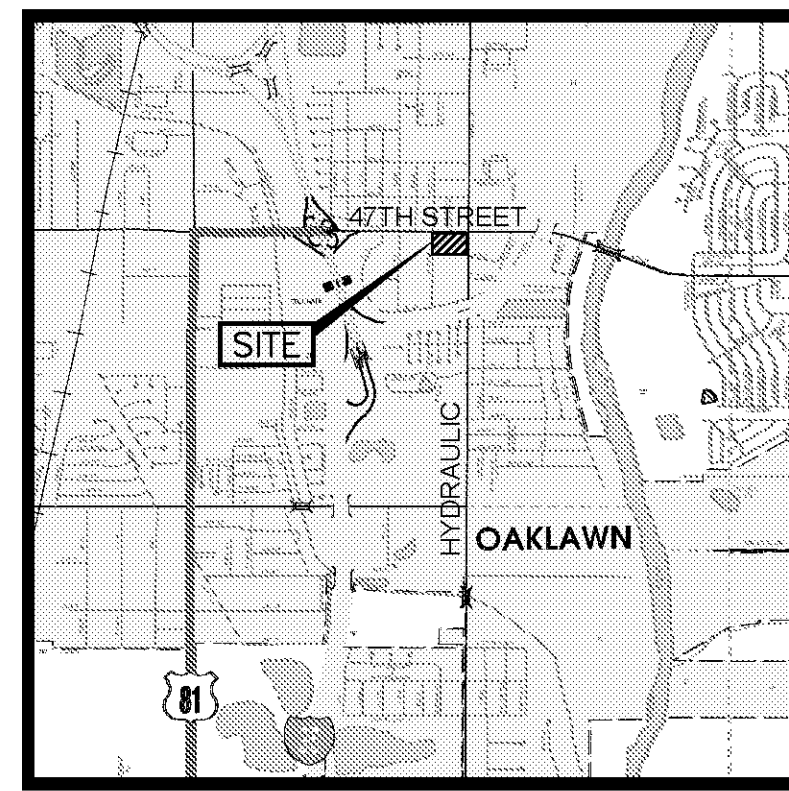


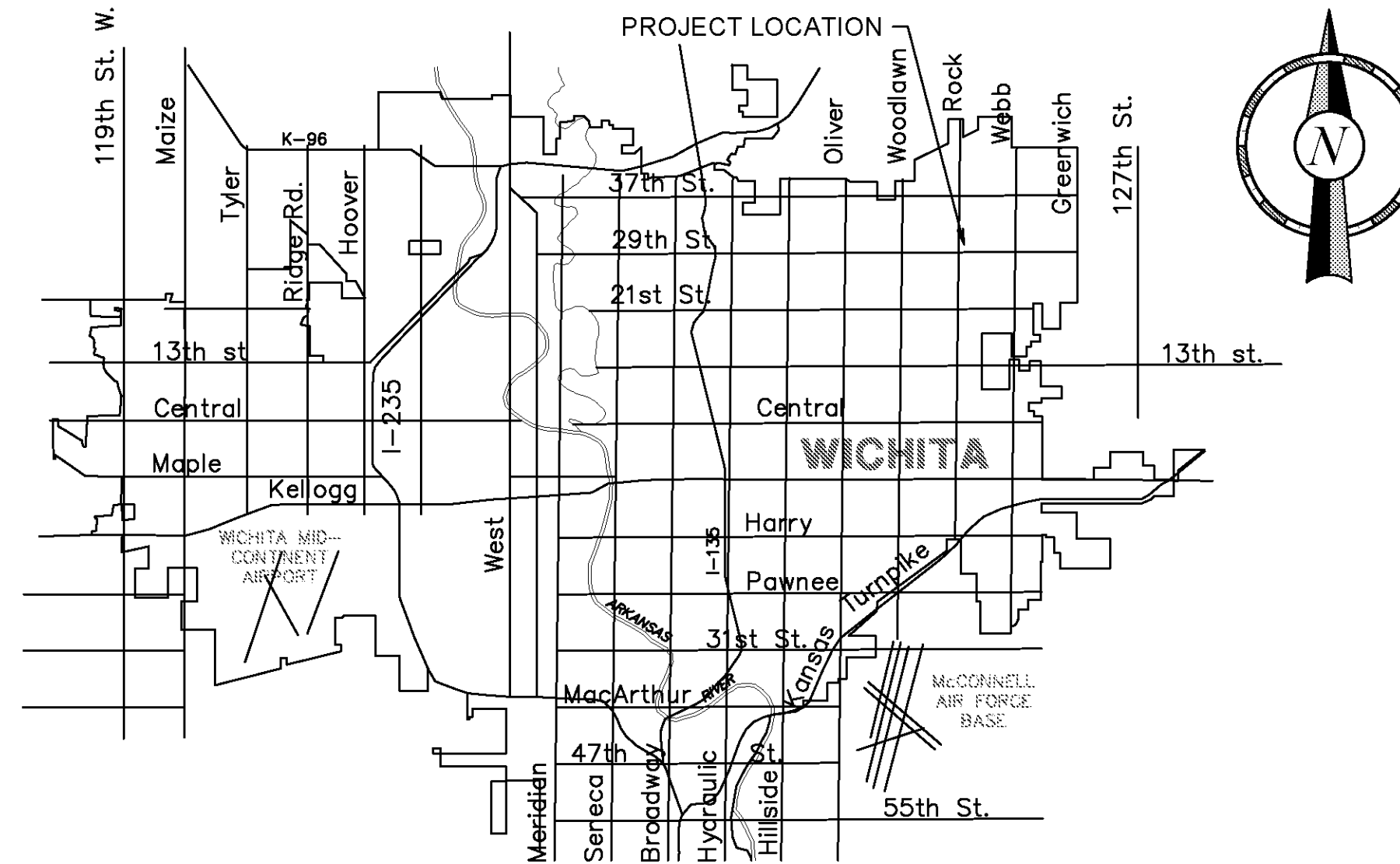
GENERAL NOTES:

- ALL ELEVATIONS SHOWN ARE BASED ON N.A.V.D. 88.
- INTERURBAN TRAFFIC GENERATED OUTSIDE THE PROJECT AREA IS NOT TO BE CARRIED THROUGH CONSTRUCTION. LOCAL RESIDENTIAL TRAFFIC GENERATED WITHIN THE PROJECT AREA IS TO BE CARRIED THROUGH CONSTRUCTION AS FURTHER PROMULGATED BY PROJECT SPECIAL PROVISIONS.
- UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, ECT. ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR OR UNLESS THE PLANS SPECIFICALLY IDENTIFY A UTILITY TO BE ADJUSTED BY ITS OWNER DURING CONSTRUCTION. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. THE PLAN LOCATIONS ARE NOT GUARANTEED AND ADDITIONAL UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.
- RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND EXCESS EXCAVATION WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE, AND SITE LOCATION. LOCATIONS THAT IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOODPLAIN WOULD REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIALS DUMPED IN WATERS OF THE UNITED STATE OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS WOULD REQUIRE ADDITIONAL ARCHAEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION.
- CONTRACTOR WILL BE REQUIRED TO PROVIDE A MINIMUM ADVANCE NOTICE OF SEVENTY-TWO(72) HOURS TO UTILITY COMPANIES PRIOR TO STARTING ANY EXCAVATION AS FOLLOWS:
 1-800-344-7233 OR KANSAS ONE CALL 687-2470(LOCAL WICHITA) THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF AN EMERGENCY:
 COX COMMUNICATIONS 262-4270 OR 263-2061
 AT&T 1-800-870-8390
 KANSAS GAS SERVICE 1-888-482-4950
 WESTAR 1-800-383-1183
 BLACK HILLS ENERGY 1-800-303-0752
 CITY OF WICHITA(WATER & SEWER) 268-4555
- THE CONTRACTOR SHALL NOTIFY PIPELINE COMPANIES AT LEAST 24 HOURS IN ADVANCE OF ANY WORK BEING PERFORMED ACROSS AND/OR ADJACENT TO PIPELINES.
- TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED BY THE CONTRACTOR WITH THE ENGINEERS APPROVAL AS CALLED OUT ON THE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO REESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE REESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS.
- THE CONTRACTOR SHALL GIVE ALL PROPERTY OWNERS AND/OR TENANTS OF DEVELOPED PROPERTY DIRECTLY ABUTTING THE CONSTRUCTION OF THIS PROJECT A MINIMUM OF TEN(10) DAYS ADVANCE NOTICE PRIOR TO START OF CONSTRUCTION.
- THE CONTRACTOR SHALL ADJUST WATER VALVE BOXES, WATER METER BOXES AND FIRE HYDRANTS AS NOTED IN THE PLANS AND AS DIRECTED BY THE ENGINEER. MATERIALS, EQUIPMENT, AND LABOR REQUIRED FOR THE ADJUSTMENT OF WATER METER AND VALVE BOXES AND FIRE HYDRANTS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED SUBSIDIARY TO OTHER ITEMS OF WORK IN THE CONTRACT. THE WATER DEPARTMENT SHALL FIELD LOCATE WATER VALVES ONE TIME DURING CONSTRUCTION WHEN REQUESTED BY THE CONTRACTOR. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO PRESERVE SUCH FIELD LOCATIONS DURING THE CONSTRUCTION PROCESS. WATER VALVES, WATER VALVE BOXES, OR FIRE HYDRANTS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR MUST EXAMINE THE CONSTRUCTION SITE PRIOR TO BIDDING AND BE SATISFIED AS TO THE WORK SHOWN FOR COMPLETION. AFTER BIDS HAVE BEEN RECEIVED, THE CONTRACTOR SHALL NOT ASSERT THAT THERE WAS A MISUNDERSTANDING OF THE QUANTITIES OF WORK OR OF THE NATURE FOR THE WORK TO BE COMPLETED.
- CONTRACTOR SHALL NOT START WORK ON THE PROJECT UNTIL THE PROJECT INSPECTOR IS ASSIGNED TO THE PROJECT AND IS PRESENT ON THE SITE. ANY WORK DONE WITHOUT INSPECTION WILL BE REQUIRED TO BE UNCOVERED FOR INSPECTION.
- THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL IN ACCORDANCE WITH THE CURRENT ADDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) TO ENSURE A SAFE WORK ZONE FOR THE TRAVELING PUBLIC.
- PAVEMENT REMOVAL AND/OR REPLACEMENT WILL BE MEASURED AND PAID FOR ON THE SQUARE FOOT BASIS REGARDLESS OF PAVEMENT TYPE AND/OR PAVEMENT THICKNESS. MINIMUM LIMITS OF SUCH PAVEMENT REMOVAL AND REPLACEMENT SHALL BE ONE FOOT BEYOND THE LIMITS OF EXCAVATION MADE FOR THE SEWER OR THE STRUCTURE, EXCEPT WHEN SUCH LIMITS OF REMOVAL ARE WITHIN THREE(3) FEET OF AN EXISTING JOINT. REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT SHALL CONFORM TO THE APPLICABLE SECTIONS OF THE CITY OF WICHITA STANDARD SPECIFICATIONS.
- ALL DRIVEWAY ENTRANCE AND CROSS ROAD PIPE WITHIN THE PROJECT LIMITS SHALL NOT BE REMOVED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE PLANS. REMOVAL OF SUCH PIPES SHALL CONFORM TO THE APPLICABLE SECTION OF THE STANDARD SPECIFICATIONS.

PAVING PRIVATE PROJECT PLANS FOR QUIKTRIP STORE #0356R HYDRAULIC ST. & 47TH AVE. WICHITA, SEDGWICK COUNTY, KANSAS 241 PPP (O.C.A. NO. 607879) GARY JANZEN, P.E., CITY ENGINEER OCTOBER 2014



Vicinity Map
 Not to Scale



LOCATION MAP

- MAILBOXES WITHIN THE LIMITS OF THE PROJECT SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AS APPROVED BY THE ENGINEER. CONTRACTOR WILL BE REQUIRED TO MAKE SATISFACTORY PROVISIONS FOR MAIL DELIVERY TO PROPERTIES AFFECTED BY THIS PROJECT DURING CONSTRUCTION.
- ALL FENCE LOCATED WITHIN THE RIGHT-OF-WAY WILL BE REMOVED AND SALVAGED BY THE CONTRACTOR. ALL MATERIALS SHALL BE SALVAGED FOR THE PROPERTY OWNER. FENCE OUTSIDE THE RIGHT-OF-WAY WHICH IS REMOVED FOR CONSTRUCTION SHALL BE REPAIRED IN A CONDITION EQUAL TO OR BETTER THAN THE ORIGINAL, AT NO ADDITIONAL COST TO THE OWNER, UNLESS OTHERWISE SPECIFIED ON THE PLANS. MATERIALS NOT SALVAGED BY THE PROPERTY OWNER SHALL BE DISPOSED OF BY THE CONTRACTOR.
- CONTRACTOR SHALL GIVE PROPERTY OWNERS ABUTTING THIS PROJECT, WHOSE YARDS WILL BE LOWER THAN THE NEW FINISHED GRADE ELEVATION AT THE RIGHT-OF-WAY LINE, AN OPPORTUNITY TO UTILIZE EXCESS EXCAVATED MATERIAL FROM THE PROJECT TO REGRADE THEIR YARDS TO DRAIN TO THE NEW PAVEMENT. CONTRACTOR WILL BE REQUIRED TO DUMP AND SPREAD THE EXCESS MATERIAL AS REQUIRED BY THE SPECIFICATIONS WHEN REQUESTED BY THE PROPERTY OWNER. THE CONTRACTOR SHALL ASCERTAIN THAT A DIRT ORDER HAS BEEN PROPERLY EXECUTED BY THE PROPERTY OWNER BEFORE ANY SUCH EXCESS MATERIAL IS DELIVERED TO SUCH PROPERTIES.
- THE CONTRACTOR SHALL ADJUST SANITARY SEWER MANHOLES AND STORM SEWER MANHOLES FOR ELEVATION AND SLOPE AS REQUIRED TO FIT PROPOSED FINISH GRADES.
- CONTRACTOR SHALL REMOVE AND REPLACE EXISTING STREET SIGNAGE AT THE BEGINNING AND END OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE HIS ACTIVITIES WITH THE CITY OF WICHITA TRAFFIC DEPARTMENT FOR TEMPORARY TRAFFIC SIGNALIZATION AND REMOVAL OF INSTALLATION OF STREET SIGNAGE.
- ALL CONSTRUCTION AND MATERIALS, UNLESS OTHERWISE NOTED, TO COMPLY WITH CITY OF WICHITA SPECIFICATIONS AND STANDARDS.
- THE COST OF CONCRETE DRIVEWAY REMOVAL SHALL INCLUDE A SAW CUT OF A MINIMUM DEPTH OF ONE FOURTH OF THE PAVEMENT THICKNESS. THE SAW CUT SHALL BE AT THE PROPERTY LINE OR THE NEAREST JOINT. THE EXACT LOCATION SHALL BE DETERMINED BY THE FIELD ENGINEER.
- CONTRACTOR SHALL REFERENCE SITE CIVIL PLANS UNDER BUILDING PERMIT NO. BLD2013-01630 FOR EROSION CONTROL PLANS & DETAILS.
- LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH EXISTING GROUND SURFACE.

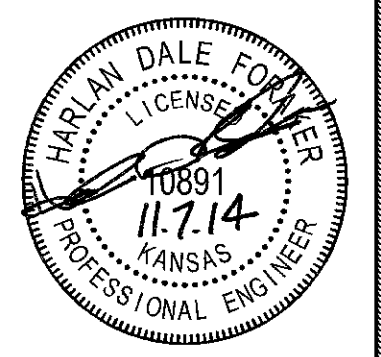
AS-BUILT PLANS
CONTRACTOR: ROCK CONCRETE
SUPERINTENDENT: TRAVIS WINNETT
FOREMAN: ROCKY BOURGET
CLIENT: CITY OF WICHITA
INSPECTOR: CALEB WILSON, SCHWAB EATON, P.A.
PDF BY: CFW 7/21/2015

BENCHMARKS:

BM - CHISELED SQUARE CUT TOP OF CURB SOUTHWEST CORNER OF 47TH STREET AND VICTORIA STREET.
 ELEV.=1272.28 N.A.V.D. 88

**PPP PLANS SHEET
 INDEX**

Sheet Number	Sheet Title
1	COVER
2	DEMO PLAN
3	SITE PLAN
4	GRADING PLAN
5	TRAFFIC CONTROL PLAN
6	Traffic Control Plan
7	TRAFFIC CONTROL DETAILS
8	TRAFFIC CONTROL DETAILS
9	PAVING DETAILS
10	CONCRETE PAVING DETAILS



PROJECT NO.:
CEED
 CERTIFIED ENGINEERING DESIGN, P.A.
 1935 W. MAPLE STREET
 WICHITA, KANSAS 67213
 PH (316)262-8808
 FAX (316)262-1699

QuikTrip No. 0356
 SWC 47TH & HYDRAULIC
 WICHITA, SEDGWICK, KS



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PROTOTYPE: P-78 (02/02/14)
DIVISION: 03
VERSION: 001
DESIGNED BY:
DRAWN BY: KSR
REVIEWED BY:

REV	DATE	DESCRIPTION

SHEET TITLE:
 COVER

SHEET NUMBER:
 1

ORIGINAL ISSUE DATE: 04/22/14

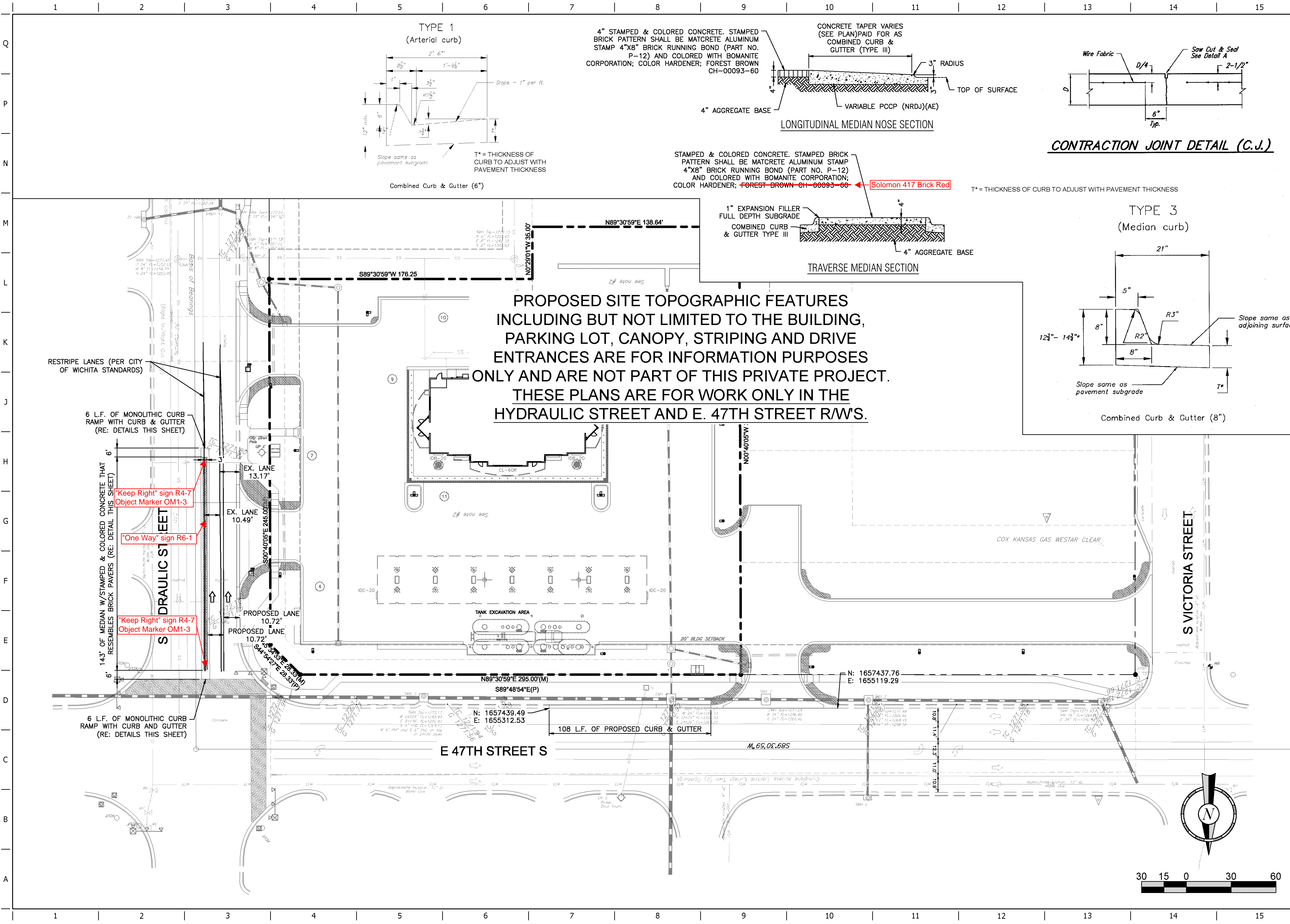
APPROVED AS NOTED
 BY CITY ENGINEER OF WICHITA
 Engineering *Rebecca Deil* 12/4/14
 NOTE TO CONTRACTORS
Inspection and testing for this project are to be provided by a Licensed Consulting Engineering Firm under contract with the Owner/Developer. Said inspection to be in accordance with the City of Wichita standard construction engineering practices and certified by a Licensed Professional Engineer. No work shall be performed in dedicated easements or public right-of-way by the Contractor without such inspection, nor shall any work be commenced without written authorization by the City Engineer.

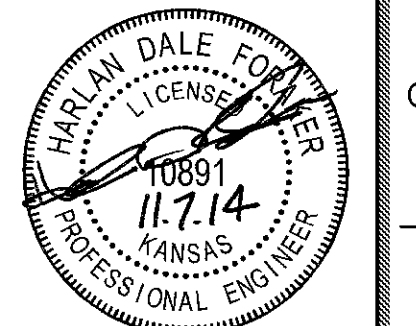


KS: 1-800-344-7233
 WICHITA: 316-687-2470


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PROJECT NO.:




CERTIFIED ENGINEERING DESIGN, P.A.

1935 W. MAPLE STREET
 WICHITA, KANSAS 67213
 PH (316)262-8808
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QuikTrip No. 0356

SWC 47TH & HYDRAULIC
 WICHITA, SEDGWICK, KS



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PROTOTYPE: P-78 (02/02/14)
DIVISION: 03
VERSION: 001
DESIGNED BY:
DRAWN BY: KSR
REVIEWED BY:

REV	DATE	DESCRIPTION

ORIGINAL ISSUE DATE: 04/22/14

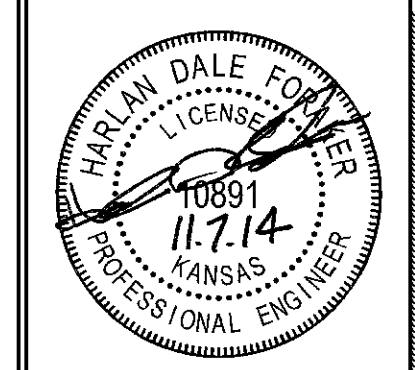
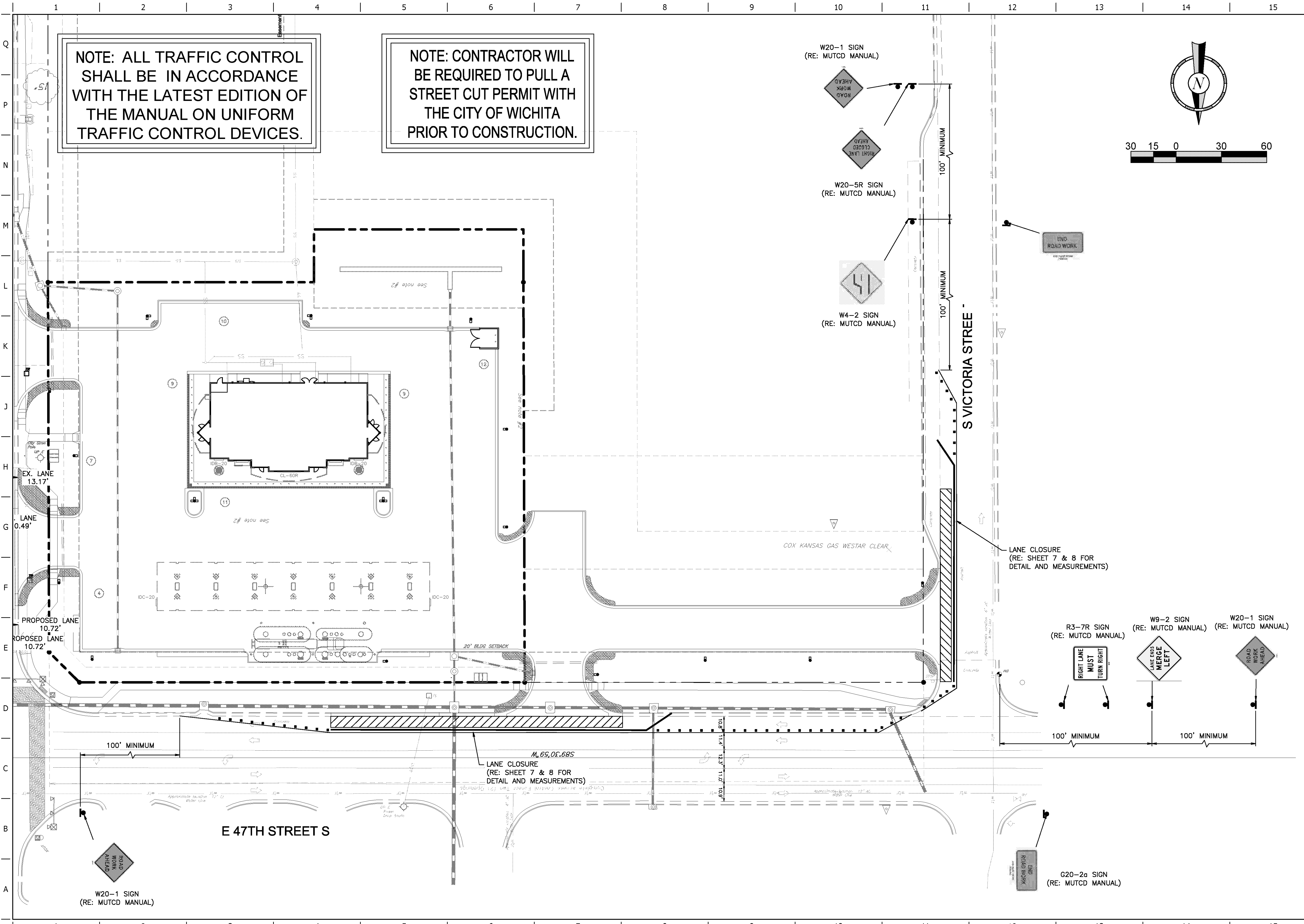
SHEET TITLE:
SITE PLAN

SHEET NUMBER:
3

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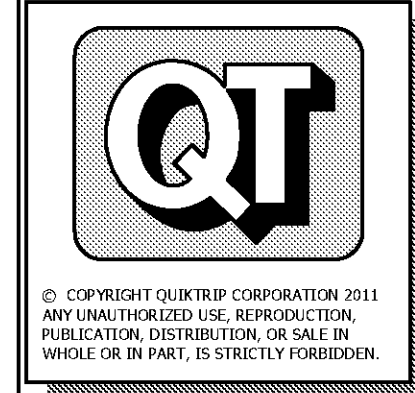
NOTE: ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

NOTE: CONTRACTOR WILL BE REQUIRED TO PULL A STREET CUT PERMIT WITH THE CITY OF WICHITA PRIOR TO CONSTRUCTION.



PROJECT NO.:
CED
 CERTIFIED ENGINEERING DESIGN, P.A.
 1935 W. MAPLE STREET
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 PH. (316)262-8808
 FAX. (316)262-1699

QuikTrip No. 0356
 SWC 47TH & HYDRAULIC
 WICHITA, SEDGWICK, KS



PROTOTYPE: P-78 (02/02/14)
 DIVISION: 03
 VERSION: 001
 DESIGNED BY:
 DRAWN BY: KSR
 REVIEWED BY:

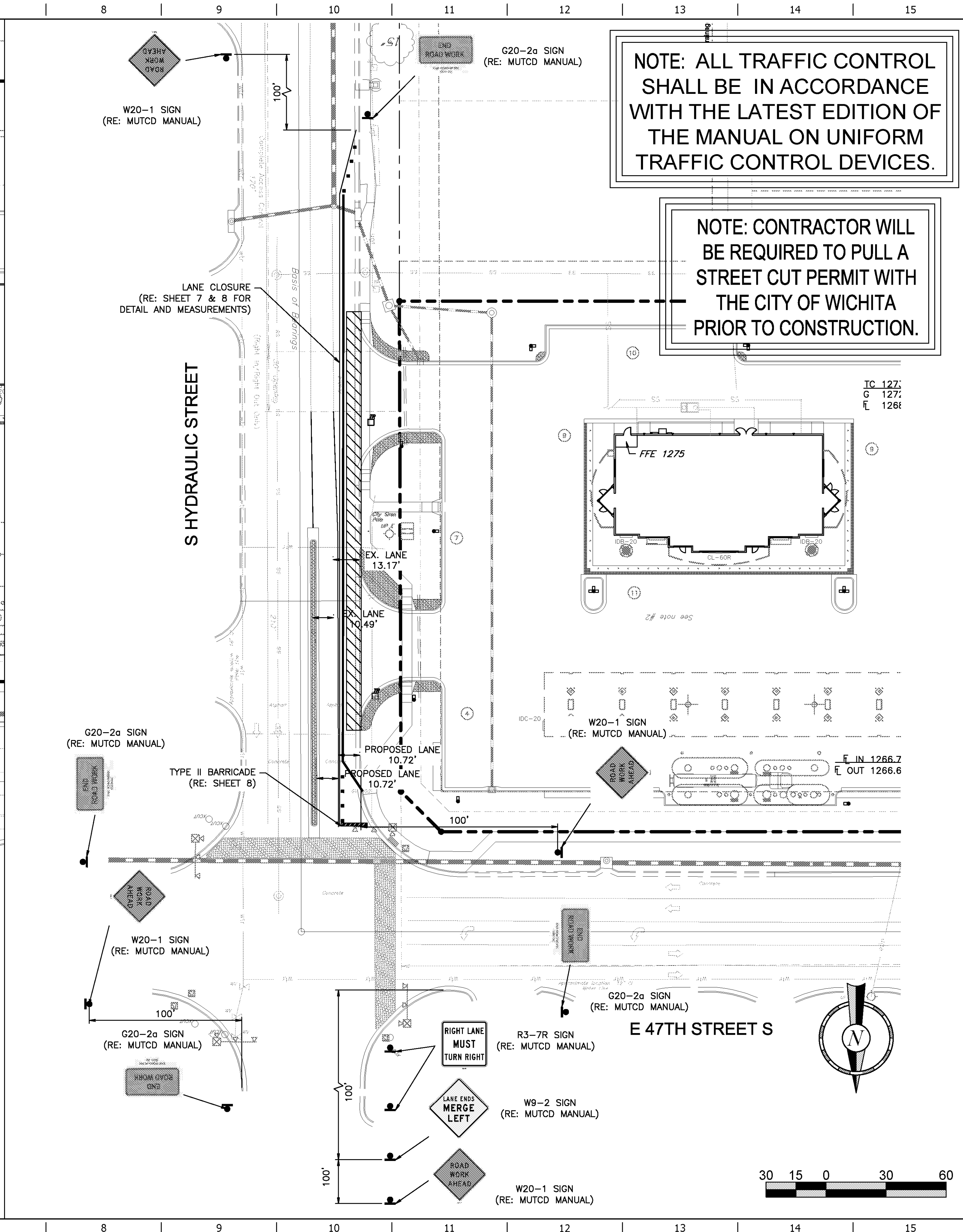
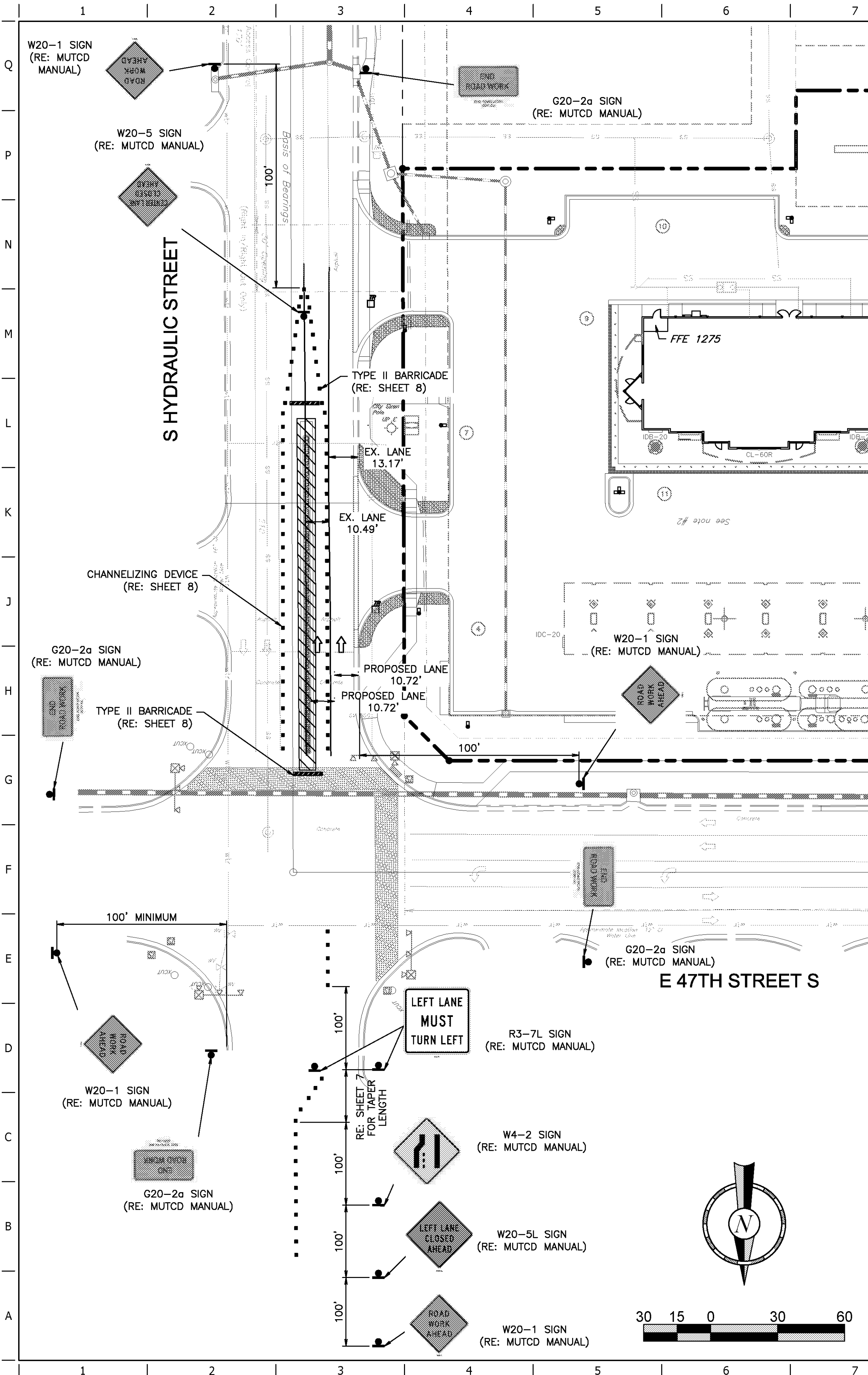
REV	DATE	DESCRIPTION

SHEET TITLE:
 TRAFFIC CONTROL PLAN

SHEET NUMBER:
5

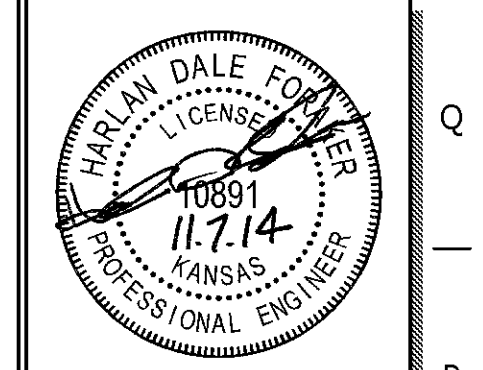
ORIGINAL ISSUE DATE: 04/22/14

FILE LOCATION: \\Drawing Files\Project AMG 9-16-13\01 #0356R.DWG\03-0356 PPP.dwg TAB NAME: Traffic Control Plan (2) USER: kagorich SAVED: 12/2/2014 3:34 PM PLOTTED: 12/2/2014 3:46 PM



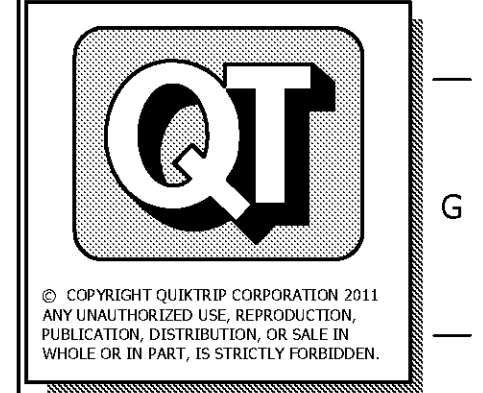
NOTE: ALL TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

NOTE: CONTRACTOR WILL BE REQUIRED TO PULL A STREET CUT PERMIT WITH THE CITY OF WICHITA PRIOR TO CONSTRUCTION.



PROJECT NO.:
CED
 CERTIFIED ENGINEERING DESIGN, P.A.
 1935 W. MAPLE STREET
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 PH (316)262-8808
 FAX (316)262-1699

QuikTrip No. 0356
 SWC 47TH & HYDRAULIC
 WICHITA, SEDGWICK, KS



PROTOTYPE: P-78 (02/02/14)
 DIVISION: 03
 VERSION: 001
 DESIGNED BY:
 DRAWN BY: KSR
 REVIEWED BY:

REV	DATE	DESCRIPTION

SHEET TITLE:
 TRAFFIC CONTROL PLAN

SHEET NUMBER:
6

ORIGINAL ISSUE DATE: 04/22/14

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS				

1. MUTCD COMPLIANCE:

ALL TEMPORARY TRAFFIC CONTROL DEVICES AND THEIR INSTALLATION AND MAINTENANCE SHALL COMPLY WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS WHICH HAS BEEN ADOPTED BY THE SECRETARY OF TRANSPORTATION. WHENEVER THE TEMPORARY TRAFFIC CONTROL STANDARDS CONFLICT WITH THE MUTCD, THE STANDARDS SHALL GOVERN.

2. DESIGN SPEED:

THOSE ITEMS DELEGATED TO TEMPORARY TRAFFIC CONTROL SHOULD BE DESIGNED AND INSTALLED USING THE POSTED/LEGAL SPEED OF THE ROADWAY PRIOR TO WORK STARTING.

3. CLEAR ZONE:

ALL CONSTRUCTION EQUIPMENT (INCLUDING VEHICLES), MATERIALS, AND DEBRIS SHALL BE STORED OUT OF THE CLEAR ZONE. WHERE THIS CANNOT BE ACHIEVED, THE CONTRACTOR SHALL PLACE APPROPRIATE SIGNS, OBJECT IDENTIFIERS, AND/OR BARRICADES AS DESIGNATED BY THE ENGINEER. TEMPORARY TRAFFIC CONTROL DEVICES NEEDED FOR THIS CONDITION SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

4. MINIMUM LANE WIDTHS:

LANE WIDTHS SHALL BE A MINIMUM OF 11' (MEASURED BETWEEN CENTERLINES OF PAVEMENT MARKINGS) OR AS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. A LANE WIDTH LESS THAN 11' MAY REQUIRE RESTRICTED ROADWAY WIDTH SIGNING.

5. FLAGGER:

A MINIMUM OF ONE FLAGGER SHALL BE STATIONED WITHIN EACH MULTI-LANE ROADWAY ACTIVITY AREA WHERE WORK IS IN A CLOSED LANE ADJACENT TO TRAFFIC AND NOT SEPARATED BY A CONCRETE SAFETY BARRIER SYSTEM.

6. PAVEMENT MARKING:

WHEN THE WORK WILL OCCUPY A LOCATION MORE THAN THREE DAYS, ALL CONFLICTING PAVEMENT MARKINGS SHALL BE REMOVED OR MASKED AND ALL TRANSITION TAPERS, CROSSOVERS, AND EDGE LINES ALONG CHANNELIZING DEVICES SHALL BE MARKED WITH SOLID 4" WIDE PAVEMENT MARKING.

7. FIRST MODULE OF IBS:

THE FIRST MODULE OF EACH INERTIAL BARRIER SYSTEM (IBS) SHALL HAVE A MINIMUM OF 2 SQ. FT. OF FLUORESCENT ORANGE ASTM TYPE IV SHEETING FACING TRAFFIC. EITHER A VERTICAL RECTANGLE OR DIAMOND SHAPE MAY BE USED.

8. PEDESTRIAN / BICYCLE SAFETY:

WORK ZONE SIGNS SHALL NOT INHIBIT PEDESTRIAN AND BICYCLE TRAFFIC ON SIDEWALKS OR OTHER AREAS DESIGNATED FOR PEDESTRIAN OR BICYCLE USE.

CONSIDERATION SHOULD BE MADE TO SEPARATE PEDESTRIAN AND BICYCLE MOVEMENTS FROM BOTH WORK SITE ACTIVITY AND VEHICULAR TRAFFIC. UNLESS A REASONABLE SAFE ROUTE THAT DOES NOT INVOLVE CROSSING THE ROADWAY CAN BE PROVIDED, PEDESTRIANS AND BICYCLISTS SHOULD BE APPROPRIATELY DIRECTED WITH ADVANCE SIGNING THAT ENCOURAGES THEM TO CROSS TO THE OPPOSITE SIDE OF THE ROADWAY. IN URBAN AND SUBURBAN AREAS WITH HIGH VEHICULAR TRAFFIC VOLUMES, THESE SIGNS SHOULD BE PLACED AT INTERSECTIONS (RATHER THAN MIDBLOCK LOCATIONS) SO THAT PEDESTRIANS AND BICYCLISTS ARE NOT CONFRONTED WITH MIDBLOCK WORK SITES THAT WILL INDUCE THEM TO ATTEMPT SKIRTING THE WORK SITE OR MAKING A MIDBLOCK CROSSING.

WHEN EXISTING PEDESTRIAN FACILITIES ARE DISRUPTED, CLOSED, OR RELOCATED, THE TEMPORARY FACILITIES SHALL BE DETECTABLE AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.

9. CHANGED STOP CONDITIONS:

ATTACH TWO FLAGS AND A RED TYPE "B" HIGH INTENSITY WARNING LIGHT TO ANY STOP SIGN THAT CREATES A NEW STOP CONDITION OR MOVES THE STOP CONDITION TO A NEW LOCATION. LEAVE FLAGS AND LIGHTS IN PLACE FOR AT LEAST THE FIRST 30 DAYS. INSTALL W3-1 (SYMBOLIC STOP AHEAD) SIGN IN ADVANCE OF STOP SIGN IF STOP SIGN IS NOT VISIBLE FOR A MINIMUM OF DISTANCE 'A' (SEE CHART ON TE710) OR IF STOP CONDITION IS MOVED TO LESS THAN DISTANCE 'A' FROM AN EXISTING STOP AHEAD SIGN.

10. LUMP SUM BIDDING:

WHEN TRAFFIC CONTROL IS BID LUMP SUM, ADDITIONAL DEVICES WILL BE PAID FOR AS EXTRA WORK.

11. NIGHTTIME LIGHTING:

WHEN NIGHTTIME WORK IS REQUIRED, FLOODLIGHTS SHOULD BE USED TO ILLUMINATE FLAGGER STATIONS, EQUIPMENT CROSSINGS, AND OTHER AREAS WHERE EXISTING LIGHTING IS NOT ADEQUATE FOR THE WORK TO BE PERFORMED SAFELY.

IN NO CASE SHALL FLOODLIGHTS BE PERMITTED TO CREATE A DISABLING GLARE FOR THE DRIVER. THE ADEQUACY OF THE FLOODLIGHT PLACEMENT AND ELIMINATION OF POTENTIAL GLARE SHOULD BE CHECKED BY DRIVING THROUGH THE PROJECT.

12. NCHRP REPORT 350 CRASHWORTHY REQUIREMENTS:

TRAFFIC CONTROL DEVICES SHALL MEET THE EVALUATION CRITERIA IN NCHRP REPORT 350 AS SUPPLEMENTED BY FHWA MEMORANDUM "IDENTIFYING ACCEPTABLE HIGHWAY SAFETY FEATURES," DATED JULY 25, 1997. AVAILABLE ON THE INTERNET AT http://safety.fhwa.dot.gov/roadway_dept/road_hardware/nchrp_350.htm

ANY DEVICE NOT ADDRESSED BY THE TE STANDARDS MAY BE APPROVED ON A CASE BY CASE BASIS BY THE ENGINEER. THE DEVICE SHALL BE ACCOMPANIED BY AND INSTALLED ACCORDING TO NCHRP REPORT 350.

THE CONTRACTOR SHALL:

1) PROVIDE TO THE ENGINEER A COPY OF THE MANUFACTURER'S SELF-CERTIFICATION THAT ANY CATEGORY 1 (i.e. - PLASTIC CONICAL DELINEATORS, TUBULAR MARKERS, DRUMS WITHOUT ATTACHMENTS) AND CATEGORY 2 (i.e. - PORTABLE SIGN STANDS (WITH SIGNS), TYPE II AND III BARRICADES, AND VERTICAL PANELS) DEVICES USED ON THE PROJECT ARE NCHRP REPORT 350 COMPLIANT.

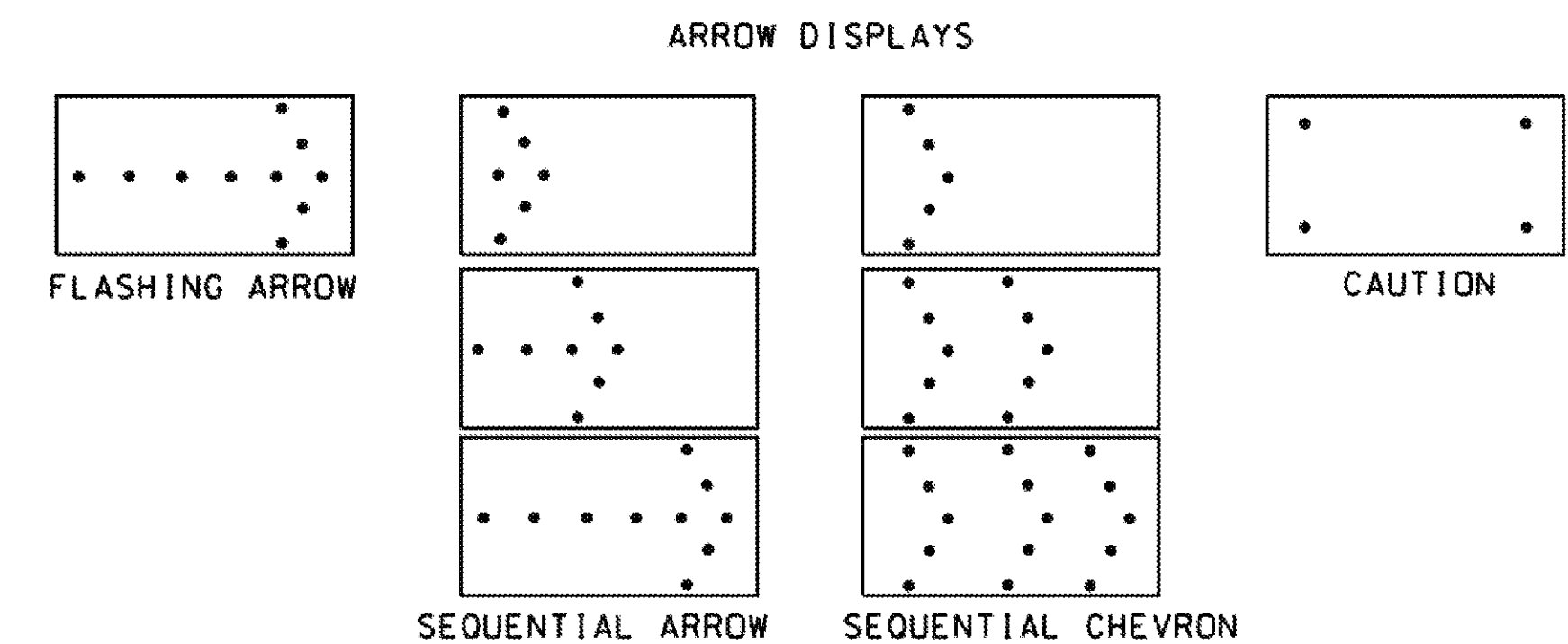
2) PROVIDE TO THE ENGINEER A COPY OF THE ENTIRE FHWA NCHRP REPORT 350 ACCEPTANCE LETTER (WZ-xxx) FOR ANY CATEGORY 2 DEVICE (i.e. - PORTABLE SIGN STANDS (WITH SIGNS), TYPE II AND III BARRICADES, AND VERTICAL PANELS) USED ON THE PROJECT. WORK ZONE FHWA NCHRP REPORT 350 ACCEPTANCE LETTERS (WZ-xxx) ARE AVAILABLE ON THE INTERNET AT: http://safety.fhwa.dot.gov/roadway_dept/road_hardware/listing.cfm?code=workzone

3) CERTIFY THAT THE TRUCK MOUNTED ATTENUATORS (TMA'S) (WHICH ARE DEFINED AS CATEGORY 3 DEVICES BY THE FHWA MEMORANDUM) WERE PURCHASED PRIOR TO OCTOBER 1, 1998, AND INCLUDE A COPY OF THE ENTIRE FHWA ACCEPTANCE LETTER STATING THAT THE TMA'S ARE NCHRP REPORT 230 COMPLIANT; OR IF THE DEVICES WERE PURCHASED AFTER OCTOBER 1, 1998, INCLUDE A COPY OF THE ENTIRE FHWA'S ACCEPTANCE LETTER STATING THAT THE TMA'S ARE NCHRP REPORT 350 COMPLIANT.

ALL CATEGORY 1 & 2 DEVICES SHALL BE NCHRP REPORT 350 COMPLIANT. TMA'S, PURCHASED PRIOR TO OCTOBER 1, 1998, MAY BE USED UNTIL THE END OF THEIR SERVICEABLE LIVES.

13. TYPE "A" LOW INTENSITY WARNING LIGHTS:

A TYPE "A" LOW INTENSITY WARNING LIGHT IS AN L.E.D. BI-DIRECTIONAL FLASHING WORK ZONE WARNING LIGHT.



ARROW DISPLAY ELEMENTS SHALL BE CAPABLE OF A MINIMUM 50 PERCENT DIMMING FROM THEIR FULL-RATED LAMP VOLTAGE. FULL LAMP VOLTAGE SHOULD BE USED DURING THE DAY AND DIMMED MODE SHALL BE USED AT NIGHT. FOR SHOULDER WORK, ROADSIDE WORK NEAR THE SHOULDER, BLOCKING THE SHOULDER, OR FOR TEMPORARY CLOSING ONE LANE ON A TWO-LANE, TWO-WAY ROADWAY, AN ARROW PANEL SHALL BE USED ONLY IN THE CAUTION MODE.

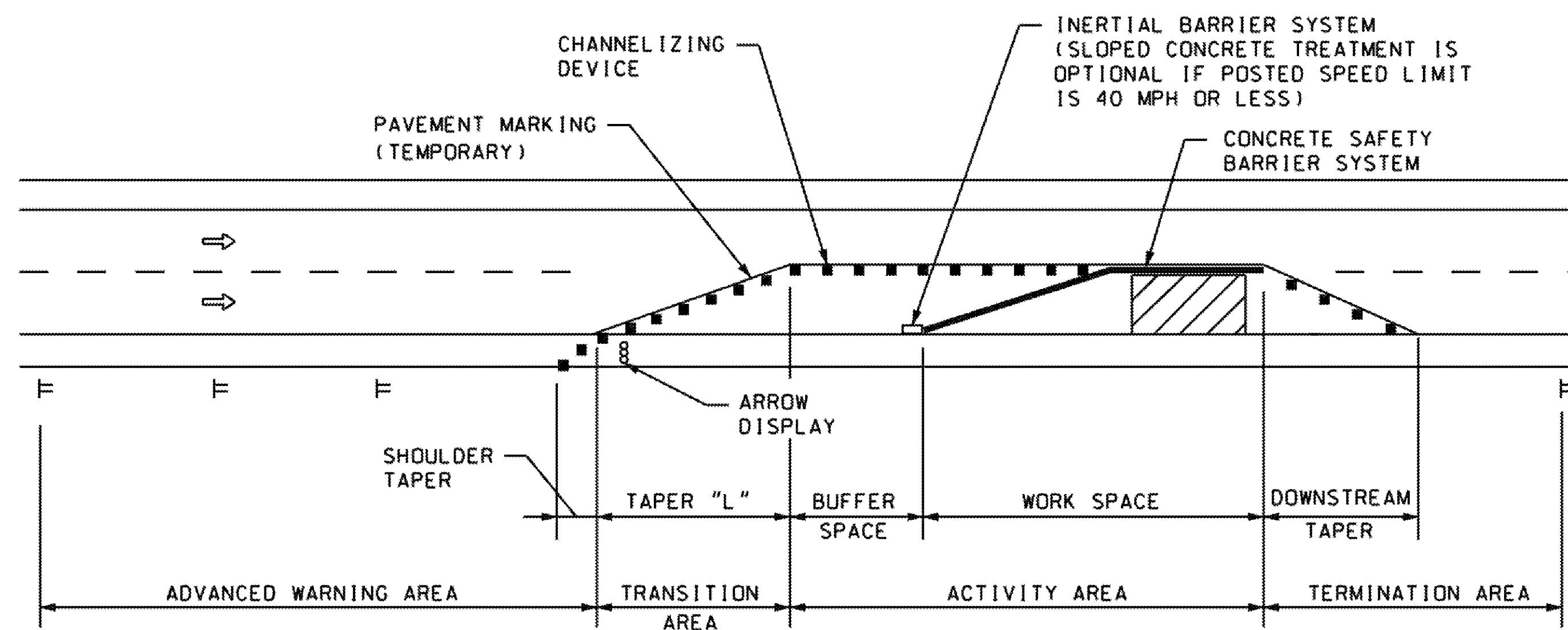
BUFFER SPACE

SPEED (MPH) *	20	25	30	35	40	45	50	55	60	65	70
LENGTH (ft)	115	155	200	250	305	360	425	495	570	645	730

NEITHER WORK ACTIVITY NOR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR IN THE BUFFER SPACE. WHEN A PROTECTION VEHICLE IS PLACED IN ADVANCE OF THE WORK SPACE, ONLY THE SPACE UPSTREAM OF THE VEHICLE CONSTITUTES THE BUFFER SPACE.

* POSTED SPEED PRIOR TO WORK STARTING

IF TEMPORARY CONCRETE SAFETY BARRIER SYSTEM IS USED TO SEPARATE APPROACHING TRAFFIC FROM THE WORK SPACE, THE BARRIER SYSTEM SHALL BE CONSIDERED PART OF THE ACTIVITY AREA. A FULL LANE WIDTH SHOULD BE AVAILABLE THROUGHOUT THE LENGTH OF THE BUFFER SPACE. SEE TYPICAL WORK ZONE COMPONENTS.



NOTE:
REFER TO STD. TE702 FOR
TAPER "L" FORMULA.

TYPICAL WORK ZONE COMPONENTS

3	4-20-09	REVISED NOTE 7	J.M.	A.A.A.
2	8-8-07	ADD NOTE 13, REVISE NOTE 8 & WZ COMPONENTS	M.B.	A.A.A.
1	12-29-05	MODIFIED BUFFER SPACE TABLE	M.B.	A.A.A.
NO.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION

GENERAL TRAFFIC CONTROL

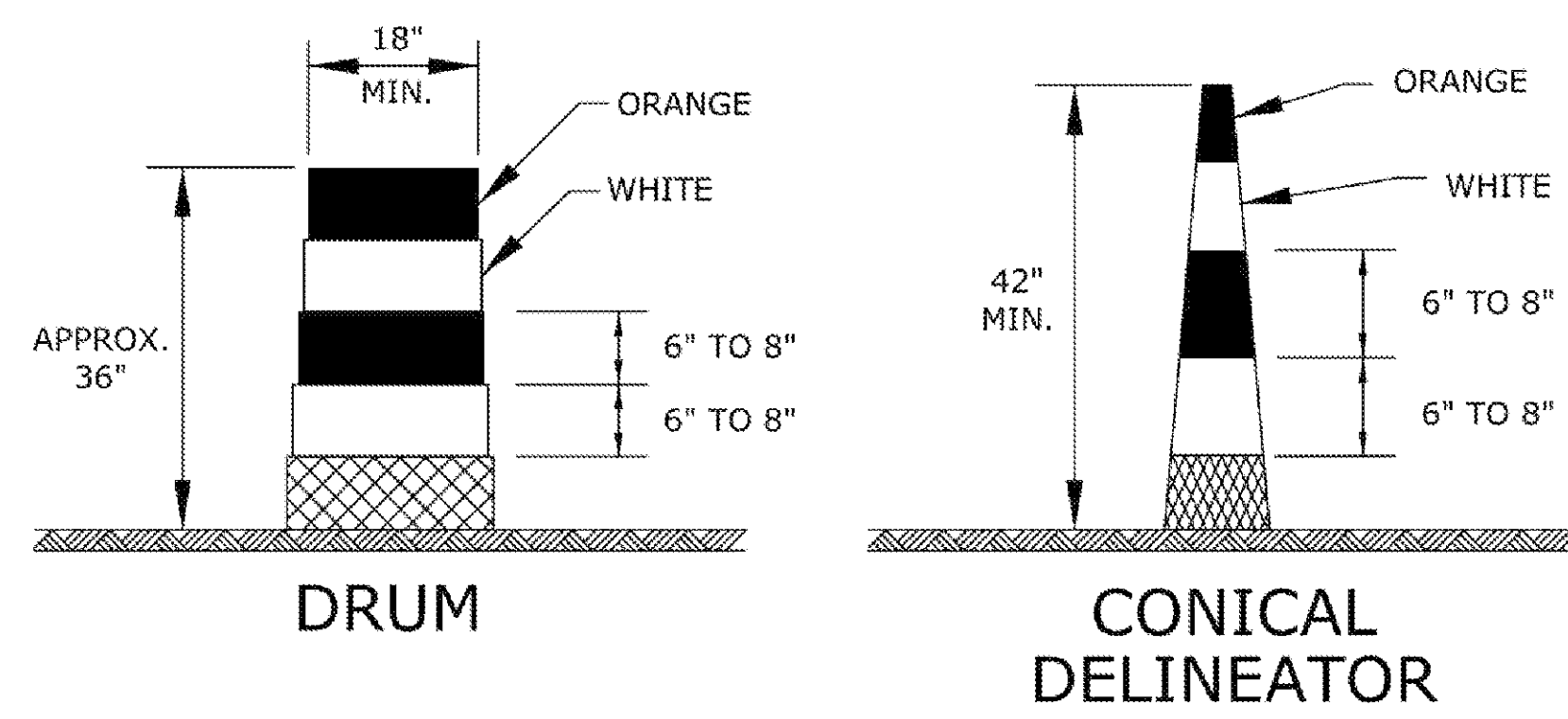
TE700 **9/1/00**

4-20-09 Anthony A. Airoldi

BLM BLM

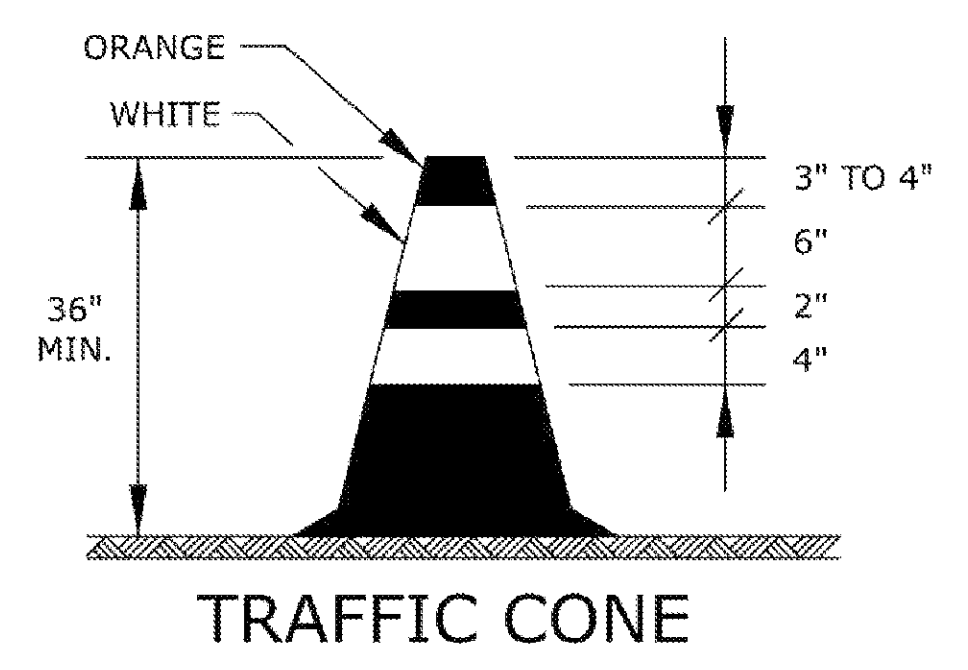
Printed By: 1. 09/15/2009 10:00 AM
 For File: 1. 09/15/2009 10:00 AM
 Job: 1. 09/15/2009 10:00 AM

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	XX-XX XX-XXXX-XX	XXXX	XXX	XXX

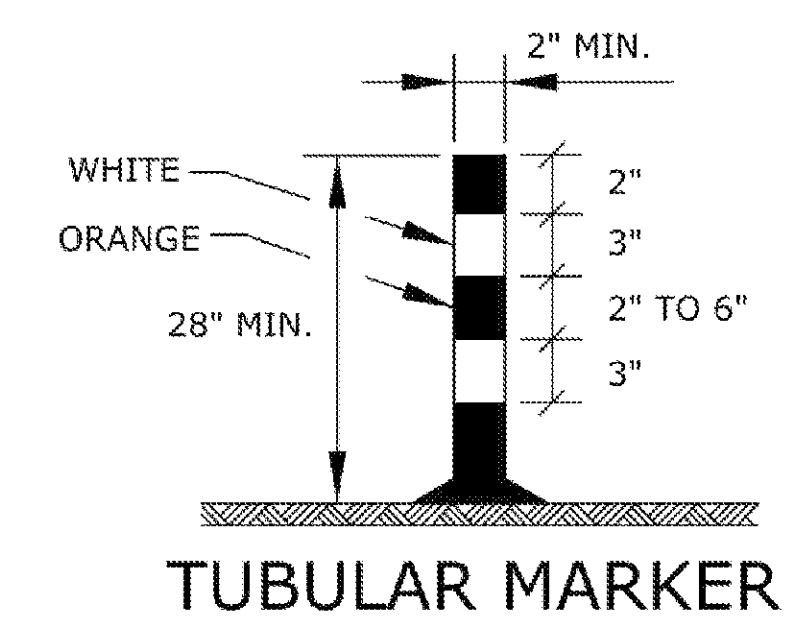


DRUMS AND CONICAL DELINEATORS SHALL HAVE AT LEAST TWO ORANGE AND TWO WHITE 6" TO 8" WIDE RETROREFLECTIVE STRIPES. ADDITIONAL STRIPES MAY BE NON-RETROREFLECTIVE. IF THERE ARE NON-RETROREFLECTIVE SPACES BETWEEN ADJACENT STRIPES, THEY SHALL BE NO MORE THAN 3" WIDE.

ALL RETROREFLECTIVE STRIPES ON DRUMS SHALL BE ASTM TYPE III SHEETING. THE WHITE STRIPES ON CONICAL DELINEATORS SHALL BE ASTM TYPE III SHEETING. ORANGE STRIPES ON ALL CONICAL DELINEATORS SHALL BE FLUORESCENT ORANGE ASTM TYPE IV SHEETING.



TRAFFIC CONES MAY BE USED AS CHANNELIZING DEVICES FOR DAYTIME OPERATIONS ONLY. THEY WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE SUBSIDIARY TO OTHER TRAFFIC CONTROL BID ITEMS. THE ENGINEER MAY REQUIRE THAT TRAFFIC CONES BE SUPPLEMENTED BY OTHER TRAFFIC CONTROL DEVICES IN CERTAIN SITUATIONS.



THE TWO WHITE RETROREFLECTIVE STRIPES SHALL BE ASTM TYPE III SHEETING. STRIPING AS SHOWN FOR UP TO 42".

TAPER FORMULAS:

$L = WS$ FOR SPEEDS OF 45 MPH OR MORE

$L = WS^2/60$ FOR SPEEDS OF 40 MPH OR LESS

WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
S = NUMERICAL VALUE OF POSTED SPEED
PRIOR TO WORK STARTING IN MPH
W = WIDTH OF OFFSET IN FEET

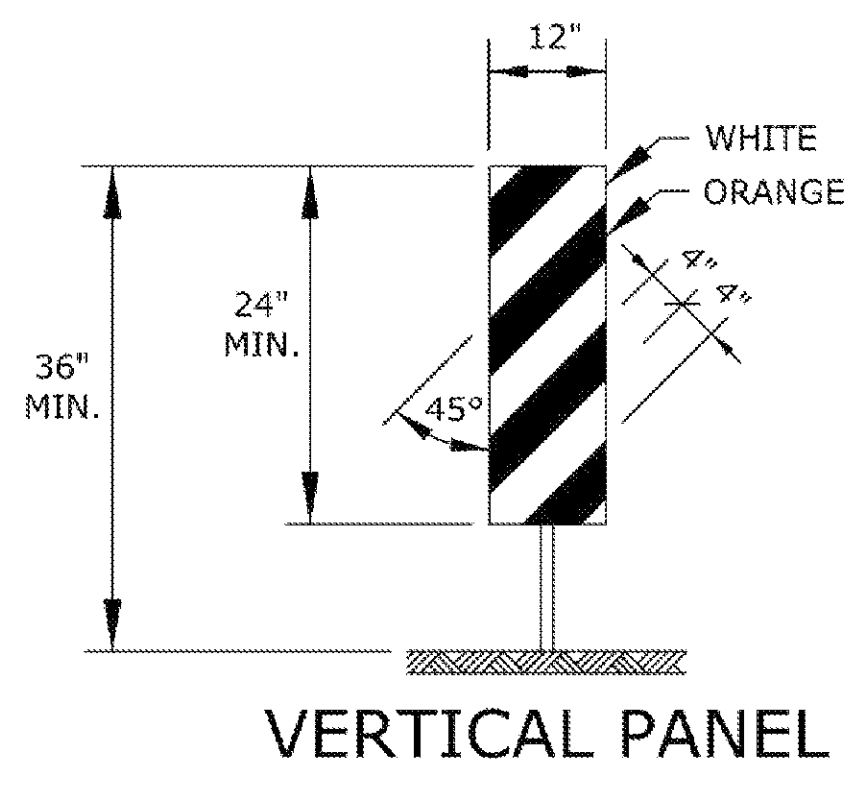
CHANNELIZER PLACEMENT:

(A) THE SPACING BETWEEN DEVICES IN TRANSITION AREA (TAPER) SHOULD NOT EXCEED A DISTANCE IN FEET EQUAL TO 1/2 THE POSTED SPEED LIMIT IN MPH PRIOR TO WORK STARTING.

(B) THE SPACING BETWEEN DEVICES IN THE ADVANCED WARNING AREA AND THE ACTIVITY AREA SHOULD NOT EXCEED A DISTANCE IN FEET EQUAL TO TWO TIMES THE POSTED SPEED LIMIT IN MPH PRIOR TO WORK STARTING.

(C) CHANNELIZING DEVICES SHALL BE PLACED FOR OPTIMUM VISIBILITY, NORMALLY AT RIGHT ANGLES TO THE TRAFFIC FLOW.

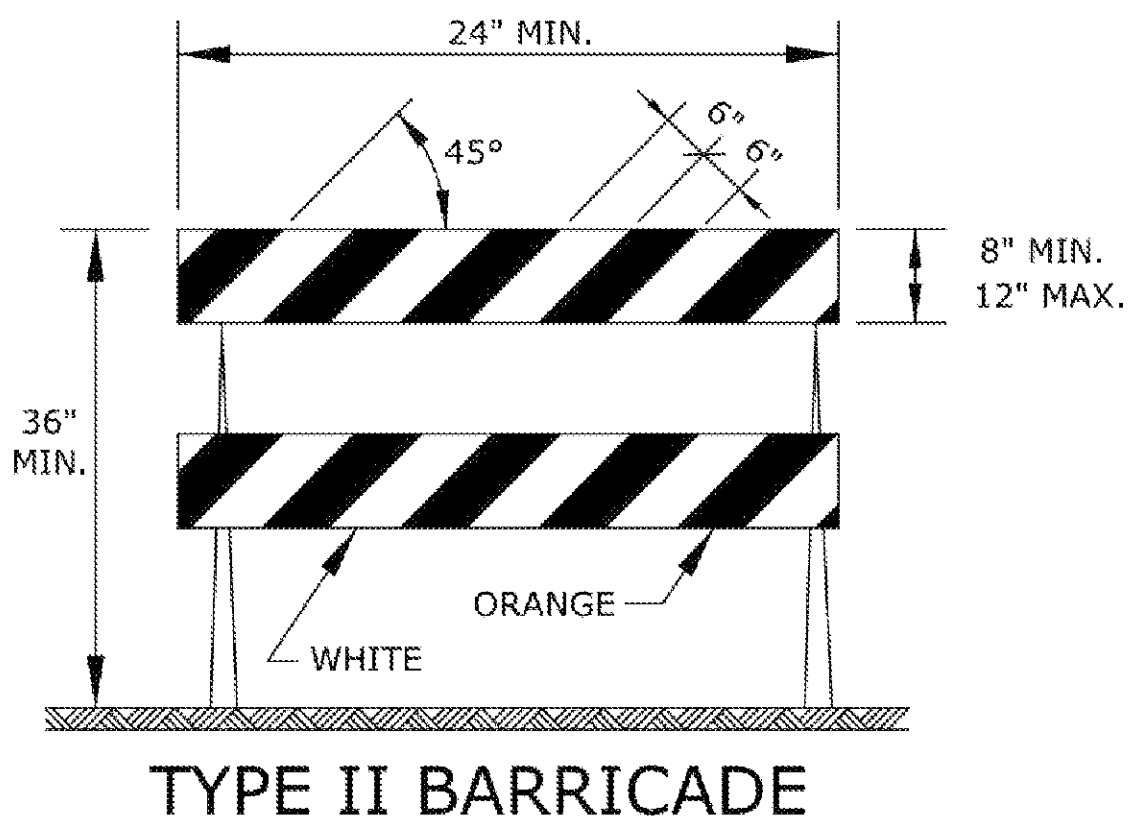
(D) CHANNELIZING DEVICES PLACED ALONG SHOULDER EDGES OR IN DROPOFFS SHALL HAVE A MINIMUM OF 24" FROM THE TOP OF THE CHANNELIZING DEVICE TO THE TOP OF THE PAVEMENT.



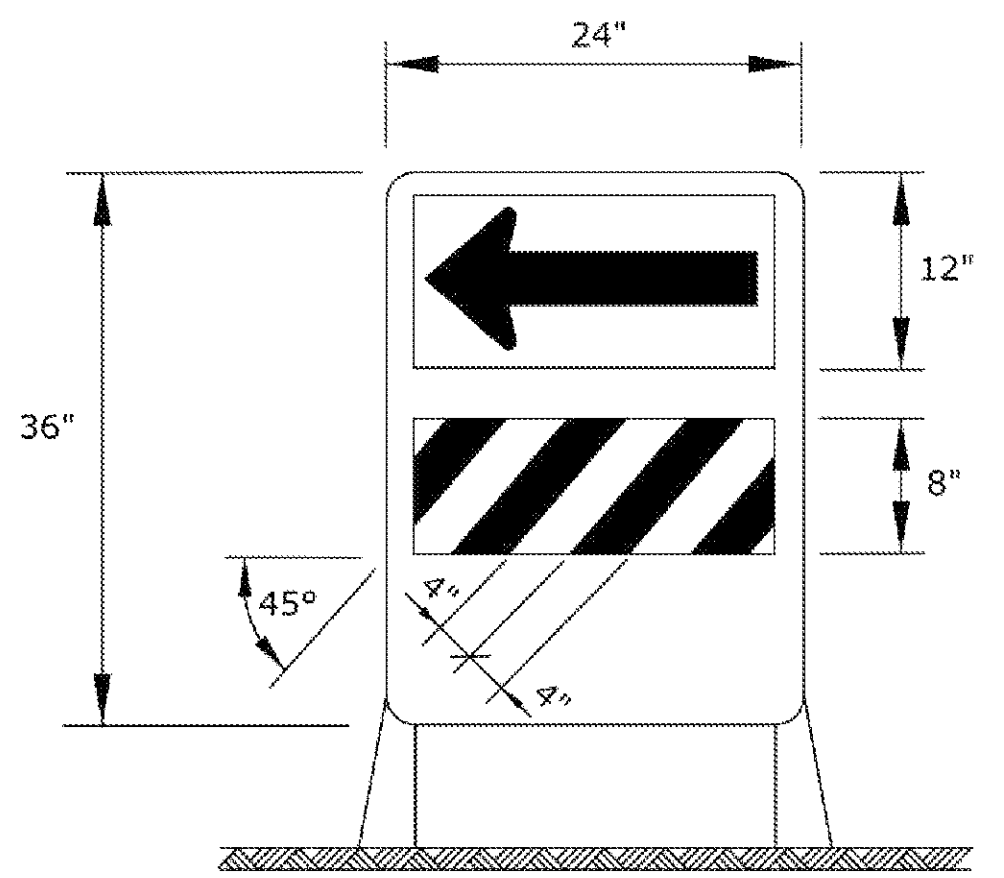
THE ENTIRE AREA OF VERTICAL PANELS, BOTH FRONT AND BACK, SHALL HAVE ASTM TYPE III SHEETING. THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

ITEM	LOCATION	CHANNELIZING DEVICES								
		CROSS-OVERS	SHOOLY DIVERSIONS	TANGENTS	TAPERS	RAMPS	HEAD TO HEAD	OBJECT IDENTIFIER	LEAD IN DEVICES	GORES
PORTABLE	DRUMS	YES	YES	YES	YES	YES	(1)	YES	YES	YES
	CONICAL DELINEATORS	YES	YES	YES	YES	YES	(1)	YES	YES	YES
	VERTICAL PANELS	(2)	(2)	(2)	(2)	(2)	(1,2)	YES	(2)	(2)
	DIRECTION INDICATOR BARRICADE	NO	NO	NO	YES	NO	NO	NO	NO	NO
	TYPE II BARRICADE	(2)	(2)	(2)	(2)	NO	NO	YES	NO	NO
FIXED	TUBULAR MARKERS	(3)	(3)	(3)	NO	(3)	YES	NO	YES	YES
	VERTICAL PANELS	(3)	(3)	(3)	(3)	(3)	(3)	YES	(2,3)	(2)

- (1) NOT ALLOWED ON CENTERLINE DELINEATION ALONG FREEWAYS OR EXPRESSWAYS.
- (2) THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.
- (3) MAY BE USED UPON THE APPROVAL OF THE ENGINEER.



FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. THE ENTIRE AREA OF BARRICADE RAILS, BOTH FRONT AND BACK, SHALL BE ASTM TYPE III SHEETING. THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



THE ARROW PANEL SHALL BE BLACK ON FLUORESCENT ORANGE ASTM TYPE IV SHEETING. THE STRIPES SHALL BE ORANGE AND WHITE ASTM TYPE III SHEETING SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. THE DIRECTION INDICATOR BARRICADE SHALL BE USED IN SERIES TO DIRECT THE MOTORIST INTO THE INTENDED LANE OF TRAVEL. THE ARROW PANEL SHOULD NOT BE VISIBLE TO OPPOSING TRAFFIC.

Plotted: 24-OCT-2012 07:53 Traffic
Drawn By: jmadrid
File: te702.dgn

3	10/16/12	Added Lead In Devices into Matrix Table	J.A.M.	K.P.
2	10/4/11	Added Dimension To Tubular Marker Detail	J.A.M.	K.P.
1	4/20/09	Channelizer Placement & Traffic Cone Detail	J.A.M.	A.A.A.
NO.	DATE	REVISIONS	BY	APP'D

KANSAS DEPARTMENT OF TRANSPORTATION

CHANNELIZING DEVICES

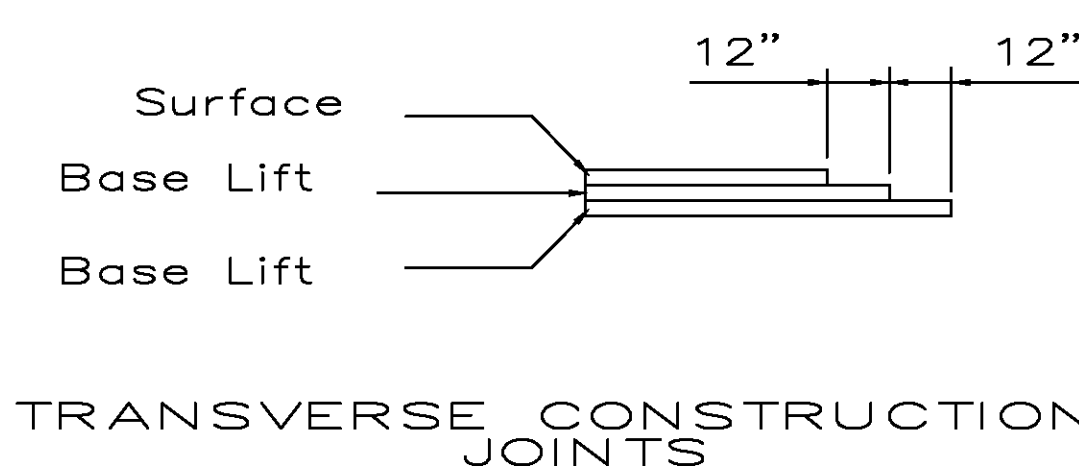
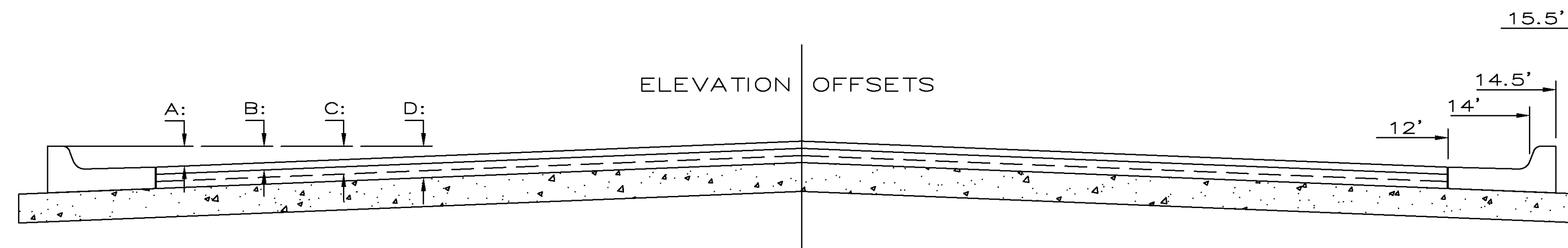
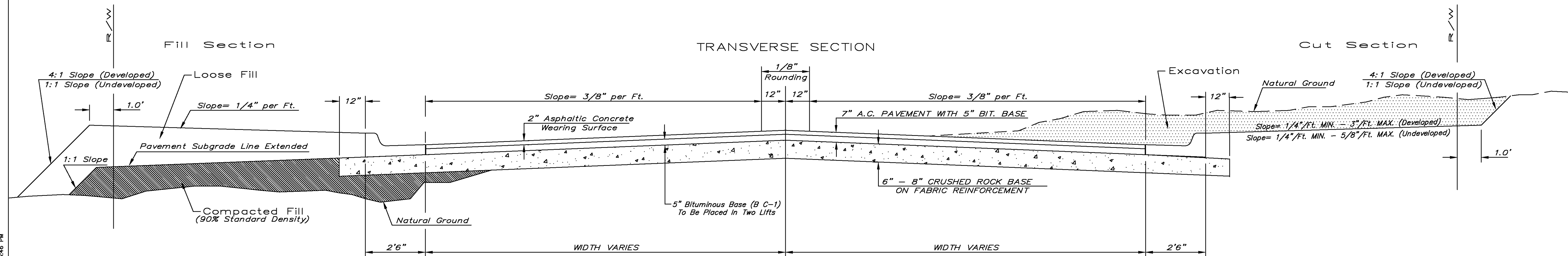
TE702

DESIGNED	L.E.R.	10/16/12	APP'D	Kristina Pyle
DESIGN CK.	DETAIL CK.	S.A.H.	QUANTITIES	TRACED
			QUAN. CK.	TRACE CK.

KDOT Graphics Certified 10-23-2012 Sh. No. XXX

KDOT Graphics Certified

TYPICAL 7" PAVEMENT W/ REINFORCED CRUSHED ROCK BASE



Transverse construction joints shall be constructed in flexible base pavements at locations where pavement joints existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

General Notes

FABRIC BASE REINFORCEMENT SHALL BE B X 1100 GEOGRID AS MANUFACTURED BY TENSAR CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CRUSHED ROCK SHALL BE UNIFORMLY GRADED FROM 1-1/2" MAXIMUM SIZE TO NOT MORE THAN 10% PASSING A NO. 200 SIEVE. ROCK QUALITY SHALL BE THE SAME AS SPECIFIED FOR COARSE AGGREGATE FOR CONCRETE MIXES.

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

Type II - Combined Curb & Gutter (6 5/8")

	Distance from Back of Curb toward Center Line														
	12" beyond	Back of Curb	2.5	4.5	6.5	8.5	10.5	12.5	14.5	15.5	17.5	17.5	18.5	18.5	20.5
A: Top of Curbs to Top of Surface Lift	-	-	0.50	0.44	0.38	0.31	0.25	0.19	0.13	0.09	0.03	0.04	0.00	0.01	-0.05
B: Top of Curbs to Top of Upper Base Lift	-	-	0.67	0.60	0.54	0.48	0.42	0.35	0.29	0.26	0.20	0.20	0.17	0.17	0.11
C: Top of Curbs to Top of Lower Base Lift	-	-	0.83	0.77	0.71	0.65	0.58	0.52	0.46	0.43	0.36	0.36	0.33	0.33	0.28
D: Top of Curbs to Top of Rock Base	1.19	1.16	1.08	1.02	0.96	0.90	0.83	0.77	0.71	0.68	0.61	0.61	0.58	0.58	0.53

CL of 35' pavement CL of 37' CL of 41' pavement pavement

Type I - Combined Curb & Gutter (6")

	Distance from Back of Curb toward Center Line														
	12" beyond	Back of Curb	2.5	4.5	6.5	8.5	10.5	12.5	14.5	15.5	17.5	17.5	18.5	18.5	20.5
A: Top of Curbs to Top of Surface Lift	-	-	0.35	0.29	0.23	0.17	0.10	0.04	-0.02	-0.05	-0.11	-0.10	-0.15	-0.14	-0.20
B: Top of Curbs to Top of Upper Base Lift	-	-	0.52	0.46	0.40	0.33	0.27	0.21	0.15	0.11	0.05	0.05	0.02	0.02	-0.04
C: Top of Curbs to Top of Lower Base Lift	-	-	0.69	0.63	0.56	0.50	0.44	0.38	0.31	0.28	0.22	0.22	0.19	0.19	0.13
D: Top of Curbs to Top of Subgrade	1.05	1.02	0.94	0.88	0.81	0.75	0.69	0.63	0.56	0.53	0.47	0.47	0.44	0.44	0.38

CL of 35' pavement CL of 37' CL of 41' pavement pavement



7" ASPHALTIC CONCRETE ON REINFORCED CRUSHED ROCK BASE

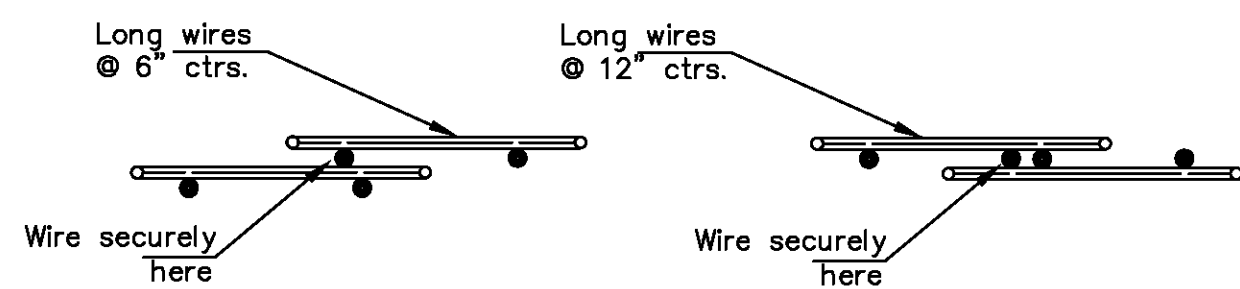
CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER 241 PPP	OCA NUMBER 607879	DATE 03/2002
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CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501	SHEET 9
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FILE LOCATION: S:\Drawing Files\Project AMG 9-16-13\13\OT #0356R\DWG\03-0356 PPP.dwg TAB NAME: Asphalt Print Details USER: gspodich SWED: 12/2/2014 3:34 PM PLOTTED: 12/2/2014 3:46 PM

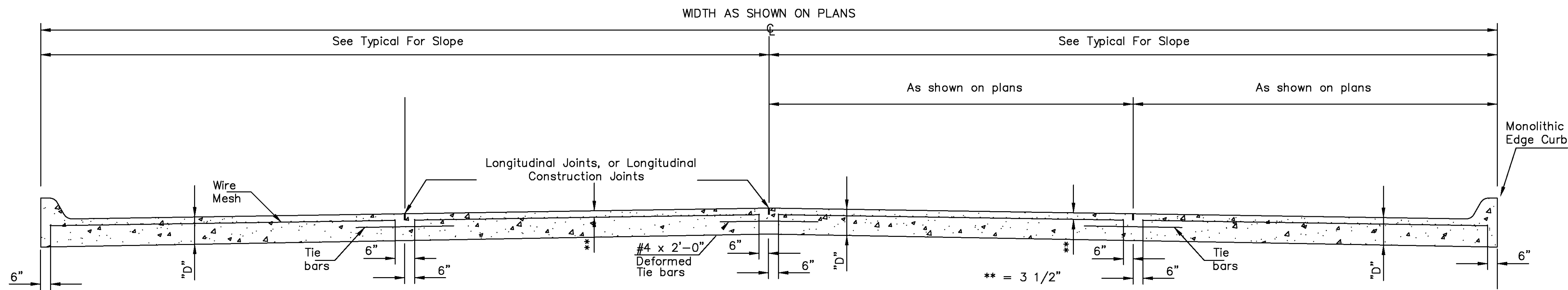
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DETAIL OF LAP FOR WIRE MESH

NOTE: The lap shall extend beyond the first transverse wire of each sheet.
The sheets shall be wired securely at the edges and at intervals not to exceed 2'-6" for the full width of the sheet. Approx. weight of wire mesh = 44 lbs. per 100 sq. ft.
Other methods for fastening the sheets of wire mesh at the laps may be used with the approval of the Engineer.

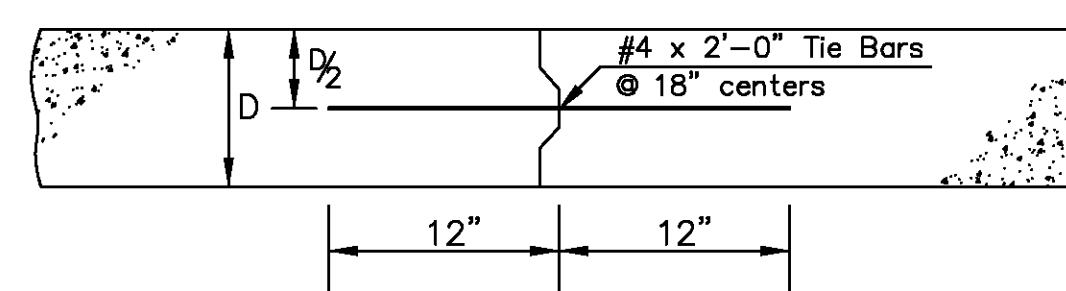
NOTE: CONTRACTION JOINTS AND EXPANSION JOINTS SHALL BE FORMED IN THE MONOLITHIC CURB AT ALL LOCATIONS WHERE SUCH JOINTS ARE PLACED IN THE PAVEMENT. ADDITIONAL CONTRACTION JOINTS SHALL BE CUT IN THE MONOLITHIC CURB IN UNIFORM LENGTHS NOT EXCEEDING TEN FOOT INTERVALS BETWEEN THOSE JOINTS WHICH COINCIDE WITH JOINTS IN THE PAVEMENT.



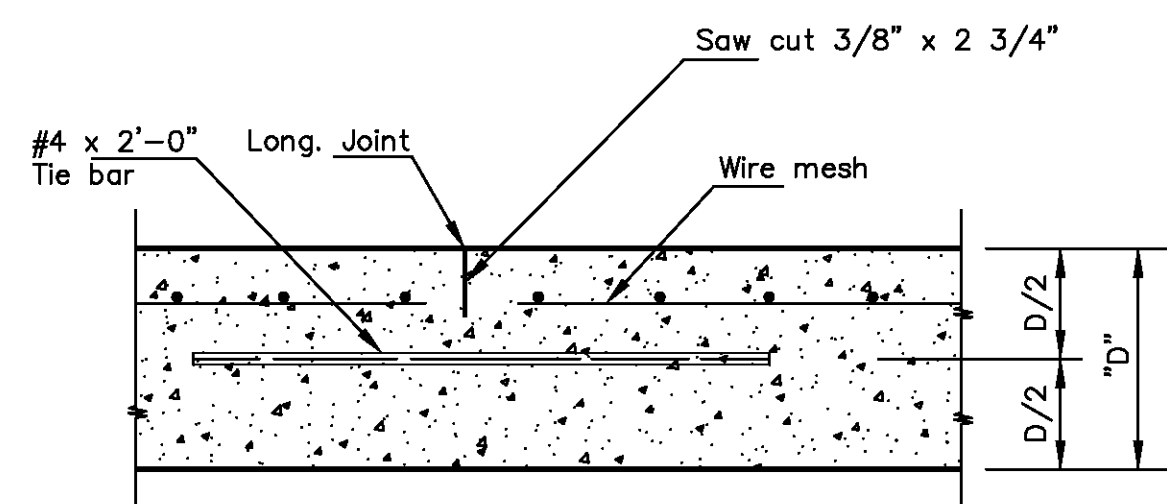
TRANVERSE SECTION

** Note: The 3 1/2" as shown on standard sheet is a nominal dimension and may be subject to a normal unevenness due to placement operations.

Note: A Construction Joint is required when the pour has been interrupted for a substantial length of time or at the end of a day's pour. Transverse Construction Joints shall be sawed and filled as specified for contraction joints.

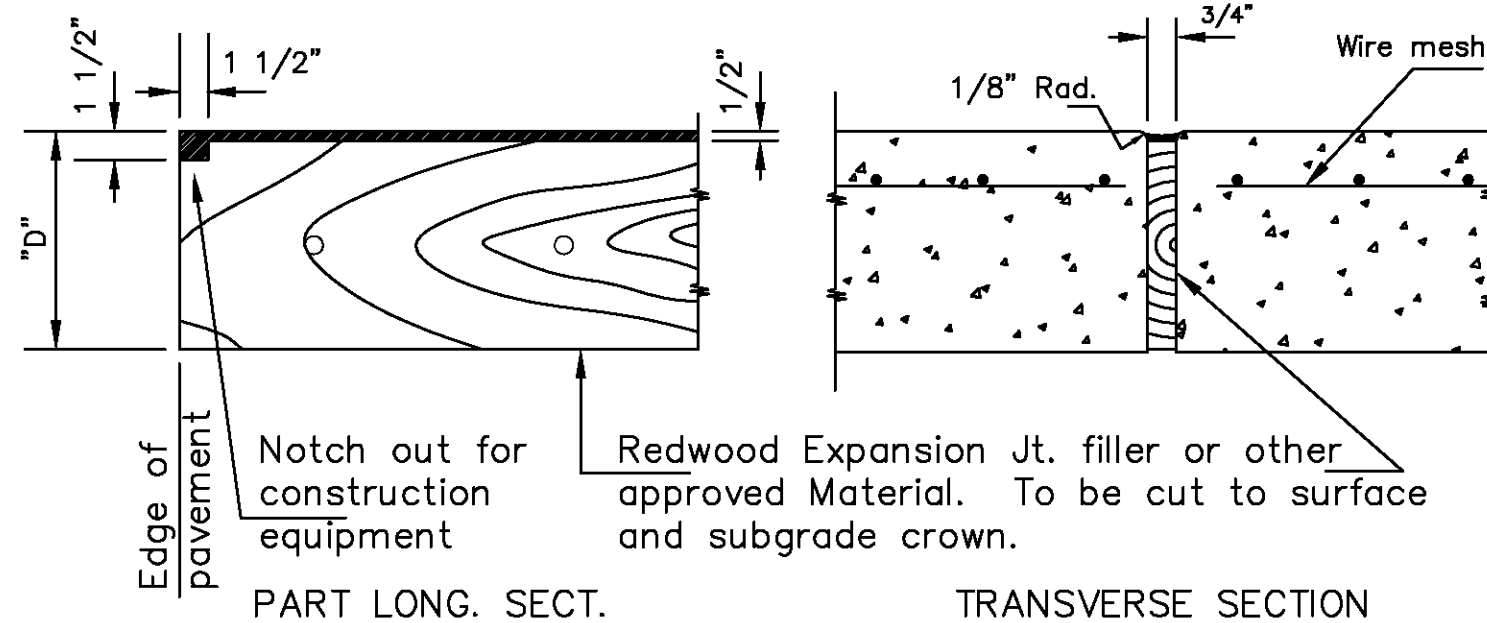


CONSTRUCTION JOINT



LONGITUDINAL JOINT DETAIL TRANSVERSE SECTION

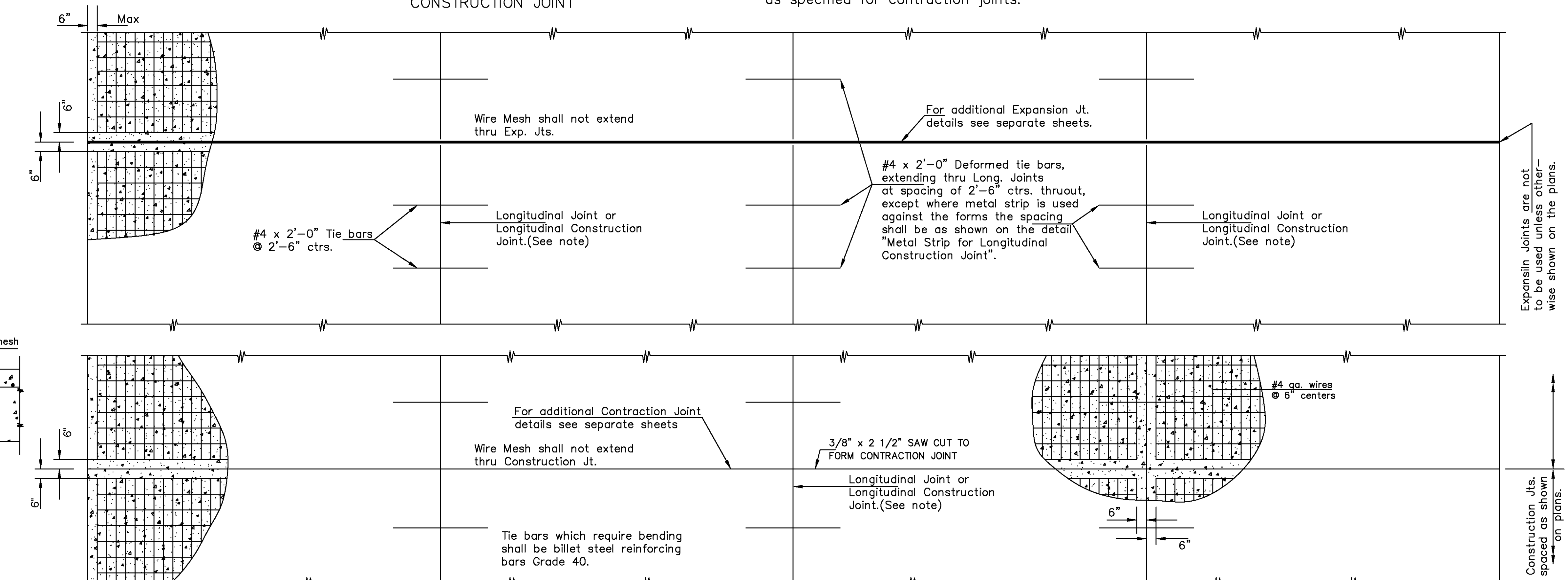
NOTE: Longitudinal joint formed by sawing 2 3/4" deep and 3/8" wide and filled as specified in standard specifications.



SECTION OF RECESSED FORM LEG

EXPANSION JOINT DETAIL

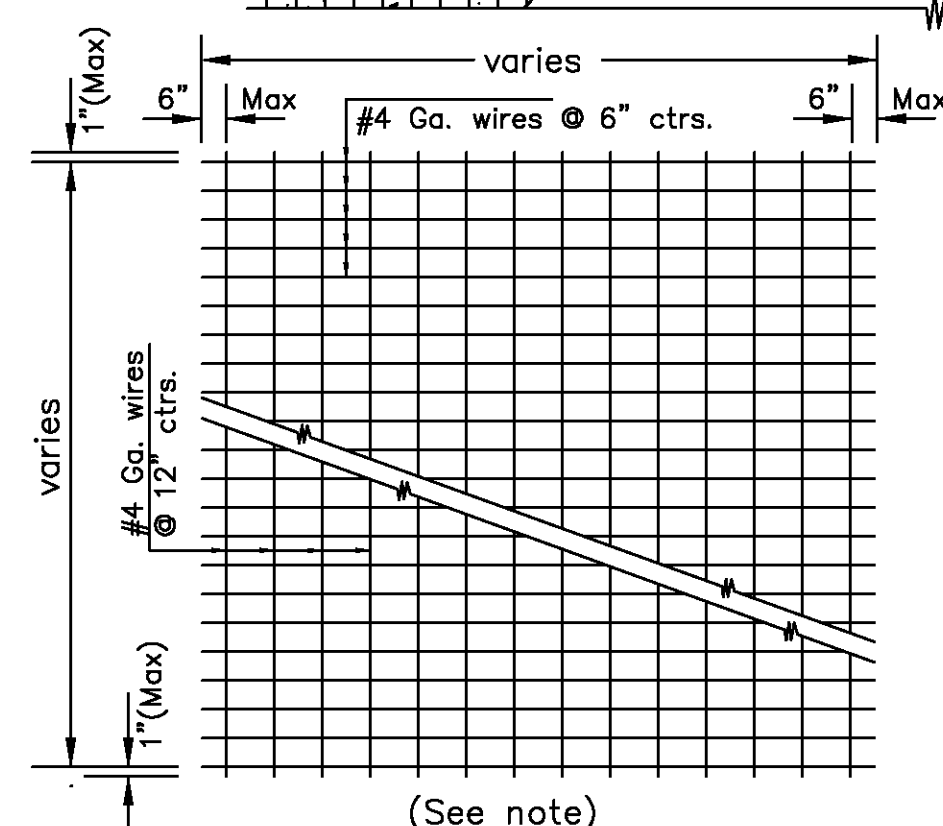
NOTE: EXPANSION JOINT MATERIAL MUST BE SECURELY HELD IN PLACE BY USING METAL STAKES, METAL PINS, OR BY OTHER APPROVED METHODS SO THAT WHEN THE PAVEMENT IS COMPLETED THE EXPANSION MATERIAL WILL BE PERPENDICULAR TO THE SURFACE OF THE COMPLETED PAVEMENT. CONCRETE SHALL BE HAND PLACED ON EACH SIDE OF THE EXPANSION JOINT MATERIAL IN A MANNER THAT WILL PRECLUDE DISPLACEMENT OF THE EXPANSION JOINT MATERIAL.



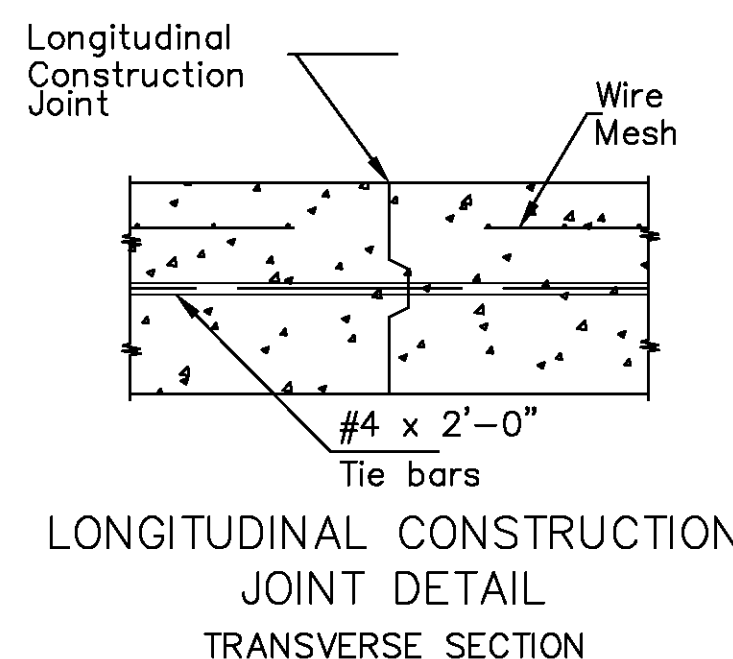
PLAN

Expansion Joints are not to be used unless otherwise shown on the plans.

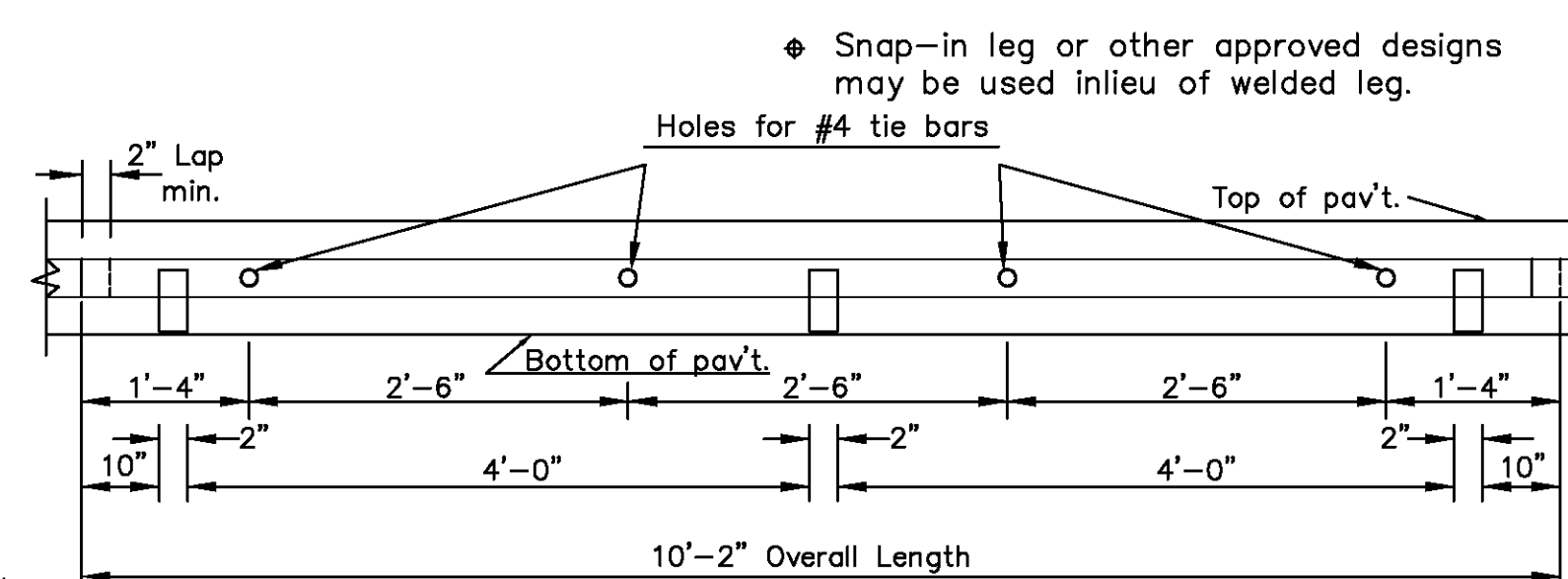
Construction Jts. spaced as shown on plans.



TYPICAL SHEET OF WELDED WIRE MESH



LONGITUDINAL CONSTRUCTION JOINT DETAIL TRANSVERSE SECTION



TO BE USED ONLY AGAINST FORMS. SHALL NOT EXTEND THROUGH CONTRACTION OR EXPANSION JOINTS. OTHER TYPES OF CONSTRUCTION SHALL BE PERMITTED WITH THE APPROVAL OF THE ENGINEER.



CONCRETE PAVEMENT DETAILS		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER 241 PPP	OCA NUMBER 607879	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 10