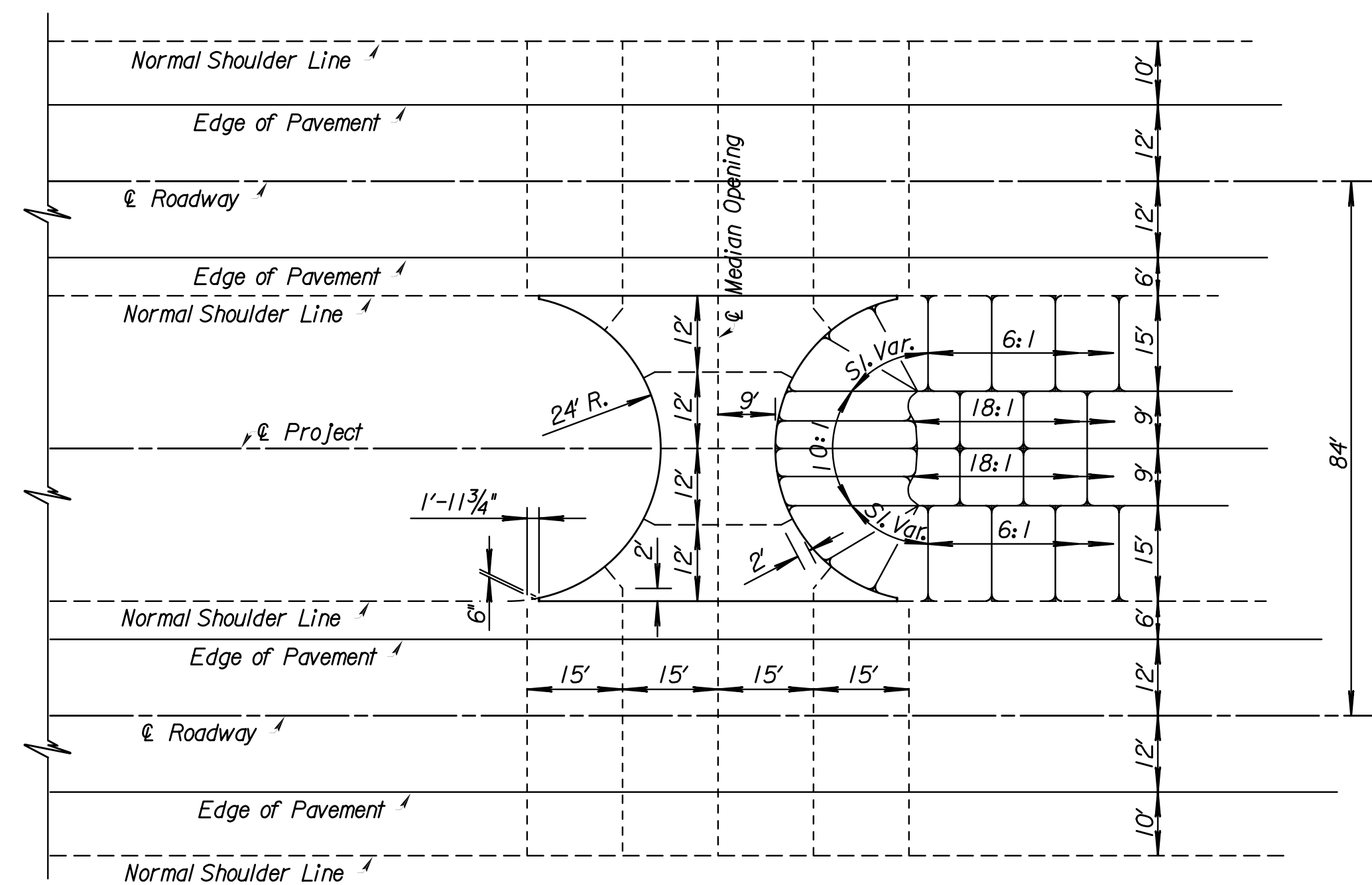
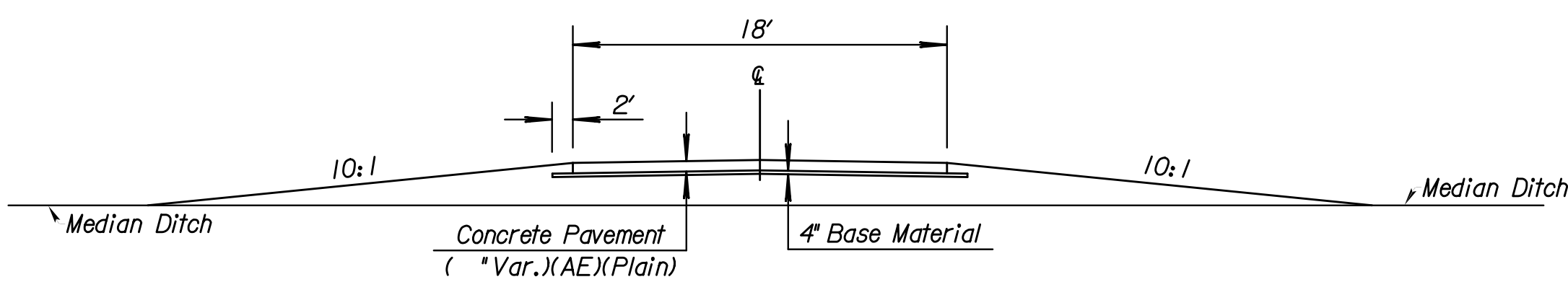


STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	87 N-0243-01	2005	28	169

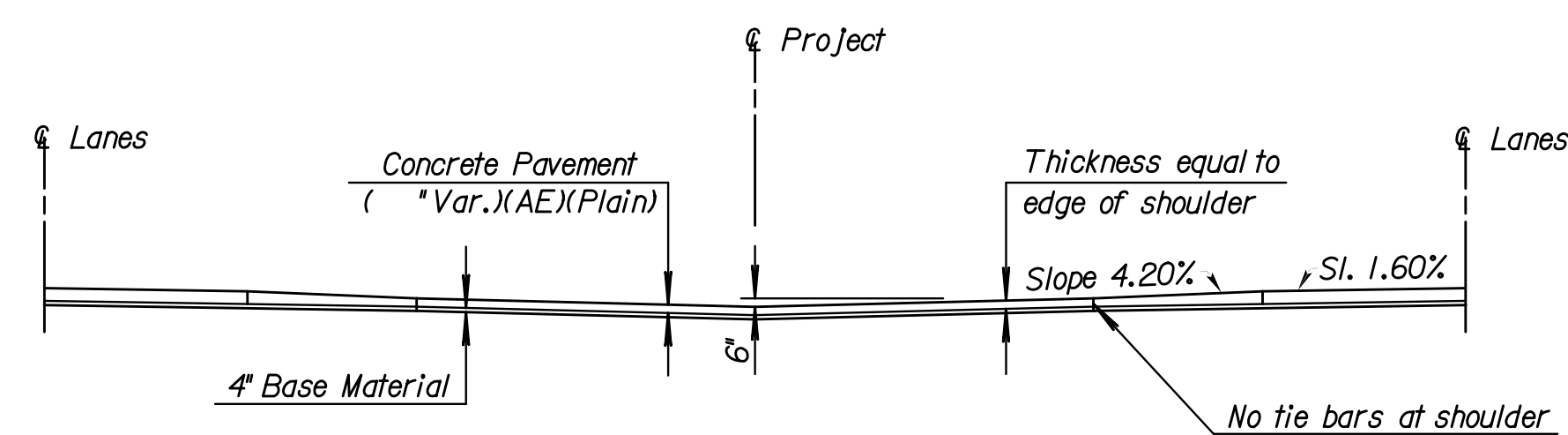
Federal Aid No. STP NO24(301)



U-TURN MEDIAN OPENING - FOUR LANES (DIVIDED)

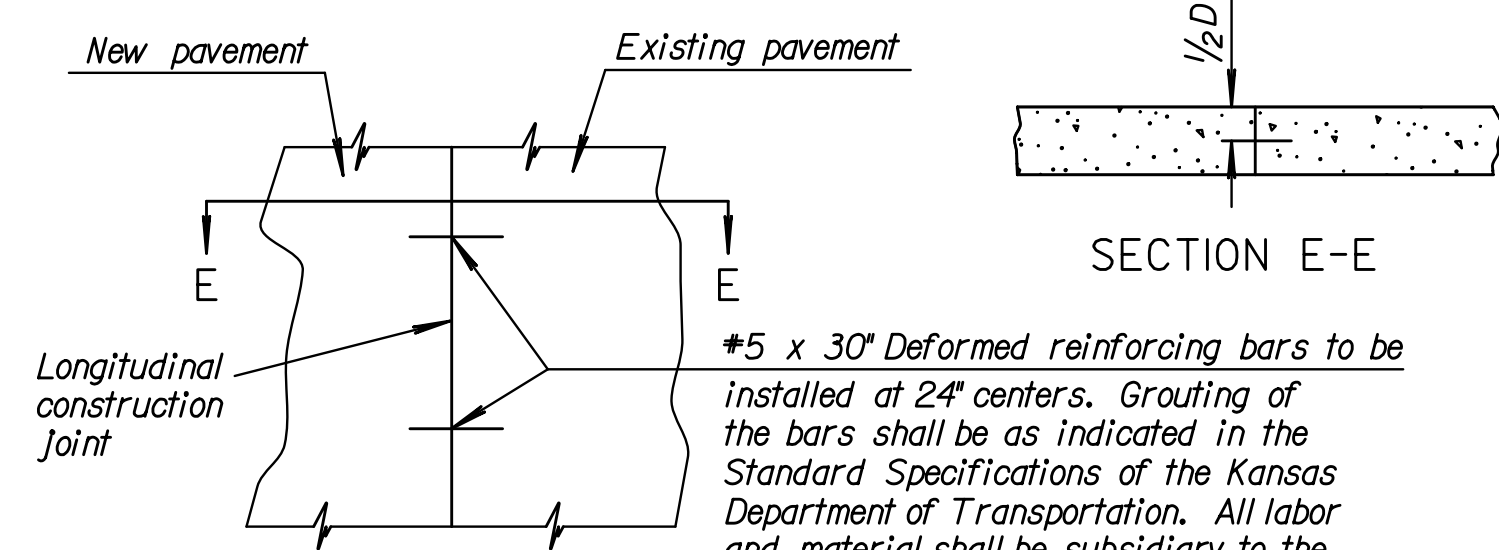


TYPICAL SECTION at PROJECT CL



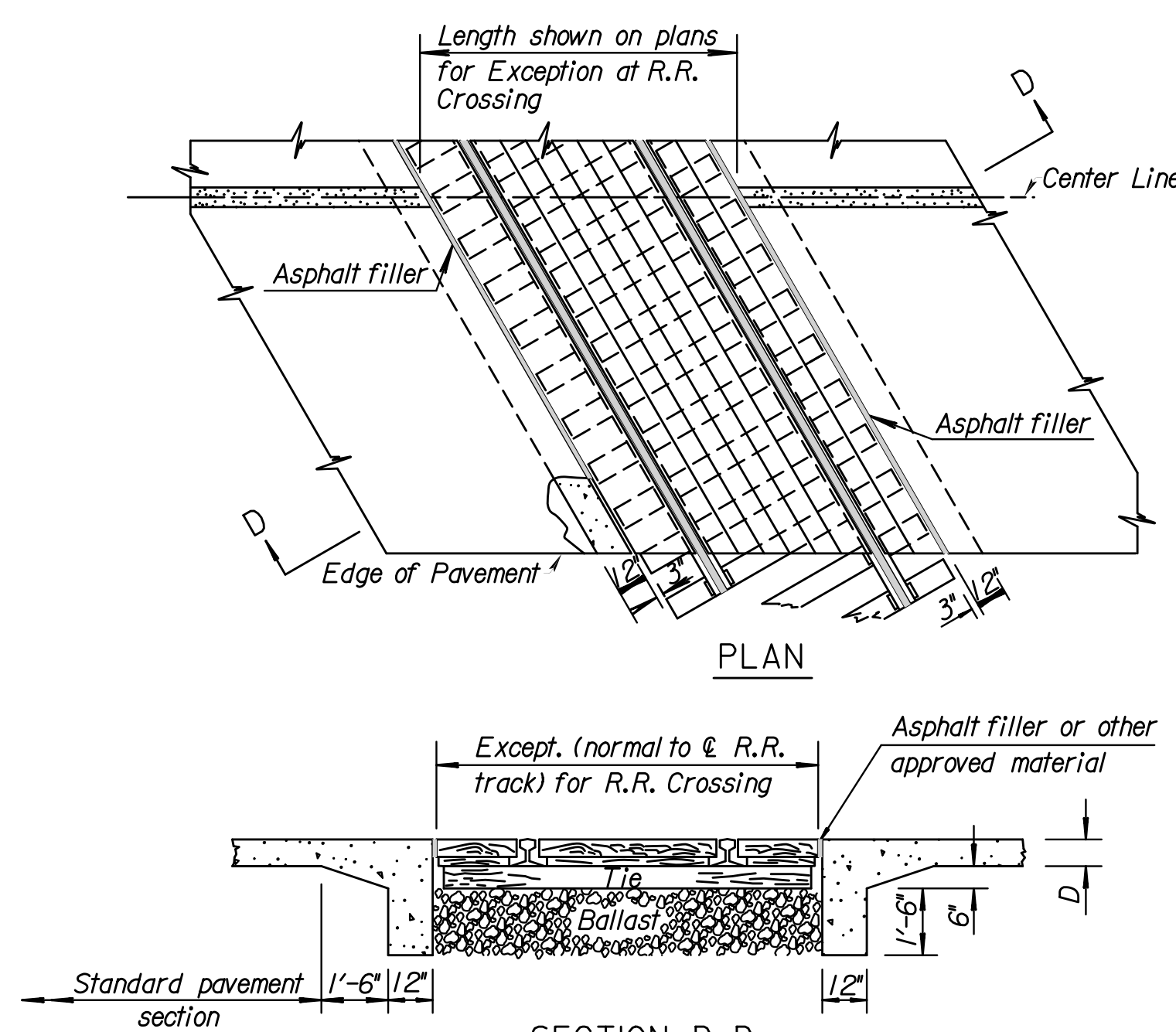
TYPICAL SECTION at CL of CROSSOVER

Note: The U-Turn Median Opening constructed to the dimensions shown shall be paid as 150 sq. yds. of Concrete Pavement (in. Var.) (AE) (Plain).



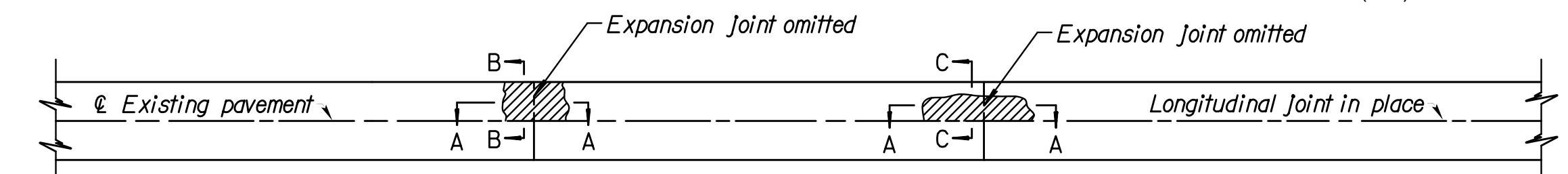
METHOD OF TYING PAVEMENT TO EXISTING PAVEMENT

PAVEMENT WIDENING

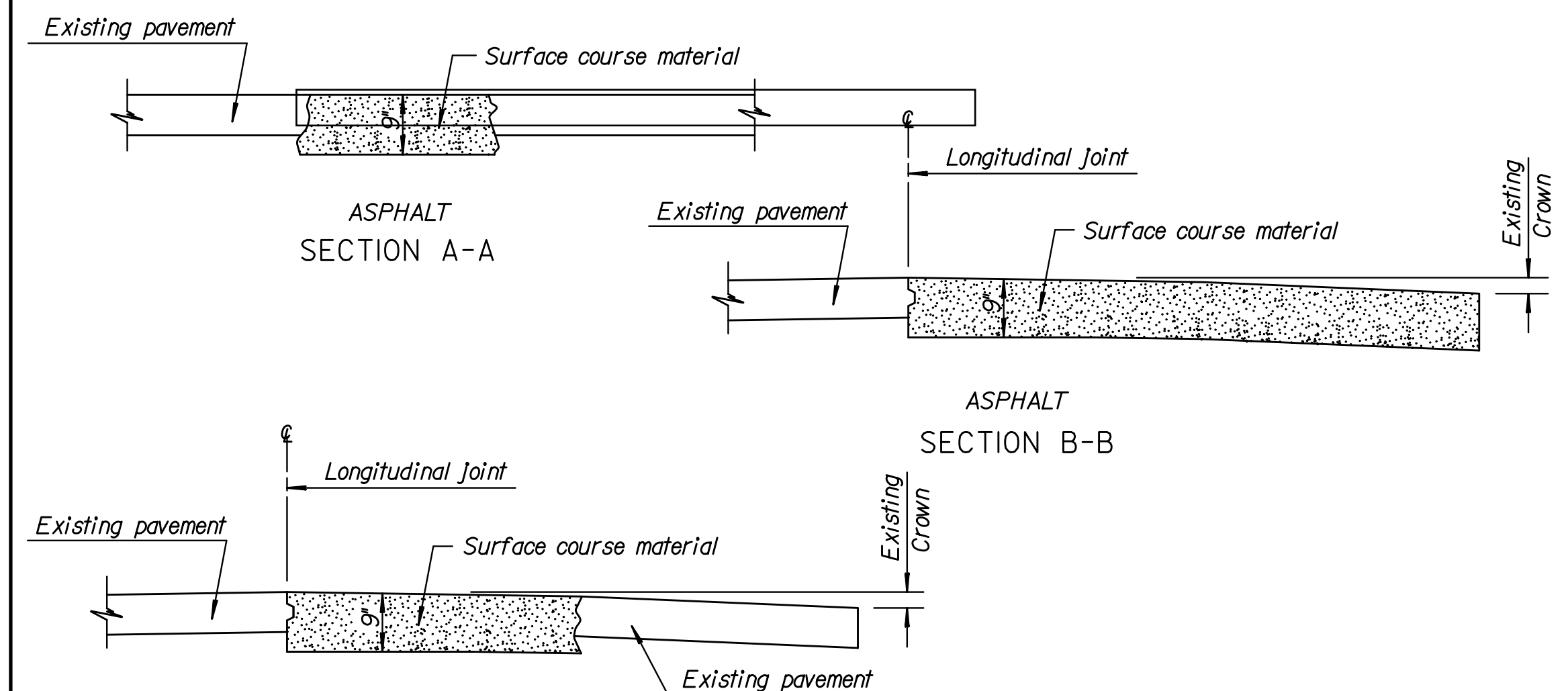


CONCRETE HEADER AT RAILROAD CROSSINGS

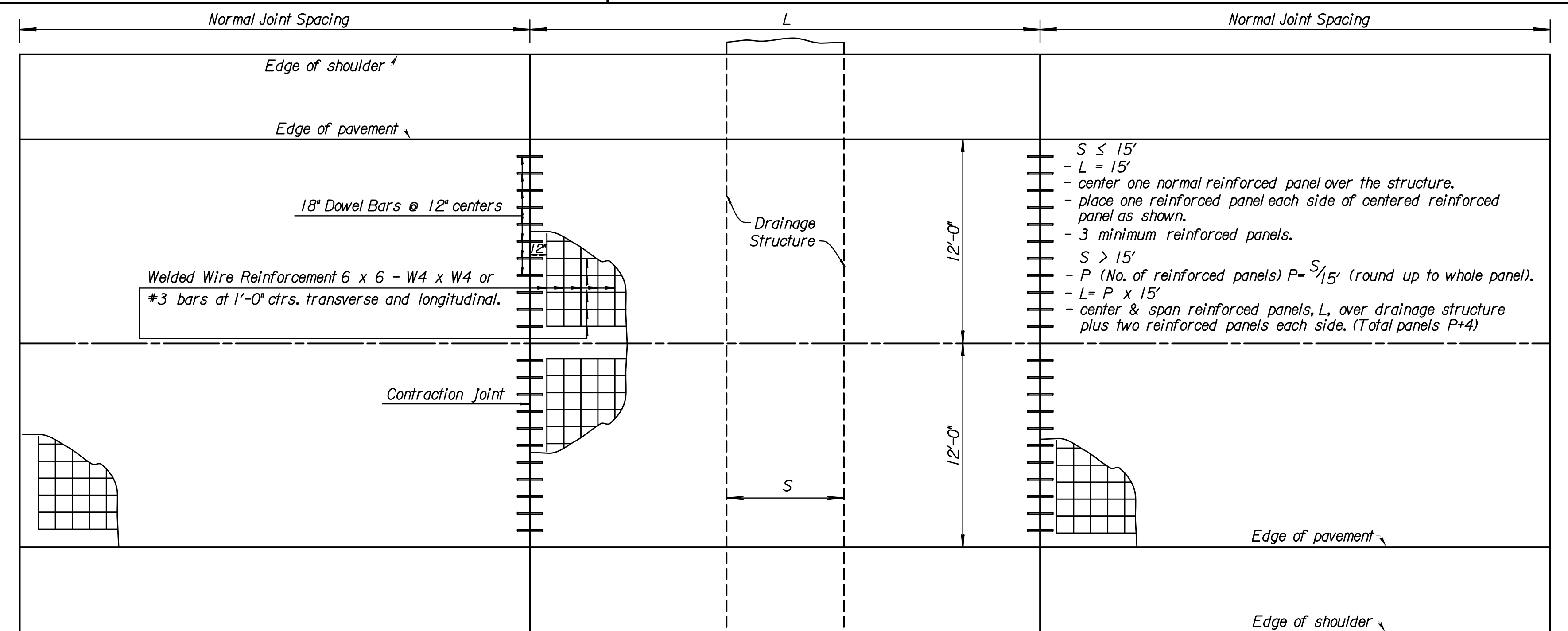
NOTE: Header to be paid for as additional area of concrete pavement, to be computed by adding two (2) feet times the width parallel to R.R. track for each header.



TYPICAL PLAN



ASPHALT PAVEMENT PATCHING



OPTIONAL REINFORCED PCCP PANEL OVER DRAINAGE STRUCTURES

(Example shown with drainage structure less than normal slab width.)

GENERAL NOTE:

This procedure is to be used to mitigate interpanel cracking in PCCP panels being placed over drainage structures. Welded wire reinforcement or #3 rebar may be used to reinforce the pavement.  
 Welded wire reinforcement should be a minimum size of 6 x 6 - W4 x W4. Material placement is 9" from centerline/shoulder joint and 12" from contraction joints. The depth of placement is half the depth of the pavement, +/- a tenth of that depth.  
 When #3 bars are used the spacing is a 12" x 12" grid with 12" spacing from centerline/shoulder joint and 12" from contraction joints. The depth of placement is half the depth of the pavement, +/- a tenth of that depth.  
 See typical sections and pavement standard drawings for normal panel details. The contractor may elect to extend the reinforced panels to any distance or reinforce other locations where potential pavement cracking may occur as coordinated with the engineer.  
 All work and material required for reinforcing PCCP panel over drainage structure or other locations not specified in the plans shall be subsidiary. See the KDOT Standard Specifications for additional details.

NO.	DATE	REVISIONS	BY	APP'D
9	1-10-07	Changed bituminous to asphalt	S.W.K.	J.O.B.
8	3-30-05	Revised reinforcing wire callout	S.W.K.	J.O.B.
7	6-16-04	Added reinf. panel over drain. struct	S.W.K.	J.O.B.
6	5-12-98	Add. Median Crossover pay quantity	R.J.S.	J.O.B.

KANSAS DEPARTMENT OF TRANSPORTATION

CONCRETE PAVEMENT AUX. DETAILS  
 REIN. PANEL OVER STRUCTURE  
 & PAVEMENT PATCHING DETAILS

DESIGNED	6-13-05	APP'D.	James O. Brewer
DESIGN CK.	DETAIL CK.	QUANTITIES	TRACED Bowser
		QUAN. CK.	TRACE CK. Seitz

Plotted: 01-OCT-2009 17:11

Drawn By: marks  
 File: rd720.dgn (rd720)