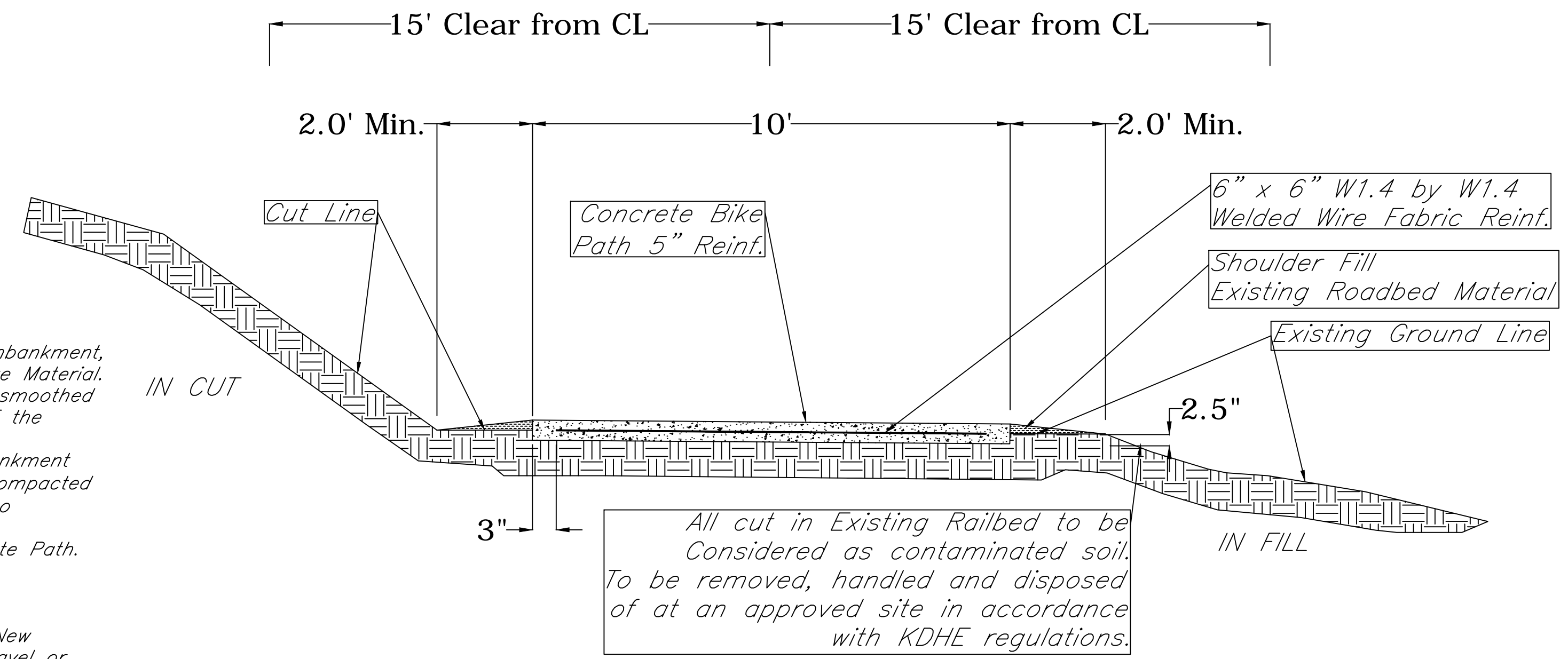


STATE	PROJECT NO.	YEAR
KANSAS	87 TE-0399-01	2014
F.A. NO.	TEA-TO39(901)	



PATH BASE

When the path is over the Existing Railroad Embankment, the Base Shall be the Existing Railbed Aggregate Material. The Aggregate Base Shall be compacted and smoothed with a Steel Faced Roller prior to placement of the pavement.

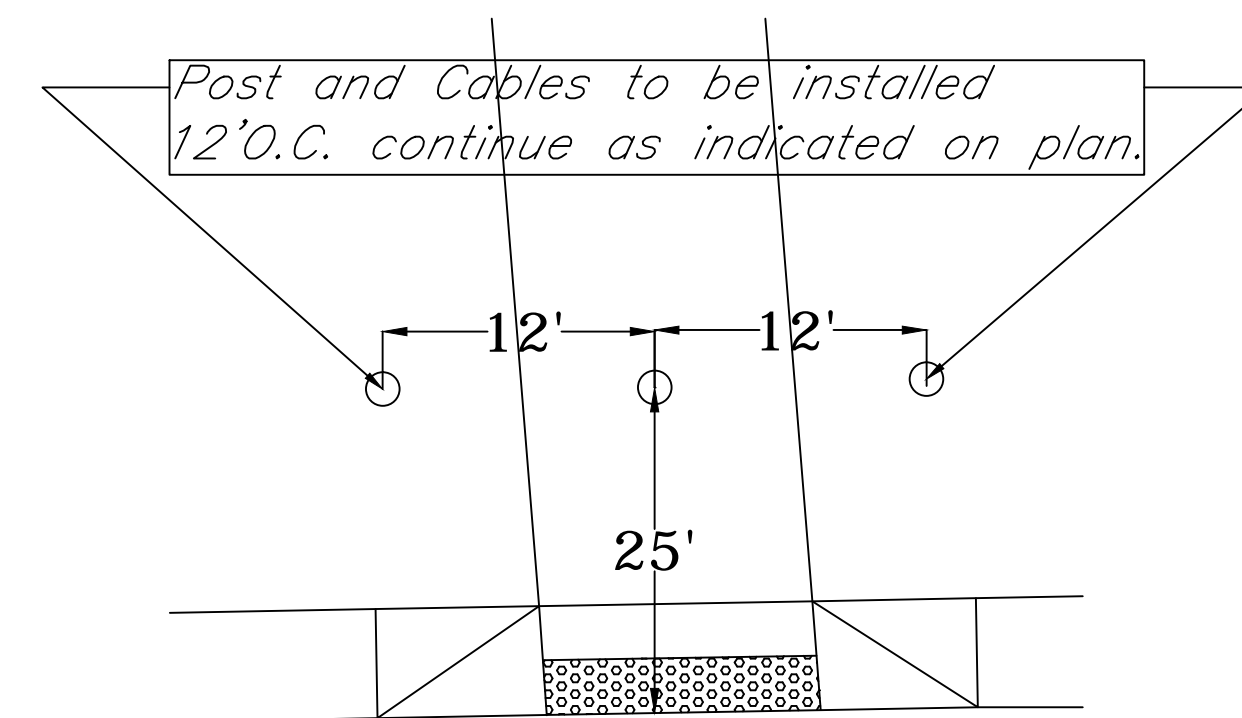
When the Path is outside of the Railroad Embankment the Base Shall be 6" of Clean Crushed Rock compacted and smoothed with a Steel Faced Roller prior to placement of the Pavement. Base Grading and Manipulation Shall be SUBSIDIARY to the Concrete Path.

SHOULDER

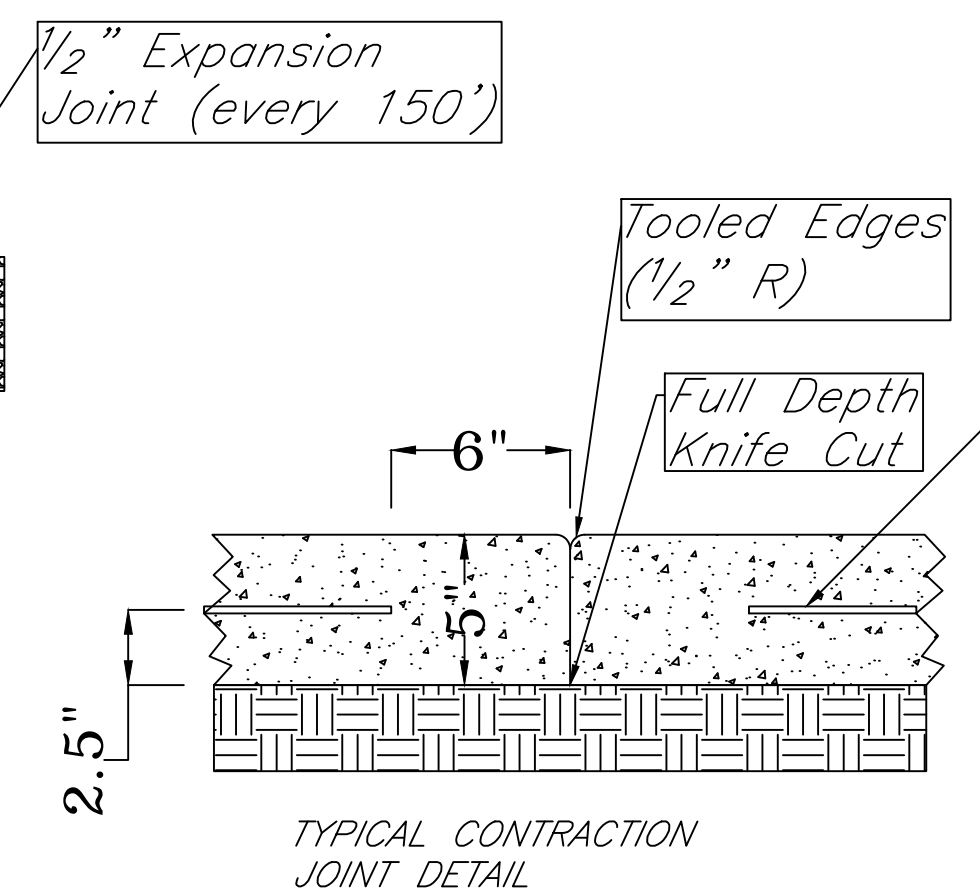
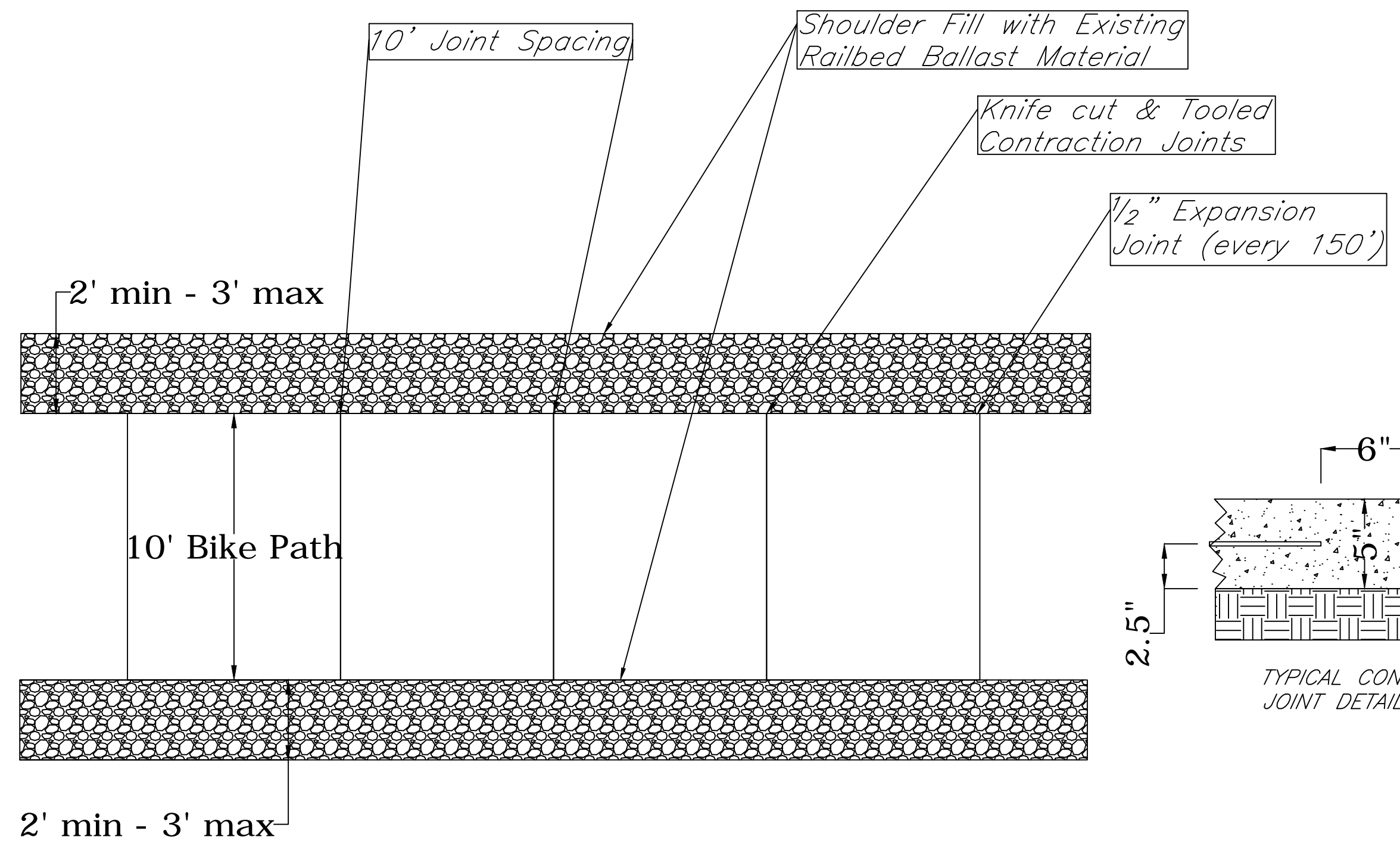
Where no Existing Railbed Aggregate exists, the Contractor is to install New Shoulder Material. New Shoulder Material Shall be KDOT AS-6 Road Gravel or approved equal. The shoulder is to be rolled for compaction and smoothness. Shoulder Material, placement and compaction is to be SUBSIDIARY to the Concrete Path.

JOINTS

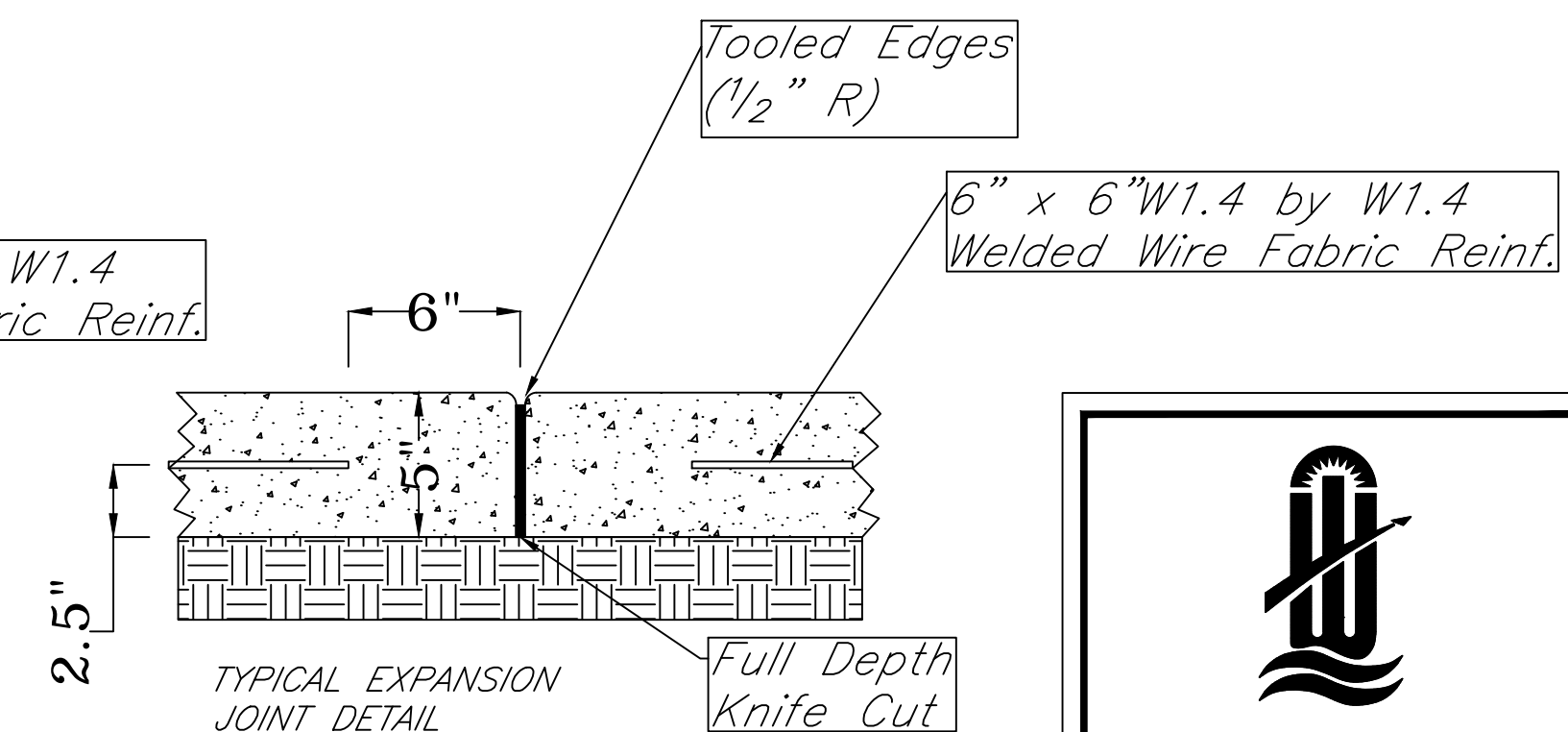
Contraction Joints are to be placed at a spacing Equal to the Width of the Path (10' typical). 1/2" Expansion Joints are to be placed at 150' intervals. (see details this sheet).



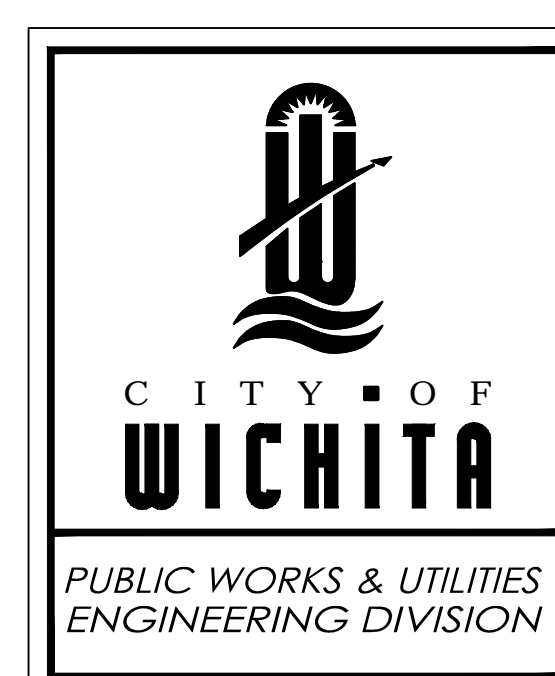
TYPICAL AND POST & CABLE DETAIL



TYPICAL CONTRACTION JOINT DETAIL



TYPICAL EXPANSION JOINT DETAIL



REVISION DATE: Sept. 2014

REDBUD MULTI-USE PATH PHASE II OLIVER to WOODLAWN		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER 472-85117	OCA NUMBER 707058	DATE 5/9/2014
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 6 of 9