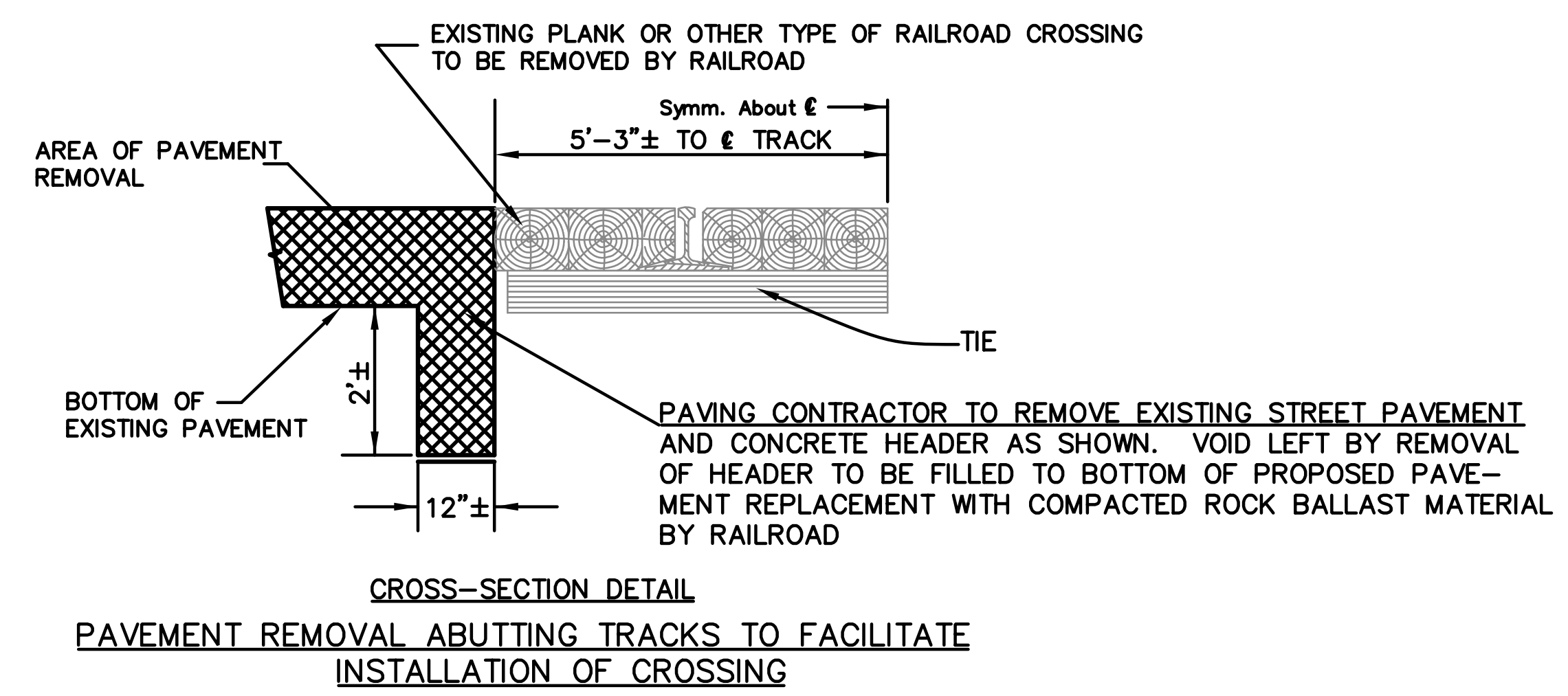


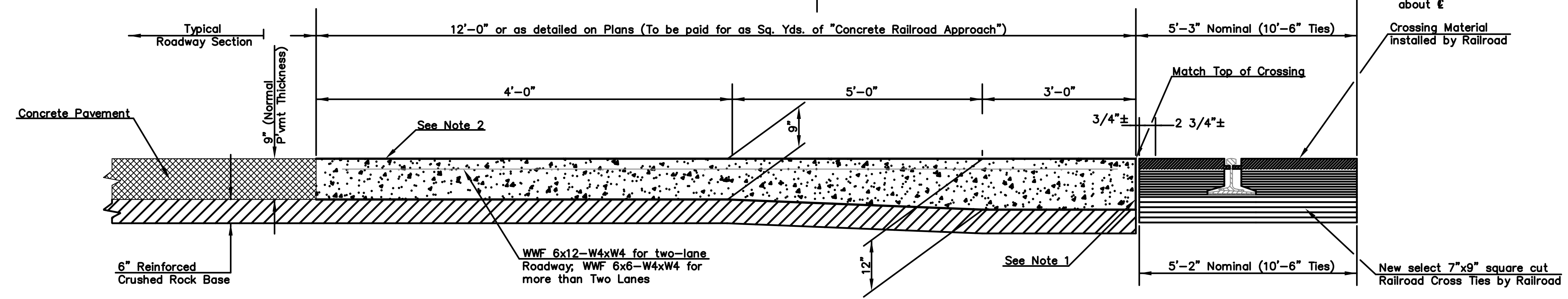
PLAN
APPROACH PAVING TO RAILROAD CROSSING

* Number of lanes & lane width as shown on plans
 † Trans. curb to no height
 ● crossing



SPECIAL NOTES

- ALL RAILROAD CROSSING MATERIAL BE FURNISHED AND INSTALLED BY THE INVOLVED RAILROAD COMPANY.
- ONE THICKNESS OF TAR PAPER SHALL BE INSTALLED BY THE PAVING CONTRACTOR ON ALL MATING SURFACES BETWEEN THE PAVEMENT AND THE RAILROAD CROSSING MATERIAL TO BREAK ANY BOND BETWEEN THE PAVEMENT AND THE RAILROAD CROSSING MATERIAL.
- EXISTING PAVEMENT SHALL BE REMOVED AND/OR CONSTRUCTED BY THE PAVING CONTRACTOR AS SHOWN BY THE PLANS AND SPECIAL DETAILS IN ALL AREAS WHERE SUCH PAVEMENT ABUTS RAILROAD CROSSINGS WHICH ARE TO BE IMPROVED. PAVEMENT IMMEDIATELY ADJACENT TO THE CROSSINGS SHALL BE REMOVED PRIOR TO THE INSTALLATION OF NEW RAILROAD CROSSING MATERIALS. PAVING CONTRACTOR SHALL COORDINATE THE PAVEMENT REMOVAL AND CONSTRUCTION AT EACH CROSSING LOCATION WITH THE INVOLVED RAILROAD COMPANY. ALL EXPOSED JOINTS BETWEEN NEW CONSTRUCTION AND EXISTING PAVEMENT, WALK, OR DRIVES SHALL BE TO NEAT LINES FORMED BY SAW CUT. REMOVAL AREAS SHOWN ON THE PLANS ARE APPROXIMATE, EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE FIELD ENGINEER AFTER THE NEW RAILROAD CROSSING MATERIAL HAS BEEN INSTALLED.
- LENGTHS OF CROSSING MATERIAL SHOWN ON THE PLANS IN MOST CASES ARE COMPUTED TO EXTEND 3 FEET BEYOND BOTH SIDES OF THE PAVED MAIN TRAFFICWAY FOR EACH LOCATION. WOOD PLANKING SHALL BE INSTALLED BY THE INVOLVED RAILROAD COMPANY OUTSIDE THE LIMITS OF THE INSTALLATION FOR SIDEWALK, DRIVEWAY, AND SHOULDER CROSSINGS WHERE NECESSARY. THE INVOLVED RAILROAD COMPANIES SHALL ADJUST THEIR RAILS TO ELEVATIONS AS SHOWN ON THE PLANS FOR EACH CROSSING LOCATION. VARIATIONS FROM THE TOP OF RAIL ELEVATIONS SHOWN WILL BE PERMITTED ONLY WHEN APPROVED BY THE FIELD ENGINEER. ALL NEW PAVEMENT AND RAILROAD CROSSING CONSTRUCTION SHALL MATCH THE FINISHED RAIL ELEVATIONS.
- THE UNIT PRICE BID FOR "CONCRETE RAILROAD APPROACH", MEASURED ON A SQ. YD. BASIS, SHALL BE FULL COMPENSATION FOR FURNISHING ALL REINFORCING STEEL AND WELDED WIRE FABRIC; FOR ALL CONCRETE; FOR ALL EXCAVATION; AND FOR ALL LABOR, TOOLS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE APPROACH.



- Note 1: One thickness of 90# tar paper for bond breaker on all mating surfaces between pavement, sidewalks, or drives and railroad crossing material; to be installed by Paving Contractor.
- Note 2: Top of pavement true to X-slope and grade established by plan elevations and match of crossing installation.

SECTION A-A

Plotted By: svb Date: 8/16/2013 8:46:17 AM
 FILE: I:\2008\08207\Standard\ds\08207-RRHEADER.DGN

No.	Revision	By	Date
37TH STREET NORTH CONCRETE HEADER AT RAILROAD CROSSING GARY JANZEN, P.E.-CITY ENGINEER CITY OF WICHITA PROJECT NO. 472-84692			
PROFESSIONAL ENGINEERING CONSULTANTS, P.A. 303 SOUTH TOPEKA WICHITA, KS 67202 316-262-2691 www.pec1.com			
Designed by	JPS	Job No.	08207
Drawn by	SVB	Date	August, 2013
			Sht. 42 of 193