

**Street Improvements for  
Westridge Village Addition  
Wichita, Kansas  
Holland Lane:  
From the N. line of Harvest Lane  
to the S. line of 17th Street North  
Michael E. Lindebak, PE, City Engineer  
City of Wichita Project No. 472-82268  
Index Code 761593  
April, 1993**

**General Notes**

- Contractor will be required to provide a minimum advance notice of forty-eight (48) hours to utility companies prior to starting any excavation as follows:

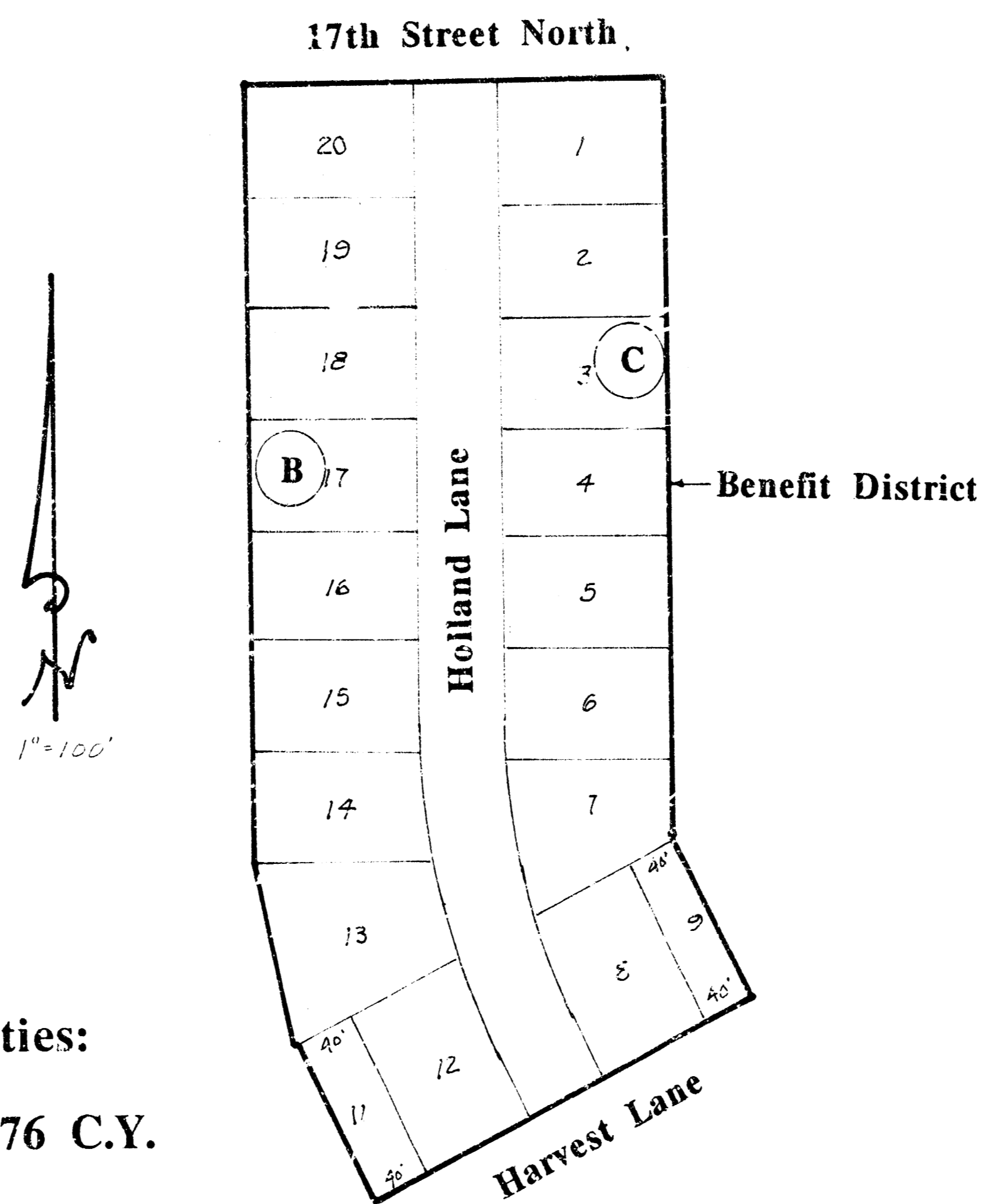
Kansas One-Call 587-2470

Contractor will also be required to provide a minimum advance notice of twenty-four (24) hours to Arksa Electric Resources @ 942-8811 prior to starting any excavation.

The Contractor must notify the following in case of an emergency:

Cablevision	262-4270 or 263-2061
KPL/Gas Service Company	263-7311
Kansas Gas and Electric	274-1141
Arksa Gas Company	942-8350 or 263-8161
Southwestern Bell Telephone Co.	1-574-2611
City of Wichita Water Dept.	708-4508
City of Wichita Sewer Maintenance	732-0262

- Existing utilities are shown based on the best information available. Other utilities may exist which are not shown. Contractor shall be responsible for the final field determination of existing utility locations. Contractor shall assume all risk for damage to existing utilities. Contractor shall hand excavate as required to locate existing underground utilities which may conflict with waterline construction. Contractor shall coordinate his operations with any utility relocation which may be required.
- Contractor shall be responsible for preserving property irons. The Contractor shall be required to re-establish any property irons which are damaged or destroyed by his construction operations. Such irons shall be re-established by a licensed land surveyor in accordance with state law.
- Contractor shall satisfy himself of subsurface conditions prior to construction.
- Trees and shrubs in public right-of-way which are in direct conflict with proposed new construction shall be removed by the Contractor with the Engineer's approval. Trees and shrubs which are not in direct conflict with proposed new construction shall be saved and protected from damage.
- Rubble from the removal of miscellaneous structures including any trees removed, tree trimmings, and excess excavation which is to be wasted, shall be disposed of on sites provided by the Contractor. These sites shall also be approved by the Engineer as to suitability, appearance and site location. Locations that, in the opinion of the Engineer, leave an unsightly appearance will not be approved. All disposal sites must be approved by the Kansas Department of Health and Environment. Material either stockpiled or disposed of in a flood plain will require a Kansas State Board of Agriculture Permit. Any material dumped in waters of the United States or wetlands is subject to U.S. Corps of Engineers permitting regulations. Any material buried or stockpiled beyond approved construction limits may require archaeological investigations unless buried in a previously approved disposal location.
- The water department shall field locate water valves one time during construction when requested by the Contractor. It shall be the Contractor's responsibility to preserve such field locations during the construction process. Water valves, water valve boxes or fire hydrants damaged during construction shall be repaired by the Contractor at his own expense.
- The Contractor shall adjust water valve boxes as directed by the Engineer. This work shall be subsidiary to other bid items.
- This project does not include any provisions for construction of driveways.
- Sawing and removing existing asphalt surface and sawing and removing existing curb ends at tie-in points shall be considered subsidiary to pavement.
- Earthwork quantity is calculated based on the crushed rock alternate. Any difference in earthwork for alternate II shall be considered as subsidiary to subgrade manipulation.



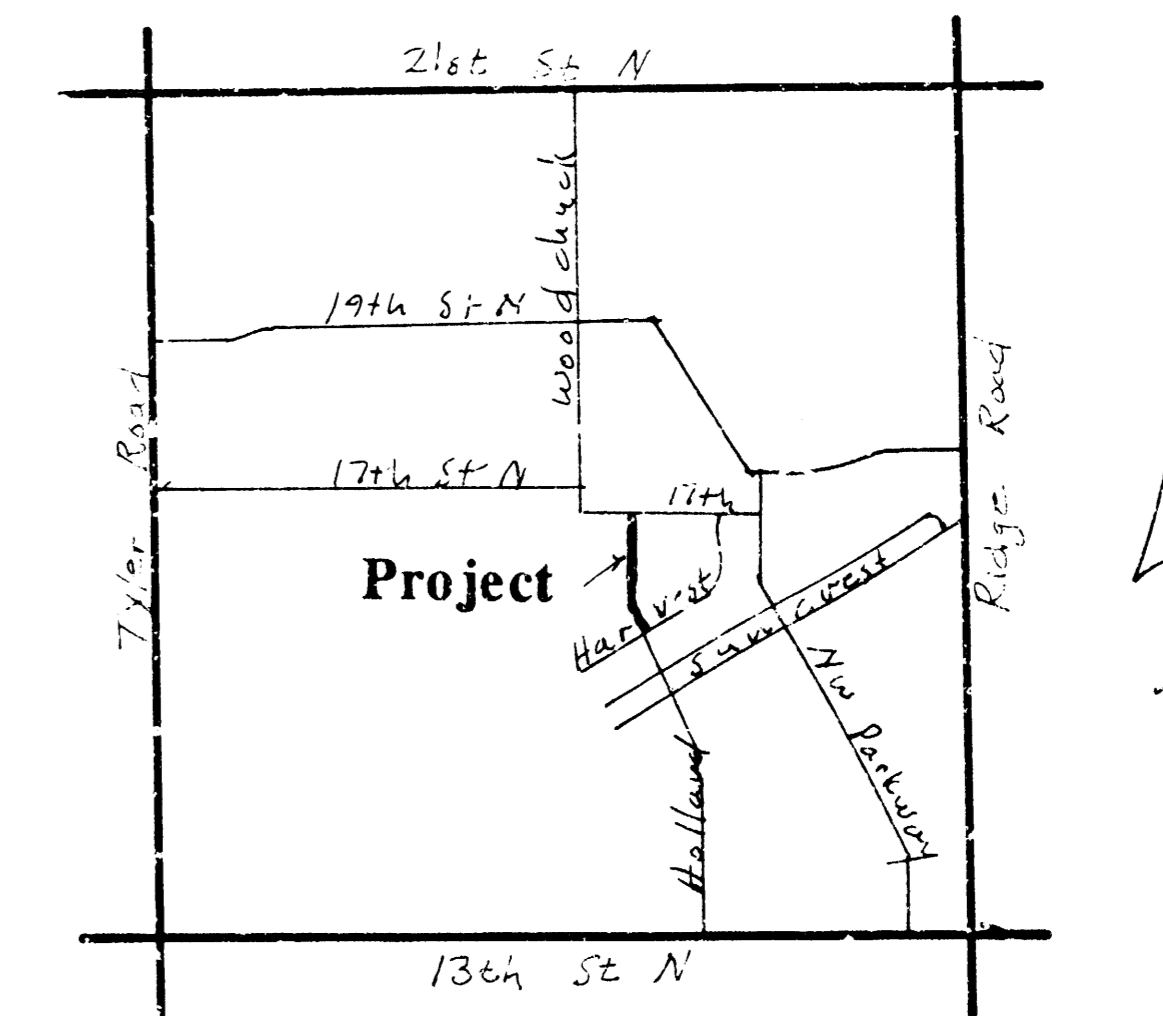
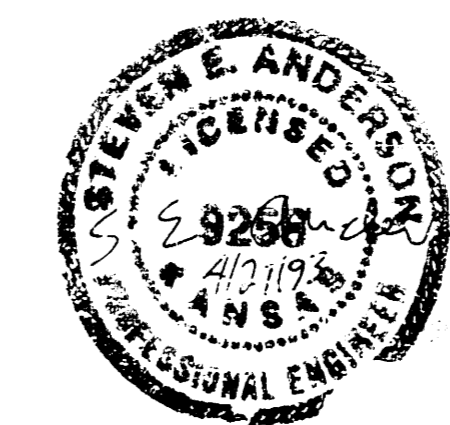
**Earthwork Quantities:**

Excavation 1076 C.Y.

Compacted Fill 6 C.Y.

Loose Fill 382 C.Y.

Plans Prepared By:  
**S. E. Anderson PE/LS**  
Consulting Engineer  
Whitewater, Kansas



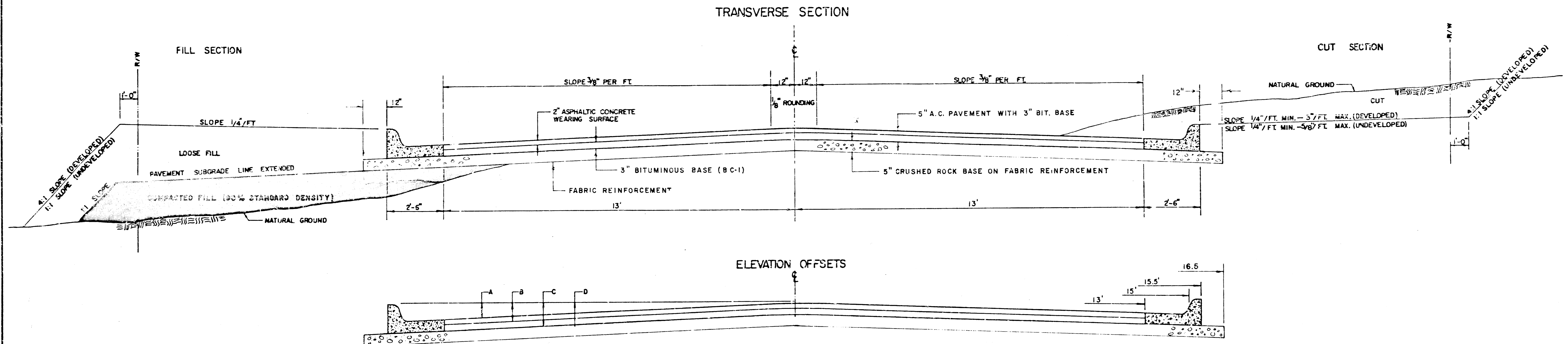
**Location Map**

**Index of Sheets**

- |       |                         |
|-------|-------------------------|
| 1.    | Title Sheet             |
| 2, 2A | Typical Section/Details |
| 3-4.  | Plan                    |
| 5-6.  | Cross Sections          |

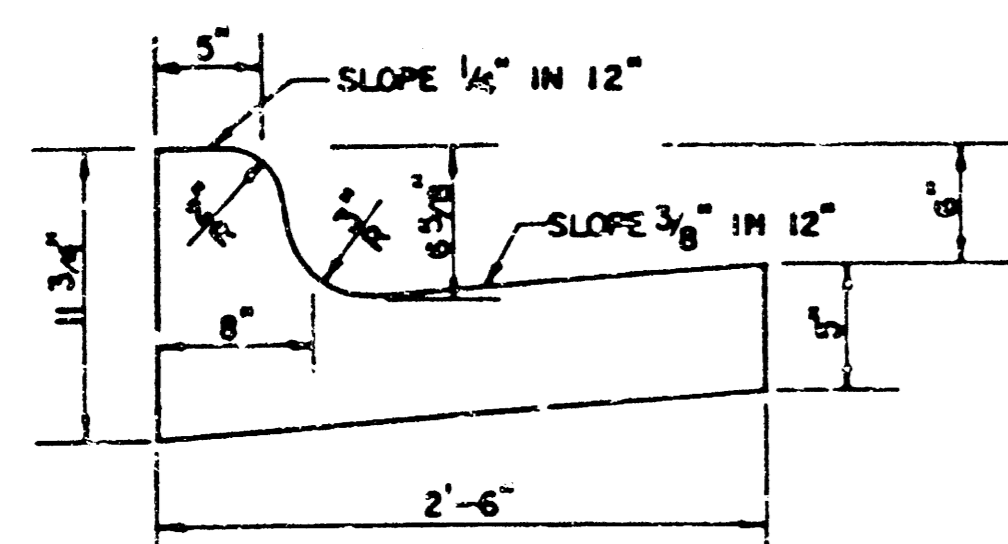
**Total Project Length: 710.56 feet**

# TYPICAL 31' PAVEMENT DETAILS

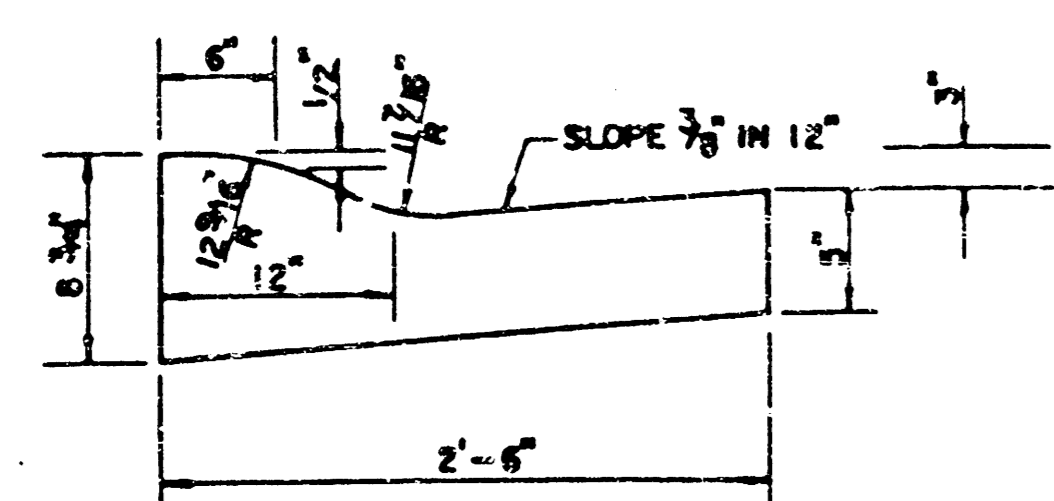


	DISTANCE FROM CENTERLINE (LT. & RT.)											
	0'	2'	4'	6'	7.5'	10'	12'	13'	15'	15.5'	15.67'	16.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	.10	.14	.21	.27	.32	.39	.46	.49	---	---	---	---
B: TOP OF CURBS TO TOP OF BASE LIFT	.27	.31	.37	.43	.48	.56	.62	.65	---	---	---	---
C: TOP OF CURBS TO TOP OF ROCK BASE	.52	.56	.62	.68	.74	.81	.88	.91	.97	.98	.99	---
D: TOP OF CURBS TO TOP OF SUBGRADE	---	---	---	---	---	---	---	---	---	---	---	---

COMBINED CURB & GUTTER



ROLL TYPE COMBINED CURB & GUTTER



## GENERAL NOTES

FABRIC BASE REINFORCEMENT SHALL BE R x 1100 BY TENSAR CORP. OR LBO201 BY TENAX CORP. OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CRUSHED ROCK SHALL BE UNIFORMLY GRADED FROM 1-1/2" MAXIMUM SIZE TO NOT MORE THAN 10% PASSING A NO. 200 SIEVE. ROCK QUALITY SHALL BE THE SAME AS SPECIFIED FOR COARSE AGGREGATE FOR CONCRETE MIXES.

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

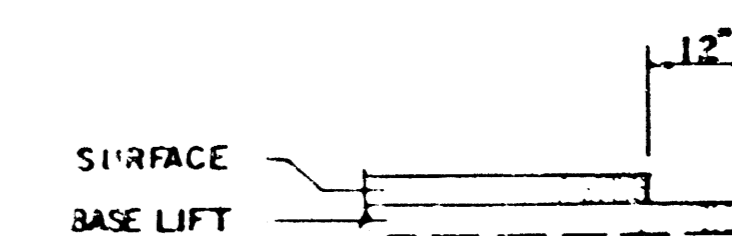
A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

## TRANSVERSE CONSTRUCTION JOINTS



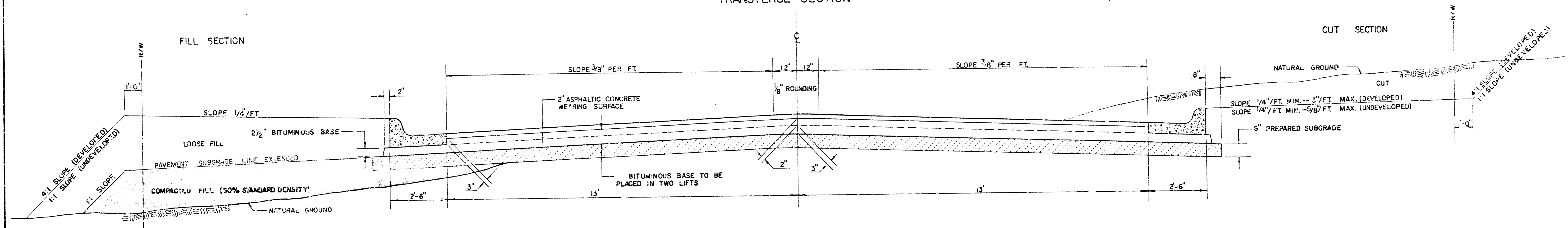
TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

REVISED 1-3-92 JKB

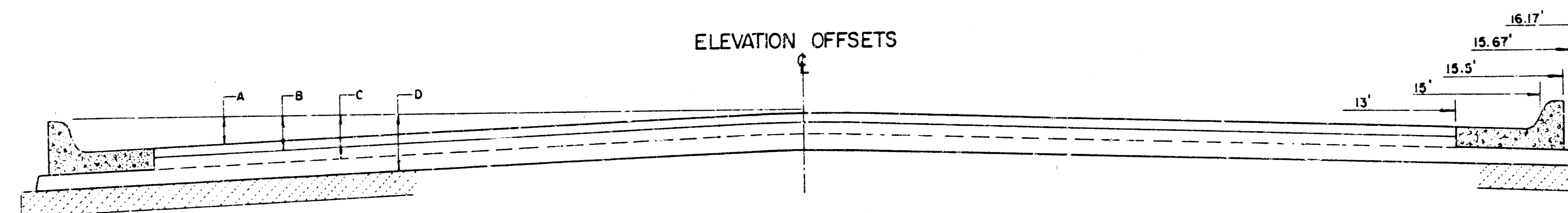
5" RESIDENTIAL ASPHALTIC CONCRETE  
PAVEMENT WITH CRUSHED ROCK BASE  
ON FABRIC REINFORCEMENT 2/6

# TYPICAL 3' PAVEMENT DETAILS

## TRANSVERSE SECTION

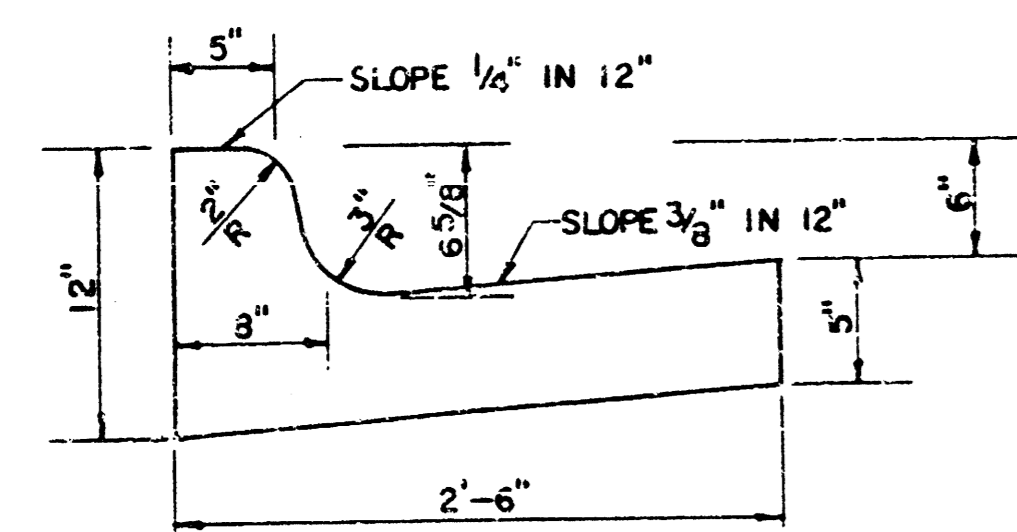


## ELEVATION OFFSETS

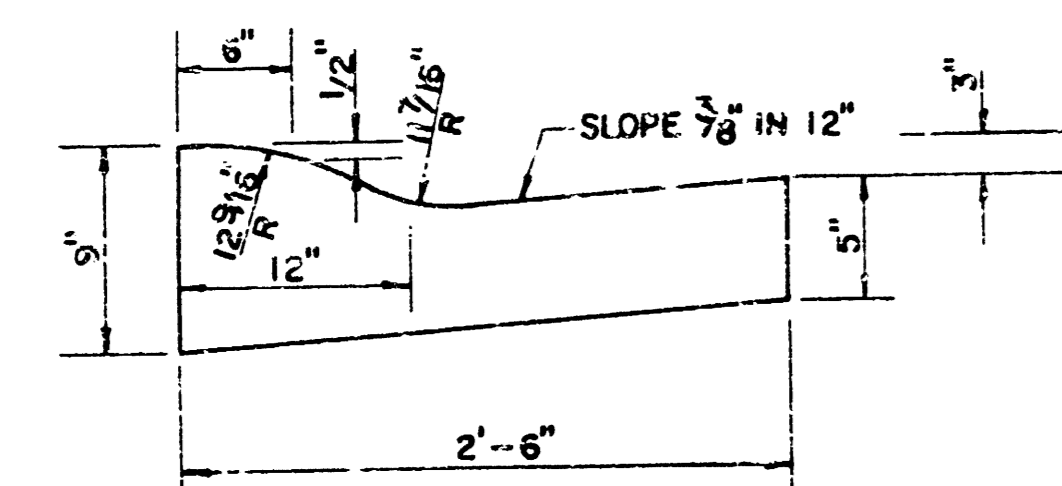


	DISTANCE FROM CENTERLINE (LT. & RT.)											
	0'	2'	4'	6'	7.5'	10'	12'	13'	15'	15.5'	15.67'	16.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.10	0.14	0.21	0.27	0.32	0.39	0.46	0.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.27	0.31	0.38	0.44	0.49	0.56	0.63	0.66	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.44	0.49	0.57	0.64	0.70	0.79	0.87	0.90	0.98	1.00	1.00	—
D: TOP OF CURBS TO TOP OF SUBGRADE	0.69	0.73	0.80	0.87	0.93	1.01	1.08	1.12	1.19	1.21	1.21	1.23

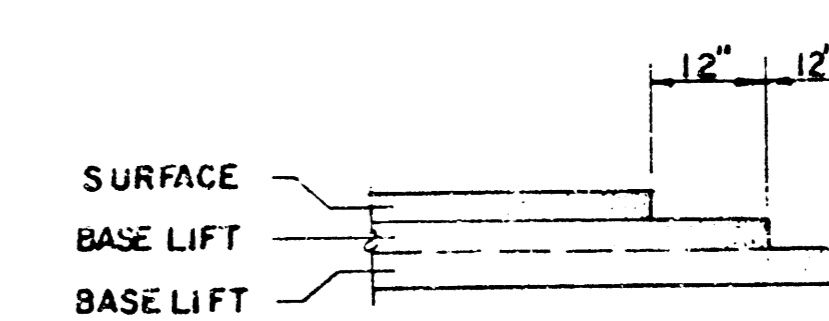
### COMBINED CURB & GUTTER



### ROLL TYPE COMBINED CURB & GUTTER



### TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

### GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

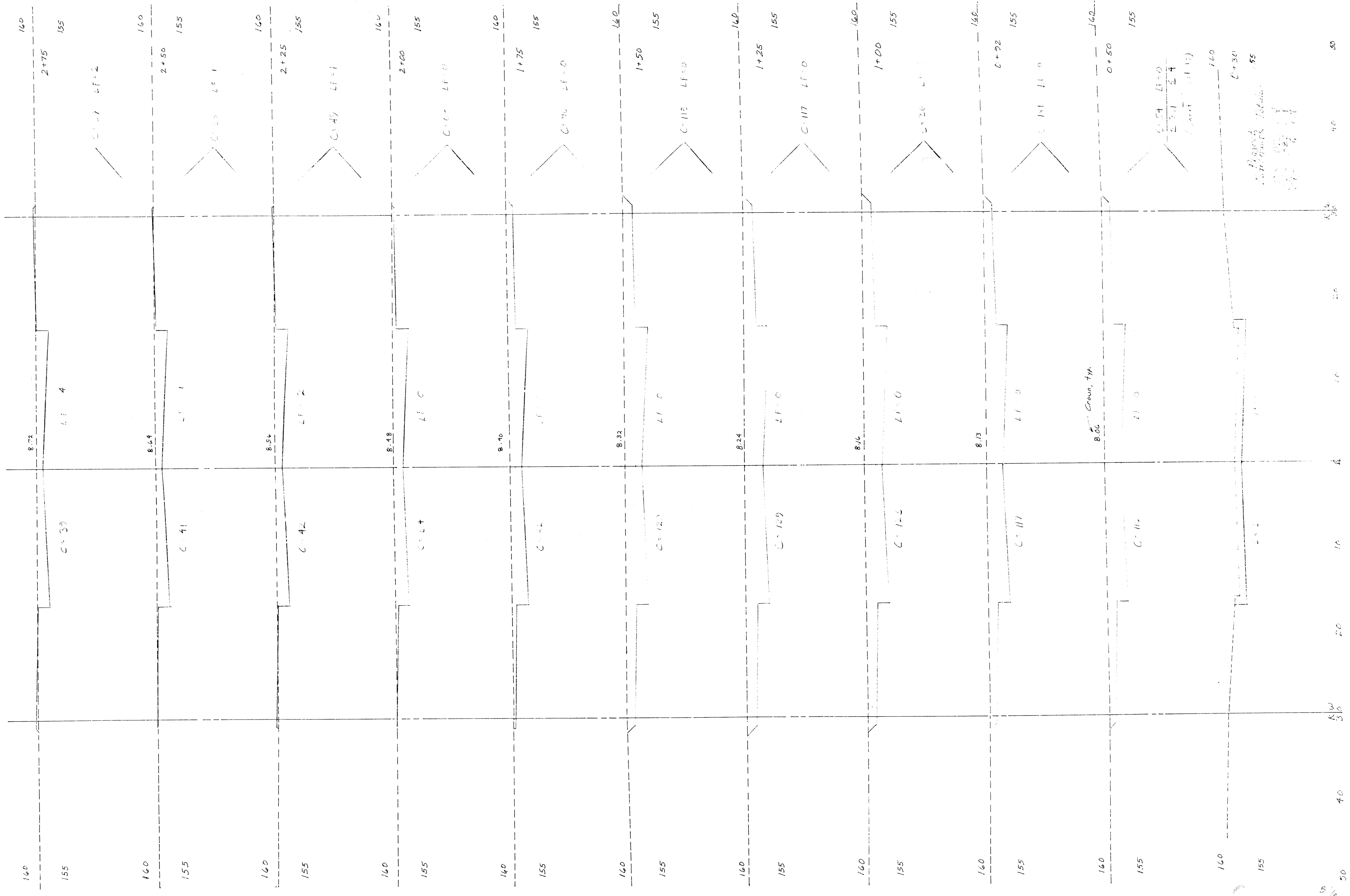
7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE CITY OF WICHITA, KANSAS

PROJECT NUMBER

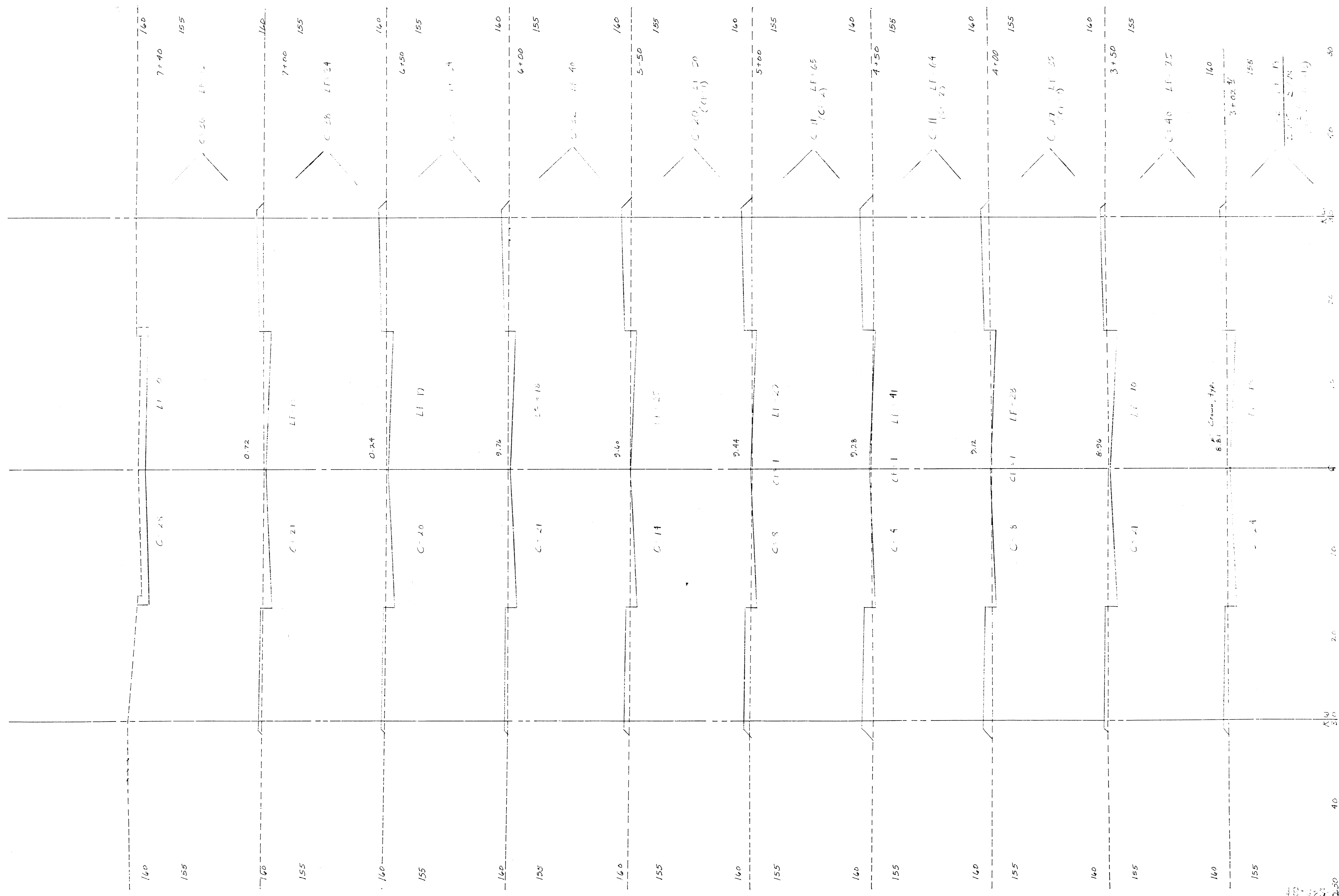
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