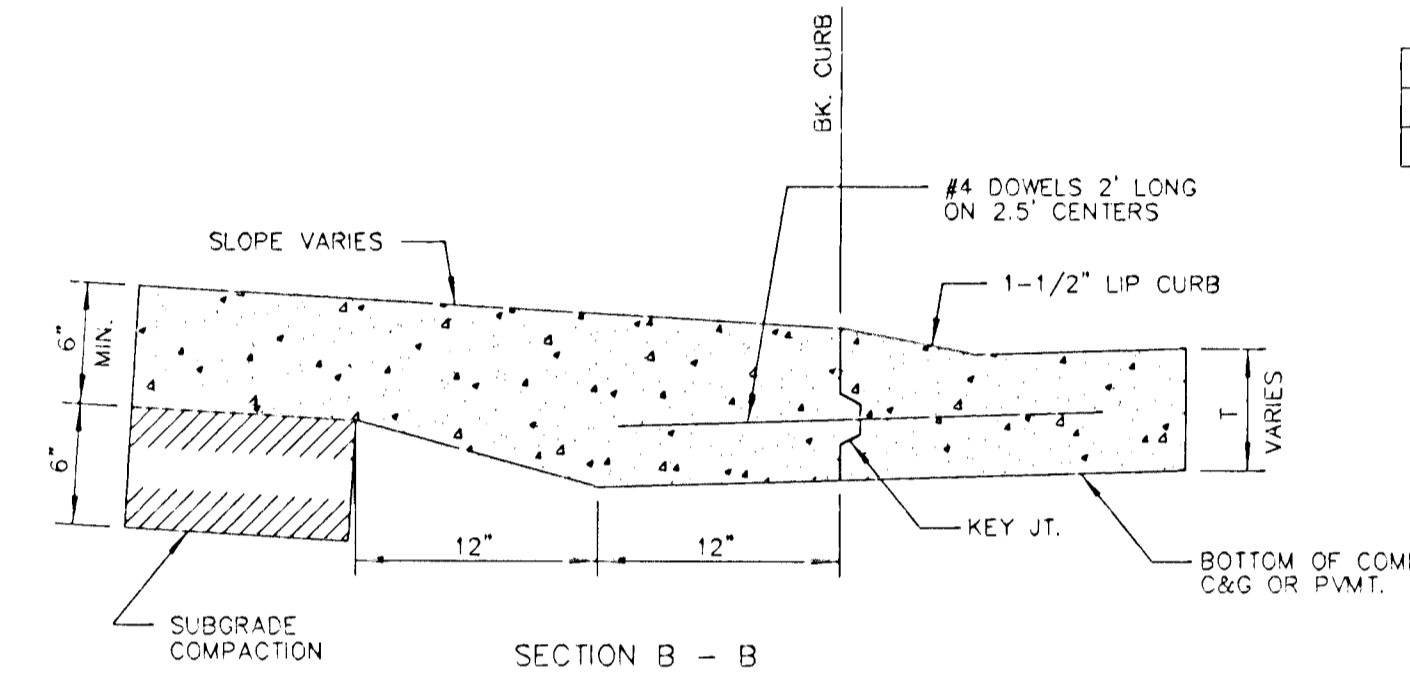
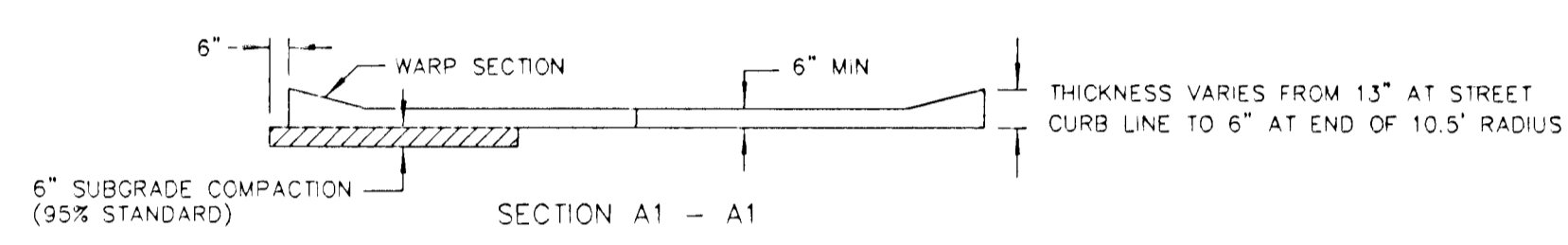
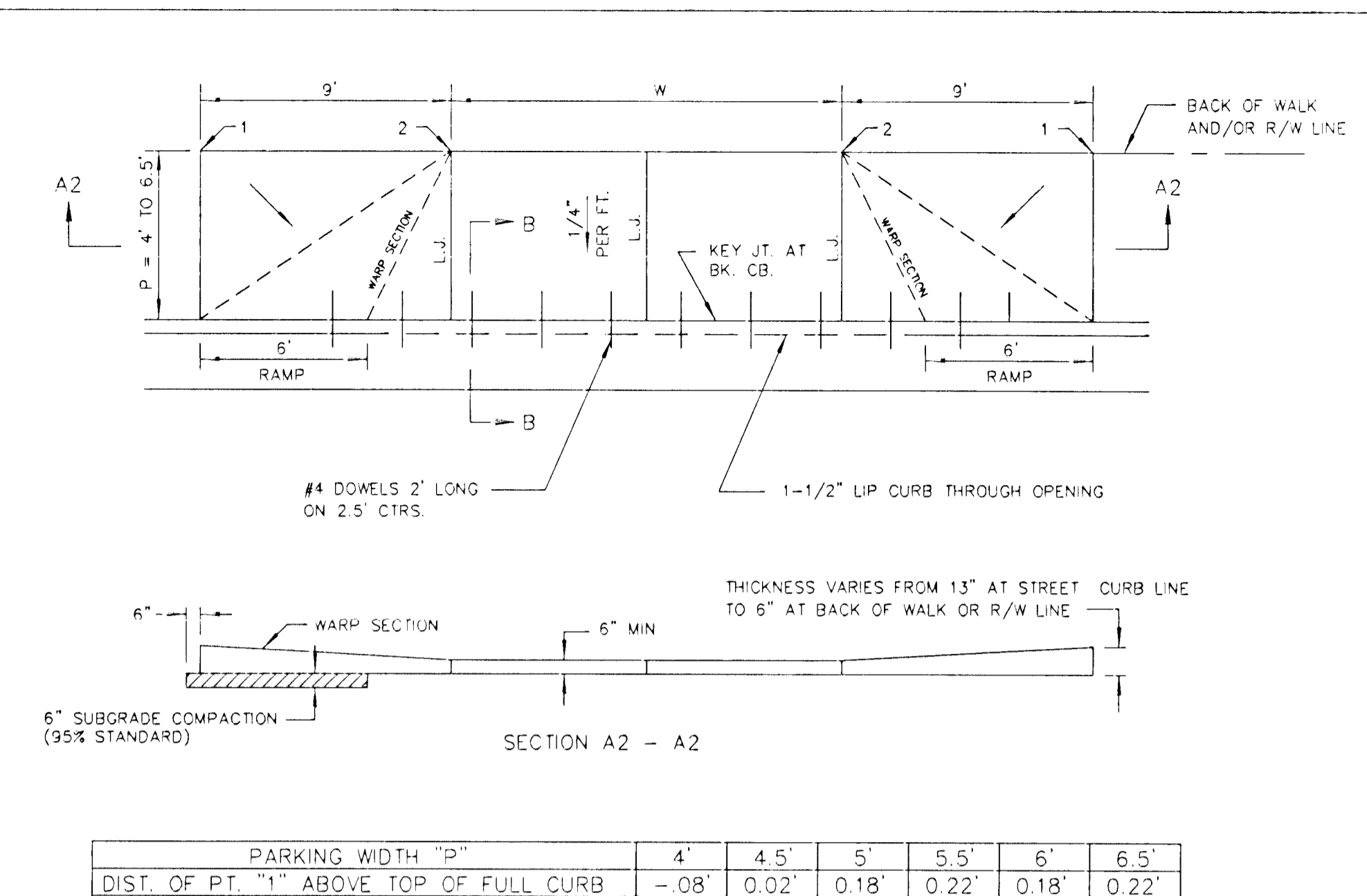
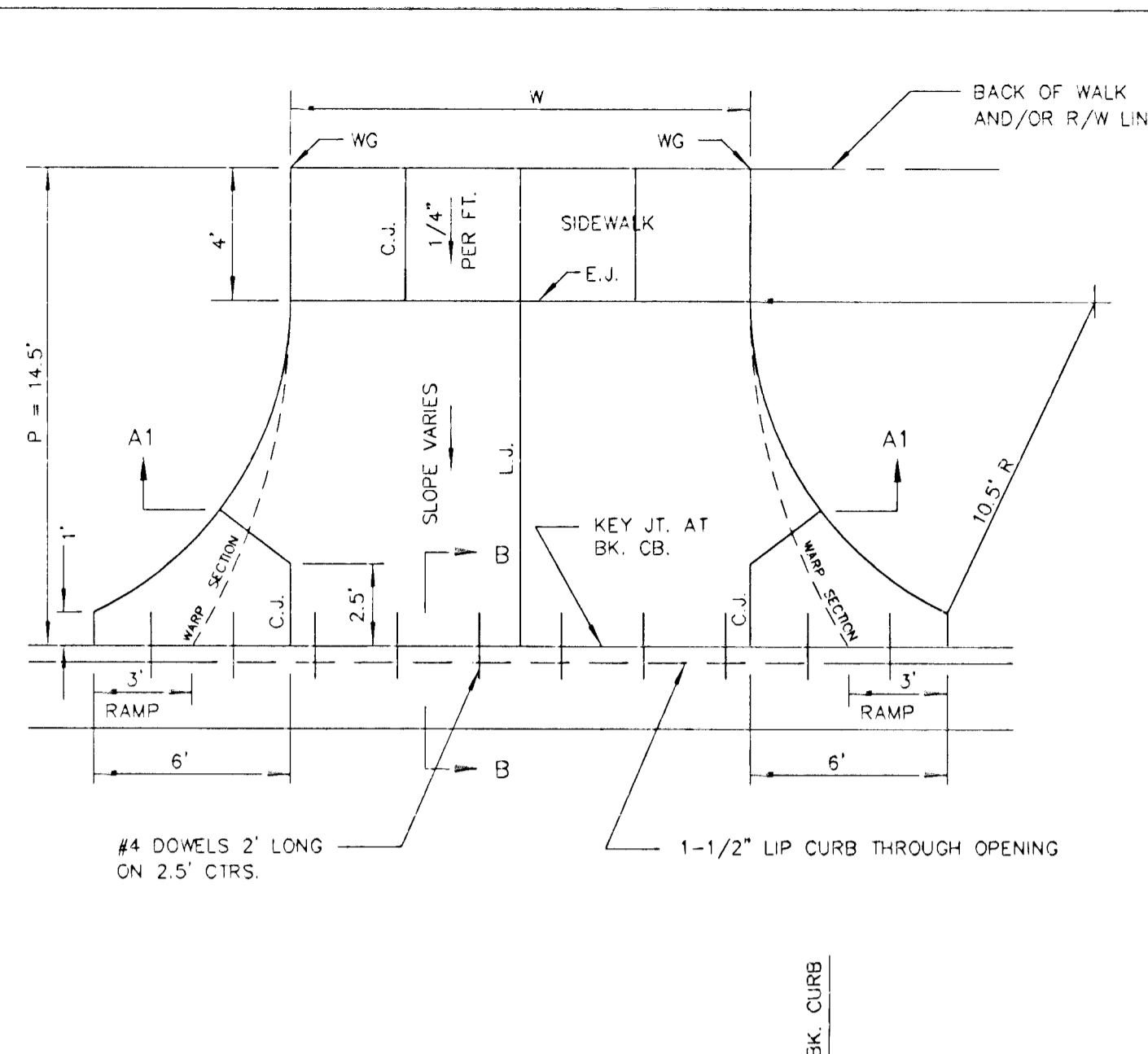
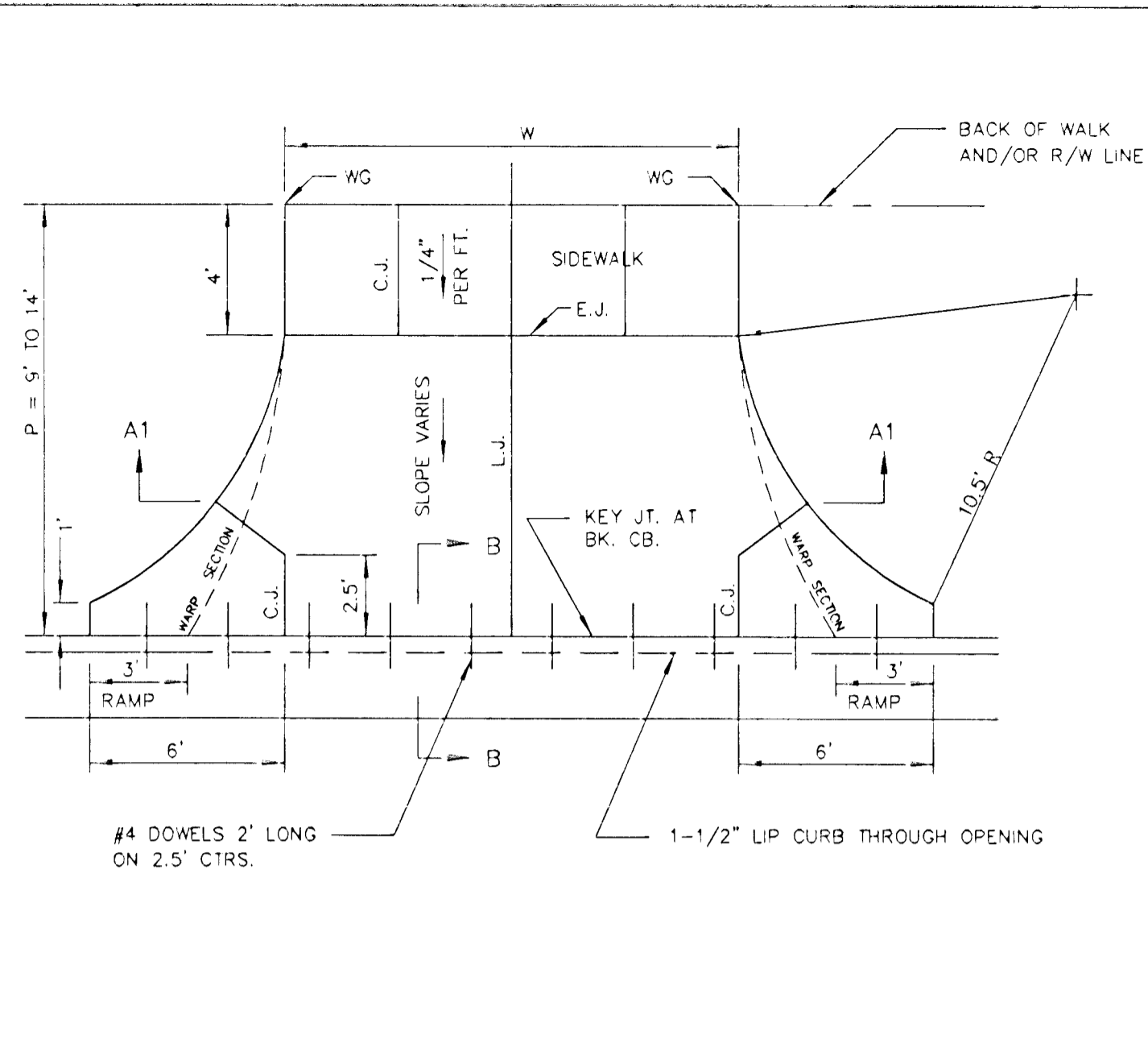
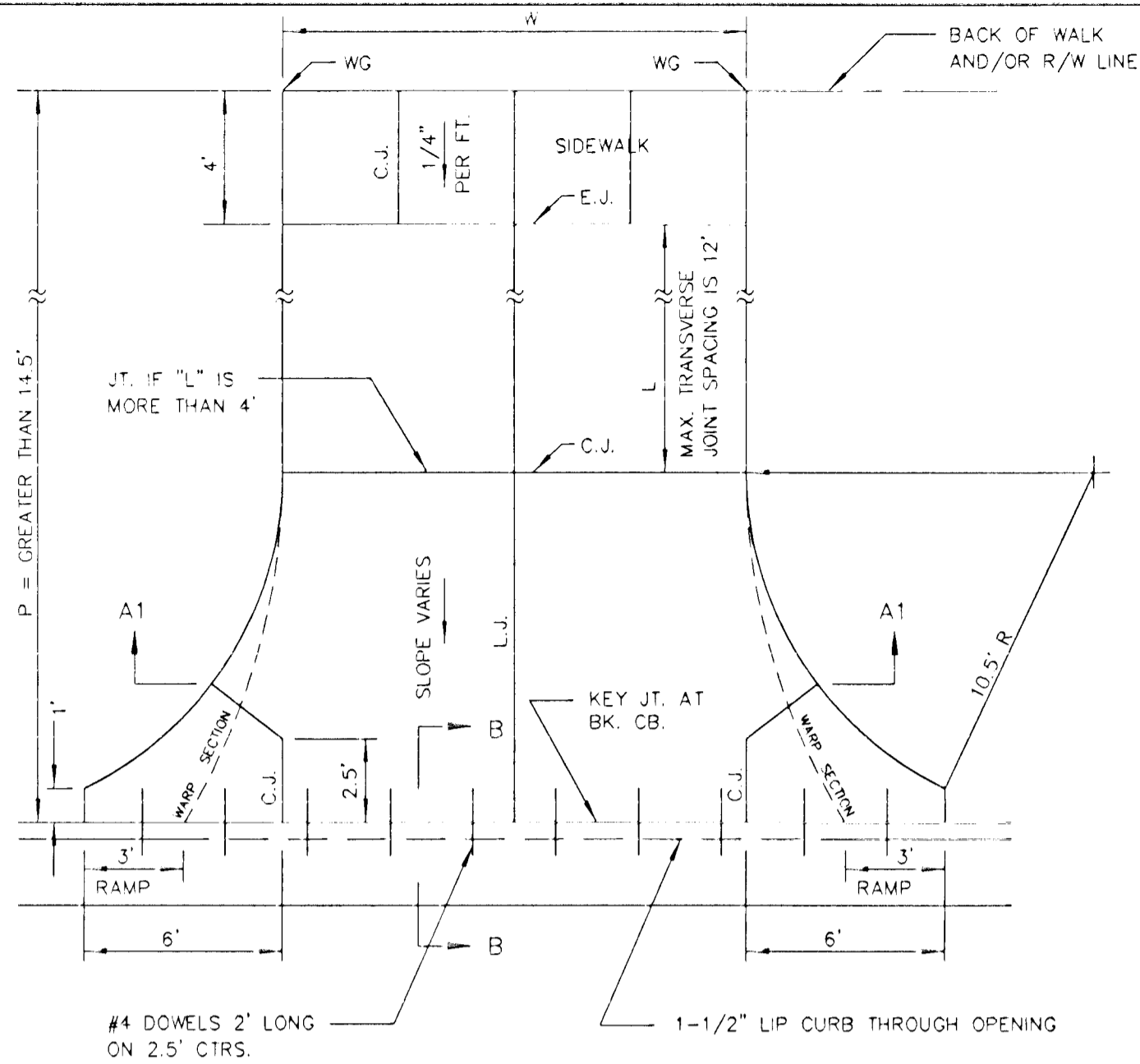
17.56'
C = 69
LF = 3

50'
C = 169
LF = 91

50'
C = 144
LF = 169

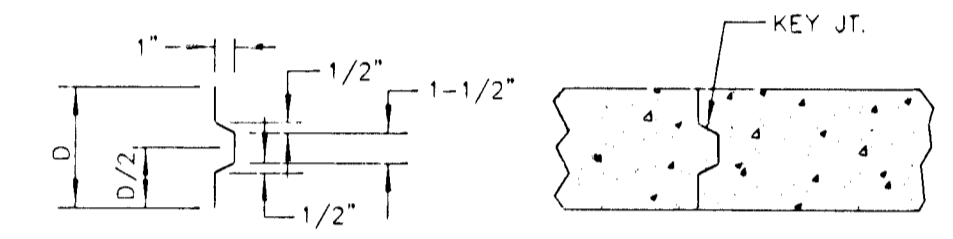
50'
C = 132
LF = 87

50'
C = 139
LF = 29



PARKING WIDTH "P"	4'	4.5'	5'	5.5'	6'	6.5'
DIST. OF PT. "1" ABOVE TOP OF FULL CURB	-08'	0.02'	0.18'	0.22'	0.18'	0.22'
DIST. OF PT. "2" BELOW TOP OF FULL CURB	-08'	0.02'	0.18'	0.22'	0.18'	0.22'

FULL RAMP DRIVE (P = 4.0' TO 6.5')

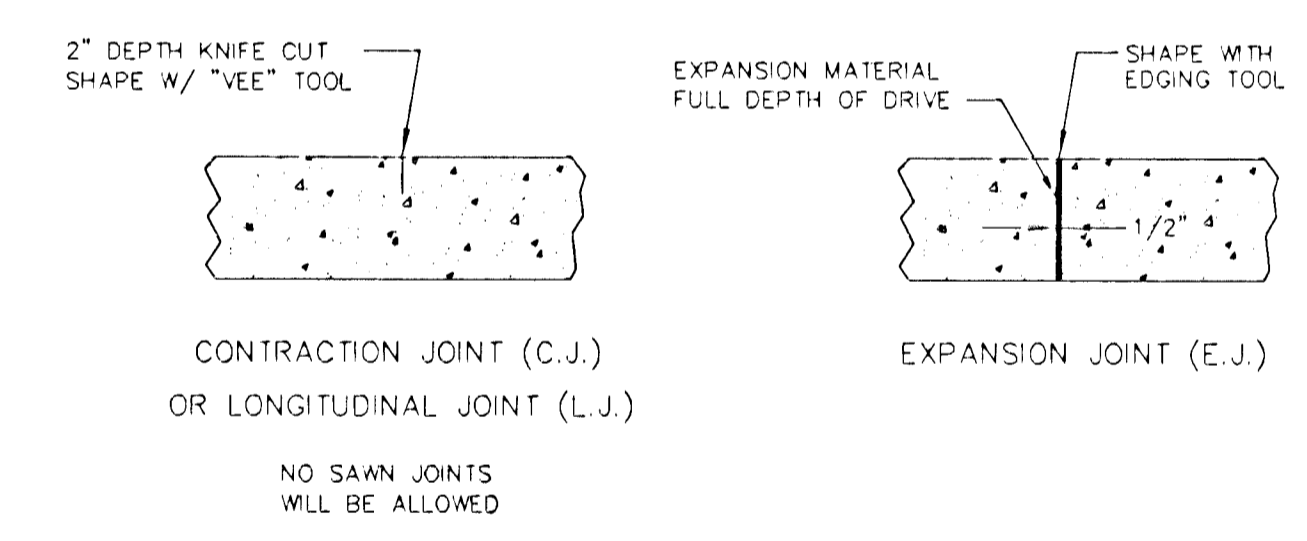


ALT. LONGITUDINAL CONSTRUCTION JOINT

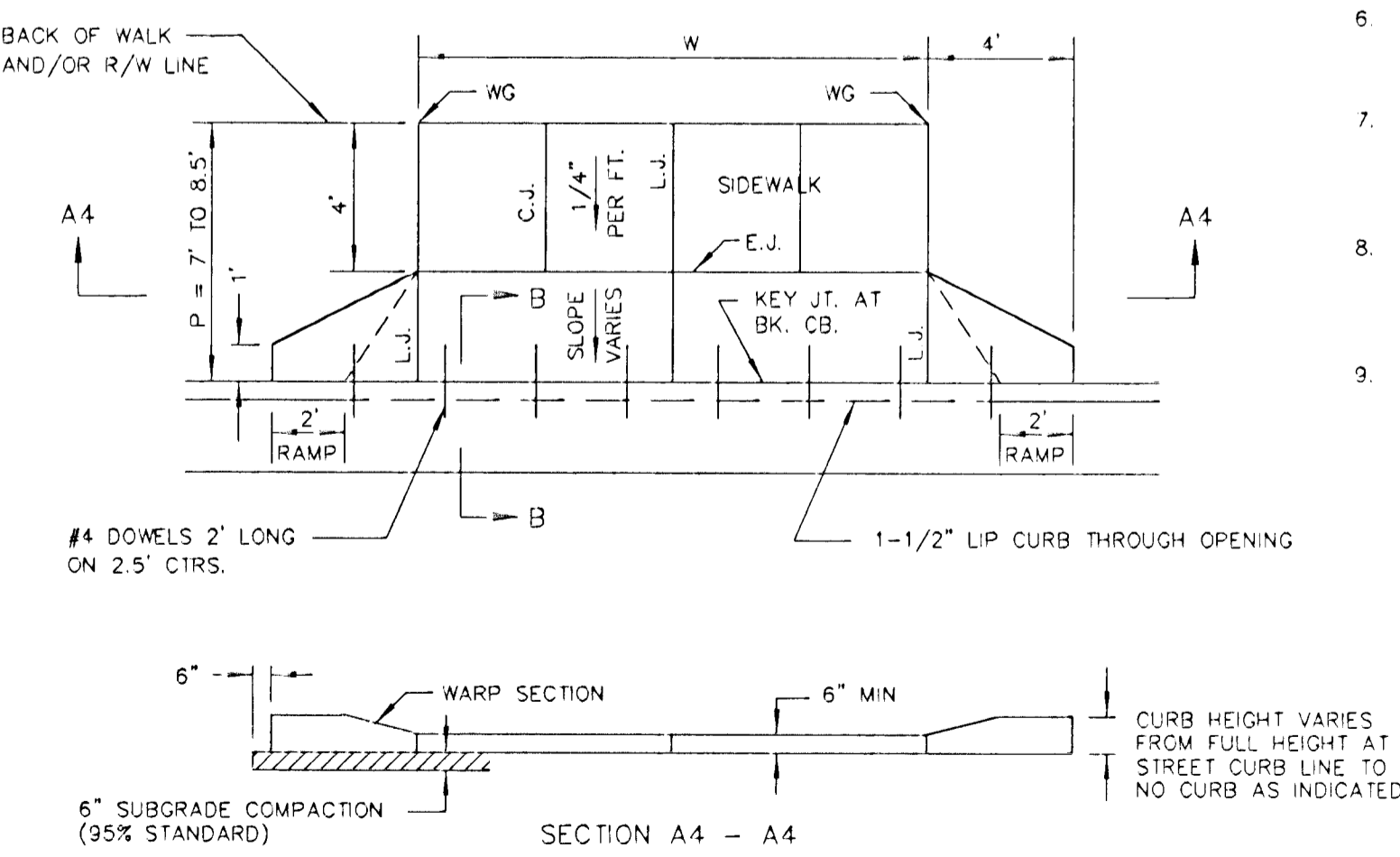
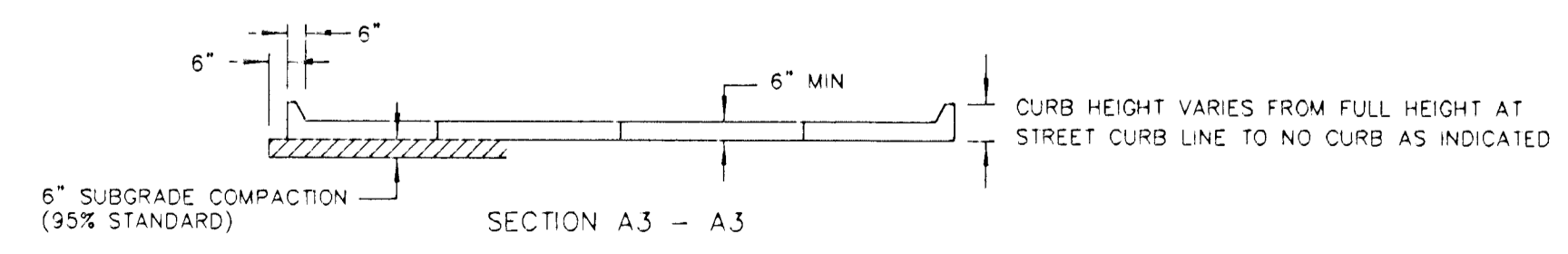
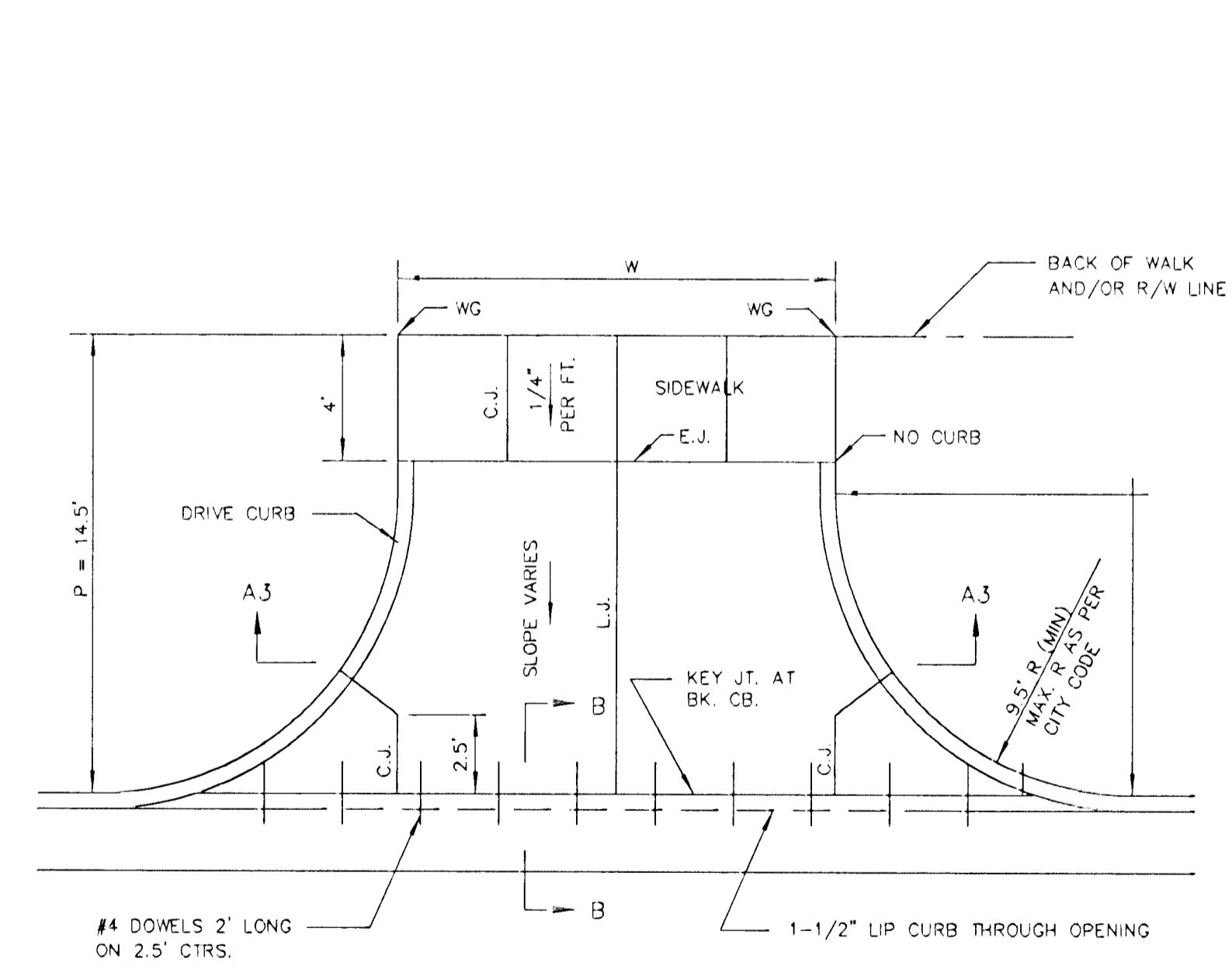
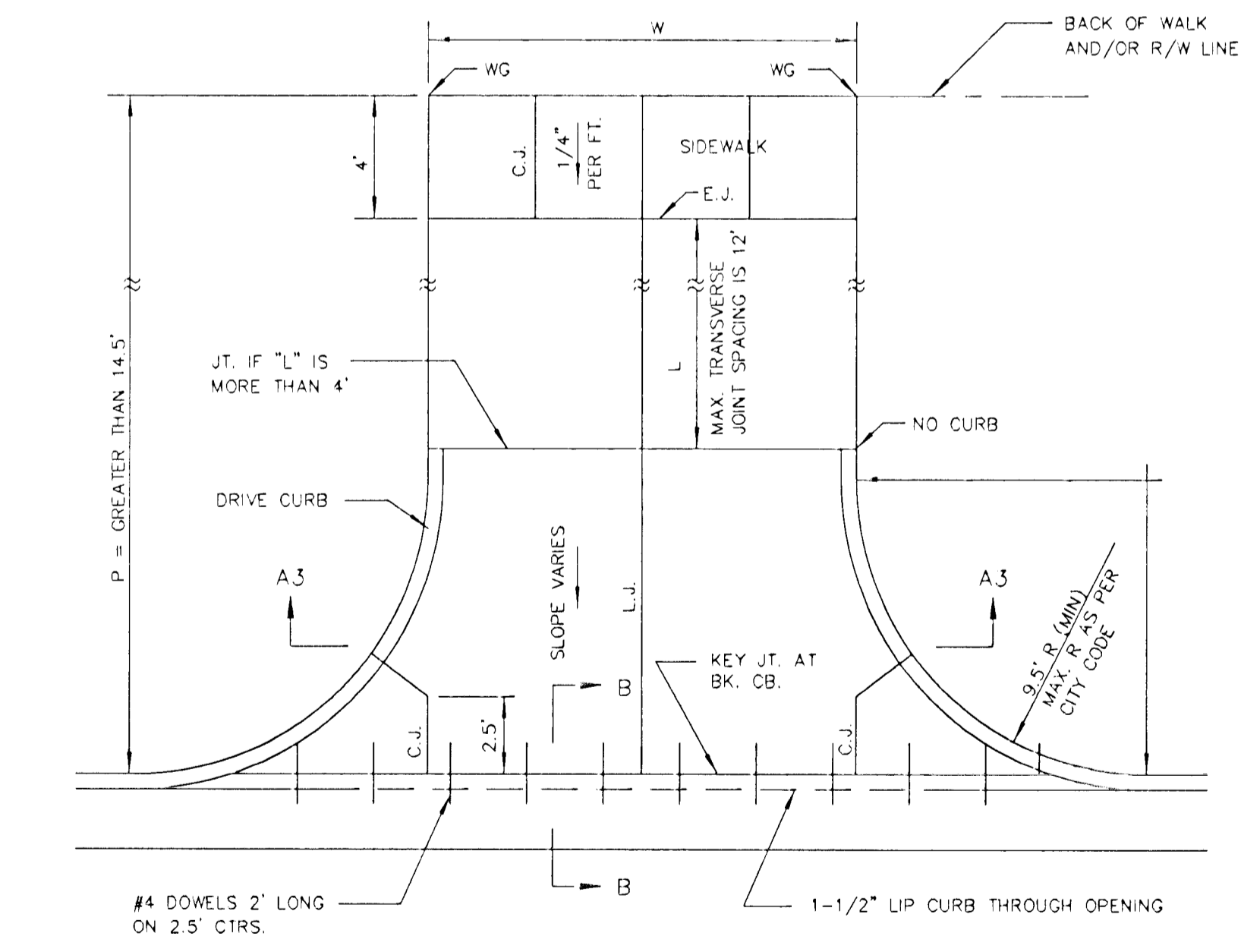
PARKING WIDTH "P"	9'	10'	11'	12'	13'	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.27'	0.27'	0.32'	0.37'	0.52'	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.27'	0.27'	0.32'	0.37'	0.52'	0.62'	0.96'	1.22'	1.48'	1.74'	2.00'	2.26'	2.52'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.19'	0.21'	0.23'	0.25'	0.30'	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-19'	-16'	-13'	-10'	-06'	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

RADIUS RAMP DRIVES (P = 9.0' & GREATER)

BACK OF CURB DETAIL



- GENERAL NOTES
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH FULL WALK.
 - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "W" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "W" DIMENSION GREATER THAN 24'.
 - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
 - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
 - DOVEL BARS SHALL BE OMITTED FROM THE KEYED CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
 - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
 - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
 - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" #4-#4 WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONCURRENCE.
 - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE GUTTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.



PARKING WIDTH "P"	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.72'	1.27'	1.77'	2.27'	2.77'	3.27'	3.77'	4.27'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.70'	1.04'	1.30'	1.56'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

FULL RADIUS DRIVES (P = 14.5' & GREATER)

PARKING WIDTH "P"	7'	7.5'	8'	8.5'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-08'	0.02'	0.18'	0.22'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-08'	0.02'	0.18'	0.22'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-15'	-16'	-17'	-17'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-25'	-20'	-20'	-20'

FULL RAMP DRIVE (P = 7.0' TO 8.5')

DSNR: G03 OPER: S40 SCALE: 1=1.00
 C:\2000\00034\DRIVEWAY 05-26-2003 04:55:18 pm

THE CITY OF WICHITA
CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202
(316) 255-4501
(316) 255-4114 FAX

**STANDARD DRIVE ENTRANCES
FULL HEIGHT CURB**

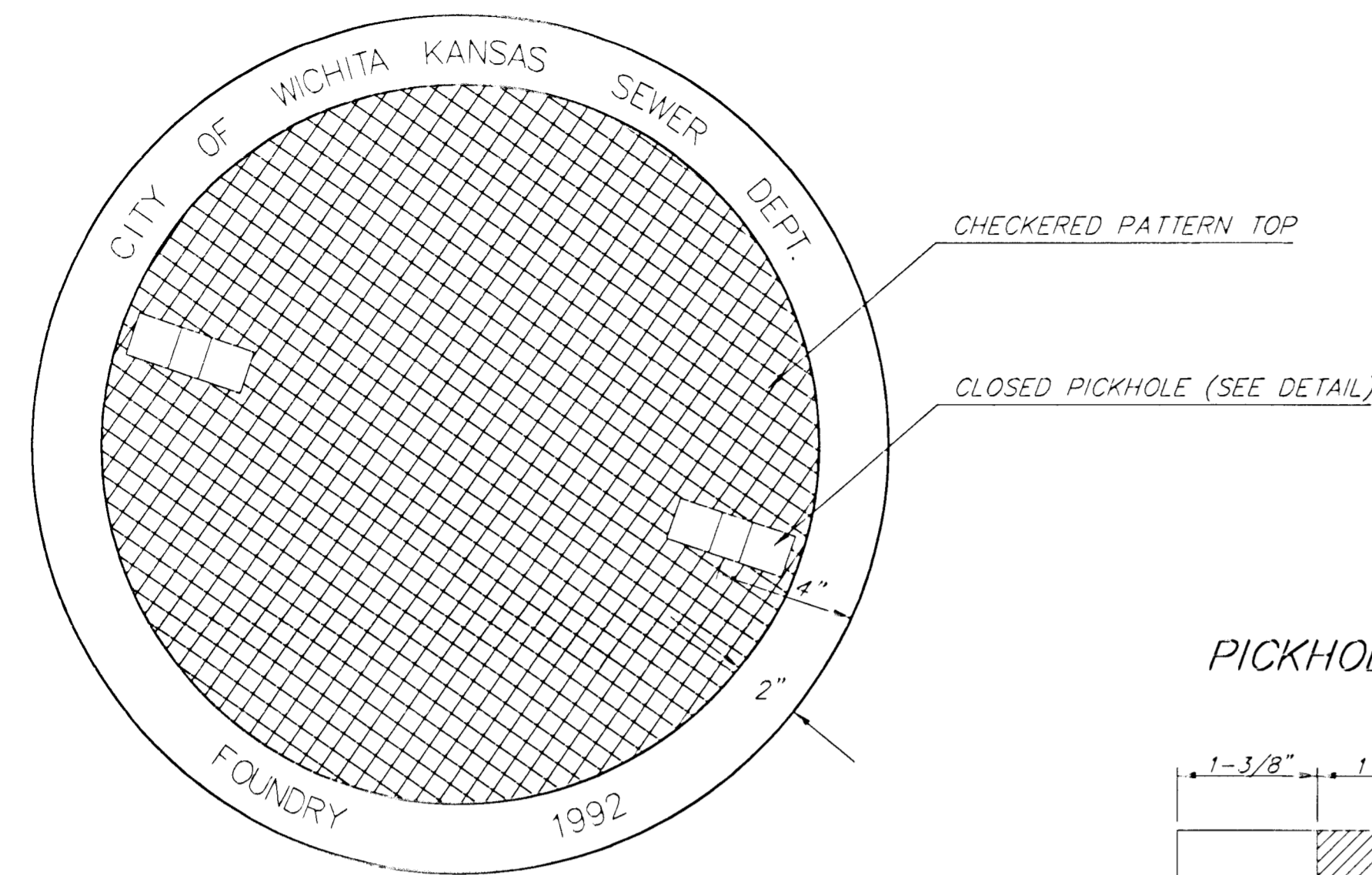
M. E. LINDEBAK P.E. - CITY ENGINEER

PROJECT NUMBER 472-83175	DCA NO. 765634
DATE MAR 96	SHEET 16 OF 24

MANHOLE COVER
Weight = 180 Lbs.

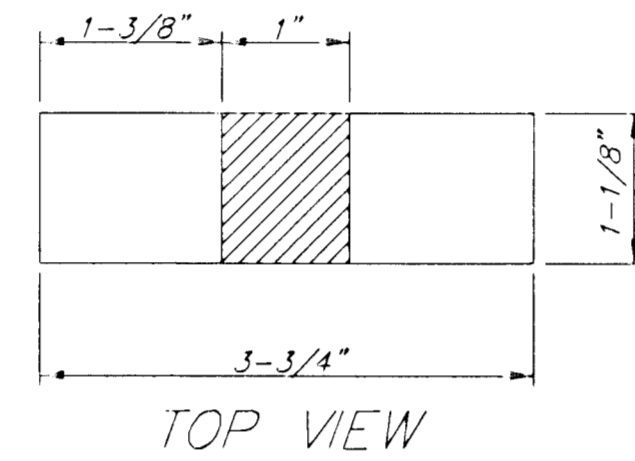
MANHOLE FRAME AND COVER DETAIL

ADOPTED AS STANDARD DESIGN BY
CITY OF WICHITA, KANSAS

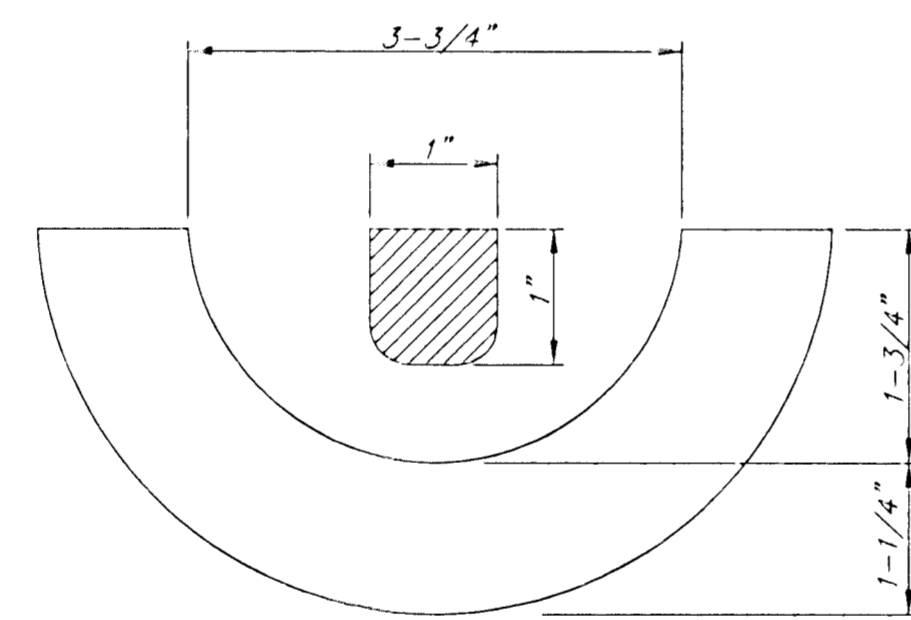


TOP VIEW

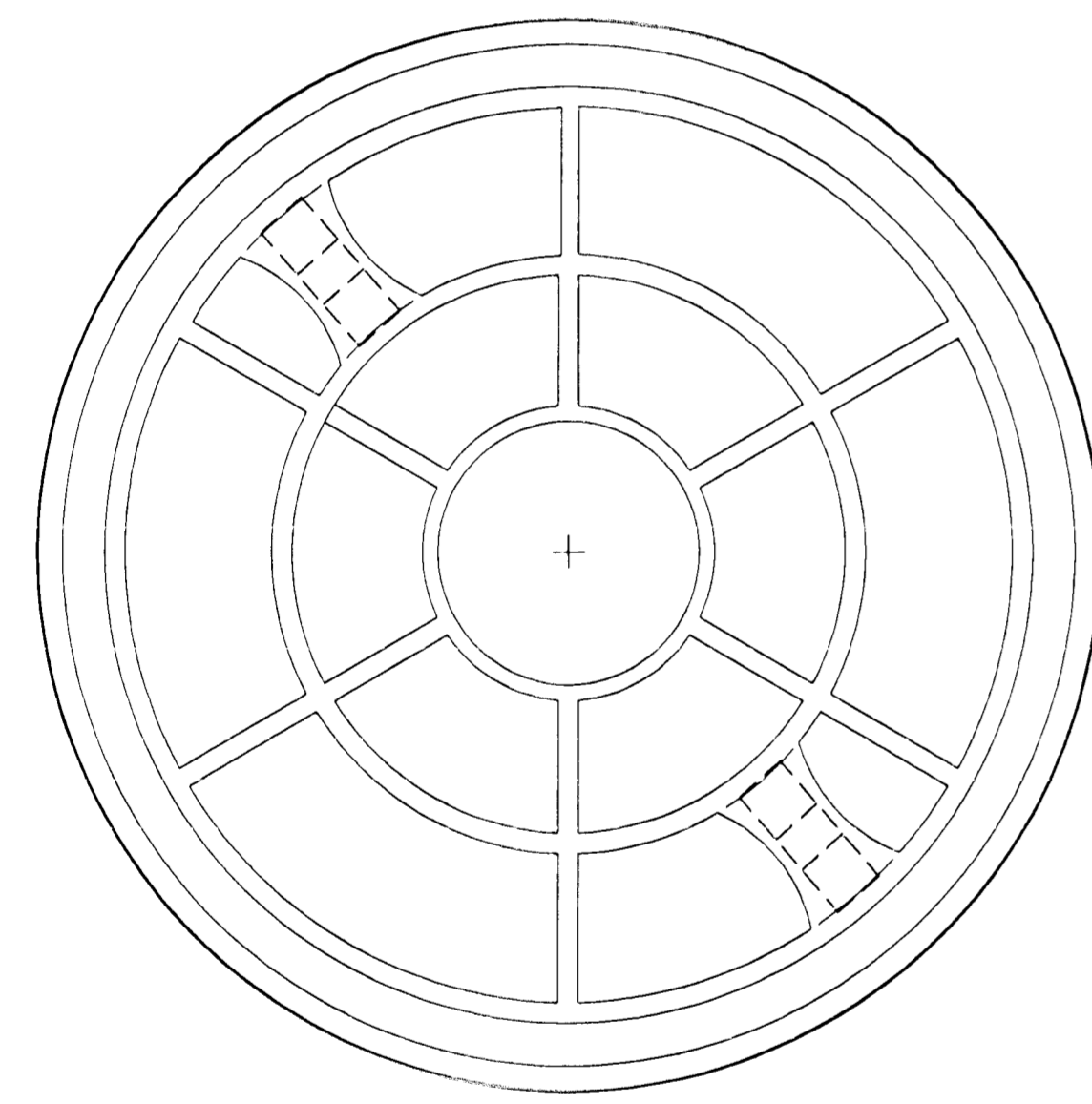
PICKHOLE DETAIL



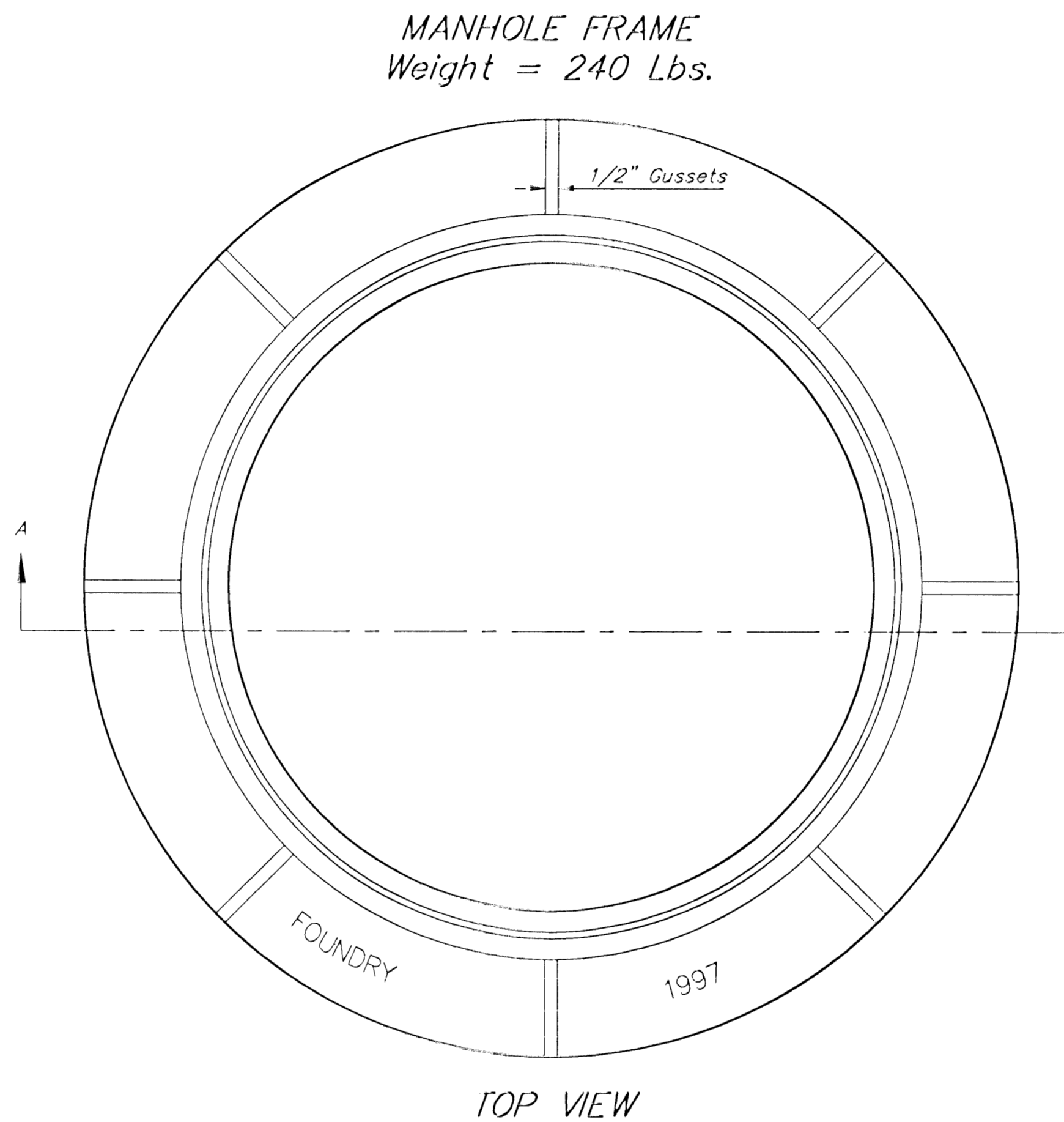
TOP VIEW



SECTION VIEW



BOTTOM VIEW

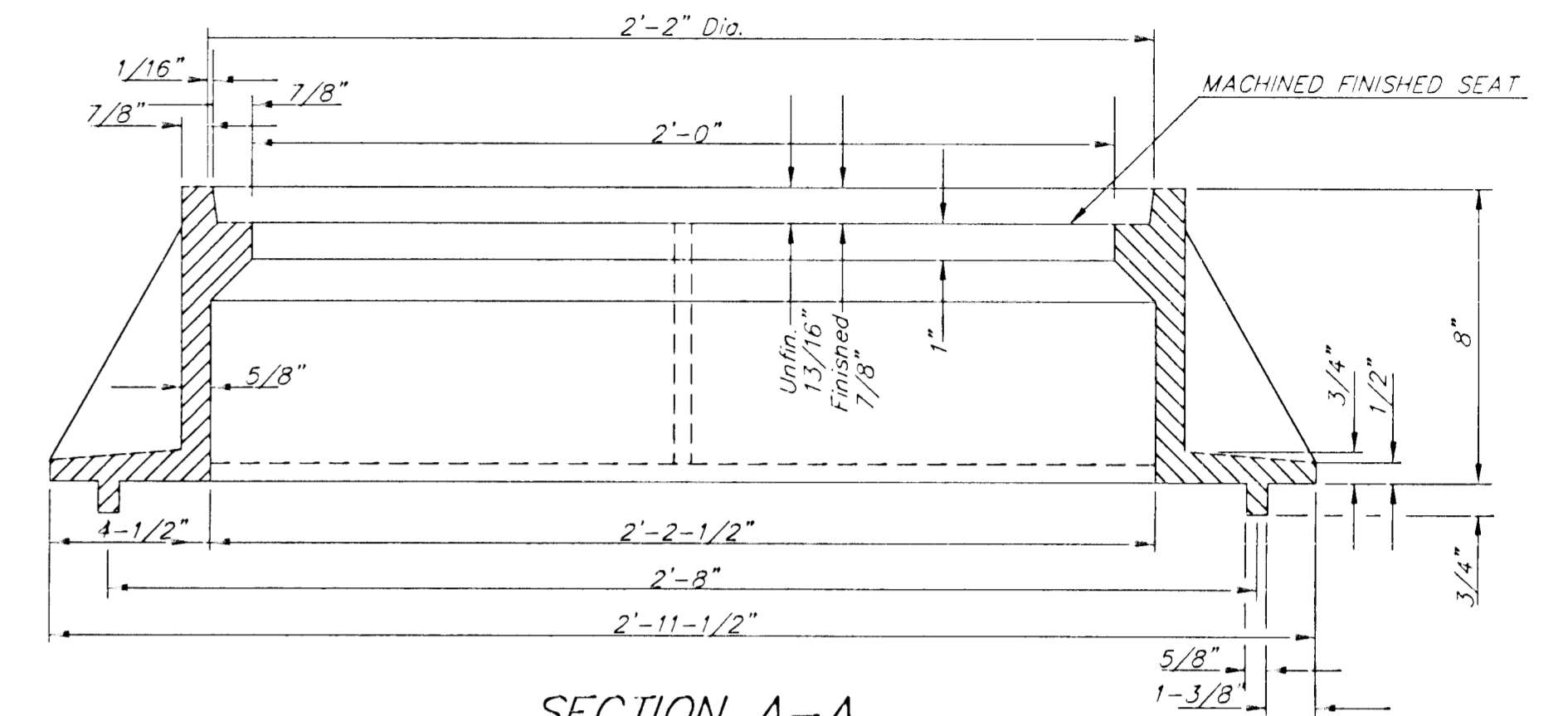


MANHOLE FRAME
Weight = 240 Lbs.

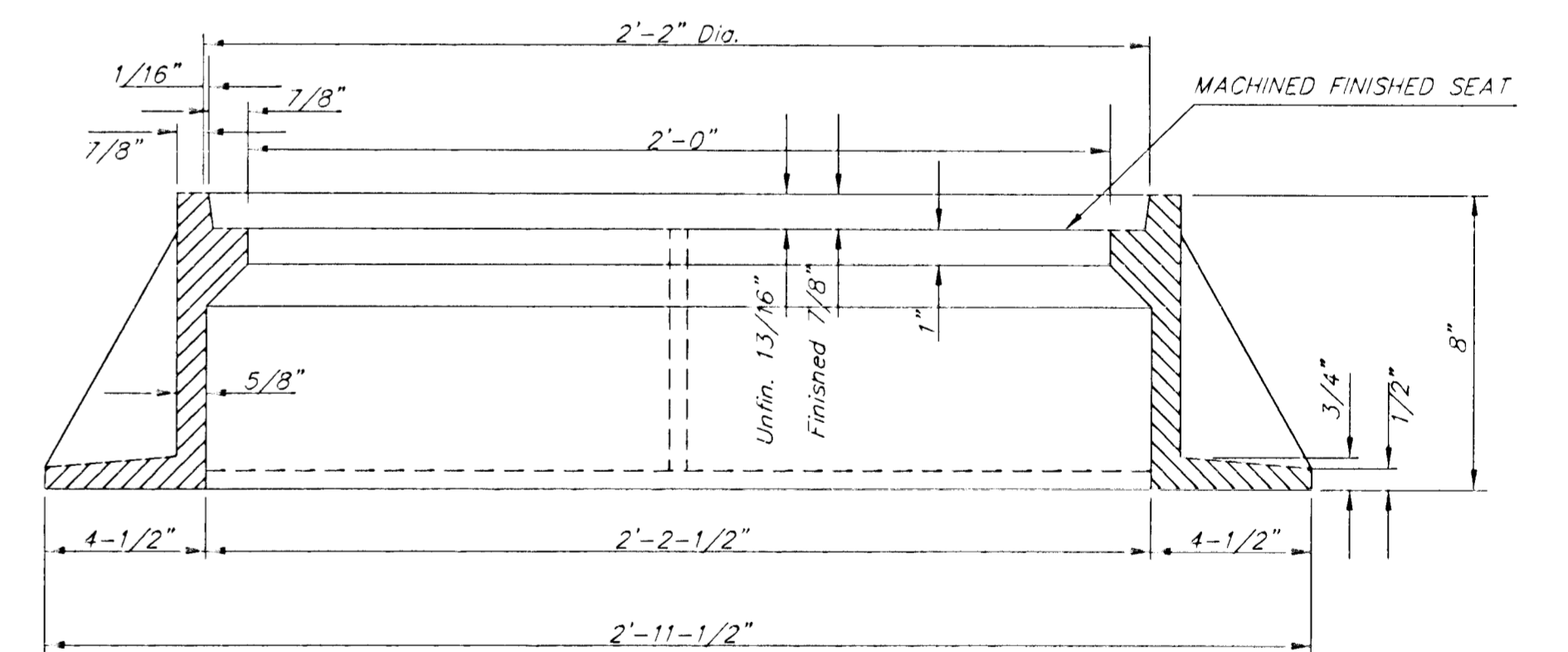
TOP VIEW

GENERAL NOTES

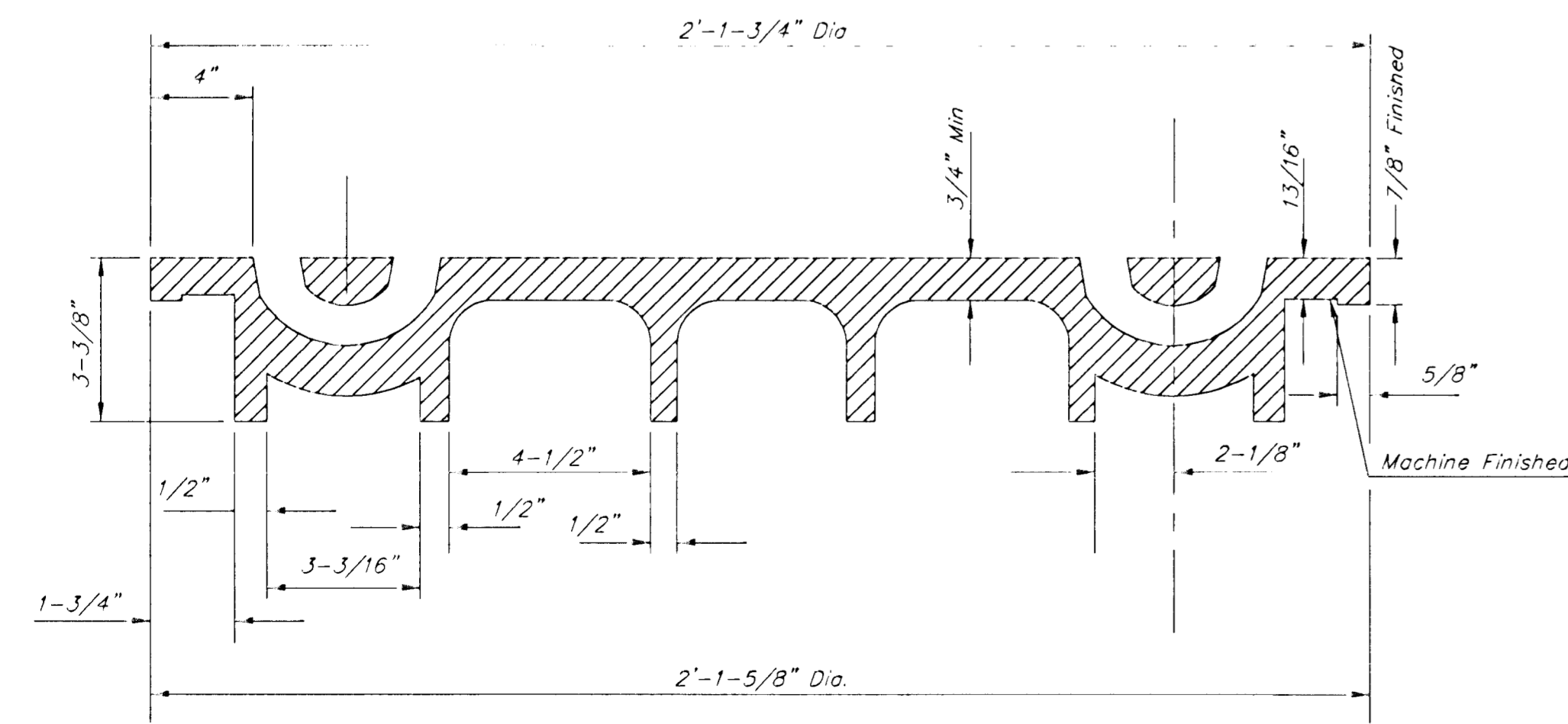
- MANHOLE CASTINGS SHALL BE MANUFACTURED USING GOOD QUALITY GRAY IRON CONFORMING TO CLASS 30 OF A.S.T.M. DESIGNATION A-48. DIMENSIONS AND WEIGHTS SHOWN ON THE DETAILED DRAWINGS SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS AND ANY DEVIATIONS FROM THE DIMENSIONS SHOWN MUST BE SPECIFICALLY APPROVED. THE FINISHED CASTINGS SHALL BE OF UNIFORM QUALITY, FREE FROM BLOWHOLES, POROSITY, HARD SPOTS, SHRINKAGE DISTORTIONS OR OTHER DEFECTS.
- MANHOLE CASTINGS SHALL WEIGH A MINIMUM OF 180 POUNDS ON THE SOLID COVER AND 240 POUNDS ON THE MANHOLE RING. THIS IS A TOTAL OF 420 POUNDS ON A RING AND COVER SET. CASTINGS WEIGHING LESS THAN THE MINIMUM SPECIFICATIONS WILL NOT BE ACCEPTED.
- MANHOLE CASTINGS SHALL BE MANUFACTURED SUCH THAT A COVER MANUFACTURED BY ANY ONE FOUNDRY WILL FIT INTERCHANGEABLY INTO A FRAME MANUFACTURED BY ANOTHER FOUNDRY AND STILL MEET ALLOWABLE CLEARANCES AND NON-ROCKING REQUIREMENTS. THIS WILL REQUIRE MANUFACTURING OF THE MATCHING FACES ON THE COVER AND THE FRAME TO CLOSE TOLERANCES.
- THE OUTSIDE CIRCUMFERENCE OF THE VERTICAL FACE OF THE COVER AND THE INSIDE CIRCUMFERENCE OF THE VERTICAL FACE IN THE FRAME RECESS SHALL BE MANUFACTURED TO TOLERANCES SUCH THAT THE CLEARANCE BETWEEN THE COVER AND FRAME WILL NOT EXCEED 1/8" AT ANY POINT AROUND THE CIRCUMFERENCE OF THE COVER. THE SEATING SURFACES BETWEEN THE COVER AND FRAME SHALL BE MACHINED SUCH THAT THESE SEATING SURFACES SHALL MAKE FULL CONTACT FOR THEIR FULL CIRCUMFERENCE TO PRECLUDE THE COVER FROM ROCKING IN THE FRAME.
- THE MANHOLE FRAME AND COVER SHALL BE MARKED WITH LETTERING INDICATING THE NAME OF THE MANUFACTURER AND THE YEAR WHEN THE COVER OR FRAME WAS CAST. THE COVER SHALL BE FURTHER IDENTIFIED WITH REGARDS TO OWNERSHIP USING LETTERS AT LEAST 1 INCH IN HEIGHT. THIS IDENTIFICATION SHALL BE "CITY OF WICHITA SEWER DEPARTMENT". THE WORD DEPARTMENT MAY BE ABBREVIATED. THE TEXTURE OF THE TOP SURFACE OF THE COVER SHALL BE MANUFACTURED IN A CHECKERED PATTERN DESIGN AS INDICATED ON THE DRAWINGS. SMOOTH BLOCKOUTS SHALL BE UTILIZED TO HIGHLIGHT THE LETTERING ON THE COVER SURFACE. THE TOTAL AREA OF SMOOTH SURFACE BLOCKOUT SHALL NOT EXCEED THE AREA AS INDICATED ON THE DRAWING. POSITIONING OF SMOOTH BLOCKOUTS AND LETTERING MAY VARY FROM THAT SHOWN ON THE DETAILED DRAWING.



SECTION A-A
MUD RING



SECTION A-A



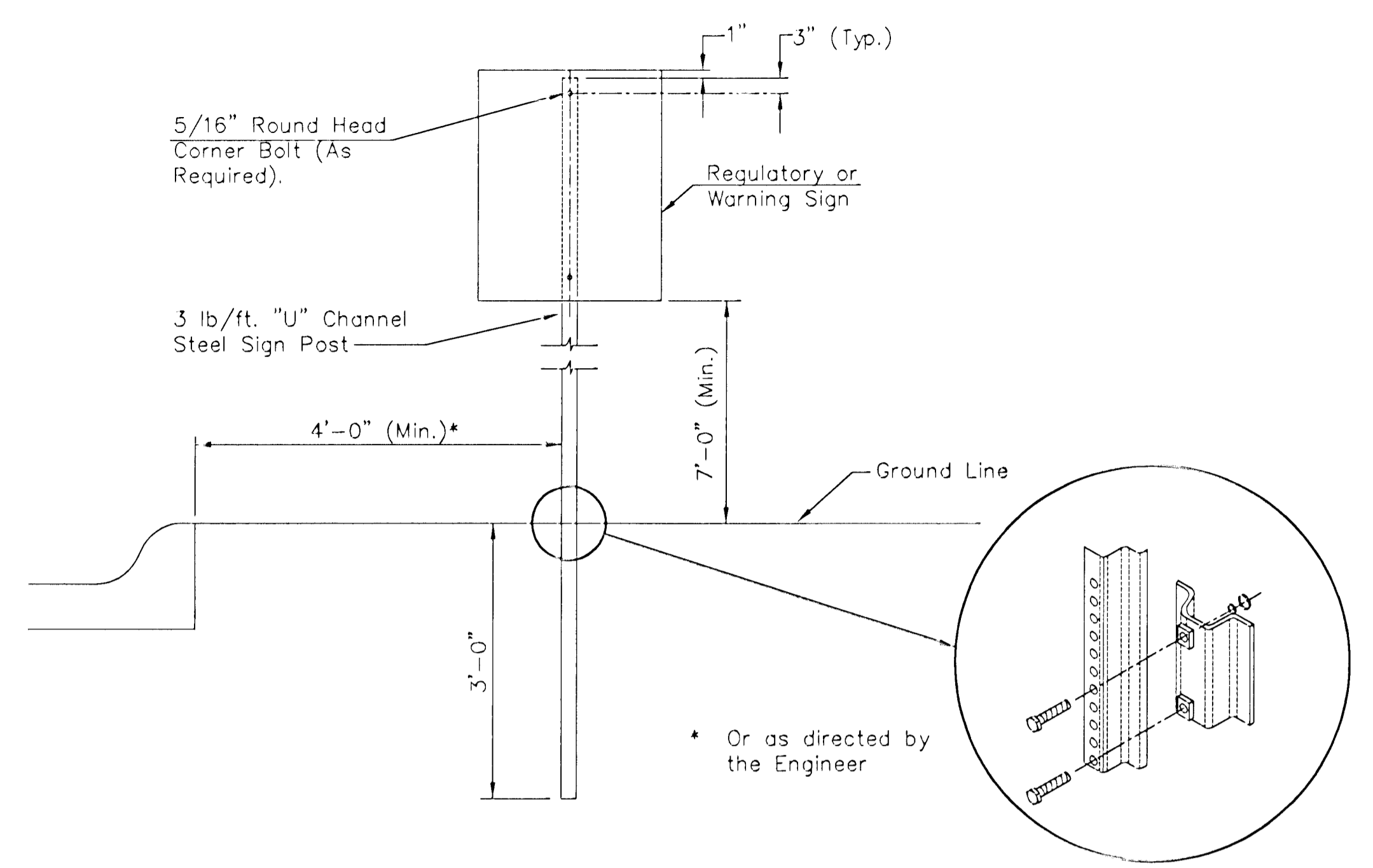
SECTION VIEW

DSNR_CDD OFFER: S40 SCALE: 1=1.00
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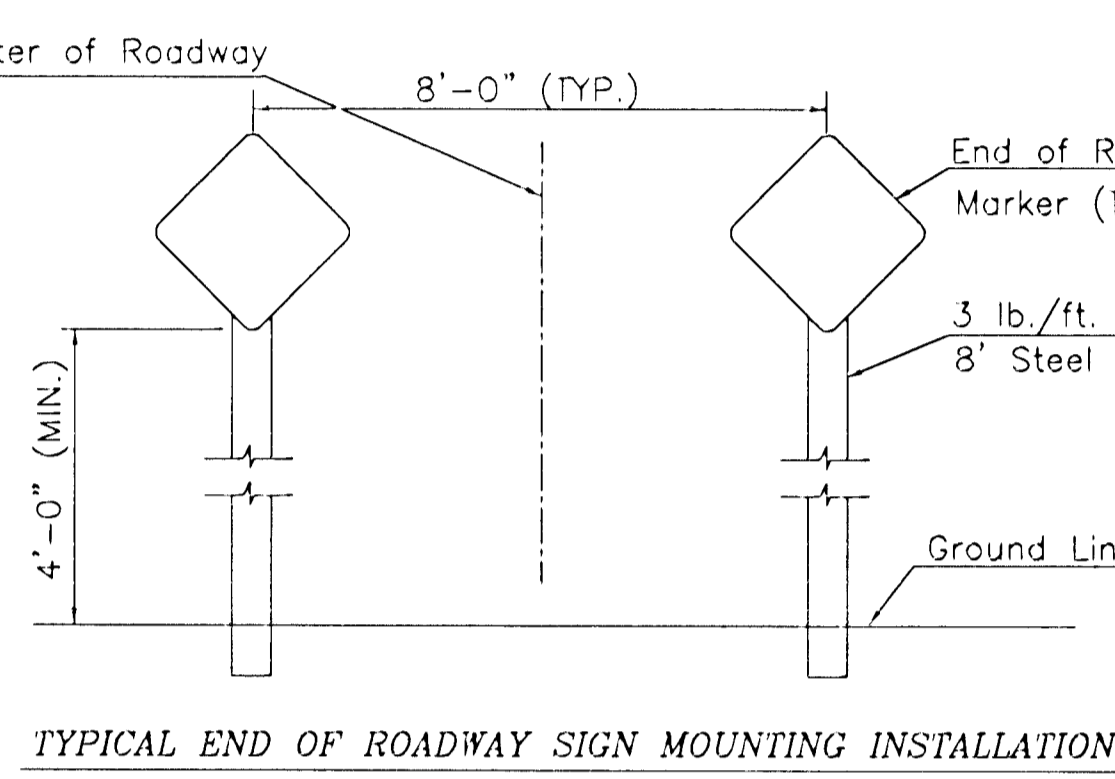
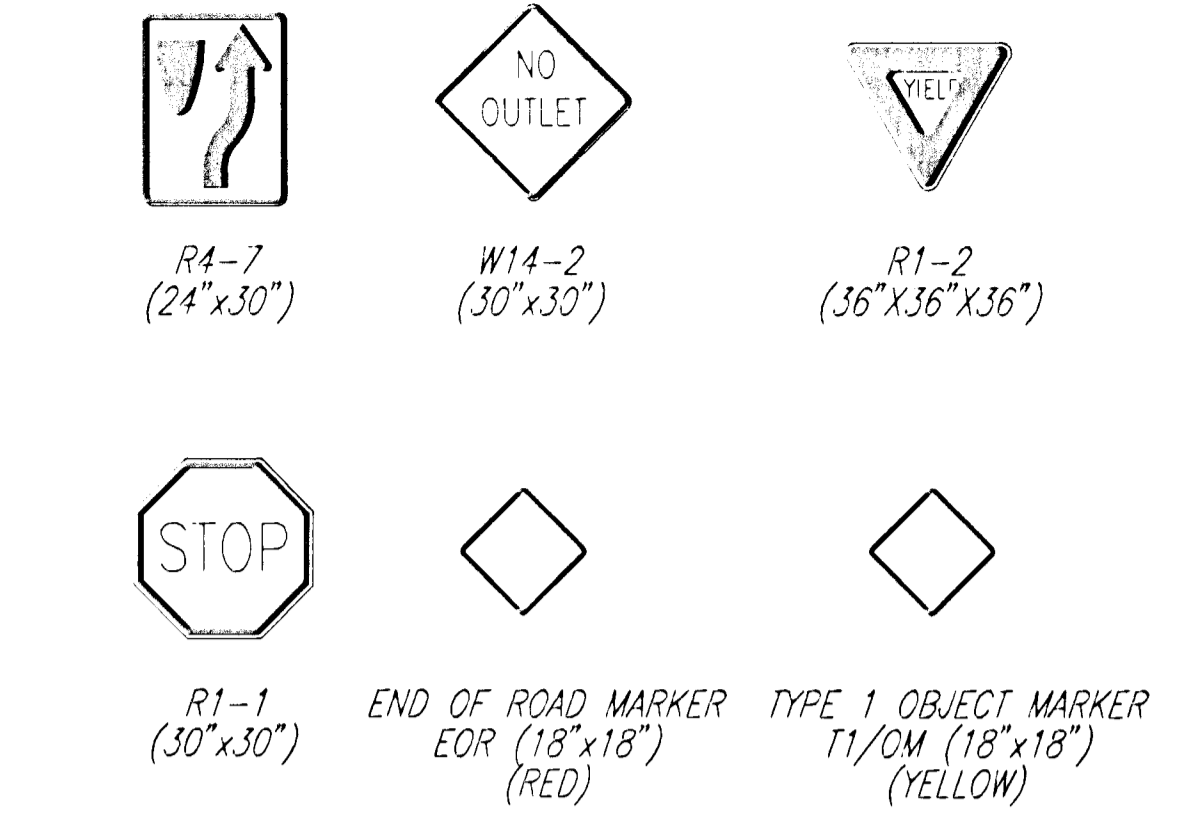
<p>THE CITY OF WICHITA</p> <p>CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 458 NORTH MAIN STREET WICHITA, KANSAS 67202 (316) 268-2501 (316) 268-4114 FAX</p>	MANHOLE FRAME AND COVER	
	M. E. LINDEBAK P.E. - CITY ENGINEER	
	PROJECT NUMBER 472-83175	OCA NO. 765634
	DATE MAR 96	SHEET 17 OF 24

NOTE: REFERENCES BELOW TO "STANDARD SPECIFICATIONS" DENOTE "STANDARD SPECIFICATION FOR STATE ROAD AND BRIDGE CONSTRUCTION EDITION 1990" BY THE KANSAS DEPARTMENT OF TRANSPORTATION.

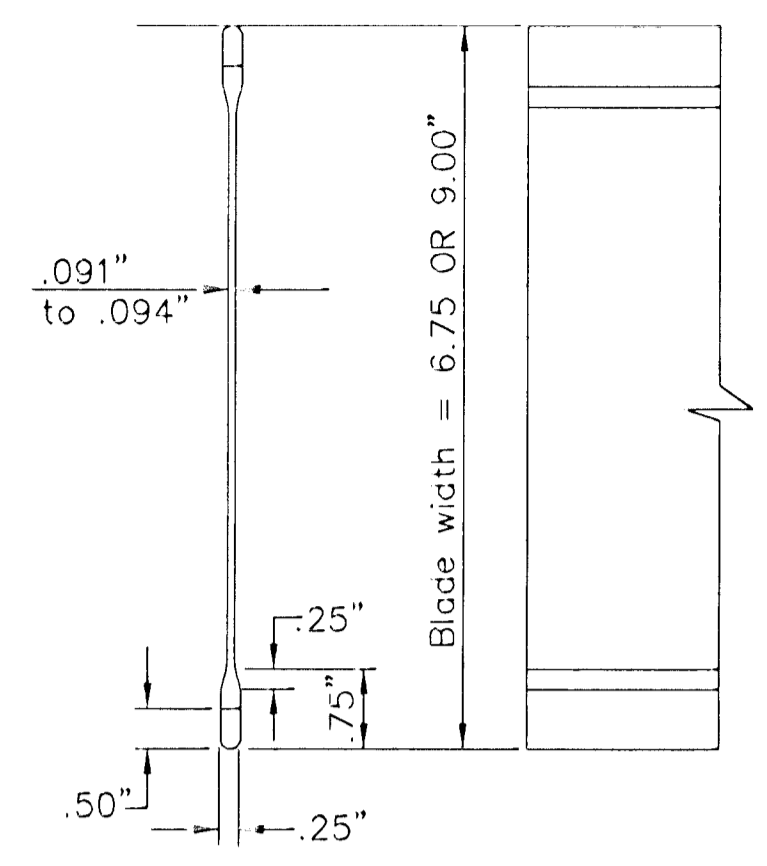
- POST ANCHORS: POSTS SHALL BE ANCHORED WITH A YIELDING BASE POST SUPPORT AS DETAILED.
- POSTS FOR TRAFFIC CONTROL SIGNS: POSTS SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1620 OF THE STANDARD SPECIFICATIONS EXCEPT THAT ALL POSTS SHALL WEIGH 3 LBS./FOOT MINIMUM.
- POSTS FOR STREET NAME SIGNS (SNS): POSTS SHALL BE 9 FEET LONG, CONSTRUCTED FROM 2 3/8" O.D. GALVANIZED STEEL PIPE WEIGHING A MINIMUM OF 3 LBS./FOOT. POSTS SHALL BE POSITIONED SO THAT THE BOTTOM BLADE IS 7 FEET ABOVE GRADE.
- POSTS FOR END OF ROADWAY SIGN TO BE 8' LONG AND INSTALLED A MINIMUM OF 4' FROM ROADWAY TO BOTTOM OF SIGN.
- SIGN BLANKS FOR TRAFFIC CONTROL SIGNS: SIGN BLANKS SHALL BE FABRICATED FROM 0.080" ALUMINUM ALLOY 6063-T6 CONFORMING TO THE REQUIREMENTS OF SUBSECTION 1626 OF THE STANDARD SPECIFICATIONS.
- SIGN BLADES FOR STREET NAME SIGNS: EXTRUDED ALUMINUM BLADES SHALL BE ALUMINUM ALLOY CONFORMING TO 6063-T6 OR 5052-H38 (ASTM SPECIFICATION B221, LATEST ISSUE). BLADES SHALL HAVE AN ALODINE OR PHOSPHATE ETCHED FINISH. BLADES SHALL HAVE SQUARE CORNERS AND NO HOLES.
MINIMUM BLADE LENGTH SHALL BE 24". MAXIMUM BLADE LENGTH SHALL BE 48". LENGTH VARIES BY INCREMENTS OF 6".
BLADES BEARING THE STREET NAMES SHALL BE FIRMLY ATTACHED TO THE MOUNTING BRACKETS USING ALLEN-TYPE SET SCREWS. THE BLADES SHALL BE ORIENTED PARALLEL TO THE STREET.
- MOUNTING BRACKETS FOR SIGNS: DIE-CAST ALUMINUM BRACKETS SHALL BE ALUMINUM ALLOY 360 HAVING A TENSILE STRENGTH OF 44,000 PSI. THE BRACKETS SHALL BE SMOOTHLY FINISHED FREE OF PITS, BURRS, AND FLAWS. EACH BRACKET SHALL BE TAPPED AND DRILLED FOR 5/16" ZINC-PLATED ALLEN-TYPE SET SCREWS HAVING SELF-LOCKING SAW-TOOTH ENDS.
- FASTENERS: ALL STEEL FASTENERS FOR TRAFFIC CONTROL SIGNS SHALL BE GALVANIZED AND SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1614 OF THE STANDARD SPECIFICATIONS.
- REFLECTIVE SHEETING: REFLECTIVE SHEETING SHALL BE TYPE II - HIGH PERFORMANCE CLASS HA IN ACCORDANCE WITH SUBSECTION 2201 OF THE STANDARD SPECIFICATIONS.
- PROCESS INK: ALL PROCESS INK SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 2202 OF THE STANDARD SPECIFICATIONS.
- DETAILS: REGULATORY AND WARNING SIGNS SHALL CONFORM TO THE DETAILS IN "STANDARD HIGHWAY SIGNS", FHWA, 1979.
- DETAILS - SNS: THE REFLECTIVE SHEETING FOR THE 6 3/4" STANDARD SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVER-WHITE #2 COPY WITH 4" UPPER CASE AND LOWER CASE PRIMARY COPY AND SUFFIX COPY. BOTH SERIES "C". FACES TO TRIM TO A 6 1/4" WIDTH. (SEE DETAIL A.)
THE REFLECTIVE SHEETING FOR THE 9" METRO SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVER-WHITE #2 COPY WITH 5" UPPER CASE AND LOWER CASE PRIMARY COPY AND SUFFIX COPY, BOTH SERIES "C". THE CARDINAL DIRECTION CENTERED DIRECTLY BELOW THE BLOCK NUMBER SHALL BE AN UPPER CASE, 3" SERIES "C" LETTER. FACES TO TRIM TO A 8 1/2" WIDTH. (SEE DETAIL B.)
FOR CUL-DE-SAC STREETS, A 9" METRO SIZE BLADE SHALL BE USED WITH THE HOUSE NUMBERS DISPLAYED BENEATH THE STREET NAME. LETTERING TO BE THE SAME AS FOR THE 6 3/4" SIZE BLADE, EXCEPT THAT THE HOUSE NUMBER INFORMATION SHALL BE 4" SERIES "C".
SHOP DRAWINGS OF LAYOUT FOR SNS SHALL BE SUBMITTED TO THE TRAFFIC ENGINEERING DIVISION OF THE CITY OF WICHITA FOR APPROVAL PRIOR TO FABRICATION. THE FINISHED SIGNS AS SUPPLIED SHALL BE OF GOOD APPEARANCE, FREE FROM RAGGED EDGES, CRACKS, SCALES OR BLISTERS AND SHALL BE CLEAN-CUT. SIGNS SHALL BE PACKED IN SUCH MANNER AS TO PREVENT DAMAGE OR DEFACTION DURING SHIPMENT OR STORAGE.
- PERMANENT TRAFFIC CONTROL AND SNS: PERMANENT TRAFFIC CONTROL AND SNS SHALL BE MEASURED AND PAID FOR AT THE LUMP SUM PRICE FOR SIGNING. THE PAYMENT AS SET FORTH ABOVE SHALL BE CONSIDERED FULL COMPENSATION FOR ALL EXCAVATION, BACKFILLING, POSTS, ANCHORS, FASTENERS, MATERIALS, LABOR, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK.



TYPICAL TRAFFIC CONTROL SIGN MOUNTING INSTALLATION
CURB AND GUTTER SECTION



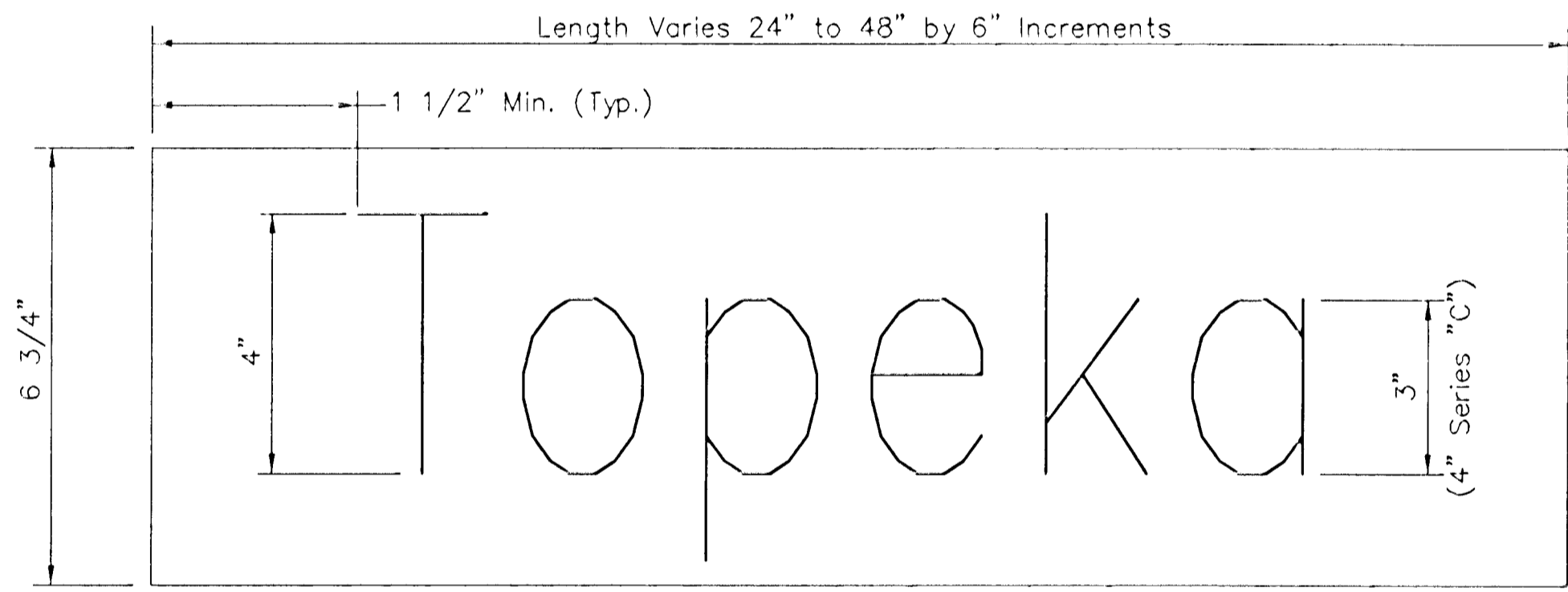
TYPICAL END OF ROADWAY SIGN MOUNTING INSTALLATION



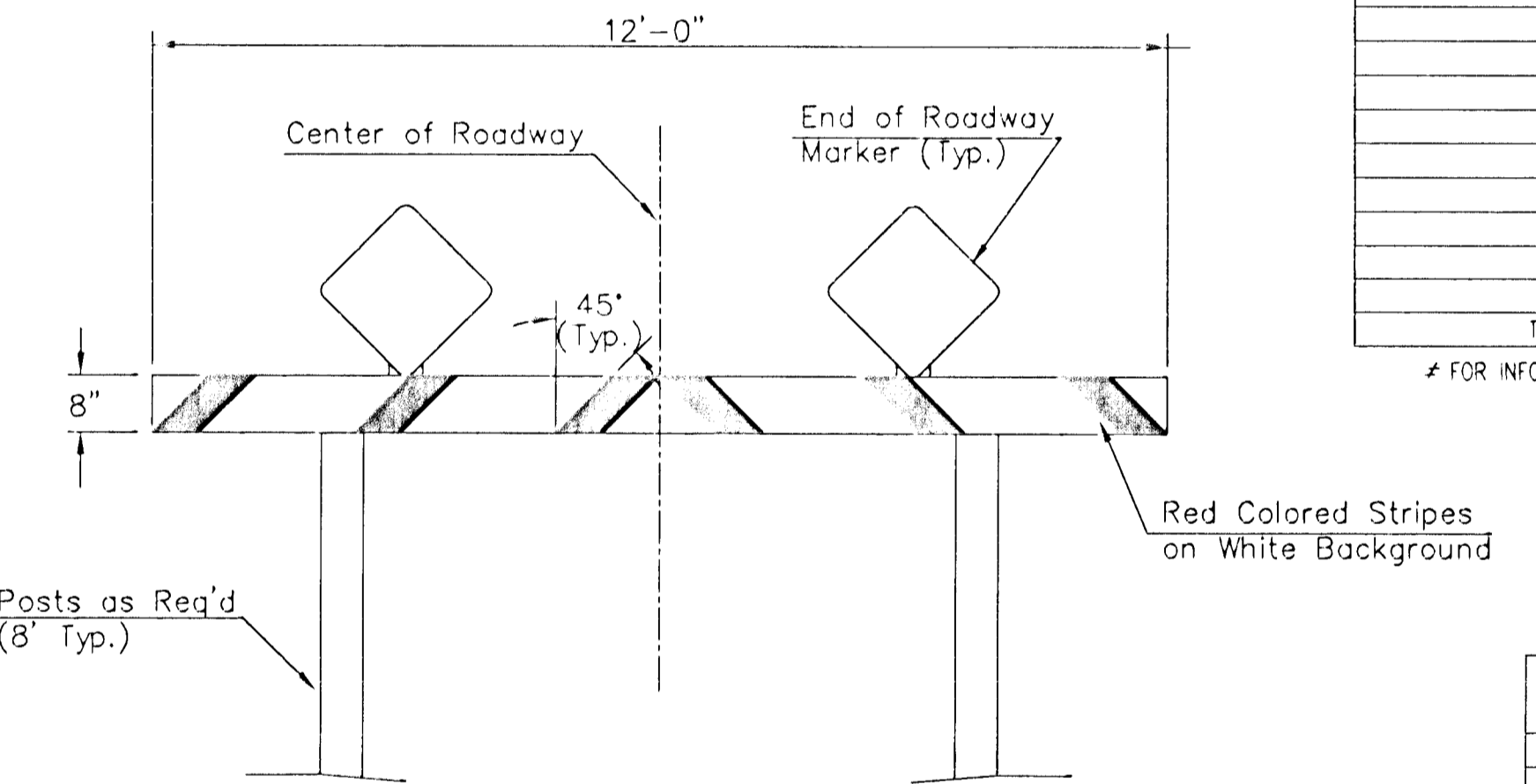
STREET NAME SIGN
BLADE DETAILS

SIGN ASSEMBLY TABLE				
STREET	STATION	OFFSET	SIGN	QUANTITY #
WILSON ESTATES PARKWAY	35+52	20.5' RT.	W11-2	1
WILSON ESTATES PARKWAY	36+52	20.5' RT.	W11A-2	1
WILSON ESTATES PARKWAY	36+68	20.5' LT.	W11A-2	1
WILSON ESTATES PARKWAY	37+68	20.5' LT.	W11-2	1
WILSON ESTATES PARKWAY	46+15	6.5' RT.	R4-7 & T1/CM	1
WILSON ESTATES PARKWAY	46+37	49' RT.	R1-1	1
WILSON ESTATES PARKWAY	47+23	13.88' RT.	R4-7 & T1/CM	1
WILSON ESTATES PARKWAY	47+29	21.5' LT.	STREET NAME	1
TOTAL				8

FOR INFORMATION ONLY



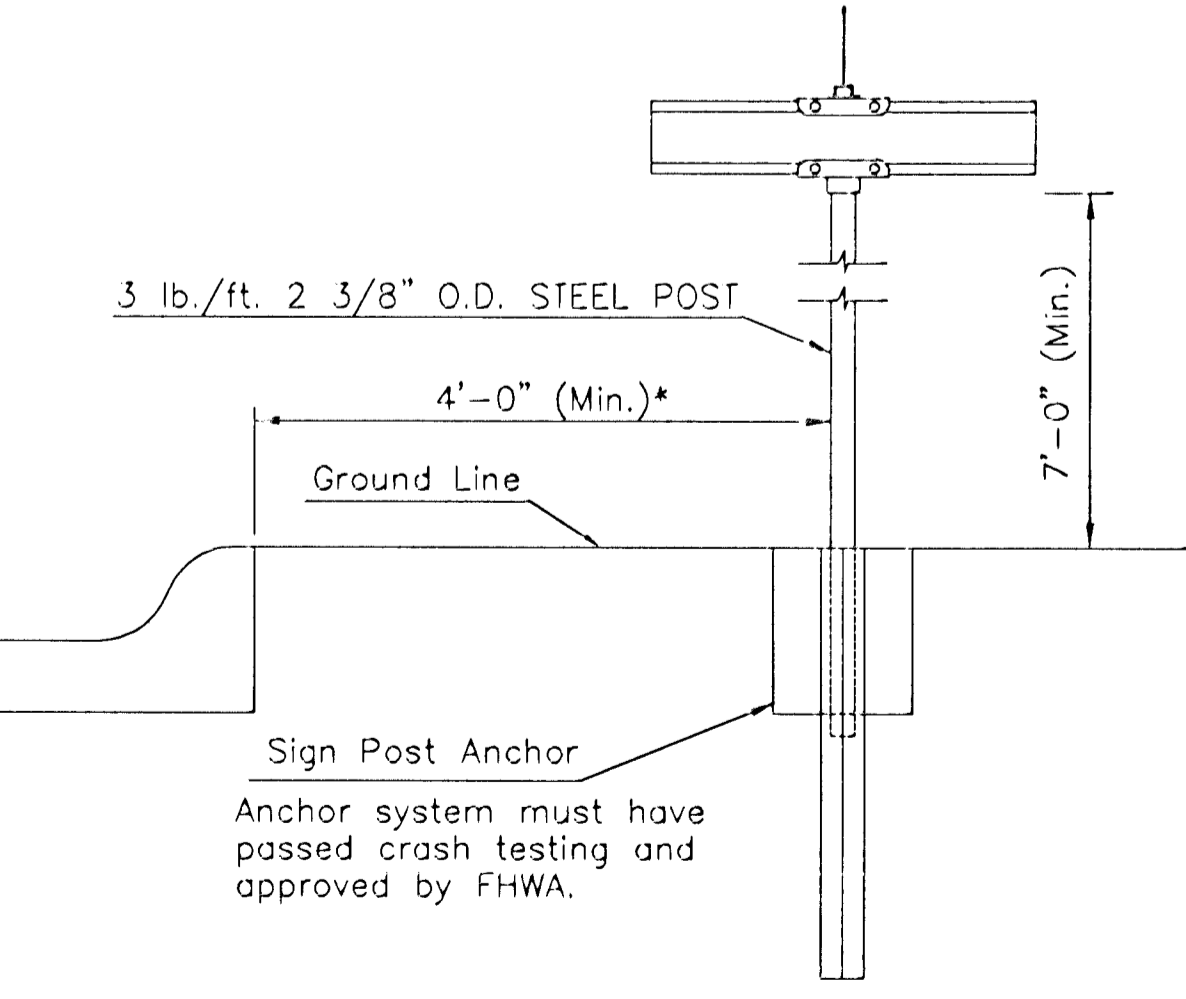
DETAIL A
6 3/4" STANDARD



DETAIL B
9" METRO

TYPE I BARRICADE DETAIL W/ E.O.R. MARKER

STREET NAME	NO. BLADES REQ'D.	
	6 3/4" STD.	9" METRO
Webb Rd (1900 N)	1	1
Wilson Estates Pkwy (3500 E)	1	1



TYPICAL STREET NAME SIGN MOUNTING INSTALLATION
CURB AND GUTTER SECTION

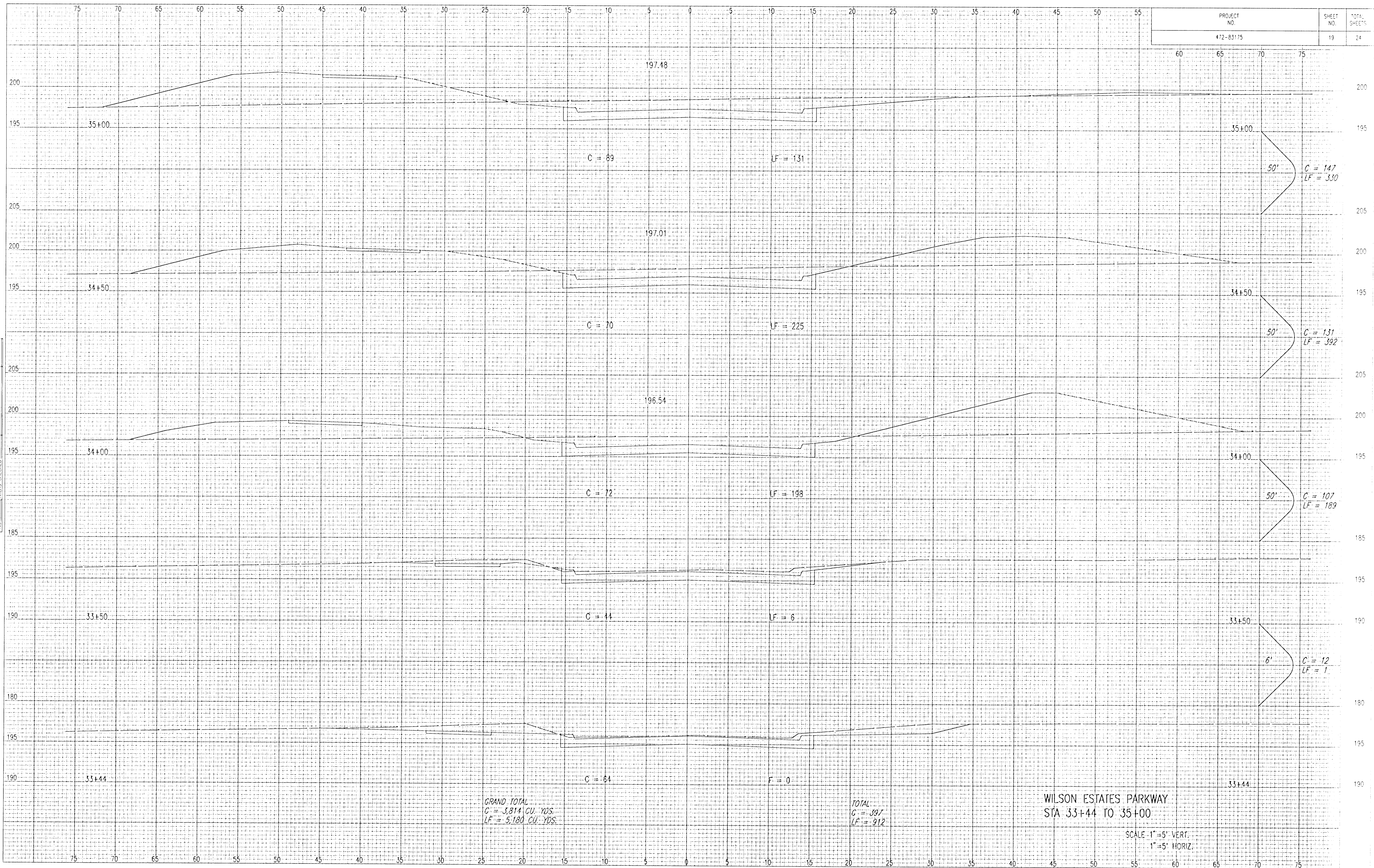
DSNR: BEB OPER. SAD SCALE: 1=1.00
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SIGNING DETAILS

SCALE: NONE	APPROVED BY	DATE: JUNE '93
DRAWN BY: TM		REVISED: JUNE '97

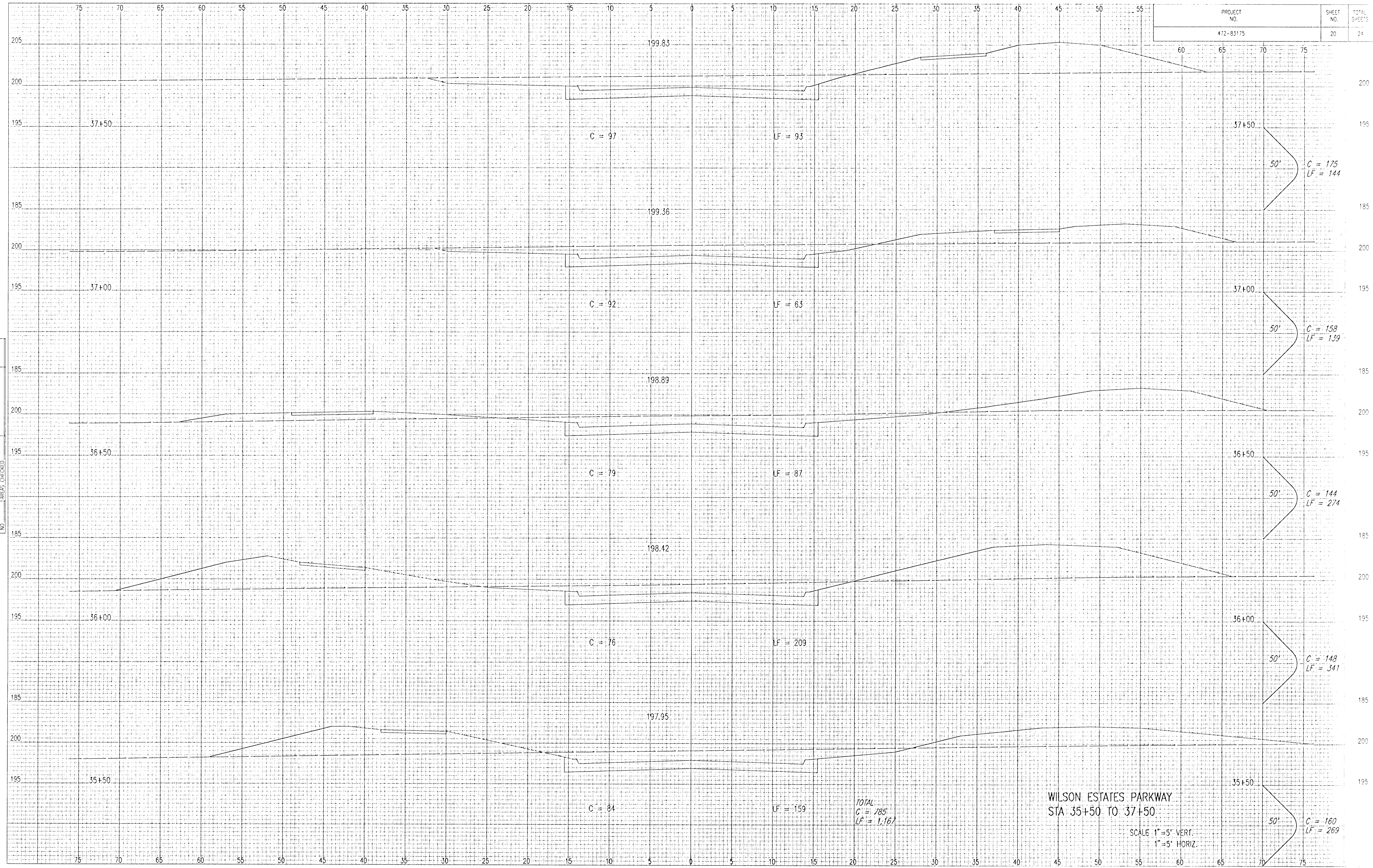
CITY OF WICHITA
DEPARTMENT OF PUBLIC WORKS
TRAFFIC ENGINEERING SECTION

NOTE BOOK TEMPLATE
AREAS CHECKED
NO



WILSON ESTATES PARKWAY
 STA 33+44 TO 35+00

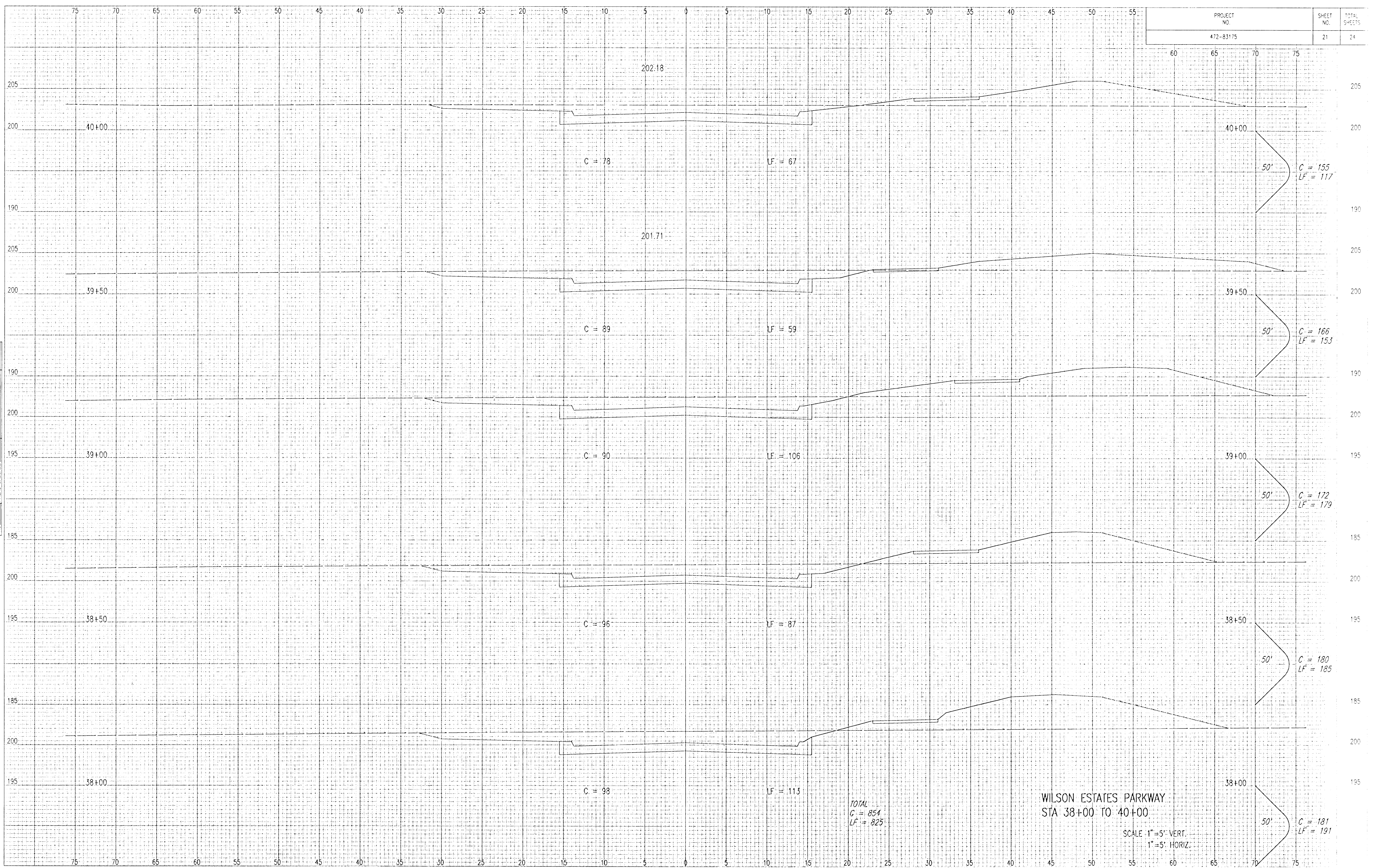
SCALE 1"=5' VERT.
 1"=5' HORIZ.



NOTE BOOK TEMPLATE
AREAS
AREAS CHECKED
NO

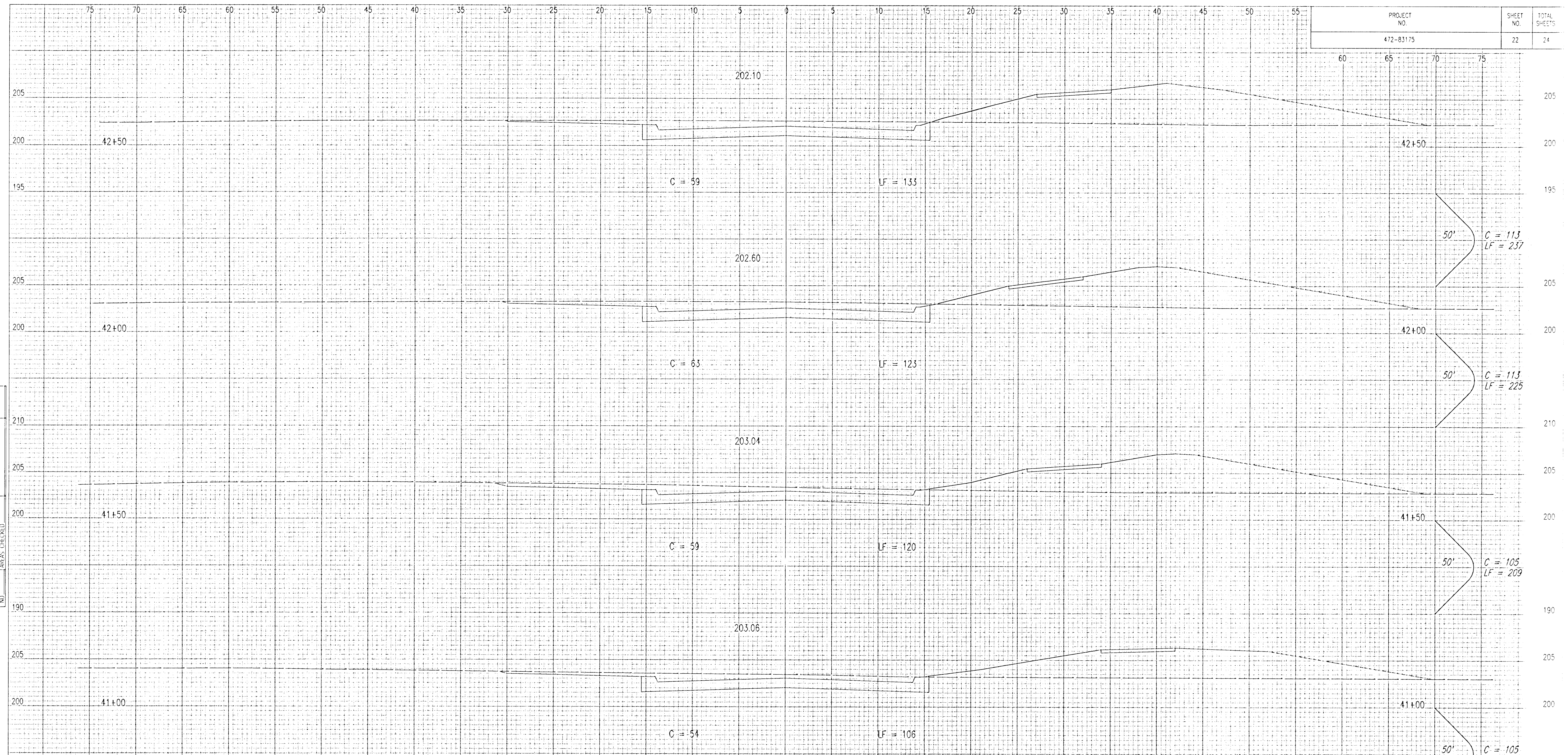
WILSON ESTATES PARKWAY
STA 35+50 TO 37+50

SCALE 1"=5' VERT.
1"=5' HORIZ.



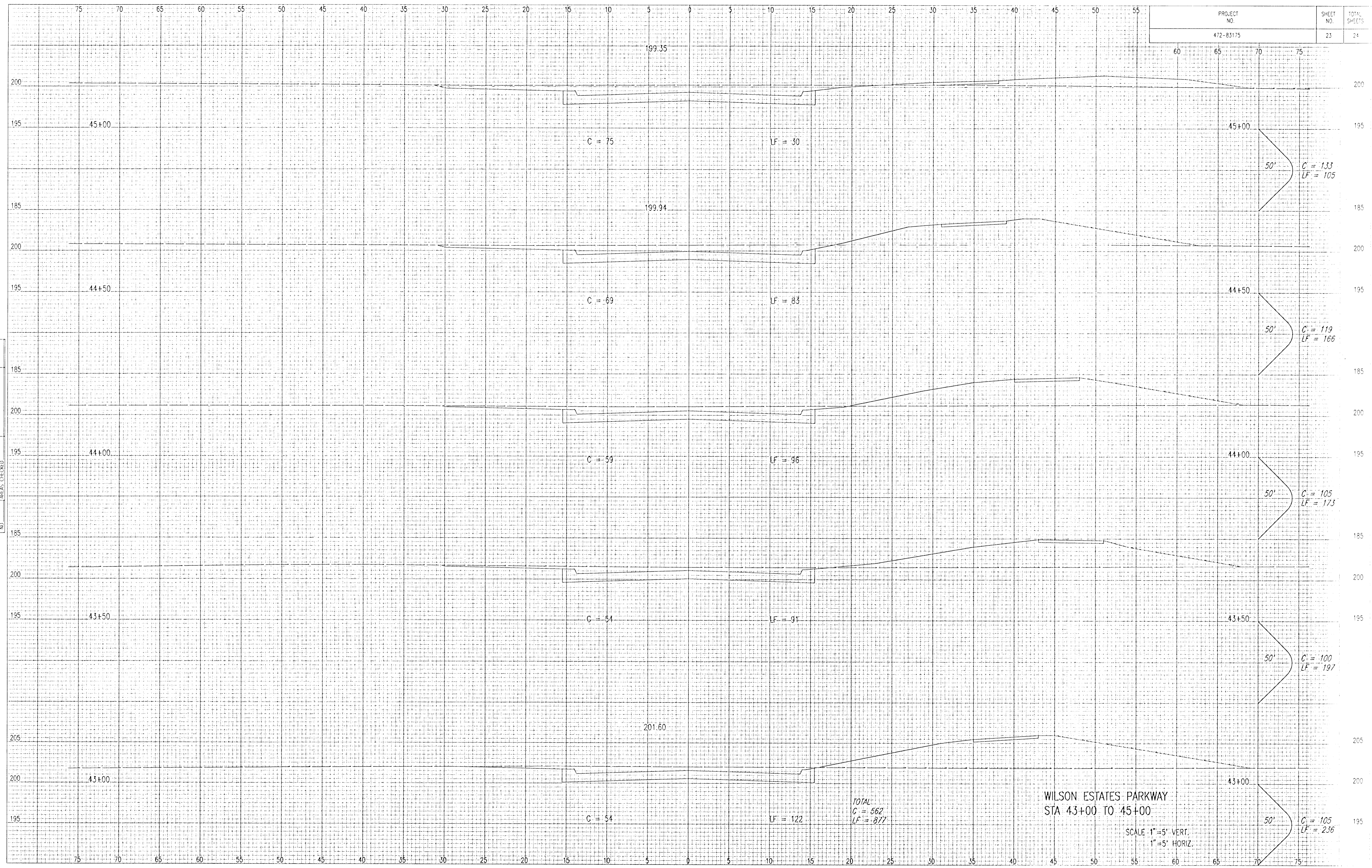
NO. 1	NO. 2	NO. 3	NO. 4
NOTE BOOK	TEMPLATE	AREAS	AREAS CHECKED

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-83175	22	24



NOTE BOOK NO.	TEMPLATE NO.	AREAS CHECKED

PROJECT NO. 472-83175	SHEET NO. 23	TOTAL SHEETS 24
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NOTE BOOK TEMPLATE AREAS CHECKED NO

TOTAL
C = 562
LF = 877

WILSON ESTATES PARKWAY
STA 43+00 TO 45+00

SCALE: 1"=5' VERT.
1"=5' HORIZ.