

50'

20' BACKYARD SETBACK LINE

5' SIDEYARD SETBACK LINE

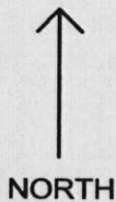
12' SIDEYARD SETBACK LINE

GARAGE

12.5'

125'

125'



BZA2004-00091

SITE PLAN

APPROVED 12-20-04 BY SJC

MAIN LIVING AREA

WINDOWELL

5.5'

scale : 1 INCH = 15 FEET

26'

25' BUILDING SETBACK LINE'

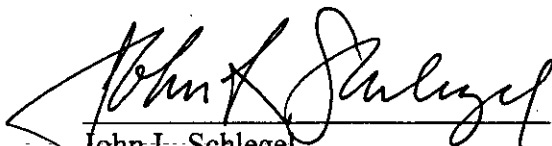
Lots 23-25, McCormicks Addition
1200 W. 2nd Street

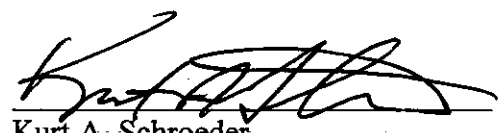
- 3) Compatibility with existing or permitted uses on abutting sites: The single-family residence is compatible with abutting sites, which also are developed with single-family residences. The encroachments into the side setbacks should not reduce compatibility with abutting sites, as many of the abutting sites are developed with older structures that do not meet current setback standards.
- 4) Effect on public health, safety or welfare: There will be no encroachment into public utility easements or right-of-way; therefore, there should not be a negative impact on the public health, safety or welfare nor will properties or improvements in the vicinity be materially injured.

Our signatures below indicate that an Administrative Adjustment to reduce the street side setback from 15 feet to 12.5 feet and the interior side setback from 6 feet to 5.5 feet for the aforementioned property is hereby granted, subject to the following conditions:

- 1) The site shall be developed in general conformance with the approved site plan.
- 2) The setback reduction shall apply only to the side yard setback encroachments illustrated on the approved site plan. All other structures or additions on the subject property shall conform to the setbacks permitted by the Unified Zoning Code unless a separate Zoning Adjustment or Variance is granted.
- 3) If the Zoning Administrator finds that there is a violation of any of the conditions of the Zoning Adjustment, the Zoning Administrator, in addition to enforcing the other remedies set forth in the Unified Zoning Code, may, with the concurrence of the Planning Director, declare that the Zoning Adjustment is null and void.

The "Development Application" sign should now be removed from the property.


John L. Schlegel
Planning Director


Kurt A. Schroeder
Superintendent of Central Inspection

Enclosure

cc: Kurt Schroeder, Office of Central Inspection
Paul Hays, Office of Central Inspection
Randy Sparkman, Office of Central Inspection
J.R. Cox, Office of Central Inspection



FILE COPY

Wichita-Sedgwick County Metropolitan Area Planning Department

December 20, 2004

Louis Confessori
Mennonite Housing
2145 N. Topeka
Wichita, KS 67217

Re: BZA2004-00091: Administrative Adjustment to reduce the street side setback from 15 feet to 12.5 feet and to reduce the interior side setback from 6 feet to 5.5 feet.

Legal Description: Lots 23 & 25, McCormick's 3rd Addition, Wichita, Sedgwick County, Kansas. Located at the northwest corner of 2nd Street North and Dodge (1200 W. 2nd St. N.).

Dear Mr. Confessori:

We have reviewed your request for an Administrative Adjustment to reduce the side setbacks on the aforementioned property. From reviewing your application, we understand that you propose to construct a single family residence on the property and that the proposed structure needs to encroach into the street side setback by 2.5 feet and the interior side setback by 0.5 feet in order to fit on the lot.

Sec. V-I.2.a. of the Unified Zoning Code allows an adjustment to reduce setbacks by up to 20 percent when the conditions required by Sec. V-I.6. of the Code are met. We find that the reduction of the setbacks as proposed meets the four conditions required by Section V-I.6. of the Unified Zoning Code as set out below:

- 1) Impact on safety and convenience of vehicular and pedestrian circulation: The proposed encroachments should not negatively impact the safety and convenience of vehicular and pedestrian circulation in the vicinity because a two-car garage will be provided to meet the parking requirement of the Unified Zoning Code; therefore, required parking spaces will not be located within the reduced street side yard.
- 2) Impact on existing uses in surrounding areas: There should be no negative impact on the existing uses in surrounding areas as a result of the reduction of the setbacks, as sufficient separation between buildings is maintained and the encroachments into the setbacks are minor.