

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	472-84071	2005	R3.12	

DATE	
BY	

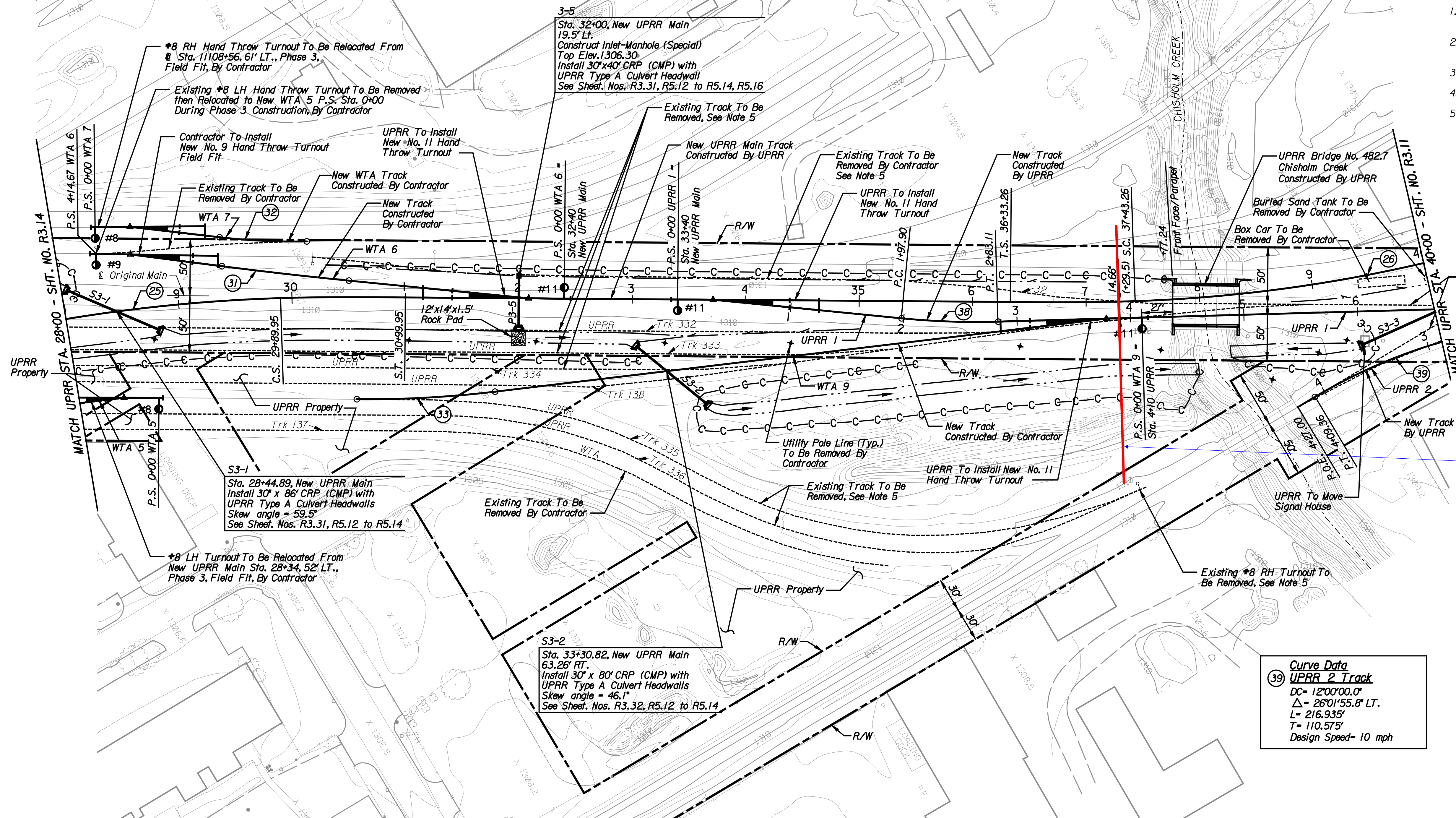
Curve Data
25) New UPRR Main Track
 DC= 400'00.0'
 $\Delta = 46^{\circ}58'24.0''$ RT.
 L= 1,064.572'
 T= 558.210'
 Ls= 110.000'
 Ts= 677.703'
 Superelevation= 2 1/2"
 Design Speed= 35 mph

Curve Data
32) WTA 7 Track
 DC= 1000'00.0'
 $\Delta = 7^{\circ}41'32.0''$ LT.
 L= 76.922'
 T= 38.568'
 Design Speed= 10 mph

Curve Data
31) WTA 6 Track
 DC= 1300'00.0'
 $\Delta = 1^{\circ}41'21.2''$ RT.
 L= 112.606'
 T= 56.313'
 Design Speed= 10 mph

Curve Data
26) New UPRR Main Track
 DC= 420'00.0'
 $\Delta = 24^{\circ}45'21.2''$ LT.
 L= 461.316'
 T= 233.082'
 Ls= 110.000'
 Ts= 345.323'
 Superelevation= 2 1/2"
 Design Speed= 35 mph

WC12
 3/4" \emptyset Rebar w/ Alum. Cap Stamped "WC12"
 & Center Punched, Flush with Ground
 N 1699093.7020 E 1650080.8808 Elev. 1310.23
 1. To Back of S. Curb of 21st ST. 6.86' N
 2. To Base of Chain Link Fence Post 6.86' E
 3. To Face of PP 35.82' SSE
 4. To Center of Curb Inlet 32.67' WNW



- NOTES:**
1. Removal of existing WTA track to be done by the contractor prior to the construction of new track.
 2. For profiles of UPRR and WTA tracks, see sheet numbers R3.25 to R3.30.
 3. For special ditch profiles, see sheet number R3.26.
 4. For additional alignment data, see sheet numbers R3.2 to R3.4.
 5. Rail Joint Bars and Turnouts from the following tracks are to be removed and stockpiled at a site selected by the UPRR. Ties, Plates and Spikes to be disposed of by the contractor.

Track 332 - 805' including Turnout
 Track 333 - 763' including Turnout
 Track 334 - 742' including Turnout
 Track 335 - 840'
 Track 336 - 962' including Turnout
 Track 138 - 1775' including Turnout
 Track 32 - 354' including Turnout
 Remaining Track Material to be disposed of.

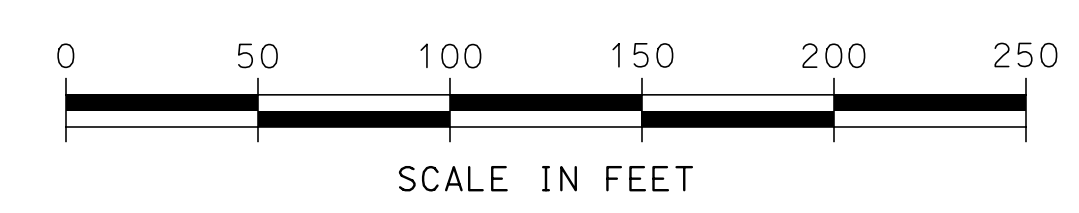
Note: Encase Chisholm Creek Sanitary Sewer.
 Change Order No. 3 (see attached c.o. copy).
 50 lf of 48" diameter steel casing.
 Void filled with sand.

This sheet designed by:
HNTB
 ARCHITECTS ENGINEERS PLANNERS

Curve Data
33) WTA 9 Track
 DC= 830'00.0'
 $\Delta = 7^{\circ}16'47.2''$ RT.
 L= 85.645'
 T= 42.919'
 Design Speed= 10 mph

Curve Data
38) UPRR 1 Track
 DC= 200'00.0'
 $\Delta = 2^{\circ}05'15.0''$ RT.
 L= 104.375'
 T= 52.196'
 Design Speed= 20 mph

Curve Data
39) UPRR 2 Track
 DC= 1200'00.0'
 $\Delta = 26^{\circ}01'55.8''$ LT.
 L= 216.935'
 T= 110.575'
 Design Speed= 10 mph



3					
2					
1					
NO.	DATE	REVISIONS	BY	APP'D	
CITY OF WICHITA					
WICHITA CENTRAL CORRIDOR					
PHASE 3 - TRACK WORK PLAN					
SHEET NO.	OF	SCALE	APP'D		
DESIGNED	REP	DETAILED	JDG	QUANTITIES	TRACED
DESIGN CK.	RSB	DETAIL CK.	REP	QUAN. CK.	TRACE CK.

Plotted on: Tuesday, January 04, 2005 8:02:27 AM
 J:\csham\Plot Queue\csham001\K0209049-F5
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