

STREETS IN COTTONWOOD VILLAGE SECOND ADDITION

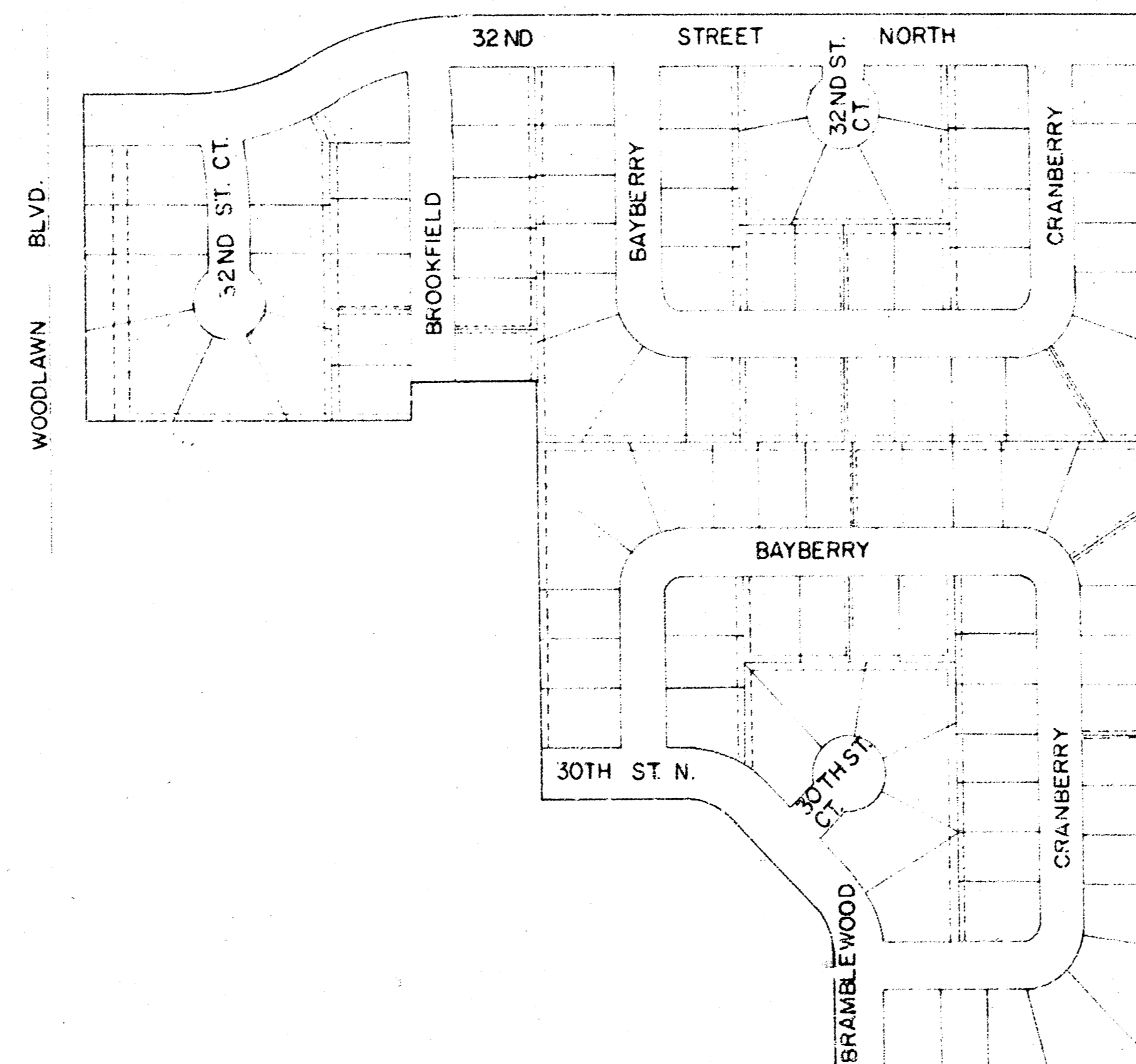
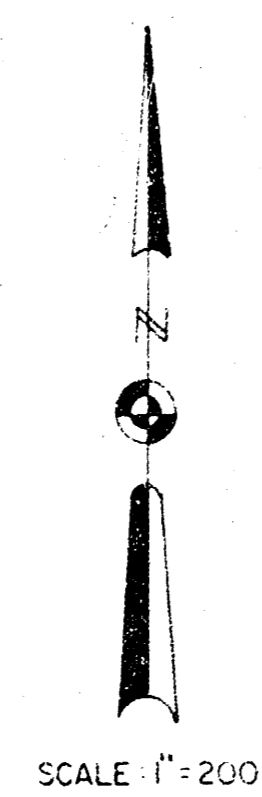
**PROJECT NO.
472-76-245-80699-000-000-001**

**CITY OF WICHITA, KANSAS
R. W. LINN CITY ENGINEER
JULY, 1978**

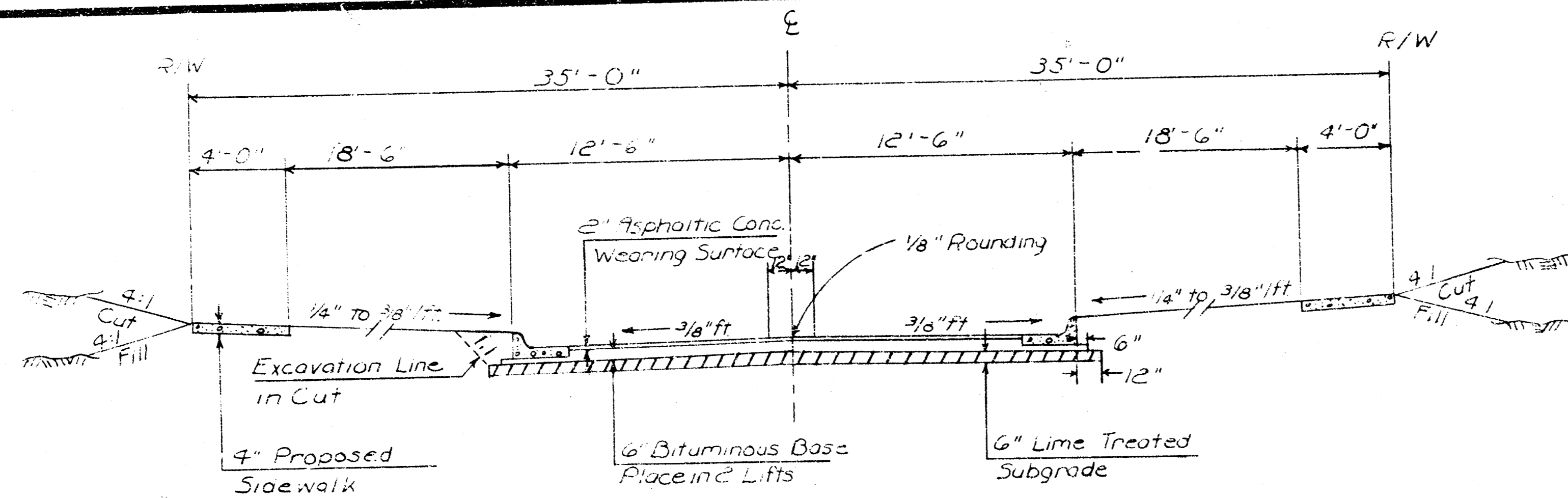
32ND ST. NORTH - E.L. WOODLAWN BLVD. TO E.L. OF ADD.
BROOKFIELD - S.L. LOT 1, BLK. 2 TO S.L. 32ND ST. NORTH
32ND ST. COURT - SERVING LOT 1 - LOT 9, BLK. 1
BAYBERRY - E.L. LOT 6, BLK. 3 TO S.L. 32ND ST. NORTH
CRANBERRY - E.L. LOT 6, BLK. 3 TO S.L. 32ND ST. NORTH
32ND ST. COURT - SERVING LOT 13 TO LOT 17, BLK. 3
30TH ST. NORTH - W.L. LOT 1, BLK. 4 TO C.L. 30TH ST. COURT
BRAMBLEWOOD - C.L. 30TH ST. COURT TO S.L. OF ADD.
30TH ST. COURT - N.L. 30TH ST. NORTH TO & INCLU. CUL-DE-SAC
BAYBERRY - N.L. 30TH ST. NORTH TO W.L. CRANBERRY
CRANBERRY - E.L. BRAMBLEWOOD TO S.L. BAYBERRY

INDEX TO DRAWINGS

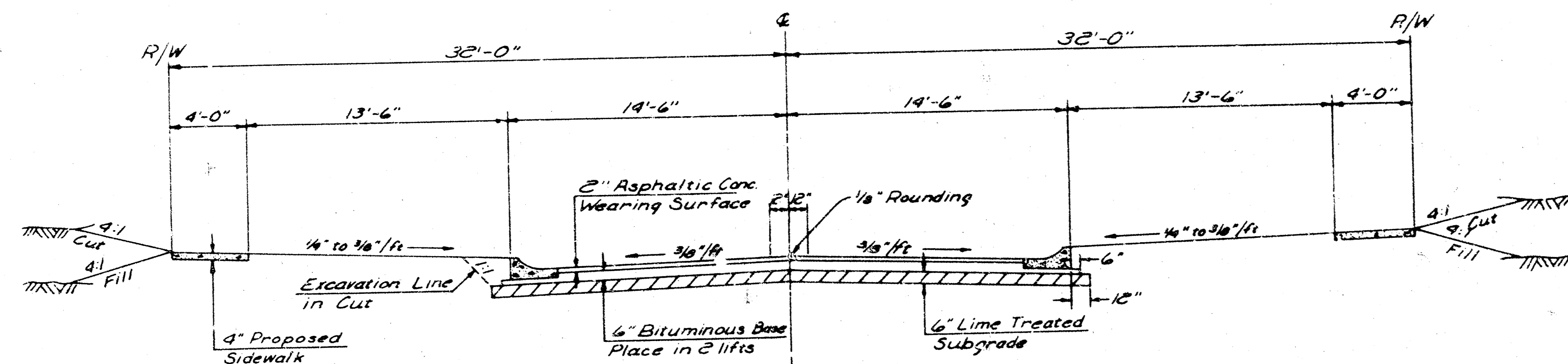
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-2A	PAVING DETAILS
3-5	32ND STREET NORTH
5	32ND STREET COURT - BLK. 3
6	32ND STREET COURT - BLK. 1
7	BROOKFIELD
8-9	BAYBERRY
10-11	CRANBERRY
12-13	30TH STREET NORTH
13	BRAMBLEWOOD
13	30TH STREET COURT
14-15	BAYBERRY
16-17	CRANBERRY
18-27	CROSS-SECTIONS



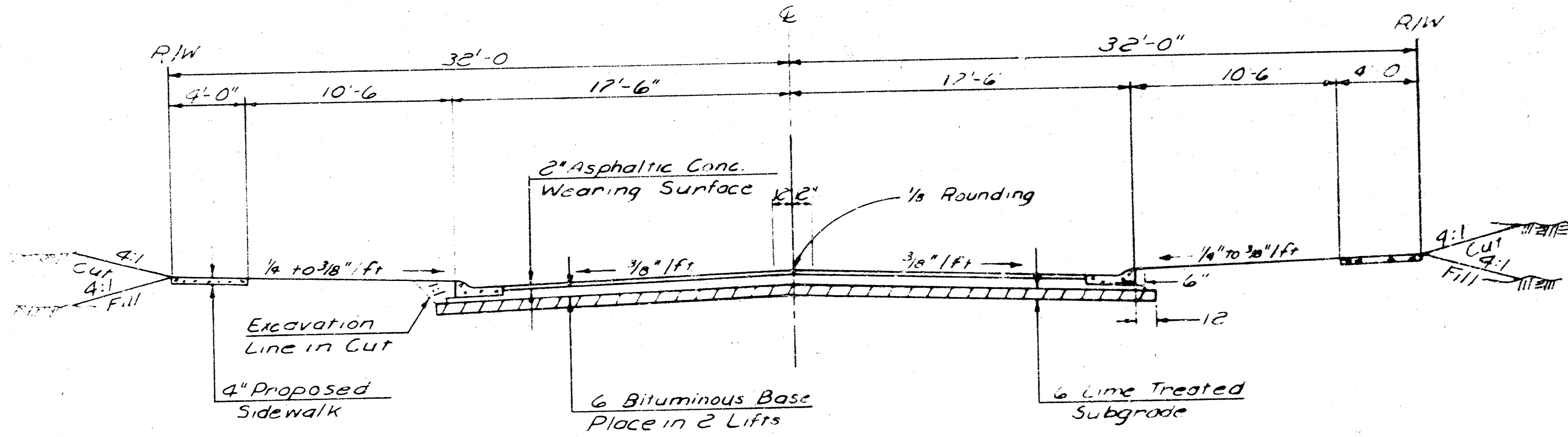
*Checked by R. W. Linn
7/18/78*



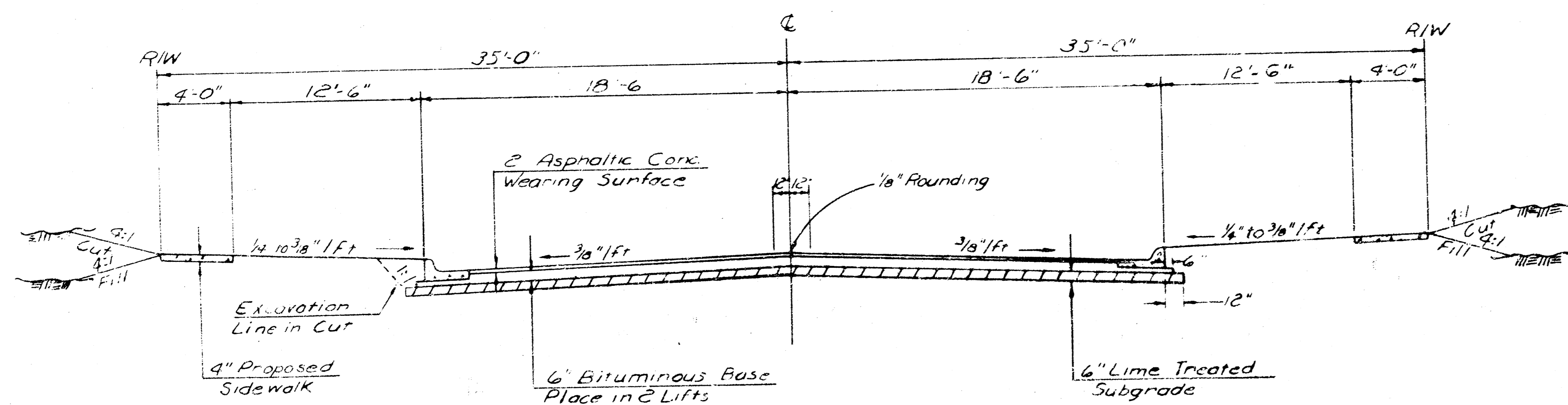
TYPICAL 25' B.B. SECTION
32ND STREET



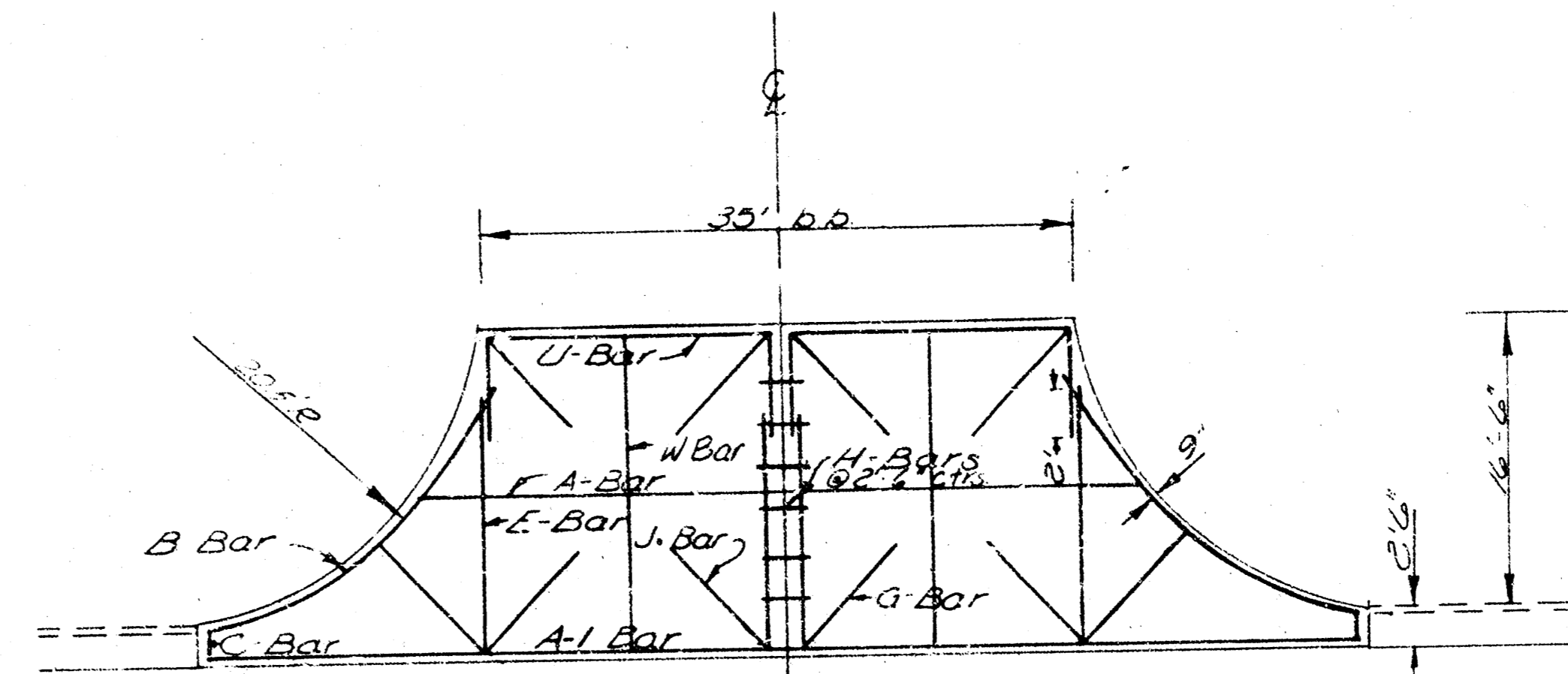
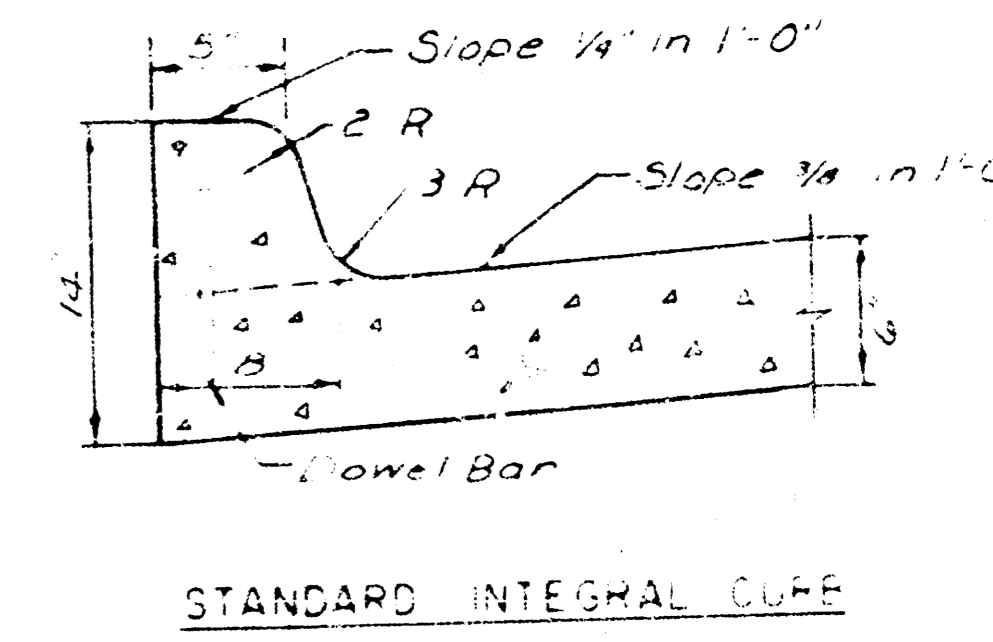
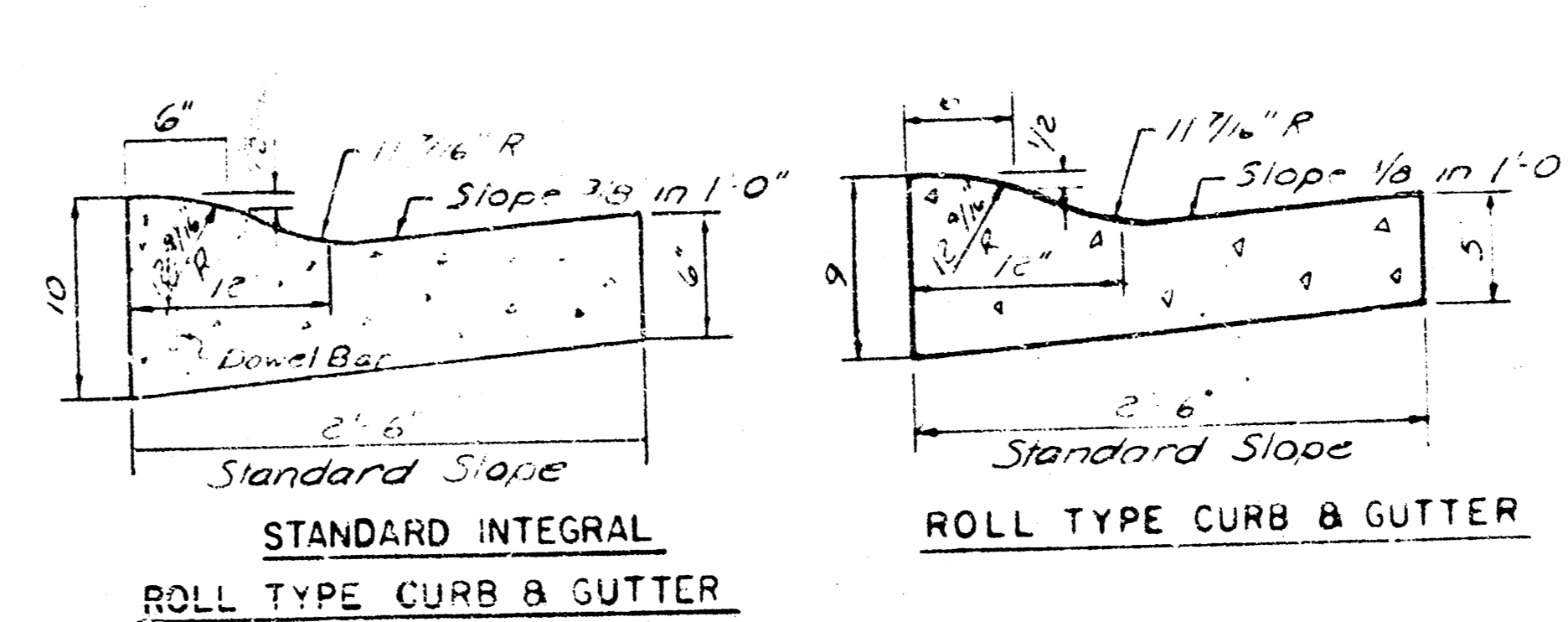
TYPICAL 29' B.B. SECTION
32ND STREET COURT



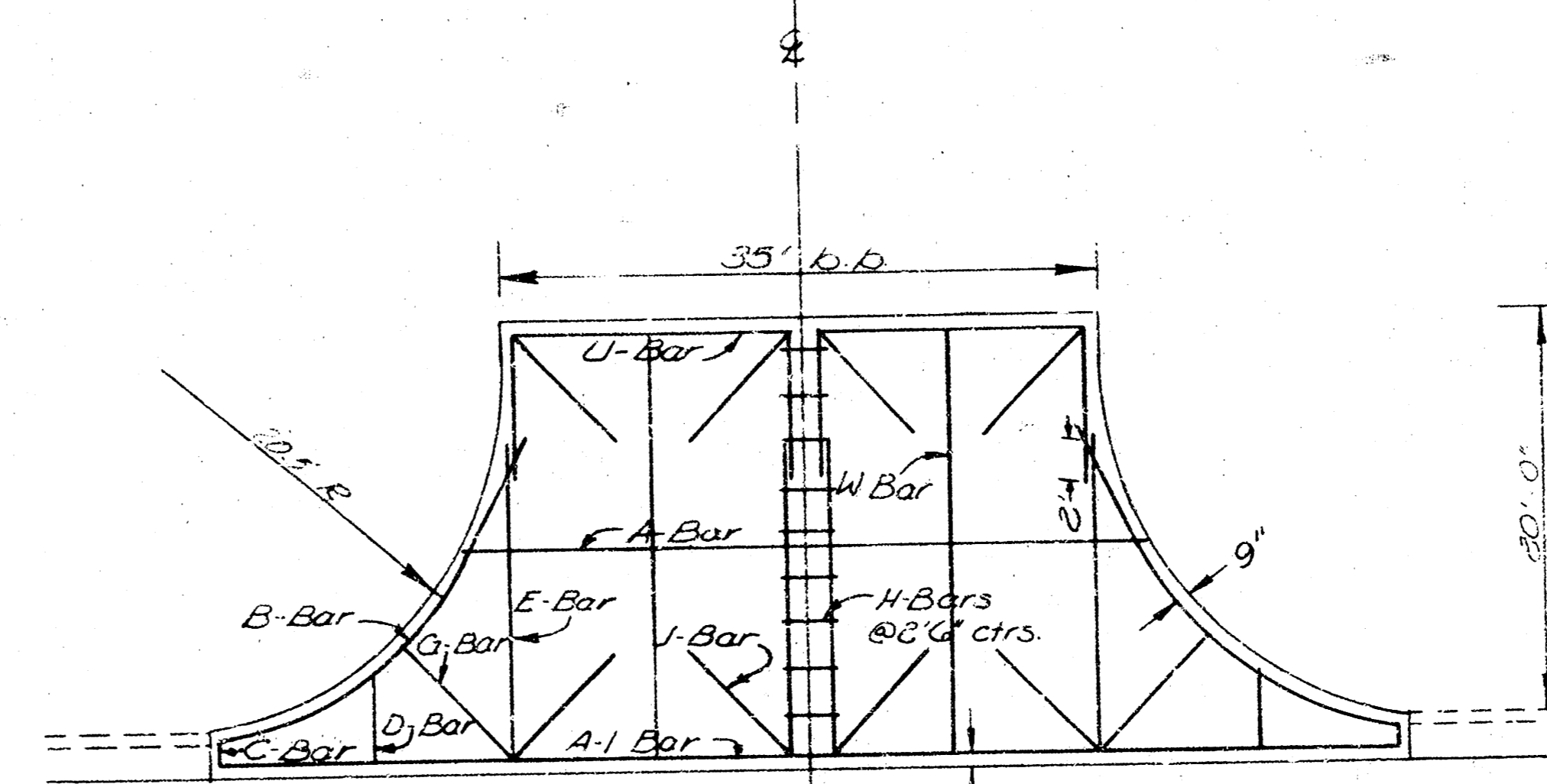
TYPICAL 35' B.B. SECTION
BAYBERRY, BROOKFIELD, CRANBERRY



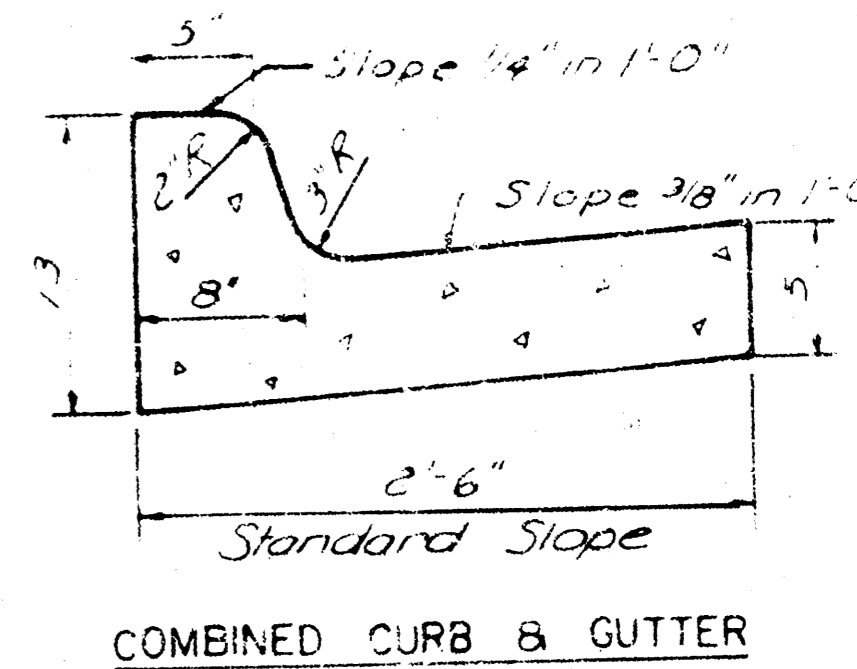
TYPICAL 37' B.B. SECTION
30TH STREET



TYPICAL REINFORCING FOR 6" CONCRETE SLAB INTERSECTION
WITH 2" ASPHALTIC CONCRETE BASE (16'-6" WIDTH)



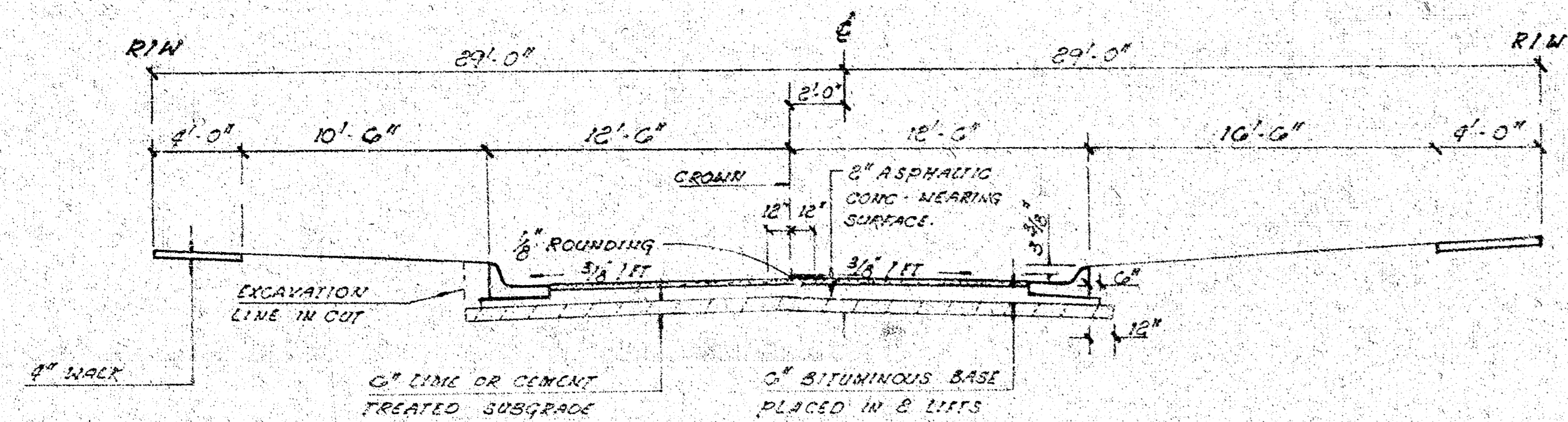
TYPICAL REINFORCING FOR 6" CONCRETE SLAB INTERSECTION
WITH 2" ASPHALTIC CONCRETE BASE (22'-6" WIDTH)



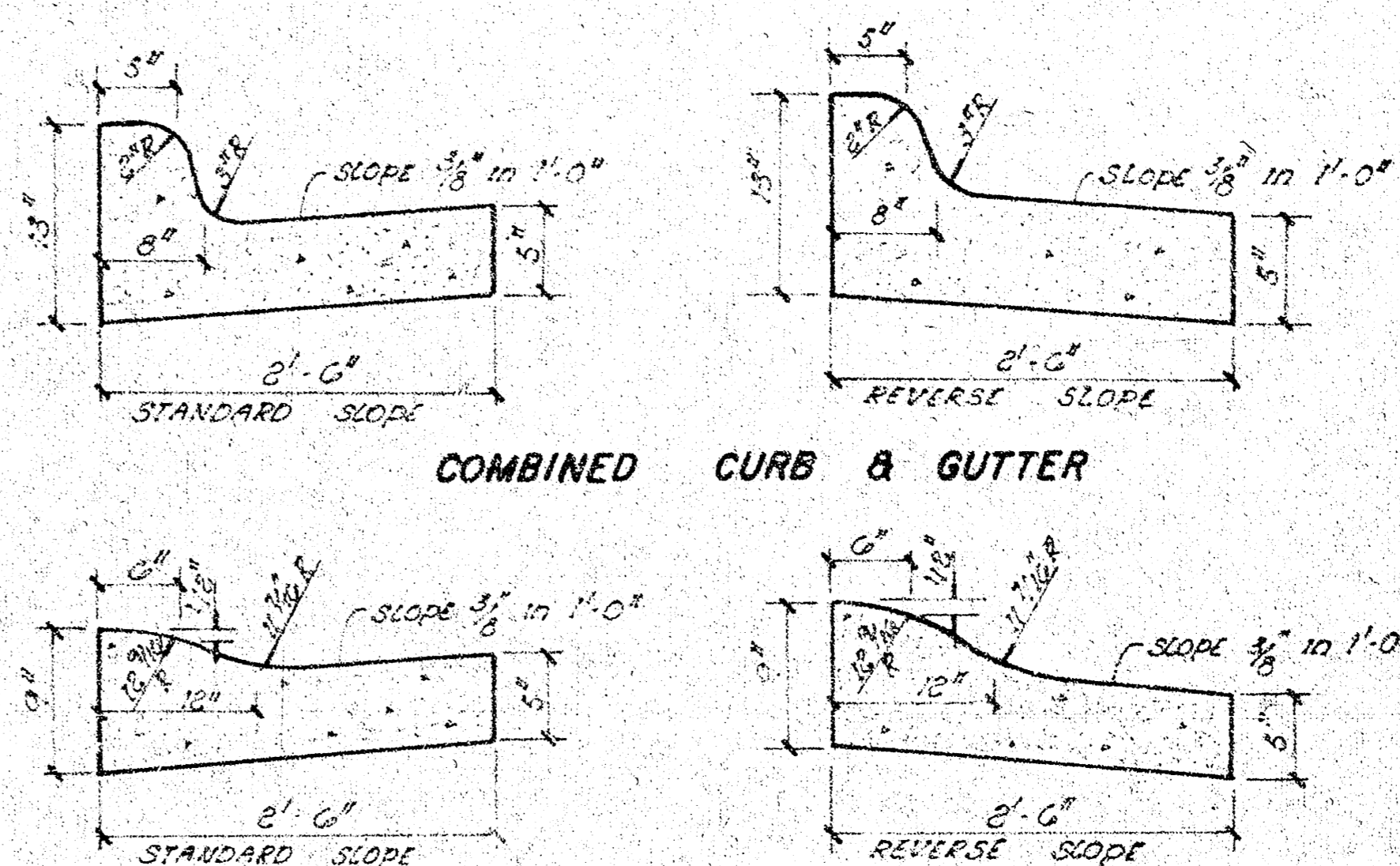
BILL OF MATERIAL					
MARK	SIZE	20'-0" WIDTH	16'-6" WIDTH	15'-6" WIDTH	SHAPE
A	6	20'-0"	2	20'-0"	
A-1	6	32'-0"	2	32'-0"	
B	6	27'-0"	2	23'-0"	
C	6	2'-0"	3	2'-0"	
D	6	6'-0"	N/A	N/A	
E	6	2'-0"	3	14'-0"	
F	6	2'-0"	10	9'-0"	
G	4	3'-0"	7	3'-0"	
H	4	2'-0"	N/A	N/A	
J	6	32'-0"	2	30'-0"	
K	6	22'-0"	2	18'-6"	
Total		490	Lin ft	377	Lin ft
#B Total		27	Lin ft	31	Lin ft

GENERAL NOTES

- A Tack Coat of Emulsified Asphalt (SS - 1H) shall be applied at an approximate rate 0.65 gallons per square yard between lifts of asphaltic materials when ordered by the Engineer. Tack Coat will not be paid for directly and shall be considered as subsidiary to price bid for Asphaltic Pavement.
- Bituminous Base and Asphaltic Concrete Wearing Surface shall be placed with a laydown machine having automatic electronic controls for crown and grade. Construction Joints in preceding lifts and such that a joint will be constructed on the pavement centerline in the top lift.
- The A.C. Pavement between combined curb and gutter shall be paid as square yards 8" A.C. Pavement (6" Bituminous Base). The Bituminous Base under the combined curb and gutter shall be paid as square yards 3" Bituminous Base.
- Sidewalks indicated on the typical section are for location on that section. SIDEWALKS SHALL NOT BE CONSTRUCTED ON THIS PROJECT.
- Contraction Joints may be constructed in Integral Curb by sawing with an approved concrete saw. The saw shall extend through the curb to the pavement. Sawn Contraction Joints shall have a maximum spacing of 10'.
- Integral Curb shall be tied to the pavement base with short deformed Dowel Bars spaced at 2' - 6" intervals. These Dowel Bars shall not be less than 1/2" or more than 3/4" in diameter.
- Transition Curb shall be paid as Roll-Type Comb. Curb & Gutter.



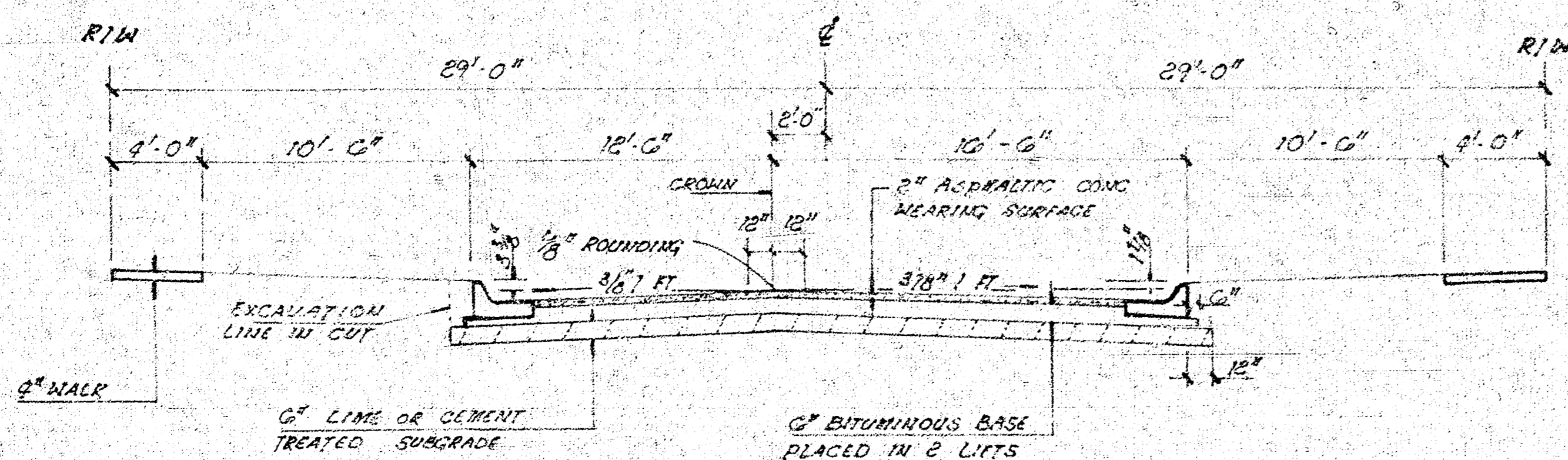
TYPICAL 25' B-B SECTION



ROLL TYPE CURB & GUTTER

TYPICAL RESIDENTIAL SECTIONS
FOR CROWN OFFSET - 58' R/W

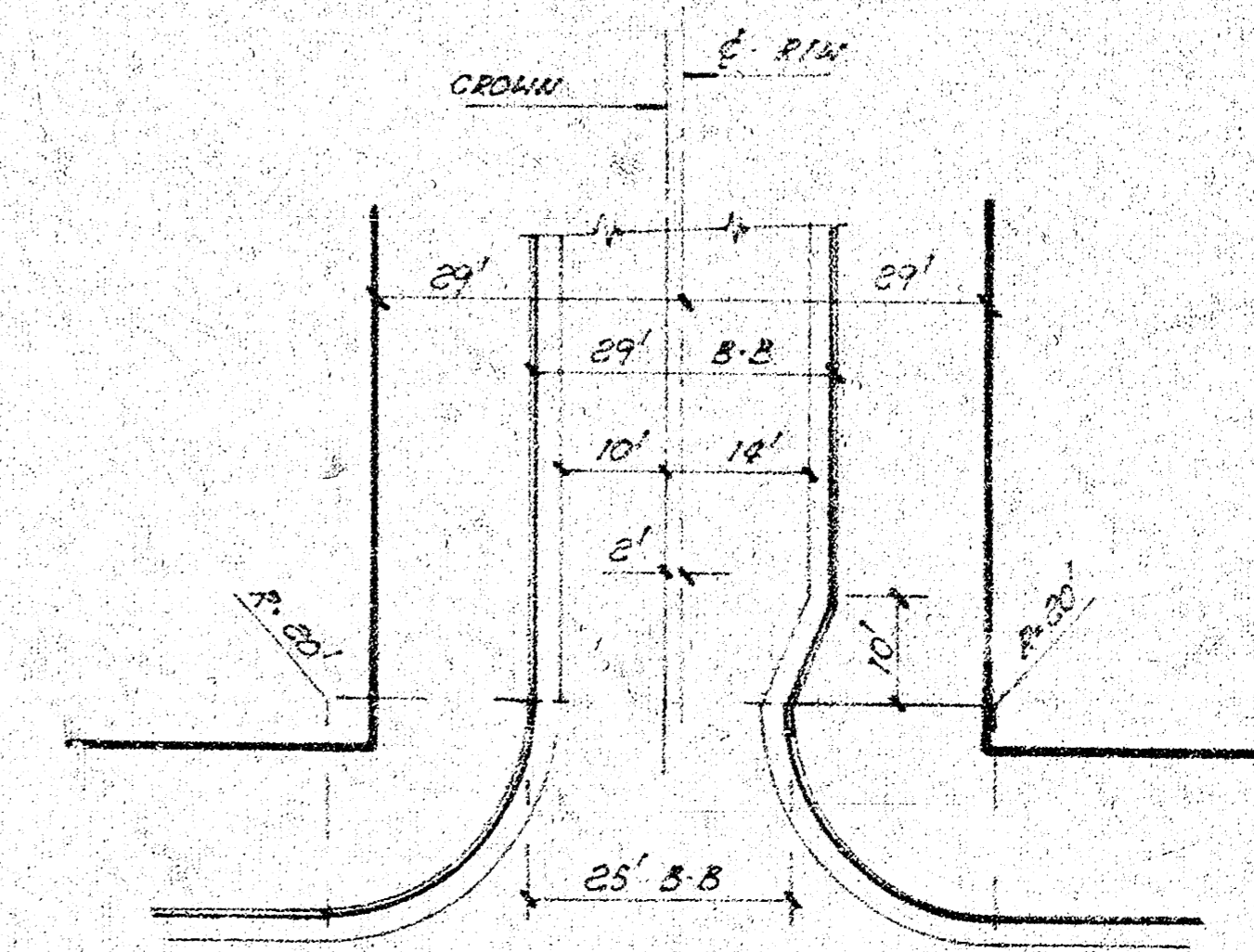
30TH ST. CT. & 32ND ST. CT.



TYPICAL 29' B-B SECTION
W PARKING ON ONE SIDE

GENERAL NOTES

1. A TACK COAT OF EMULSIFIED ASPHALT (SS - 1H) SHALL BE APPLIED AT AN APPROXIMATE RATE 0.05 GALLONS PER SQUARE YARD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.
2. BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1 FOOT WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.
3. THE A.C. PAVEMENT BETWEEN COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS 6" A.C. PAVEMENT (6" BITUMINOUS BASE). THE BITUMINOUS BASE UNDER THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS 7" BITUMINOUS BASE.
4. SIDEWALKS INDICATED ON THE TYPICAL SECTION ARE FOR LOCATION ON THAT SECTION. SIDEWALKS SHALL NOT BE CONSTRUCTED ON THIS PROJECT.
5. CONTRACTION JOINTS MAY BE CONSTRUCTED IN INTERVAL CURBS BY SAWING WITH AN APPROVED CONCRETE SAW. THE SAW SHALL EXTEND THROUGH THE CURB TO THE PAVEMENT. SAWED CONTRACTION JOINTS SHALL HAVE A MAXIMUM SPACING OF 10'.
6. INTERVAL CURBS SHALL BE TIED TO THE PAVEMENT BASE WITH SHORT DEFORMED DOWEL BARS SPACED AT 2' x 6" INTERVALS. THESE DOWEL BARS SHALL NOT BE LESS THAN 1/2" OR MORE THAN 3/4" IN DIAMETER.
7. TRANSITION CURB SHALL BE PAID AS ROLL-TYPE COMB. CURB AND GUTTER.



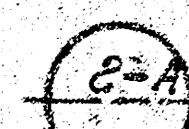
PLAN VIEW (TYPICAL)

CITY OF WICHITA, KANSAS

DEPARTMENT OF PUBLIC WORKS - ENGINEERING

J. W. LINN

CITY ENGINEER

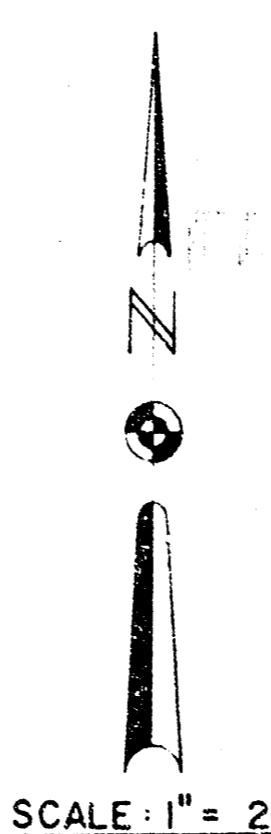


DATE: DEC 1 1978

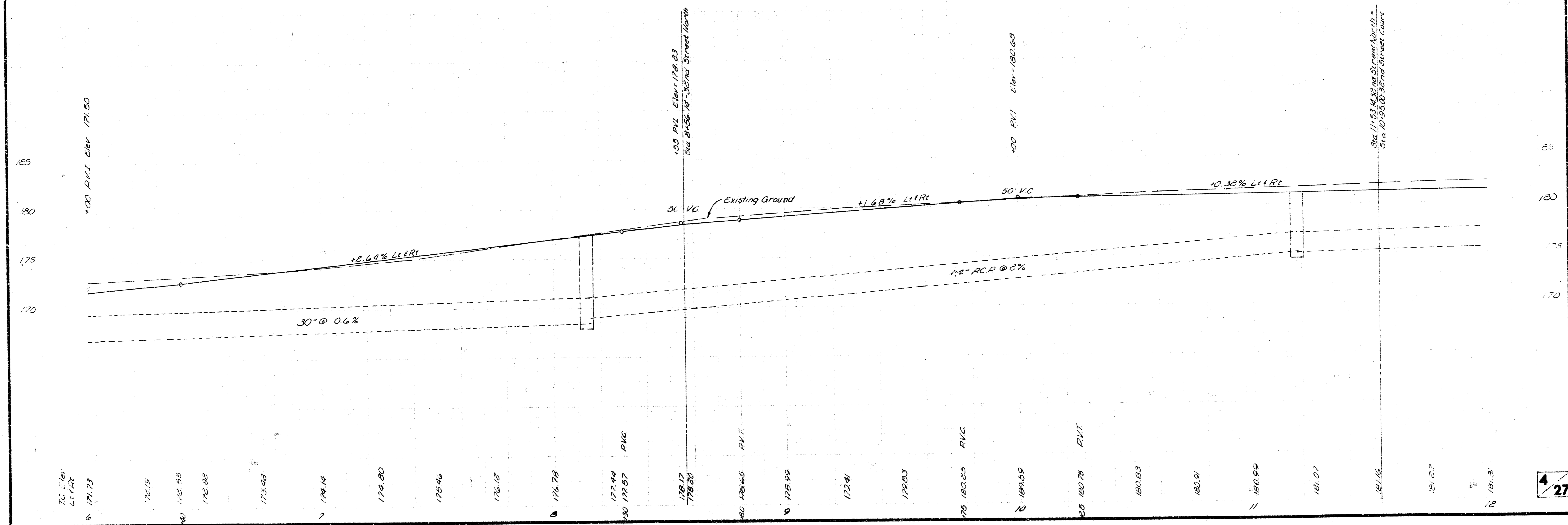
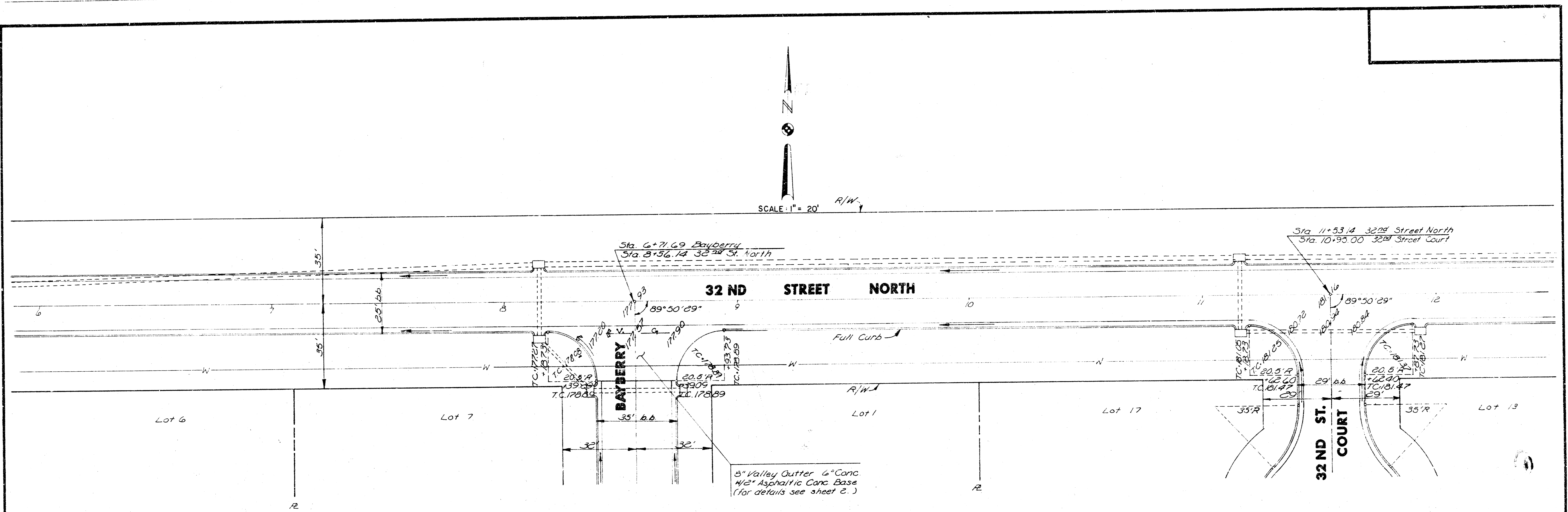
PROJ. NO. 472-76-265-80699-000-000-001

Curve Data Based on 323.20' R (E) Δ 30°00'00"

E Sta	E Arc	Chord Length	Deflection	Total Deflection
Left	Right	@ 10' Int. F.C.		
1+94.00				
2+00.00	4.00	5.68	0°27'38"	0°27'38"
2+25.00	25.00	23.65	1°55'09"	2°22'47"
2+50.00	25.00	23.65	1°55'09"	4°17'55"
2+75.00	25.00	23.65	1°55'09"	6°13'04"
3+00.00	25.00	23.65	1°55'09"	8°08'13"
3+25.00	25.00	23.65	1°55'09"	10°03'22"
3+50.00	25.00	23.65	1°55'09"	11°58'30"
3+75.00	25.00	23.65	1°55'09"	13°53'39"
4+00.00	25.00	23.65	1°55'09"	15°48'48"
4+25.00	25.00	23.65	1°55'09"	17°43'57"
4+50.00	25.00	23.65	1°55'09"	19°39'06"
4+75.00	25.00	23.65	1°55'09"	21°34'15"
5+00.00	25.00	23.65	1°55'09"	23°29'24"
5+25.00	25.00	23.65	1°55'09"	25°24'33"
5+50.00	25.00	23.65	1°55'09"	27°19'42"
5+75.00	25.00	23.65	1°55'09"	29°14'51"
6+00.00	25.00	23.65	1°55'09"	31°10'00"
6+25.00	25.00	23.65	1°55'09"	33°05'09"
6+50.00	25.00	23.65	1°55'09"	35°00'18"
6+75.00	25.00	23.65	1°55'09"	36°55'27"
7+00.00	25.00	23.65	1°55'09"	38°50'36"
7+25.00	25.00	23.65	1°55'09"	40°45'45"
7+50.00	25.00	23.65	1°55'09"	42°40'54"
7+75.00	25.00	23.65	1°55'09"	44°36'03"
8+00.00	25.00	23.65	1°55'09"	46°31'12"
8+25.00	25.00	23.65	1°55'09"	48°26'21"
8+50.00	25.00	23.65	1°55'09"	50°21'30"
8+75.00	25.00	23.65	1°55'09"	52°16'39"
9+00.00	25.00	23.65	1°55'09"	54°11'48"
9+25.00	25.00	23.65	1°55'09"	56°06'57"
9+50.00	25.00	23.65	1°55'09"	58°02'06"
9+75.00	25.00	23.65	1°55'09"	59°57'15"
10+00.00	25.00	23.65	1°55'09"	61°52'24"
10+25.00	25.00	23.65	1°55'09"	63°47'33"
10+50.00	25.00	23.65	1°55'09"	65°42'42"
10+75.00	25.00	23.65	1°55'09"	67°37'51"
11+00.00	25.00	23.65	1°55'09"	69°33'00"
11+25.00	25.00	23.65	1°55'09"	71°28'09"
11+50.00	25.00	23.65	1°55'09"	73°23'18"
11+75.00	25.00	23.65	1°55'09"	75°18'27"
12+00.00	25.00	23.65	1°55'09"	77°13'36"
12+25.00	25.00	23.65	1°55'09"	79°08'45"
12+50.00	25.00	23.65	1°55'09"	81°03'54"
12+75.00	25.00	23.65	1°55'09"	82°59'03"
13+00.00	25.00	23.65	1°55'09"	84°54'12"
13+25.00	25.00	23.65	1°55'09"	86°49'21"
13+50.00	25.00	23.65	1°55'09"	88°44'30"
13+75.00	25.00	23.65	1°55'09"	90°39'39"
14+00.00	25.00	23.65	1°55'09"	92°34'48"
14+25.00	25.00	23.65	1°55'09"	94°29'57"
14+50.00	25.00	23.65	1°55'09"	96°25'06"
14+75.00	25.00	23.65	1°55'09"	98°20'15"
15+00.00	25.00	23.65	1°55'09"	100°15'24"
15+25.00	25.00	23.65	1°55'09"	102°10'33"
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16+50.00	25.00	23.65	1°55'09"	111°46'18"
16+75.00	25.00	23.65	1°55'09"	113°41'27"
17+00.00	25.00	23.65	1°55'09"	115°36'36"
17+25.00	25.00	23.65	1°55'09"	117°31'45"
17+50.00	25.00	23.65	1°55'09"	119°26'54"
17+75.00	25.00	23.65	1°55'09"	121°22'03"
18+00.00	25.00	23.65	1°55'09"	123°17'12"
18+25.00	25.00	23.65	1°55'09"	125°12'21"
18+50.00	25.00	23.65	1°55'09"	127°07'30"
18+75.00	25.00	23.65	1°55'09"	129°02'39"
19+00.00	25.00	23.65	1°55'09"	130°57'48"
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19+75.00	25.00	23.65	1°55'09"	136°43'15"
20+00.00	25.00	23.65	1°55'09"	138°38'24"
20+25.00	25.00	23.65	1°55'09"	140°33'33"
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20+75.00	25.00	23.65	1°55'09"	144°23'51"
21+00.00	25.00	23.65	1°55'09"	146°19'00"
21+25.00	25.00	23.65	1°55'09"	148°14'09"
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21+75.00	25.00	23.65	1°55'09"	152°04'27"
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23+25.00	25.00	23.65	1°55'09"	163°35'21"
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23+75.00	25.00	23.65	1°55'09"	167°25'39"
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24+25.00	25.00	23.65	1°55'09"	171°15'57"
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26+25.00	25.00	23.65	1°55'09"	186°37'09"
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26+75.00	25.00	23.65	1°55'09"	190°27'27"
27+00.00	25.00	23.65	1°55'09"	192°22'36"
27+25.00	25.00	23.65	1°55'09"	194°17'45"
27+50.00	25.00	23.65	1°55'09"	196°12'54"
27+75.00	25.00	23.65	1°55'09"	198°08'03"
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28+25.00	25.00	23.65	1°55'09"	201°58'21"
28+50.00	25.00	23.65	1°55'09"	203°53'30"
28+75.00	25.00	23.65	1°55'09"	205°48'39"
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29+50.00	25.00	23.65	1°55'09"	211°34'06"
29+75.00	25.00	23.65	1°55'09"	213°29'15"
30+00.00	25.00	23.65	1°55'09"	215°24'24"
30+25.00	25.00	23.65	1°55'09"	217°19'33"
30+50.00	25.00	23.65	1°55'09"	219°14'42"
30+75.00	25.00	23.65	1°55'09"	221°09'51"
31+00.00	25.00	23.65	1°55'09"	223°05'00"
31+25.00	25.00	23.65	1°55'09"	224°50'09"
31+50.00	25.00	23.65	1°55'09"	226°45'18"
31+75.00	25.00	23.65	1°55'09"	228°40'27"
32+00.00	25.00	23.65	1°55'09"	230°35'36"
32+25.00	25.00	23.65	1°55'09"	232°30'45"
32+50.00	25.00	23.65	1°55'09"	234°25'54"
32+75.00	25.00	23.65	1°55'09"	236°21'03"
33+00.00	25.00	23.65	1°55'09"	238°16'12"
33+25.00	25.00	23.65	1°55'09"	240°11'21"
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33+75.00	25.00	23.65	1°55'09"	244°01'39"
34+00.00	25.00	23.65	1°55'09"	245°56'48"
34+25.00	25.00	23.65	1°55'09"	247°51'57"
34+50.00	25.00	23.65	1°55'09"	249°47'06"
34+75.00	25.00	23.65	1°55'09"	251°42'15"
35+00.00	25.00	23.65	1°55'09"	253°37'24"
35+25.00	25.00	23.65	1°55'09"	255°32'33"
35+50.00	25.00	23.65	1°55'09"	257°27'42"
35+75.00	25.00	23.65	1°55'09"	259°22'51"
36+00.00	25.00	23.65	1°55'09"	261°18'00"
36+25.00	25.00	23.65	1°55'09"	263°13'09"
36+50.00	25.00	23.65	1°55'09"	265°08'18"
36+75.00	25.00	23.65	1°55'09"	267°03'27"
37+00.00	25.00	23.65	1°55'09"	268°58'36"
37+25.00	25.00	23.65	1°55'09"	270°53'45"
37+50.00	25.00	23.65	1°55'09"	272°48'54"
37+75.00	25.00	23.65	1°55'09"	274°44'03"
38+00.00	25.00	23.65	1°55'09"	276°39'12"
38+25.00	25.00	23.65	1°55'09"	278°34'21"
38+50.00	25.00	23.65	1°55'09"	280°29'30"
38+75.00	25.00	23.65	1°55'09"	282°24'39"
39+00.00	25.00	23.65	1°55'09"	284°19'48"
39+25.00	25.00	23.65	1°55'09"	286°14'57"
39+50.00	25.00	23.65	1°55'09"	288°10'06"
39+75.00	25.00	23.65	1°55'09"	290°05'15"
40+00.00	25.00	23.65	1°55'09"	292°00'24"
40+25.00	25.00	23.65	1°55'09"	293°55'33"
40+50.00	25.00	23.65	1°55'09"	295°50'42"
40+75.00	25.00	23.65	1°55'09"	297°45'51"
41+00.00	25.00	23.65	1°55'09"	299°41'00"
41+25.00	25.00	23.65	1°55'09"	301°36'09"
41+50.00	25.00	23.65	1°55'09"	303°31'18"
41+75.00	25.00	23.65	1°55'09"	305°26'27"
42+00.00	25.00	23.65	1°55'09"	307°21'36"
42+25.00	25.00	23.65	1°55'09"	309°16'45"
42+50.00	25.00	23.65	1°55'09"	311°11'54"
42+75.00	25.00	23.65	1°55'09"	313°07'03"
43+00.00	25.00	23.65	1°55'09"	315°02'12"
43+25.00	25.00	23.65	1°55'09"	316°57'21"
43+50.00	25.00	23.65	1°55'09"	318°52'30"
43+75.00	25.00	23.65	1°55'09"	320°47'39"
44+00.00	25.00	23.65	1°55'09"	322°42'48"
44+25.00	25.00	23.65	1°55'09"	324°37'57"
44+50.00	25.00	23.65	1°55'09"	326°33'06"
44+75.00	25.00	23.65	1°55'09"	328°28'15"
45+00.00	25.00	23.65	1°55'09"	330°23'24"
45+25.00	25.00	23.65	1°55'09"	332°18'33"
45+50.00	25.00	23.65	1°55'09"	334°13'42"
45+75.00	25.00	23.65	1°55'09"	336°08'51"
46+00.00	25.00	23.65	1°55'09"	338°04'00"
46+25.00	25.00	23.65	1°55'09"	340°00'09"
46+50.00	25.00	23.65	1°55'09"	341°55'18"
46+75.00	25.00	23.65	1°55'09"	343°50'27"
47+00.00	25.00	23.65	1°55'09"	345°45'36"
47+25.00	25.00	23.65	1°55'09"	347°40'45"
47+50.00	25.00	23.65	1°55'09"	349°35'54"
47+75.00	25.00	23.65	1°55'09"	351°31'03"
48+00.00	25.00	23.65	1°55'09"	353°26'12"
48+25.00	25.00	23.65	1°55'09"	355°21'21"
48+50.00	25.00	23.65	1°55'09"	357°16'30"
48+75.00	25.00	23.65	1°55'09"	359°11'39"
49+00.00	25.00	23.65	1°55'09"	361°06'48"
49+25.00	25.00	23.65	1°55'09"	363°01'57"
49+50.00	25.00	23.65	1°55'09"	364°57'06"
49+75.00	25.00	23.65	1°55'09"	



SCALE: 1" = 20' R/W

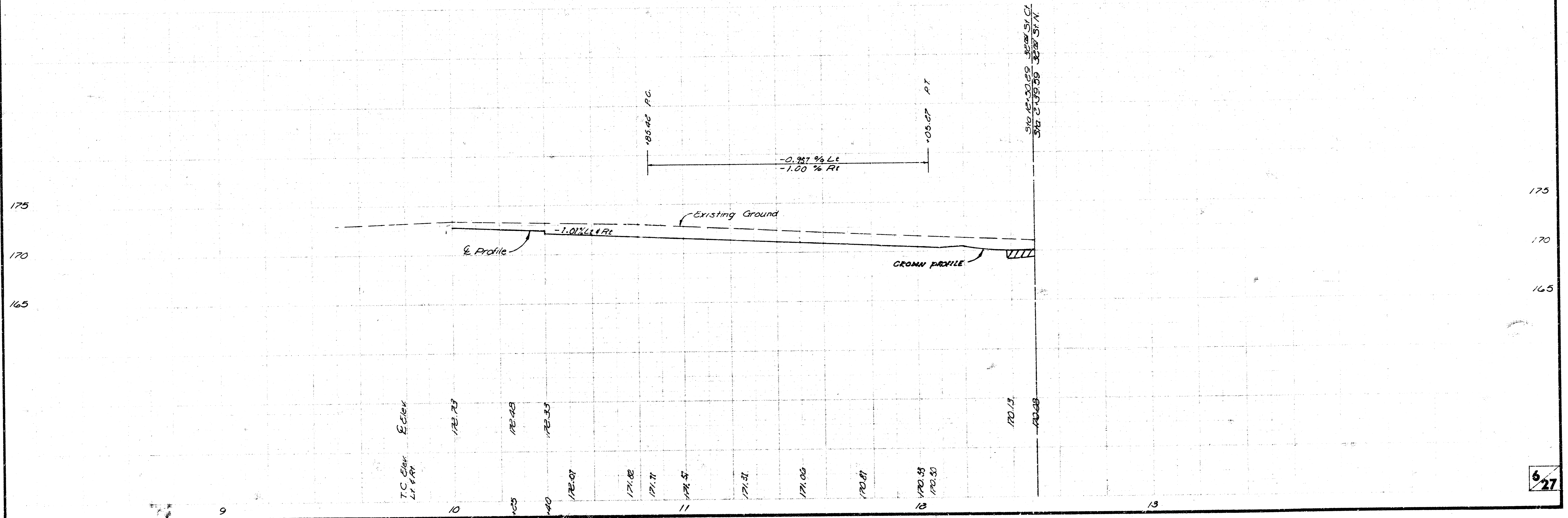
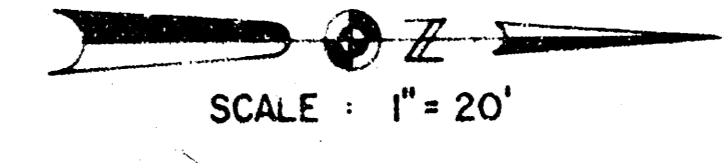
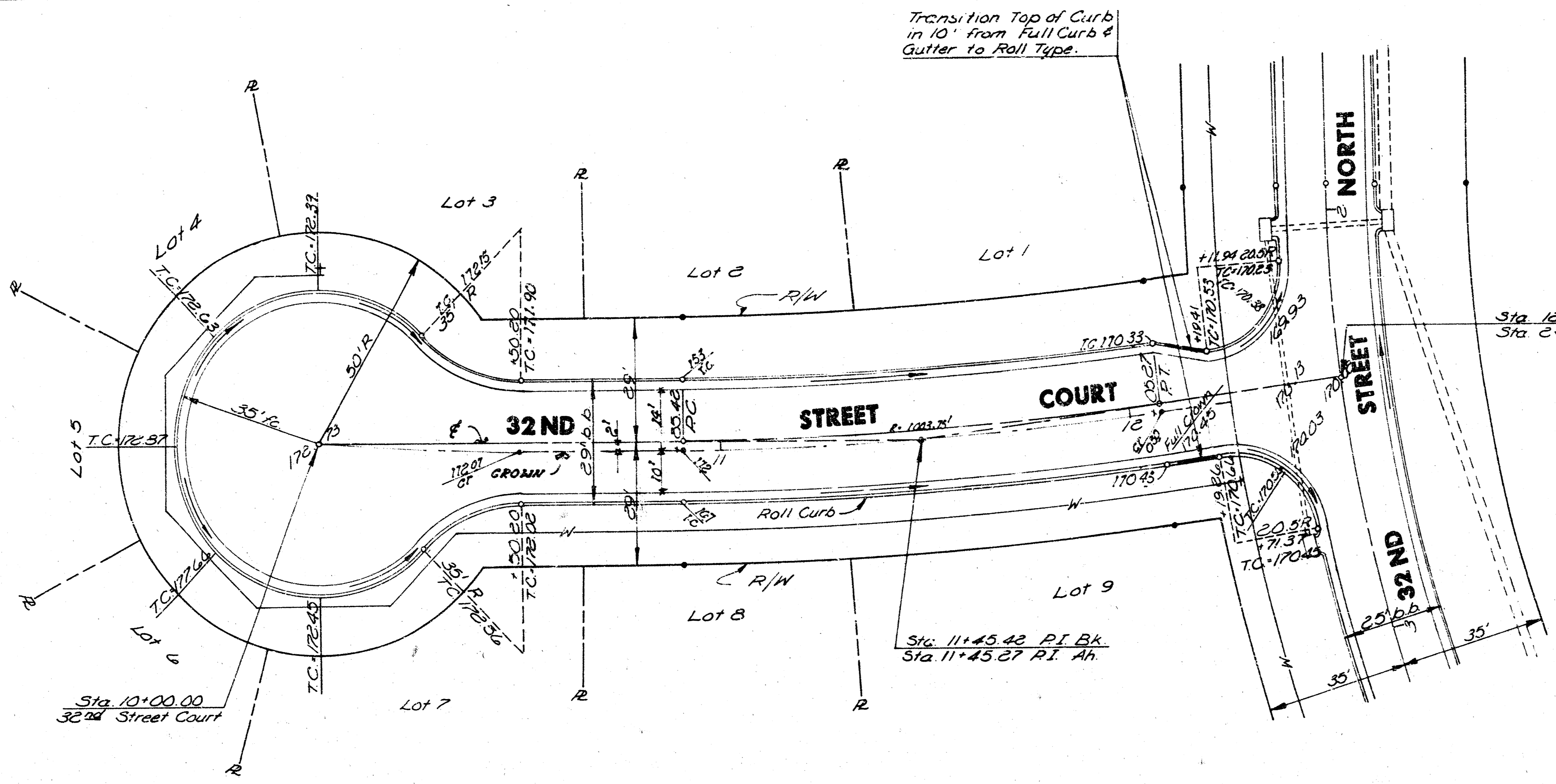


Curve Data Based on 1003.75' R(E) Δ=6°50'29"

Sta. Lf	E Arc Lt	Chord Length B' @ Lt F.C.	Deflection	Total Deflection
10+85.42	14.58	14.06	0°24'58"	0°24'58"
11+00.00	25.00	24.44	0°42'49"	1°07'47"
11+25.00	25.00	24.44	0°42'49"	1°50'35"
11+50.00	25.00	24.44	0°42'49"	2°33'24"
12+00.00	25.00	24.44	0°42'49"	3°16'13"
12+05.27	5.27	24.44	0°09'01"	3°25'14"

Curve Data Based on 1003.75' R(E) Δ=6°50'29"

Sta. Rt	E Sta Rt	Chord Length B' @ Rt F.C.	Deflection	Total Deflection
10+85.42	14.58	14.06	0°24'58"	0°24'58"
11+00.00	25.00	25.54	0°42'49"	1°07'47"
11+25.00	25.00	25.54	0°42'49"	1°50'35"
11+50.00	25.00	25.54	0°42'49"	2°33'24"
12+00.00	25.00	25.54	0°42'49"	3°16'13"
12+05.27	5.27	5.38	0°09'01"	3°25'14"

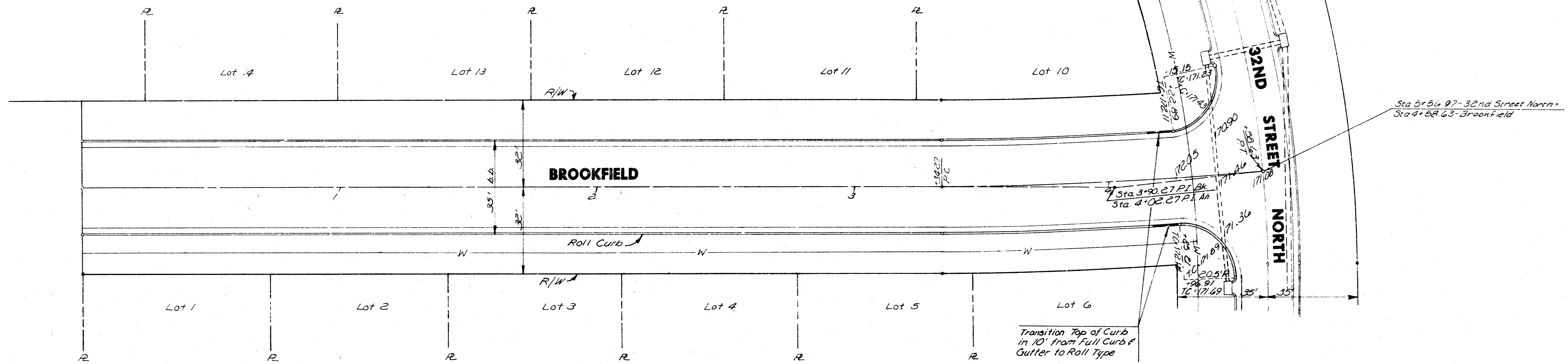
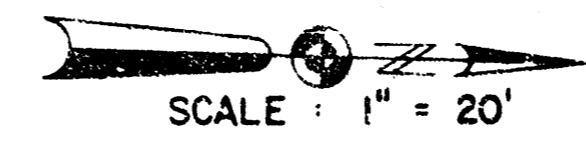


Curb Data Based on 1507.15' R (E) Δ = 5°10'00"

Sta.	Arc Lt.	Chord Length B' @ Lt. P.C.	Deflection	Total Deflection
3+34.27				
3+50.00	15.73	15.47	0°17'56"	0°17'56"
3+75.00	25.00	24.59	0°28'31"	0°46'27"
4+00.00	25.00	24.58	0°28'31"	1°14'58"
4+25.21	22.88	22.51	0°22'50"	1°41'04"

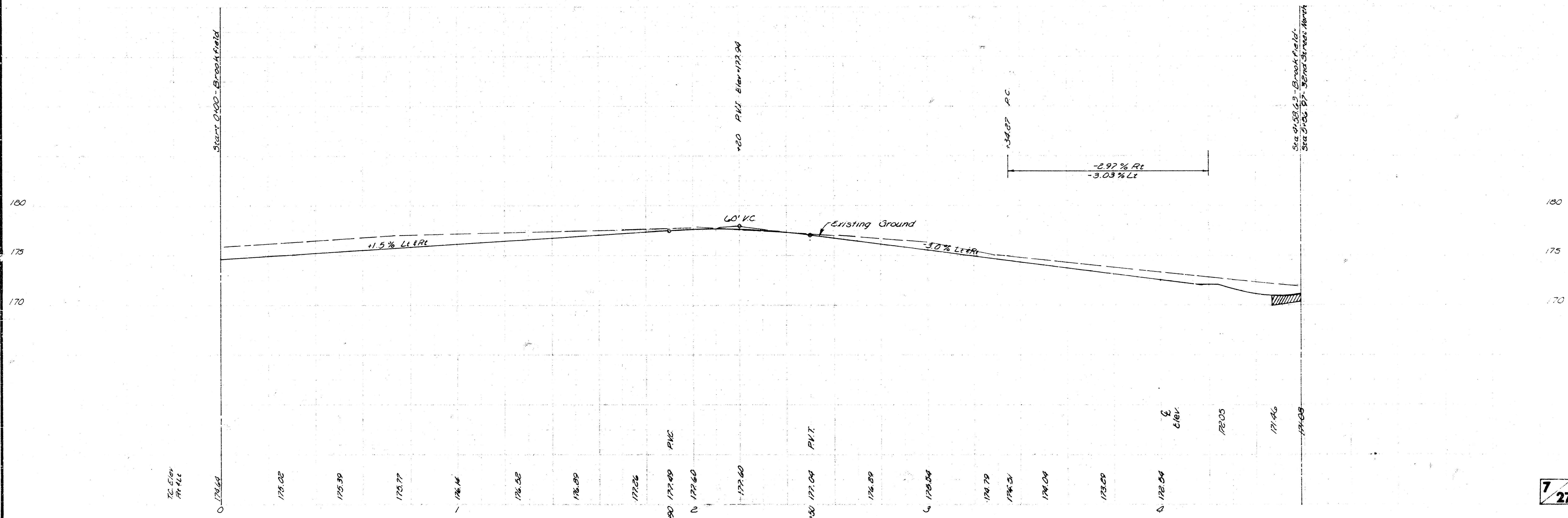
Curb Data Based on 1507.15' R (E) Δ = 5°10'00"

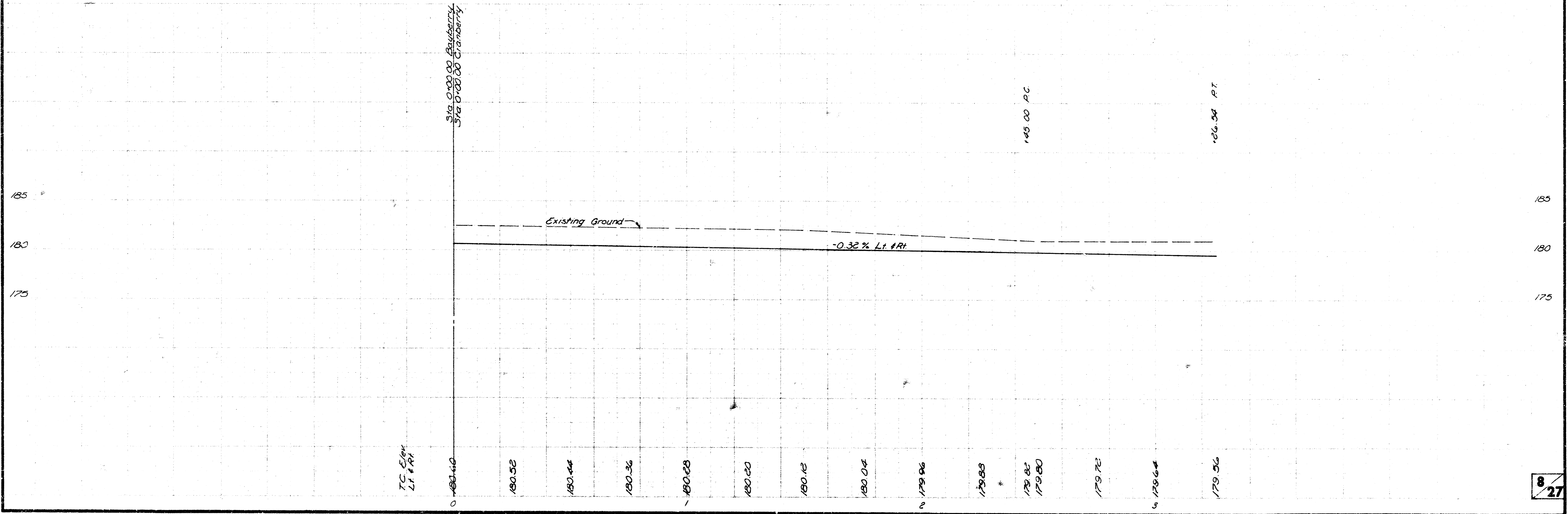
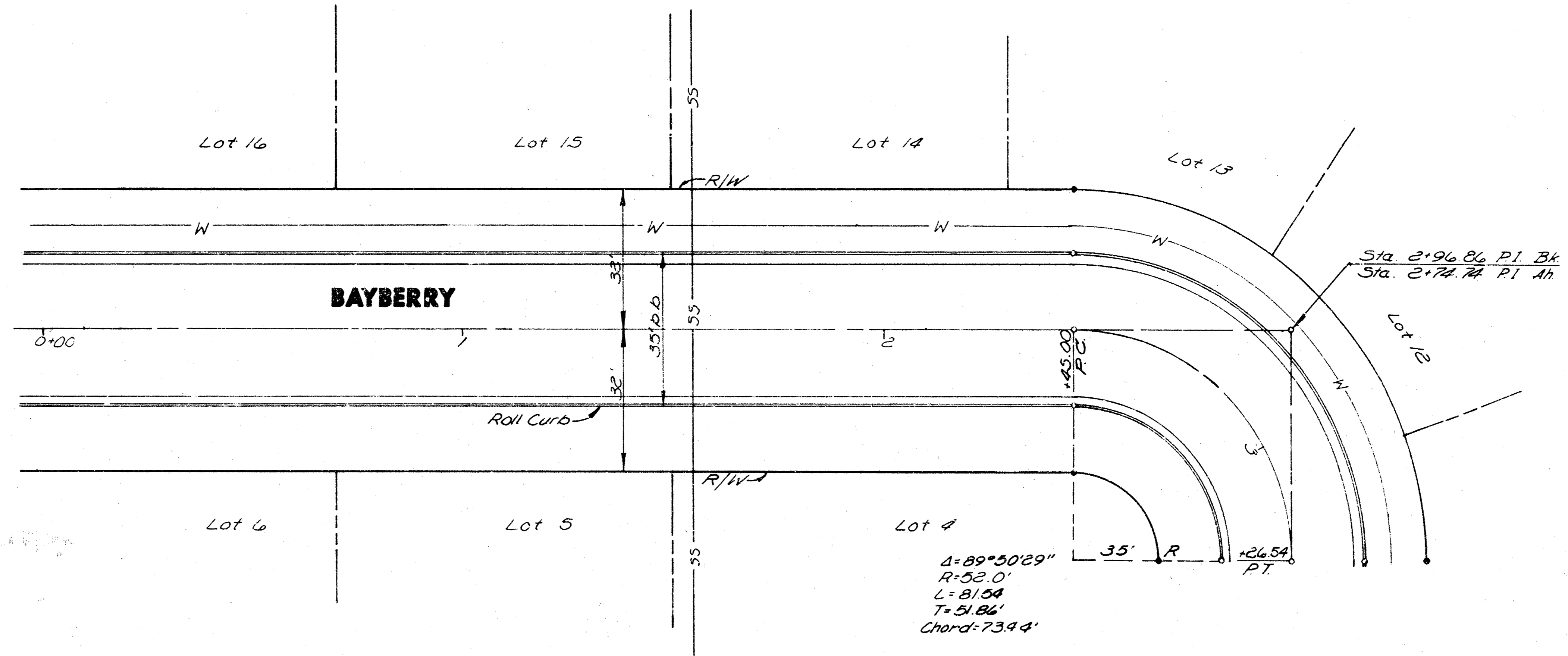
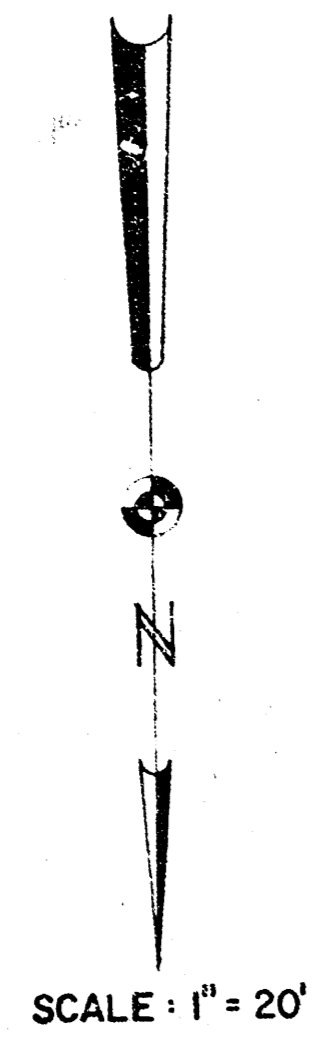
Sta.	Arc Lt.	Chord Length B' @ Lt. P.C.	Deflection	Total Deflection
3+34.27				
3+50.00	15.73	15.98	0°17'56"	0°17'56"
3+75.00	25.00	25.41	0°28'31"	0°46'27"
4+00.00	25.00	25.41	0°28'31"	1°14'58"
4+25.21	25.20	25.66	0°28'45"	1°43'43"

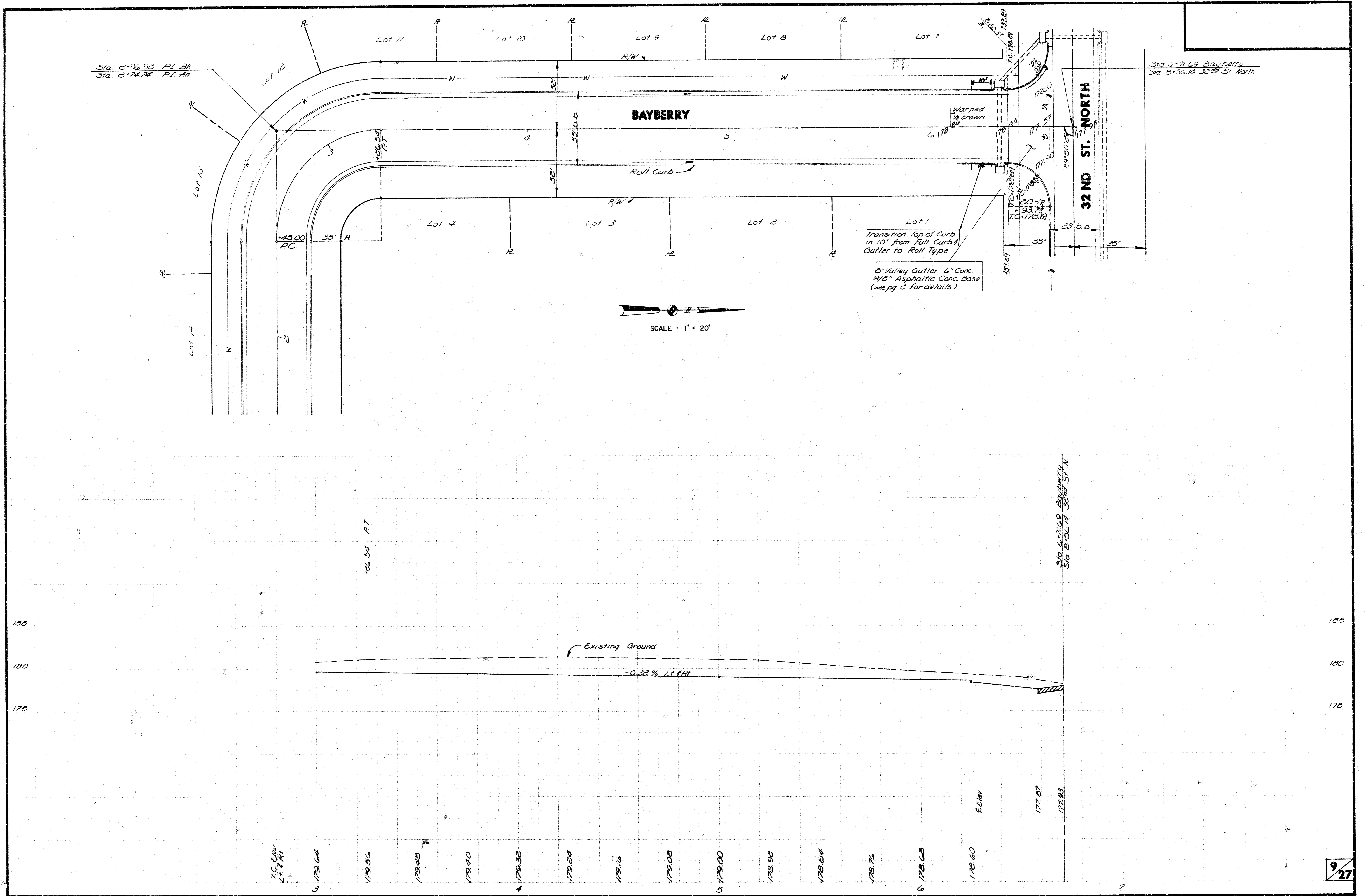


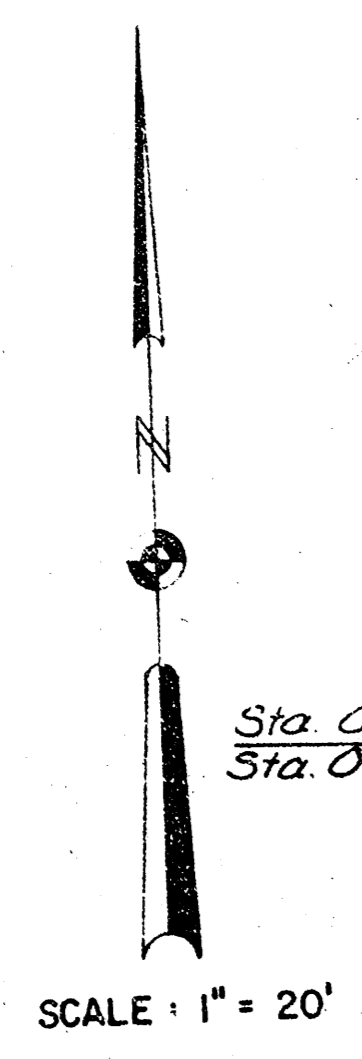
Sta 5+56.97 - 32nd Street North =
Sta 4+58.63 - Brookfield

Transition Top of Curb
in 10' from Full Curb &
Gutter to Roll Type

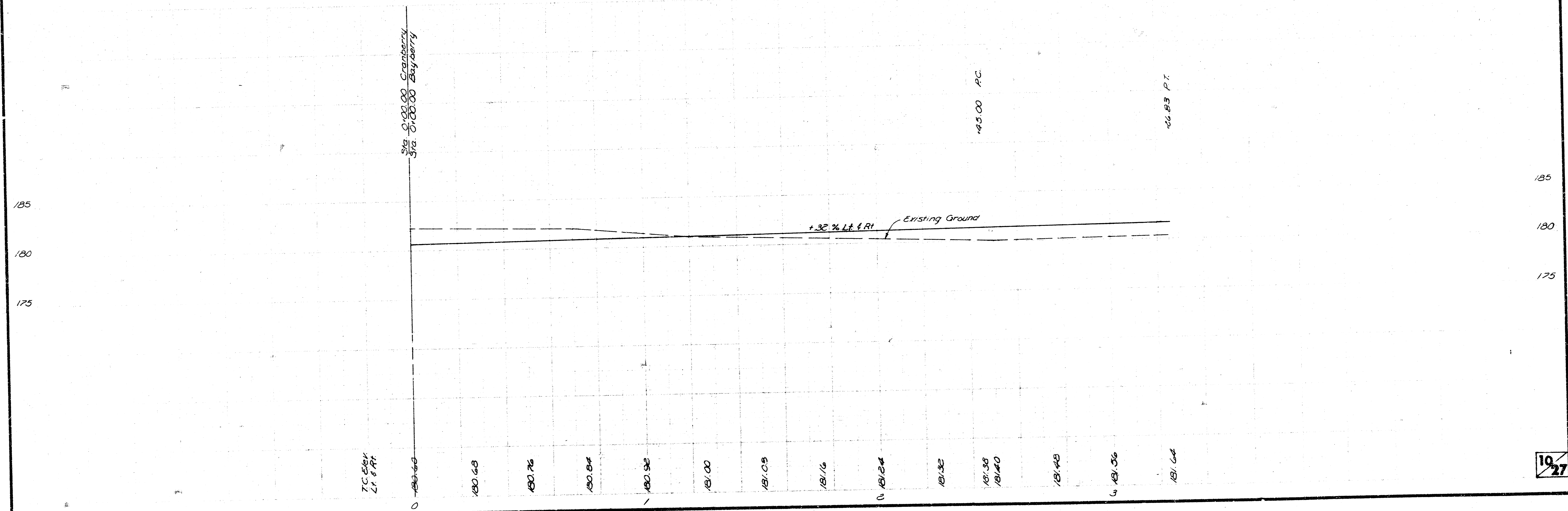
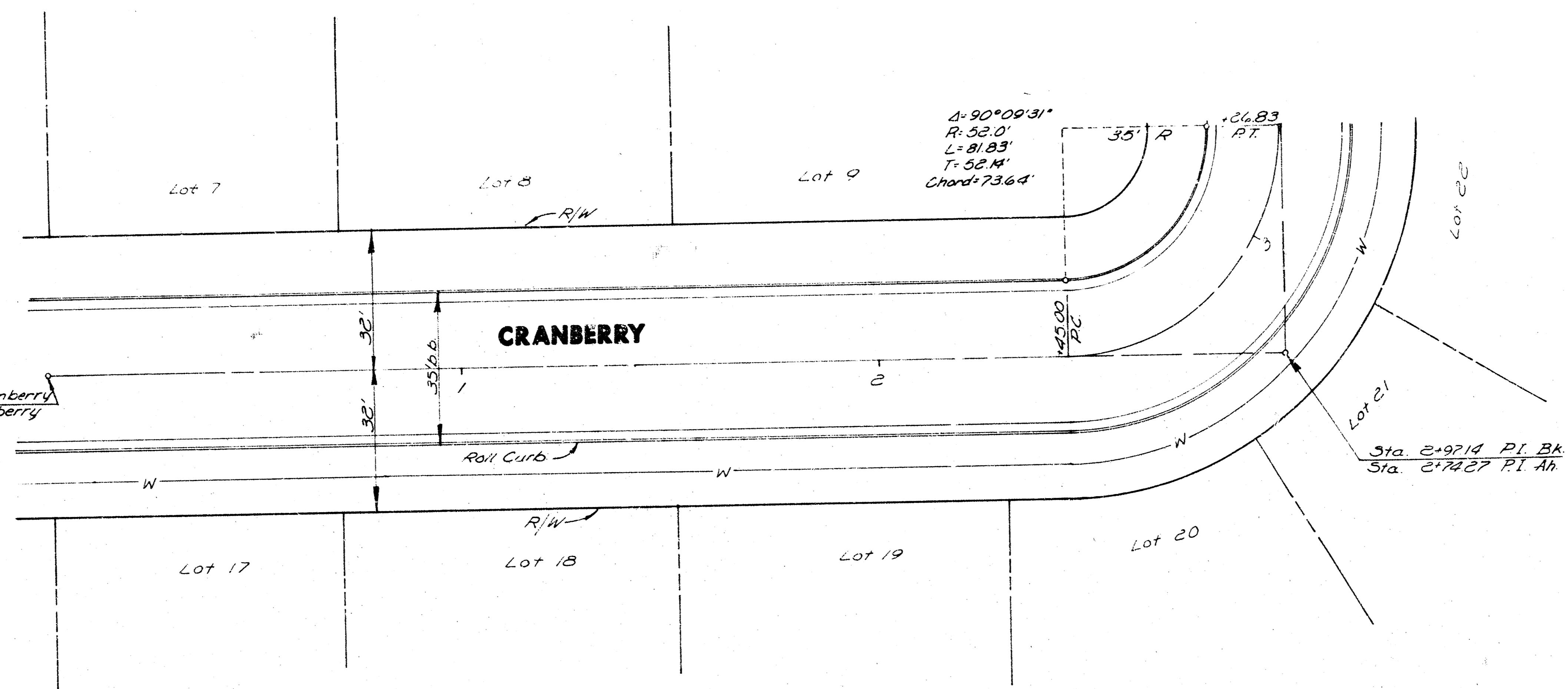


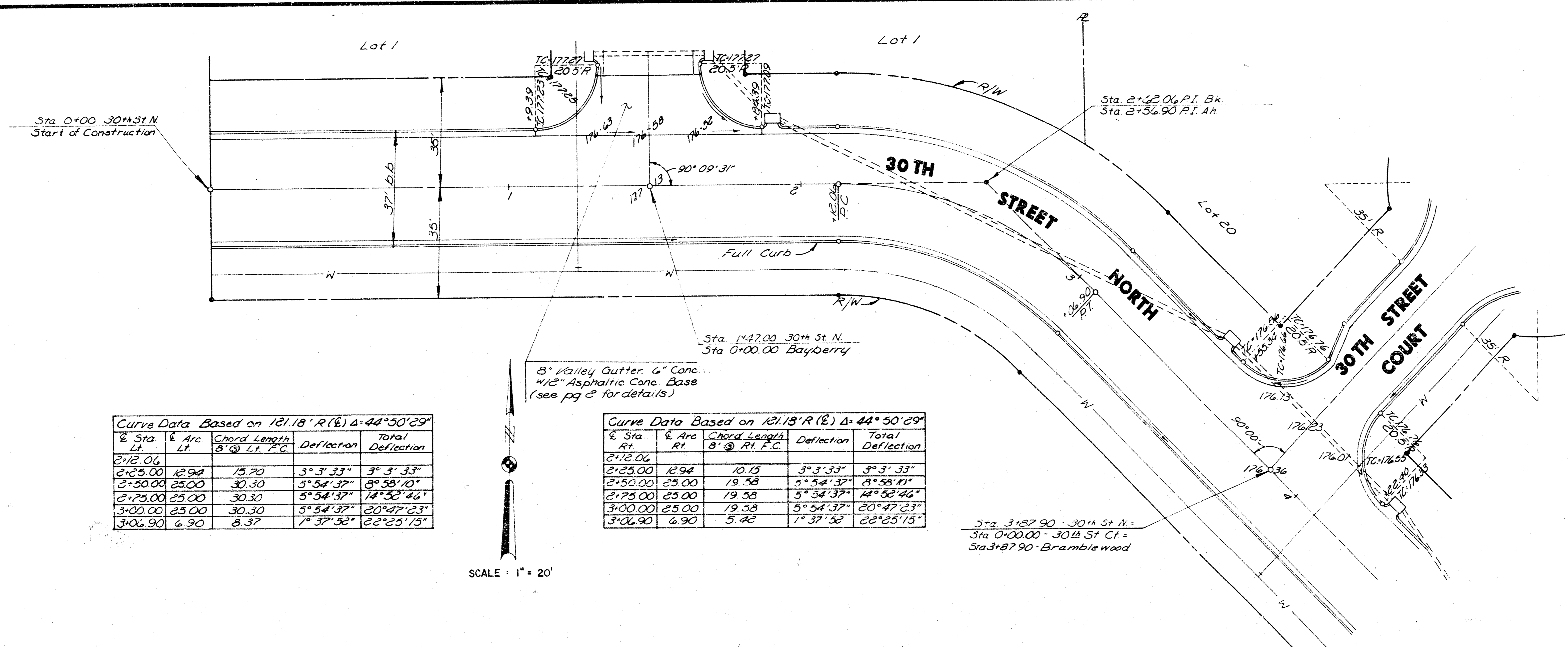






Sta. 0+00 Cranberry
Sta. 0+00 Bayberry





Curve Data Based on 121.13' R (E) Δ = 44° 30' 29"

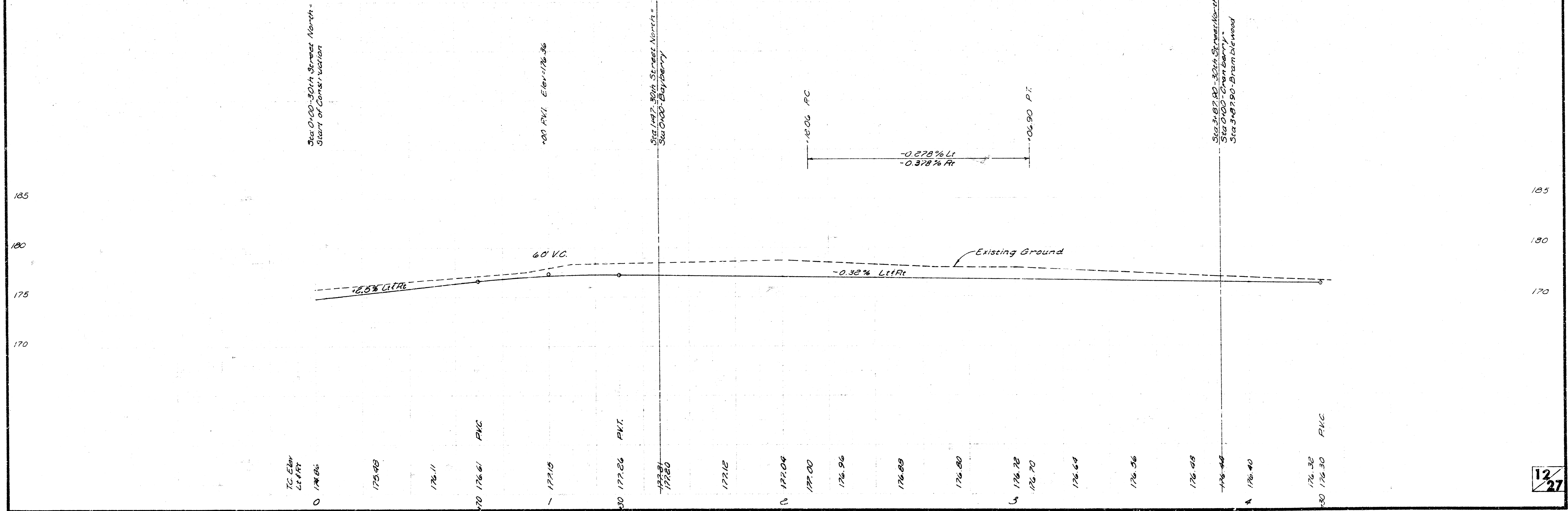
Sta. & Lt.	% Arc Lt.	Chord Length B' @ Lt. F.C.	Deflection	Total Deflection
2+12.06	12.94	15.20	3° 3' 33"	3° 3' 33"
2+50.00	25.00	30.30	5° 54' 37"	8° 58' 10"
2+75.00	25.00	30.30	5° 54' 37"	14° 52' 46"
3+00.00	25.00	30.30	5° 54' 37"	20° 47' 23"
3+26.90	6.90	8.37	1° 37' 52"	22° 25' 15"

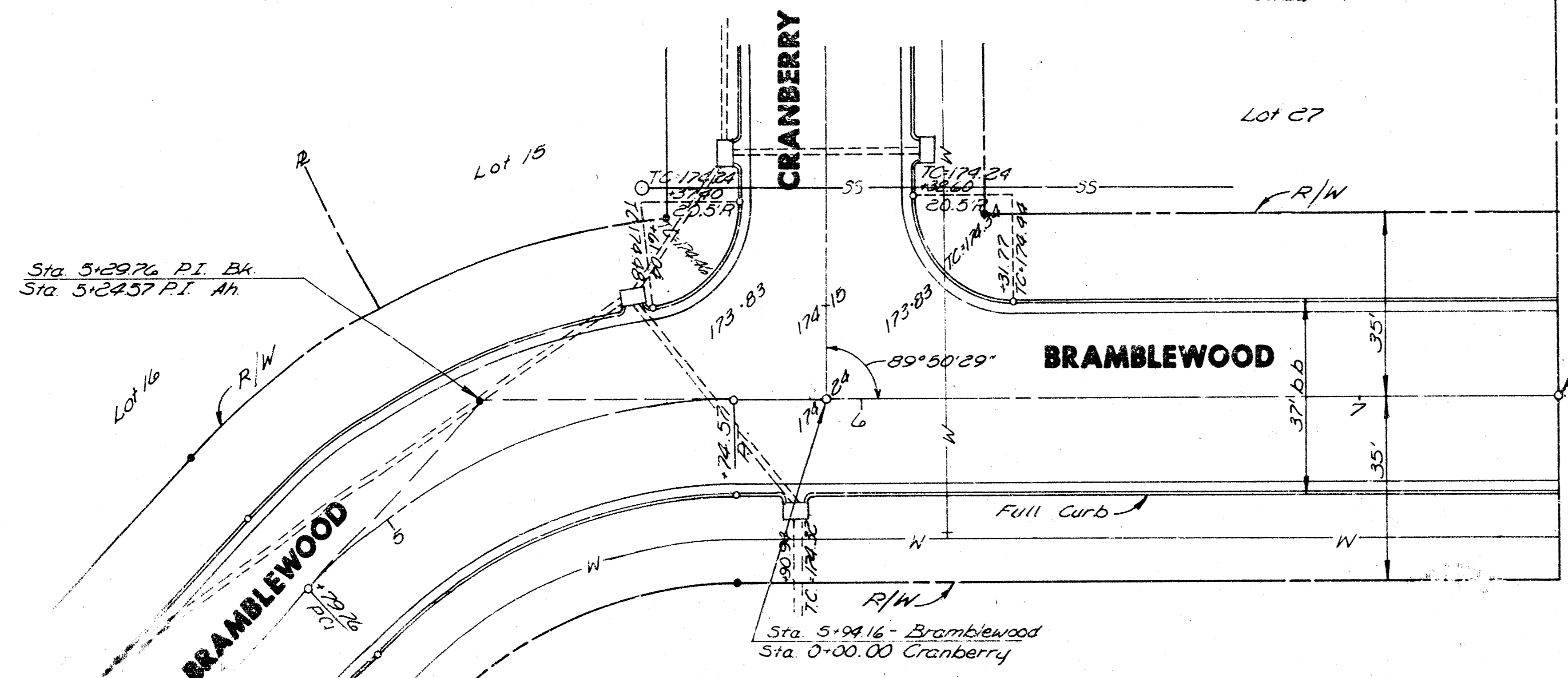
Curve Data Based on 121.13' R (E) Δ = 44° 50' 29"

Sta. & Rt.	% Arc Rt.	Chord Length B' @ Rt. F.C.	Deflection	Total Deflection
2+12.06	12.94	10.15	3° 3' 33"	3° 3' 33"
2+50.00	25.00	19.58	5° 54' 37"	8° 58' 10"
2+75.00	25.00	19.58	5° 54' 37"	14° 52' 46"
3+00.00	25.00	19.58	5° 54' 37"	20° 47' 23"
3+26.90	6.90	5.46	1° 37' 52"	22° 25' 15"

8" Valley Gutter, 6" Conc.
 1/2" Asphaltic Conc. Base
 (see pg 2 for details)

SCALE: 1" = 20'





SCALE: 1" = 20'

Sta 5+29.76 P.I. BK
Sta 5+24.57 P.I. NH

Sta 7+41.16 Bramblewood
End of Construction

Sta 0+00.00 - 30th St Ct
Sta 3+87.90 - 30th St N
Sta 3+87.90 - Bramblewood

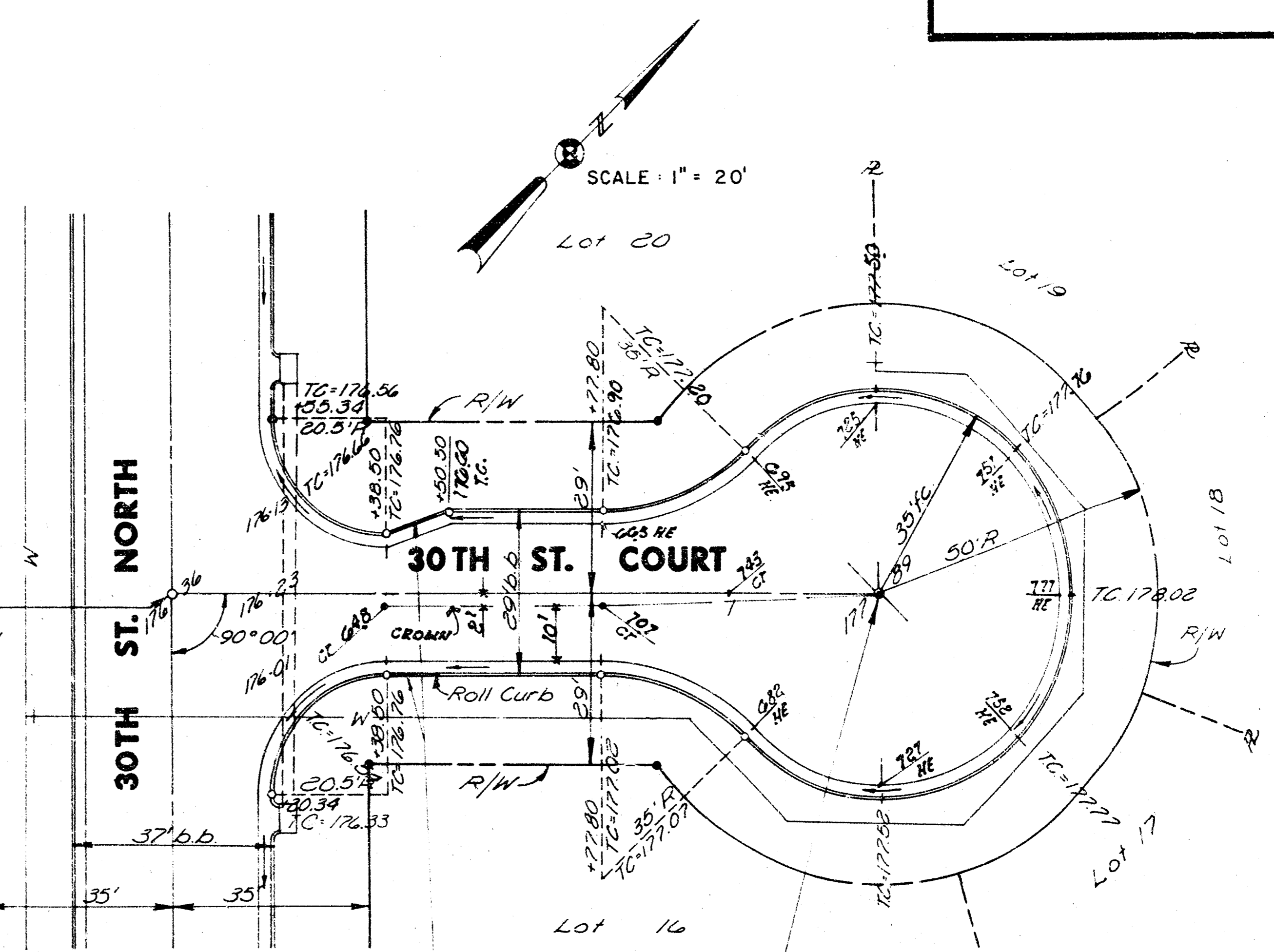
Sta 5+94.16 - Bramblewood
Sta 0+00.00 Cranberry

Curve Data Based on 120.71' R (E) Δ 45° 00' 00"

E Sta	E Arc Lt	Chord Length	Deflection	Total Deflection
LT	LT	@ Lt F.C.		
4+79.76				
5+00.00	20.24	24.56	4° 48' 13"	4° 48' 13"
5+25.00	25.00	30.32	5° 56' 00"	10° 44' 12"
5+50.00	25.00	30.32	5° 56' 00"	16° 40' 12"
5+74.57	24.57	29.80	5° 49' 32"	22° 30' 04"

Curve Data Based on 120.71' R (E) Δ 45° 00' 00"

E Sta	E Arc Rt	Chord Length	Deflection	Total Deflection
RT	RT	@ Rt F.C.		
4+79.76				
5+00.00	20.24	15.85	4° 48' 13"	4° 48' 13"
5+25.00	25.00	19.57	5° 56' 00"	10° 44' 12"
5+50.00	25.00	19.57	5° 56' 00"	16° 40' 12"
5+74.57	24.57	19.23	5° 49' 32"	22° 30' 04"



SCALE: 1" = 20'

Transition top of curb in 10' from full curb & gutter to roll type

Sta 0+00 - 30th Street Court*
Sta 3+87.90 - 30th Street North
Sta 3+87.90 - Bramblewood

128.8 - E. of Cut-off Sign

