

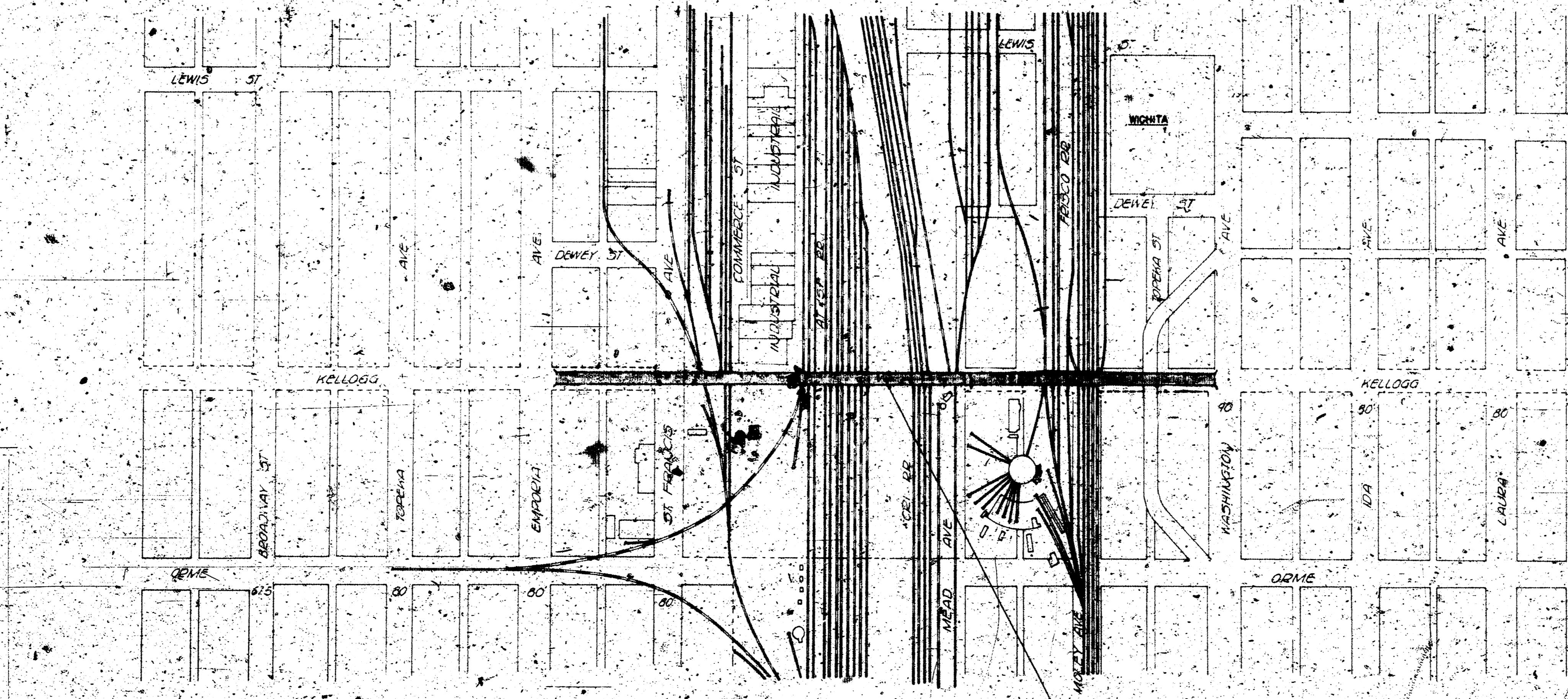
STATE OF KANSAS  
STATE HIGHWAY COMMISSION


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SHEET NO. 1 Title Sheet  
2 Bridge Repair Sheet  
3 Traffic Control Sheet

BRIDGE REPAIR

PLAN AND PROFILE OF PROPOSED  
STATE HIGHWAY

MAINTENANCE PROJECT  
SEDGWICK COUNTY  
54-87-5997-(1)



NOTE: TRAFFIC TO BE CARRIED THROUGH CONSTRUCTION

CITY OF WICHITA  
311. 139 145 Repair  
Br. No. 52-87-26-29

CONVENTIONAL SIGNS

COUNTY LINE	RETAINING WALL	50
CITY, TOWNSHIP, OR BOROUGH	BASE OR SURVEY LINE	50
PROPERTY OR INTERSECTION LINE	LEVEE	50
SECTION LINE	CULVERTS	50
SECTION LINE	DROP INLETS	50
SECTION LINE	TROLLEY POLE	50
SECTION LINE	POWER LINE	50
SECTION LINE	TELEPHONE OR TELEGRAPH LINE	50
SECTION LINE	MARSH	50
SECTION LINE	HEDGE	50
SECTION LINE	TREES	50
SECTION LINE	GRADE ELEVATION	50

GROSS LENGTH OF PROJECT FT.

EXCEPTIONS

ADDITIONS

NET LENGTH OF PROJECT FT. MILES

NET LENGTH OF BRIDGES FT. MILES

NET LENGTH OF ROAD FT. MILES

RECOM. FOR APPROVAL DATE

ENGINEER OF DESIGN  
STATE HIGHWAY COMMISSION OF KANSAS

APPROVED DATE

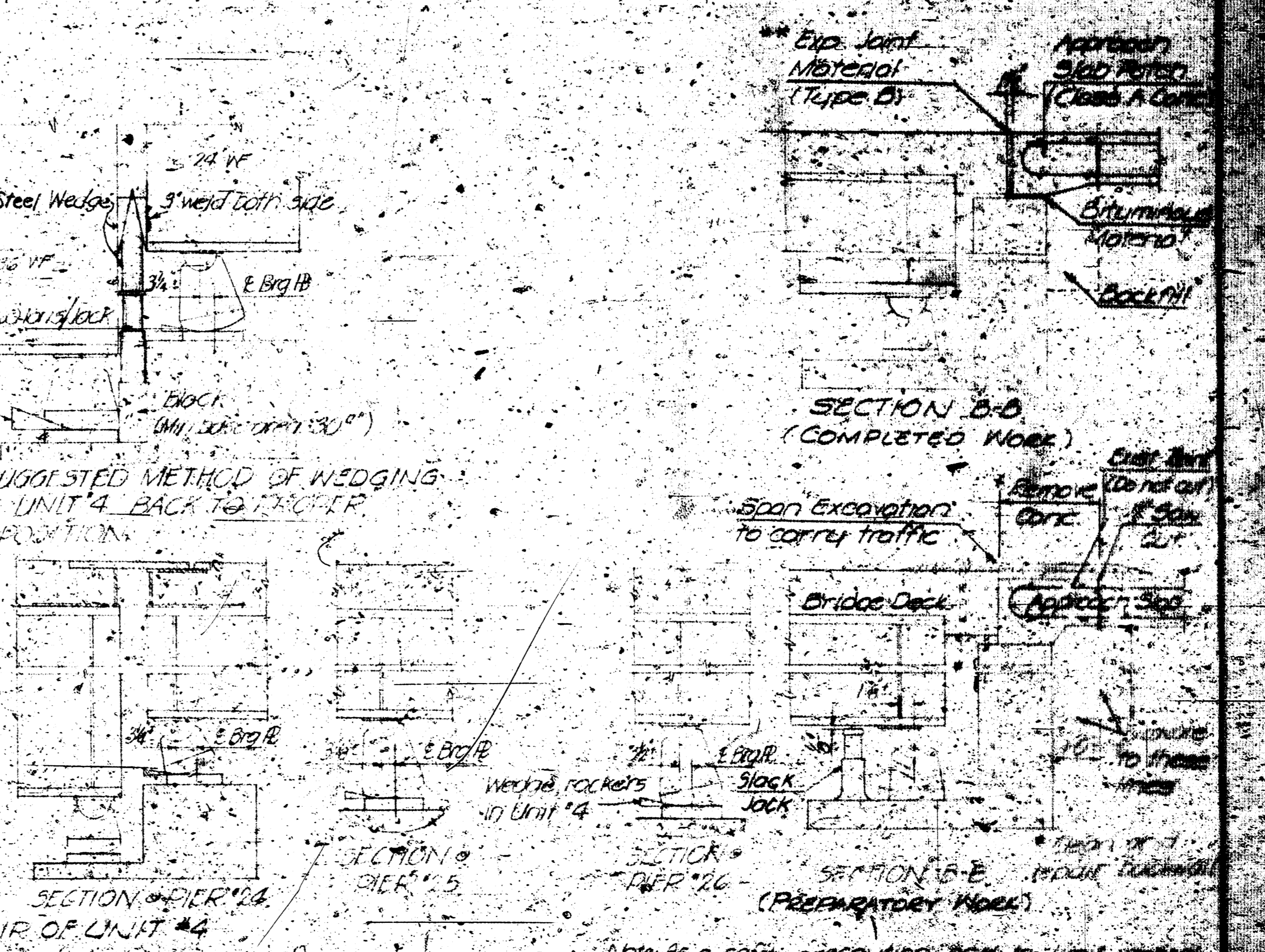
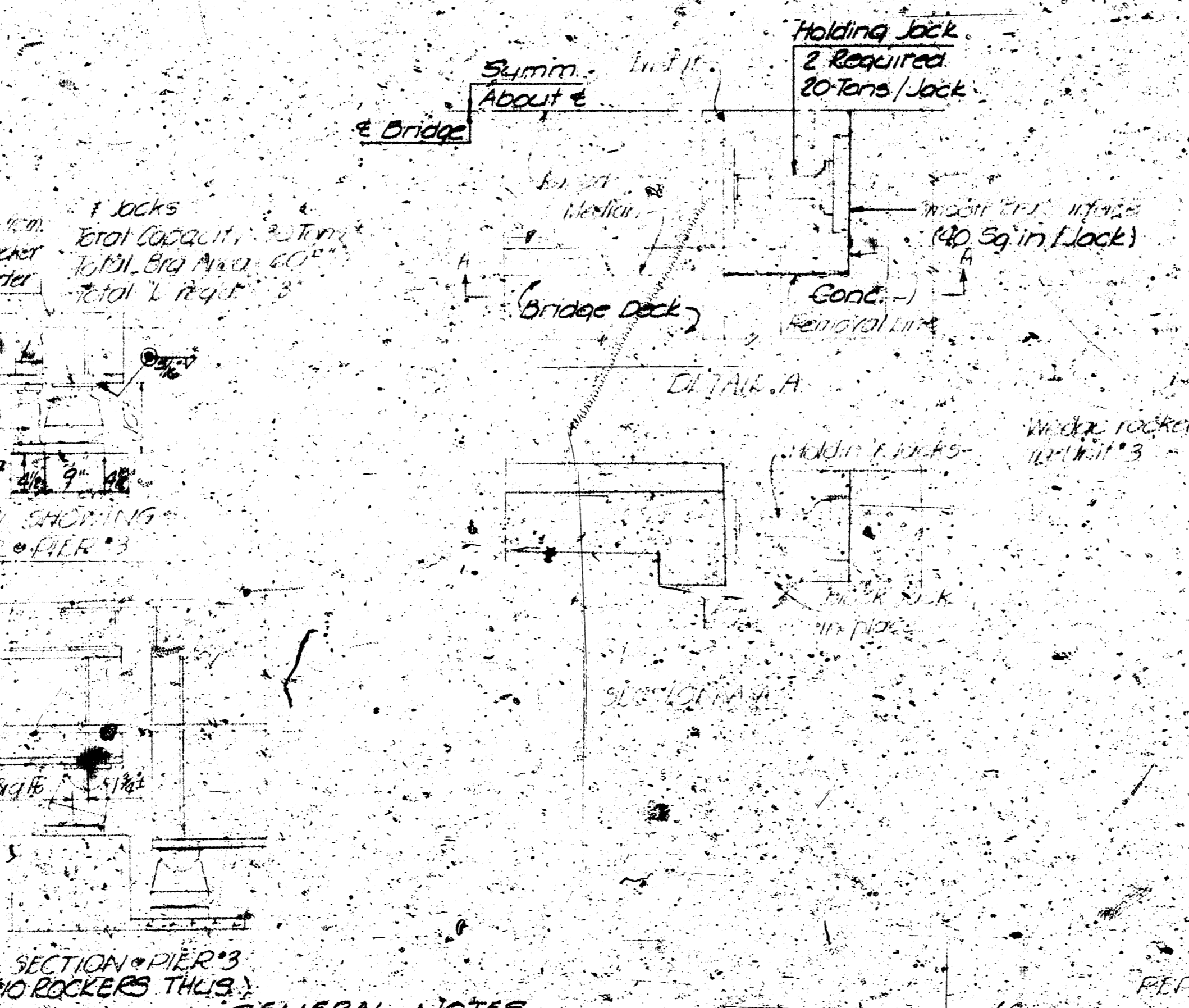
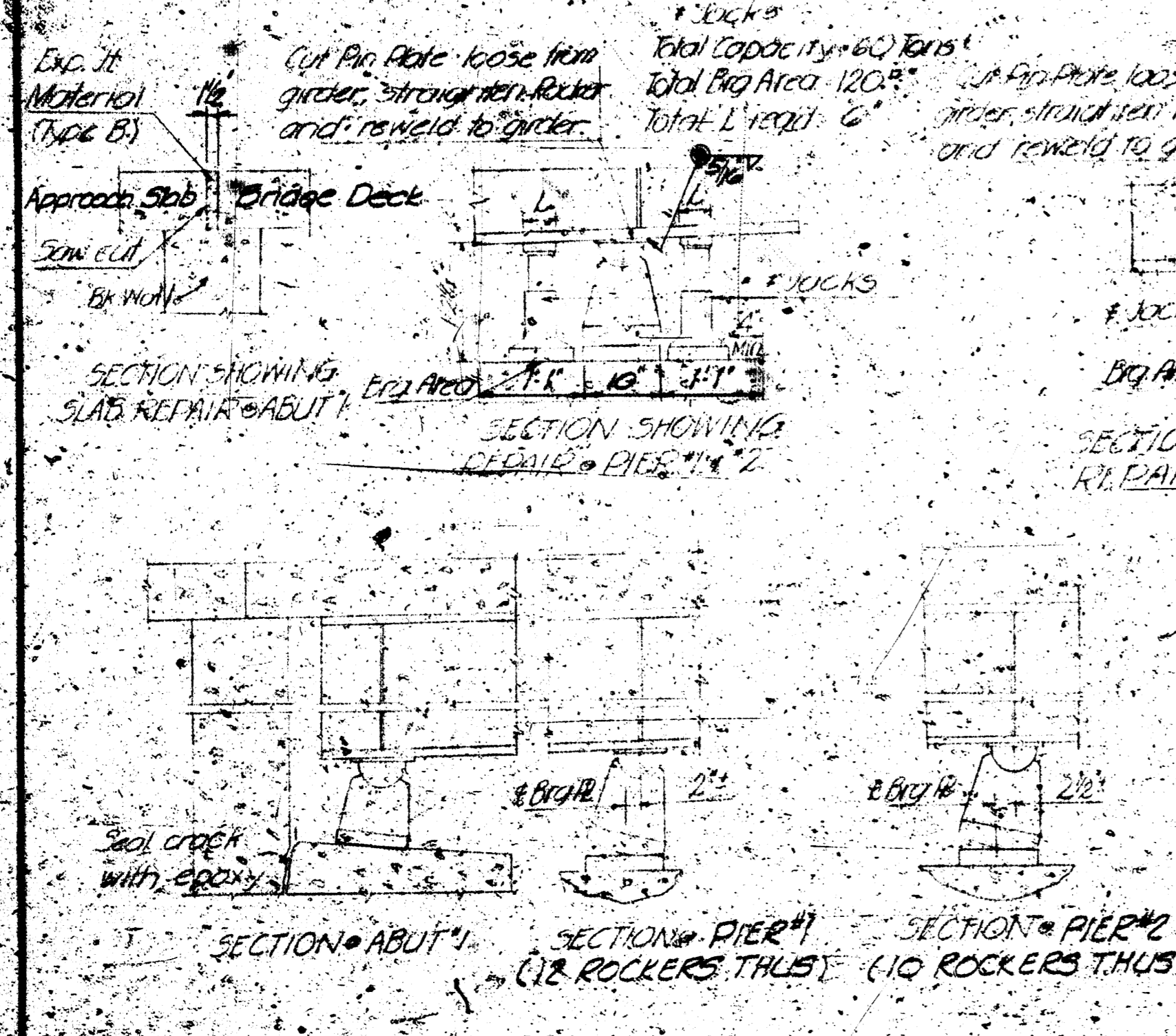
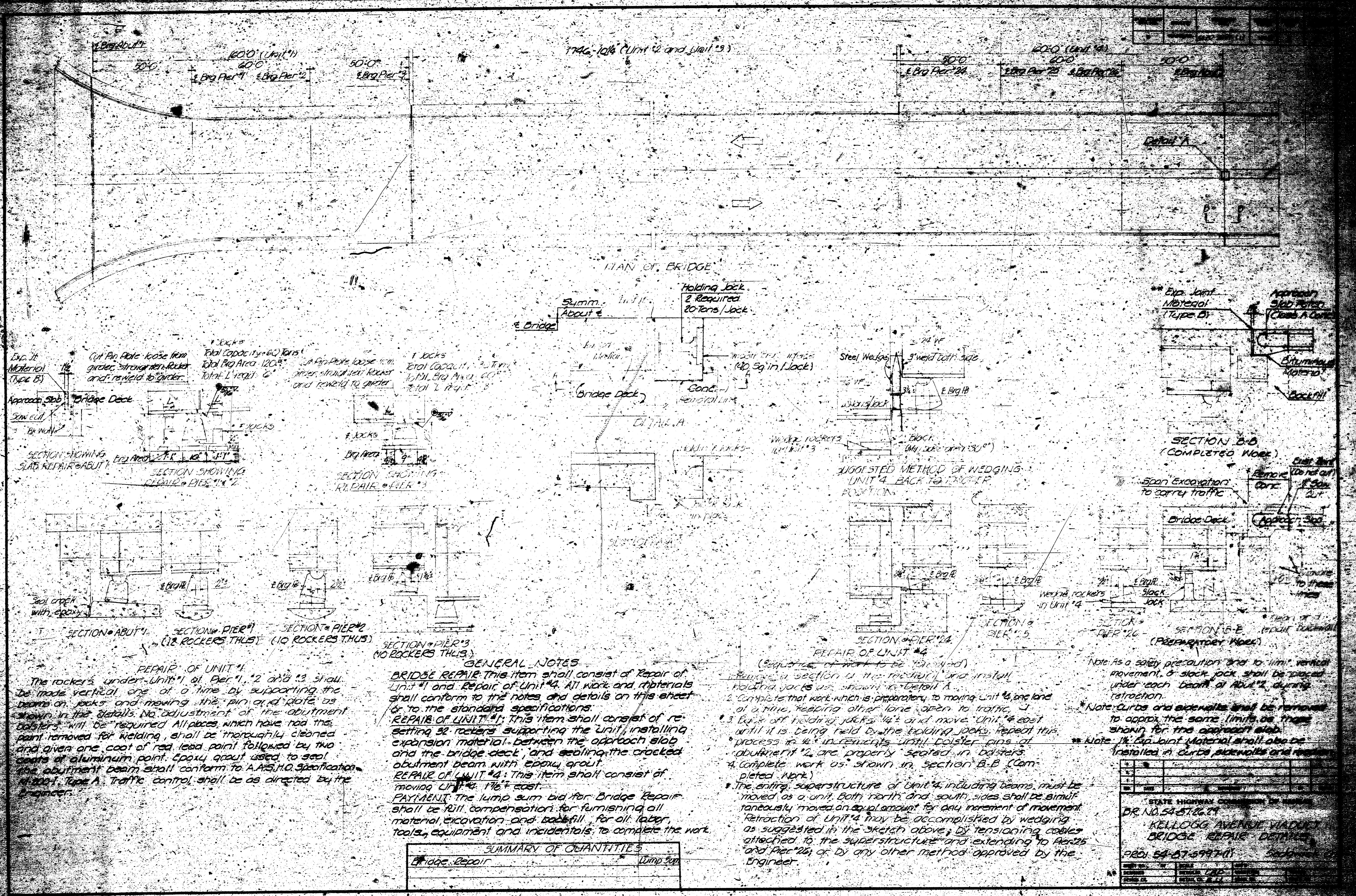
STATE HIGHWAY ENGINEER  
STATE HIGHWAY COMMISSION OF KANSAS

RECOM. FOR APPROVAL DATE

DISTRICT ENGINEER  
BUREAU OF PUBLIC ROADS  
DEPARTMENT OF COMMERCE

APPROVED DATE

DIVISION ENGINEER  
BUREAU OF PUBLIC ROADS  
DEPARTMENT OF COMMERCE



**REPAIR OF UNIT 4**  
 The rockers under Unit 4, at Pier 1, 2 and 3 shall be made vertical one at a time by supporting the beams on jacks and moving the pair of plates as shown in the details. No adjustment of the abutment bolsters will be required. All places which have had the paint removed for welding, shall be thoroughly cleaned and given one coat of red lead paint followed by two coats of aluminum paint. Epoxy grout used to seal the abutment beam shall conform to AASHTO Specification of 1994, Type A. Traffic control shall be as directed by the Engineer.

**GENERAL NOTES**  
**BRIDGE REPAIR** This item shall consist of repair of Unit 4 and repair of Unit 4. All work and materials shall conform to the notes and details on this sheet & to the standard specifications.  
**REPAIR OF UNIT 4:** This item shall consist of resetting 32 rockers supporting the Unit, installing expansion material between the approach slab and the bridge deck, and sealing the cracked abutment beam with epoxy grout.  
**REPAIR OF UNIT 4:** This item shall consist of moving Unit 4. 116' cost.  
**PAYMENT** The lump sum bid for Bridge Repair shall be full compensation for furnishing all material, excavation and backfill, for all labor, tools, equipment and incidentals, to complete the work.

**REPAIR OF UNIT 4**  
 (Equipment of work to be furnished)  
 1. Remove in section 11 the rockers and install holding jacks as shown in detail A.  
 2. Complete that work which is preparatory to moving Unit 4 one lane at a time, leaving other lane open to traffic.  
 3. Begin off holding jacks 1/2" and move Unit 4 east until it is being held by the holding jacks. Repeat this process in 1/2" increments until bolster pins of abutment 2 are properly seated in bolsters.  
 4. Complete work as shown in Section B-B (Completed Work).  
 5. The entire superstructure of Unit 4, including beams, must be moved as a unit. Both north and south sides shall be simultaneously moved on a platform for any increment of movement. Retraction of Unit 4 may be accomplished by wedging as suggested in the sketch above, by tensioning cables attached to the superstructure and extending to Piers 2 and Pier 2a or by any other method approved by the Engineer.

Note: As a safety precaution prior to unit vertical movement, a stock jack shall be placed under each beam at Abut 2 during retraction.  
 Note: Curbs and sidewalks shall be removed to approx. the same limits as those shown for the approach slab.  
 Note: 1/2" cap joint material shall also be installed in curbs sidewalks and road.

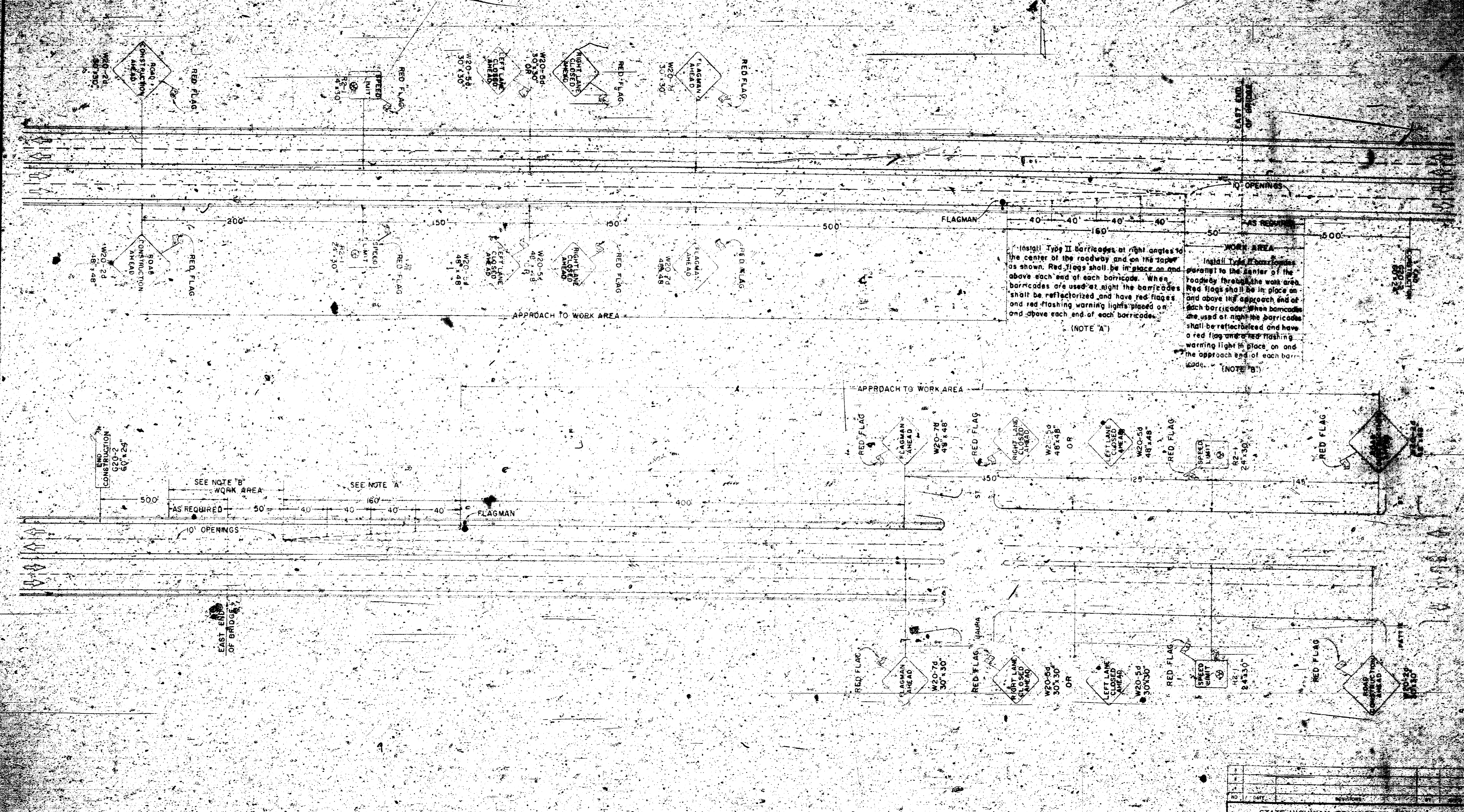
**SUMMARY OF QUANTITIES**

Bridge Repair	Lump Sum

STATE HIGHWAY COMMISSION BY ORDER  
 DR. NO. 54-5766-74  
 KELLOGG AVENUE, MADISON  
 BRIDGE REPAIR - DISTRICT  
 P&S 54-5766-74

**TRAFFIC CONTROL SIGNING  
FOR TRAFFIC MOVEMENT THROUGH  
BRIDGE REPAIR ON FOUR-LANE HIGHWAY**

PROJ. 54-67-597(1)



Install Type II Barricades at right angles to the center of the roadway and on the top as shown. Red flags shall be placed on and above each end of each barricade. When barricades are used at night the barricades shall be reflectorized and have red flags and red flashing warning lights placed on and above each end of each barricade.

(NOTE 'A')

Install Type II Barricades at right angles to the center of the roadway and on the top as shown. Red flags shall be placed on and above the approach end of each barricade. When barricades are used at night the barricades shall be reflectorized and have a red flag with red flashing warning light in place on and the approach end of each barricade.

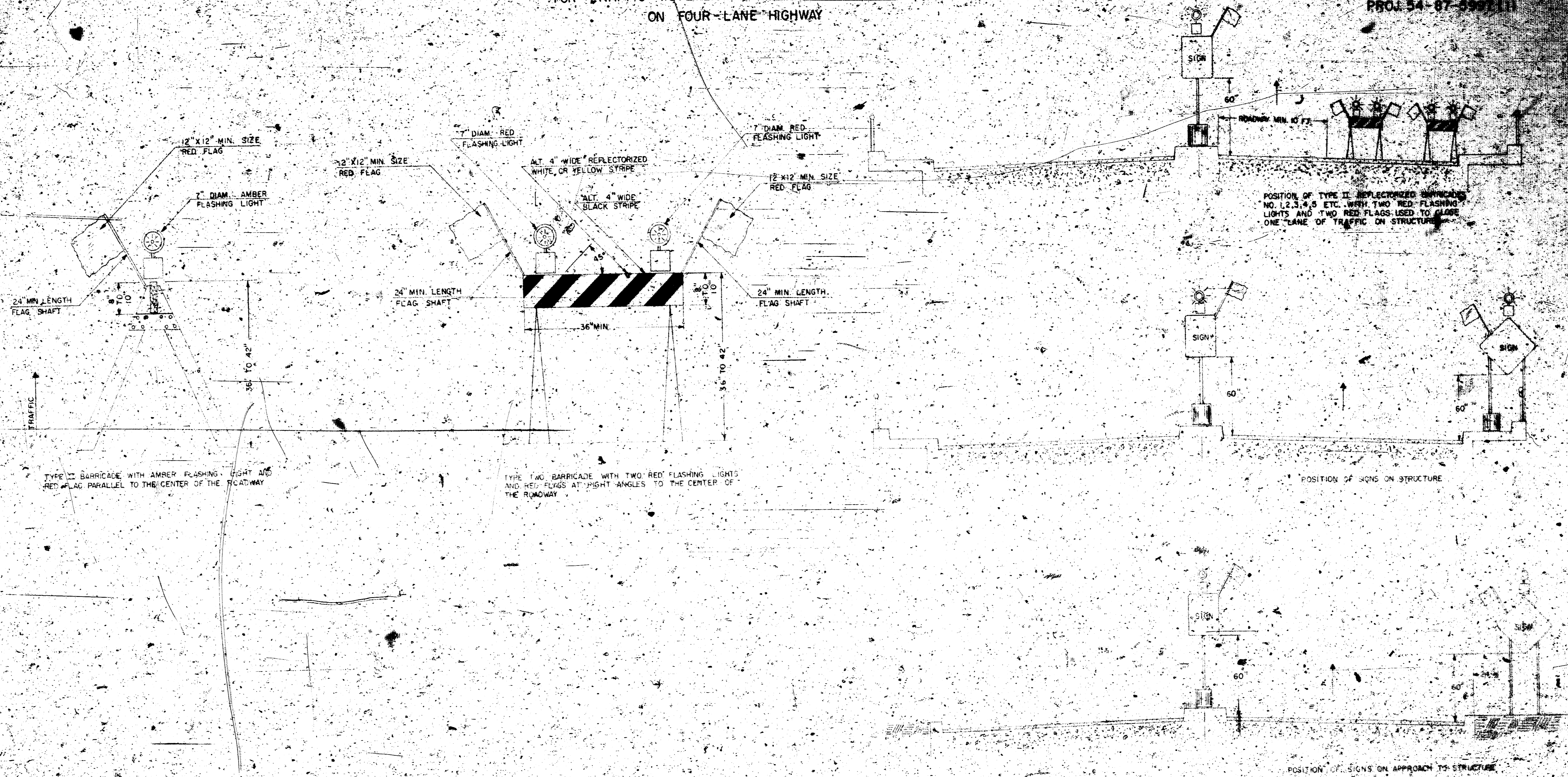
(NOTE 'B')

NOTE  
POSTED SPEED LIMIT TO BE DETERMINED BY DIVISION ENGINEER

STATE HIGHWAY COMMISSION OF ILLINOIS CONSTRUCTION TRAFFIC CONTROL DEVICES AND LOCATIONS FOR A FOUR-LANE BRIDGE			
DATE	SCALE	BY	CHECKED

TRAFFIC CONTROL DEVICES  
FOR TRAFFIC MOVEMENT THROUGH BRIDGE REPAIR  
ON FOUR-LANE HIGHWAY

PROJ. 54-67-527(11)



NOTE:  
All traffic control devices and flagman equipment shall comply to Section A7 of the Standard Specification Edition of 1960 and to any supplemental Specification to this Section.  
Whenever work is in progress at night all barricades and signs shall be reflectorized and have flashing warning lights installed as shown.  
The amber and red flashing lights shall have 2 inch minimum diameter lenses and shall be regulated to flash 50 to 55 times per minute. The length of dwell shall not be less than 25 per cent on time.  
All barricades, signs, red flags and flashing warning lights when used shall be securely erected and maintained in good condition at all times.  
When no work is in progress on the roadway at night over the work area or for any other reason, the opening in the bridge floor shall be covered with a steel plate, securely fastened, and the roadway opened to traffic. All signs shall then be removed or securely covered with opaque material.

STATE HIGHWAY COMMISSION OF KANSAS			
CONSTRUCTION TRAFFIC CONTROL DEVICES AND LOCATIONS FOR A FOUR-LANE BRIDGE			
SHEET NO.	OF	CONTRACT NO.	SECTION
DESIGNED BY	DATE	APPROVED BY	DATE
REVISION NO.	DESCRIPTION	DATE	BY