

CITY OF WICHITA, KANSAS

MICHAEL E. LINDEBAK, P.E. - CITY ENGINEER

STREET IMPROVEMENTS

PARKRIDGE - S.L. LOT 38, BLK. 1 & RADIALLY FROM SE CORNER LOT 17, BLK. 6, ECHO HILLS 2ND ADDITION TO THE S. L. 21ST STREET N.

PARKRIDGE COURT - E.L. PARKRIDGE TO & INCLUDING CUL-DE-SAC SERVING LOTS 4-9, BLK. 1

PARKRIDGE COURT - E.L. PARKRIDGE TO & INCLUDING CUL-DE-SAC SERVING LOTS 10-23, BLK. 1

PARKRIDGE COURT - E.L. PARKRIDGE TO & INCLUDING CUL-DE-SAC SERVING LOTS 24-36, BLK. 1

(WOODBRIIDGE 2ND ADDITION)

CITY OF WICHITA PROJECT NO. 472-76-245-81515-000-000-001

INDEX OF SHEETS

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- 4 WOODBRIDGE 2ND ADDITION PLAT
- 5-10 PLAN SHEETS
- 11 VALLEY GUTTER & WHEELCHAIR RAMP DETAILS
- 12 STANDARD DRIVE ENTRANCES
- 13-20 CROSS SECTIONS

BENCH MARK

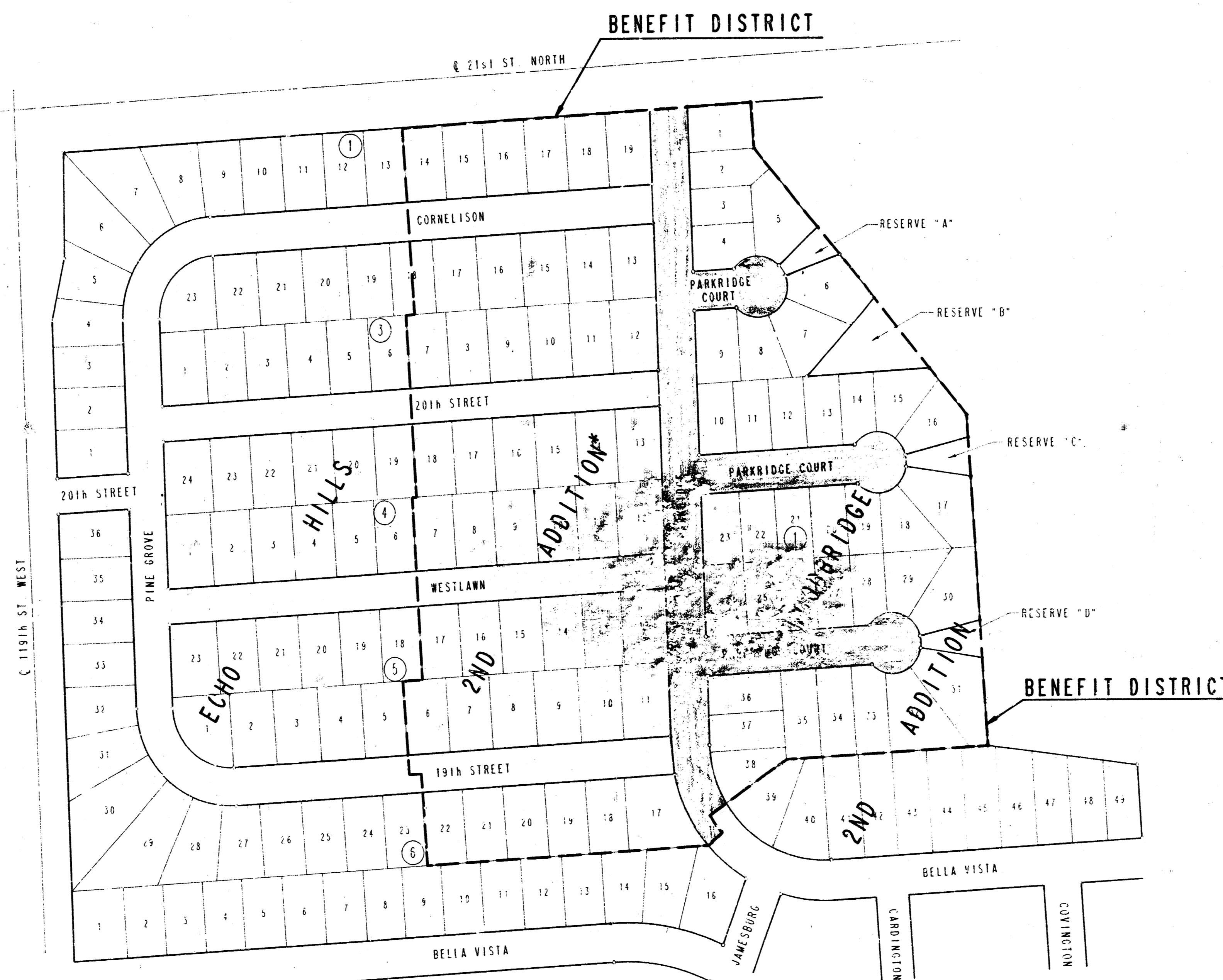
BM #1 CHIS. "0" 1" CENTER SOUTH HEADWALL ON 21ST ST. N. AT NE COR. WOODBRIDGE 2ND ADDITION. ELEV. 163.55

SCALE: 1"=150'

EARTHWORK

EXCAVATION	
X-SECTIONS	3,807.0 CU. YDS.
10X	381.0 CU. YDS.
TOTAL	4,188.0 CU. YDS.
COMPACTED FILL	
X-SECTIONS	119.0 CU. YDS.
10X	12.0 CU. YDS.
TOTAL	131.0 CU. YDS.
LOOSE FILL	
X-SECTIONS	369.0 CU. YDS.
10X	37.0 CU. YDS.
TOTAL	406.0 CU. YDS.
MANIPULATED FILL	
X-SECTIONS	65.0 CU. YDS.
10X	7.0 CU. YDS.
TOTAL	72.0 CU. YDS.

9,341.42 SQ. YDS. SUBGRADE STAB. MANIPULATION



GENERAL NOTES

UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, AND ETCETERA ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO OR DURING CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

A SAW CUT OF AT LEAST ONE-HALF THE DEPTH OF EXISTING SURFACE COURSES OR ONE-FOURTH THE DEPTH OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ABUTS AN EXISTING SURFACE COURSE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THAT SURFACE OR PAVEMENT IS REQUIRED. SAWED JOINT TO FACILITATE REMOVAL WITHIN THREE (3) FEET OF EXISTING JOINTS WILL NOT BE PERMITTED AND FOR SUCH INSTANCES THE LIMITS OF REMOVAL SHALL EXTEND TO THE EXISTING JOINT. SUCH SAW CUTS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE REMOVAL OF THE SURFACE OR PAVEMENT.

RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND PAVEMENT REMOVAL WHICH IS TO BE WASTED SHALL BE DISPOSPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE, AND SITE LOCATION. LOCATIONS THAT, IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED.

LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH THE EXISTING GROUND SURFACE.

CONTRACTOR SHALL GIVE PROPERTY OWNERS ADJUTING THIS PROJECT, WHOSE YARDS WILL BE LOWER THAN THE NEW FINISHED GRADE ELEVATIONS AT THE RIGHT-OF-WAY LINE, AN OPPORTUNITY TO UTILIZE EXCESS EXCAVATED MATERIAL FROM THE PROJECT TO REGRADE THEIR YARDS TO DRAIN TO THE NEW PAVEMENT. CONTRACTOR WILL BE REQUIRED TO DUMP AND SPREAD THE EXCESS MATERIAL BY THE SPECIFICATIONS WHEN REQUESTED BY THE PROPERTY OWNER. THE CONTRACTOR SHALL ASCERTAIN THAT A DIRT ORDER FORM HAS BEEN PROPERLY EXECUTED BY THE PROPERTY OWNER BEFORE ANY SUCH EXCESS MATERIAL IS DELIVERED TO SUCH PROPERTIES.

THE CONTRACTOR WILL BE PERMITTED TO BID ONLY ONE OF THE ALTERNATE TYPES OF SUBGRADE TREATMENT. THE TYPE BID BY THE SUCCESSFUL BIDDER WILL BE THE TYPE OF SUBGRADE TREATMENT USED TO CONSTRUCT THE PROJECT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

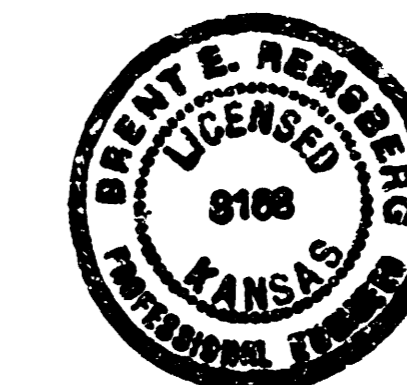
ADEQUATE TRAFFIC CONTROL SHALL BE PROVIDED ON 21ST STREET IN THE VICINITY OF PARKRIDGE STREET DURING CONSTRUCTION.

ROLL CURB SHALL BE DEPRESSED THROUGH ANY DRIVES THAT ARE TO BE CONSTRUCTED AS A PART OF THIS CONTRACT.

* THE STREET GEOMETRICS SHOWN IN THE PLANS ALONG THE WEST SIDE OF PARKRIDGE ARE BASED ON A PROPOSED REPLAT OF ECHO HILLS 2ND ADDITION. CONSTRUCTION SHALL CONFORM TO THE PROPOSED PLATTED DIMENSIONS RATHER THAN EXISTING RIGHT-OF-WAY WIDTHS. SEE SHEET NO. 4 FOR A PORTION OF THE PROPOSED REPLAT.

FEBRUARY, 1986

PLANS PREPARED BY
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

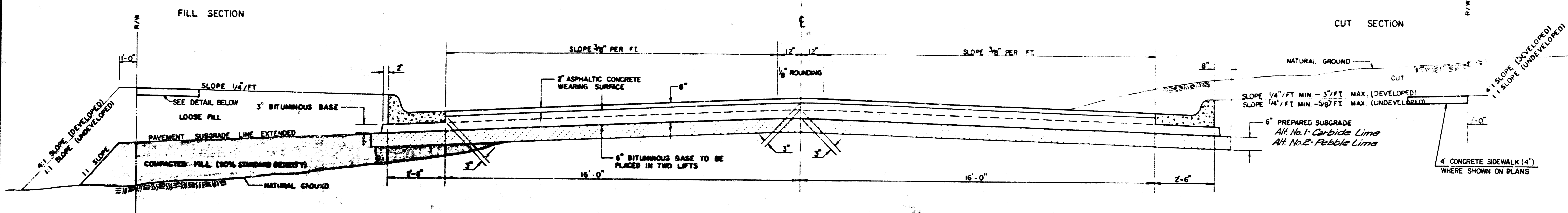


APPROVED	DATE
<div style="display: flex; justify-content: space-between; align-items: center;"> <div style="text-align: center;"> </div> <div style="text-align: right;"> <p>M.E. LINDEBAK, P.E. CITY ENGINEER WICHITA, KANSAS</p> </div> </div>	

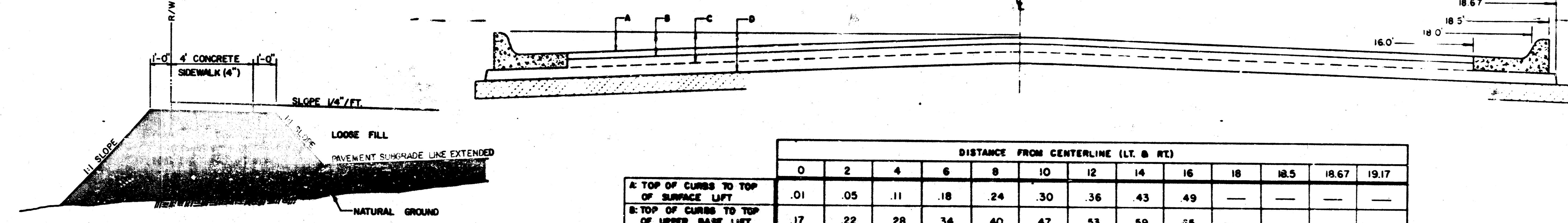
TYPICAL 37' PAVEMENT DETAILS

2/20

TRANSVERSE SECTION



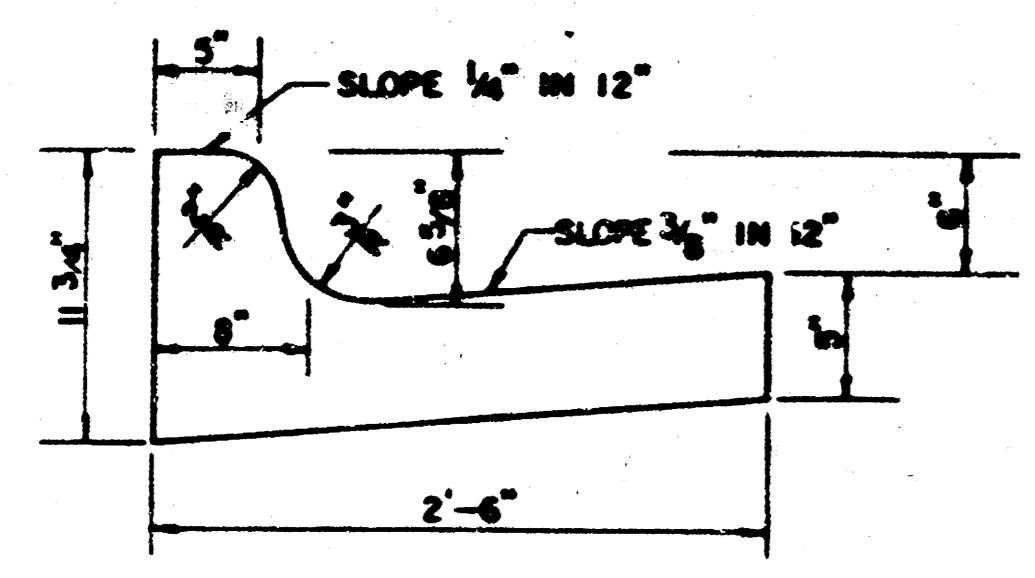
ELEVATION OFFSETS



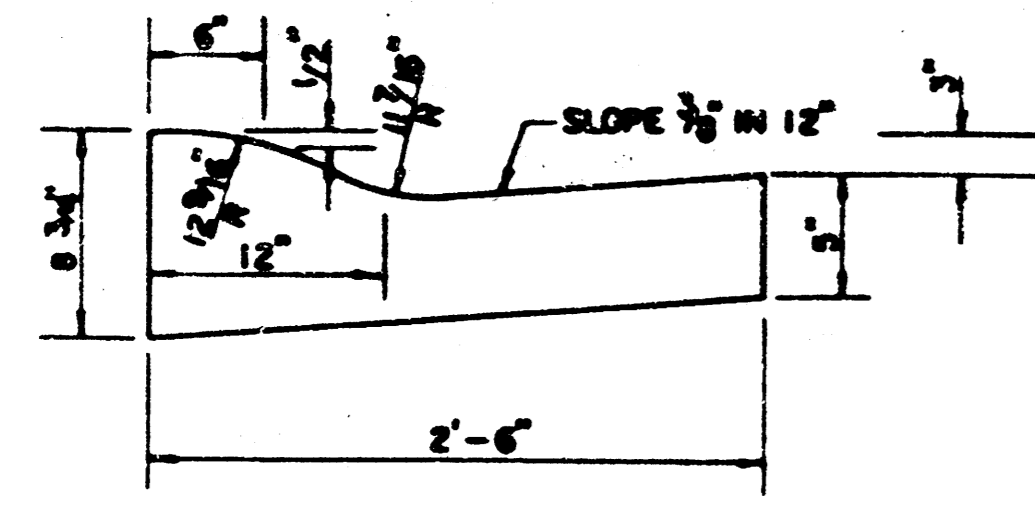
	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0	2	4	6	8	10	12	14	16	18	18.5	18.67	19.17
A: TOP OF CURBS TO TOP OF SURFACE LIFT	.01	.05	.11	.18	.24	.30	.36	.43	.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	.17	.22	.28	.34	.40	.47	.53	.59	.65	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	.42	.47	.53	.59	.65	.72	.78	.84	.90	.97	.98	.99	—
D: TOP OF CURBS TO TOP OF SUBGRADE	.67	.72	.78	.84	.90	.97	1.03	1.09	1.15	1.22	1.23	1.24	1.25

COMPACTED FILL DETAIL WHEN SIDEWALK IS CONSTRUCTED

COMBINED CURB & GUTTER



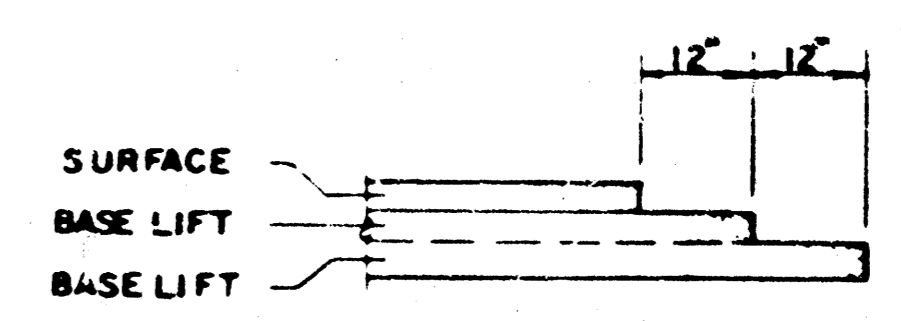
ROLL TYPE COMBINED CURB & GUTTER



GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 8" ASPHALTIC CONCRETE (6" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 3" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

TRANSVERSE CONSTRUCTION JOINTS

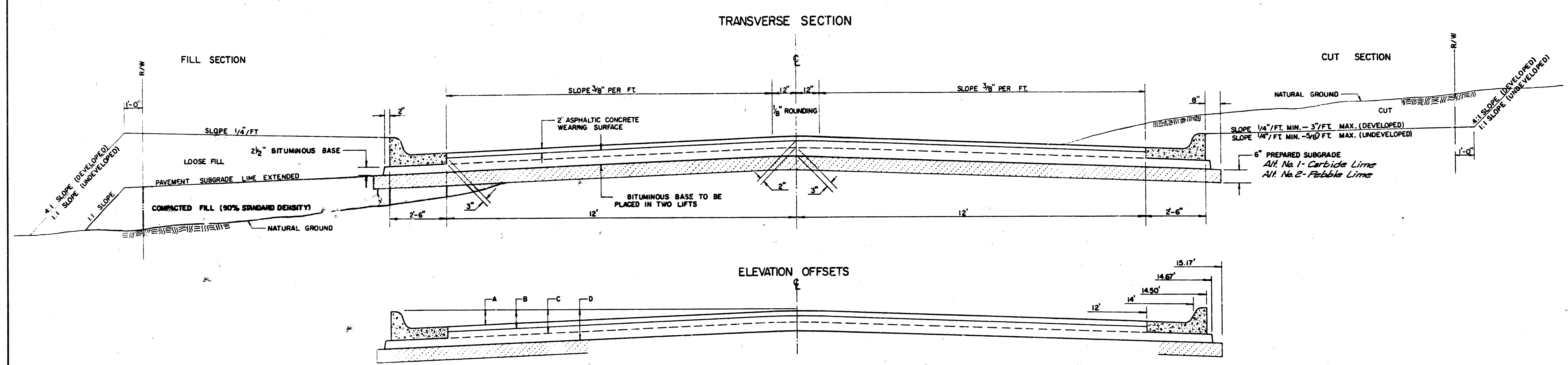


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 8" ASPHALTIC CONCRETE (6" BITUMINOUS BASE).

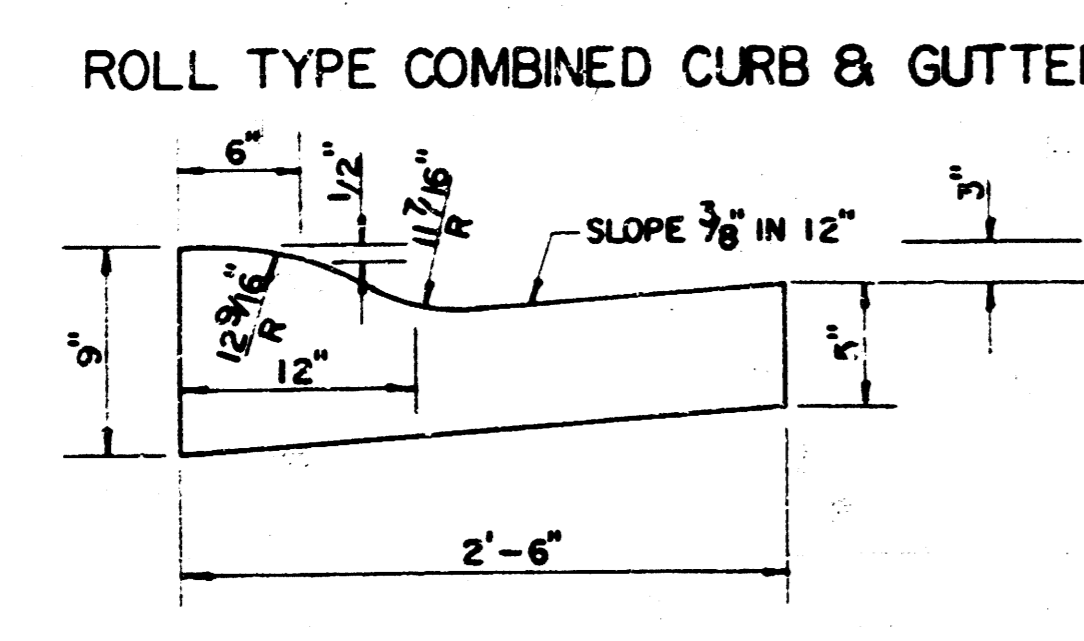
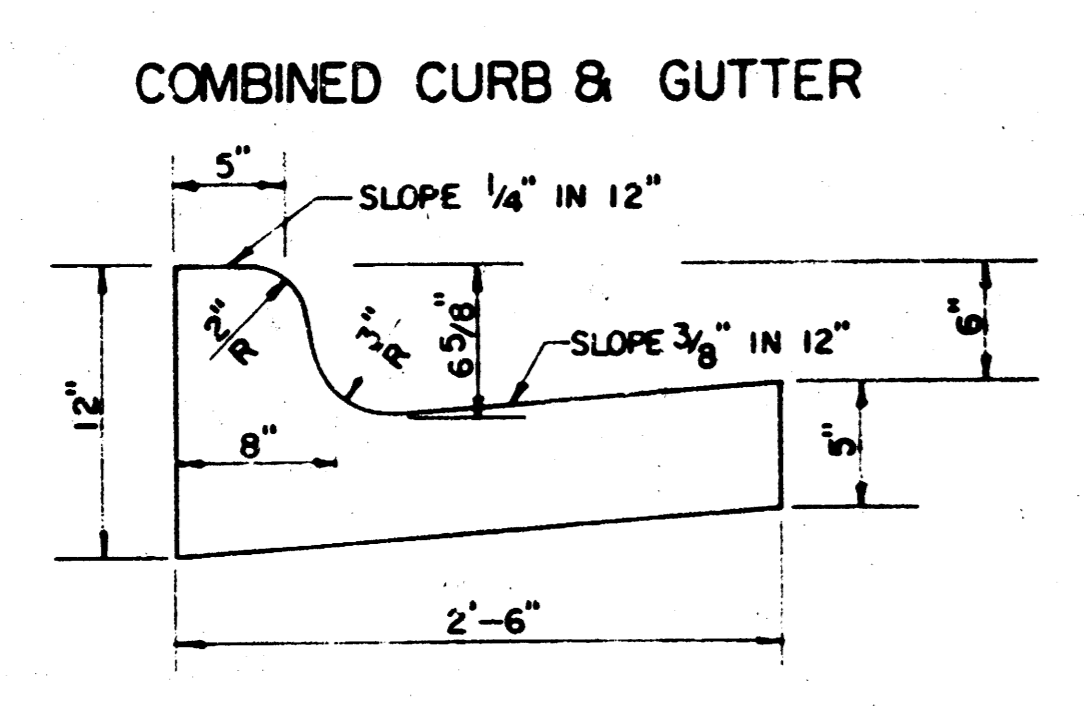
8 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 6 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 472 76 245 81515 000 000 001

TYPICAL 29' PAVEMENT DETAILS

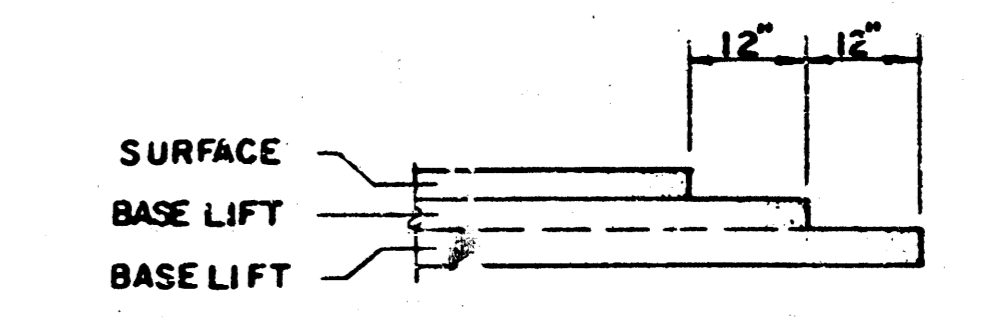
3/20



	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	14.67'	15.17'	
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	—	—	—	—	
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	—	—	—	—	
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.47	0.52	0.60	0.68	0.71	0.75	0.83	0.90	0.98	1.00	1.01	—	
D: TOP OF CURBS TO TOP OF SUBGRADE	0.72	0.77	0.84	0.91	0.94	0.98	1.05	1.12	1.19	1.21	1.21	1.23	



TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BIDDING PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

GENERAL NOTES

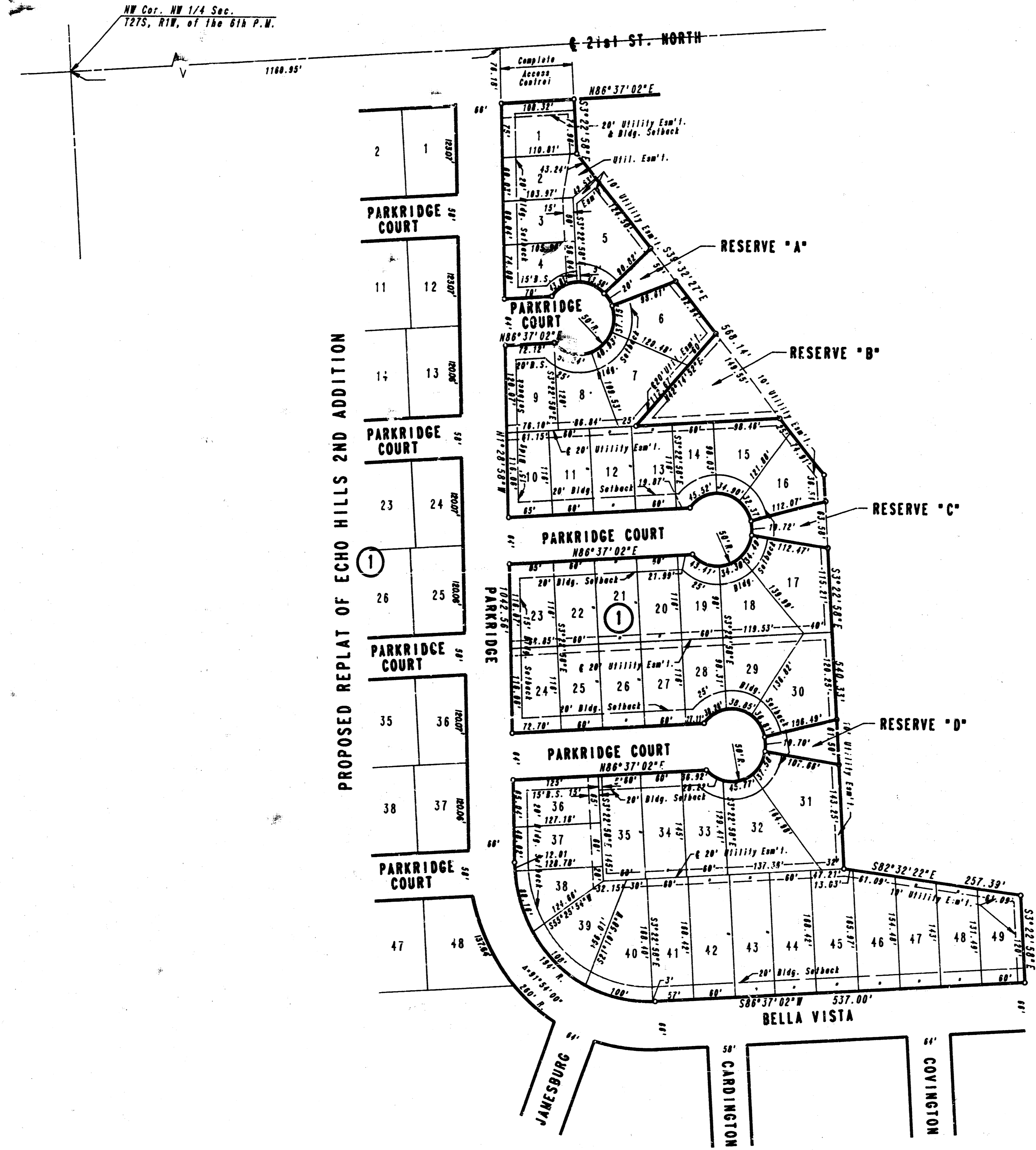
- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
 CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 472 76 245 81513 000 000 001 3/20

WOODBIDGE 2ND ADDITION

TO WICHITA, SEDGWICK COUNTY, KANSAS

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-87*15-000-000-001	4	20



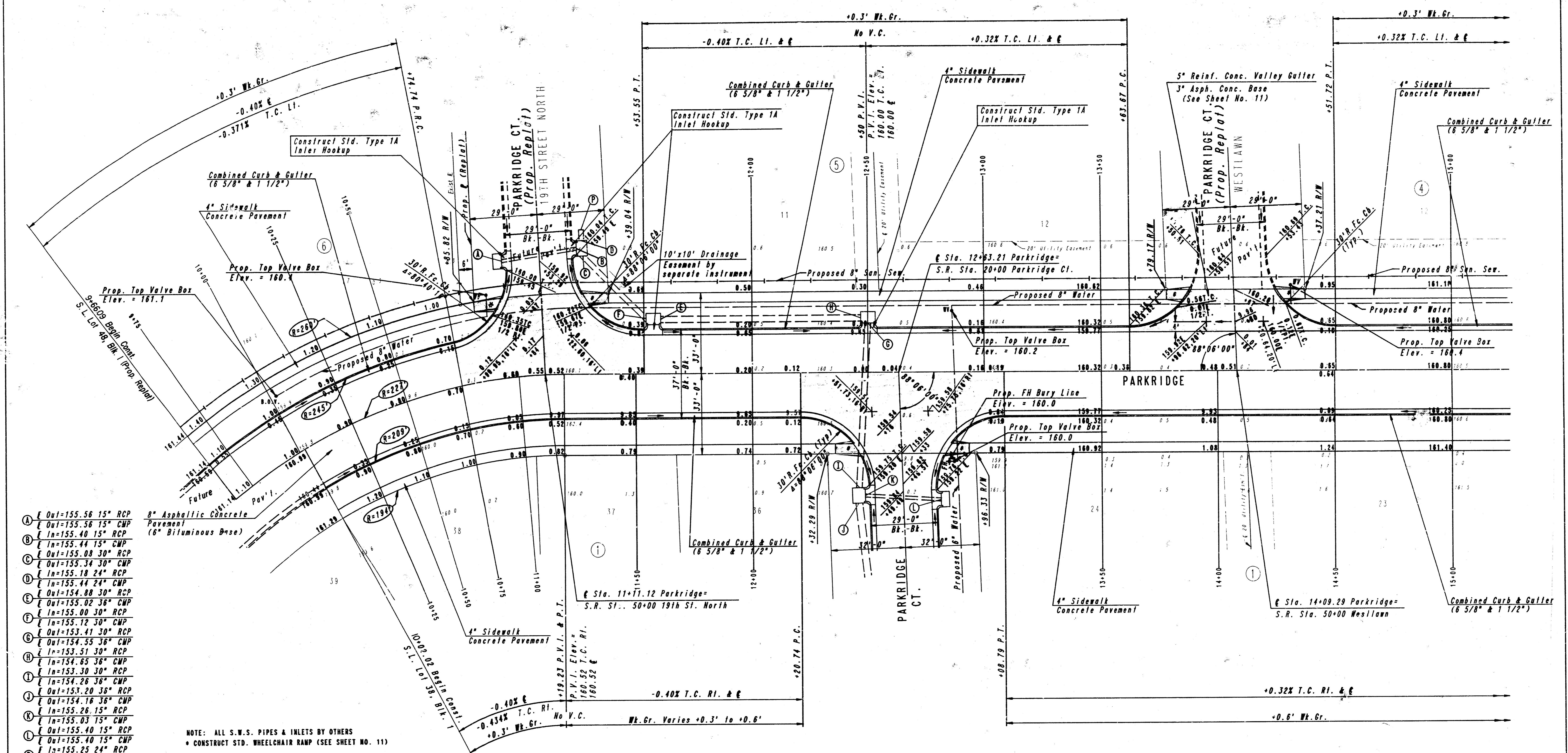
SCALE: 1" = 100'
 ○ = IRON SET
 B.M. - CHIS. "D", NE COR. HEADWALL
 120' ± W. OF INTERSECTION 21ST ST. &
 MAIZE RD. ELEV. = 1351.065 M.S.L.

PROPOSED REPLAT OF ECHO HILLS 2ND ADDITION

WOODBIDGE 2ND ADDITION
PLAT

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

Designed by	Checked by
Drawn by	Date Feb., 1986 Job No. 85427-1



- Ⓐ Out=155.56 15" RCP
In=155.56 15" CMP
- Ⓑ In=155.40 15" RCP
Out=155.44 15" CMP
- Ⓒ Out=155.08 30" RCP
In=155.34 30" CMP
- Ⓓ In=155.18 24" RCP
Out=155.44 24" CMP
- Ⓔ Out=154.88 30" RCP
In=155.02 36" CMP
- Ⓕ In=155.00 30" RCP
Out=155.12 30" CMP
- Ⓖ Out=153.41 30" RCP
In=153.51 30" RCP
- Ⓗ In=154.65 36" CMP
Out=153.30 30" RCP
- Ⓙ In=154.26 36" CMP
Out=153.20 36" RCP
- Ⓚ Out=154.16 36" CMP
In=155.26 15" RCP
- Ⓛ In=155.03 15" CMP
Out=155.40 15" RCP
- Ⓜ Out=155.40 15" RCP
In=155.25 24" RCP
- Ⓝ In=155.48 24" CMP

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS
 * CONSTRUCT STD. WHEELCHAIR RAMP (SEE SHEET NO. 11)

Ⓒ CURVE DATA

Δ=38°39'11.5" D=25°14'26" R=227.00' L=153.14' T=79.61' E=13.56'

CURVE DATA BASED ON Ⓒ RADIUS Δ/2=19°19'36"

Ⓒ STATION	Ⓒ ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	Ⓒ TOTAL DEFLECTION
		8' OFF LEFT FACE CURB	8' OFF RIGHT FACE CURB		
9+66.00					
9+75.00	8.91	9.93		1°07'28"	1°07'28"
10+00.00	25.00	27.85		3°09'18"	4°16'46"
10+25.00	25.00	27.85	22.13	3°09'18"	7°26'04"
10+50.00	25.00	27.85	22.13	3°09'18"	10°35'22"
10+74.74	24.74	27.56	21.90	30°07'20.5"	13°42'42"
10+75.00	0.26	0.26	0.23	0°01'58.5"	13°44'40"
11+00.00	25.00	27.85	22.13	3°09'18"	16°53'58"
11+11.12	11.12	12.39	9.85	1°24'12"	18°18'10"
11+19.23	8.11	9.04	7.10	1°01'25"	19°19'36"

19TH STREET NORTH & PARKRIDGE

INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 226.28 S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
- 28.80 S.Y. 3" Bituminous Base
- 104.37 L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
- L.F. Monolithic Edge Curb (—)
- 2 Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 257.78 S.Y. Subgrade Stabilization Manipulation
- 2.58 Tons Corbide Lime Subgrade Stabilization
- 1.93 Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

PARKRIDGE CT. & PARKRIDGE

INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 246.74 S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
- 19.21 S.Y. 3" Bituminous Base
- 62.83 L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
- L.F. Monolithic Edge Curb (—)
- 2 Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 269.95 S.Y. Subgrade Stabilization Manipulation
- 2.70 Tons Corbide Lime Subgrade Stabilization
- 2.02 Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

WESTLAWN & PARKRIDGE

INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 158.53 S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
- S.Y. 3" Bituminous Base
- L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
- 82.83 L.F. Monolithic Edge Curb (6 5/8")
- 2 Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 269.95 S.Y. Subgrade Stabilization Manipulation
- 2.70 Tons Corbide Lime Subgrade Stabilization
- 2.02 Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- 108.72 S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

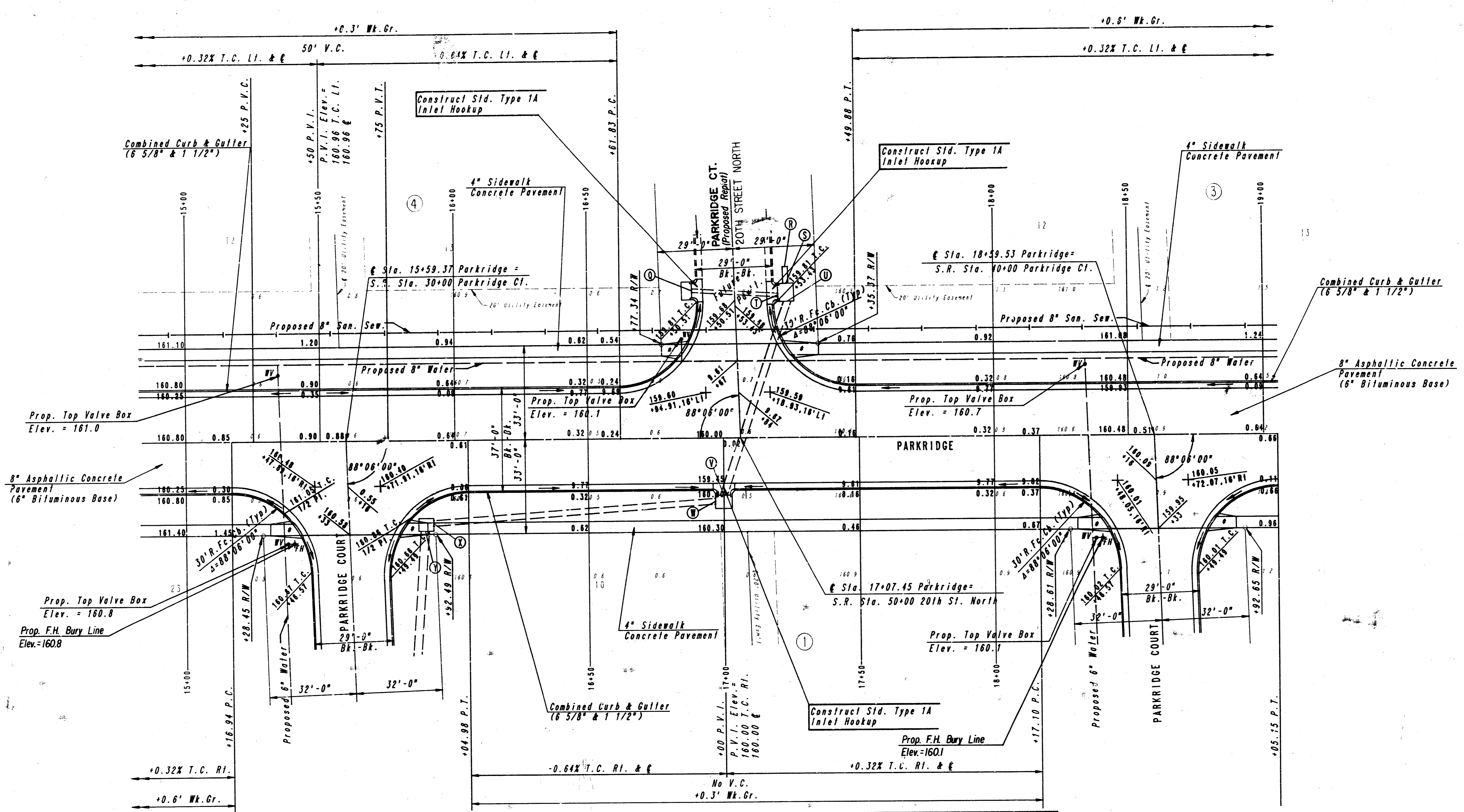
PARKRIDGE
 STA. 10+00 TO STA. 15+00

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

Designed by	BER	Checked by	
Drawn by	JGP	Date	FEB, 1986

Job No. 85427-1

Scale: 1"=20'



- ① Out=155.34 15" RCP
- ② Out=155.34 15" RCP
- ③ In=154.98 24" CMP
- ④ In=154.78 24" CMP
- ⑤ In=154.95 24" RCP
- ⑥ In=154.74 24" CMP
- ⑦ In=155.05 15" RCP
- ⑧ In=154.87 15" RCP
- ⑨ Out=154.85 30" RCP
- ⑩ Out=154.64 36" CMP
- ⑪ In=154.70 30" RCP
- ⑫ In=154.33 36" CMP
- ⑬ Out=154.60 30" RCP
- ⑭ In=153.57 30" RCP
- ⑮ In=153.78 36" CMP
- ⑯ Out=153.47 30" RCP
- ⑰ Out=153.68 36" CMP

PARKRIDGE CT. & PARKRIDGE INTERSECTION QUANTITIES	
—	S.Y. — Unreinforced Concrete Pavement
246.74	S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
19.27	S.Y. 3" Bituminous Base
62.83	L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
—	L.F. Monolithic Edge Curb (—)
2	Eq. Standard Wheelchair Ramp Construction
—	S.F. 4" Sidewalk Concrete Pavement
289.95	S.Y. Subgrade Stabilization Manipulation
2.70	Tons Carbide Lime Subgrade Stabilization
2.02	Tons Pebble Quicklime Subgrade Stabilization
—	Tons Cement Subgrade Stabilization
—	Tons Fly Ash Subgrade Stabilization
—	Tons Prohector Fines Subgrade Stabilization
—	S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

20TH STREET NORTH & PARKRIDGE INTERSECTION QUANTITIES	
—	S.Y. — Unreinforced Concrete Pavement
246.74	S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
19.27	S.Y. 3" Bituminous Base
119.25	L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
—	L.F. Monolithic Edge Curb (—)
2	Eq. Standard Wheelchair Ramp Construction
—	S.F. 4" Sidewalk Concrete Pavement
289.95	S.Y. Subgrade Stabilization Manipulation
2.81	Tons Carbide Lime Subgrade Stabilization
2.11	Tons Pebble Quicklime Subgrade Stabilization
—	Tons Cement Subgrade Stabilization
—	Tons Fly Ash Subgrade Stabilization
—	Tons Prohector Fines Subgrade Stabilization
—	S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

PARKRIDGE CT. & PARKRIDGE INTERSECTION QUANTITIES	
—	S.Y. — Unreinforced Concrete Pavement
246.74	S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
19.27	S.Y. 3" Bituminous Base
62.83	L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
—	L.F. Monolithic Edge Curb (—)
2	Eq. Standard Wheelchair Ramp Construction
—	S.F. 4" Sidewalk Concrete Pavement
289.95	S.Y. Subgrade Stabilization Manipulation
2.70	Tons Carbide Lime Subgrade Stabilization
2.02	Tons Pebble Quicklime Subgrade Stabilization
—	Tons Cement Subgrade Stabilization
—	Tons Fly Ash Subgrade Stabilization
—	Tons Prohector Fines Subgrade Stabilization
—	S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

NOTE: ALL S.W.S. PIPE & INLETS BY OTHERS
 • CONSTRUCT STD. WHEELCHAIR RAMP (SEE SH. NO. 11)

PARKRIDGE
 STA. 15+00 TO STA. 19+05.15

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

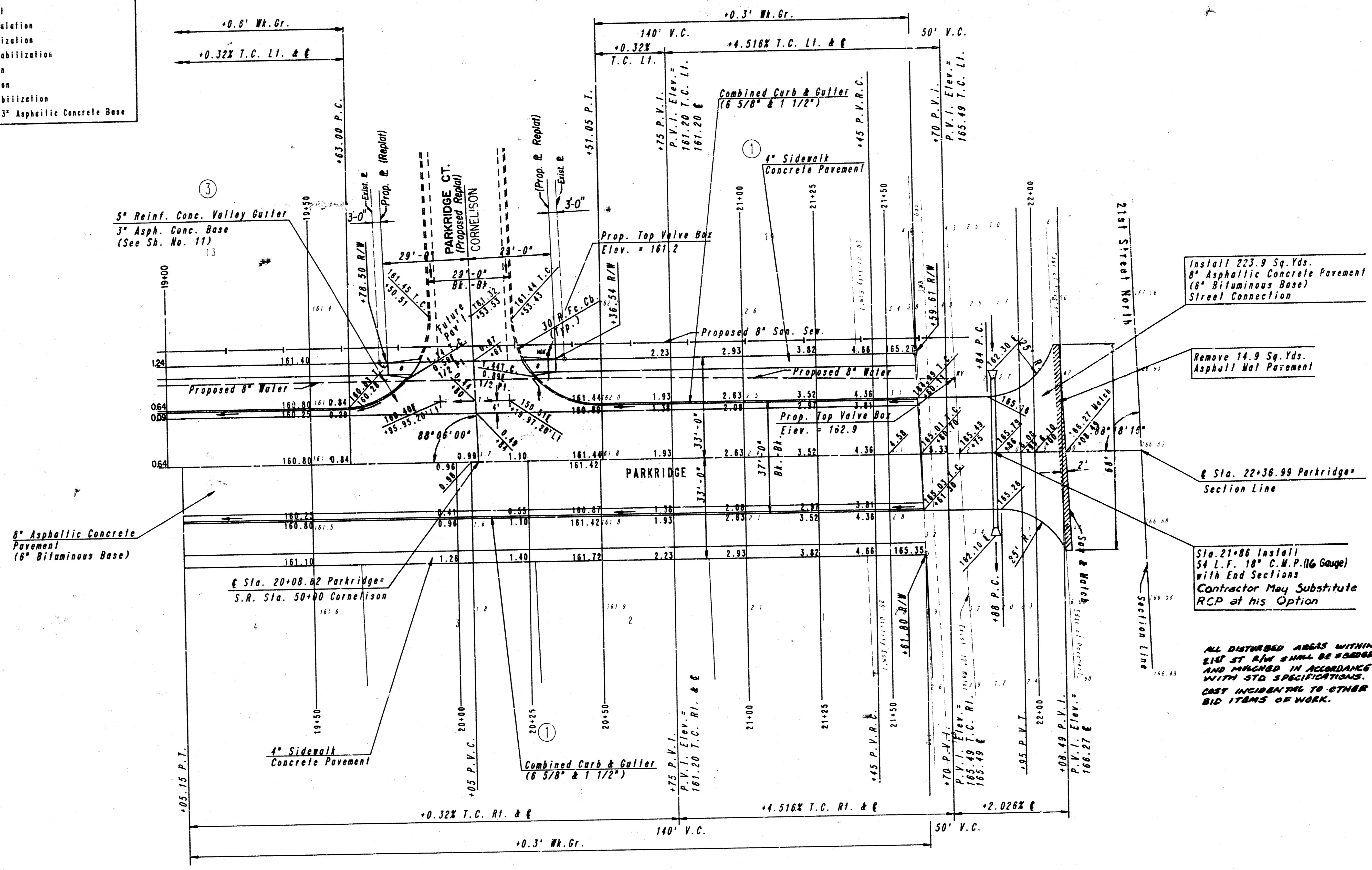
Designed by	BER	Checked by	
Drawn by	JGP	Date	FEB, 1988

Job No. 85427-1

CORNELISON & PARKRIDGE INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 158.53 S.Y. 8" Asphaltic Conc. Pavement (6" Bituminous Base)
- S.Y. — Bituminous Base
- S.F. — Combined Curb & Gutter (6 5/8" & 1 1/2")
- 62.83 L.F. Monolithic Edge Curb (6 5/8")
- 2 Ea. Standard Wheelchair Ramp Construction
- S.F. — 4" Sidewalk Concrete Pavement
- 289.95 S.Y. Subgrade Stabilization Manipulation
- 2.70 Tons Carbide Lime Subgrade Stabilization
- 2.02 Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheated Fines Subgrade Stabilization
- 108.72 S.Y. Reinf. V.C. 5" Concrete & 3" Asphaltic Concrete Base

Scale: 1"=20'



Install 223.9 Sq. Yds. 8" Asphaltic Concrete Pavement (6" Bituminous Base) Street Connection

Remove 14.9 Sq. Yds. Asphalt Mat Pavement

Sta. 22+36.99 Parkridge = Section Line

Sta. 21+86 Install 34 L.F. 18" C.W.P. (16 Gauge) with End Sections Contractor May Substitute RCP at his Option

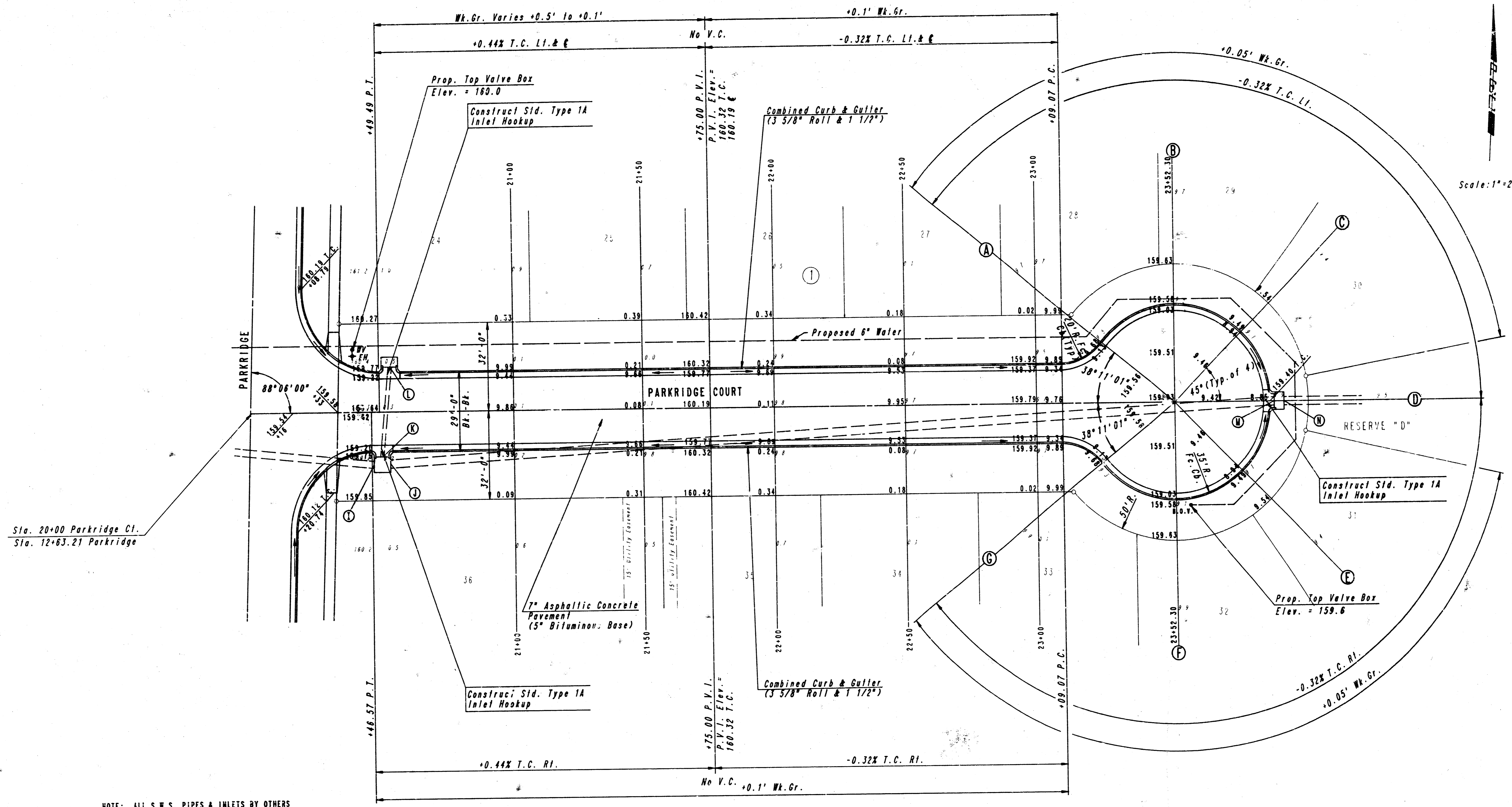
ALL DISTURBED AREAS WITHIN 21ST ST R/W SHALL BE SAVED AND MULCHED IN ACCORDANCE WITH STD SPECIFICATIONS. COST INCIDENTAL TO OTHER BID ITEMS OF WORK.

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS
• CONSTRUCT STD. WHEELCHAIR RAMP (SEE SH. NO. 11)

PARKRIDGE
STA. 19+05.15 TO STA. 22+36.99

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by BER	Checked by
Drawn by JGP	Date FEB, 1986 Job No. 85427-1



Sta. 20+00 Parkridge Cl.
Sta. 12+63.21 Parkridge

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS

INTERSECTION QUANTITIES	
— S.Y. —	Unreinforced Concrete Pavement
44.14 S.Y. 7"	Asphaltic Conc. Pavement (5" Bituminous Base)
9.29 S.Y. 2 1/2"	Bituminous Base
31.42 L.F.	Combined Curb & Gutter (6 5/8" & 1 1/2")
— L.F.	Monolithic Edge Curb (—)
—	Eq. Standard Wheelchair Ramp Construction
— S.F.	4" Sidewalk Concrete Pavement
55.18 S.Y.	Subgrade Stabilization Manipulation
0.55 Tons	Carbide Lime Subgrade Stabilization
0.41 Tons	Pebble Quicklime Subgrade Stabilization
— Tons	Cement Subgrade Stabilization
— Tons	Fly Ash Subgrade Stabilization
— Tons	Preheater Fines Subgrade Stabilization
— S.Y. Reinf. V.C.	Concrete & Asphaltic Concrete Base

- ① In=153.30 30" RCP
- ② In=154.26 36" CWP
- ③ Out=153.20 36" RCP
- ④ Out=154.76 36" CWP
- ⑤ In=155.26 15" RCP
- ⑥ In=155.03 15" RCP
- ⑦ Out=155.40 15" RCP
- ⑧ Out=152.70 36" RCP
- ⑨ In=152.69 36" RCP
- ⑩ Out=152.70 36" RCP

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL CURB

PARKRIDGE COURT
STA. 20+00 TO STA. 23+52.30

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by BER	Checked by
Drawn by JGP	Date FEB, 1986
Job No. 85427-1	

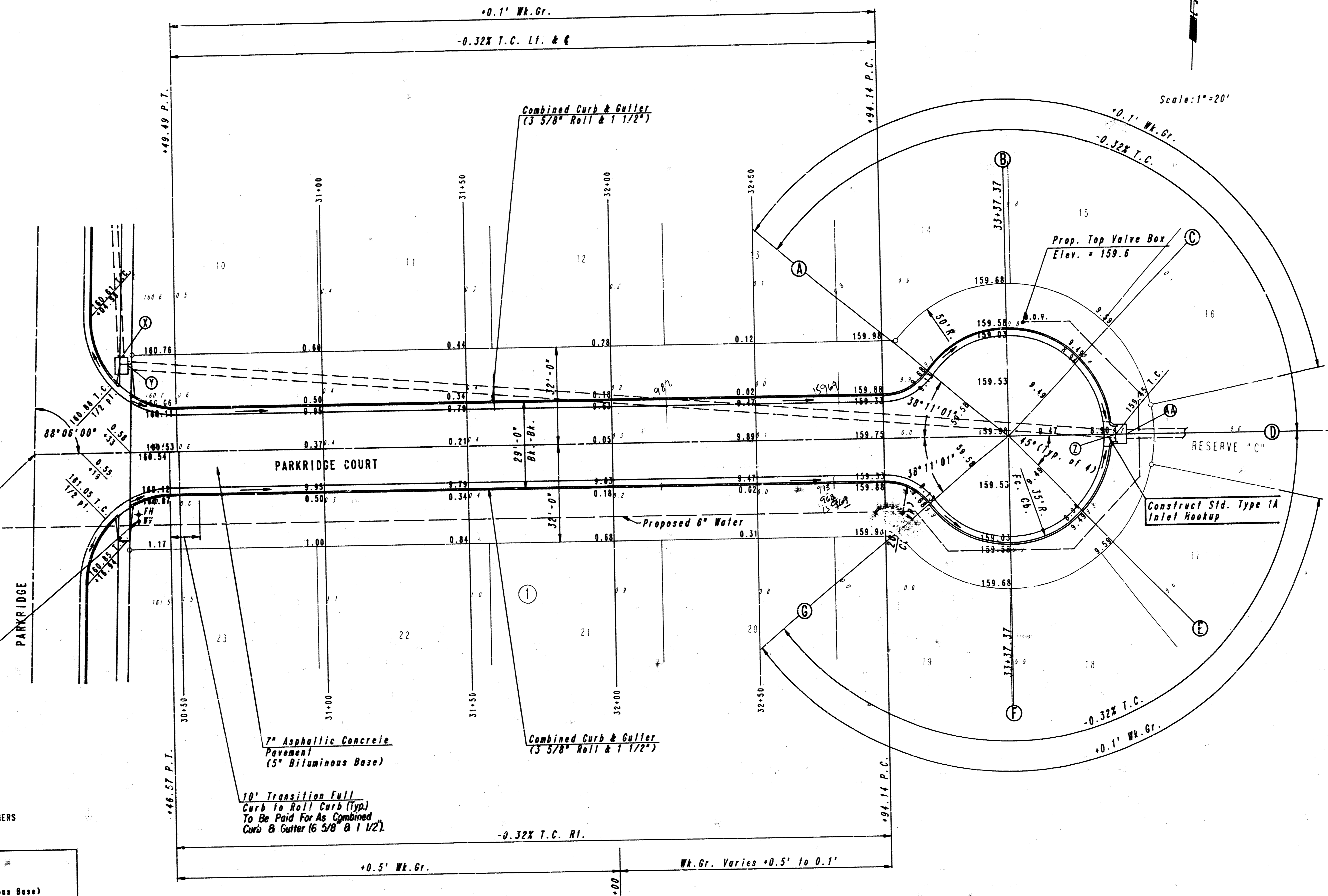
- ① In=153.57 30" RCP
- ② In=153.78 36" CMP
- ③ Out=153.47 30" RCP
- ④ Out=153.68 36" CMP
- ⑤ In=152.67 30" RCP
- ⑥ In=151.67 36" CMP
- ⑦ Out=152.37 36" RCP
- ⑧ Out=151.37 36" CMP

Sta. 30+00 Parkridge Cl. =
Sta. 15+59.37 Parkridge

Prop. Top Valve Box
Elev. = 160.8

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS

INTERSECTION QUANTITIES	
—	S.Y. — Unreinforced Concrete Pavement
64.14 S.Y.	7" Asphaltic Conc. Pavement (5" Bituminous Base)
9.29 S.Y.	2 1/2" Bituminous Base
31.42 L.F.	Combined Curb & Gutter (6 5/8" & 1 1/2")
—	L.F. Monolithic Edge Curb (—)
—	Ca. Standard Wheelchair Ramp Construction
—	S.F. 4" Sidewalk Concrete Pavement
55.18 S.Y.	Subgrade Stabilization Manipulation
0.55 Tons	Carbide Lime Subgrade Stabilization
0.41 Tons	Pebble Quicklime Subgrade Stabilization
—	Tons Cement Subgrade Stabilization
—	Tons Fly Ash Subgrade Stabilization
—	Tons Preheater Fines Subgrade Stabilization
—	S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base



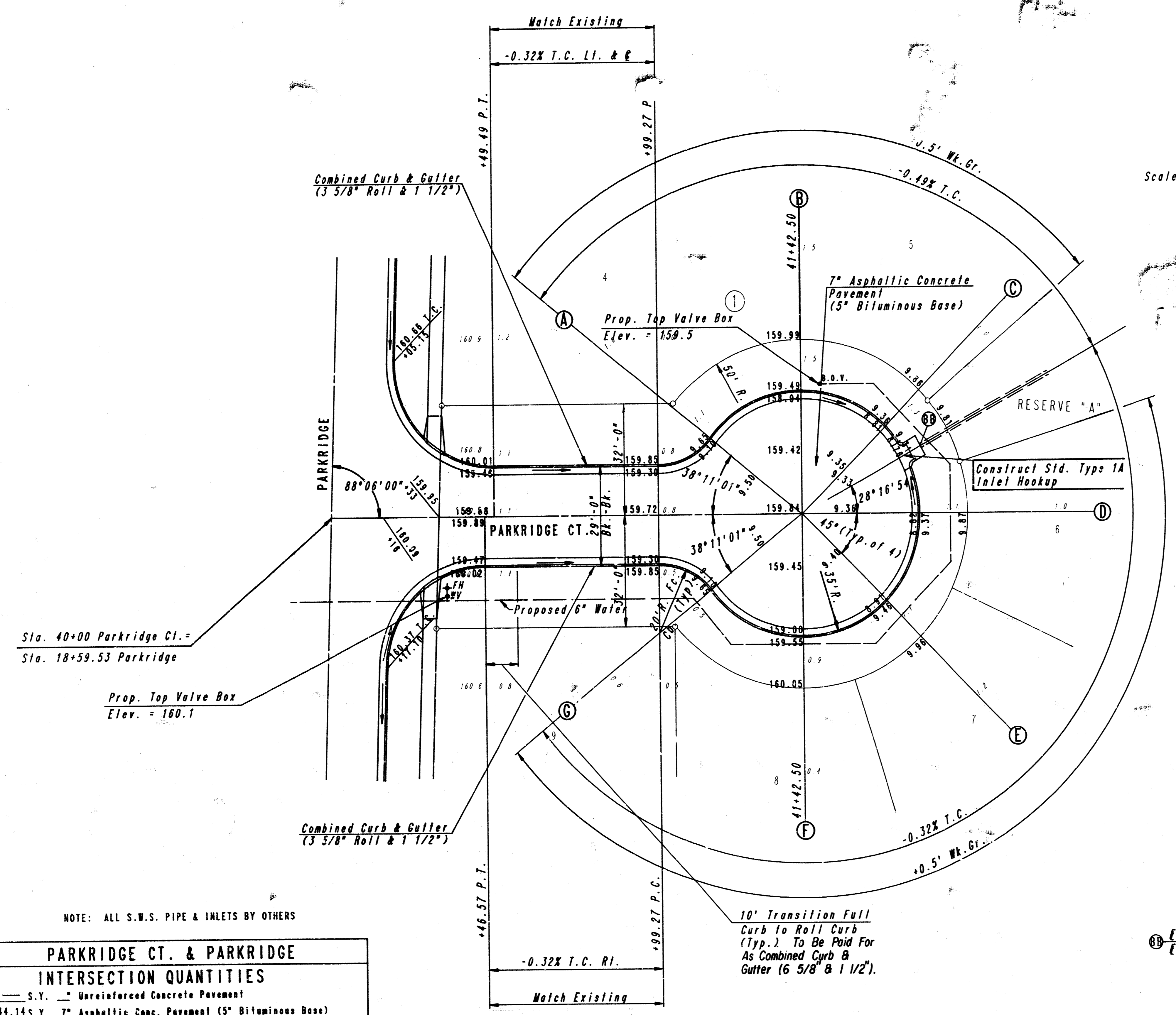
Scale: 1"=20'

NOTE: THIS SHEET TO BE CONSTRUCTED WITH ROLL TYPE CURB

PARKRIDGE COURT
STA. 30+00 TO STA. 33+37.37

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by	BER	Checked by	
Drawn by	JGP	Date	FEB. 1988
		Job No.	85427-1



Scale: 1"=20'

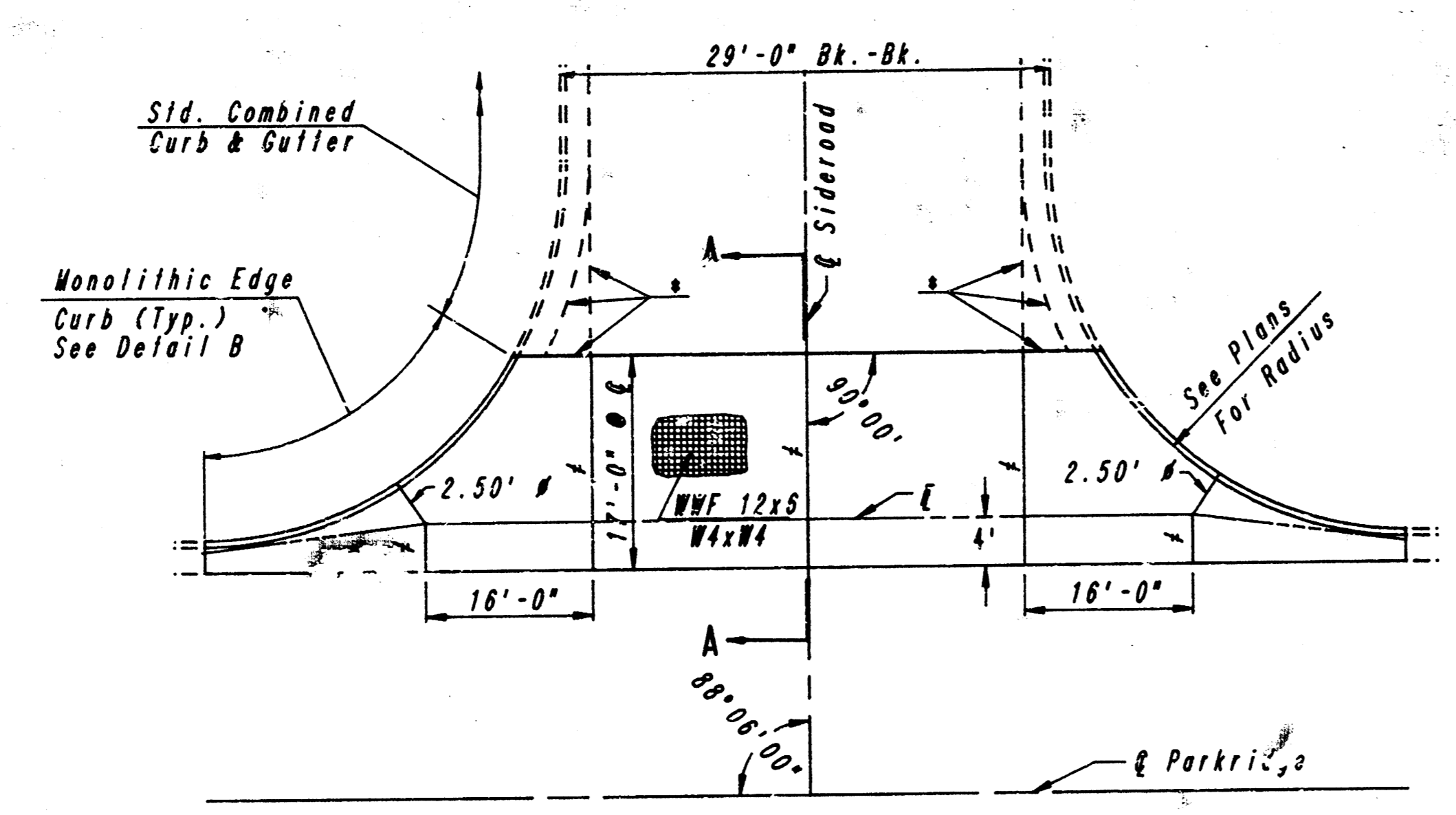
NOTE: ALL S.W.S. PIPE & INLETS BY OTHERS

PARKRIDGE CT. & PARKRIDGE	
INTERSECTION QUANTITIES	
—	S.Y. — Unreinforced Concrete Pavement
44.14	S.Y. — 7" Asphaltic Conc. Pavement (5" Bituminous Base)
9.29	S.Y. — 2 1/2" Bituminous Base
31.42	L.F. Combined Curb & Gutter (6 5/8" & 1 1/2")
—	L.F. Monolithic Edge Curb (—)
—	Eq. Standard Wheelchair Ramp Construction
—	S.F. 4" Sidewalk Concrete Pavement
55.18	S.Y. Subgrade Stabilization Manipulation
0.55	Tons Carbide Lime Subgrade Stabilization
0.41	Tons Pebble Quicklime Subgrade Stabilization
—	Tons Cement Subgrade Stabilization
—	Tons Fly Ash Subgrade Stabilization
—	Tons Preheated Fines Subgrade Stabilization
—	S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL CURB

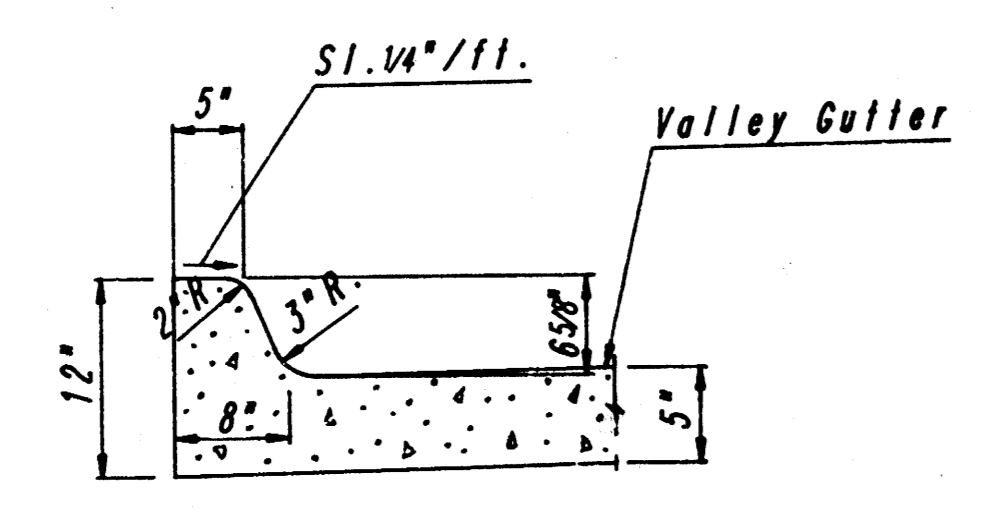
PARKRIDGE COURT STA. 40+00 TO STA. 41+42.50	
PROFESSIONAL ENGINEERING CONSULTANTS, P.A. ENGINEERS WICHITA, KANSAS	
Designed by BER	Checked by JGP
Drawn by JGP	Date FEB, 1988 Job No. 85427-1

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81515-000-001	11	20

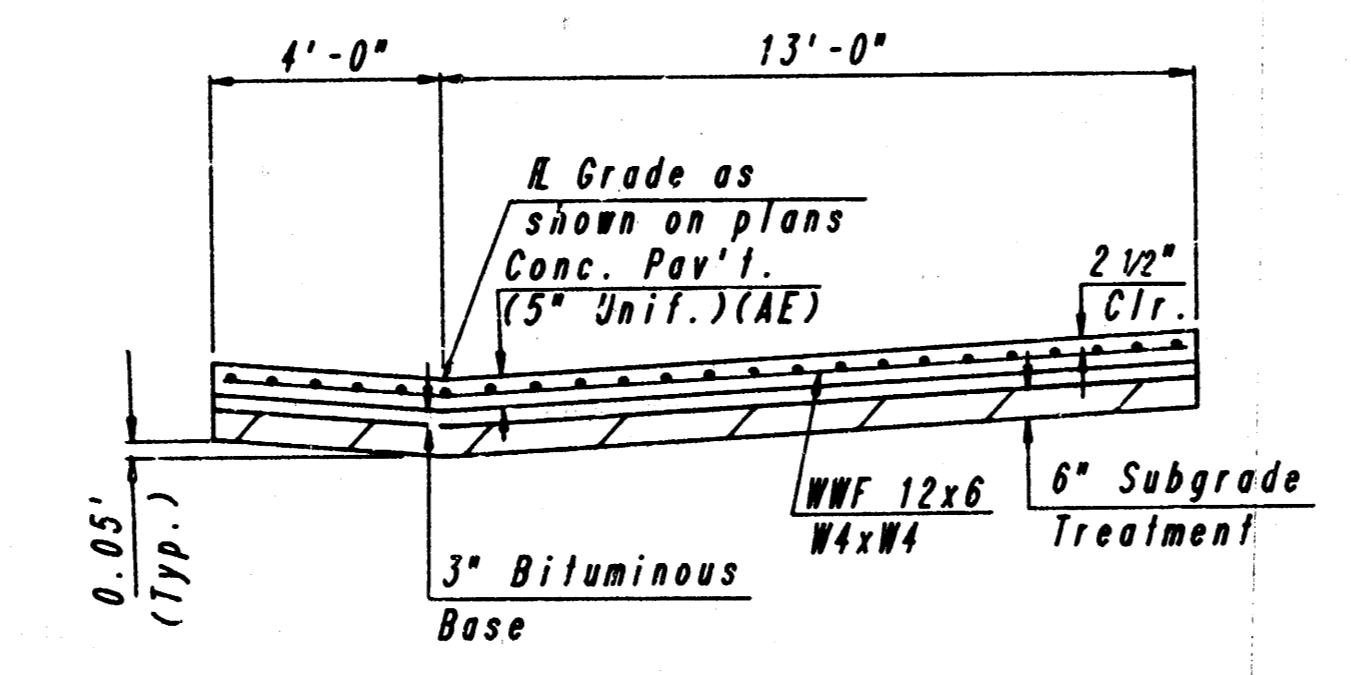


VALLEY GUTTER

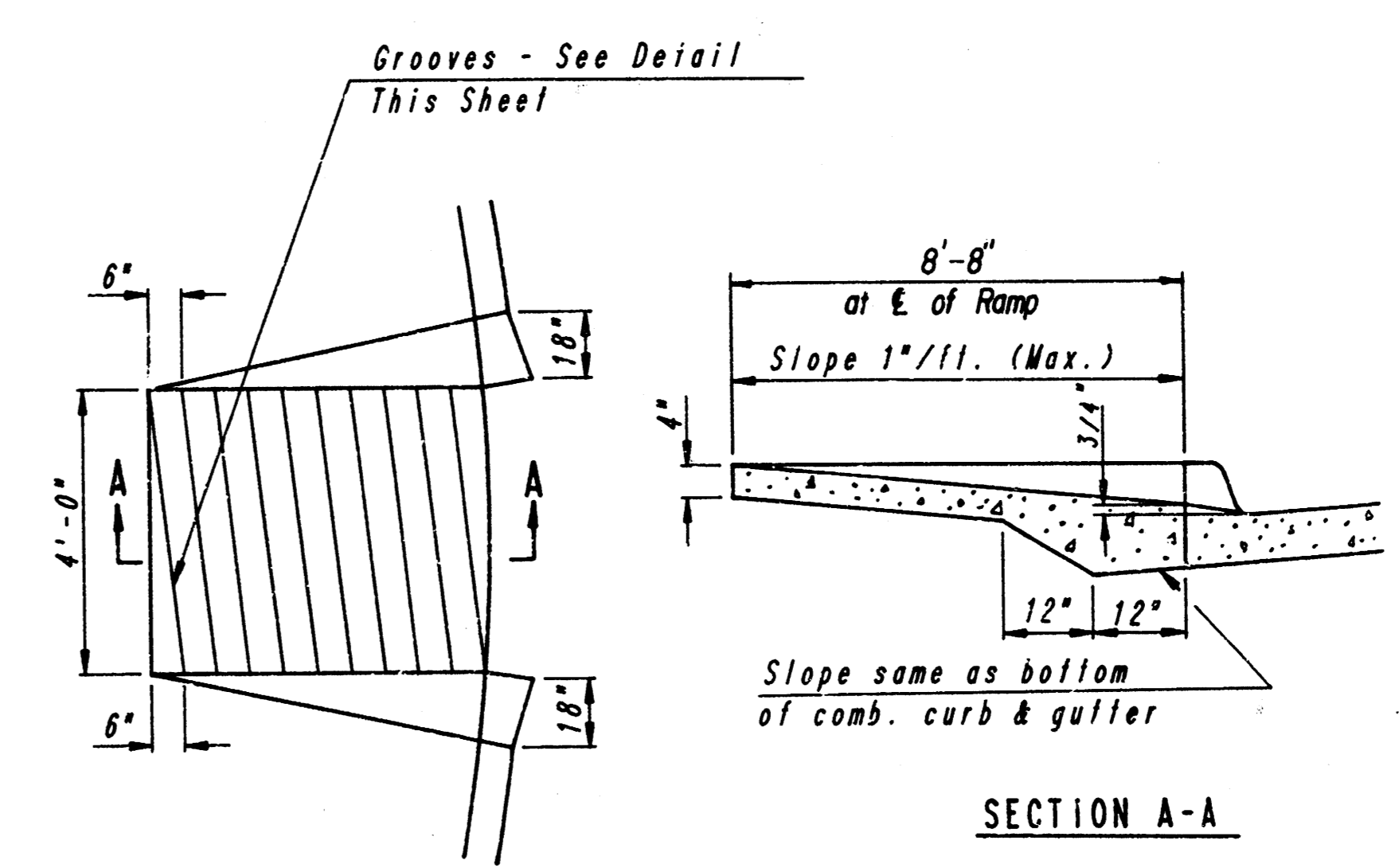
4 OMIT TIE BARS
 • TO BE POURED MONOLITHICALLY WITH ADJACENT COMBINED CURB & GUTTER. SUBSIDIARY TO UNIT PRICE BID PER LIN. FT. FOR COMBINED CURB & GUTTER
 # LONGITUDINAL JOINT OR LONGITUDINAL CONSTRUCTION JOINT, AT CONTRACTOR'S OPTION.



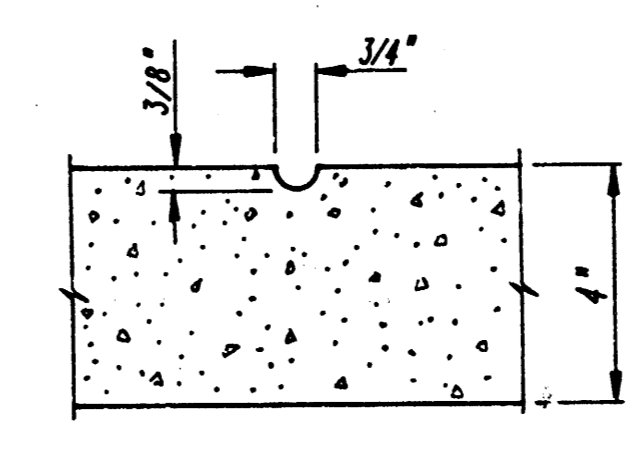
DETAIL B



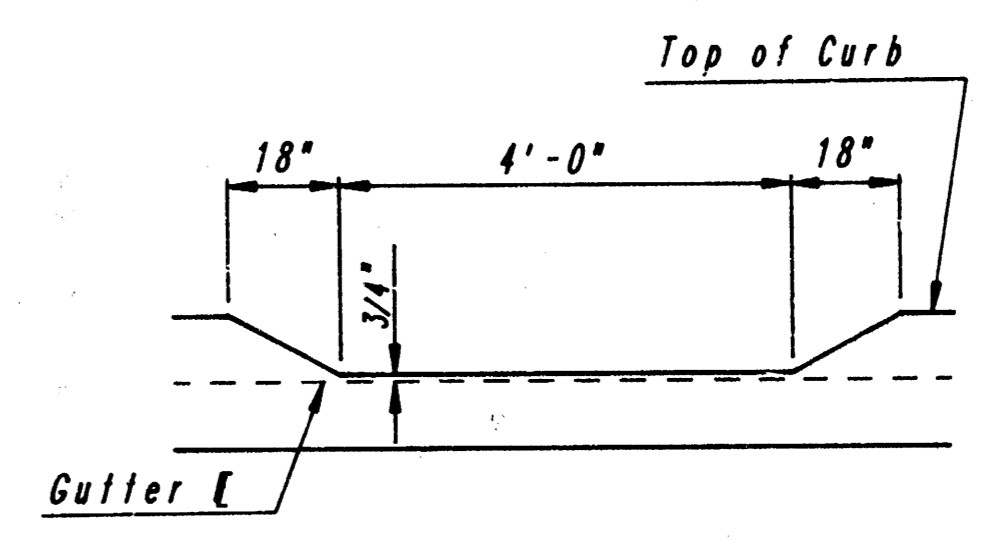
SECTION A-A



WHEELCHAIR RAMP DETAIL



GROOVE DETAIL



DEPRESSED CURB DETAIL

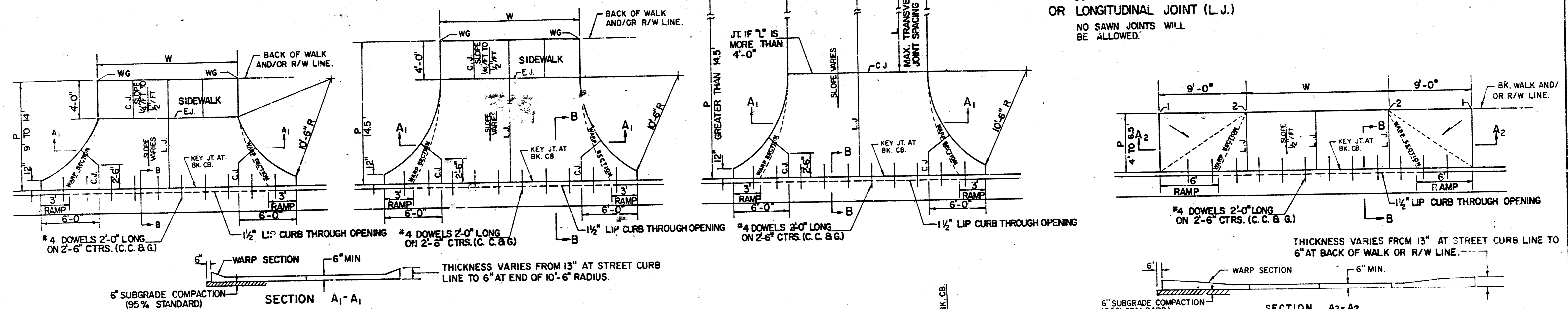
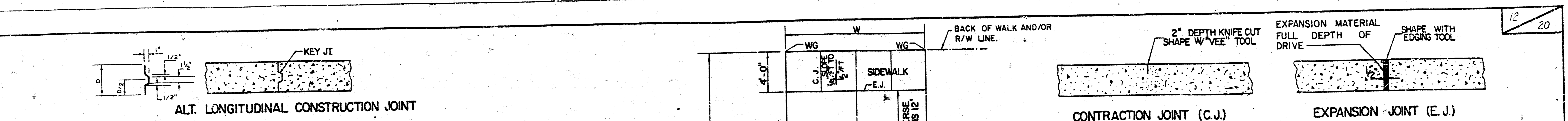
TYPICAL WHEELCHAIR RAMP DETAILS

VALLEY GUTTER & WHEELCHAIR RAMP DETAILS

PROFESSIONAL ENGINEERING CONSULTANTS, P. A.
 ENGINEERS
 WICHITA, KANSAS

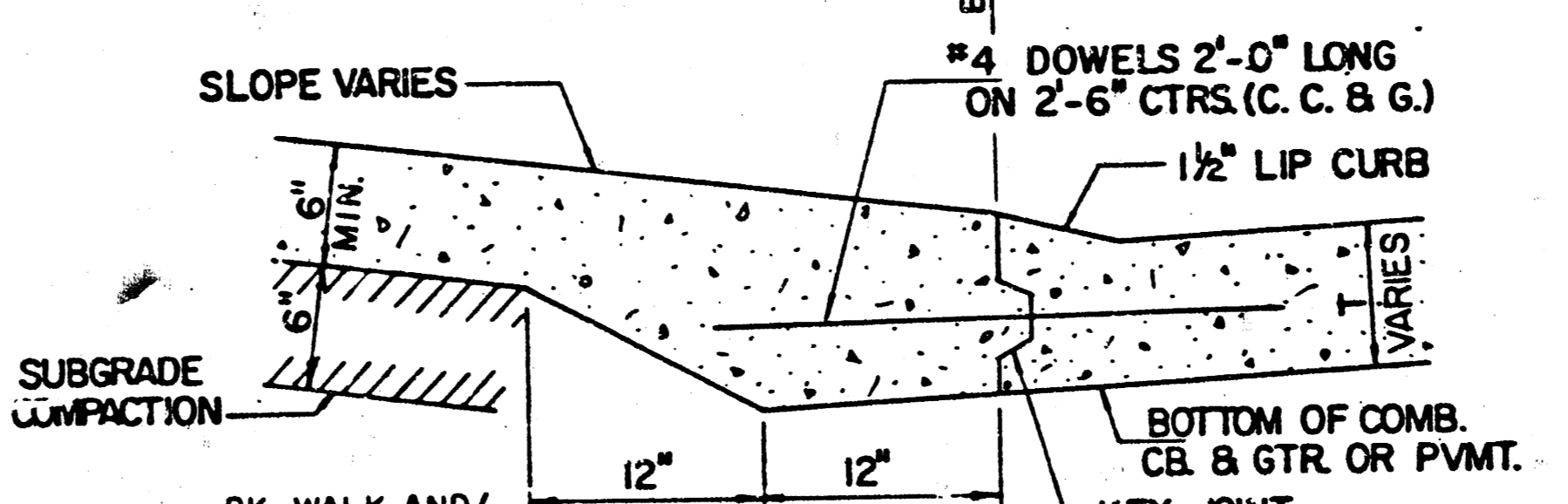
Designed by	BER	Checked by	
Drawn by	DEP	Date	FEB., 1986

Job No. 85427-1



PARKING WIDTH "P"	9'	10'	11'	12'	13'	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35'	0.35'	0.40'	0.45'	0.60'	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35'	0.35'	0.40'	0.45'	0.60'	0.70'	1.04'	1.30'	1.56'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.19'	0.21'	0.23'	0.25'	0.27'	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-1.9'	-1.6'	-1.3'	-1.0'	-0.6'	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

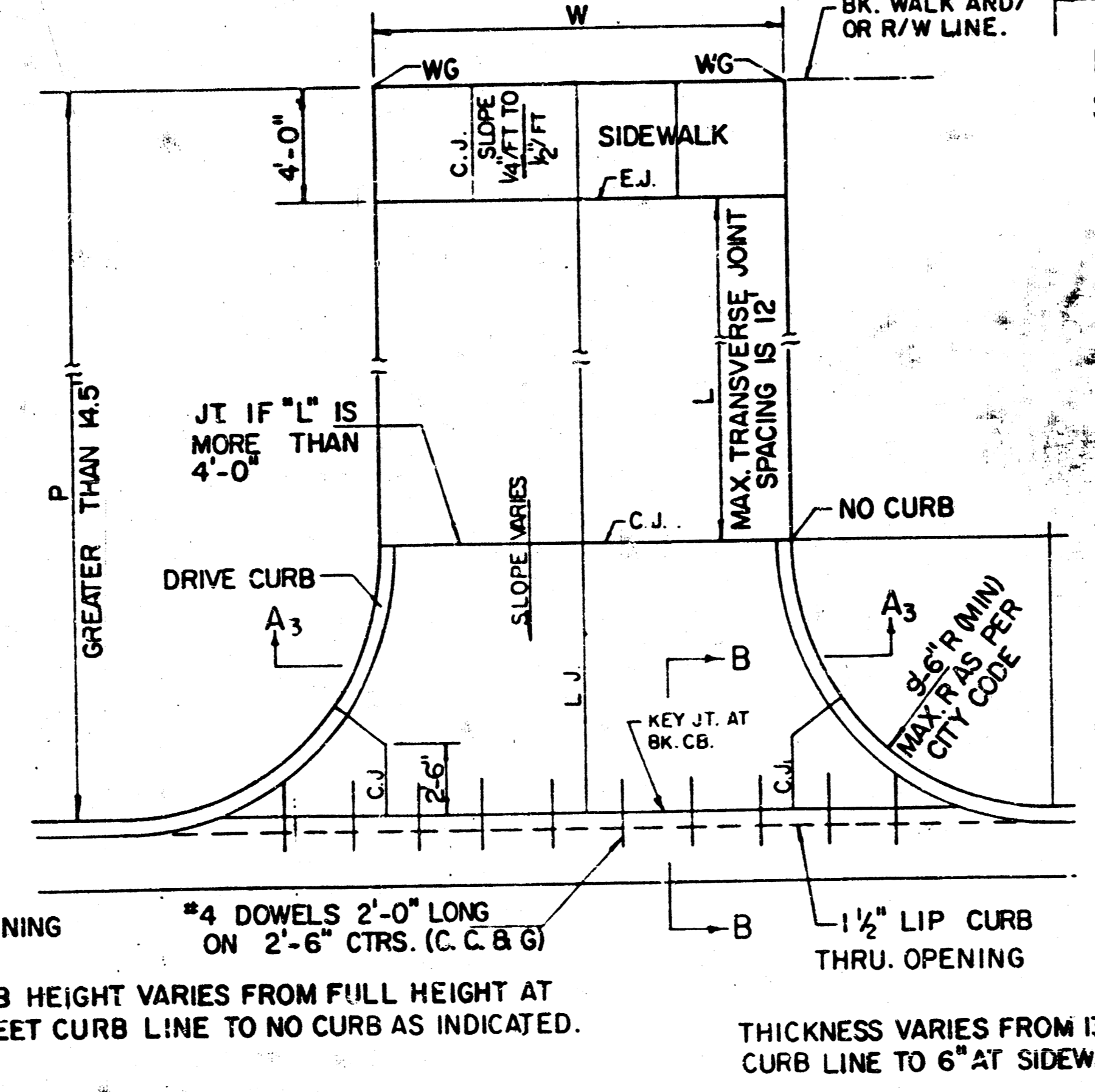
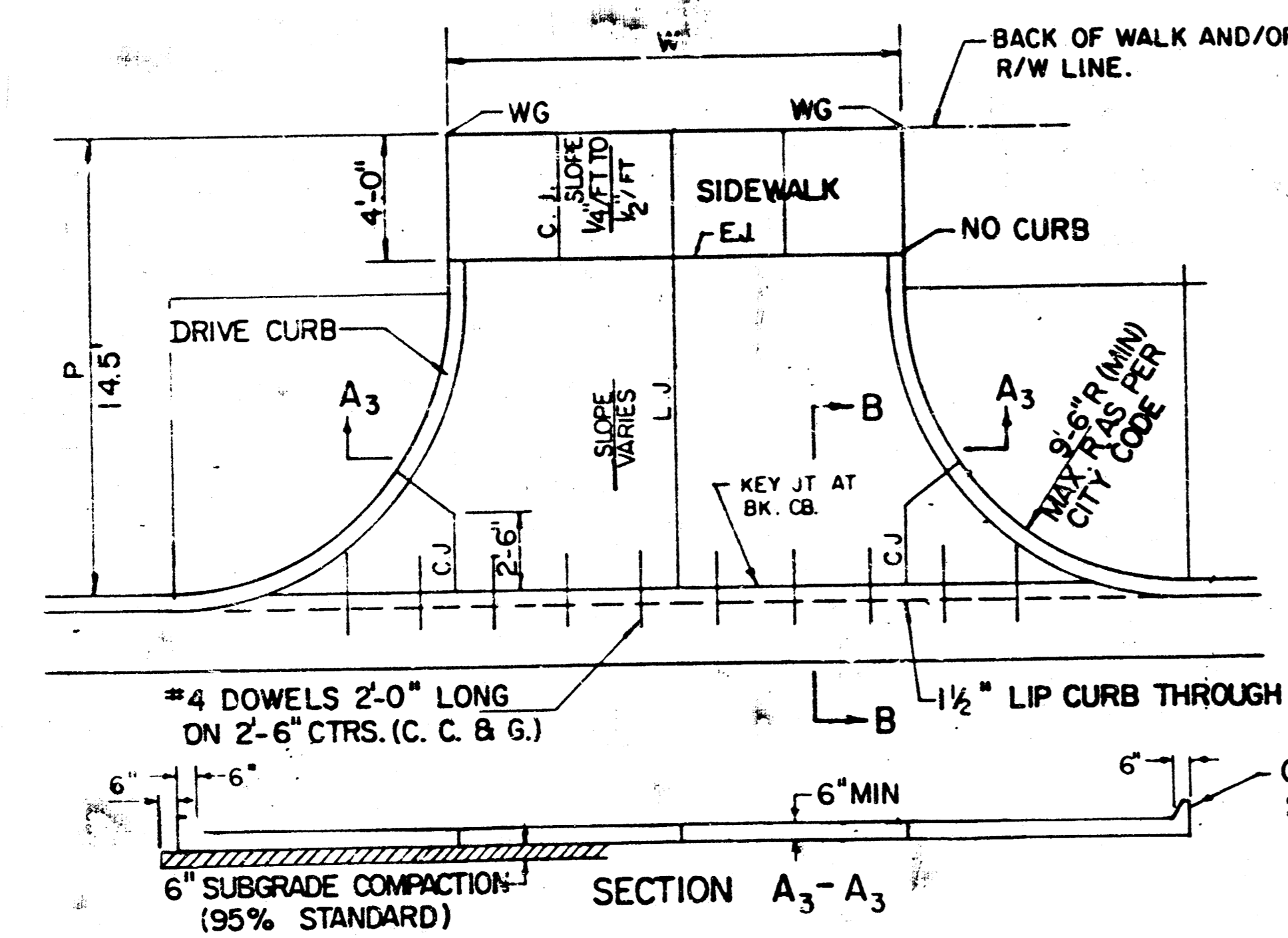
RADIUS RAMP DRIVES (P = 9.0' & GREATER)



PARKING WIDTH "P"	4'	4.5'	5'	5.5'	6'	6.5'
DIST. OF PT. "1" ABOVE TOP OF FULL CB.	0.08'	0.09'	0.10'	0.12'	0.13'	0.14'
DIST. OF PT. "2" BELOW TOP OF FULL CB.	-0.26'	-0.24'	-0.22'	-0.20'	-0.18'	-0.16'

FULL RAMP DRIVE (P=4.0' TO 6.5')

- GENERAL NOTES
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH ROLL CURB AND/OR FULL WALK.
 - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "P" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "P" DIMENSION GREATER THAN 24'.
 - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL UNWINDS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
 - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
 - DOWEL BARS SHALL BE OMITTED FROM THE KEVED CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONDUCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
 - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
 - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ADJUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ADJUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
 - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH #12" W-8" HELD WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONFORMANCE.
 - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE GUTTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.



PARKING WIDTH "P"	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.70'	1.04'	1.30'	1.56'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

FULL RADIUS DRIVES (P=14.5' & GREATER)

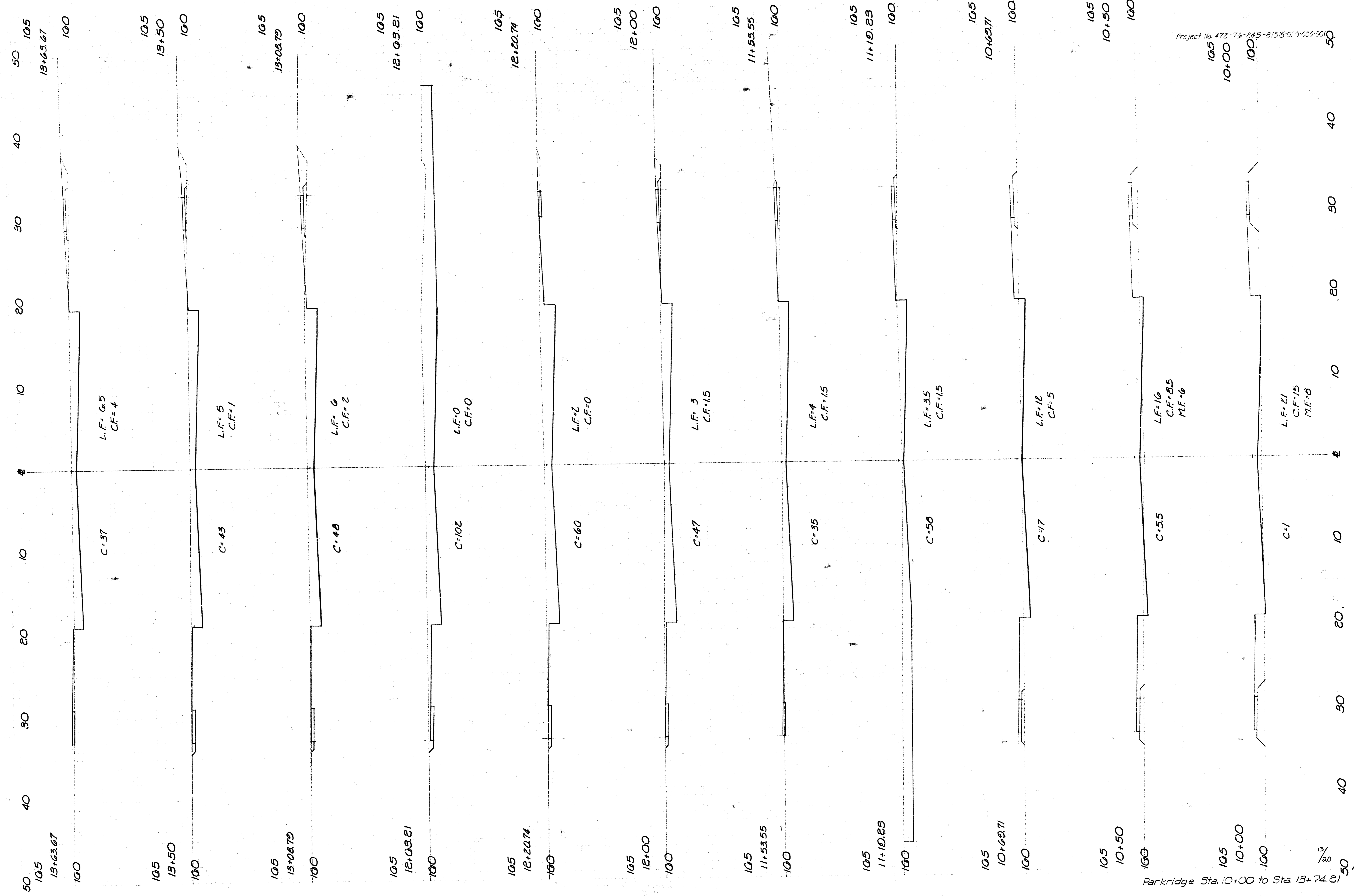
PARKING WIDTH "P"	7'	7.5'	8'	8.5'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.10'	0.20'	0.30'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.10'	0.20'	0.30'
OPTIMUM MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-0.15'	-0.16'	-0.17'	-0.17'
ABSOLUTE MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-0.25'	-0.20'	-0.20'	-0.20'

FULL RAMP DRIVE (P=7.0' TO 8.5')

REVISED OCTOBER 1985
SCALE: 1"=5'

STANDARD DRIVE ENTRANCES
FULL HEIGHT CURB
CITY OF WICHITA, KANSAS

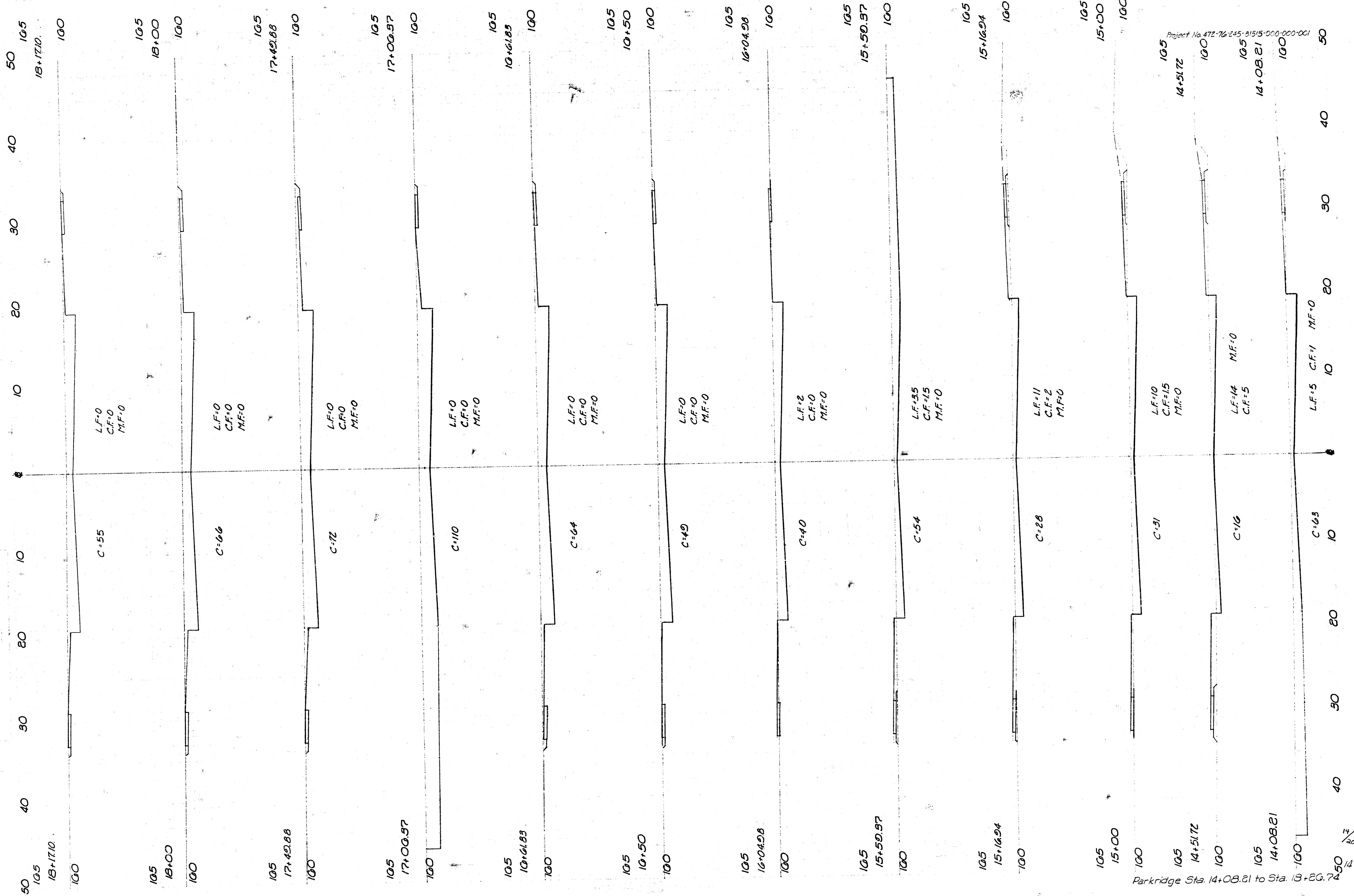
PROJECT NUMBER
472-76-245-81515-000-000-001



Project No. 472-75-245-81550/1000-100

Parkridge Sta. 10+00 to Sta. 13+74.21

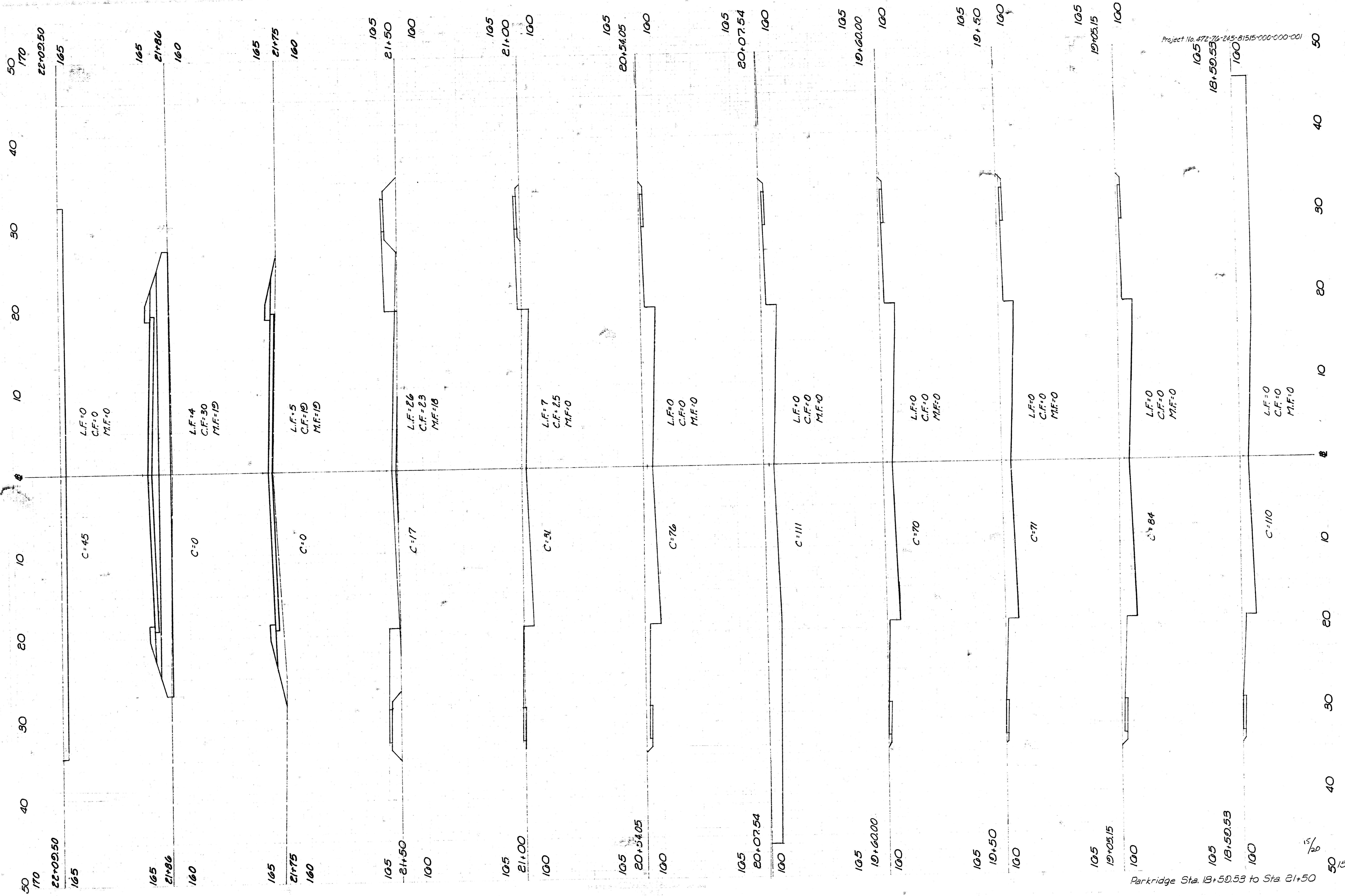
13/20 50



Project No. 472-76-245-31515-000-000-001

Parkridge Sta. 14+08.21 to Sta. 13+26.74

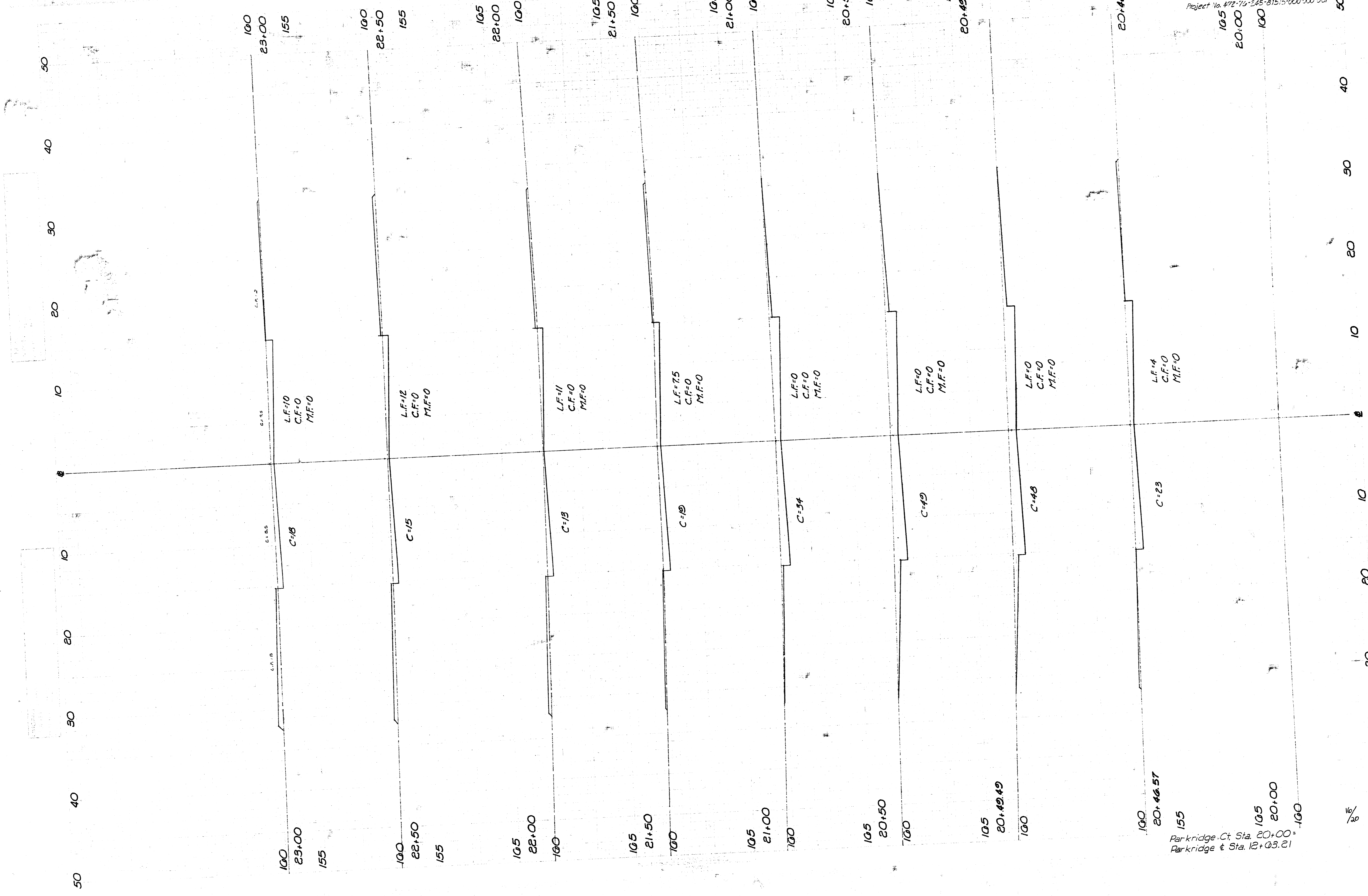
50
40
30
20
10
0
10
20
30
40
50



Project No. 472-76-245-81515-000-000-001

Parkridge Sta. 18+50.53 to Sta. 21+50

1/20
50 15



Project No. 472-74-245-815/5-000/000/001

Parkridge Ct. Sta. 20+00 =
 Parkridge & Sta. 12+03.21

100 23+00 155
 100 22+50 155
 105 22+00 100
 105 21+50 100
 105 21+00 100
 105 20+50 100
 105 20+42.49 100
 100 20+46.57 155
 105 20+00 100
 100

100 23+00 155
 L.F.=10
 C.F.=0
 M.F.=0
 C-18

100 22+50 155
 L.F.=12
 C.F.=0
 M.F.=0
 C-15

105 22+00 100
 L.F.=11
 C.F.=0
 M.F.=0
 C-13

105 21+50 100
 L.F.=7.5
 C.F.=0
 M.F.=0
 C-10

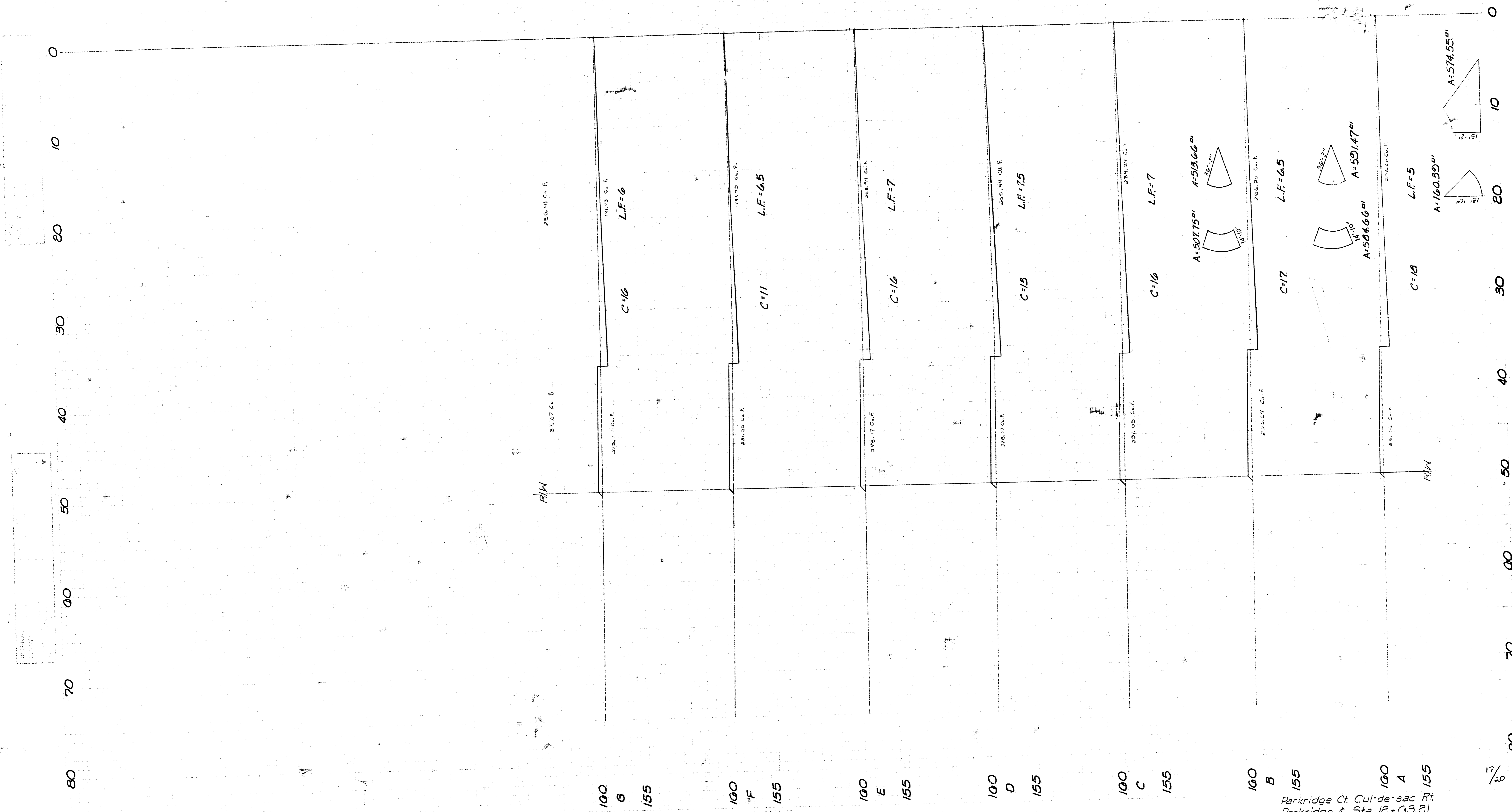
105 21+00 100
 L.F.=0
 C.F.=0
 M.F.=0
 C-54

105 20+50 100
 L.F.=0
 C.F.=0
 M.F.=0
 C-49

105 20+42.49 100
 L.F.=0
 C.F.=0
 M.F.=0
 C-48

100 20+46.57 155
 L.F.=4
 C.F.=0
 M.F.=0
 C-23

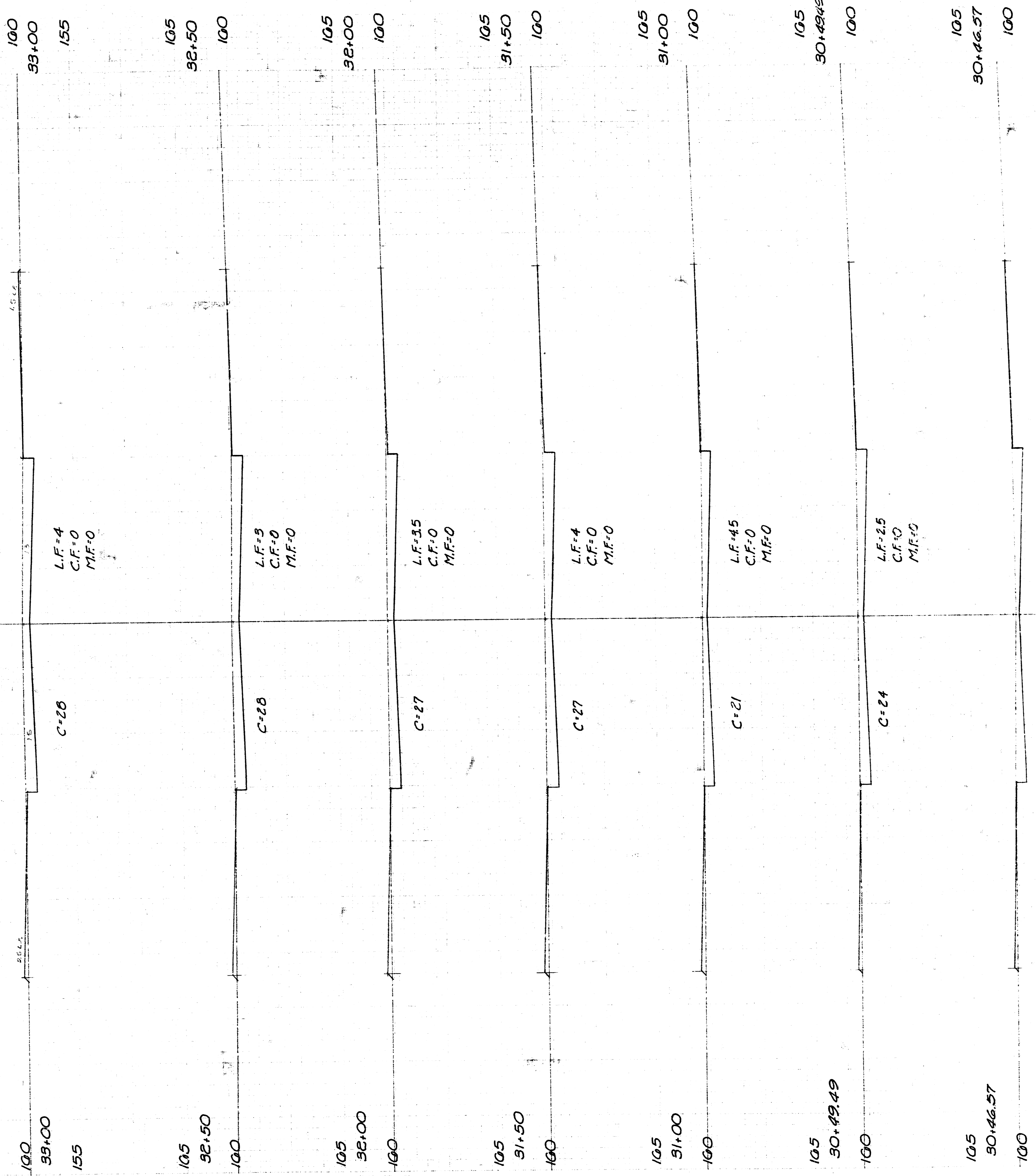
100
 50



L.F.=1 C.X.
 C=11 C.X.
 100 G 155
 L.F.=8 C.X.
 C=7 C.X.
 100 F 155
 L.F.=9 C.X.
 C=7 C.X.
 100 E 155
 L.F.=9 C.X.
 C=8 C.X.
 100 D 155
 L.F.=9 C.X.
 C=9 C.X.
 100 C 155
 L.F.=8 C.X.
 C=11 C.X.
 100 B 155
 L.F.=2 C.X.
 C=11 C.X.
 100 A 155

Parkridge Ct Cul-de-sac Rt.
 Parkridge & Sta. 12+03.21

50 40 30 20 10 0 10 20 30 40 50



Project No. 472-74-245-31515-000-000-001

105
90+00
100

Parkridge Ct Sta. 30+00+
Parkridge & Sta. 15+50.57

105
90+00
100

50

40

30

20

10

0

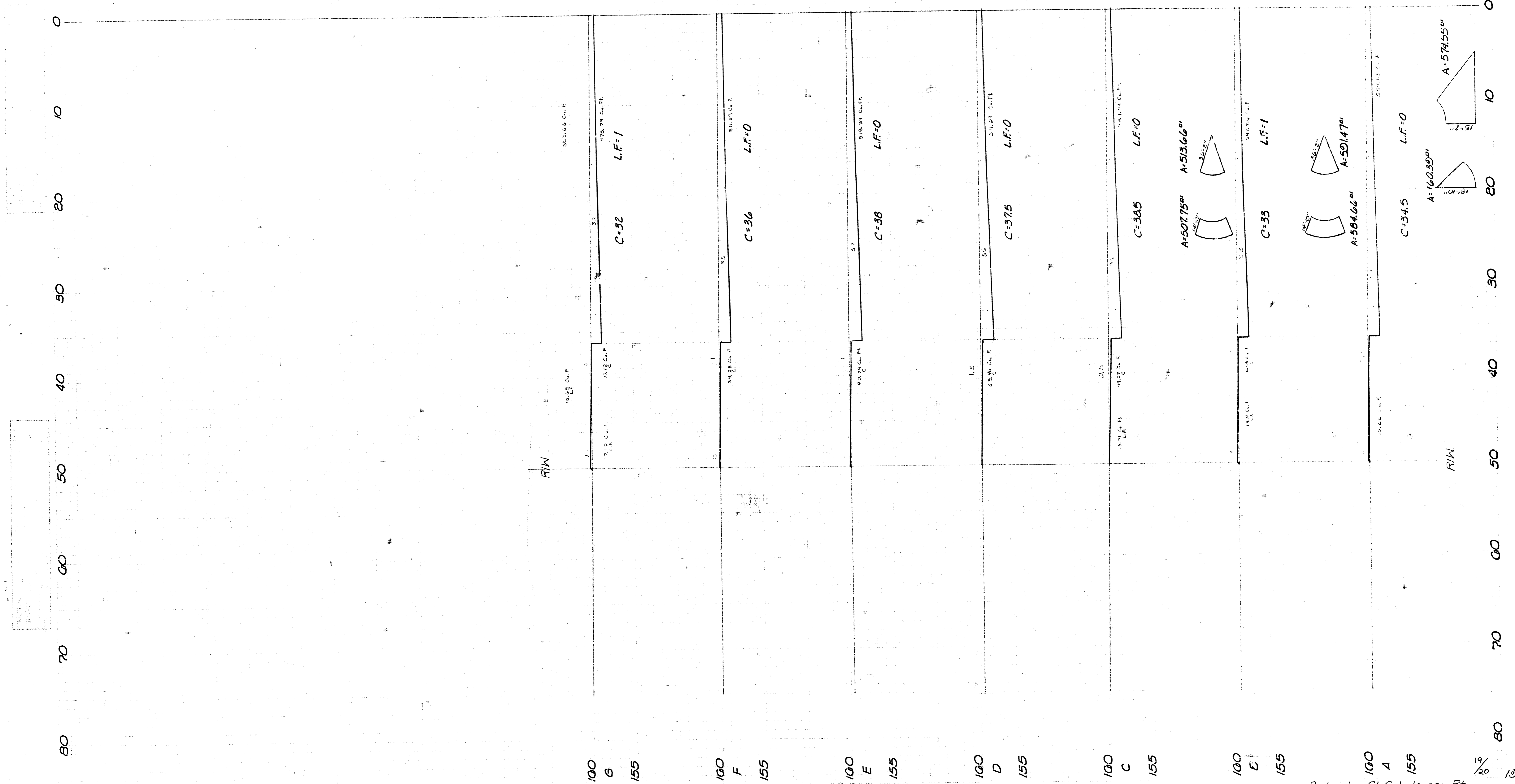
10

20

30

40

50



L.F.=0.4 C.Y.
C=10 C.Y.

100 G
L.F.=0.6 C.Y.
C=10 C.Y. 155

100 F
L.F.=0 C.Y.
C=20 C.Y. 155

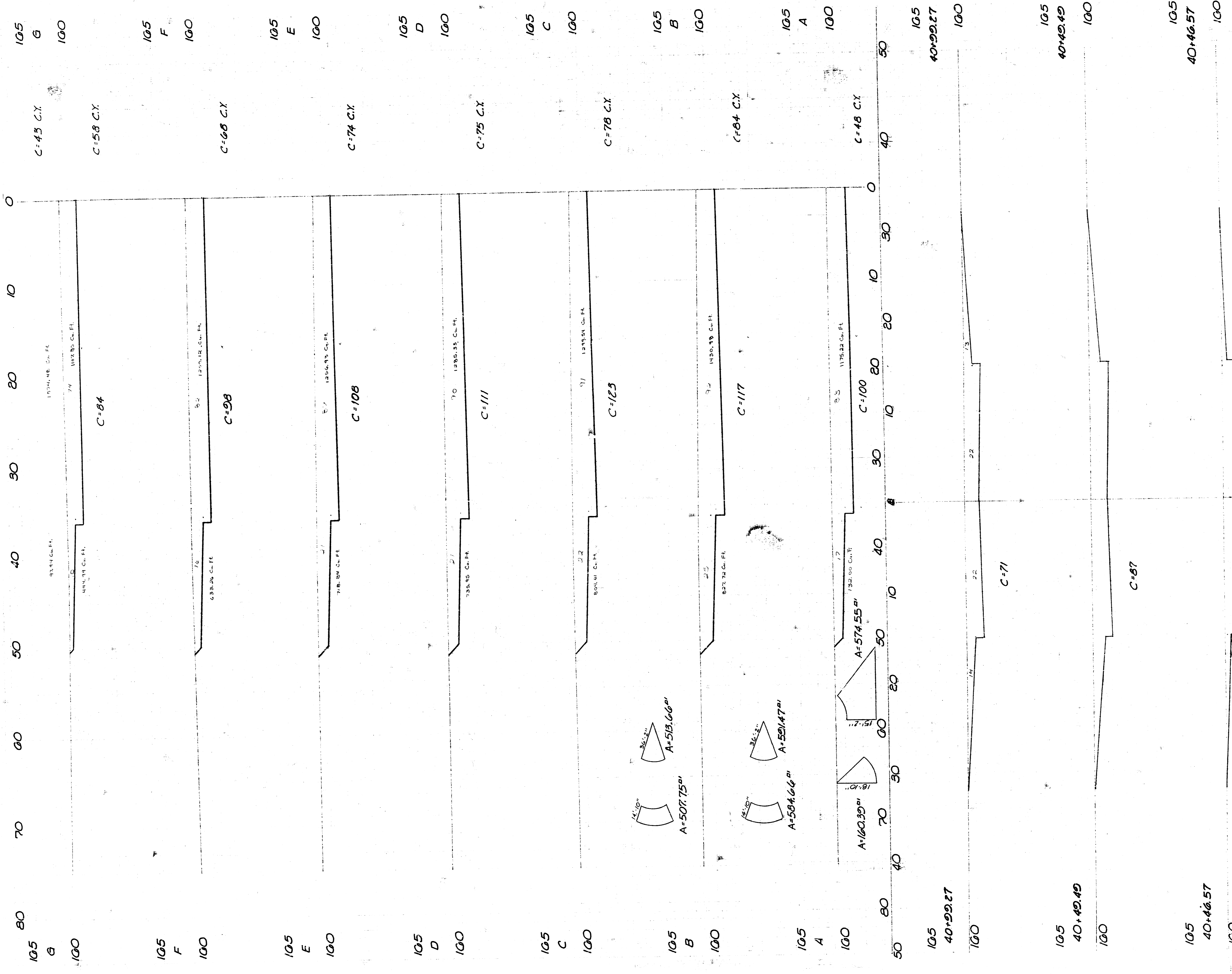
100 E
L.F.=0 C.Y.
C=21 C.Y. 155

100 D
L.F.=0 C.Y.
C=21 C.Y. 155

100 C
L.F.=0.7 C.Y.
C=20 155

100 B
L.F.=0.7 C.Y.
C=20 C.Y. 155

100 A
L.F.=0.4 C.Y.
C=21 C.Y. 155



Project No. 472-76-245-81515-000-000-001
 105
 40+00
 100

Parkridge Ct Sta 40+00
 Parkridge & Sta 18+59.53

