

GENERAL NOTES

1. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE SAFETY REGULATIONS. ALL CONSTRUCTION SHALL BE COMPLETED FOLLOWING CURRENT CITY STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. CONTRACTOR WILL BE REQUIRED TO PROVIDE NOTICE TO UTILITY COMPANIES A MINIMUM OF SEVENTY-TWO (72) HOURS PRIOR TO ANY EXCAVATION, AS FOLLOWS:

KANSAS ONE-CALL 687-2470

THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF AN EMERGENCY:

AT&T	1-800-246-8464
BLACK HILLS ENERGY	1-800-694-8989
CITY OF WICHITA WATER & SEWER	1-316-219-8921
CITY OF WICHITA STORMWATER	1-316-268-4090
CITY OF WICHITA TRAFFIC	1-316-268-4034
COX COMMUNICATIONS	1-888-249-3530
KANSAS GAS SERVICE	1-888-482-4950
WESTAR ENERGY	1-800-544-4857

3. UTILITY SERVICE LINES, POLES, ETC. ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR OR UNLESS THE PLANS SPECIFICALLY IDENTIFY A UTILITY TO BE ADJUSTED BY ITS OWNER DURING CONSTRUCTION. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

4. RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND EXCESS EXCAVATION WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE AND SITE LOCATION. LOCATIONS, IN THE OPINION OF THE ENGINEER, THAT WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WILL REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OR WETLANDS IS SUBJECT TO U.S. CORPS. OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS WILL REQUIRE ADDITIONAL ARCHAEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION.

5. TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED BY THE CONTRACTOR WITH THE CITY ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.

6. THE CONTRACTOR SHALL GIVE ALL PROPERTY OWNERS AND/OR TENANTS OF DEVELOPED PROPERTY ADJUTING THE CONSTRUCTION OF THIS PROJECT A MINIMUM OF TEN (10) DAYS NOTICE PRIOR TO START OF CONSTRUCTION.

7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS.

8. THE ENGINEERING DIVISION SHALL FIELD LOCATE WATER VALVES ONE TIME DURING CONSTRUCTION WHEN REQUESTED BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PRESERVE SUCH FIELD LOCATIONS DURING THE CONSTRUCTION PROCESS. WATER VALVES, VALVE BOXES OR FIRE HYDRANTS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY CONTRACTOR AT HIS OWN EXPENSE. VALVE BOXES AND WATER METERS WITHIN THE PROJECT LIMITS SHALL BE ADJUSTED TO MATCH FINAL GRADES BY THE CONTRACTOR.

9. THE CONTRACTOR SHALL NOTIFY THE INSPECTING ENGINEER AND TOM MASON AT 316-268-4574 WITH THE CITY OF WICHITA WITH THE ANTICIPATED CONSTRUCTION START DATE AND NOTIFY THEM OF PROJECT COMPLETION. STAKING AND INSPECTION FOR THIS PROJECT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

10. IF TRAFFIC WILL BE IMPACTED BY CONSTRUCTION, A TRAFFIC CONTROL PLAN MUST BE SUBMITTED AND APPROVED BY THE CITY TRAFFIC ENGINEER AT TRAFFIC@WICHITA.GOV BEFORE CONSTRUCTION CAN BEGIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL MEASURES TO FACILITATE CONSTRUCTION. ALL CONSTRUCTION ZONE MARKINGS AND SIGNAGE SHALL CONFORM TO THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS PUBLISHED BY THE US DEPT. OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION. ALL COSTS ASSOCIATED WITH CONSTRUCTION MARKINGS AND SIGNAGE SHALL BE THE CONTRACTORS RESPONSIBILITY.

11. ALL ELEVATIONS SHOWN ARE NAVD 88.

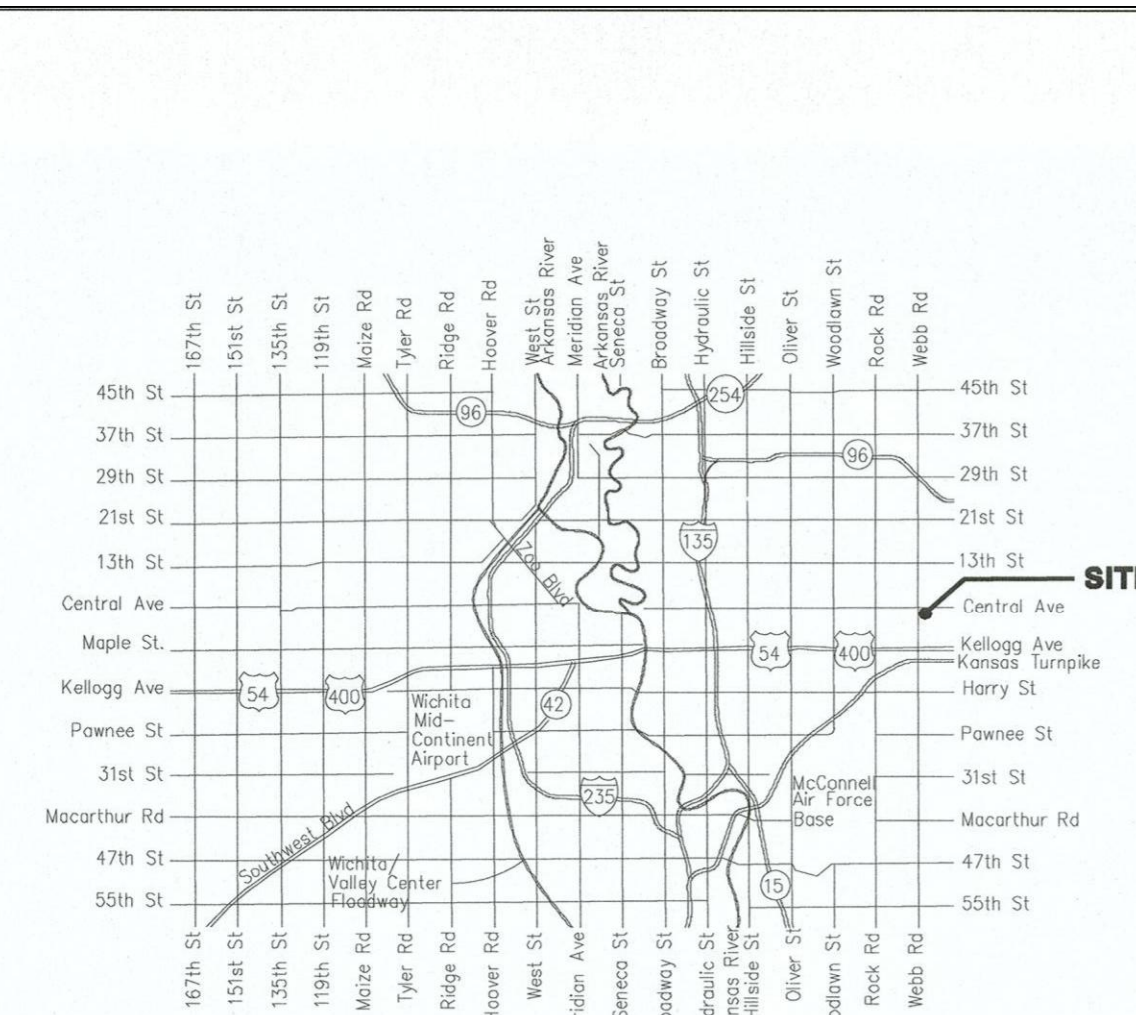
12. ALL AREAS DISTURBED DURING CONSTRUCTION THAT WILL NOT BE UNDER PROPOSED PAVEMENT SHALL BE RESTORED TO MATCH EXISTING CONDITIONS.

PAVING IMPROVEMENTS to serve TEXTRON AVIATION EAST CAMPUS CITY OF WICHITA, KANSAS

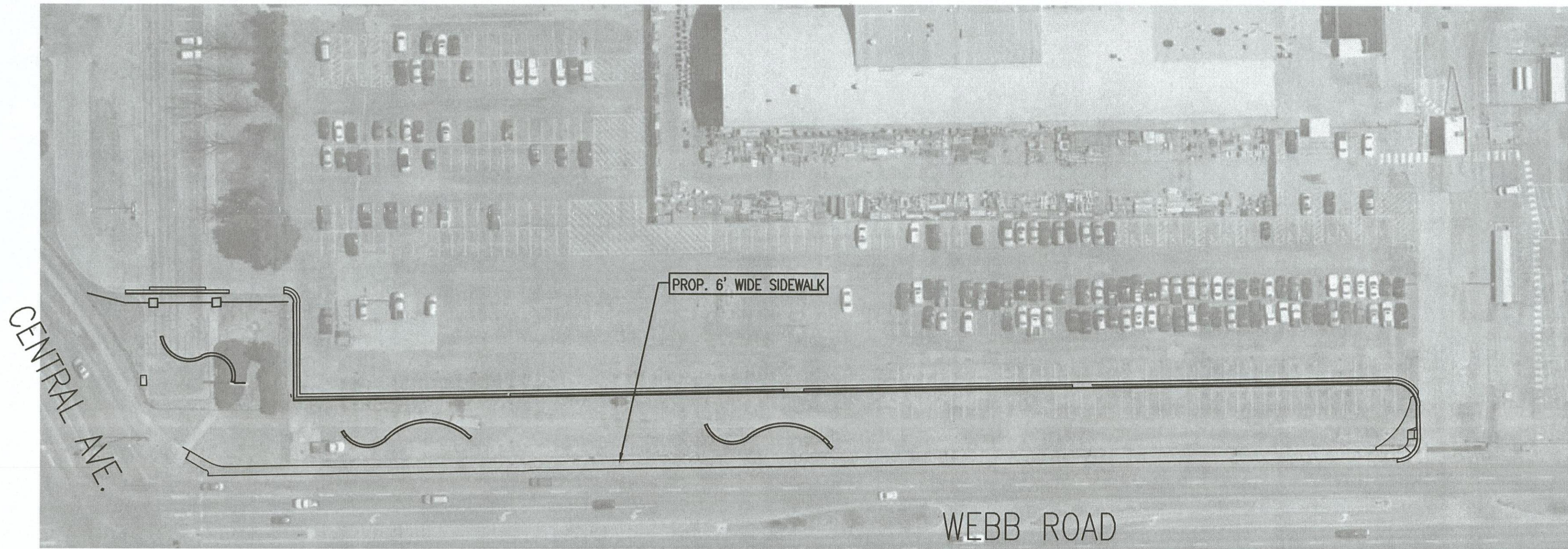
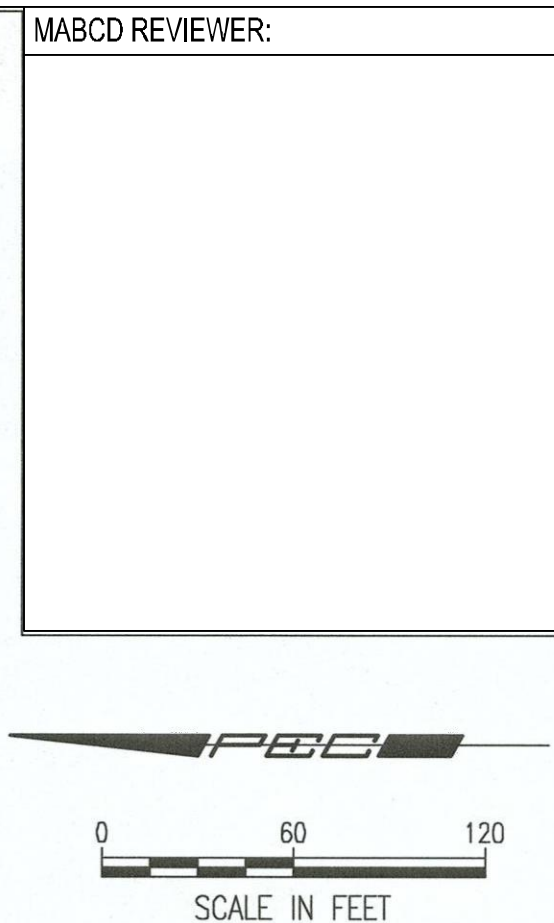
Gary Janzen, P.E. City Engineer
Project Number
291 PPP (132003)

SHEET INDEX

- SHEET NO. CP-101 PPP TITLE SHEET
- SHEET NO. CP-102 PAVING PLAN
- SHEET NO. CP-501 PAVING DETAILS
- SHEET NO. CP-502 WHEELCHAIR RAMP DETAILS
- SHEET NO. CS-101 GEOMETRY PLAN
- SHEET NO. SA-101 SITE ARCHITECTURAL OVERALL PLAN
- SHEET NO. SA-102 SITE ARCHITECTURAL PLAN - NORTH
- SHEET NO. CD-101 DEMOLITION PLAN
- SHEET NO. CG-101 OVERALL GRADING PLAN
- SHEET NO. CG-102 ENLARGED GRADING PLANS
- SHEET NO. CG-501 EROSION CONTROL PLAN
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- SHEET NO. CT-501 TRAFFIC CONTROL STANDARD TET00
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- SHEET NO. CT-506 TRAFFIC CONTROL STANDARD TET44
- EROSION CONTROL BMP DETAILS
(available on the City's website)



Vicinity Map



CONTROL POINTS

- PL No. 101
MAG NAIL IN ASPHALT
N: 1,688,796.4580, E: 1,680,968.0380
1. ±100' NE TO INTERSECTION OF CENTRAL & WEBB RD.
2. 3.8' NORTH TO BLACK FENCE
3. 24' EAST TO ELECTRIC PANEL
4. 19.7' WEST TO CENTER OF SQUARE VAULT LID
- PL No. 102
#4 BAR W/ PEC CP CAP, FLUSH W/ GROUND
N: 1,688,934.1100, E: 1,681,474.1110
1. LOCATED SW OF THE TEXTRON AVIATION ACTIVITY CENTER BUILDING
2. 9.5' NE TO CORNER OF BUILDING
3. 24.7' S-SE TO LIGHT POLE BASE
4. 16.4' W-NW TO NE CORNER OF SIDEWALK
- PL No. 103
MAG NAIL IN ASPHALT
N: 1,688,449.3190, E: 1,680,941.6180
1. LOCATED ON EAST SIDE OF WEBB RD. BETWEEN MKEC BUILDINGS ON THE WEST SIDE OF WEBB RD.
2. 10.5' WEST TO CENTER OF CURB INLET
3. 12.40' W-SW TO CHISELED SQUARE, BMK #203
- PL No. 104
MAG NAIL IN ASPHALT
N: 1,687,952.9650, E: 1,680,951.1960
1. ±20' SOUTH OF WEST GATE TRUCK ENTRANCE
2. 11.2' WEST TO CENTER OF CURB INLET
3. 13.2' W-SW TO CHISELED SQUARE, BMK #204

BENCHMARKS

- BM #201 - CHISELED SQUARE ON SOUTHEAST CORNER OF CONCRETE PAD FOR TRANSFORMER P13744 LOCATED ±200' SOUTHEAST OF INTERSECTION OF CENTRAL & WEBB. ELEV.=1364.67 (NAVD88)
- BM #202 - CHISELED SQUARE ON SOUTH TOP OF LIGHT POLE BASE LOCATED IN THE SIDEWALK OFF THE SOUTHWEST CORNER OF THE TEXTRON AVIATION ACTIVITY CENTER BUILDING. ELEV.=1363.87 (NAVD88)
- BM #203 - CHISELED SQUARE ON SOUTHWEST CORNER OF CURB INLET, LOCATE ON EAST SIDE OF WEBB RD., BETWEEN MKEC BUILDINGS ON THE WEST SIDE OF WEBB RD. ELEV.=1360.96 (NAVD88)
- BM #204 - CHISELED SQUARE ON SOUTHWEST CORNER OF CURB INLET, LOCATED ±20' SOUTH OF WEST GATE TRUCK ENTRANCE. ELEV.=1364.90 (NAVD88)

APPROVED AS NOTED
BY WICHITA PUBLIC WORKS
ENGINEERING DIVISION

Engineering Neth Gatchery 7.30.19

NOTE TO CONTRACTORS

Inspection and testing for this project is to be provided by a Licensed Consulting Engineering Firm under contract with the Owner/Developer. Said inspection to be in accordance with the City of Wichita standard construction engineering practices and certified by a Licensed Professional Engineer in the state of Kansas. No work shall be performed by the Contractor without such inspection nor shall any work be commenced without written authorization by City Engineering. All Construction and Materials shall comply with the current City of Wichita Specifications and Standards and Special Provisions. (on file and available at Wichita.gov).

An approved copy of these plans signed by City staff are required on-site.

AS BUILT

Contractor: Pearson Construction
Inspector: Matt Perez
Date: 12-5-2019

117 E. Lewis,
Wichita, KS 67202 (316)264-0242

JULY 2019

PEC
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
303 SOUTH TOPEKA WICHITA, KS 67202
316-262-2691 www.pec1.com

Date	Description
08/01/19	Addendum #1

TEXTRON AVIATION
EAST CAMPUS - WEBB RD.
LANDSCAPING IMPROVEMENTS
(CENTRAL AVE TO TRUCK ENTRANCE)
WICHITA, KS

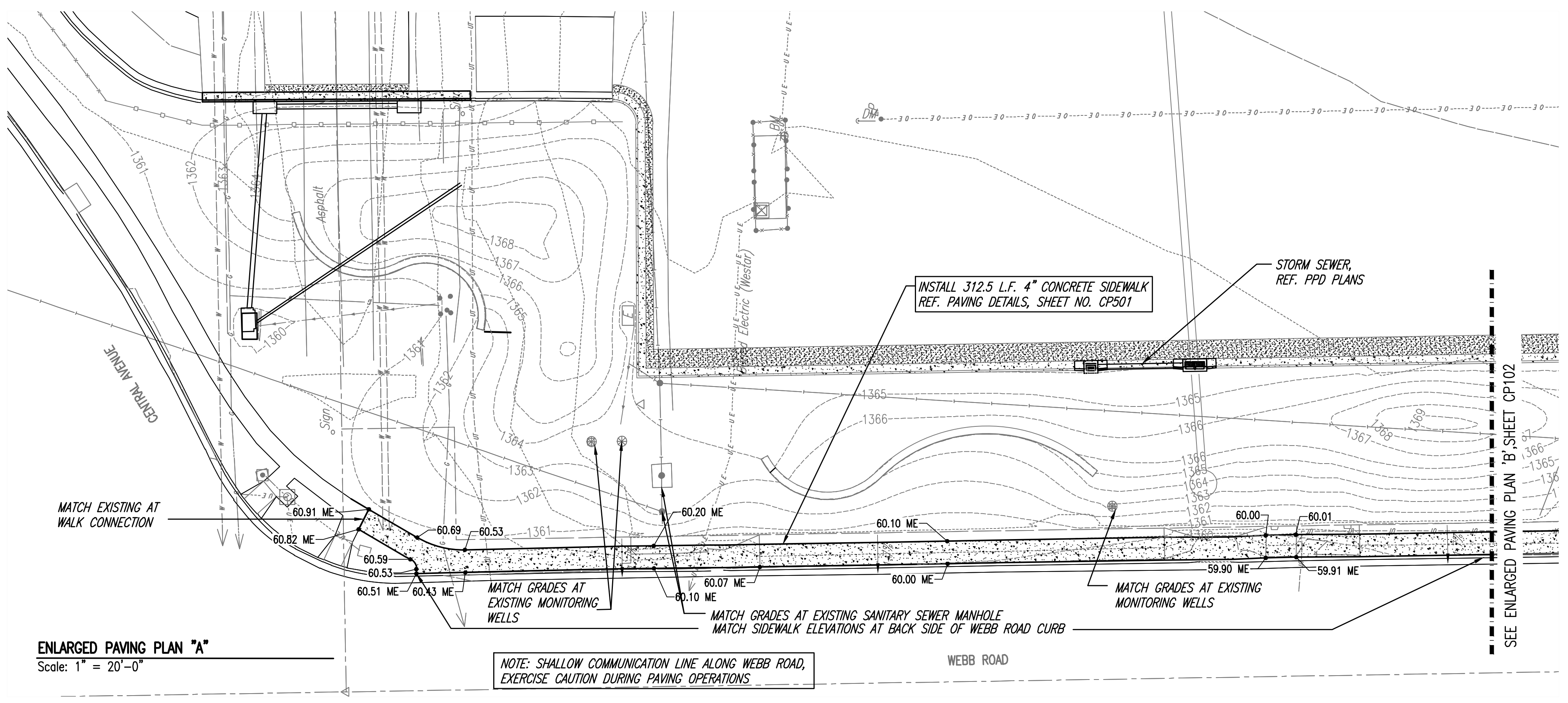
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DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
PPP TITLE SHEET

CP-101

EAST CAMPUS - TXTAV
CENTRAL AVE. & WEBB RD.

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- LEGEND**
- 53.60 PROPOSED PAVEMENT ELEVATION (+ 1300 = NAVD88 ELEVATION)
 - 50.3 PROPOSED GROUND ELEVATION (+ 1300 = NAVD88 ELEVATION)
 - BREAK LINE
 - SWALE FLOW LINE
 - FLOW DIRECTION
 - ASPHALT
 - CONCRETE CURB AND GUTTER
 - 8" STAMPED CONCRETE
 - HP = HIGH POINT
 - TC = TOP OF CURB
 - FL = FLOW LINE
 - ME = MATCH EXISTING CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS AND REPORT ALL DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

MABCD REVIEWER:

SCALE IN FEET

0 20 40

- ADA NOTES**
- SIDEWALKS SHALL HAVE RUNNING SLOPE NOT STEEPER THAN 1:20. THE CROSS SLOPE OF WALKING SURFACES SHALL NOT BE STEEPER THAN 1:48.
 - CURB RAMPS SHALL NOT EXCEED SIX INCHES IN HEIGHT OR HAVE RUNNING SLOPE STEEPER THAN 1:12. CROSS SLOPE SHALL OF RAMPS SHALL NOT BE STEEPER THAN 1:48.
 - ADA ACCESSIBLE PARKING STALL & ACCESS AISLE SHALL NOT EXCEED 1:48 SLOPE IN ALL DIRECTIONS.
 - CONTRACTOR SHALL ADHERE TO THE LATEST ADA REGULATIONS AND REPORT ANY TO THE ENGINEER FOR RESOLUTION PRIOR TO CONSTRUCTION.
 - REFERENCE ARCHITECTURAL PLANS FOR DOOR THRESHOLD DETAILS. ELEVATIONS SHOWN ON THIS DRAWING ARE TO FINISHED FLOOR.

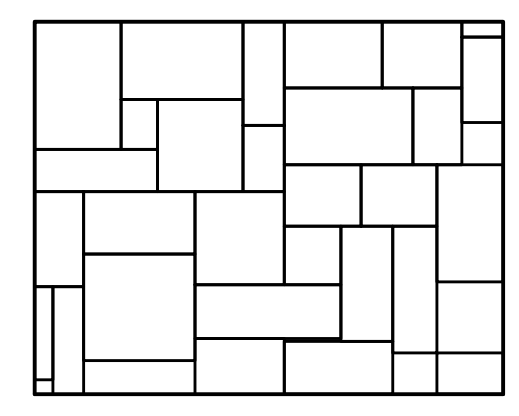
- NOTES**
- REFERENCE SHEET NO. CT-101 FOR TRAFFIC CONTROL PLAN.
 - THE CONTRACTOR SHALL ONLY BE ALLOWED TO CLOSE ONE LANE OF NORTH BOUND TRAFFIC ON WEBB ROAD FOR A PERIOD NO LONGER THAN 2 WEEKS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY OF WICHITA.
 - THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 5 DAY NOTICE OF LANE CLOSURE TO THE CITY OF WICHITA ENGINEERING DEPARTMENT.
 - THE CONTRACTOR SHALL PROTECT THE EXISTING CURB AND GUTTER ALONG WEBB ROAD DURING CONSTRUCTION. IN THE EVENT THE EXISTING CURB AND GUTTER IS DAMAGED DURING CONSTRUCTION, THE CONTRACTOR SHALL REPAIR THE EXISTING CURB AND GUTTER BY REMOVE AND REPLACING THE CURB AND GUTTER TO THE NEAREST JOINT. THIS COST SHALL BE CONSIDERED SUBSIDIARY TO THE PROJECT.

AS BUILTS

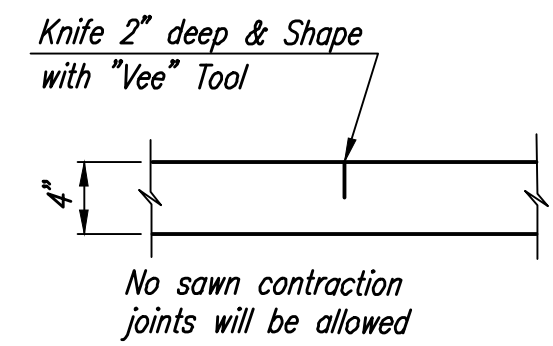
Contractor: Pearson Construction
Inspector: Matt Perez
Date: 12-5-2019

KEMILLER ENGINEERING PA

117 E. Lewis,
Wichita, KS 67202 (316)264-0242



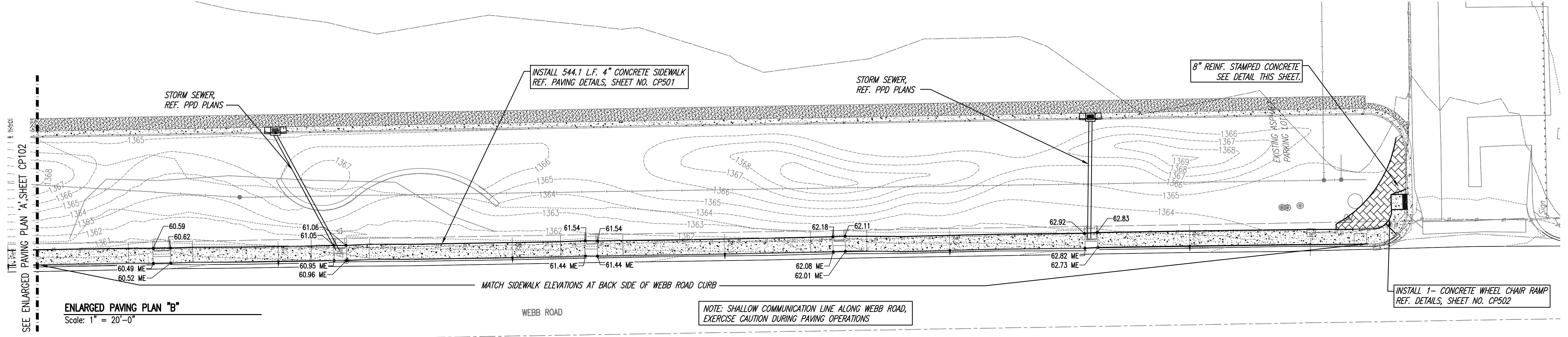
ASHLAR STONE STAMP PATTERN



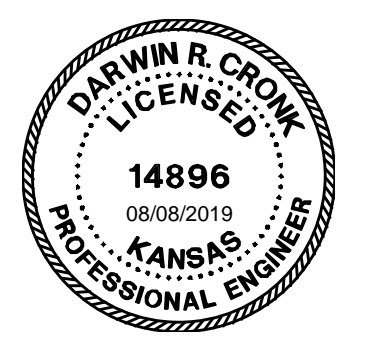
CONTRACTION JOINT

COLOR HARDENER: A-50 SLATE GRAY LITHOCHROME COLOR HARDENER (BY SCHOFIELD SYSTEMS OR APPROVED EQUAL).
RATE: BROADCAST 50LB (1 BAG) PER 100 SQ.FT. OVER FRESHLY PLACED CONCRETE.
STAMP PATTERN: ASHLAR STONE

8" STAMPED CONCRETE



ENTIRE SHEET



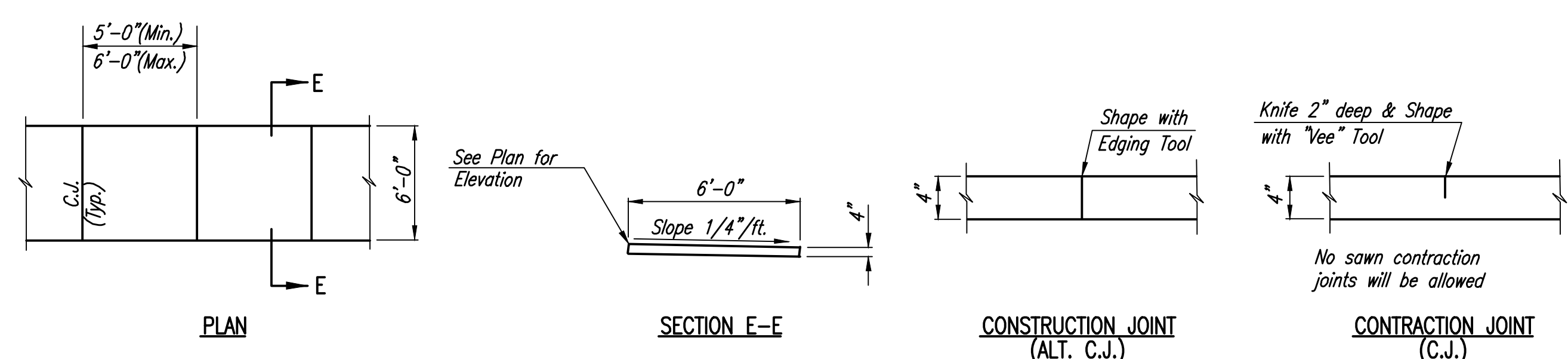
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2 8/8/19	ADDENDUM 3

JOB NUMBER:	198003-007
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DRAWN:	KMH
CHECKED:	NLS

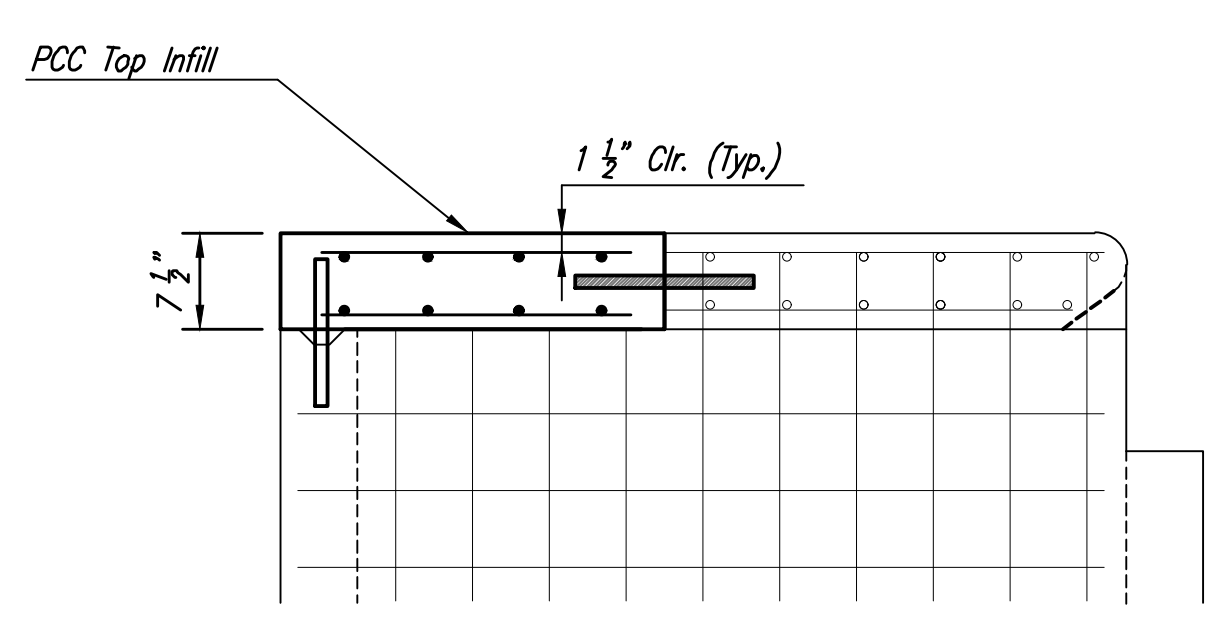
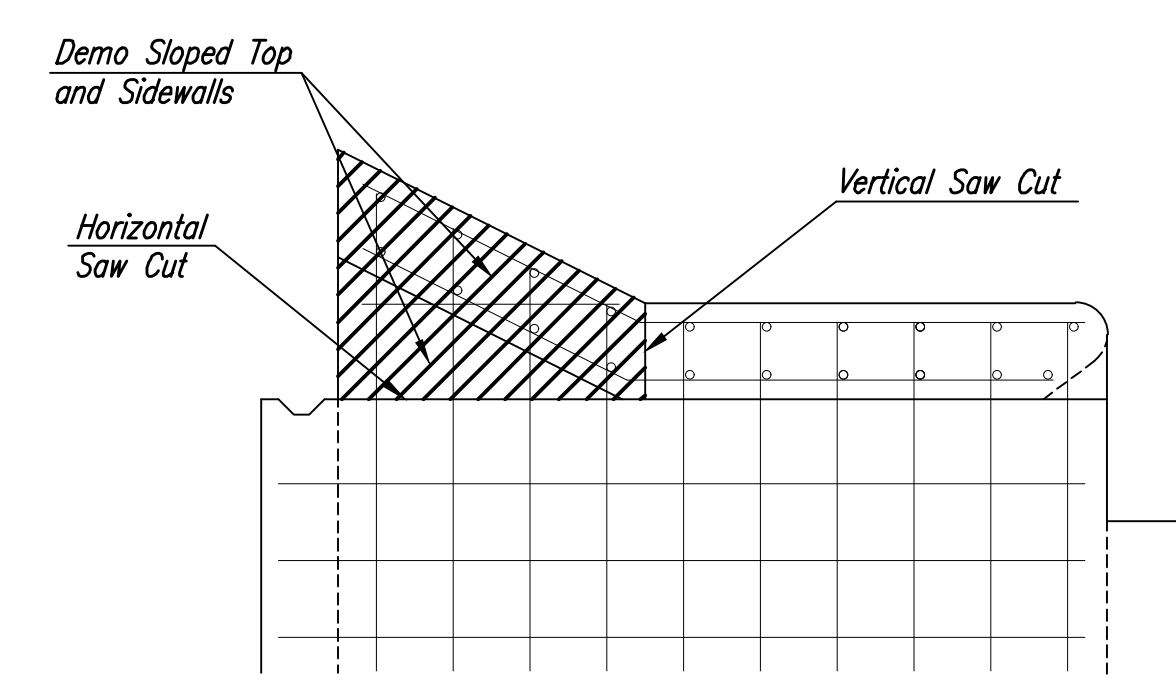
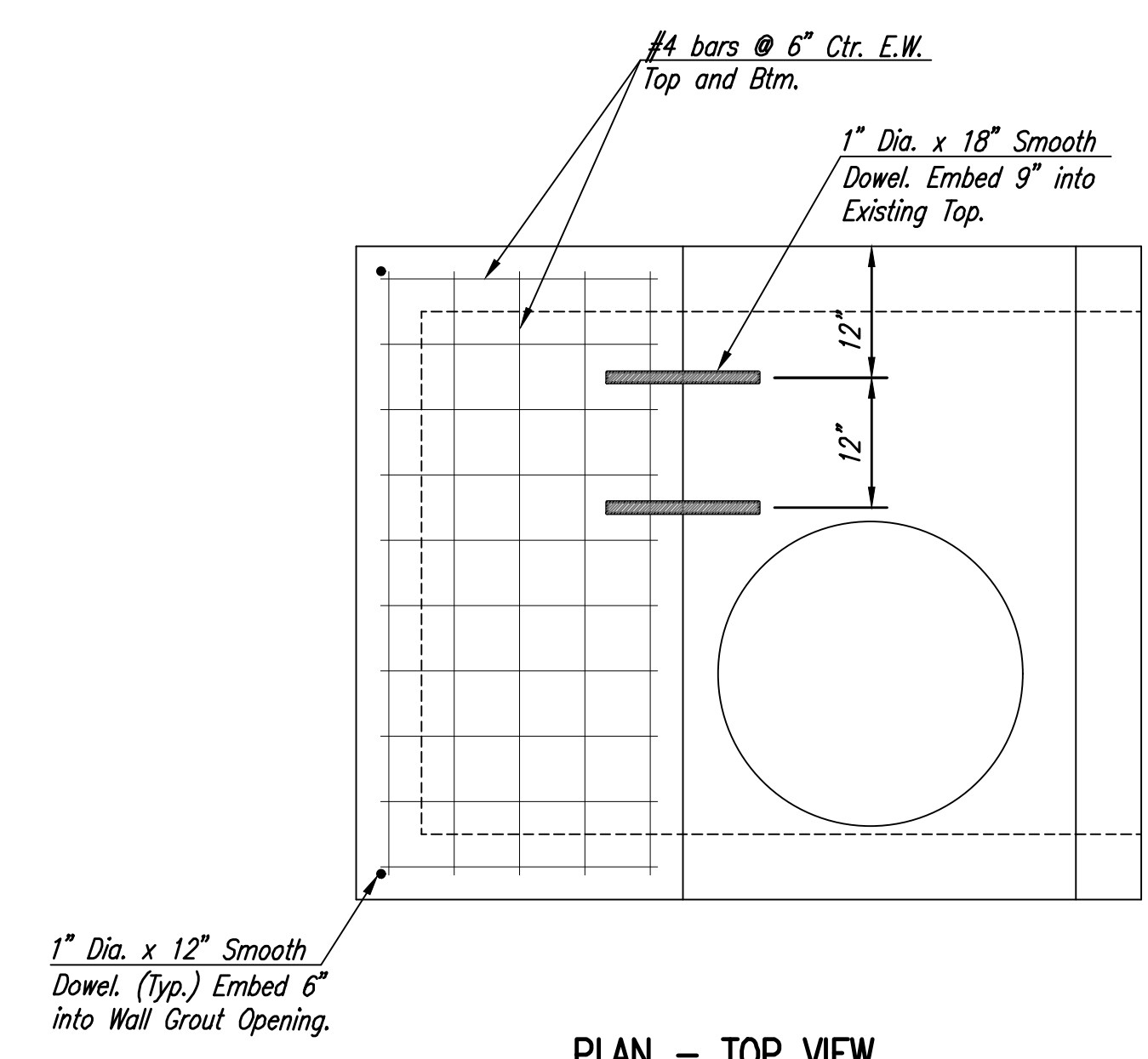
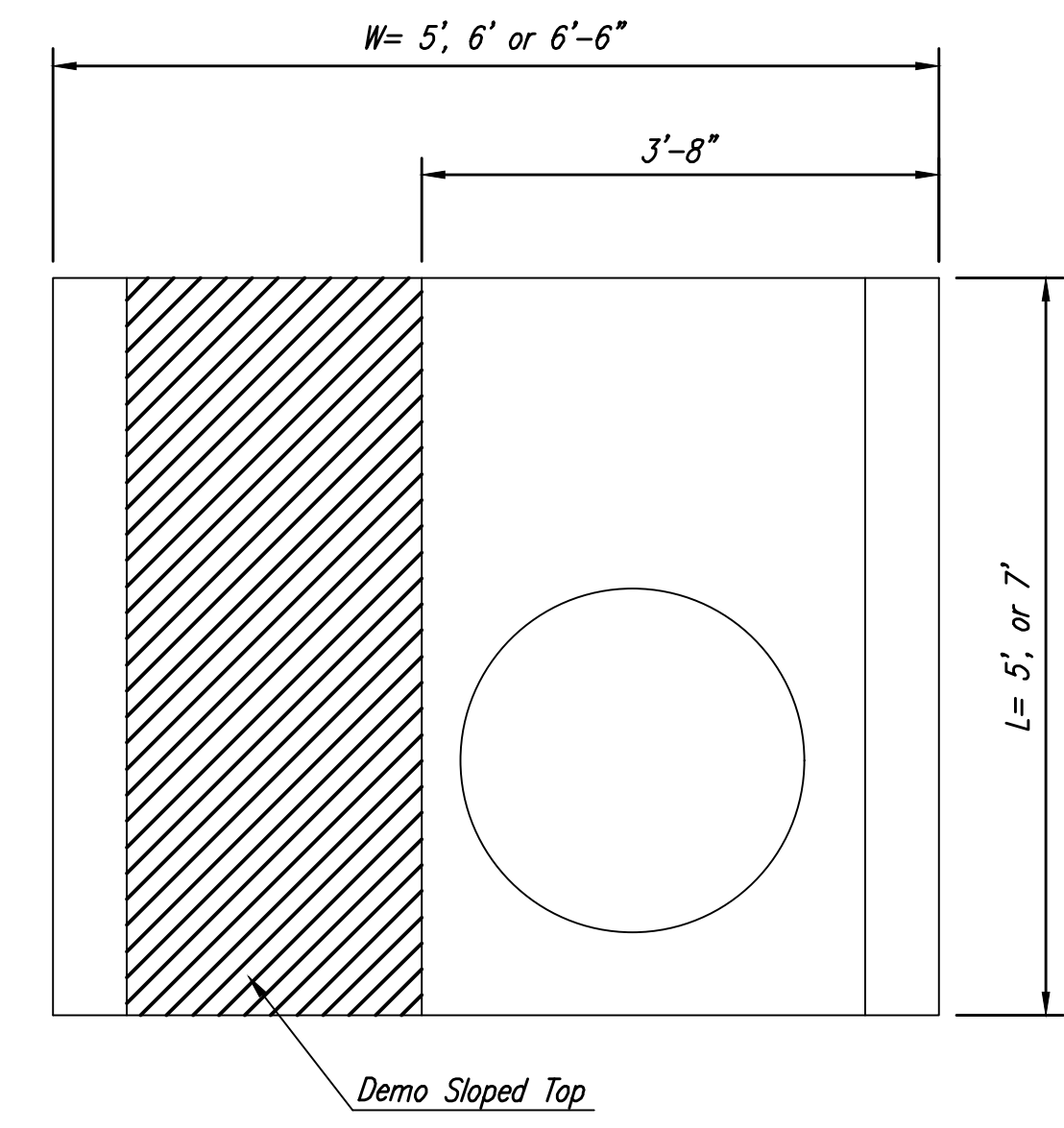
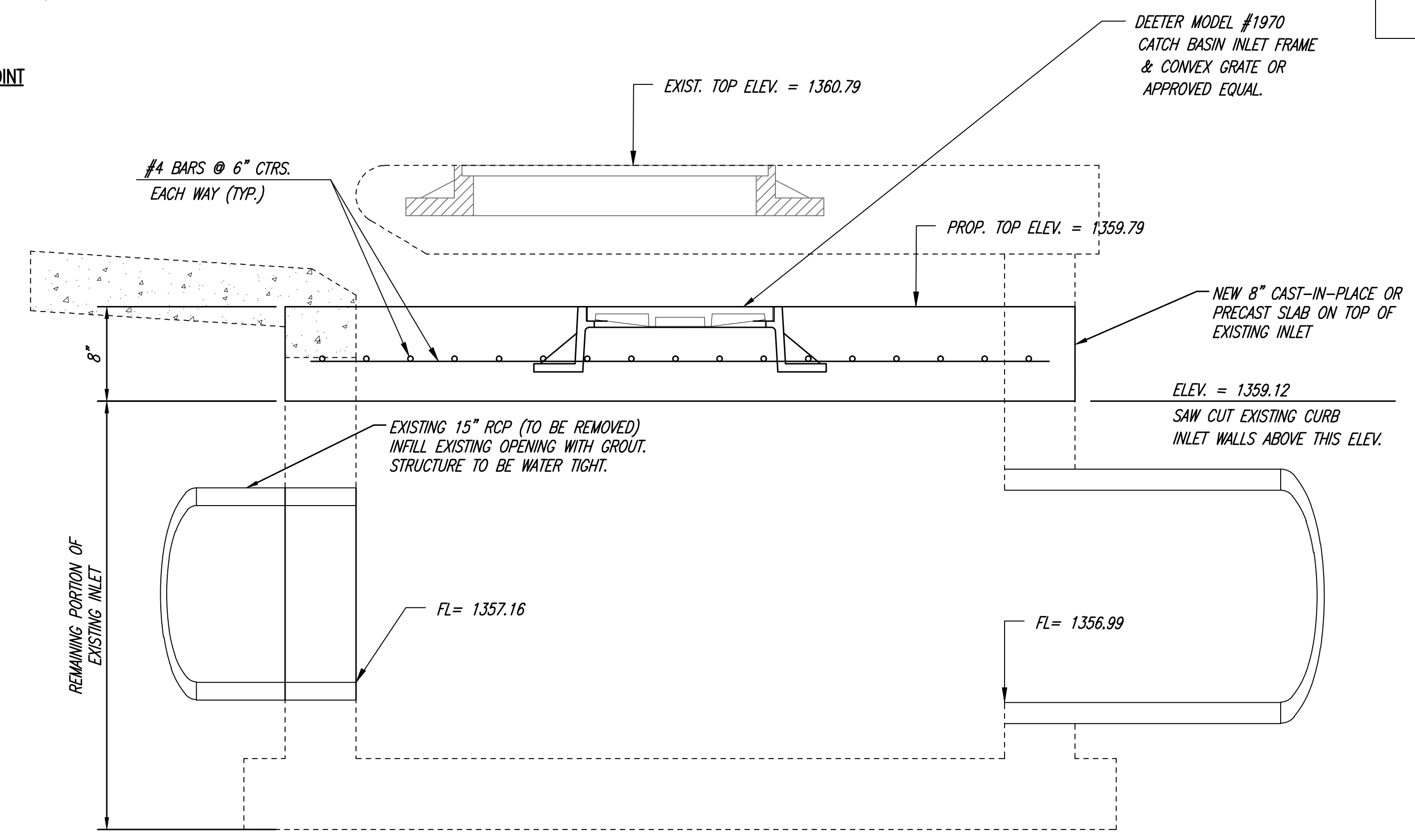
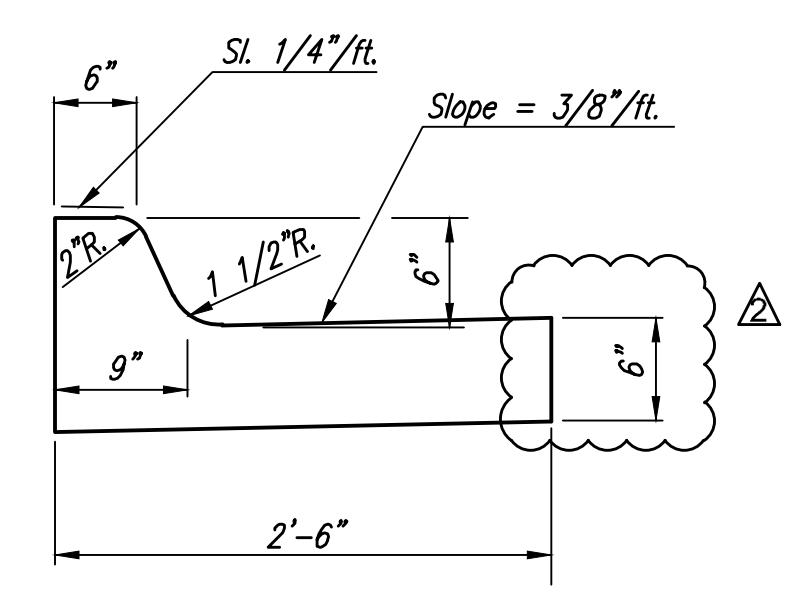
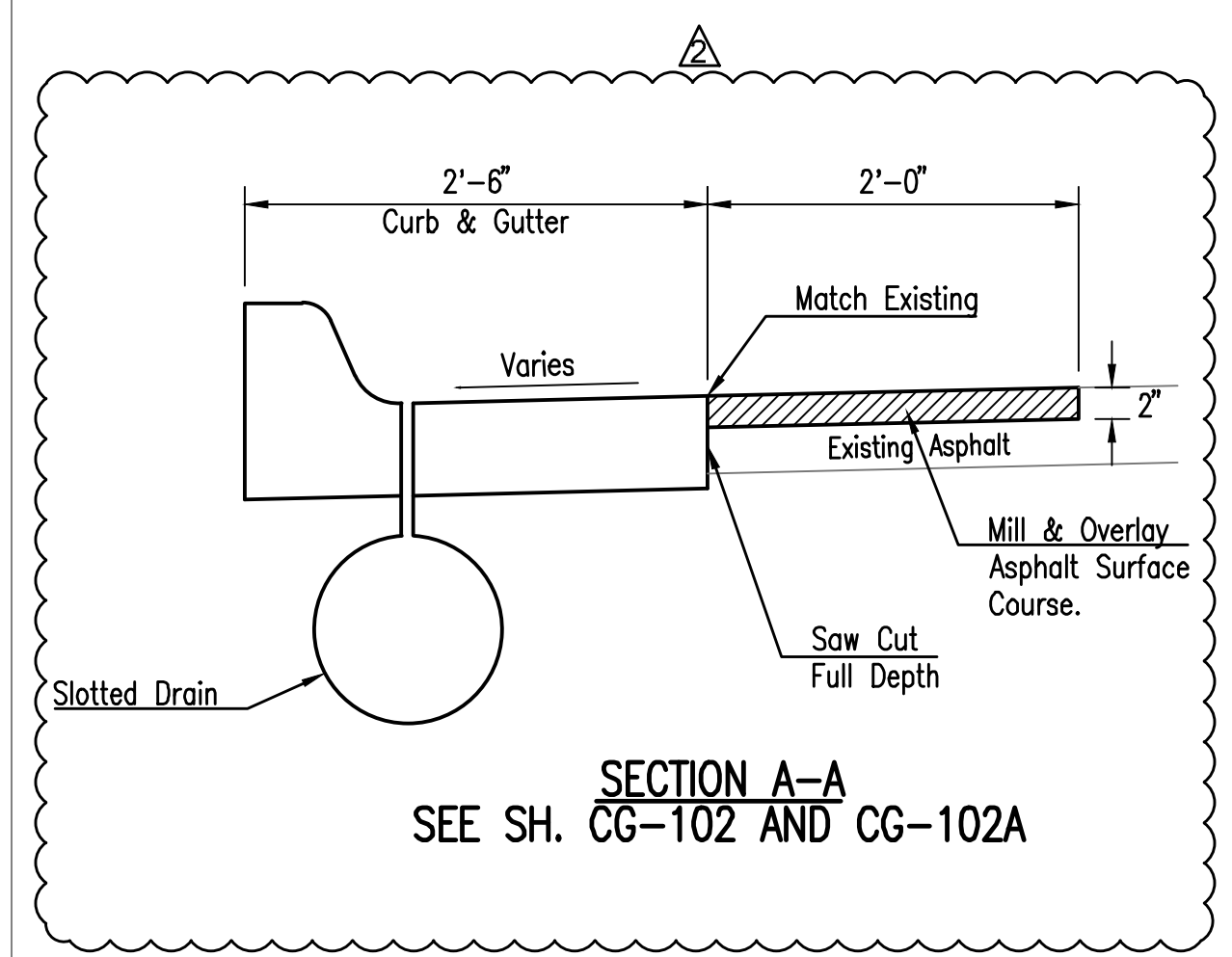
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PAVING PLAN

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MABCD REVIEWER:



4" CONCRETE SIDEWALK DETAILS



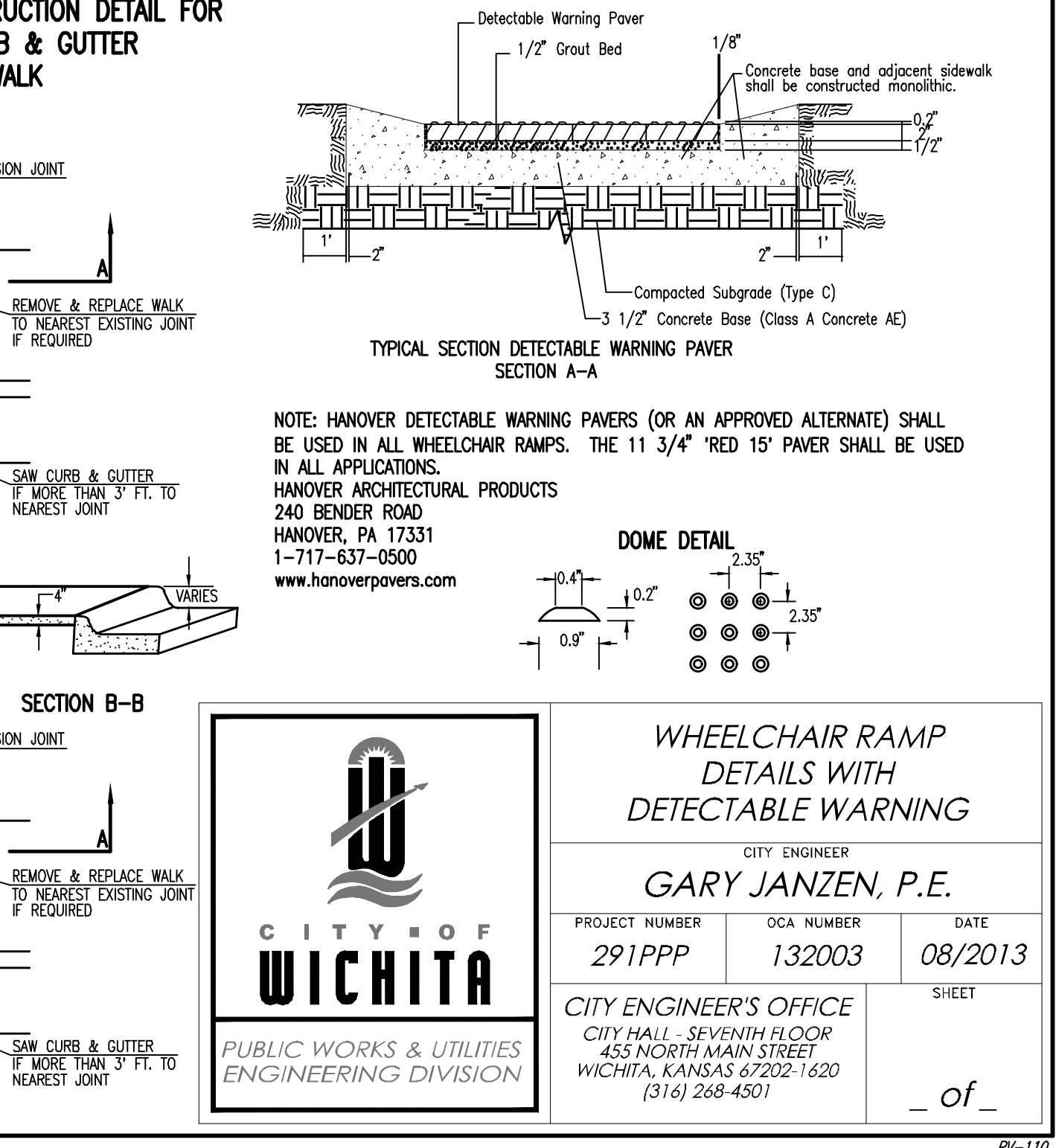
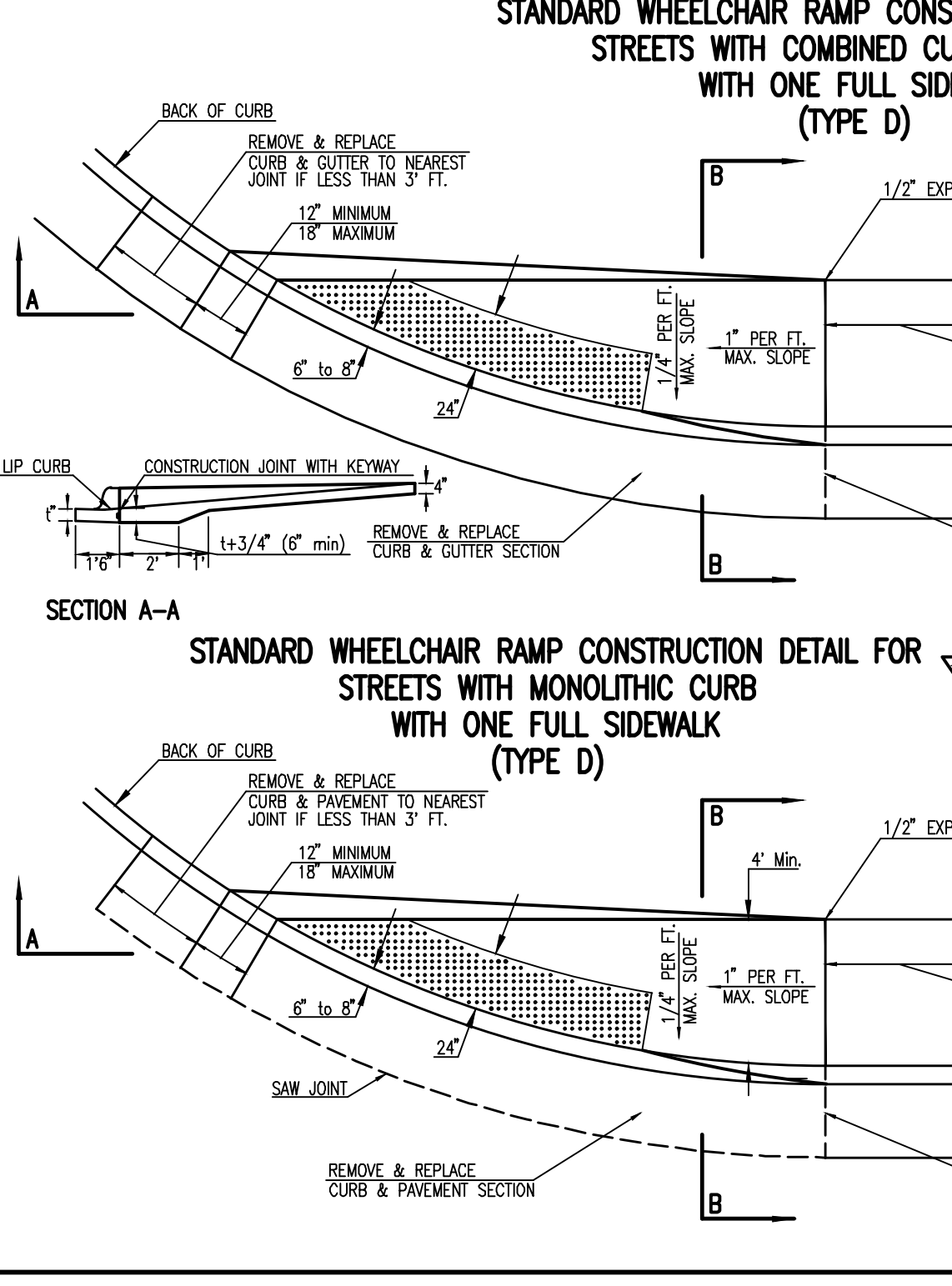
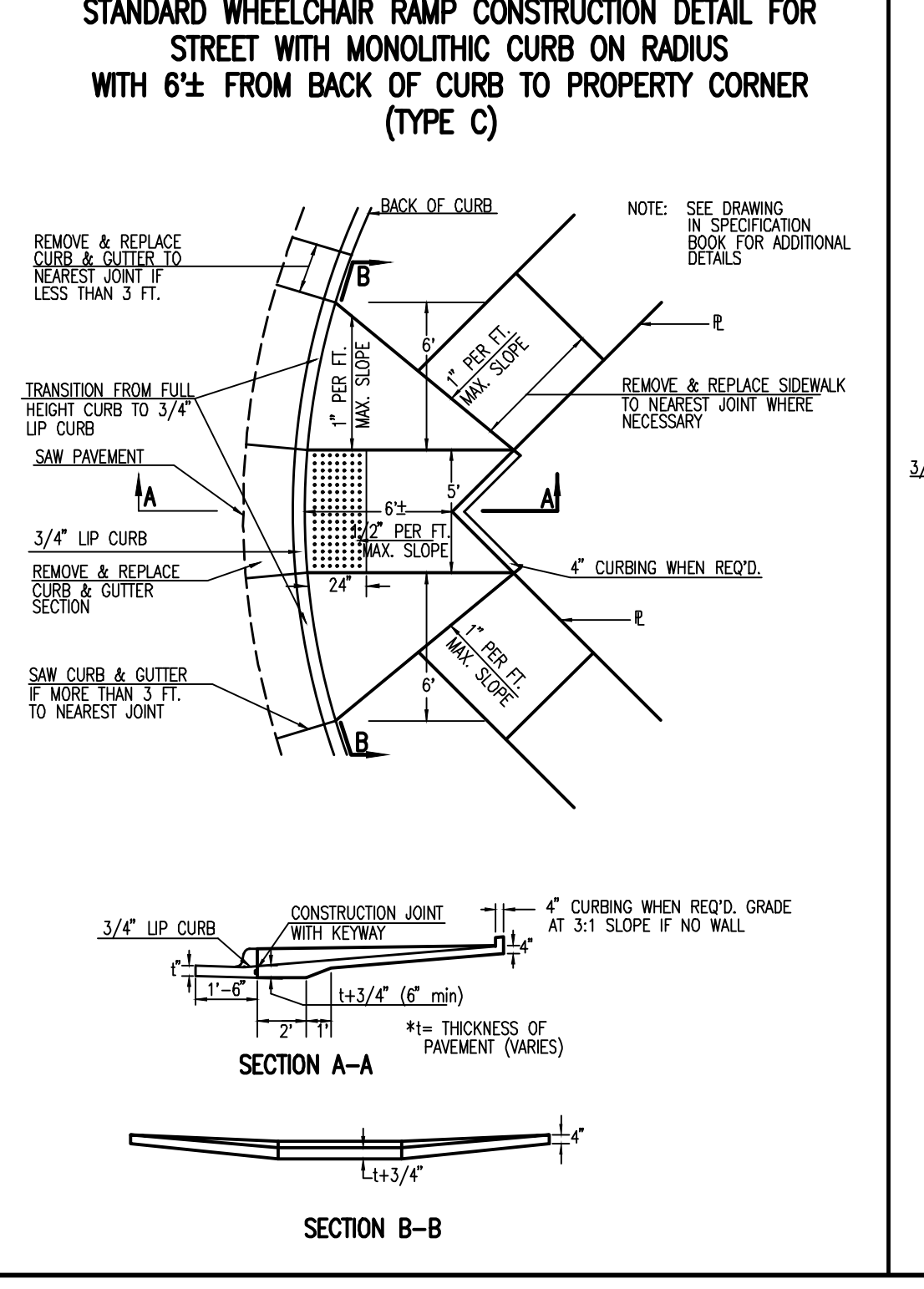
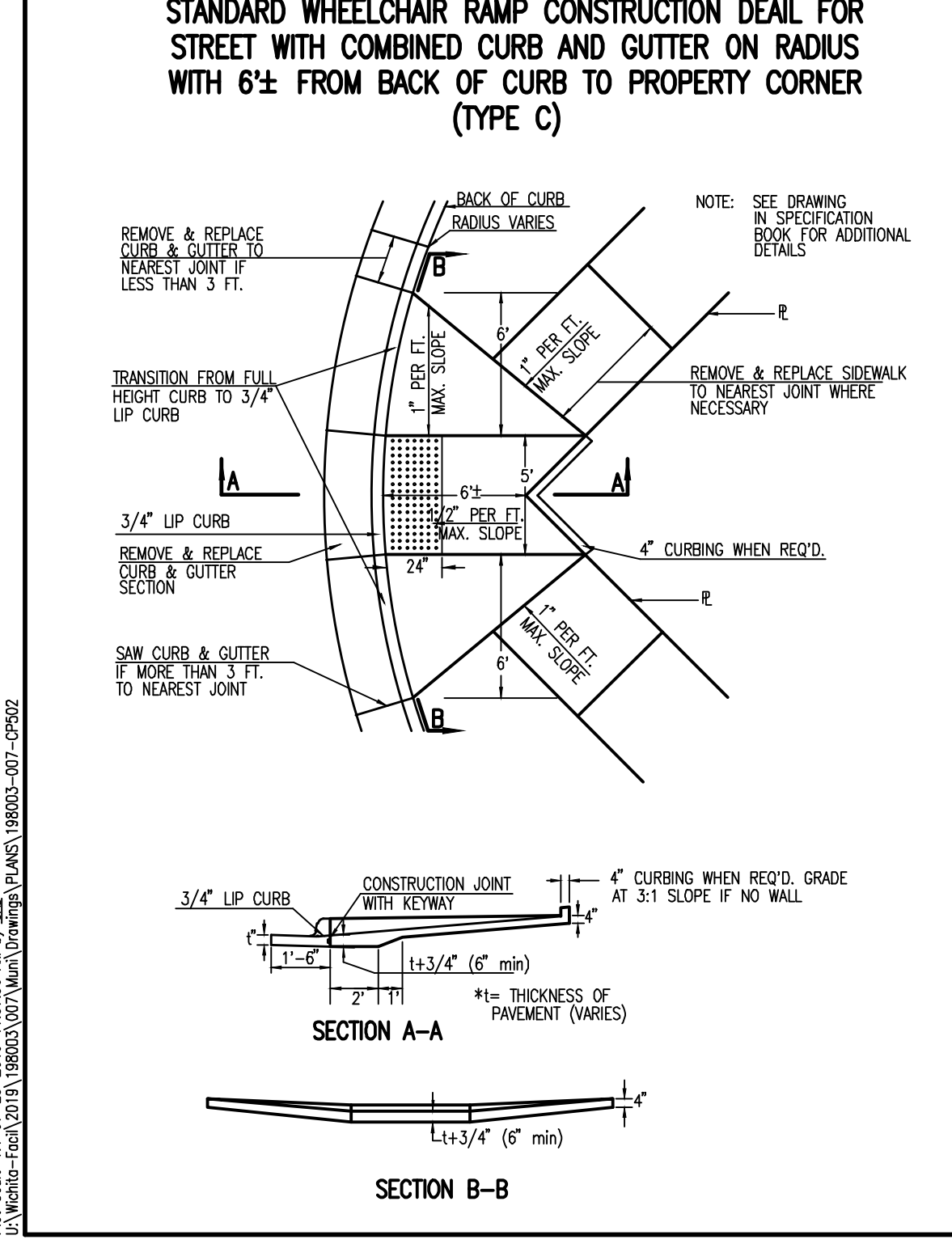
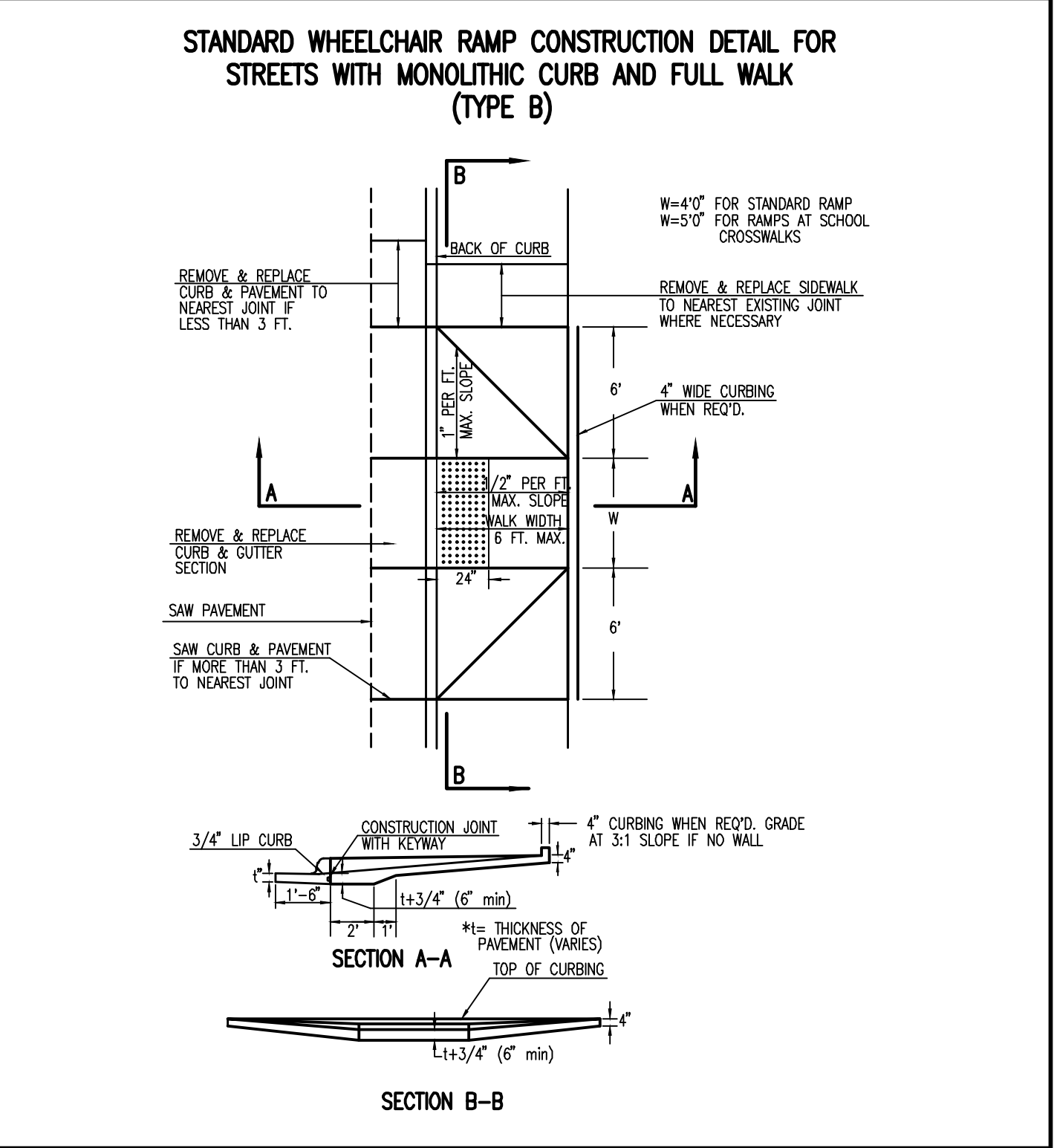
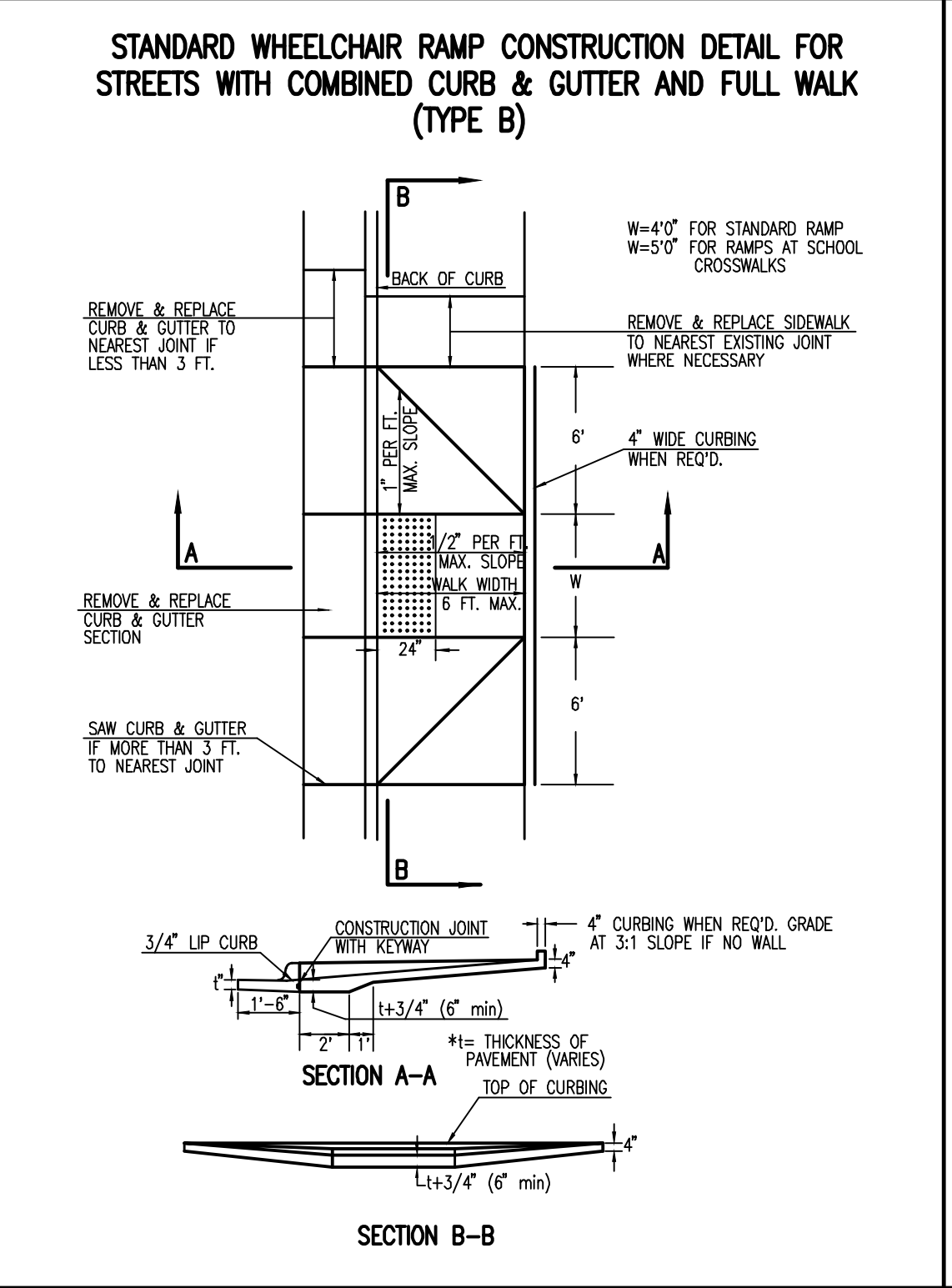
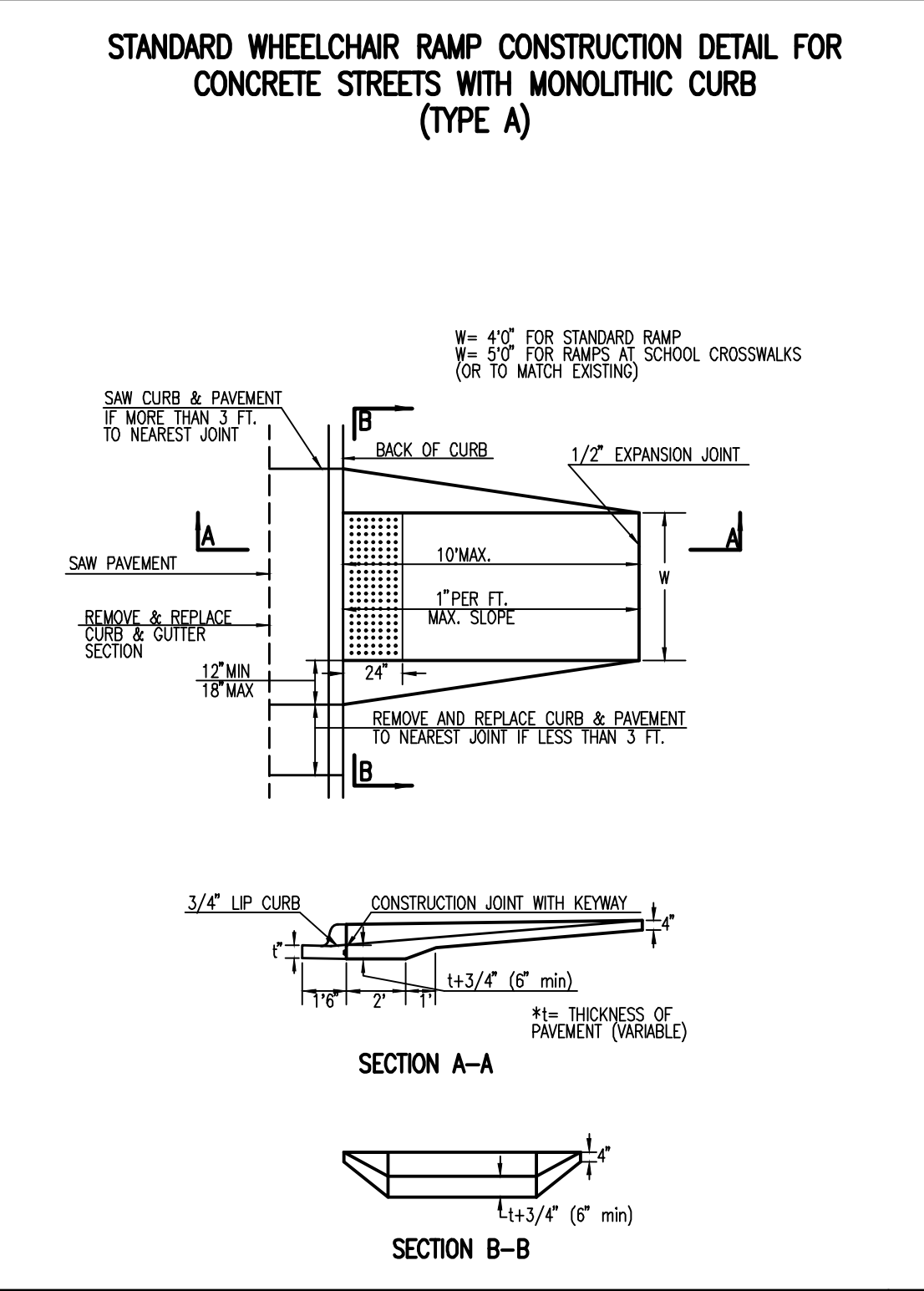
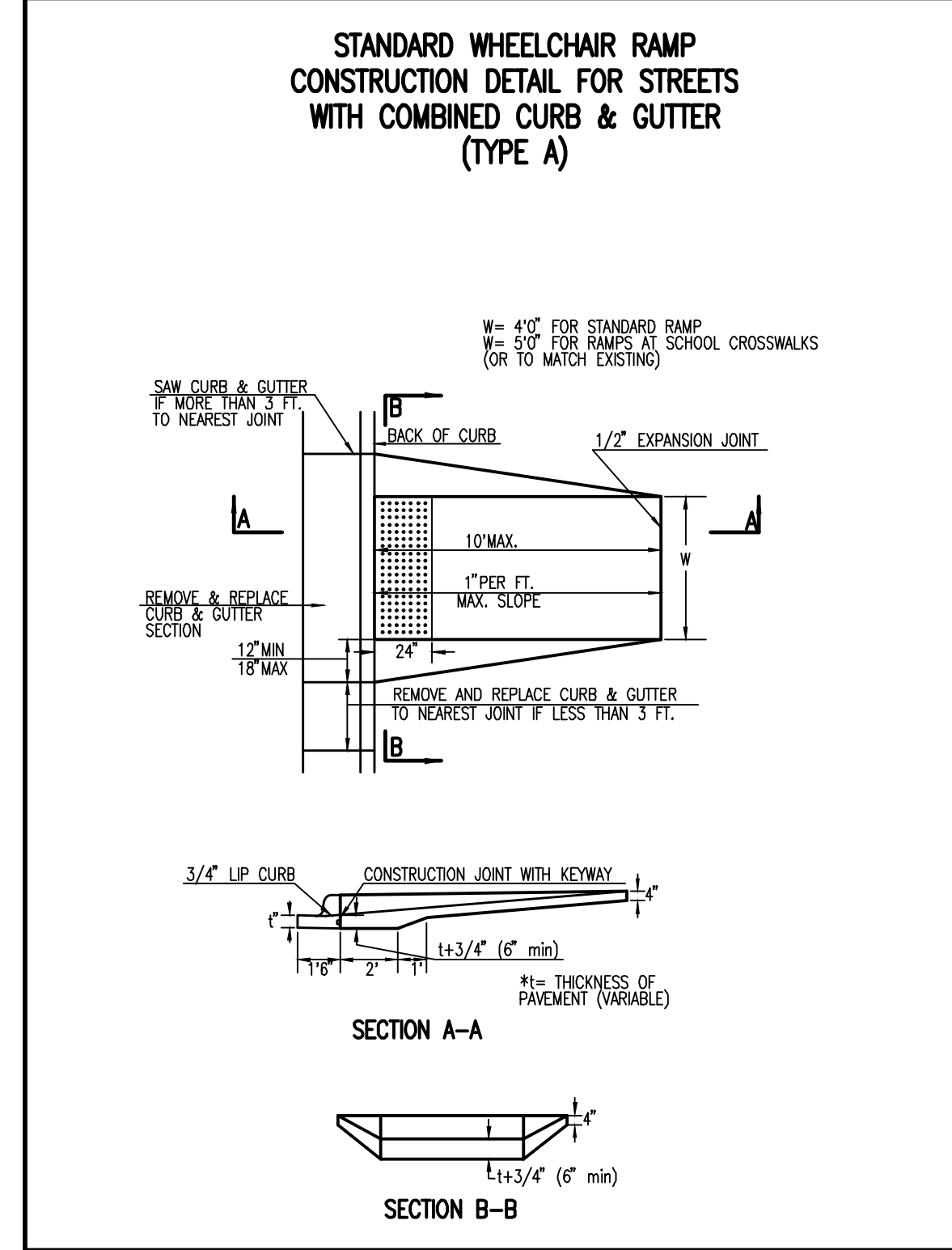
EXISTING CURB INLET MODIFICATION DETAILS

Date	Description
2 8/8/19	ADDENDUM 3

JOB NUMBER:	198003-007
DATE:	07/25/19
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CHECKED:	NLS

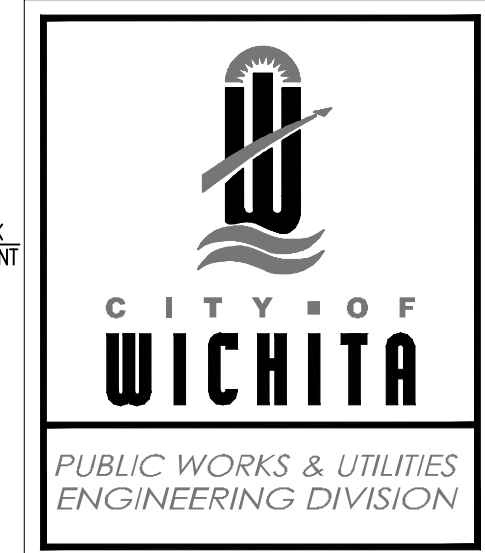
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Date	Description

TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS



WHEELCHAIR RAMP DETAILS WITH DETECTABLE WARNING

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER 291PPP	OCA NUMBER 132003	DATE 08/2013
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CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 425 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

SHEET
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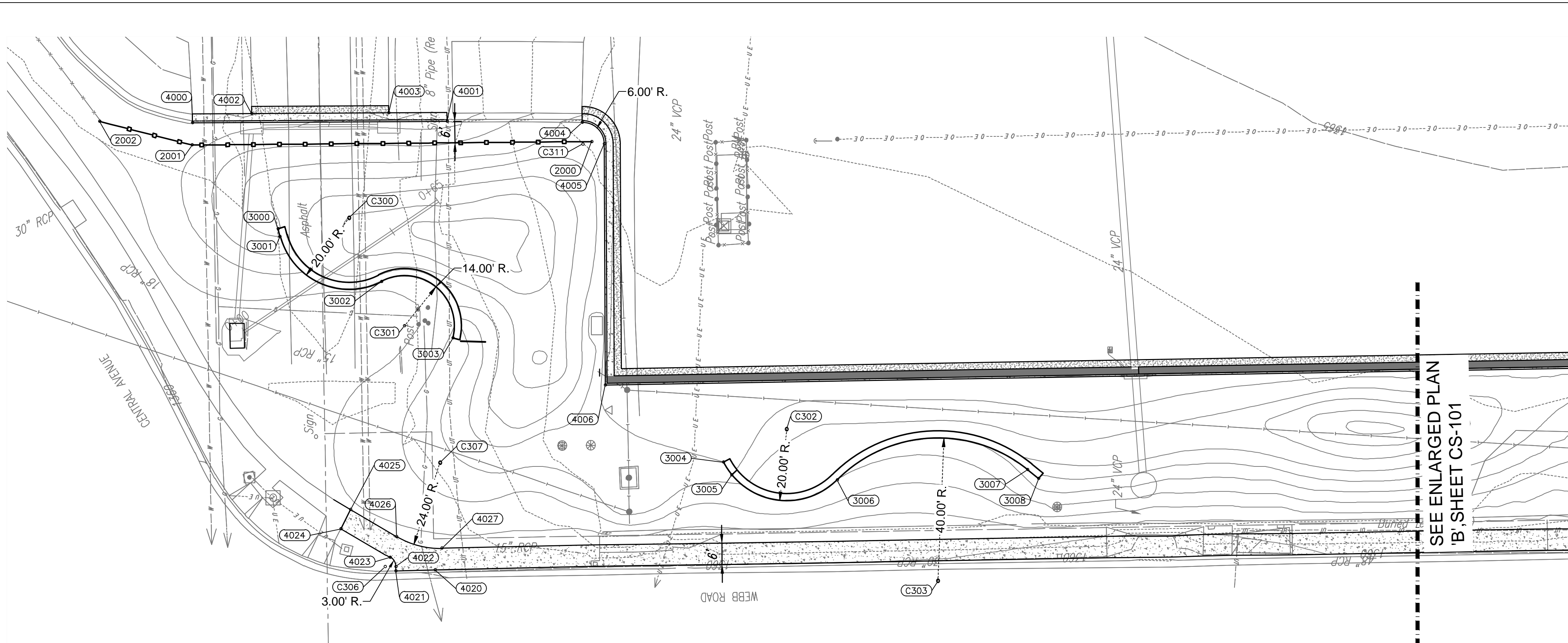
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BID DOCUMENTS
 WHEELCHAIR RAMP DETAILS

CP-502
 EAST CAMPUS - TKTAV
 CENTRAL AVE. & WEBB RD.

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ENLARGED PLAN "A"
Scale: 1" = 20'-0"

POINT	NORTHING	EASTING
C300	1,688,869.5097	1,681,021.8658
C301	1,688,853.9800	1,680,991.6196
C302	1,688,746.8933	1,680,962.6359
C303	1,688,704.5276	1,680,920.1488
C304	1,688,456.4220	1,680,969.8097
C305	1,688,416.9891	1,680,924.5875
C306	1,688,859.4181	1,680,924.0942
C307	1,688,844.0286	1,680,953.2860
C308	1,688,036.8375	1,680,949.9826
C309	1,688,074.9243	1,680,986.1221

POINT	NORTHING	EASTING
C310	1,688,039.3621	1,680,972.6158
C311	1,688,804.0087	1,681,042.6078

C300 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO THE CENTER OF RADIUS.

POINT	NORTHING	EASTING
4000	1,688,913.4015	1,681,048.5234
4001	1,688,842.0369	1,681,048.9455
4002	1,688,896.7546	1,681,051.1219
4003	1,688,858.3520	1,681,051.3490
4004	1,688,804.2120	1,681,048.6110
4005	1,688,798.0088	1,681,042.6338
4006	1,688,797.7160	1,680,975.0886
4007	1,688,040.3404	1,680,988.1013
4008	1,688,025.6946	1,680,979.9267
4009	1,688,023.8662	1,680,972.9746

POINT	NORTHING	EASTING
4010	1,688,027.4581	1,680,978.9834
4011	1,688,051.3762	1,680,944.2953
4012	1,688,051.3934	1,680,942.2951
4013	1,688,036.8314	1,680,942.4889
4014	1,688,029.3395	1,680,950.1562
4015	1,688,029.3395	1,680,956.1578
4016	1,688,023.4800	1,680,956.2935
4017	1,688,023.3411	1,680,950.2951
4018	1,688,027.4308	1,680,940.2994
4019	1,688,036.6292	1,680,936.4842

POINT	NORTHING	EASTING
4020	1,688,845.4076	1,680,923.2475
4021	1,688,856.3907	1,680,922.9849
4022	1,688,856.4190	1,680,924.1659
4023	1,688,858.0191	1,680,926.7480
4024	1,688,871.7646	1,680,934.7487
4025	1,688,869.0710	1,680,940.1240
4026	1,688,856.1391	1,680,932.5656
4027	1,688,843.4892	1,680,929.2921
5011	1,689,488.5524	1,680,955.1934
5012	1,689,499.3297	1,680,954.5680

POINT	NORTHING	EASTING
5013	1,689,519.9148	1,680,945.6465
5014	1,689,563.3500	1,680,952.7775
5015	1,689,566.9799	1,680,960.4330
5016	1,689,568.3264	1,680,968.8980
5017	1,689,611.6486	1,680,966.8809
5018	1,689,622.0971	1,680,972.1877
5020	1,689,659.5799	1,680,964.8549
5021	1,689,667.9663	1,680,969.6298
5022	1,689,673.4354	1,680,976.0814
5023	1,689,676.2508	1,680,982.7321

POINT	NORTHING	EASTING
5024	1,689,646.8492	1,680,988.2393
5025	1,689,666.8296	1,681,004.0739
5028	1,689,695.8737	1,681,018.8990
5029	1,689,233.7344	1,680,942.2743
5030	1,689,236.4767	1,680,945.1439
5031	1,689,239.3667	1,680,947.8815
5032	1,689,289.7287	1,680,949.2561
5033	1,689,320.7011	1,680,958.3224
5034	1,689,607.9784	1,681,043.2984
5035	1,689,719.9048	1,681,042.3227

POINT	NORTHING	EASTING
5036	1,689,745.4166	1,681,048.8295

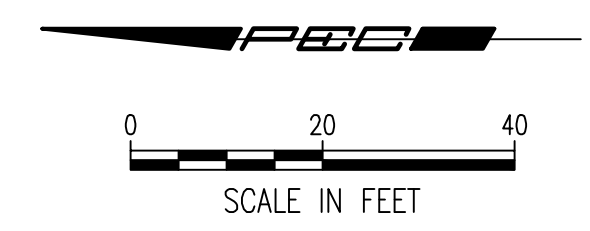
5000 = COORDINATE POINT NO.

POINT	NORTHING	EASTING
3000	1,688,889.4186	1,681,018.8990
3001	1,688,888.8317	1,681,016.7026
3002	1,688,860.3746	1,681,004.0739
3003	1,688,840.3942	1,680,988.2393
3004	1,688,764.6579	1,680,953.4479
3005	1,688,762.2770	1,680,949.8554
3006	1,688,732.7714	1,680,948.4735
3007	1,688,679.4357	1,680,951.3000
3008	1,688,676.3446	1,680,948.8101
3009	1,688,474.7607	1,680,961.8290
3010	1,688,441.8046	1,680,955.9546
3012	1,688,389.8582	1,680,953.9800
3013	1,688,386.9588	1,680,951.3062

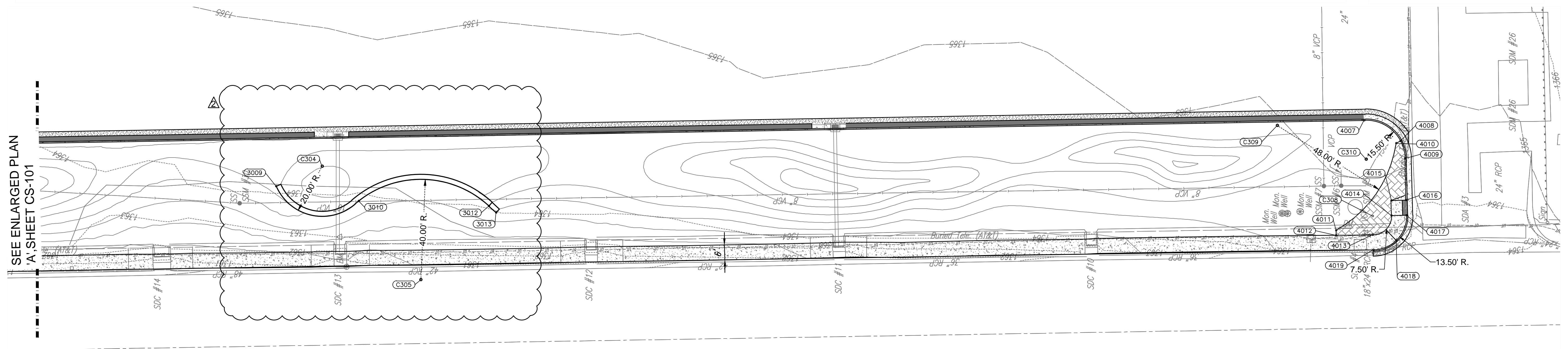
3000 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO FACE OF WALL.

POINT	NORTHING	EASTING
2000	1,688,801.5233	1,681,043.2984
2001	1,688,913.4497	1,681,042.3227
2002	1,688,939.3901	1,681,048.9389

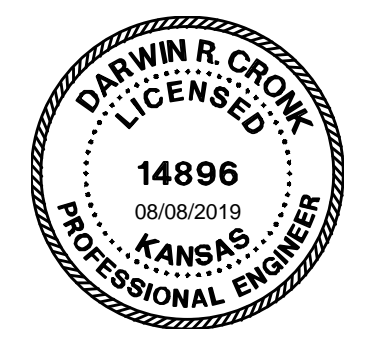
2000 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO CENTER OF POST.



MABCD REVIEWER:



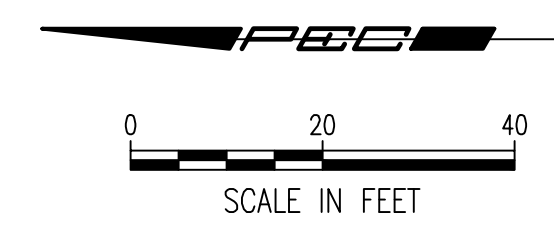
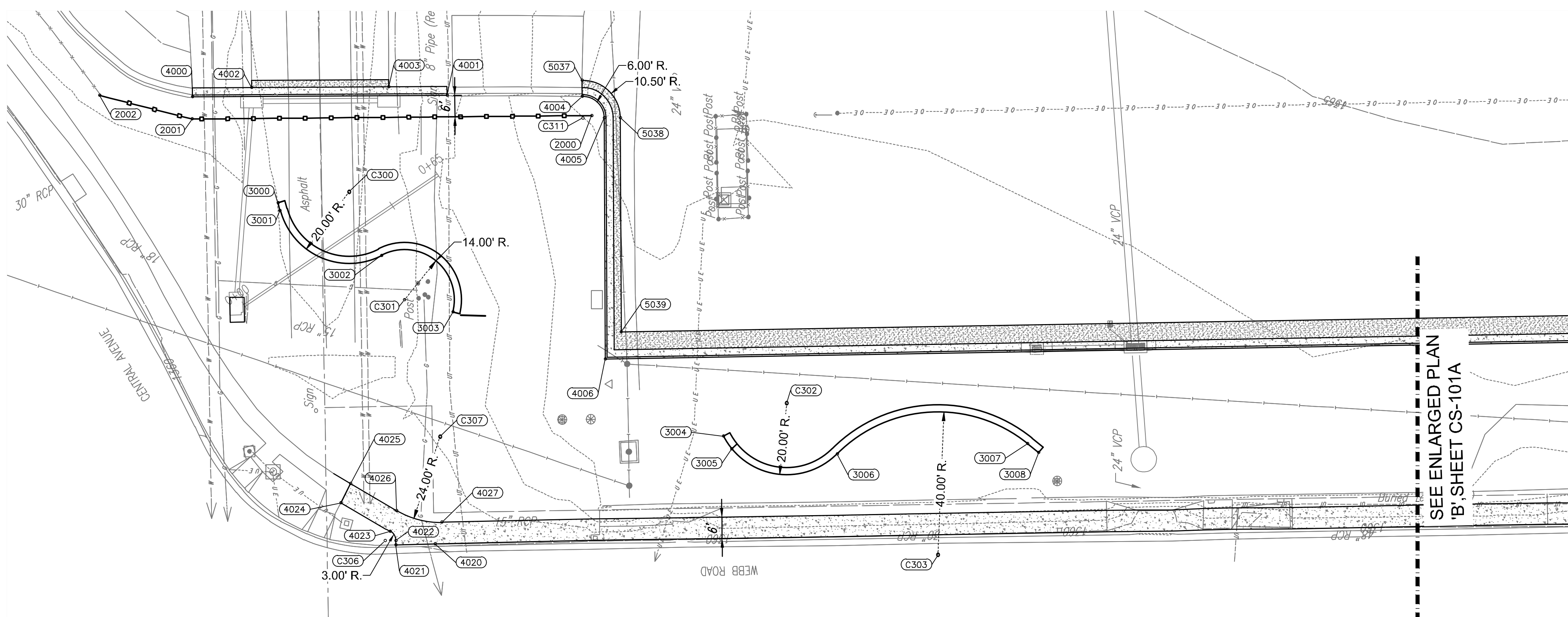
ENLARGED PLAN "B"
Scale: 1" = 20'-0"



Date	Description
8/8/19	ADDENDUM 3

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
GEOMETRY PLAN



MABCD REVIEWER:

ENLARGED PLAN "A"
Scale: 1" = 20'-0"

POINT	NORTHING	EASTING
C300	1,688,869.5097	1,681,021.8658
C301	1,688,853.9800	1,680,991.6196
C302	1,688,746.8933	1,680,962.6359
C303	1,688,704.5276	1,680,920.1488
C304	1,688,456.4220	1,680,969.8097
C305	1,688,416.9891	1,680,924.5875
C306	1,688,859.4181	1,680,924.0942
C307	1,688,844.0286	1,680,953.2860
C308	1,688,036.8375	1,680,949.9826
C309	1,688,074.9243	1,680,986.1221

POINT	NORTHING	EASTING
C310	1,688,039.3621	1,680,972.6158
C311	1,688,804.0087	1,681,042.6078

C300 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO THE CENTER OF RADIUS.

POINT	NORTHING	EASTING
4000	1,688,913.4015	1,681,048.5234
4001	1,688,842.0369	1,681,048.9455
4002	1,688,896.7546	1,681,051.1219
4003	1,688,858.3520	1,681,051.3490
4004	1,688,804.2120	1,681,048.6110
4005	1,688,798.0088	1,681,042.6338
4006	1,688,797.7160	1,680,975.0886
4007	1,688,040.3404	1,680,988.1013
4008	1,688,025.6946	1,680,979.9267
4009	1,688,023.8662	1,680,972.9746

POINT	NORTHING	EASTING
4010	1,688,027.4581	1,680,978.9834
4011	1,688,051.3762	1,680,944.2953
4012	1,688,051.3934	1,680,942.2951
4013	1,688,036.8314	1,680,942.4889
4014	1,688,029.3395	1,680,950.1562
4015	1,688,029.3395	1,680,956.1578
4016	1,688,023.4800	1,680,956.2935
4017	1,688,023.3411	1,680,950.2951
4018	1,688,027.4308	1,680,940.2994
4019	1,688,036.6292	1,680,936.4842

POINT	NORTHING	EASTING
4020	1,688,845.4076	1,680,923.2475
4021	1,688,856.3907	1,680,922.9849
4022	1,688,856.4190	1,680,924.1659
4023	1,688,858.0191	1,680,926.7480
4024	1,688,871.7646	1,680,934.7487
4025	1,688,869.0710	1,680,940.1240
4026	1,688,856.1391	1,680,932.5656
4027	1,688,843.4892	1,680,929.2921
5011	1,689,488.5524	1,680,955.1934
5012	1,689,499.3297	1,680,954.5680

POINT	NORTHING	EASTING
5013	1,689,519.9148	1,680,945.6465
5014	1,689,563.3500	1,680,952.7775
5015	1,689,566.9799	1,680,960.4330
5016	1,689,568.3264	1,680,968.8980
5017	1,689,611.6486	1,680,966.8809
5018	1,689,622.0971	1,680,972.1877
5020	1,689,659.5799	1,680,964.8549
5021	1,689,667.9663	1,680,969.6298
5022	1,689,673.4354	1,680,976.0814
5023	1,689,676.2508	1,680,982.7321

POINT	NORTHING	EASTING
5024	1,689,646.8492	1,680,988.2393
5025	1,689,666.8296	1,681,004.0739
5028	1,689,695.8737	1,681,018.8990
5029	1,689,233.7344	1,680,942.2743
5030	1,689,236.4767	1,680,945.1439
5031	1,689,239.3667	1,680,947.8815
5032	1,689,289.7287	1,680,949.2561
5033	1,689,320.7011	1,680,958.3224
5034	1,689,607.9784	1,681,043.2984
5035	1,689,719.9048	1,681,042.3227

POINT	NORTHING	EASTING
5036	1,689,745.4166	1,681,048.8295
5037	1,688,804.1534	1,681,053.1106
5038	1,688,793.5088	1,681,042.6534
5039	1,688,793.2873	1,680,982.6658
5040	1,688,039.7572	1,680,995.6124
5041	1,688,039.7057	1,680,992.6129
5042	1,688,022.3848	1,680,983.1881
5043	1,688,022.4359	1,680,978.7399

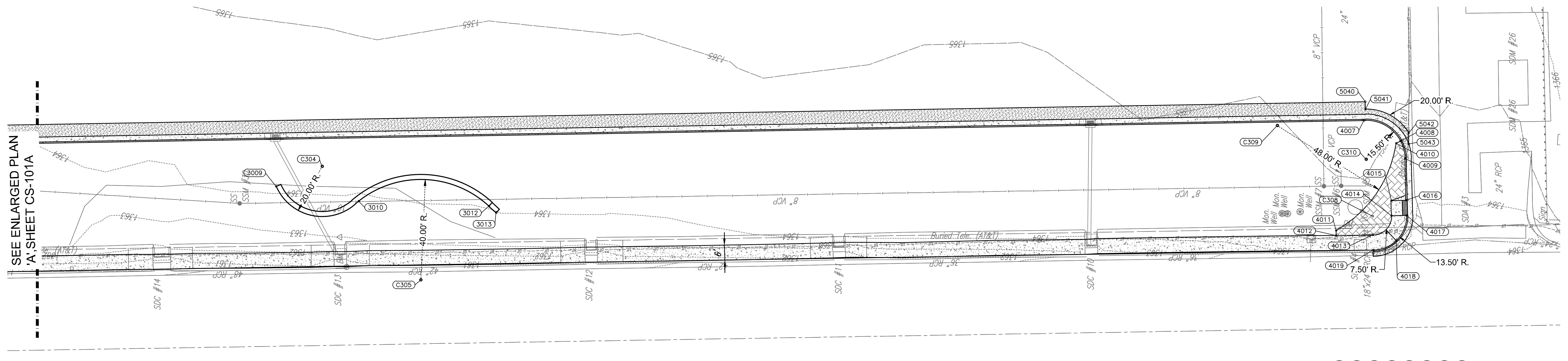
5000 = COORDINATE POINT NO.

WALL COORDINATE LIST		
POINT	NORTHING	EASTING
3000	1,688,889.4186	1,681,018.8990
3001	1,688,888.8317	1,681,016.7026
3002	1,688,860.3746	1,681,004.0739
3003	1,688,840.3942	1,680,988.2393
3004	1,688,764.6579	1,680,953.4479
3005	1,688,762.2770	1,680,949.8554
3006	1,688,732.7714	1,680,948.4735
3007	1,688,679.4357	1,680,951.3000
3008	1,688,676.3446	1,680,948.8101
3009	1,688,474.7607	1,680,961.8290
3010	1,688,441.8046	1,680,955.9546
3012	1,688,389.8582	1,680,953.9800
3013	1,688,386.9588	1,680,951.3062

3000 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO FACE OF WALL.

FENCE COORDINATE LIST		
POINT	NORTHING	EASTING
2000	1,688,801.5233	1,681,043.2984
2001	1,688,913.4497	1,681,042.3227
2002	1,688,939.3901	1,681,048.9389

2000 = COORDINATE POINT NO.
NOTE: COORDINATE POINTS ARE TO CENTER OF POST.



ENLARGED PLAN "B"
Scale: 1" = 20'-0"

ENTIRE SHEET

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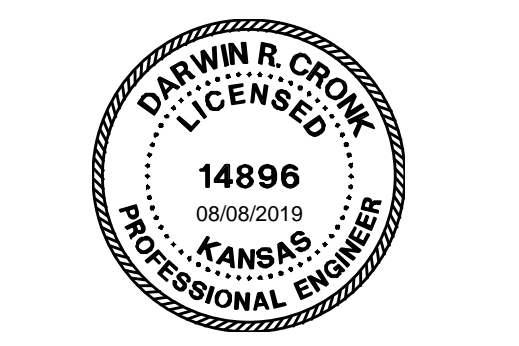
TEXTRON AVIATION
EAST CAMPUS - WEBB RD.
LANDSCAPING IMPROVEMENTS
(CENTRAL AVE TO TRUCK ENTRANCE)
WICHITA, KS

JOB NUMBER: 198003-007
DATE: 07/25/19
DRAWN: KMH
CHECKED: NLS

BID DOCUMENTS
GEOMETRY PLAN

CS-101A
EAST CAMPUS - TKTAV
CENTRAL AVE. & WEBB RD.

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Date	Description
2 8/8/19	ADDENDUM 3

MABCD REVIEWER:

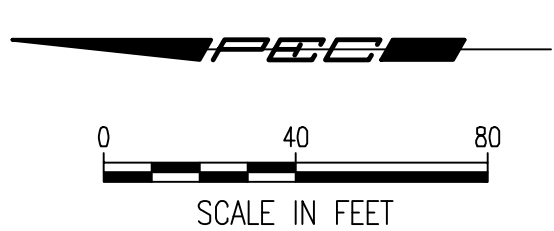


Date	Description
8/8/19	ADDENDUM 3

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
 DEMOLITION PLAN

- DEMOLITION NOTES:
- DEMOLITION LIMITS ARE APPROXIMATE. CONTRACTOR SHALL REMOVE CONCRETE PAVEMENT TO NEAREST EXISTING JOINT.
 - THE CONTRACTOR SHALL ANTICIPATE ADDITIONAL ITEMS TO BE DEMOLISHED WITHIN REMOVAL LIMITS. CONTRACTOR SHALL VISIT SITE TO VERIFY REQUIREMENTS. ALL DEMOLITION IS SUBSIDIARY TO THE PROJECT.
 - CONTRACTOR SHALL PROVIDE A FULL DEPTH SAW CUT NEAT AT ALL EXISTING CONCRETE AND ASPHALT WHERE IT ABUTS NEW PAVEMENT AND WHERE EXISTING PAVEMENT IS TO REMAIN.
 - DEMOLITION SHALL INCLUDE REMOVAL OF EXISTING PAVEMENT AND EXCAVATION OF EXISTING SUBGRADE MATERIAL TO A DEPTH SUFFICIENT FOR INSTALLATION OF THE PROPOSED PAVEMENT SECTION.
 - ERADICATE ALL EXISTING PARKING STRIPING WITHIN 20 FEET OF THE NEW CURB LINE.



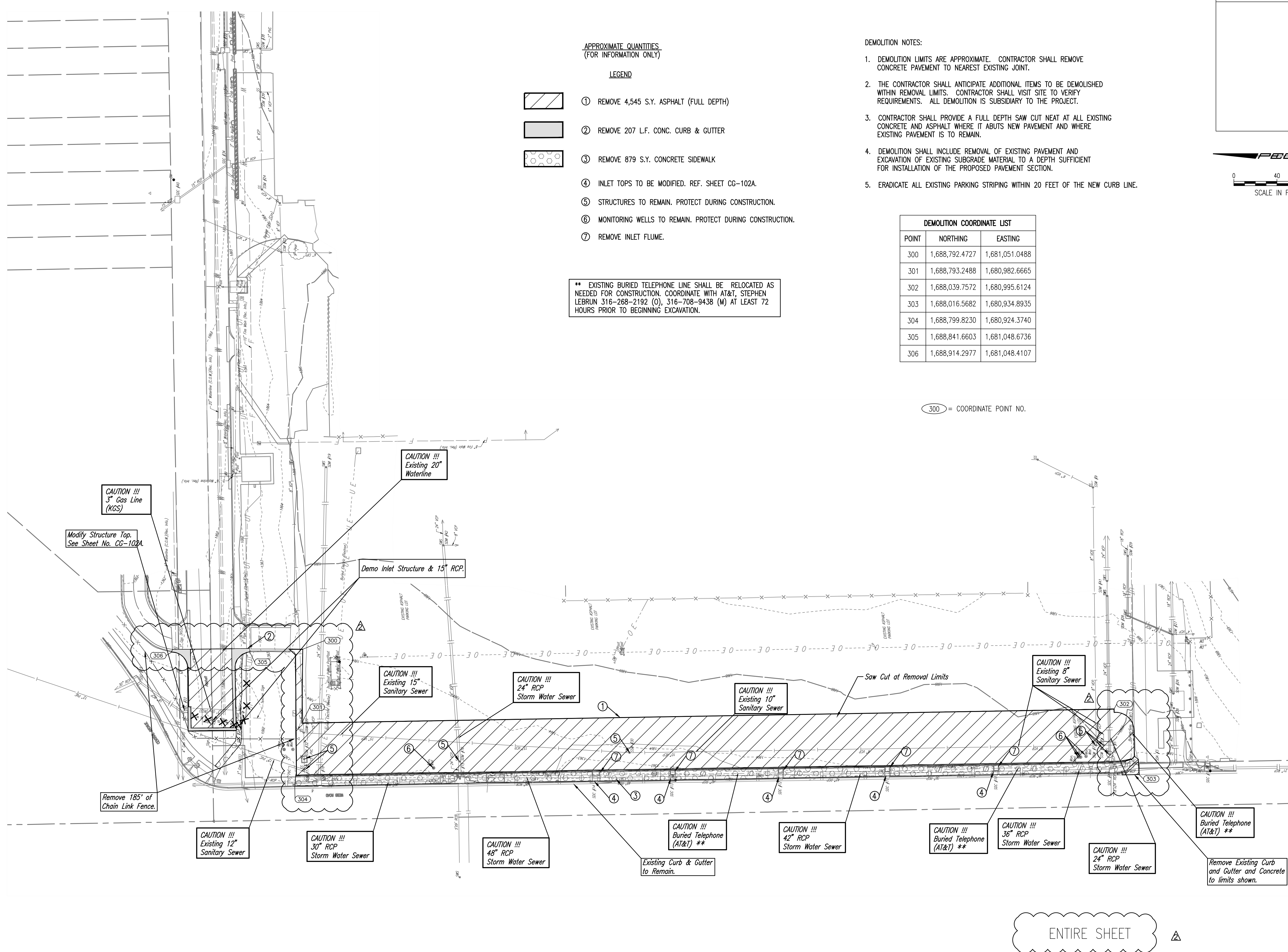
- APPROXIMATE QUANTITIES
 (FOR INFORMATION ONLY)
- LEGEND
- ① REMOVE 4,545 S.Y. ASPHALT (FULL DEPTH)
 - ② REMOVE 207 L.F. CONC. CURB & GUTTER
 - ③ REMOVE 879 S.Y. CONCRETE SIDEWALK
 - ④ INLET TOPS TO BE MODIFIED. REF. SHEET CG-102A.
 - ⑤ STRUCTURES TO REMAIN. PROTECT DURING CONSTRUCTION.
 - ⑥ MONITORING WELLS TO REMAIN. PROTECT DURING CONSTRUCTION.
 - ⑦ REMOVE INLET FLUME.

** EXISTING BURIED TELEPHONE LINE SHALL BE RELOCATED AS NEEDED FOR CONSTRUCTION. COORDINATE WITH AT&T, STEPHEN LEBRUN 316-268-2192 (O), 316-708-9438 (M) AT LEAST 72 HOURS PRIOR TO BEGINNING EXCAVATION.

DEMOLITION COORDINATE LIST

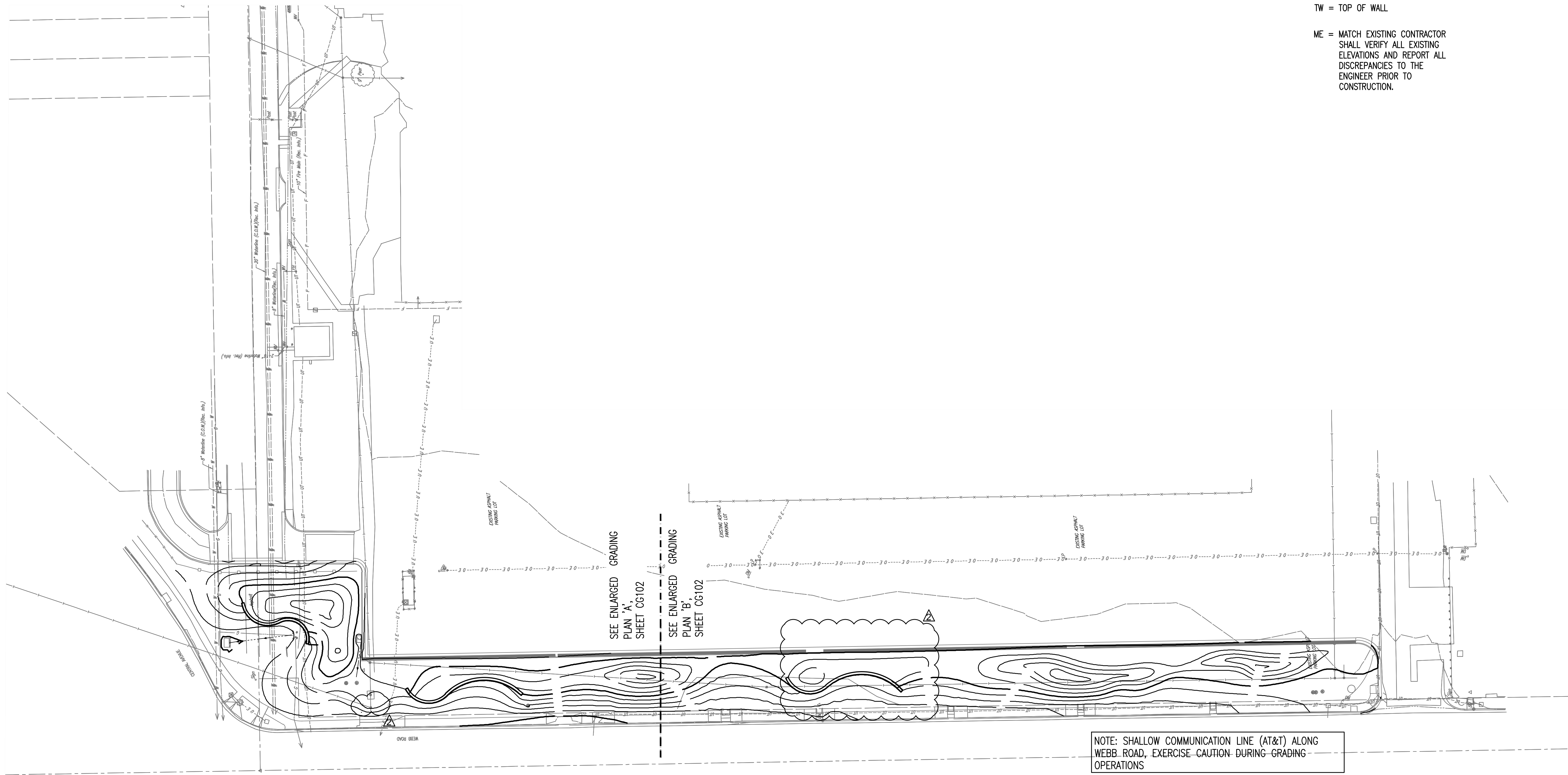
POINT	NORTHING	EASTING
300	1,688,792.4727	1,681,051.0488
301	1,688,793.2488	1,680,982.6665
302	1,688,039.7572	1,680,995.6124
303	1,688,016.5682	1,680,934.8935
304	1,688,799.8230	1,680,924.3740
305	1,688,841.6603	1,681,048.6736
306	1,688,914.2977	1,681,048.4107

③00 = COORDINATE POINT NO.



ENTIRE SHEET

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NOTE: SHALLOW COMMUNICATION LINE (AT&T) ALONG WEBB ROAD, EXERCISE CAUTION DURING GRADING OPERATIONS

OVERALL GRADING PLAN
 Scale: 1" = 40'-0"

LEGEND

- 53.60 PROPOSED PAVEMENT ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 50.3 PROPOSED GROUND ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 1350 — EXISTING MAJOR CONTOUR
- - - 1351 - - - EXISTING MINOR CONTOUR
- 1350 — PROPOSED MAJOR CONTOUR
- 1351 — PROPOSED MINOR CONTOUR
- · · · — BREAK LINE
- · · · — SWALE FLOW LINE
- ← FLOW DIRECTION
- HP = HIGH POINT
- TC = TOP OF CURB
- FL = FLOW LINE
- TW = TOP OF WALL
- ME = MATCH EXISTING CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS AND REPORT ALL DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

MABCD REVIEWER:

SCALE IN FEET

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 316-262-2691 www.pec1.com



Δ	Date	Description
2	8/8/19	ADDENDUM 3

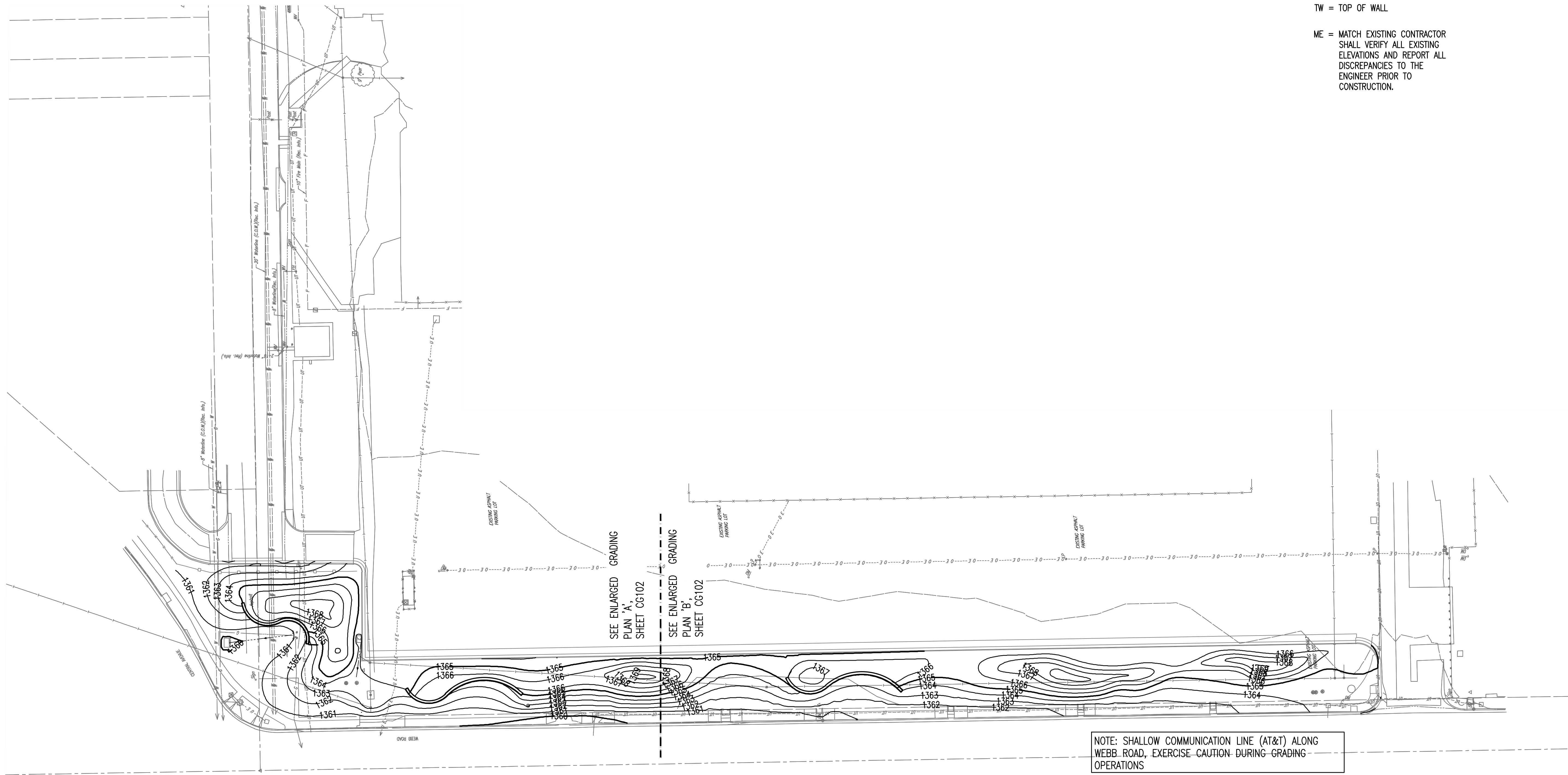
TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
 OVERALL GRADING PLAN

CG-101
 EAST CAMPUS - TXTAV
 CENTRAL AVE. & WEBB RD.

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NOTE: SHALLOW COMMUNICATION LINE (AT&T) ALONG WEBB ROAD, EXERCISE CAUTION DURING GRADING OPERATIONS

OVERALL GRADING PLAN
Scale: 1" = 40'-0"

ENTIRE SHEET

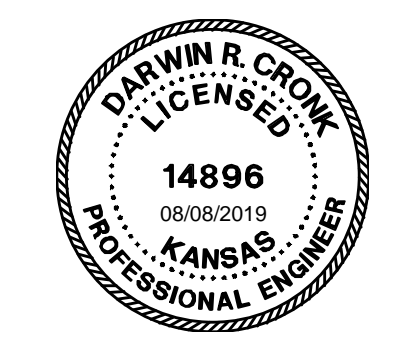
LEGEND

- 53.60 PROPOSED PAVEMENT ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 50.3 PROPOSED GROUND ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 1.350 — EXISTING MAJOR CONTOUR
- - - 1.351 - - - EXISTING MINOR CONTOUR
- 1.350 — PROPOSED MAJOR CONTOUR
- 1.351 — PROPOSED MINOR CONTOUR
- · · · — BREAK LINE
- · · · — SWALE FLOW LINE
- ← FLOW DIRECTION
- HP = HIGH POINT
- TC = TOP OF CURB
- FL = FLOW LINE
- TW = TOP OF WALL
- ME = MATCH EXISTING CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS AND REPORT ALL DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

MABCD REVIEWER:

SCALE IN FEET

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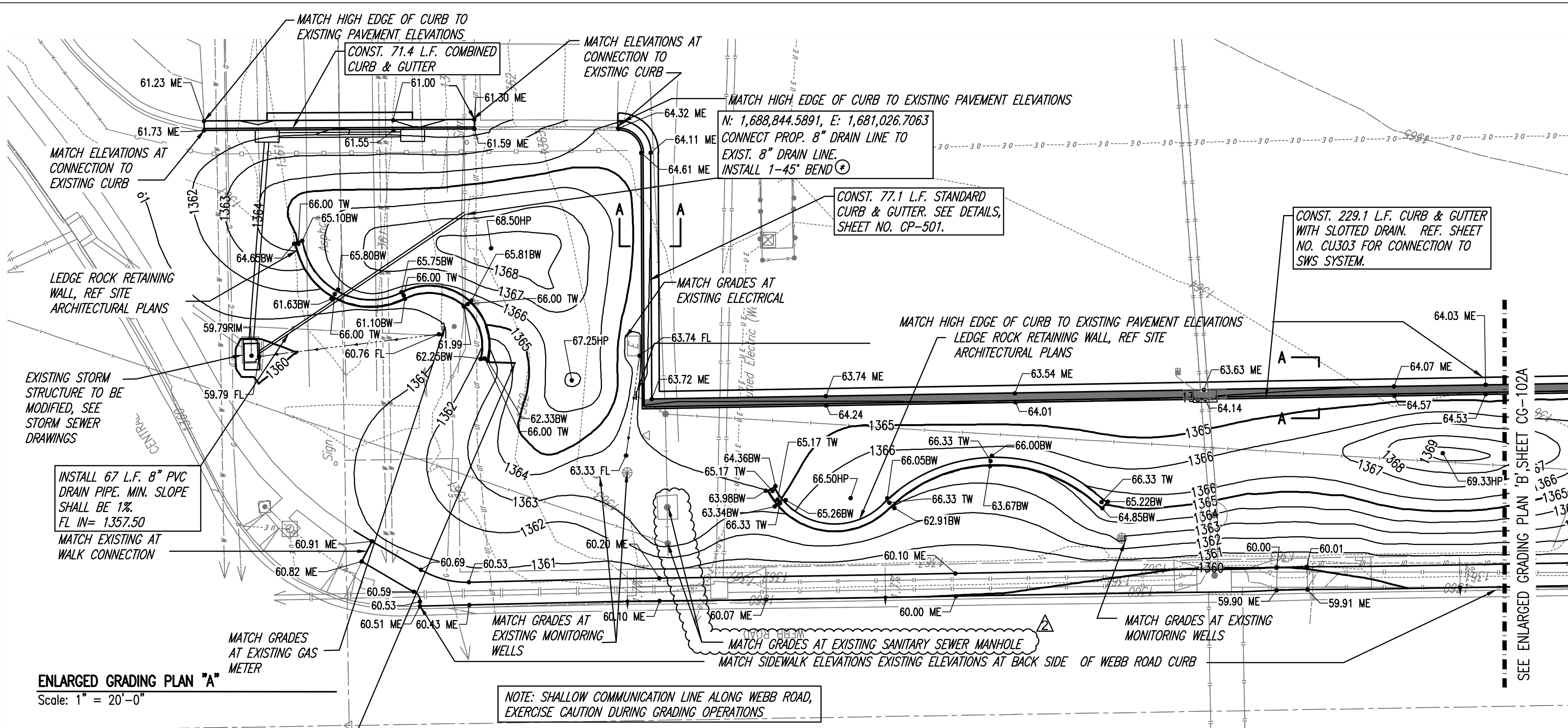
Δ	Date	Description
2	8/8/19	ADDENDUM 3

TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
 OVERALL GRADING PLAN

CG-101A
 EAST CAMPUS - TKTAV
 CENTRAL AVE. & WEBB RD.

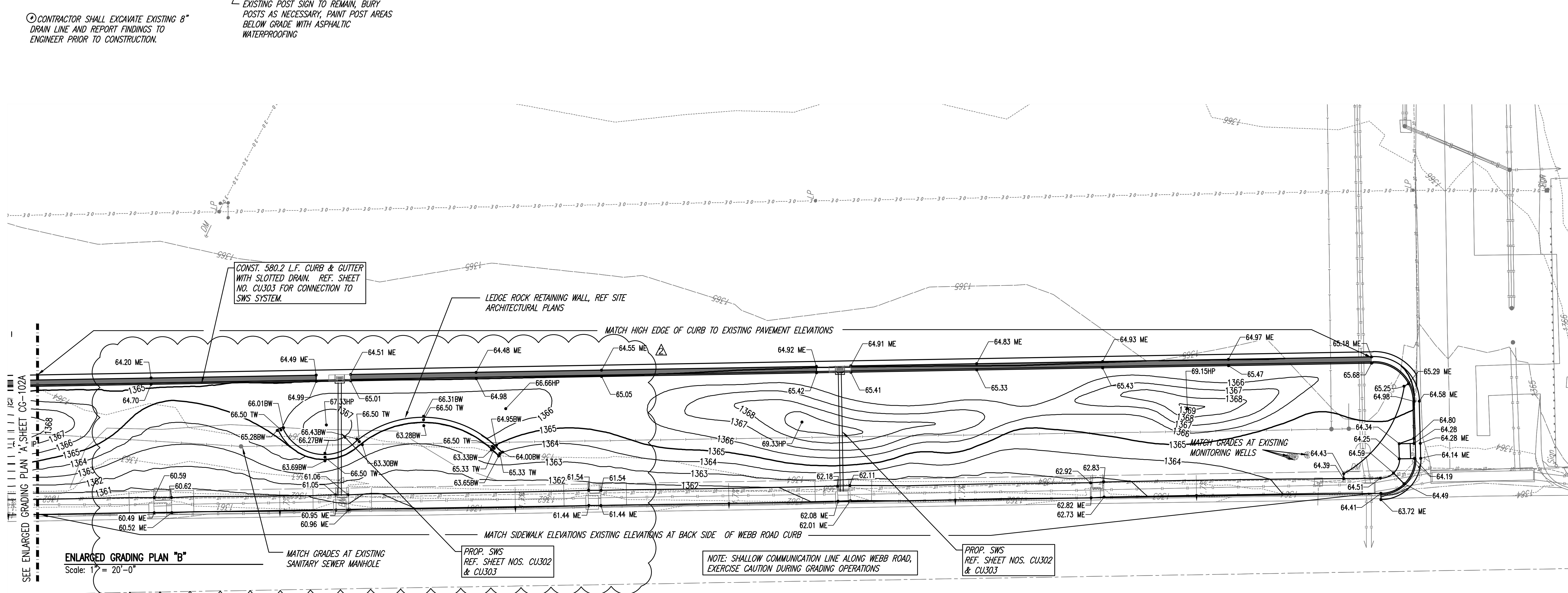


LEGEND

- 53.60 PROPOSED PAVEMENT ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 50.3 PROPOSED GROUND ELEVATION (+ 1300 = NAVD88 ELEVATION)
- 1350 EXISTING MAJOR CONTOUR
- 1351 EXISTING MINOR CONTOUR
- 1350 PROPOSED MAJOR CONTOUR
- 1351 PROPOSED MINOR CONTOUR
- BREAK LINE
- SWALE FLOW LINE
- FLOW DIRECTION
- SLOTTED DRAIN
- HP = HIGH POINT
- TC = TOP OF CURB
- FL = FLOW LINE
- TW = TOP OF WALL
- BW = BOTTOM OF WALL
- ME = MATCH EXISTING CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS AND REPORT ALL DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

SCALE IN FEET

SEE SH. CP-501 FOR SECTION A-A



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Date	Description
2 8/8/19	ADDENDUM 3

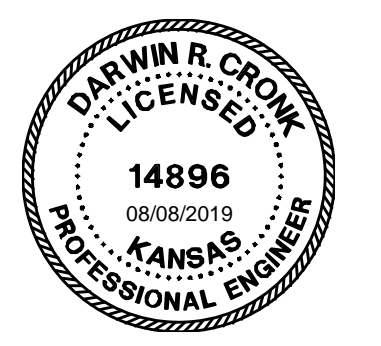
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 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
 ENLARGED GRADING PLANS

CG-102
 EAST CAMPUS - TXTAV
 CENTRAL AVE. & WEBB RD.

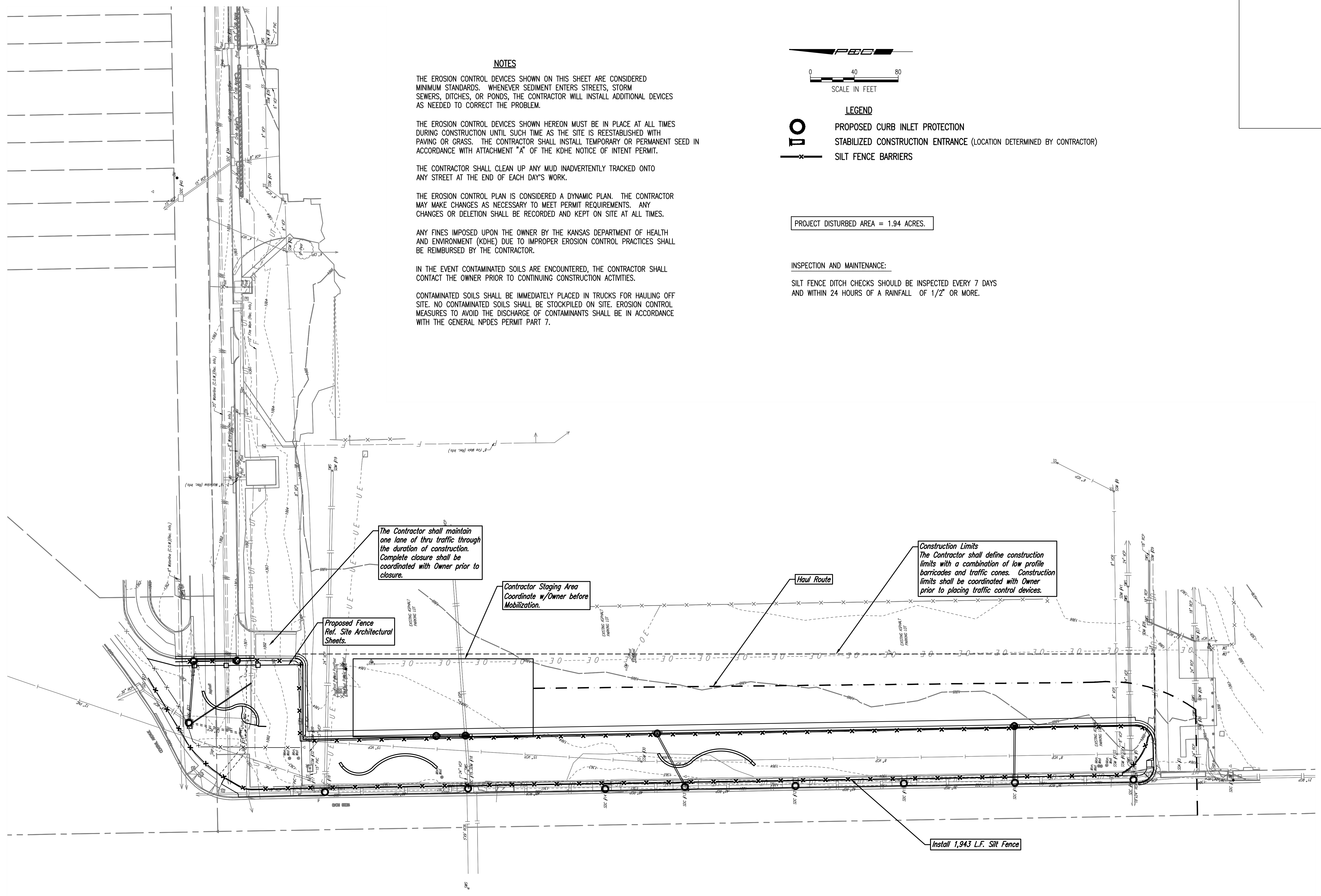
MABCD REVIEWER:



Δ	Date	Description
2	8/8/19	ADDENDUM 3

JOB NUMBER: 198003-007
 DATE: 07/25/19
 DRAWN: KMH
 CHECKED: NLS

BID DOCUMENTS
 EROSION CONTROL PLAN



NOTES

THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED MINIMUM STANDARDS. WHENEVER SEDIMENT ENTERS STREETS, STORM SEWERS, DITCHES, OR PONDS, THE CONTRACTOR WILL INSTALL ADDITIONAL DEVICES AS NEEDED TO CORRECT THE PROBLEM.

THE EROSION CONTROL DEVICES SHOWN HEREON MUST BE IN PLACE AT ALL TIMES DURING CONSTRUCTION UNTIL SUCH TIME AS THE SITE IS REESTABLISHED WITH PAVING OR GRASS. THE CONTRACTOR SHALL INSTALL TEMPORARY OR PERMANENT SEED IN ACCORDANCE WITH ATTACHMENT "A" OF THE KDHE NOTICE OF INTENT PERMIT.

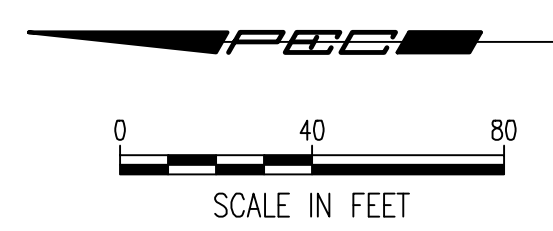
THE CONTRACTOR SHALL CLEAN UP ANY MUD INADVERTENTLY TRACKED ONTO ANY STREET AT THE END OF EACH DAY'S WORK.

THE EROSION CONTROL PLAN IS CONSIDERED A DYNAMIC PLAN. THE CONTRACTOR MAY MAKE CHANGES AS NECESSARY TO MEET PERMIT REQUIREMENTS. ANY CHANGES OR DELETION SHALL BE RECORDED AND KEPT ON SITE AT ALL TIMES.

ANY FINES IMPOSED UPON THE OWNER BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT (KDHE) DUE TO IMPROPER EROSION CONTROL PRACTICES SHALL BE REIMBURSED BY THE CONTRACTOR.

IN THE EVENT CONTAMINATED SOILS ARE ENCOUNTERED, THE CONTRACTOR SHALL CONTACT THE OWNER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES.

CONTAMINATED SOILS SHALL BE IMMEDIATELY PLACED IN TRUCKS FOR HAULING OFF SITE. NO CONTAMINATED SOILS SHALL BE STOCKPILED ON SITE. EROSION CONTROL MEASURES TO AVOID THE DISCHARGE OF CONTAMINANTS SHALL BE IN ACCORDANCE WITH THE GENERAL NPDES PERMIT PART 7.



- LEGEND**
- PROPOSED CURB INLET PROTECTION
 - STABILIZED CONSTRUCTION ENTRANCE (LOCATION DETERMINED BY CONTRACTOR)
 - SILT FENCE BARRIERS

PROJECT DISTURBED AREA = 1.94 ACRES.

INSPECTION AND MAINTENANCE:

SILT FENCE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE.

The Contractor shall maintain one lane of thru traffic through the duration of construction. Complete closure shall be coordinated with Owner prior to closure.

Contractor Staging Area Coordinate w/Owner before Mobilization.

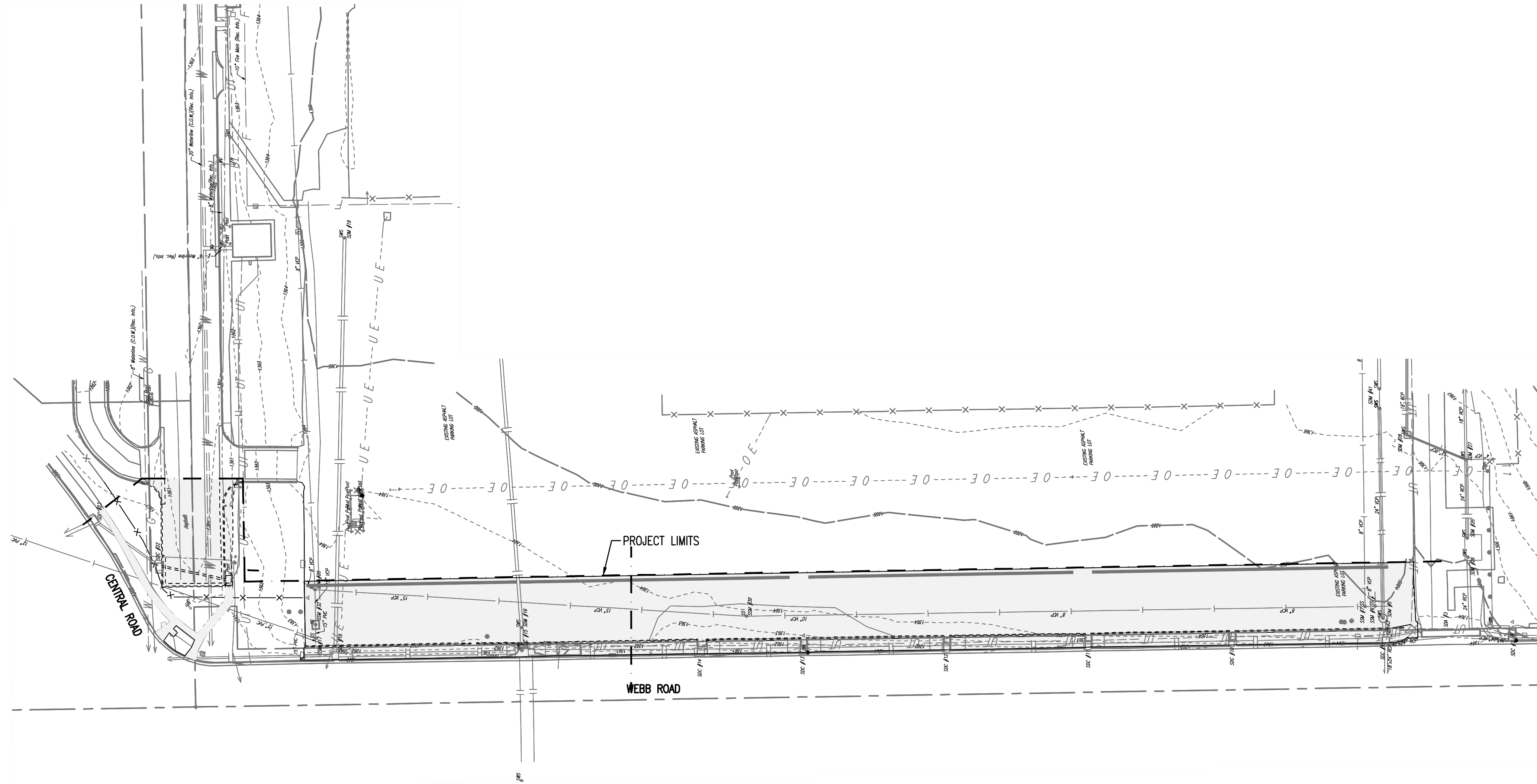
Haul Route

Construction Limits
 The Contractor shall define construction limits with a combination of low profile barricades and traffic cones. Construction limits shall be coordinated with Owner prior to placing traffic control devices.

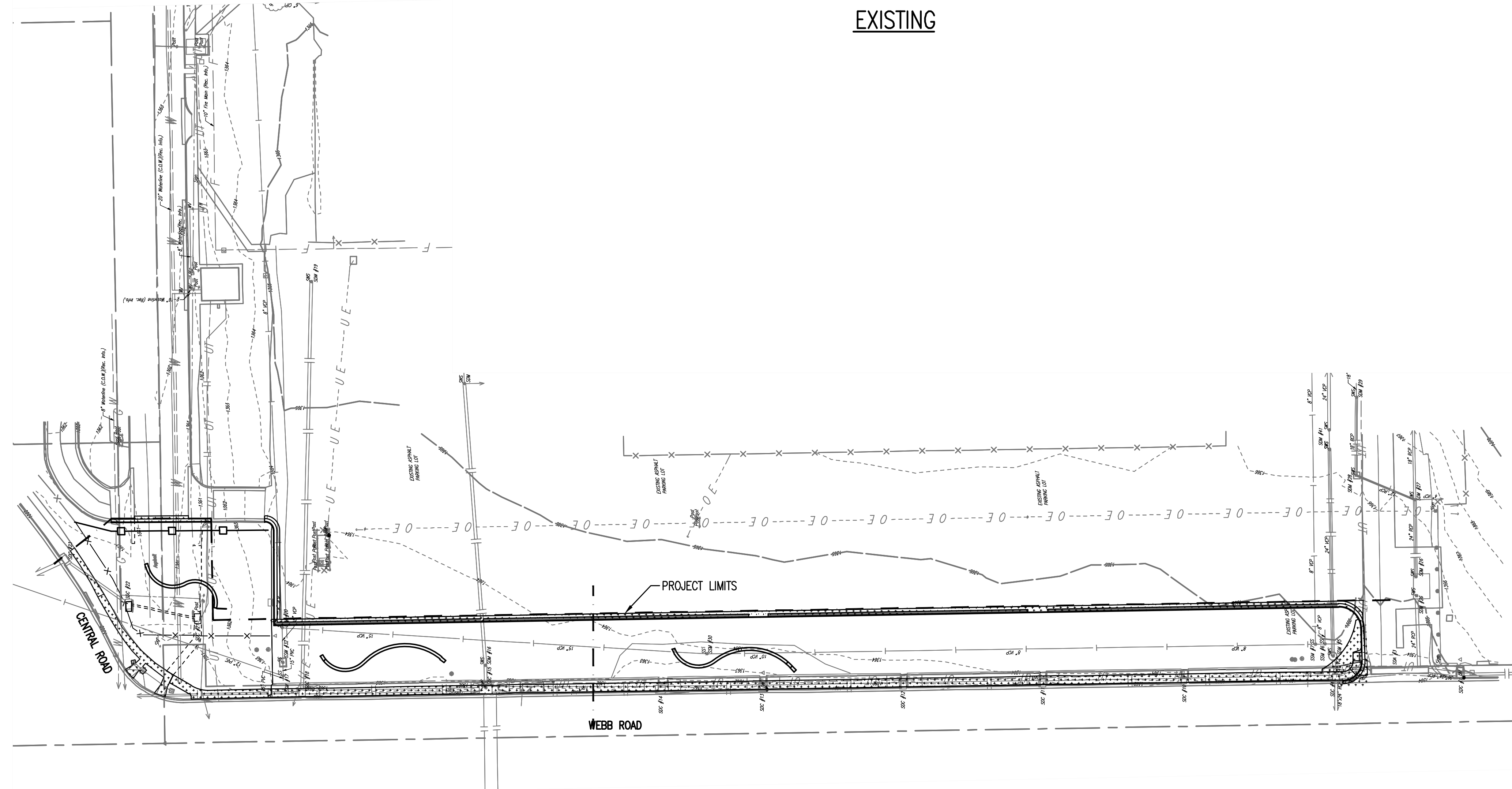
Install 1,943 L.F. Silt Fence

Save: 08-08-2019 8:26:37 AM by: S4D
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 U:\Wichita-Tech\2019\198003\007\Main\Drawings\PLANS\198003-007-C0501A

ENTIRE SHEET

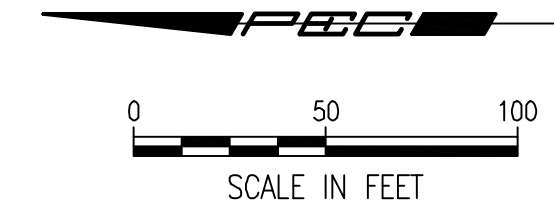


EXISTING



PROPOSED

MABCD REVIEWER:



LEGEND

- EXISTING IMPERVIOUS AREA
- PROPOSED IMPERVIOUS AREA

EXISTING IMPERVIOUS AREA = 48,072 S.F. (1.10 AC)
 PROPOSED IMPERVIOUS AREA = 8,724 S.F. (0.20 AC)



Date	Description

TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS

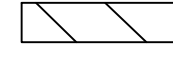




ERU PLAN

CG-502
 EAST CAMPUS - TXTAV
 CENTRAL AVE. & WEBB RD.

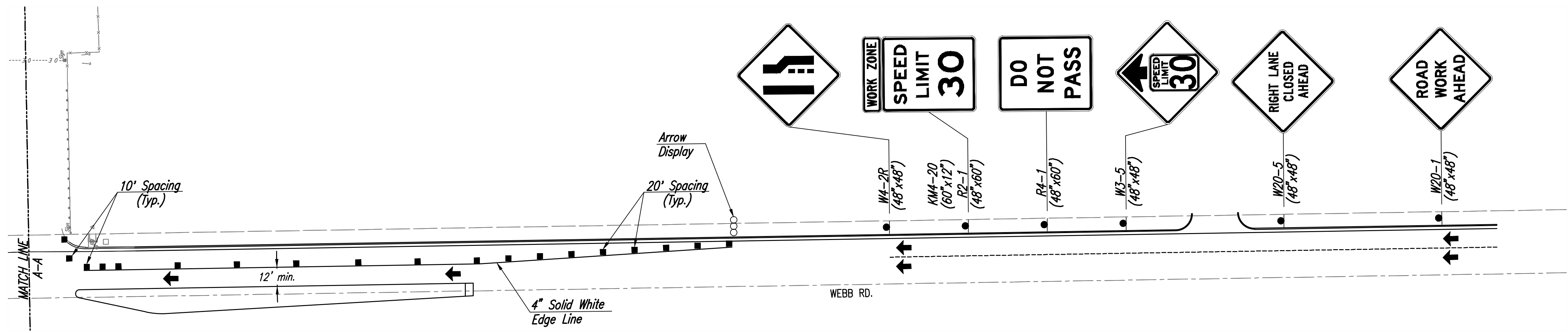
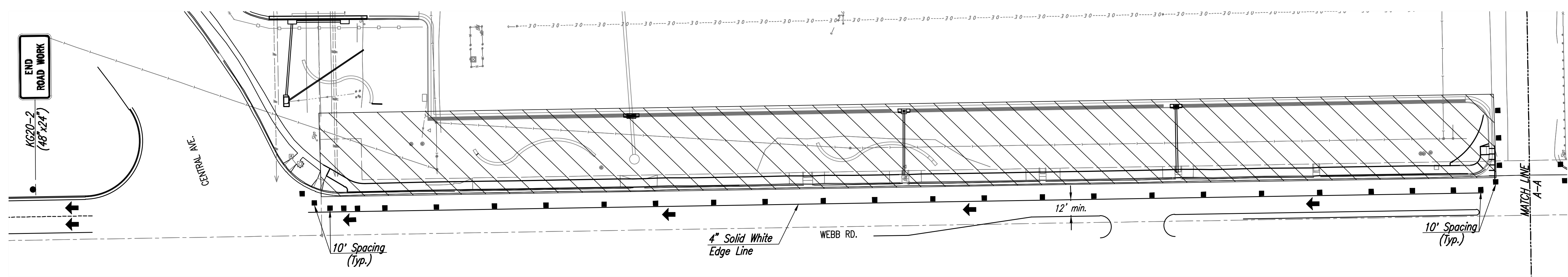
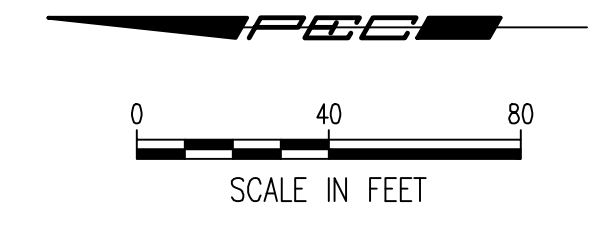
PEC
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 303 SOUTH TOPEKA WICHITA, KS 67202
 316-262-2691 www.pec1.com

Note: Utilities are not shown on Traffic Control Plans. The Contractor is responsible for verification as to type and location of underground utilities as may be necessary to avoid damage thereto. The Contractor is responsible for repairing any damage to underground utilities.

Moving temporary concrete safety barrier, channelizing devices, and signs, and additional traffic control may be necessary to construct side roads and entrances. Unless closure is permitted, maintain all weather access at all times during construction. This shall be considered Subsidiary.

- LEGEND**
-  WORK AREA
 -  CHANNELIZING DEVICES AT 40' SPACING UNLESS OTHERWISE NOTED
 -  TRAFFIC CONTROL SIGN
 -  TYPE III BARRICADE WITH TYPE A LOW INTENSITY FLASHING WARNING LIGHTS
 -  DIRECTION OF TRAFFIC

MABCD REVIEWER:



PEC
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 303 SOUTH TOPEKA WICHITA, KS 67202
 316-262-2691 www.pec1.com



Date	Description

TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS
 TRAFFIC CONTROL PLAN

CT-101
 EAST CAMPUS - TXTAV
 CENTRAL AVE. & WEBB RD.

Save: 07-23-2019 1:58:47 PM by: SLD
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Date	Description
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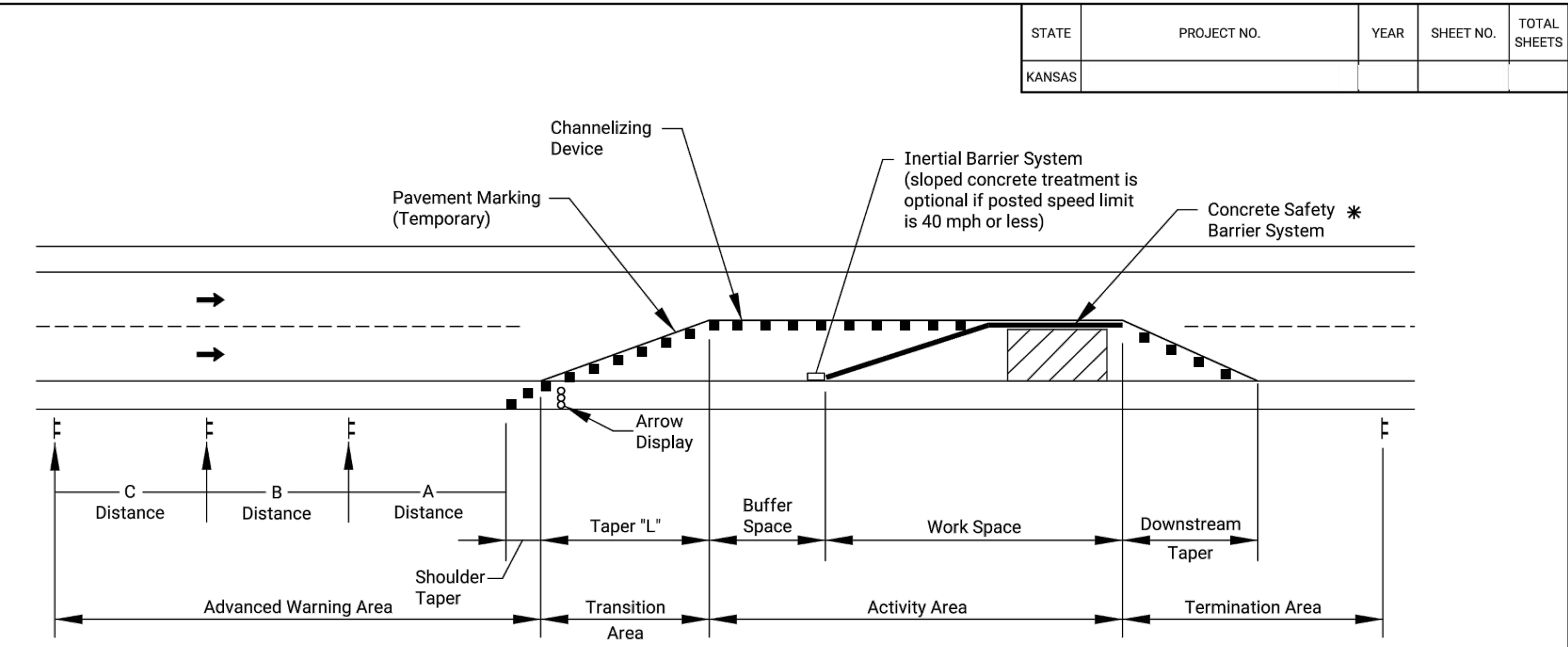
JOB NUMBER:	198003-007
DATE:	07/25/19
DRAWN:	KMH
CHECKED:	NLS

BID DOCUMENTS

TRAFFIC CONTROL
 STANDARD TE700

CT-501
 EAST CAMPUS - TTXAV
 CENTRAL AVE. & WEBB RD.

- 1) Design Speed: Those items delegated to temporary traffic control should be designed and installed using the posted/legal speed of the roadway prior to work starting.
- 2) Minimum Lane Width: Lane widths shall be a minimum of 11' (measured between centerlines of pavement markings) or as shown on the plans, or as directed by the engineer. A lane width less than 11' may require restricted roadway width signing.
- 3) Consideration should be made to separate pedestrian and, if needed, bicycle movements from both work site activity and vehicular traffic. Unless a reasonable safe route that does not involve crossing the roadway can be provided, pedestrians should be appropriately directed with advance signing that encourages them to cross to the opposite side of the roadway. In urban and suburban areas with high vehicular traffic volumes, these signs should be placed at intersections (rather than midblock locations) so that pedestrians are not confronted with midblock work sites that will induce them to attempt skirting the work site or making a midblock crossing.
- 4) When existing pedestrian facilities are disrupted, closed, or relocated, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility.
- 5) When the driving surface open to traffic is milled or is a temporary surface made of loose material, or when directed by the engineer a W8-15 (Grooved Pavement) or W8-7 (Loose Gravel) sign shall be used on mainline approaches. This sign should be placed a "C" distance after the W20-1 (Road Work Ahead) sign. A W8-15p motorcycle plaque shall be used to supplement the W8-15 or W8-7 signs. All signs shall be displayed as long as the condition is present.
- 6) Alternative temporary rumble strip options may be available. Please contact the Temporary Traffic Control Unit for more information at 785-296-1179 or 785-296-1183.



TYPICAL WORK ZONE COMPONENTS

* When concrete barrier system is used, portable channelizing devices are not needed along the tangent barrier section.

Minimum advance warning sign spacing (in feet):

SPEED (MPH) *	A	B	C
URBAN (40 MPH OR LOWER)	100	100	100
URBAN (45 MPH OR HIGHER)	350	350	350
RURAL (55 MPH OR LOWER)	500	500	500
RURAL (60 MPH OR HIGHER)	750	750	750
EXPRESSWAY/FREEWAY	1000	1500	2640

Taper Formulas:
 $L = WS$ for speeds of 45 MPH or more
 $L = WS^2/60$ for speeds of 40 MPH or less
 Where: L = Minimum length of taper in feet
 S = Numerical value of posted speed prior to work starting in MPH
 W = Width in offset feet

Shifting Taper=1/2 L
 Shoulder Taper=1/3 L

* Posted speed prior to work starting
 The minimum spacing between signs shall be no less than 100', unless directed by the engineer.
 The spacing between any signs may be increased beyond the minimum values in the table above as approved by the engineer in order to maximize visibility.

- Channelizer Placement:
- (1) The spacing between devices in transition area (taper) should not exceed a distance in feet equal to 1/2 the posted speed limit in mph prior to work starting.
 - (2) The spacing between devices in the advanced warning area and the activity area should not exceed a distance in feet equal to two times the posted speed limit in mph prior to work starting.
 - (3) Channelizing devices shall be placed for optimum visibility, normally at right angles to the traffic flow.
 - (4) Place directional indicator barricades in series to direct traffic onto the new path. The arrow sign should not be visible to opposing traffic.
 - (5) Alternating diagonal orange and white striping must slope downward in the direction traffic is expected to pass.

Buffer Space

SPEED (MPH) *	20	25	30	35	40	45	50	55	60	65	70	75
LENGTH (ft)	115	155	200	250	305	360	425	495	570	645	730	820

* Posted speed prior to work starting
 Neither work activity nor storage of equipment, vehicles, or material should occur in the buffer space. When a protection vehicle is placed in advance of the work space, only the space upstream of the vehicle constitutes the buffer space.
 If temporary concrete safety barrier system is used to separate approaching traffic from the work space, the barrier system shall be considered part of the activity area. A full lane width should be available throughout the length of the buffer space. See typical work zone components above.

NO.	DATE	REVISIONS	BY	APP'D
3				
2	03/13/18	W8-15p usage changed to Shall		R.W.B. E.G.K.
1	08/18/15	Channelizer spacing info		R.W.B. K.E.

KANSAS DEPARTMENT OF TRANSPORTATION

**TRAFFIC CONTROL
 GENERAL NOTES**

TE700

DESIGNED	B.A.H.	DATE	03/13/18	APP'D	Eric Koehler
DESIGN CK.		DETAIL CK.		QUANTITIES	TRACED
				QUAN. CK.	TRACE CK.

KDOT Graphics Certified 03-13-2018

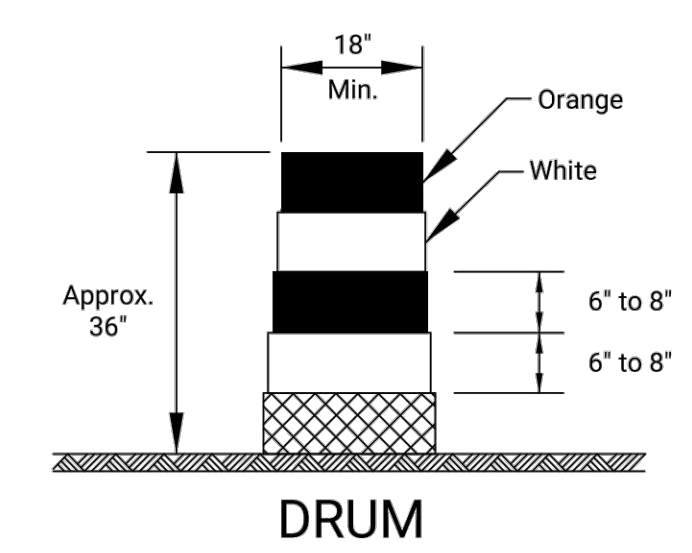
KDOT Graphics Certified

MABCD REVIEWER:

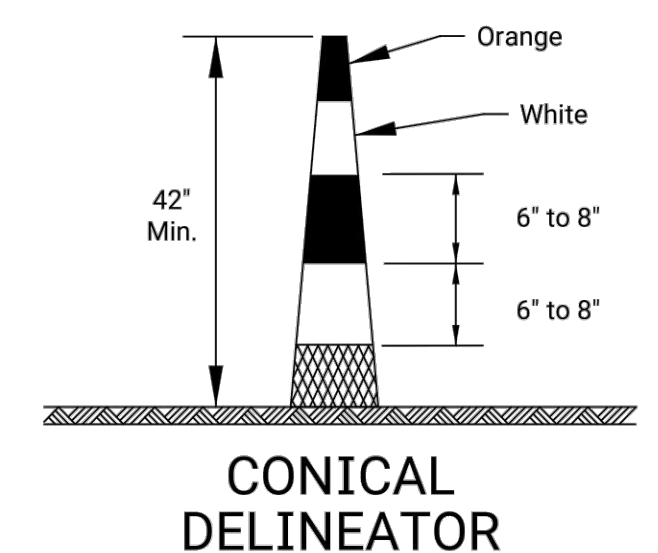


Date	Description
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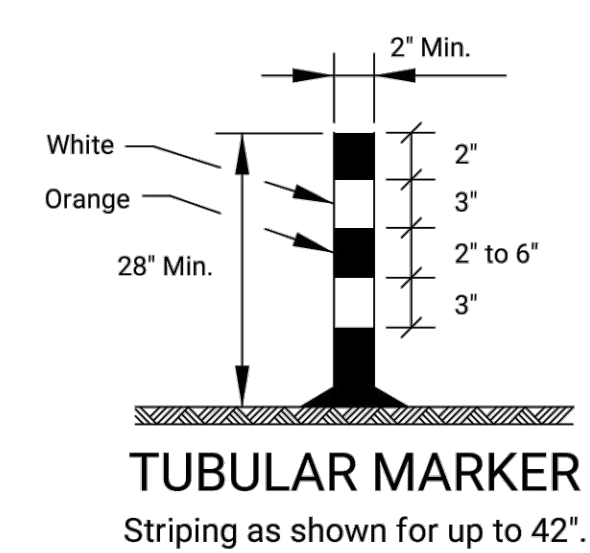
STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS				



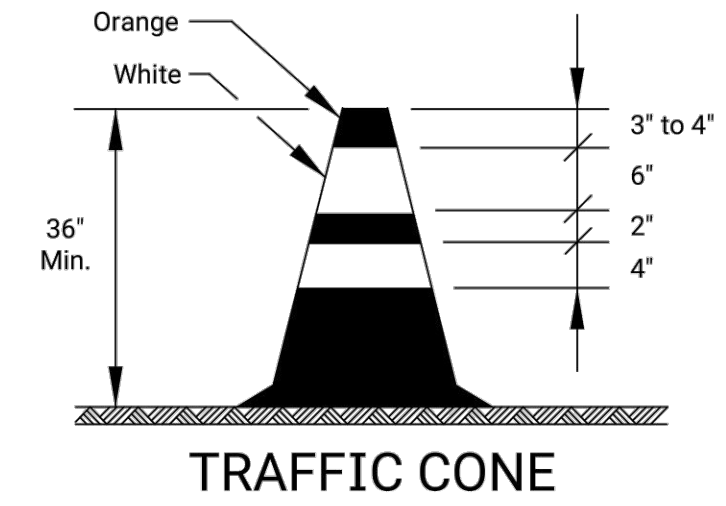
DRUM



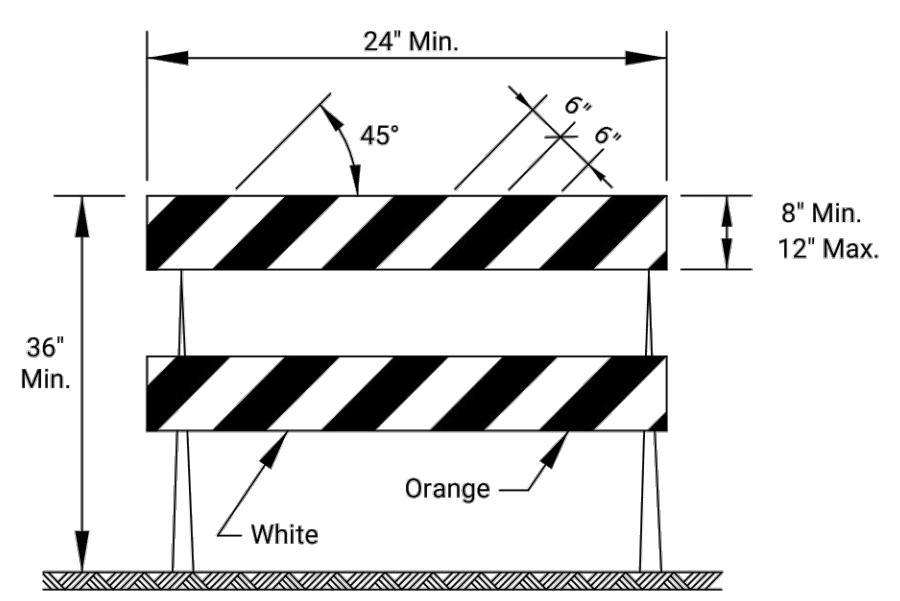
CONICAL DELINEATOR



TUBULAR MARKER
 Striping as shown for up to 42".

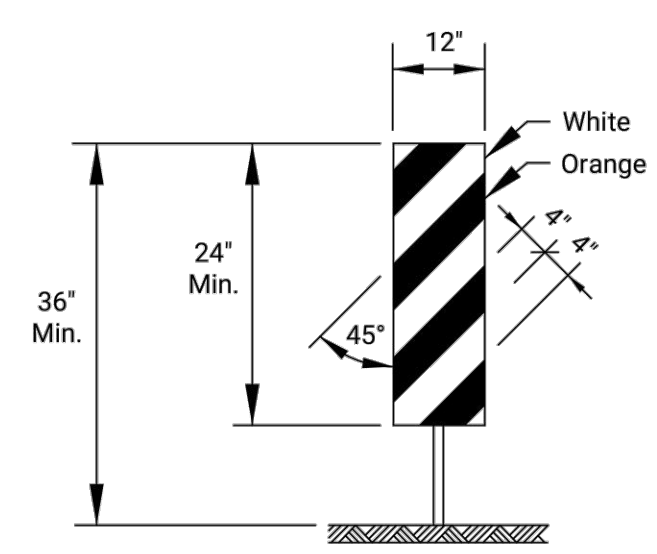


TRAFFIC CONE



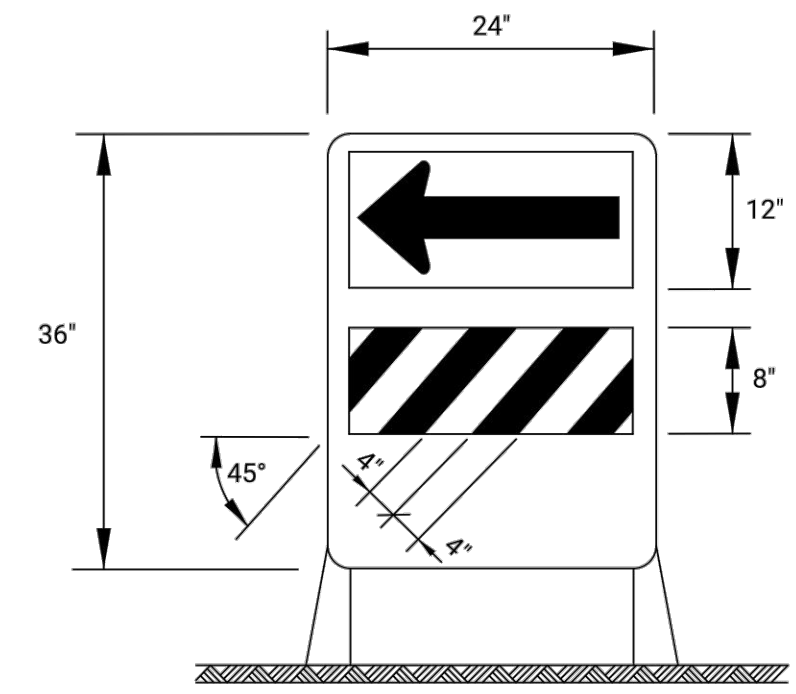
TYPE 2 BARRICADE

For rails less than 36" long, 4" wide stripes may be used. All stripes shall slope downward to the traffic side for channelization.



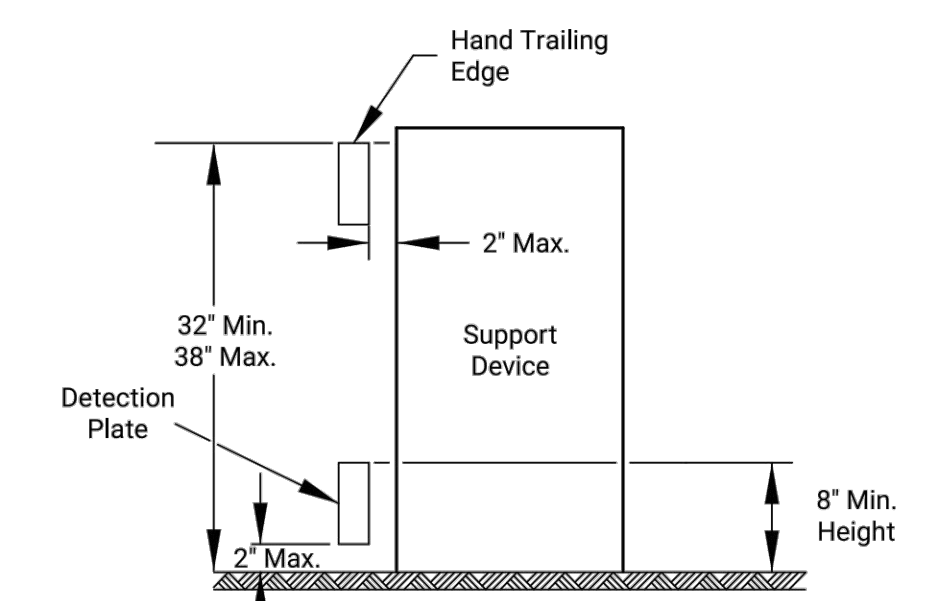
VERTICAL PANEL

The stripes shall slope downward to the traffic side for channelization.



DIRECTION INDICATOR BARRICADE

The stripes shall slope downward in the direction traffic is to pass. The direction indicator barricade shall be used in series to direct the motorist into the intended lane of travel.



PEDESTRIAN CHANNELIZER

- Support device shall not project beyond the detection plate into the pathway.
- Hand trailing edges and detection plates are optional for continuous walls.
- Interconnect pedestrian channelizers to prevent displacement and to provide continuous guidance through or around work.
- Alternate pathways shall be firm, stable, and slip resistant.
- Treat height differentials > 1/2" in the surfaces of alternate paths with a firm, stable, and slip resistant temporary ramp having a slope of 12:1 or flatter and having a width equal to the alternate path.
- Use alternating orange/white on interconnected devices.

Item	Location	Location									
		Cross-overs	Shoofly Divisions	Tangents	Tapers	Ramps	Head to Head	Object Identifier	Lead-in Devices	Gores	
Portable	Drums	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Conical Delineators	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes	
	Vertical Panels	(2)	(2)	(2)	(2)	(2)	(1,2)	Yes	(2)	(2)	
	Direction Indicator Barricade	No	No	No	Yes	No	No	No	No	No	
	Type 2 Barricade	(2)	(2)	(2)	(2)	No	No	Yes	No	No	
	Traffic Cones	No	No	(4)	(4)	(4)	No	(4)	(4)	(4)	
Fixed	Tubular Markers	(3)	(3)	(3)	No	(3)	Yes	No	Yes	Yes	
	Vertical Panels	(3)	(3)	(3)	(3)	(3)	(3)	Yes	(2,3)	(2)	

- Not allowed on centerline delineation along freeways or expressways.
- The stripes shall slope downward to the traffic side for channelization.
- May be used upon the approval of the engineer.
- Daytime operations only.

NO.	DATE	REVISIONS	BY	APP'D
3				
2				
1				

KANSAS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL CHANNELIZING DEVICES

TE702

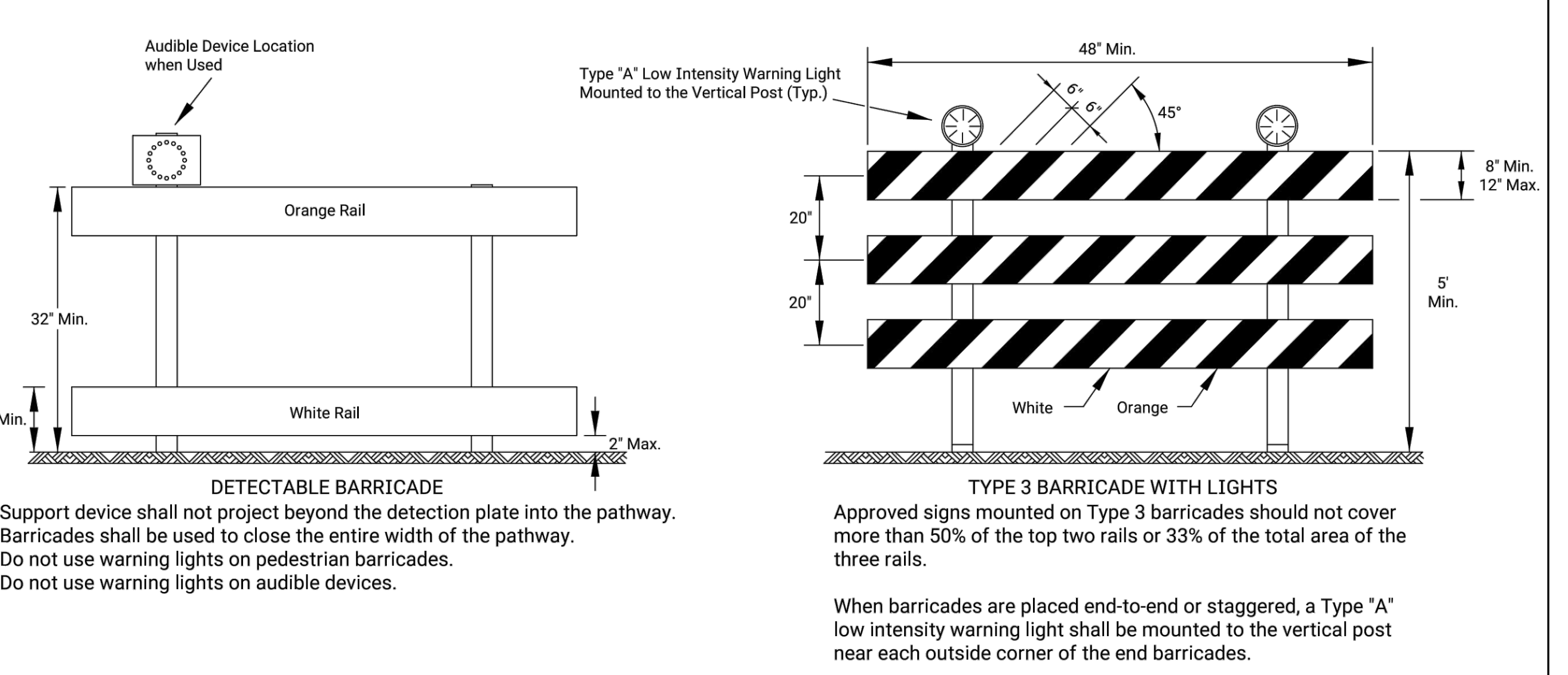
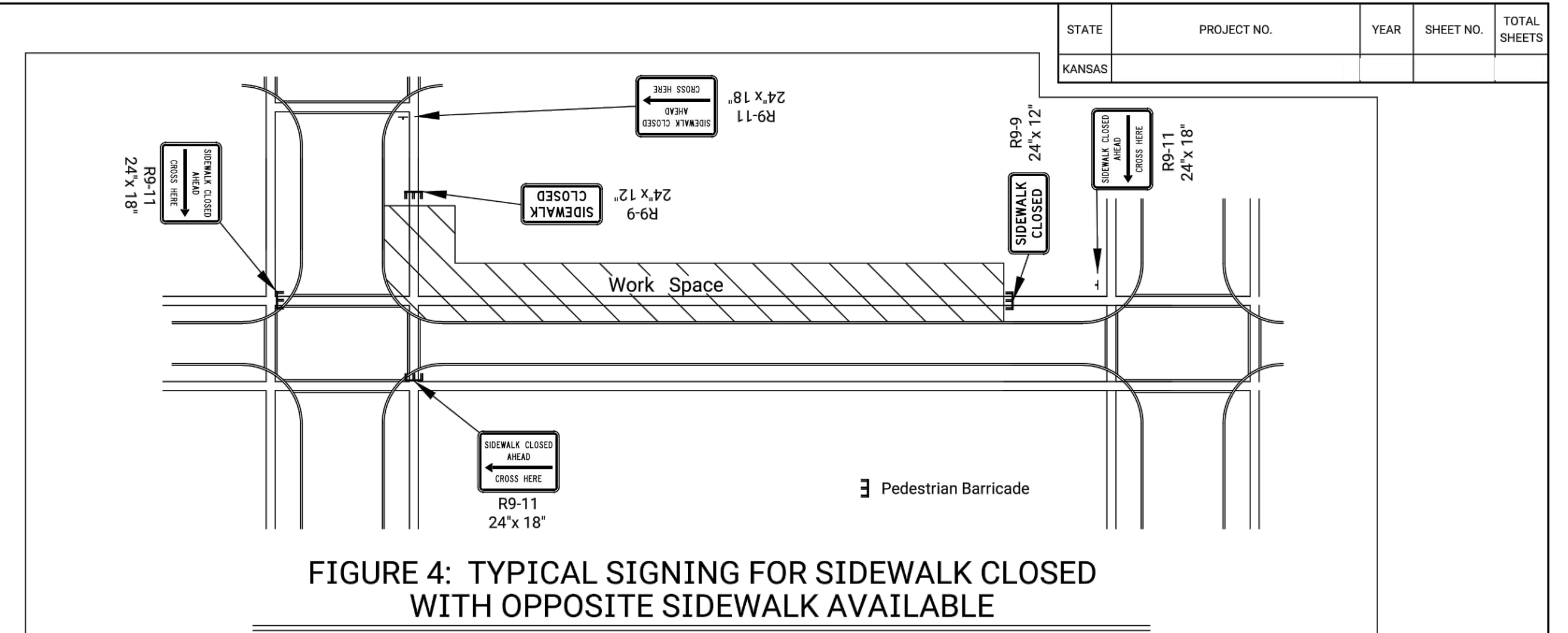
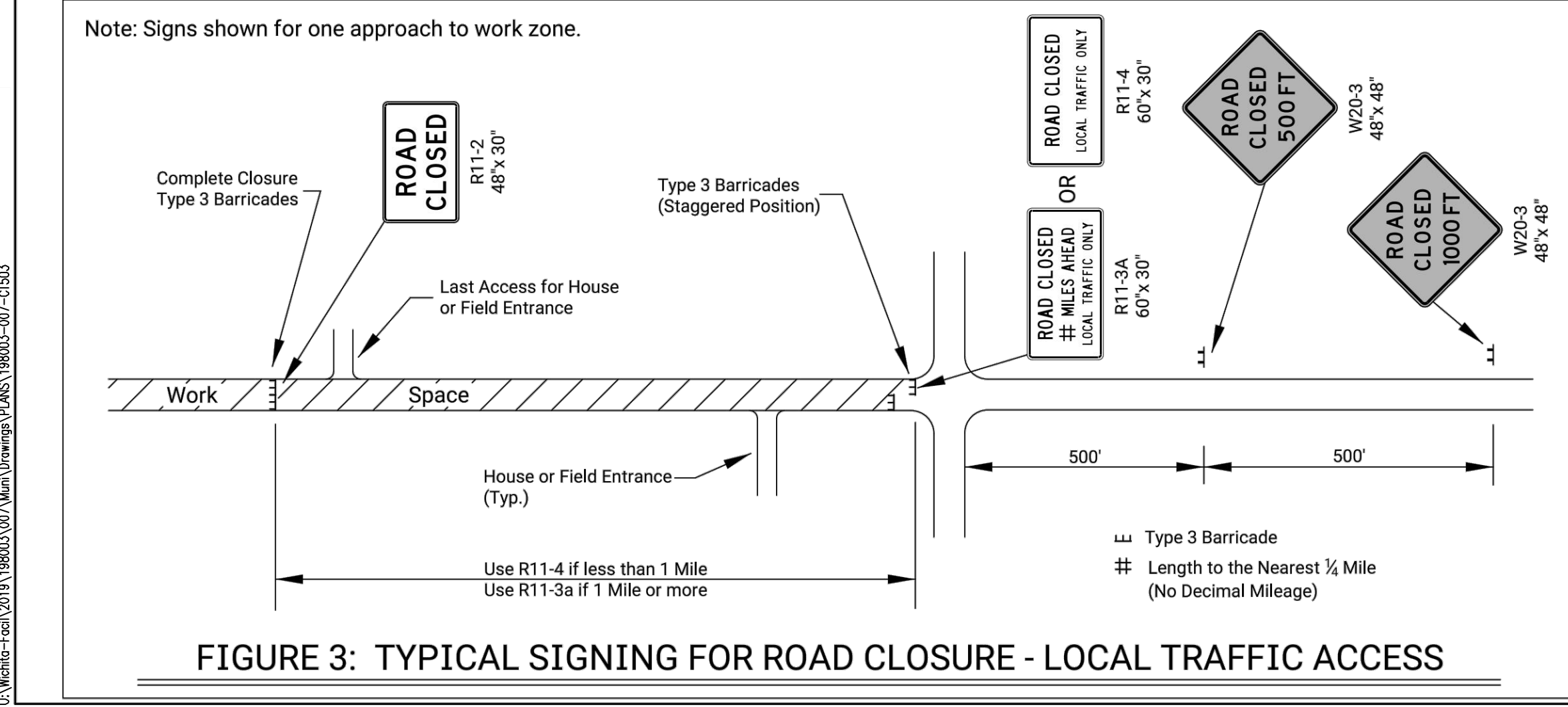
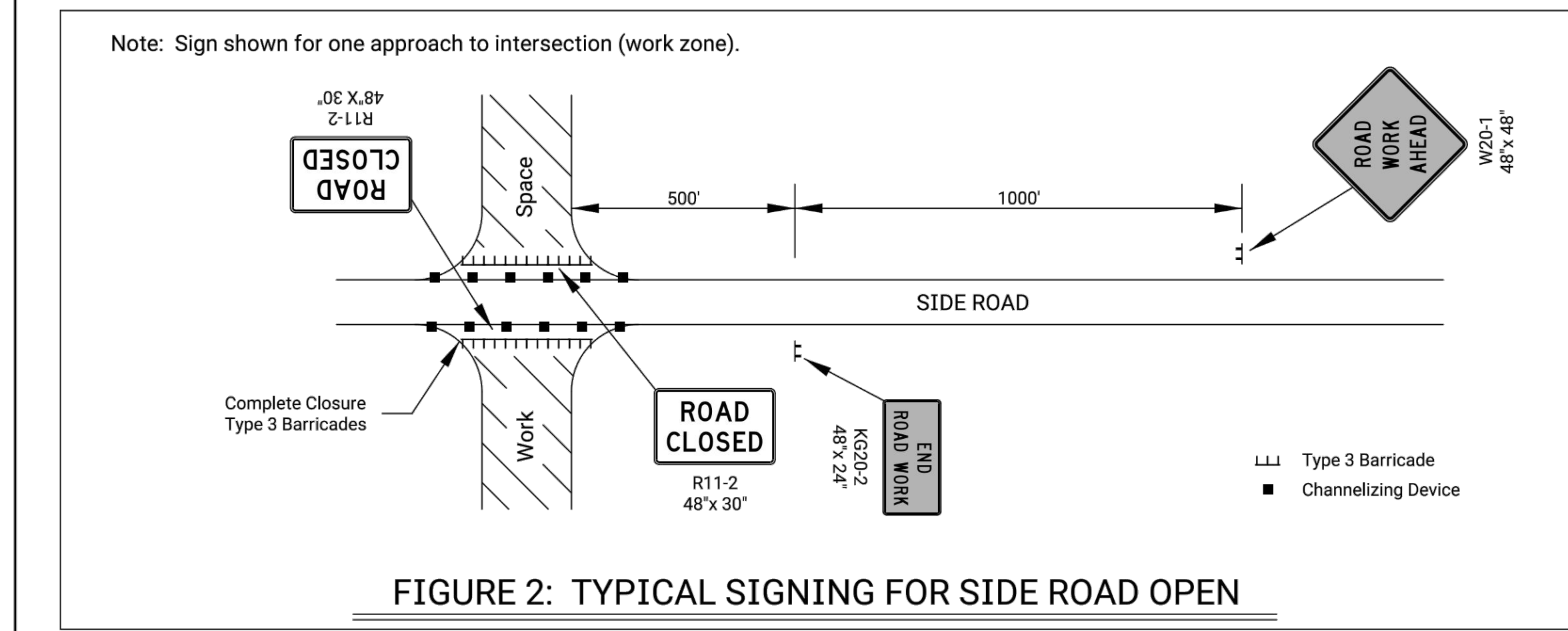
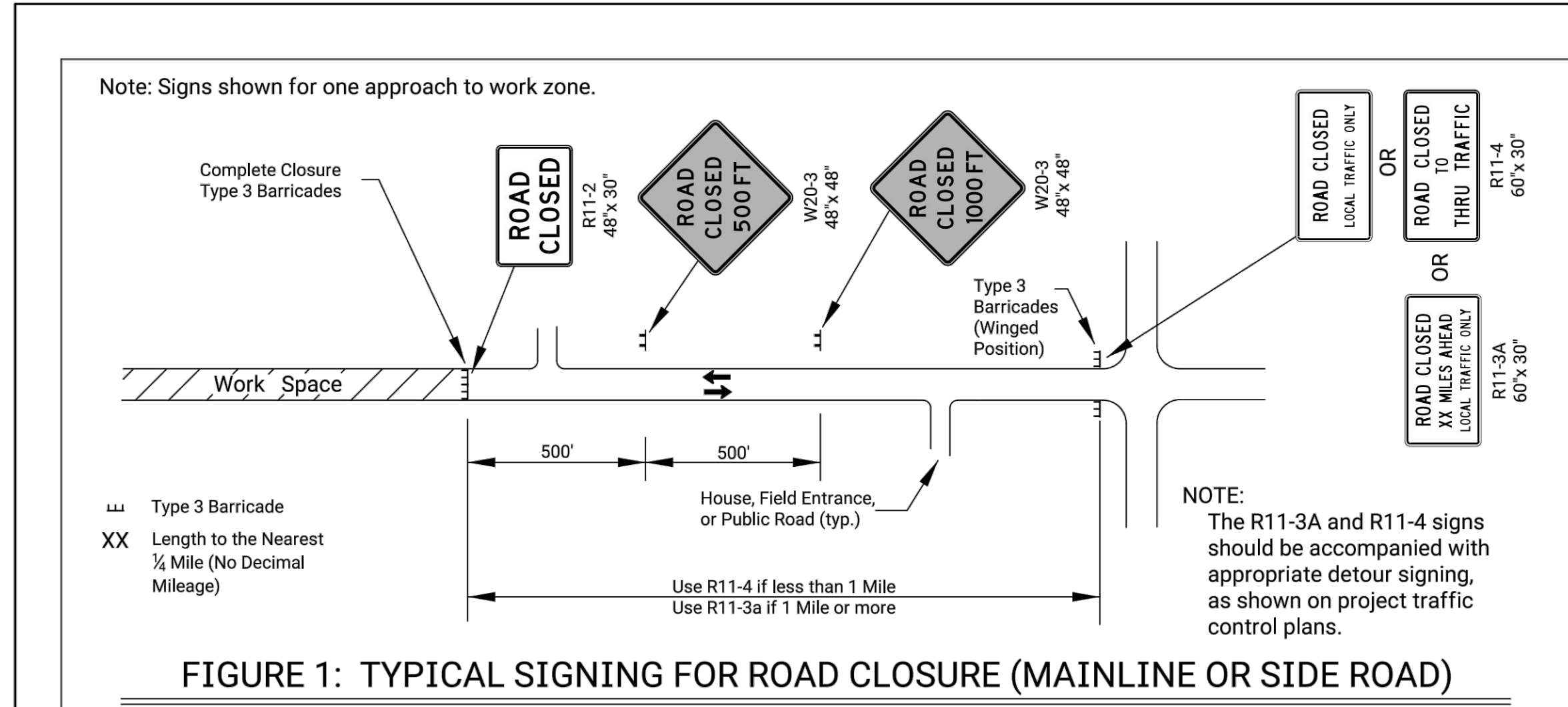
DESIGNED: L.R. [REDACTED] 06/01/15 APP'D: Kristina Erickson
 DESIGN CK: [REDACTED] R.W.B. QUANTITIES TRACED
 DETAIL CK: [REDACTED] QUAN. CK. TRACE CK.

KDOT Graphics Certified 03-29-2018

JOB NUMBER: 198003-007
 DATE: 07/25/19
 DRAWN: KMH
 CHECKED: NLS

BID DOCUMENTS

TRAFFIC CONTROL STANDARD TE702



ROAD CLOSED GENERAL NOTES

As shown in Figure 1, at the point where thru traffic must detour and local traffic can proceed to the location where the roadway is completely closed, the R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) or R11-4 (ROAD CLOSED LOCAL TRAFFIC ONLY or ROAD CLOSED TO THRU TRAFFIC) sign shall be used with Type 3 barricades (winged position), placed on the shoulders of roadway.

As shown in Figure 3, when local traffic must be allowed access into the work zone, Type 3 barricades shall be longitudinally staggered to maintain the appearance of a closed roadway. A second line of end-to-end Type 3 barricades shall be placed just beyond the last access point in the work zone, to completely close the roadway.

The R11-4 (ROAD CLOSED TO THRU TRAFFIC or ROAD CLOSED LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is less than 1 mile.

The R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is 1 mile or greater.

The words "BRIDGE OUT" (or BRIDGE CLOSED) may be substituted for the words "ROAD CLOSED" on the R11-3a or R11-4 sign where applicable.

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS				

NO.	DATE	REVISIONS	BY	APP'D
3				
2				
1				

KANSAS DEPARTMENT OF TRANSPORTATION

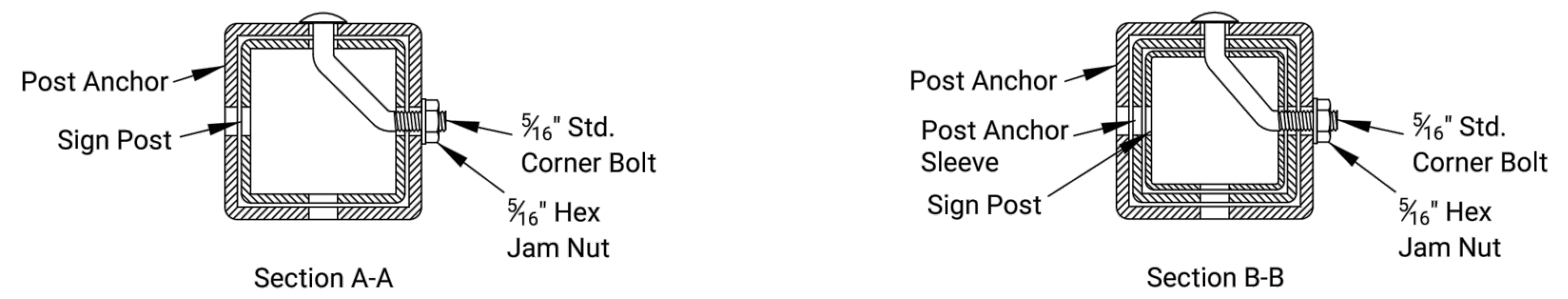
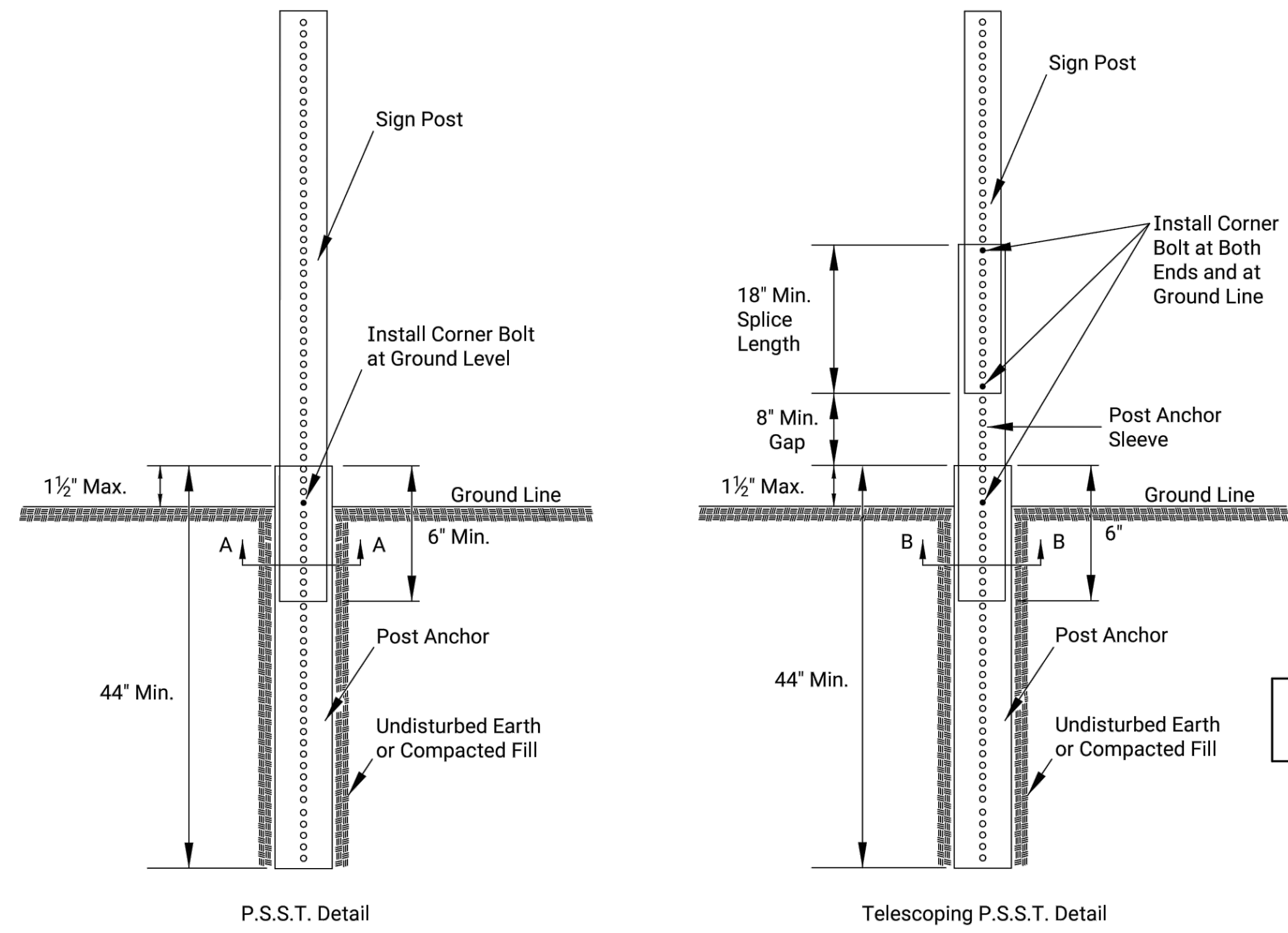
TRAFFIC CONTROL CLOSURES

TE704

DESIGNED: B.A.H. | CHECKED: R.W.B. | QUANTITIES: K. K. | TRACED: K. K.
 DESIGN CK: DETAIL CK: QUAN. CK: TRACE CK:

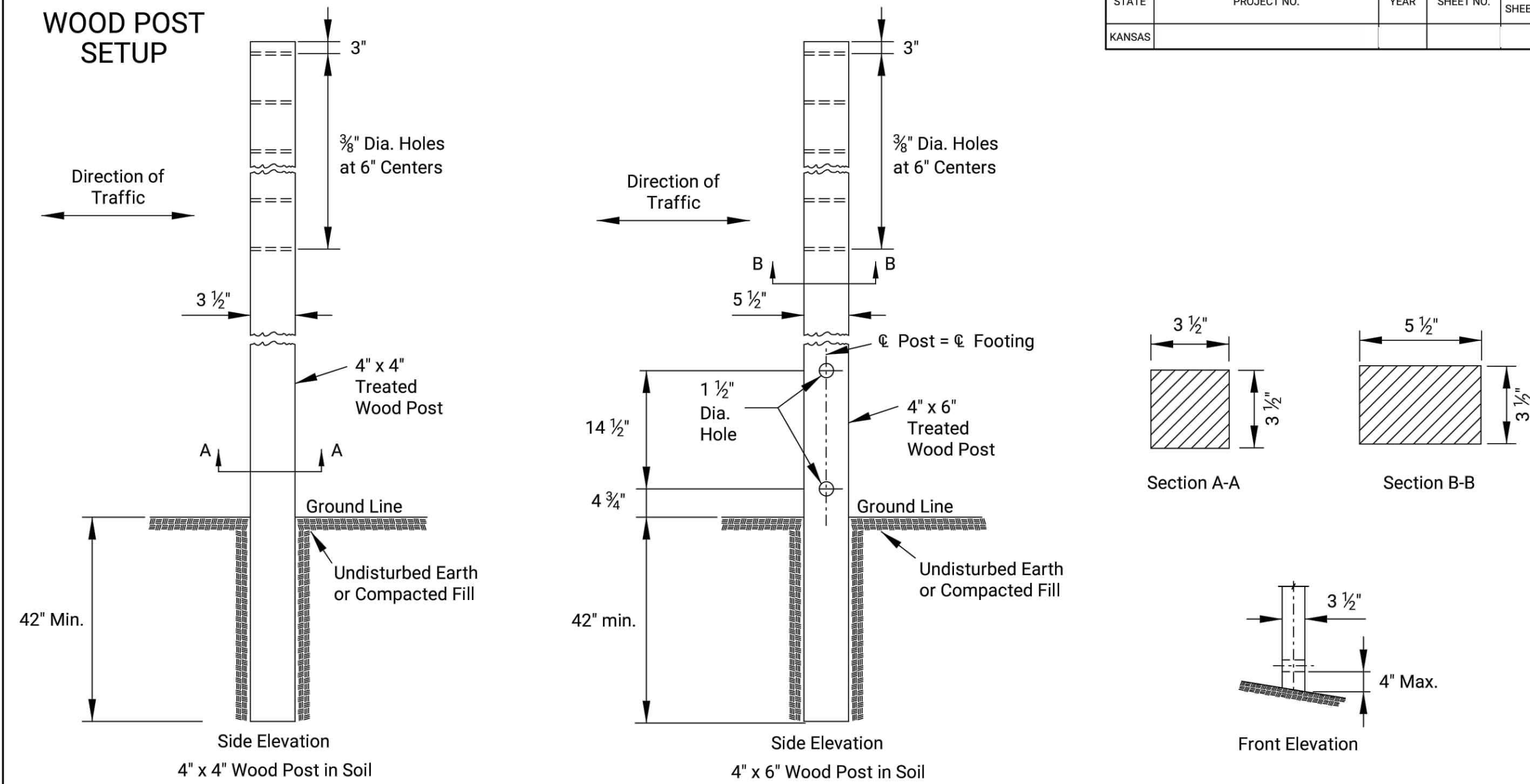
KDOT Graphics Certified 03-29-2018

PERFORATED SQUARE STEEL TUBE (P.S.S.T.) POST SETUP



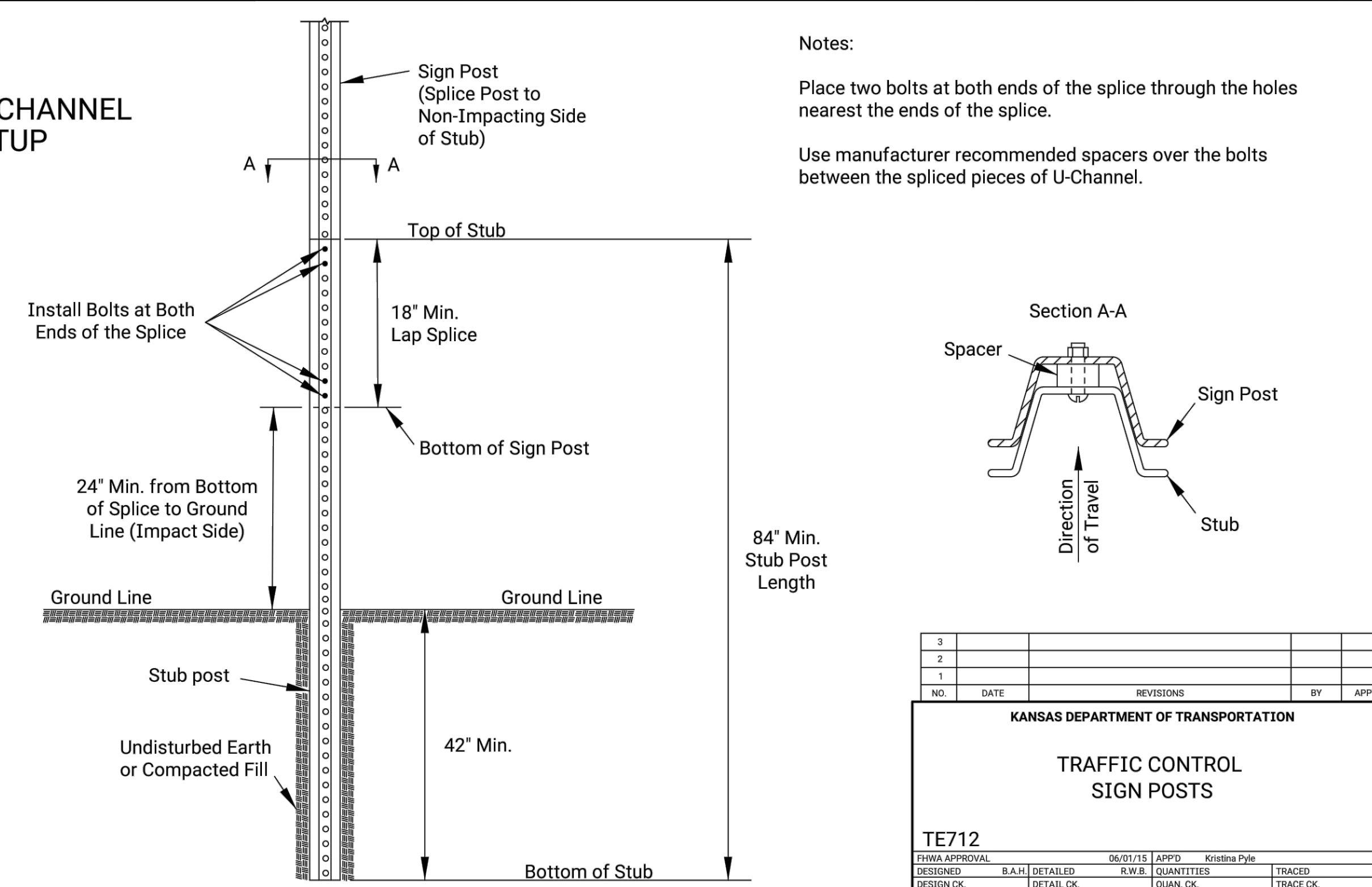
Details for 2", 2 1/4", or 2 1/2" sign posts
 Place bolts in the same corner along each sign post.

WOOD POST SETUP



See TE710 for Additional Details and Requirements

3 LB/F U-CHANNEL SETUP



Notes:
 Place two bolts at both ends of the splice through the holes nearest the ends of the splice.
 Use manufacturer recommended spacers over the bolts between the spliced pieces of U-Channel.

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS				

NO.	DATE	REVISIONS	BY	APP'D
3				
2				
1				

KANSAS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL SIGN POSTS

TE712

DESIGN APPROVAL	B.A.H. DETAILED	06/01/15	APPROV	03/28/18	Pyke
DESIGN CK.			R.W.B. QUANTITIES		TRACED
			QUAN. CK.		TRACE CK.

KDOT Graphics Certified 03-29-2018

MABCD REVIEWER:

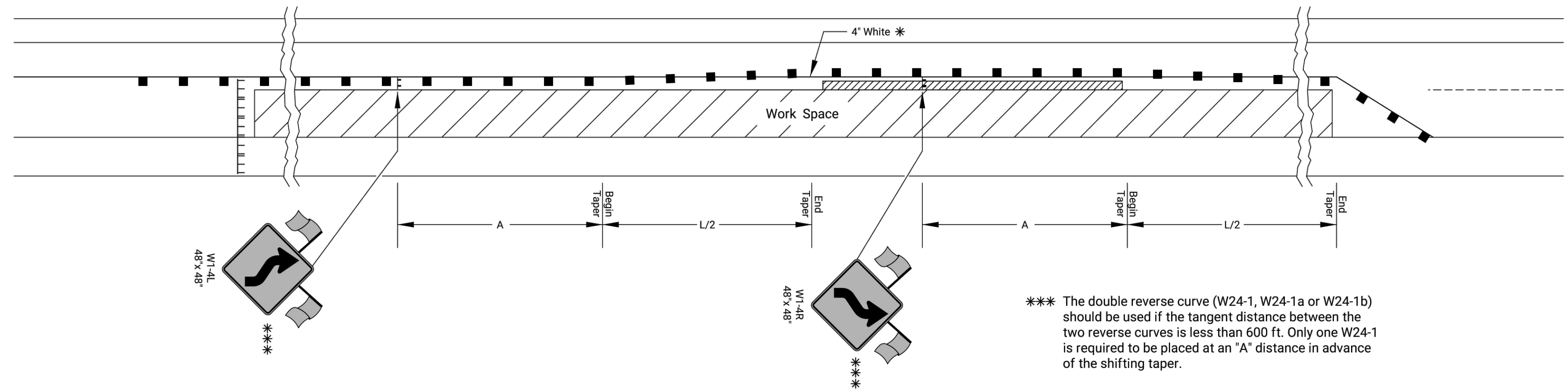


Date	Description
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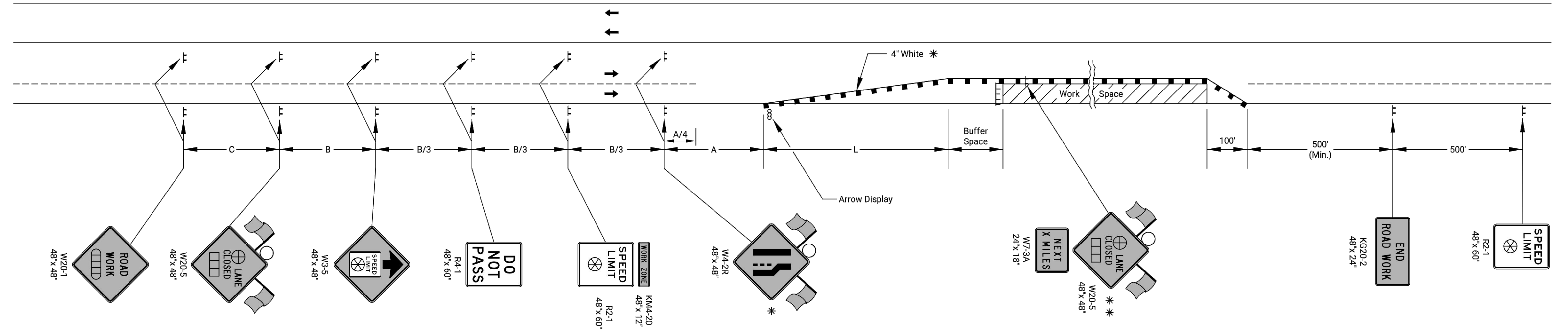
SHIFTING TAPER DETAIL

Add signs and devices as shown for work inside a closed lane that extends near to (or into) the open traffic lane.

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS				



*** The double reverse curve (W24-1, W24-1a or W24-1b) should be used if the tangent distance between the two reverse curves is less than 600 ft. Only one W24-1 is required to be placed at an "A" distance in advance of the shifting taper.



- ⊔ Type 3 Barricades
- X Length to the Nearest Whole Mile
- Channelizing Device
- ⊔ Ahead, 1500 ft, or 1 mile
- ⊔ Ahead, 1000 ft, 1500 ft, or 1/2 mile
- ⊕ Right or Left
- ⊗ Speed to be determined by the Engineer
- Type "A" Low Intensity Warning Light

- * For left lane closures use W4-2L and yellow edge line along channelizing devices.
- * * The W20-5 (⊕ Lane Closed) and W7-3A (Next X Miles) signs should be placed at 2 mile increments on a project of 4 miles or longer.

Left-side signs shall be omitted for a four-lane undivided highway.

One flagger should be stationed within each multi-lane roadway activity area where work is in a closed lane adjacent to traffic and not separated by a concrete safety barrier system.

NO.	DATE	REVISIONS	BY	APP'D
3				
2				
1	03/13/18	W24-1 usage changed to Should	R.W.B.	E.G.K.

KANSAS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
 LANE CLOSURE ON MULTI LANE HWY

TE744

DESIGN APPROVAL	B.A.H.	03/13/18	APP'D	Eric Koehler
DESIGNED			R.W.B.	QUANTITIES
DESIGN CK.			QUAN. CK.	TRACE CK.

KDOT Graphics Certified 03-29-2018

TEXTRON AVIATION
 EAST CAMPUS - WEBB RD.
 LANDSCAPING IMPROVEMENTS
 (CENTRAL AVE TO TRUCK ENTRANCE)
 WICHITA, KS

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CHECKED:	NLS

BID DOCUMENTS

TRAFFIC CONTROL
 STANDARD TE744

CT-506
 EAST CAMPUS - TXTAV
 CENTRAL AVE. & WEBB RD.