

**3RD STREET
WATER MAIN REPLACEMENT
FROM CENTRAL CORRIDOR TO I-135**

TYP. SECTIONS & TRENCH DETAILS

SHEET TITLE
448-90217
PROJECT NUMBER

JRA | JSB | JRA
DESIGNED | DRAWN | CHECKED

ISSUED
June 2012

REVISED

SHEET NO.

TRAFFIC CONTROL AND CONSTRUCTION SEQUENCING NOTES

3RD STREET MAY BE COMPLETELY CLOSED DURING CONSTRUCTION PROVIDED THAT THE CONTRACTOR ADHERES TO THE CONDITIONS OF THE NOTES ON THIS SHEET.

CONTRACTOR SHALL NOTIFY ADJACENT RESIDENCES AND BUSINESSES OF THE CONSTRUCTION ACTIVITIES AND EXPECTED DURATION OF STREET CLOSURE PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL NOT CLOSE MORE THAN TWO ADJACENT SIDE STREETS AT ANY TIME DURING CONSTRUCTION. A TEMPORARY PAVEMENT CONSISTING OF 5" COMMERCIAL GRADE ASPHALT MAY BE USED, AT THE ENGINEER'S DISCRETION, TO RE-OPEN SIDE STREETS AFTER THE WATER LINE HAS BEEN INSTALLED AND PRIOR TO PERMANENT PAVEMENT REPLACEMENT. THE COST OF PLACING AND REMOVING TEMPORARY PAVEMENT WILL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

HYDRAULIC AVENUE SHALL REMAIN OPEN TO TWO-WAY TRAFFIC AT ALL TIME DURING CONSTRUCTION. SEE TRAFFIC CONTROL DETAILS ON SHEET 36.

IF WORK ON PENNSYLVANIA IS NOT COMPLETED DURING SCHOOL BREAKS, IT SHALL BE KEPT OPEN TO ONE-LANE TRAFFIC AT ALL TIME DURING CONSTRUCTION. SEE TRAFFIC CONTROL DETAIL ON SHEET 36. THE CONTRACTOR WILL BE RESPONSIBLE FOR FLAGGING TRAFFIC DURING WORK ON PENNSYLVANIA.

WATER LINE CONSTRUCTION BETWEEN PENNSYLVANIA AND HYDRAULIC IS TO BE COORDINATED WITH USD 259 (JAY DORSETT, 973-2014). ALL WORK, INCLUDING RESTORATION, IN THIS AREA SHALL BE COMPLETE DURING THE SCHOOL SUMMER BREAK. CONTRACTOR WILL CONSTRUCT A TEMPORARY ENTRANCE OFF OF INDIANA AVENUE FOR UNITED RENTALS. THE TEMPORARY ENTRANCE WILL INCLUDE CURB REMOVAL AND EVENTUAL REPLACEMENT, 32 S.Y. OF 5" COMMERCIAL-GRADE ASPHALT DRIVE AND A TEMPORARY GATE AND FENCE RESTORATION. COST SHALL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

CONTRACTOR SHALL MAINTAIN DRIVE-THROUGH ACCESS AT JET BARBEQUE THROUGHOUT CONSTRUCTION. THE DRIVEWAY SHALL BE CONSTRUCTED HALF-AT-A-TIME. A TEMPORARY PAVEMENT CONSISTING OF 5" COMMERCIAL GRADE ASPHALT SHALL BE USED TO PROVIDE ACCESS FROM THE BUSINESS TO THE INTERSECTION OF 3RD AND WABASH. COST SHALL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

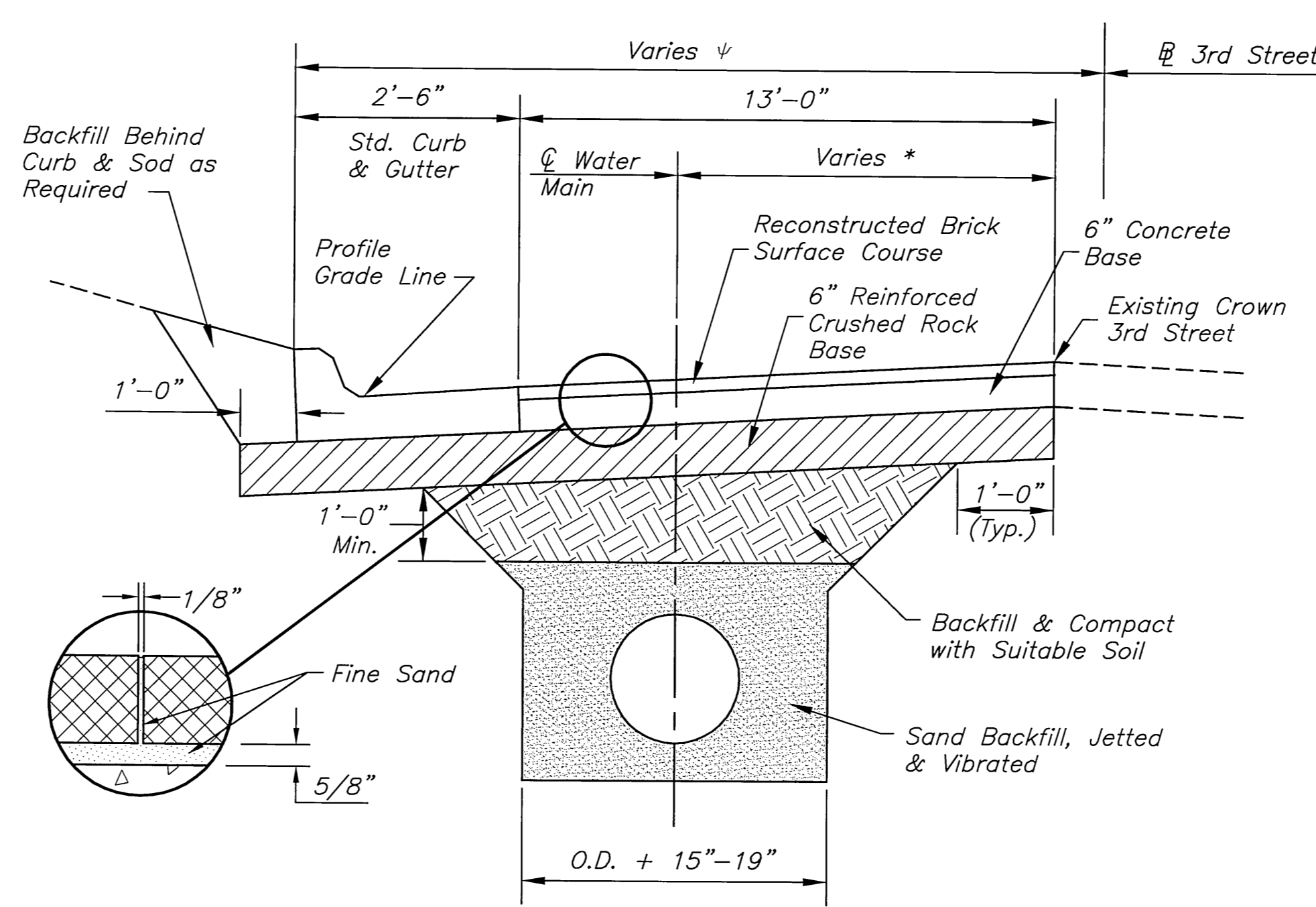
THE PAVEMENT MARKING ON HYDRAULIC SHALL BE REPLACED AFTER THE SURFACE COURSE IS CONSTRUCTED. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC, UNLESS OTHERWISE APPROVED BY THE ENGINEER. PAVEMENT MARKINGS SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND SHALL MEET KDOT'S 2007 EDITION OF THE STANDARD SPECIFICATION SECTION 806. FULL TRAFFIC MAY NOT BE RESTORED (AND SUBSTANTIAL PROJECT COMPLETION ACHIEVED) UNTIL ALL PAVEMENT MARKINGS ARE IN PLACE. SHOULD CONSTRUCTION TIMING BE SUCH THAT RESTORATION OF TRAFFIC BECOMES NECESSARY DURING TEMPERATURES PROHIBITING THE INSTALLATION OF THERMOPLASTIC MARKINGS, THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY MARKINGS UNTIL SUCH TIME THAT THERMOPLASTIC MARKINGS MAY BE PROPERLY INSTALLED. EXCEPT FOR THE MATERIAL REQUIREMENT, TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED EQUIVALENT, IN EVERY MANNER (I.E. DIMENSION, FREQUENCY, SPACING, ETC.), TO THE PERMANENT MARKING LAYOUT. THE COST FOR TEMPORARY PAVEMENT MARKINGS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM FOR "PAVEMENT MARKINGS."

CONTRACTOR IS RESPONSIBLE FOR REMOVING AND REPLACING ALL OBJECTS CONFLICTING WITH CONSTRUCTION WHEN WORKING ON SCHOOL DISTRICT PROPERTY, INCLUDING FENCES, PLAYGROUND EQUIPMENT, SIDEWALKS, ETC. ANY ITEM DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO COST TO OWNER.

CONTRACTOR TO COORDINATE DRIVEWAY CLOSURES WITH RESIDENTS AND BUSINESSES.

BRICK SURFACE COURSE NOTES:

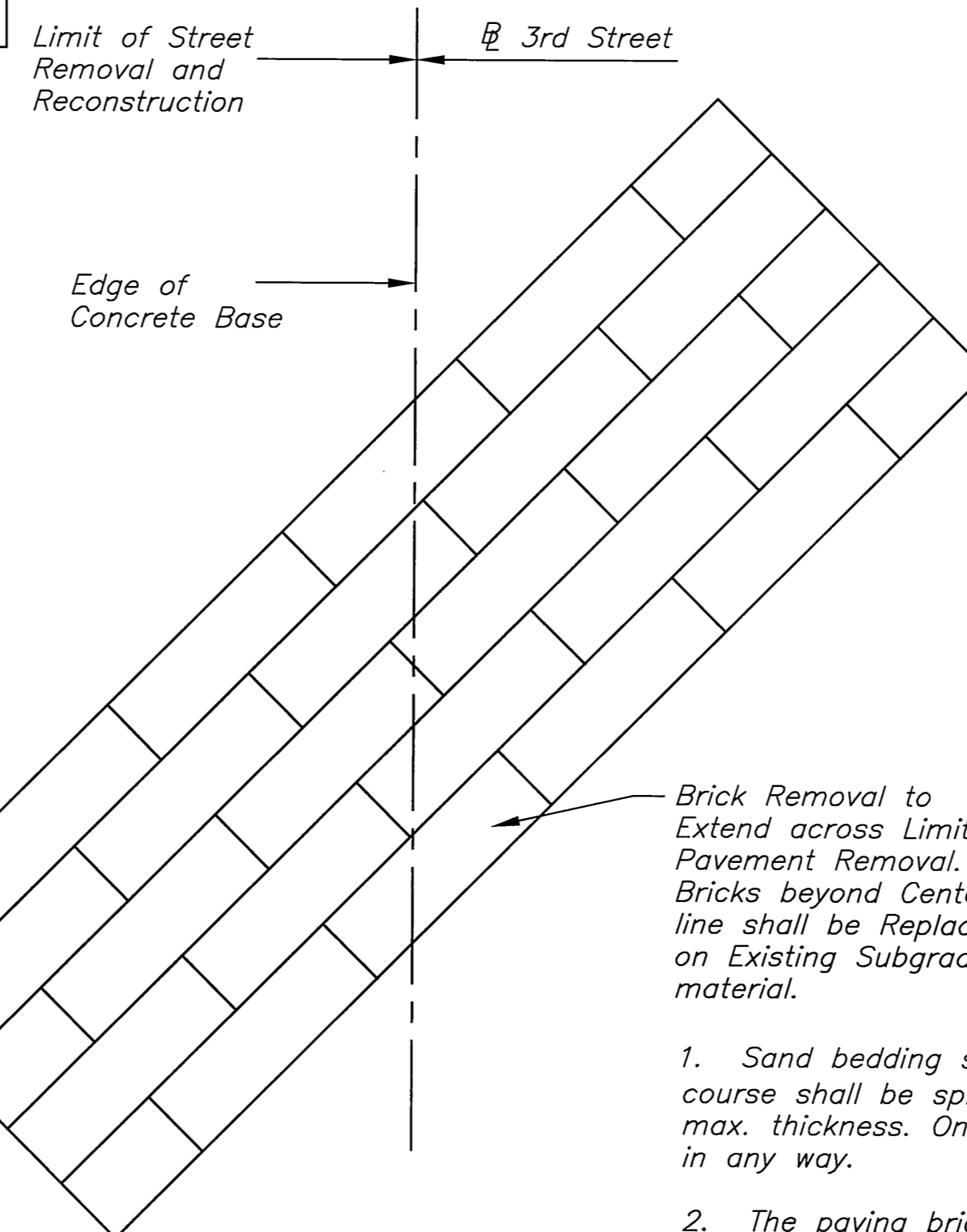
1. Sand bedding shall meet the specification requirements for fine aggregate. An uncompacted sand laying course shall be spread evenly over the area to be paved and then screeded to a level of approximately 3/4" max. thickness. Once screeded and leveled to the desired elevation, the sand laying course shall not be disturbed in any way.
2. The paving brick shall be installed in a 45° Angled Soldier-Course pattern. Joint spaces shall be kept uniform approximately 1/8 inch thick. The gaps at the edge of the paved surface shall be filled with stones cut to fit. Cutting shall be accomplished to leave a clean edge toward the traffic surface, using a masonry saw. Whenever possible, no cuts should result with a paver less than one-third of its original dimension.
3. Paving brick shall be vibrated to their final level in the sand laying course by two or three passes of a vibrating compactor capable of 3000 to 5000 pounds compaction force with the surface clean and joints open.
4. After vibration, clean concrete sand shall be spread over the paving stone surface, allowed to dry, and vibrated into the joints with additional passes of the plate vibrator so as to completely fill the joints. A light coating of sand shall be swept over the completed surface and left to weather in.



TYPICAL TRENCH AND STREET REPAIR SECTION

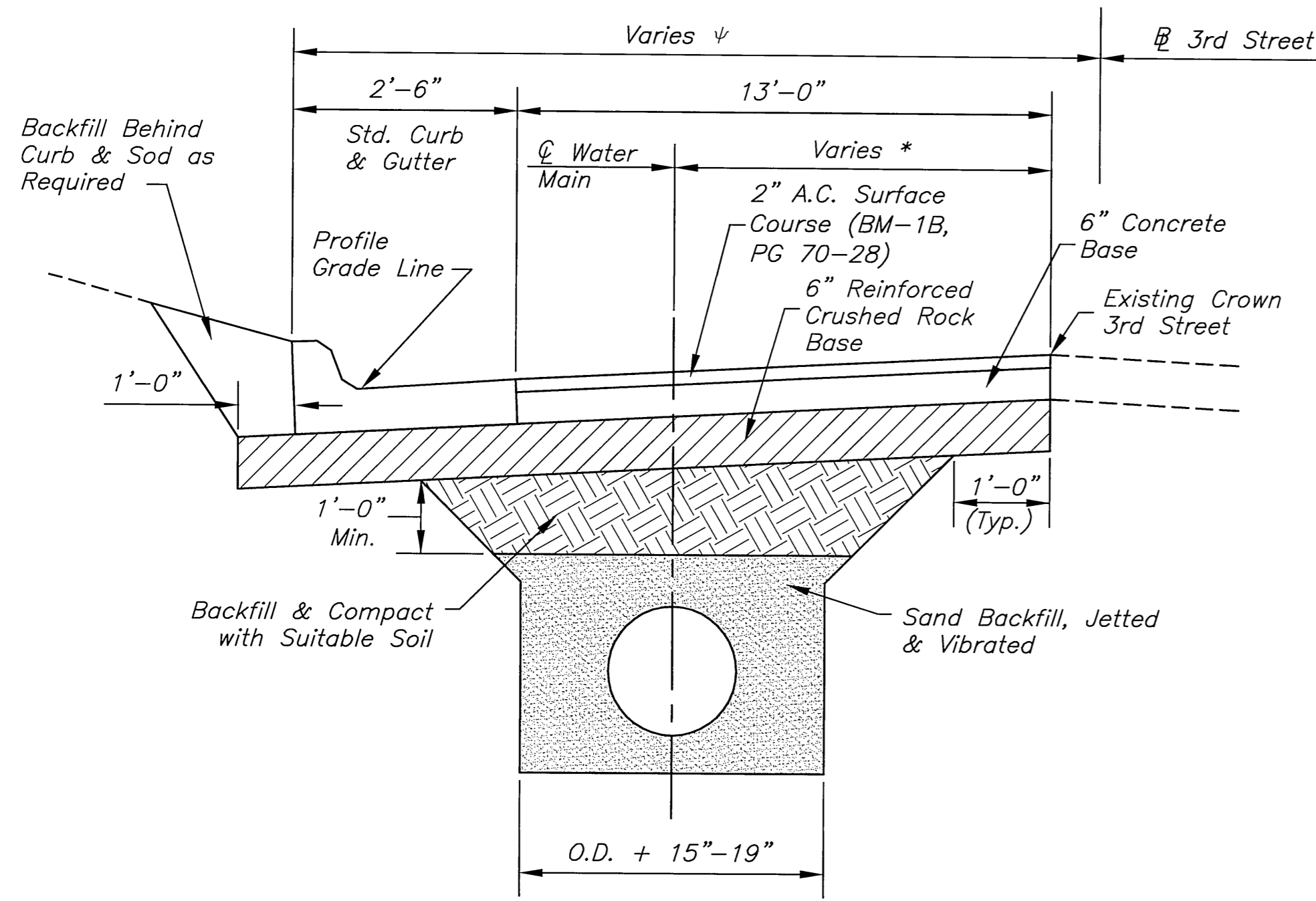
Sta. 10+48.74 to Sta. 12+72.94
Sta. 13+71.02 to Sta. 19+62.42
Sta. 20+31.68 to Sta. 30+06.01
Sta. 34+17.20 to Sta. 38+43.43

NOTE:
ANY EARTHWORK ASSOCIATED WITH STREET REPAIR OR REPLACEMENT SHALL BE CONSIDERED SUBSIDIARY TO PAVING ITEMS.



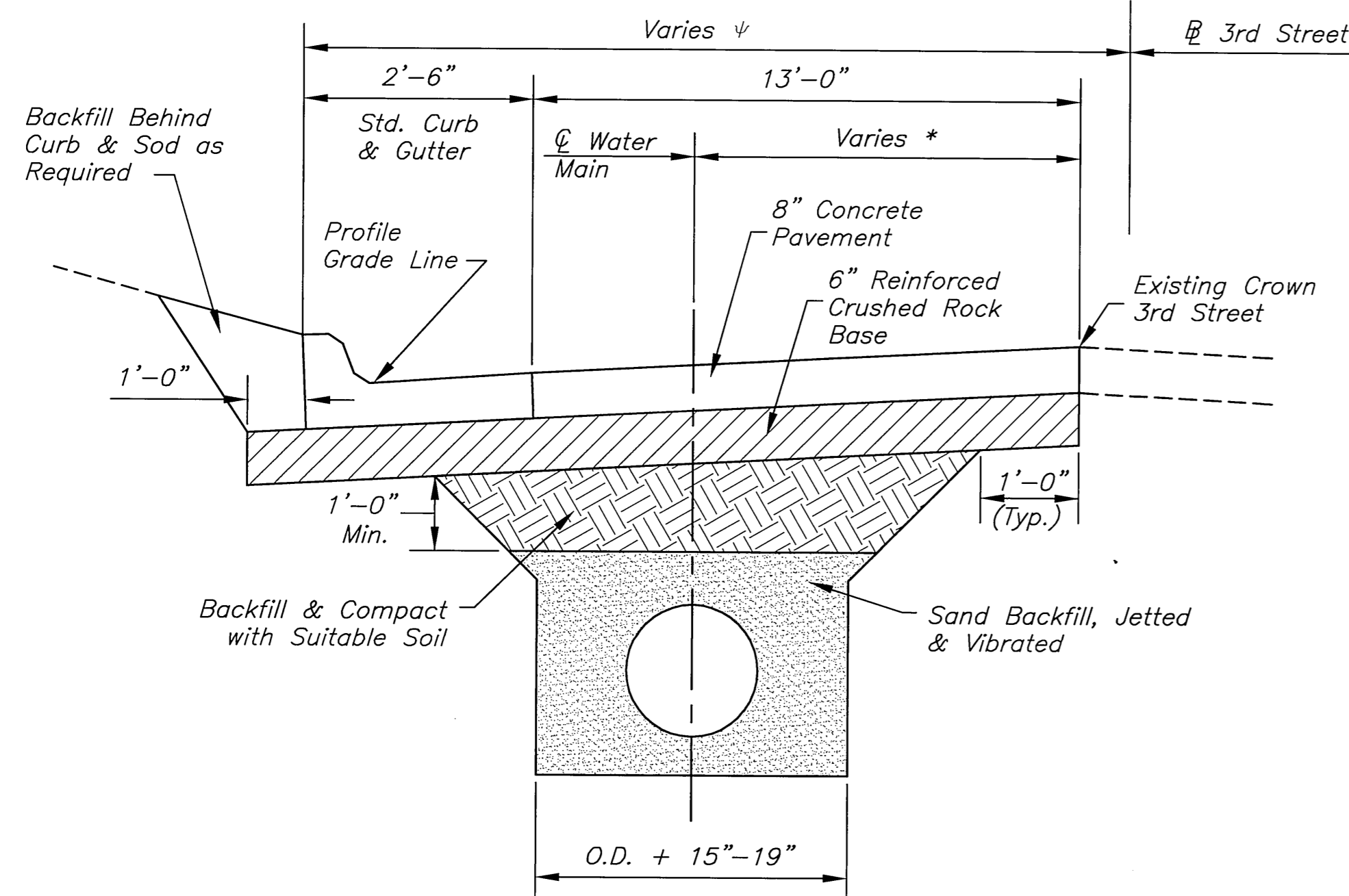
NOTE:
Concrete Base Course shall be constructed slightly rounded near the Crown to avoid an uneven resting surface for Bricks crossing the Center line.

9'-0" From Sta. 10+48.74 to Sta. 30+06.01
ψ 19'-11" From Sta. 33+01.69 to Sta. 38+81.48
* 9'-0" From Sta. 10+48.74 to Sta. 30+06.01
6'-0" From Sta. 33+01.69 to Sta. 38+81.48



TYPICAL TRENCH AND STREET REPAIR SECTION

STA. 19+62.42 to STA. 20+31.68
STA. 33+01.69 to STA. 34+17.20



TYPICAL TRENCH AND STREET REPAIR SECTION

STA. 12+72.94 to STA. 13+71.02
STA. 38+43.43 to STA. 38+81.48