

FHWA REGION NO.	STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
7	KANSAS	472-84071	2005	S3.6	

CP 2501 POT Sta. 46+13.96 Found 3/4" Iron Pin in Thimble at Point N 1688519.6426 E 1650589.9145
 1. @ Central
 2. East R/W of St. Francis
 3. No other Ties

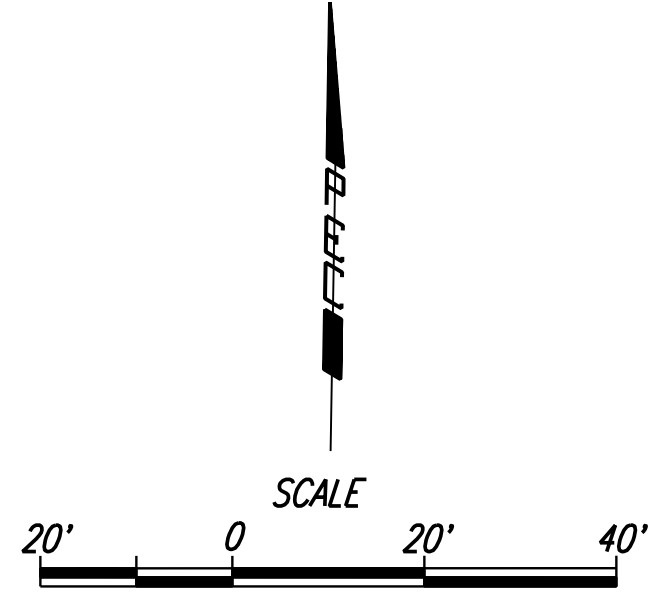
CP 2500 PI Sta. 49+14.44 Deflection Angle = 0°01'10" Lt. Found 3/4" Iron Pin in Thimble at Point N 1688525.2636 E 1650890.3729
 1. @ Central
 2. West R/W of Santa Fe
 3. No other Ties

CP 106 POT Sta. 50+00.00 = @ New Mainline 2 Sta. 11182+92.01 N 1688526.8930 E 1650975.9139
 1. No Monument Set

CP 2912 Found Chiseled "+" at Point N 1688578.9782 E 1651336.3598
 1. North R/W of Central
 2. West R/W of Mead
 3. No other Ties

CP 2502 Found 3/4" Iron Pin in Thimble at Point N 1688547.7085 E 1652068.7200
 1. N 1/4 Corner Sec. 21 T27S R1E
 2. @ Central
 3. @ Washington
 4. No other Ties

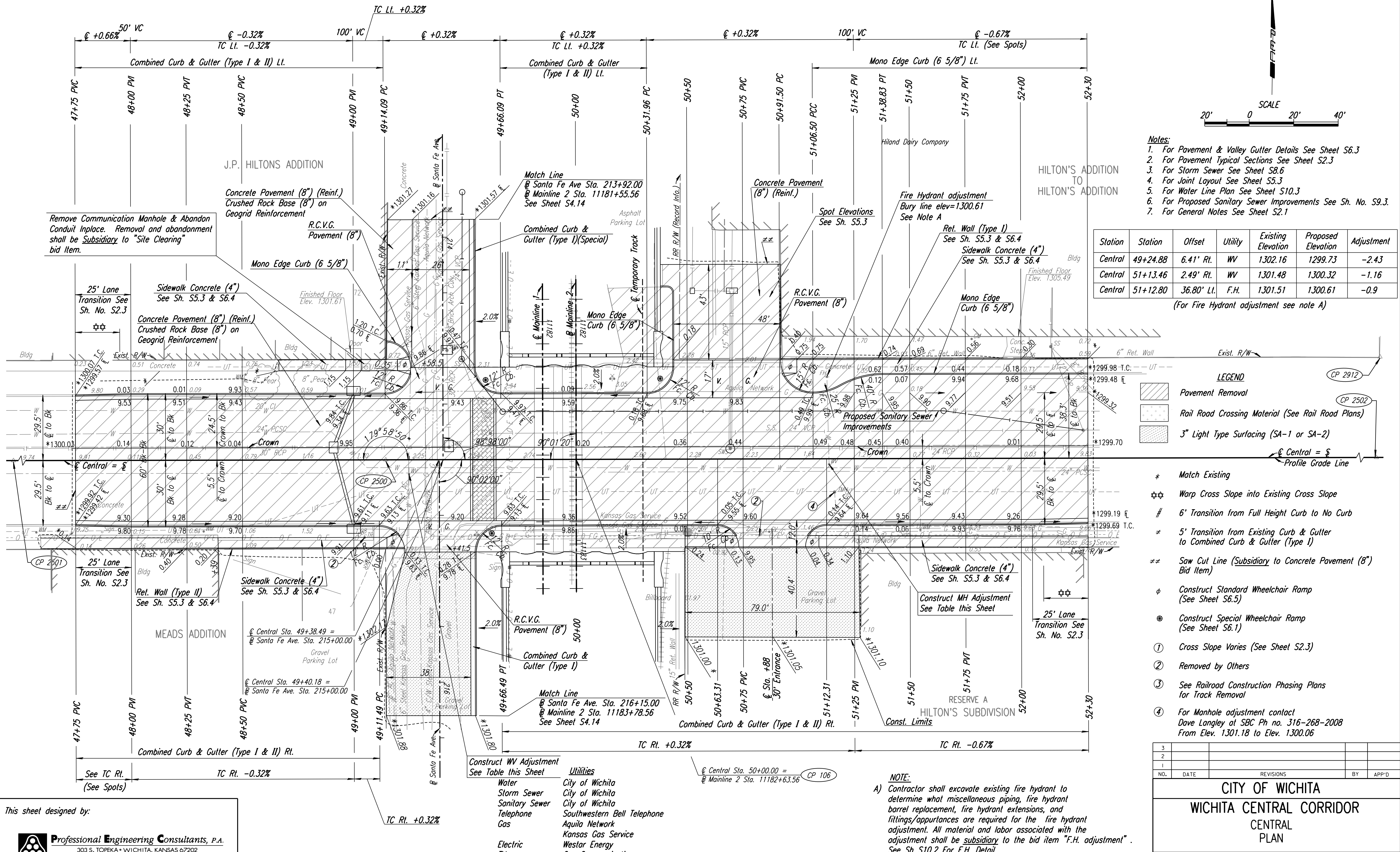
DATE	BY



- Notes:**
1. For Pavement & Valley Gutter Details See Sheet S6.3
 2. For Pavement Typical Sections See Sheet S2.3
 3. For Storm Sewer See Sheet S8.6
 4. For Joint Layout See Sheet S5.3
 5. For Water Line Plan See Sheet S10.3
 6. For Proposed Sanitary Sewer Improvements See Sh. No. S9.3
 7. For General Notes See Sheet S2.1

Station	Station	Offset	Utility	Existing Elevation	Proposed Elevation	Adjustment
Central	49+24.88	6.41' Rt.	WV	1302.16	1299.73	-2.43
Central	51+13.46	2.49' Rt.	WV	1301.48	1300.32	-1.16
Central	51+12.80	36.80' Lt.	F.H.	1301.51	1300.61	-0.9

(For Fire Hydrant adjustment see note A)



LEGEND

- Pavement Removal
- Rail Road Crossing Material (See Rail Road Plans)
- 3" Light Type Surfacing (SA-1 or SA-2)

Central = \$ Profile Grade Line

- * Match Existing
- ⊗ Warp Cross Slope into Existing Cross Slope
- # 6' Transition from Full Height Curb to No Curb
- ≠ 5' Transition from Existing Curb & Gutter to Combined Curb & Gutter (Type I)
- ≠≠ Saw Cut Line (Subsidiary to Concrete Pavement (8") Bid Item)
- ⊕ Construct Standard Wheelchair Ramp (See Sheet S6.5)
- ⊗ Construct Special Wheelchair Ramp (See Sheet S6.1)
- ① Cross Slope Varies (See Sheet S2.3)
- ② Removed by Others
- ③ See Railroad Construction Phasing Plans for Track Removal
- ④ For Manhole adjustment contact Dave Langley at SBC Ph no. 316-268-2008 From Elev. 1301.18 to Elev. 1300.06

NO.	DATE	REVISIONS	BY	APP'D
3				
2				
1				

CITY OF WICHITA
WICHITA CENTRAL CORRIDOR
CENTRAL PLAN

DESIGNED	DSB	SCALE	1" = 20'	APP'D	
DESIGN CK.	DETAL CK.	QUANTITIES	SVB	QUANTITIES	TRACED
		QUAN./CK.			TRACE CK.

This sheet designed by:

Professional Engineering Consultants, P.A.
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- Utilities**
- Water City of Wichita
 - Storm Sewer City of Wichita
 - Sanitary Sewer City of Wichita
 - Telephone Southwestern Bell Telephone
 - Gas Aquila Network
 - Electric Westar Energy
 - TV Cox Communications

NOTE:

A) Contractor shall excavate existing fire hydrant to determine what miscellaneous piping, fire hydrant barrel replacement, fire hydrant extensions, and fittings/appurtenances are required for the fire hydrant adjustment. All material and labor associated with the adjustment shall be subsidiary to the bid item "F.H. adjustment". See Sh S10.2 For F.H. Detail

BM 7-A - "X" Chiseled on Top of the Southeast Bolt of a Traffic Signal Pole Base, Northeast Corner of St. Francis & Central Elevation=1299.40

BM WC-7 - Top of 5/8" Bar with Wilson & Company Aluminum Cap, Stamped WC-7, Northeast Corner of Central and BNSF Railroad Elevation=1301.90

BM 7-B - Chiseled "□" on the Center of a Storm Drain Curb Inlet, Northeast Corner of Central & Mead Elevation=1298.53

DSNR: RWM OPER: SVB SCALE: 1"=20' 1:/1999/99189/bid set 3/SHEETS/S3.6_PLAN_CENTRAL.DGN LAST REV: 12-10-2004 BY: gdr