

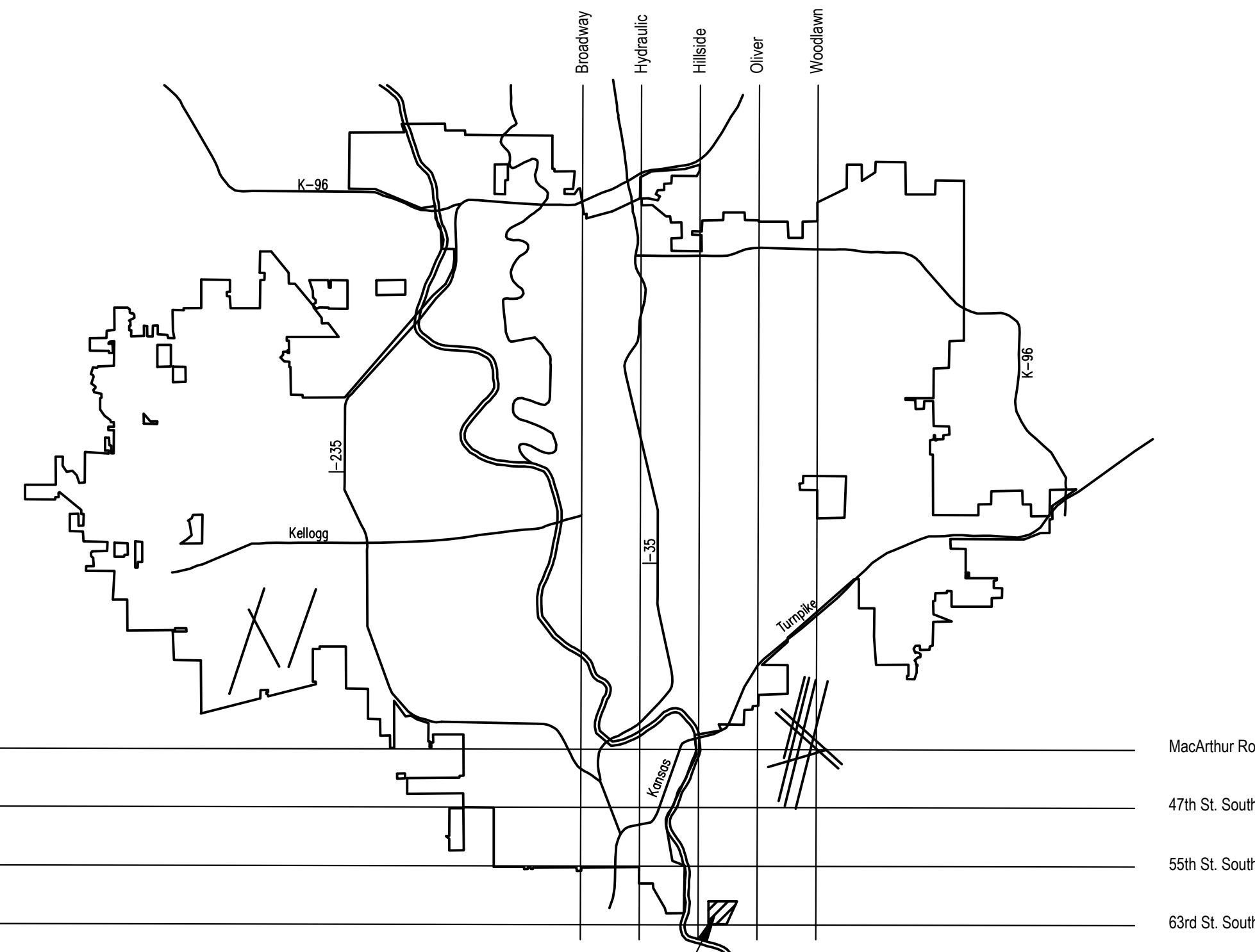
CONSTRUCTION PLANS FOR PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS TO SERVE SWANEY FARM ADDITION - PHASE 1

CITY OF WICHITA ENGINEERING PROJECT NO. 472-2024-086026
ORG CODE: 47476424, MUNIS NO.: E4081

CITY OF WICHITA, KS
PAUL GUNZELMAN, P.E. - CITY ENGINEER

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LOCATION MAP
PROJECT LOCATION
SWANEY FARM ADDITION

DEVELOPER CONTACT

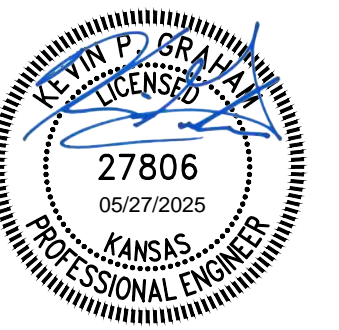
MR. ERIC GILBERT
ARTISTIC BUILDERS
11100 FREMONT CIRCLE
MULVANE, KS 67110
316-650-7536
EMGILBERT13@GMAIL.COM

SUBDIVISION BENCH MARKS (SBM)

NO.	STREET AND STATION	FROM CL	DESCRIPTION	ELEVATION
1	YALE ST., STA. 36+48.13	17.50' RT.	P.C. TOP OF CURB, YALE ST.	
2	MERILYN ST., STA. 58+20.17	14.50' LT.	E. END, NORTHEAST CURB RETURN, MERILYN ST.	
3	MERILYN ST., STA. 63+05.03	14.50' LT.	P.I. TOP OF CURB, MERILYN ST.	
4	BLUFF ST., STA. 67+56.80	14.50' LT.	N. END, P.T. TOP OF CURB, BLUFF ST.	
5	PATRICK ST., STA. 30+37.19	14.50' RT.	E. END, SOUTHEAST CURB RETURN, PATRICK ST.	

THE COST OF THE BENCH MARK DISC, INCLUDING INSTALLATION, SHALL BE INCIDENTAL TO THE CURB.

MAY 2025



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION
PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

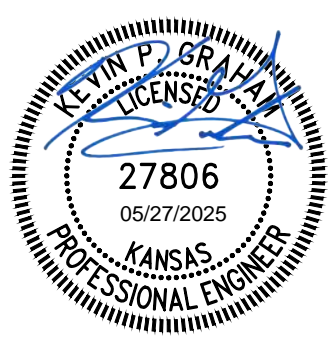
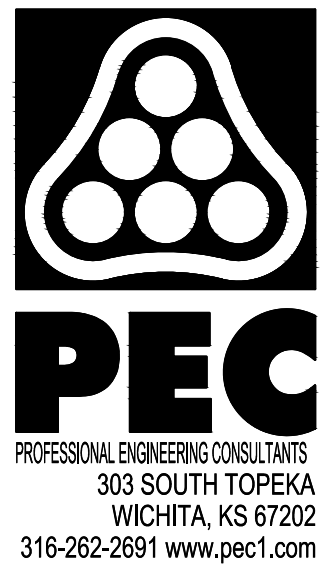
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PM	KPG	
DESIGNED BY	KPG	
DRAWN BY	BJS	
CHECKED BY	KMS	

TITLE SHEET

CP001
01 OF 71

SAVED 5/27/2025 9:28:19 AM BY KEVIN GRAHAM
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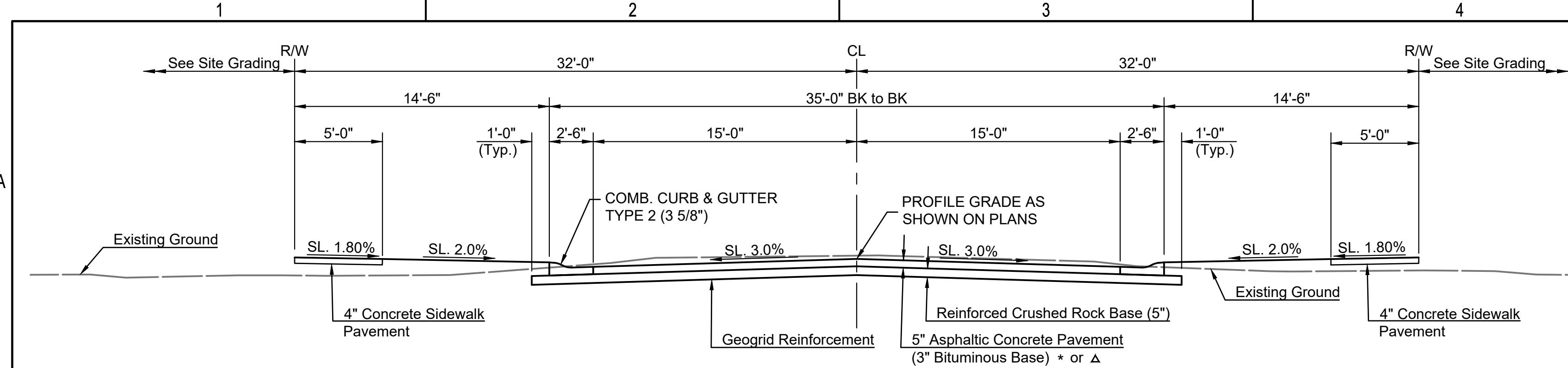
1	2	3	4	5	6
GENERAL NOTES			TRAFFIC HANDLING/CONSTRUCTION SCHEDULE		
<p>1. ALL CONSTRUCTION AND MATERIALS TO COMPLY WITH CITY OF WICHITA STANDARD CONSTRUCTION SPECIFICATIONS AND DETAILS, UNLESS OTHERWISE INCLUDED IN THE CONTRACT DOCUMENTS.</p> <p>2. EACH BIDDER SHALL VISIT THE SITE OF THE PROJECT BEFORE SUBMITTING THE PROPOSAL FOR THIS WORK SO THAT THEY WILL BE FULLY INFORMED OF THE EXISTING FIELD CONDITIONS AND THE OBSTACLES WHICH MIGHT BE ENCOUNTERED. UPON AWARD OF THE CONTRACT THE CONTRACTOR WILL NOT BE GRANTED ANY ADDITIONAL COMPENSATION WITH REGARDS TO TIME AND MONEY FOR CONDITIONS THAT MAY HAVE BEEN EVALUATED DURING ANY INSPECTION OF THE SITE.</p> <p>3. AT LEAST 72 HOURS PRIOR TO BEGINNING ANY EXCAVATION (EXCLUDING WEEKENDS AND HOLIDAYS), THE CONTRACTOR SHALL CONTACT THE KANSAS ONE-CALL SYSTEM, A UTILITY LOCATION SERVICE, AT (316)-687-2470 OR 811 TO REQUEST THE LOCAL UTILITY COMPANIES TO LOCATE ANY EXISTING LINES WITHIN THE PROJECT AREA.</p> <p>4. THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF AN EMERGENCY:</p> <p>EMERGENCY DISPATCH: 911 COX COMMUNICATIONS: 888-249-3530 EVERGY: 800-383-1183 AT&T: 800-286-8313 KANSAS GAS SERVICE: 888-482-4950</p> <p>5. THE CONTRACTOR SHALL GIVE ALL PROPERTY OWNERS AND/OR TENANTS OF DEVELOPED PROPERTY DIRECTLY ADJUTING THE CONSTRUCTION OF THIS PROJECT A MINIMUM OF SEVEN (7) DAYS ADVANCE NOTICE PRIOR TO THE START OF CONSTRUCTION.</p> <p>6. THE CONTRACTOR SHALL NOT START WORK ON THE PROJECT UNTIL THE PROJECT INSPECTOR IS ASSIGNED AND IS PRESENT ON THE SITE. ANY WORK DONE WITHOUT INSPECTION WILL BE REQUIRED TO BE UNCOVERED FOR INSPECTION AT THE CONTRACTORS EXPENSE.</p> <p>7. ALL ELEVATIONS SHOWN ARE NAVD88 DATUM. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL RE-ESTABLISH CONTROL POINTS AND BENCH MARKS AND VERIFY THEIR ACCURACY.</p> <p>8. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE DRAWINGS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. IT SHOULD BE NOTED THAT OTHER BURIED LINES AND CABLES MAY EXIST WHICH ARE NOT SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL HAVE ALL BURIED LINES LOCATED AND FLAGGED IN THE FIELD PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL CONTACT THE ENGINEER AND REVIEW ANY BURIED LINES LOCATED IF CONFLICTS EXIST. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION DURING TRENCHING OPERATIONS TO AVOID DAMAGING THESE LINES. ANY LINES DAMAGED SHALL BE REPLACED OR REPAIRED IMMEDIATELY AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.</p> <p>9. THE CONTRACTOR SHALL EXPOSE AND VERIFY THE VERTICAL AND HORIZONTAL LOCATION OF EXISTING UTILITIES THAT ARE IN POTENTIAL CONFLICT WITH THE PROPOSED IMPROVEMENTS. THE UTILITY LOCATES SHALL BE PERFORMED PRIOR TO THE START OF CONSTRUCTION AND ANY DISCREPANCIES SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.</p>	<p>10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS AND SECTION CORNERS. THE CONTRACTOR SHALL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS.</p> <p>11. EASEMENTS AND RIGHTS-OF-WAY PROVIDED BY THE OWNER FOR THE PROJECT ARE SHOWN ON THE DRAWINGS. IF NECESSARY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THE ACQUISITION OF ANY ADDITIONAL TEMPORARY EASEMENTS OR RIGHTS-OF-WAY DESIRED TO USE IN COMPLETING THE WORK.</p> <p>12. THE CONTRACTOR SHALL CONTAIN THEIR OPERATIONS TO PERMIT LOCAL AND EMERGENCY TRAFFIC THROUGH AND ACROSS CONSTRUCTION AT ALL TIMES. THE CONTRACTOR SHALL UTILIZE WARNING SIGNS, FLASHING LIGHTS, BARRICADES, AND FLAGMEN IN COMPLIANCE WITH THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).</p> <p>13. RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES INCLUDING ANY TREES REMOVED, TREE TRIMMINGS, AND EXCESS EXCAVATION WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES PROVIDED BY THE CONTRACTOR. THESE SITES SHALL ALSO BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE, AND SITE LOCATION. LOCATIONS THAT, IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WILL REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES, FLOODWAYS, OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS MAY REQUIRE ARCHAEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED DISPOSAL LOCATION.</p> <p>14. THE CONTRACTOR SHALL AVOID REMOVAL OR TRIMMING OF ANY TREES OR SHRUBS WHERE POSSIBLE. WHERE THE CONTRACTOR BELIEVES THE REMOVAL OR TRIMMING IS UNAVOIDABLE, THIS WORK SHALL BE COORDINATED WITH THE ENGINEER. TREE TRIMMING/REMOVAL SHALL BE COMPLETED IN ACCORDANCE WITH U.S. FISH AND WILDLIFE SERVICE AND KANSAS DEPARTMENT OF WILDLIFE, PARKS, AND TOURISM RESTRICTIONS.</p> <p>15. THE CONTRACTOR SHALL RESTORE ALL DITCHES, SWALES, ROAD SHOULDERS, AND BANKS TO THEIR ORIGINAL SLOPES AND GRADES EXCEPT AS SHOWN OTHERWISE. WHERE EXISTING ENTRANCE PIPE, DRAINAGE PIPE, SIGNS, FENCES, LANDSCAPING, ETC., CONFLICT WITH THE PROPOSED WORK HEREIN, THEY SHALL BE REMOVED AND REPLACED OR RESET, UNLESS OTHERWISE NOTED ON THE DRAWINGS.</p> <p>16. THE CONTRACTOR SHALL INSTALL AND/OR MAINTAIN EROSION CONTROL METHODS AS SPECIFIED ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING EROSION CONTROL THROUGH THE COMPLETION OF THIS PROJECT. INSTALLATION OF THESE EROSION CONTROL DEVICES DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF ABATING SOIL EROSION.</p> <p>17. THE CONTRACTOR SHALL TAKE CARE TO PREVENT SILT AND DEBRIS FROM ENTERING ANY STORM DRAINAGE SYSTEM DURING CONSTRUCTION. PIPES OR STRUCTURES WHICH CONTAIN MATERIALS FROM THE CONTRACTORS ACTIVITIES SHALL BE THOROUGHLY CLEANED BY THE CONTRACTOR, AT THEIR OWN EXPENSE, PRIOR TO THE FINAL INSPECTION.</p>	<p>18. RECONSTRUCTION OF EROSION CONTROL MEASURES WHICH ARE DESTROYED BY WIND, FLOOD, FIRE, OR BY THE ACTIONS OF THE CONTRACTOR OR OTHERS SHALL BE PERFORMED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST. WHERE ADJUSTMENTS IN QUANTITIES ARE REQUIRED BY FIELD CONDITIONS, THERE SHALL BE NO ADJUSTMENT IN UNIT PRICE.</p> <p>19. ALL GRASSED AREAS DISTURBED BY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS SHALL BE REPLANTED WITH GRASS AND FERTILIZED IN ACCORDANCE WITH THE CITY OF WICHITA SPECIFICATIONS. EXISTING GRASSED AREAS DISTURBED BY CONSTRUCTION SHALL BE REPLANTED WITH THE SAME TYPE OF GRASS AS WAS REMOVED, UNLESS OTHERWISE SPECIFIED.</p> <p>20. THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITIES WITH TEMPORARY RYE GRASS. RYE GRASS SEED SHALL BE PLANTED AT A MINIMUM RATE OF SIX (6) POUNDS PER ONE THOUSAND (1,000) SQUARE FEET. THIS TEMPORARY SEEDING MAY BE OMITTED ONLY IF PERMANENT SEEDING/SODDING IS APPLIED. TEMPORARY SEEDING OR PERMANENT SEEDING/SODDING SHALL BE APPLIED WITHIN 14 DAYS AFTER THE AREA HAS BEEN DISTURBED. REFER TO SEEDING PLAN IN STORM WATER DRAIN IMPROVEMENTS PROJECT (458-2024-085570).</p> <p>21. IF APPLICABLE, CONTRACTOR SHALL MAINTAIN UNINTERRUPTED UTILITY SERVICE TO ADJACENT FACILITIES DURING CONSTRUCTION, UNLESS OTHERWISE APPROVED BY OWNER.</p> <p>22. WRITTEN REQUEST TO THE UTILITY OWNER WILL BE REQUIRED 72 HOURS PRIOR TO A SCHEDULED UTILITY OUTAGE. THE FIRE DEPARTMENT MUST BE NOTIFIED OF ANY FIRE HYDRANTS OR WATER MAINS TAKEN OUT OF SERVICE.</p> <p>23. ALL RCB, STORM SEWER, WATERLINE AND SANITARY SEWER EXCAVATION UNDER PROPOSED PAVEMENT SHALL BE SAND FILLED AND FLUSHED (JETTED AND VIBRATED) WITH WATER PER THE REQUIREMENTS LISTED IN THE STANDARD SPECIFICATIONS FOR THE CITY OF WICHITA, UNLESS FLOWABLE FILL OR OTHER IMPROVED BACKFILL MATERIAL IS OTHERWISE SPECIFIED. THE SAND FILL SHALL START AT THE TOP OF IMPROVED BEDDING (PER STANDARD SPECIFICATIONS) AND BE BROUGHT UP UNIFORMLY TO AN ELEVATION 12 INCHES ABOVE THE TOP OF PIPE OR 2 FEET BELOW THE BOTTOM OF PROPOSED PAVEMENT, WHICHEVER IS HIGHER. HOWEVER, IN NO INSTANCE SHALL THE SAND FILL BE BROUGHT UP TO LESS THAN 6 INCHES BELOW THE PAVEMENT SUB-BASE MATERIAL. STORM SEWER ADJACENT TO THE BACK OF CURB SHALL ALSO BE SAND FILLED PER THIS PROVISION, TO WITHIN 2 FEET OF FINAL GRADE.</p> <p>24. IF TRAFFIC WILL BE IMPACTED BY CONSTRUCTION, A TRAFFIC CONTROL PLAN MUST BE SUBMITTED AND APPROVED BY THE CITY TRAFFIC ENGINEER, TRAFFIC@WICHITA.GOV BEFORE CONSTRUCTION CAN BEGIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL MEASURES TO FACILITATE CONSTRUCTION. ALL CONSTRUCTION ZONE MARKINGS AND SIGNAGE SHALL CONFORM TO THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS PUBLISHED BY THE US DEPT. OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION. ALL COSTS ASSOCIATED WITH CONSTRUCTION MARKINGS AND SIGNAGE SHALL BE THE CONTRACTORS RESPONSIBILITY.</p>	<p>THE CONTRACTOR SHALL INSTALL THE STORM SEWER IMPROVEMENTS PRIOR TO ASPHALT PAVING OPERATIONS. DEMOLITION WORK CAN BE DONE CONCURRENTLY.</p> <p>THE CONTRACTOR SHALL PROVIDE A DETAILED CONSTRUCTION SEQUENCING AND TRAFFIC CONTROL PLAN FOR ENGINEER REVIEW AND APPROVAL PRIOR TO START OF CONSTRUCTION.</p> <p>THE CONTRACTOR SHALL PROVIDE DAILY VEHICULAR ACCESS TO PROPERTIES WITHIN THE PROJECT AREA THROUGHOUT CONSTRUCTION. CONTRACTOR SHALL ALSO NOTIFY PROPERTY OWNERS SEVENTY-TWO (72) HOURS IN ADVANCE BEFORE PAVING DRIVEWAYS.</p>		



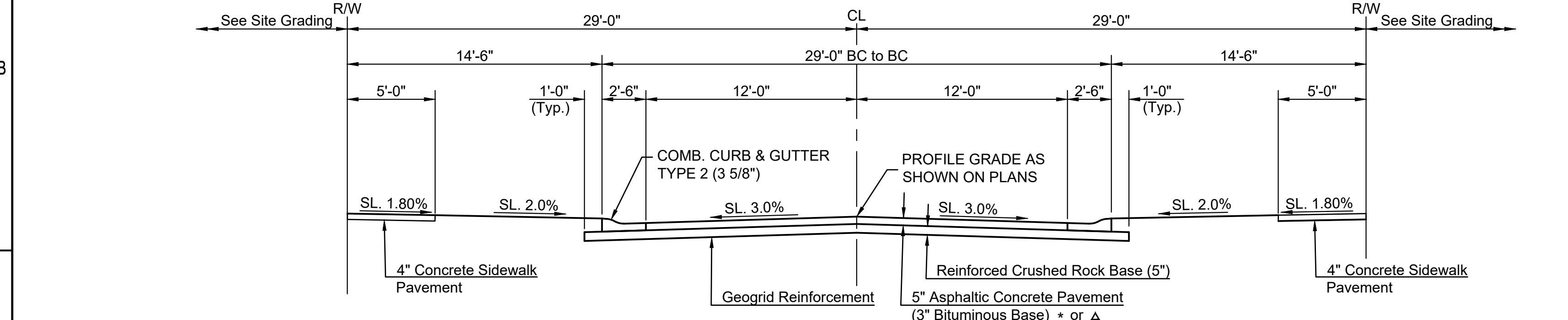
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION
PHASE 1
PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		
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DATE	MAY 2025	
PM	KPG	
DESIGNED BY	KPG	
DRAWN BY	BJS	
CHECKED BY	KMS	

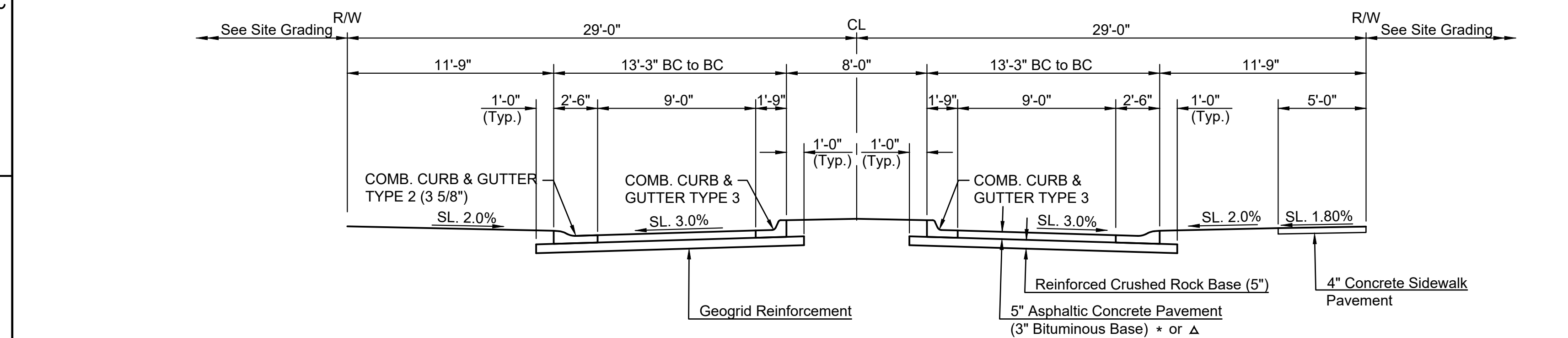
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A
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C
D
E
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PLANS\030_PAVING PHASE 1\04-200605-004-CP005 TYPICAL SECTIONS.DWG
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 SAVER 3/27/2025 7:55:44 AM BY KEVIN GRAHAM



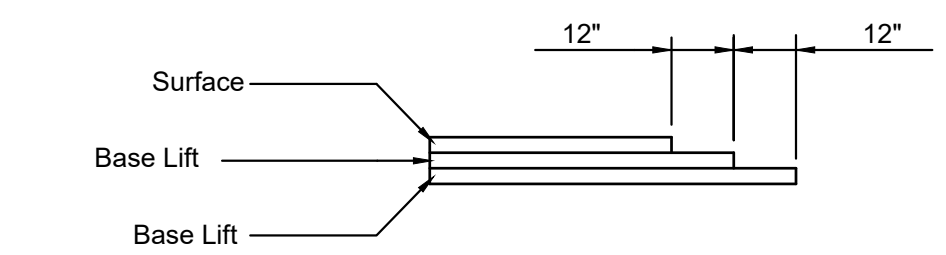
TYPICAL SECTION
 61st St. South Sta. 10+44.87 to Sta. 13+40.33
 Yale Sta. 25+00.00 to Sta. 29+46.36
 Yale Sta. 30+53.45 to Sta. 37+45.91



TYPICAL SECTION
 Merilyn Street Sta. 55+44.46 to Sta. 62+12.96
 Merilyn Street Sta. 63+05.03 to Sta. 67+74.80
 Patrick Sta. 60+37.19 to Sta. 65+46.19



TYPICAL SECTION
 Merilyn Street Sta. 62+12.96 to Sta. 63+05.03



TRANSVERSE CONSTRUCTION JOINTS

Transverse construction joints shall be constructed in flexible base pavement at locations where pavement joins existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards of pavement.

GENERAL NOTES:
 Geogrid reinforcement and aggregate base to comply with Sec. 404 of the City of Wichita Standard Specifications for the Construction of Public Projects. Geogrid reinforcement shall be installed in accordance with manufacturer's recommendations. Crushed rock shall be uniformly graded from 1 - 1/2" maximum size to not more than 10% passing a No. 200 sieve. Rock quality shall be the same as specified for coarse aggregate for concrete mixes.

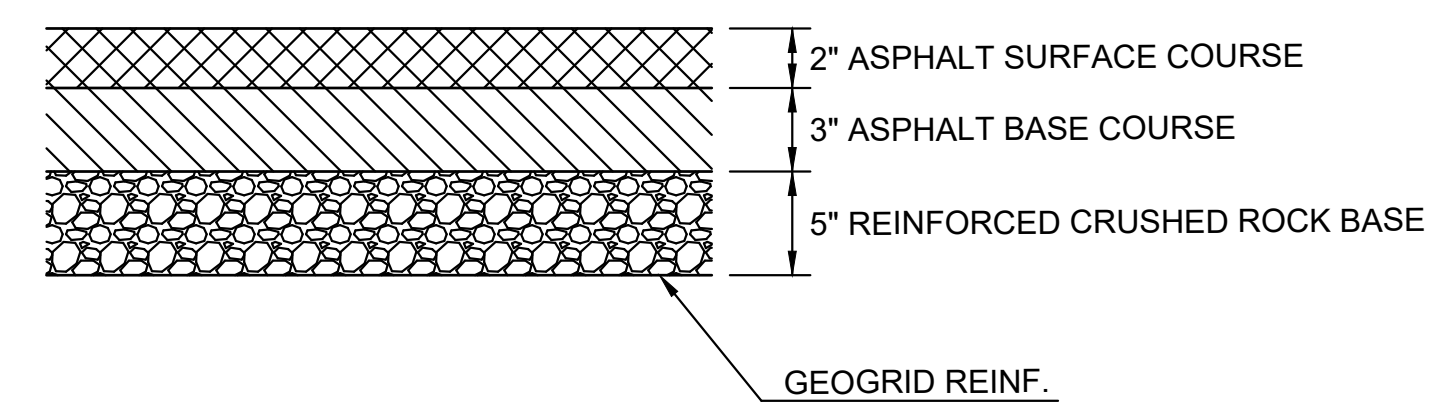
Rock base is to be compacted and smoothed with a steel faced roller prior to placement of asphalt. Tack coat will not be applied to rock base.

A tack coat of emulsified asphalt (SC-1H or CSS-1H) shall be applied to an approximate rate of 0.05 gallons per square yard between each lifts of asphaltic material.

Bituminous base and asphaltic concrete wearing surface shall be placed with a laydown machine having automatic controls for line and grade.

Construction joints in each lift shall be staggered a minimum distance of one (1) foot from joints in preceding lifts and placed so that a joint will be constructed on the centerline of the top lift.

The asphaltic concrete pavement between the combined curb and gutter shall be paid as square yards of of pavement.



PAVEMENT OPTION
ASPHALTIC CONCRETE PAVEMENT

EARTHWORK SUMMARY (C.Y.)		
	Excavation	Compacted Fill (95%)
Yale	702 CY	702 CY
Merilyn	618 CY	618 CY
Patrick	266 CY	266 CY
61st. St.	196 CY	196 CY
GRAND TOTAL	1,782 CY	1,782 CY

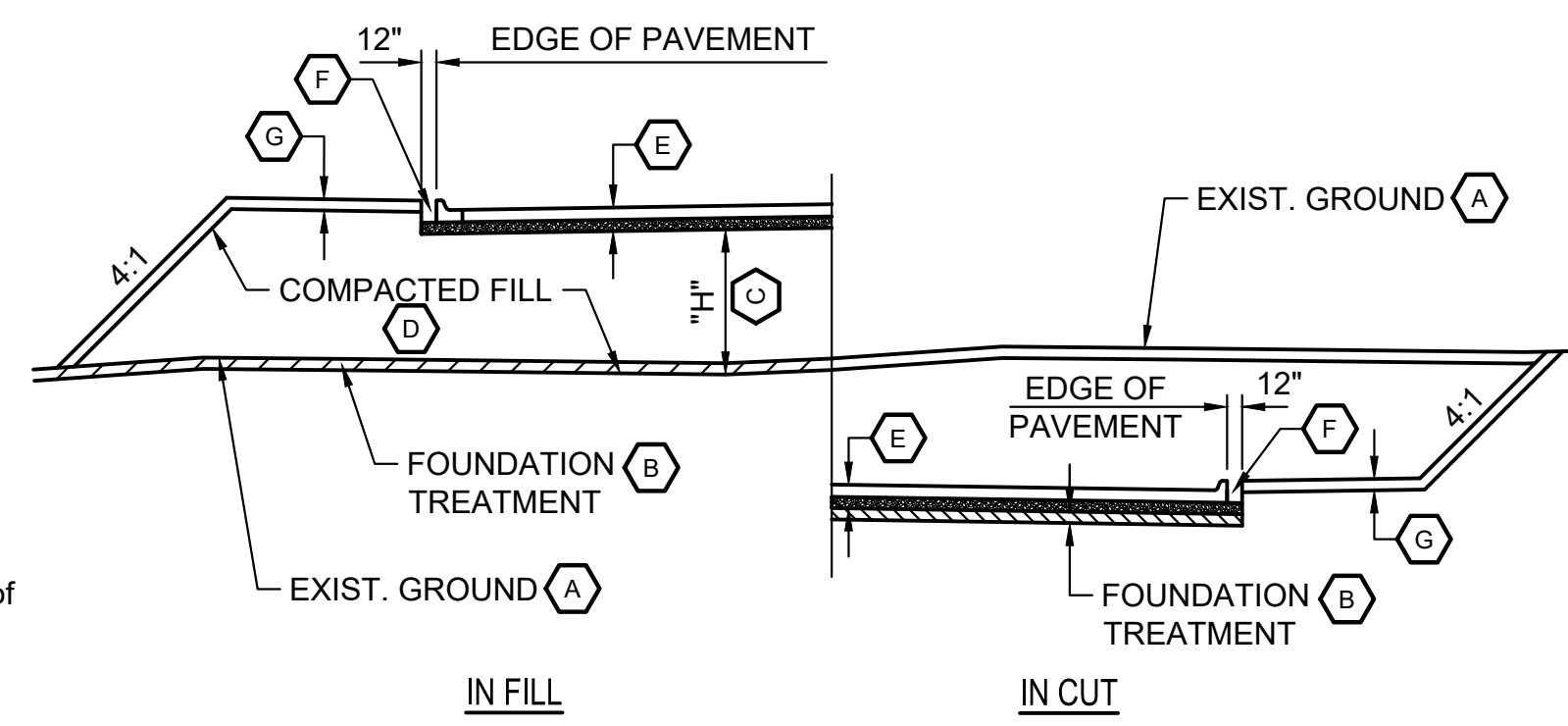
NO SHRINKAGE FACTOR HAS BEEN APPLIED.

REFER TO STORM WATER DRAIN PLANS FOR SWANEY FARM ADDITION FOR MASS GRADING (458-2024-085570).

THE QUANTITIES SHOWN FOR "EXCAVATION" ARE MEASURED FROM THE BOTTOM OF SUBGRADE LINE TO THE FINISHED SUBGRADE LINE (ROCK BASE THICKNESS). THIS MATERIAL IS INTENDED TO BE PLACED AS BACKFILL BETWEEN THE BACK OF CURB AND THE EDGE OF THE STREET RIGHT-OF-WAY.

ANY DOUBLE-HANDLING AND/OR STOCKPILING OF EXCAVATED MATERIAL WHICH MAY BE REQUIRED WILL NOT BE MEASURED OR PAID FOR SEPARATELY, BUT SHALL BE SUBSIDIARY TO THE PROJECT.

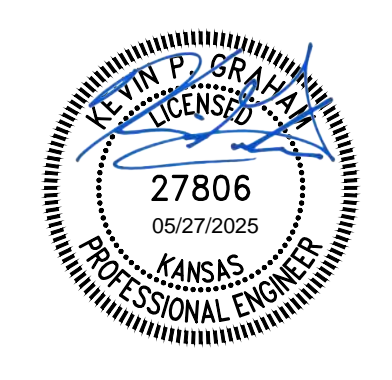
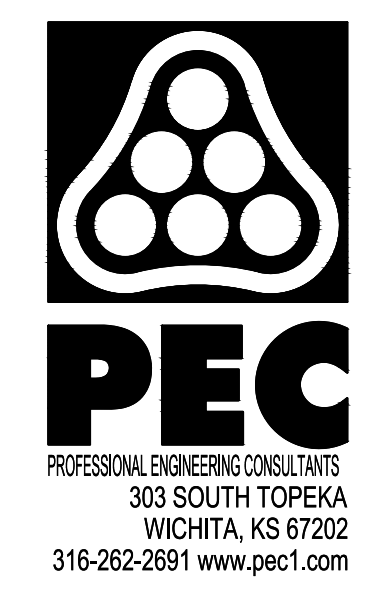
- * Non-arterial streets
BC-1, SC-1 AND PG 64-22
 - Δ Arterial streets
BM-2 PG 64-22(Base) PG 70-28(Surface)
- Base Course thicker than 4" shall be installed in two lifts



FOUNDATION TREATMENT & COMPACTION DIAGRAM

- (A) STRIP TOPSOIL.
- (B) SCARIFY AND COMPACT (6 INCHES) TO 95% ASTM D698.
- (C) "H" EQUALS FULL EMBANKMENT HEIGHT.
- (D) COMPACT TO 95% ASTM D698.
- (E) PAVEMENT SECTION, SEE TYPICAL SECTIONS THIS SHEET.
- (F) BACKFILL WITH SATISFACTORY SOIL. COMPACT TO 95% ASTM D698.
- (G) PLACE TOPSOIL.

FOUNDATION TREATMENT & COMPACTION SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM "EXCAVATION".



PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
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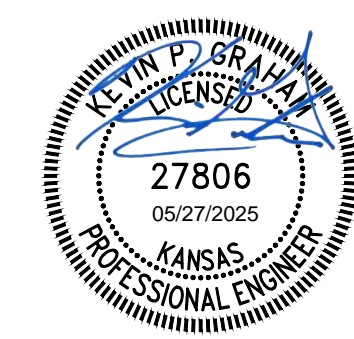
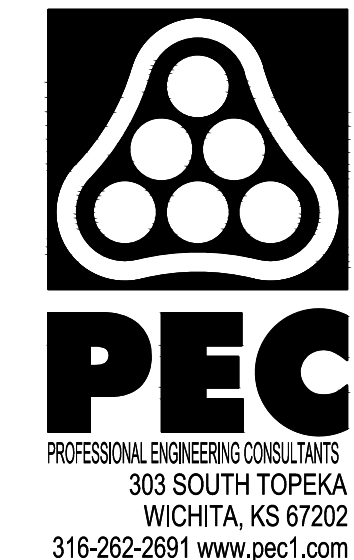
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DRAWN BY	BJS
CHECKED BY	KMS

TYPICAL SECTIONS
CP004
 04 OF 71

SAVED 8/9/2024 3:49:44 PM BY BILL SEXSON
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 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\06-200605-004-CP-102 COORDINATE GEOMETRY TABLES.DWG

	1	2	3	4	5	6																		
A	CENTERLINE-MERILYN COORDINATE LIST			CENTERLINE-YALE COORDINATE LIST			CENTERLINE-PATRICK COORDINATE LIST			CENTERLINE-61ST ST. SOUTH COORDINATE LIST														
	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING												
	6000	332,597.9654	17,559,699.8139	6100	332,070.0694	17,559,986.8143	6200	332,226.5699	17,559,986.0195	6300	333,310.1160	17,559,698.5808												
	6001	332,598.2550	17,559,769.4471	6101	332,226.5699	17,559,986.0195	6201	332,228.4772	17,560,163.5953	6301	333,312.1157	17,559,844.7902												
	6002	332,590.7756	17,559,807.8998	6102	333,218.7854	17,559,980.9805	6202	332,331.9944	17,560,265.4877	6302	333,347.9743	17,559,940.5122												
	6003	332,569.8501	17,559,917.1558	6103	333,315.3563	17,559,945.1154	6203	332,573.5751	17,560,264.2608	6303	333,406.2502	17,560,008.8470												
	6004	332,570.5710	17,559,984.2725	6104	333,337.3071	17,559,926.3957																		
	6005	332,573.5783	17,560,264.2608																					
	6006	332,575.1333	17,560,409.0371																					
	6007	332,566.7775	17,560,562.1664																					
B	BACK OF CURB RT.-MERILYN COORDINATE LIST			BACK OF CURB LT.-MERILYN COORDINATE LIST			BACK OF CURB LT.-YALE COORDINATE LIST			BACK OF CURB RT.-YALE COORDINATE LIST			BACK OF CURB RT.-PATRICK COORDINATE LIST			BACK OF CURB LT.-PATRICK COORDINATE LIST			BACK OF CURB RT.-61ST ST. COORDINATE LIST					
	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING
	7000	332,553.5353	17,559,711.9508	7100	332,642.7456	17,559,712.0479	7200	332,057.4307	17,559,969.3783	7300	332,057.6084	17,560,004.3779	7400	332,212.4702	17,560,023.3662	7450	332,241.4685	17,560,023.0548	7500	333,263.5952	17,559,710.7308			
	7001	332,553.5409	17,559,713.9508	7101	332,642.7382	17,559,714.0479	7201	332,061.4306	17,559,969.3580	7301	332,061.6084	17,560,004.3575	7401	332,213.9781	17,560,163.7510	7451	332,242.9764	17,560,163.4396	7501	333,263.5903	17,559,742.4085			
	7002	332,583.6249	17,559,743.7987	7102	332,612.6273	17,559,744.1997	7202	332,516.9362	17,559,967.0447	7302	332,192.8682	17,560,003.6909	7402	332,332.0680	17,560,279.9875	7452	332,331.9207	17,560,250.9879	7502	333,293.5150	17,559,742.4085			
	7003	332,583.6276	17,559,744.3466	7103	332,612.7548	17,559,769.3737	7203	332,526.8736	17,559,969.5567	7303	332,212.4661	17,560,022.9812	7403	332,539.4593	17,560,278.9343	7453	332,539.3121	17,560,249.9347	7503	333,293.5282	17,559,743.6349			
	7004	332,583.7552	17,559,769.5206	7104	332,604.1907	17,559,813.4029	7204	332,536.3262	17,559,971.9462	7304	332,212.4702	17,560,023.3662							7504	333,294.6167	17,559,844.9782			
	7005	332,577.3605	17,559,802.3966	7105	332,584.3493	17,559,916.9990	7205	332,604.6888	17,559,971.5991	7305	332,241.4685	17,560,023.0548										7505	333,308.4524	17,559,909.8614
	7006	332,555.3509	17,559,917.3126	7106	332,584.7112	17,559,950.7002	7206	332,614.1167	17,559,969.1136	7306	332,260.8684	17,560,003.3456										7506	333,298.9398	17,559,935.9053
	7007	332,555.7111	17,559,950.8475	7107	332,604.3264	17,559,971.6009	7207	332,624.0281	17,559,966.5008	7307	332,517.1139	17,560,002.0442										7507	333,320.3139	17,559,963.6208
C	BACK OF CURB LT.-61ST ST. COORDINATE LIST			BACK OF SIDEWALK-MERILYN COORDINATE LIST			BACK OF SIDEWALK-YALE COORDINATE LIST			BACK OF SIDEWALK-PATRICK COORDINATE LIST			BACK OF SIDEWALK-61ST ST. COORDINATE LIST											
	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING	POINT	NORTHING	EASTING									
	7550	333,358.5404	17,559,710.9368	8000	332,627.2546	17,559,769.3002	8100	332,070.0060	17,559,954.8143	8200	332,255.8703	17,560,013.8304	8300	333,279.3463	17,559,773.3402									
	7551	333,358.5394	17,559,712.9368	8001	332,617.6067	17,559,818.9041	8101	332,549.0751	17,559,952.3812	8201	332,257.4756	17,560,163.2838	8301	333,280.1175	17,559,845.1339									
	7552	333,328.5262	17,559,743.2590	8002	332,598.8484	17,559,916.8443	8102	332,591.3795	17,559,952.1664	8202	332,331.8471	17,560,236.4881	8302	333,297.4517	17,559,920.7203									
	7553	333,329.6147	17,559,844.6023	8003	332,599.2274	17,559,952.1265	8103	332,594.2274	17,559,952.1519	8203	332,544.2668	17,560,235.4093	8303	333,329.0573	17,559,967.6454									
	7554	333,361.2897	17,559,929.1566	8004	332,541.2809	17,559,957.4209	8104	332,599.2274	17,559,952.1265				8304	333,381.9019	17,560,029.6113									
	7555	333,419.5658	17,559,997.4915	8005	332,541.3423	17,559,963.1328	8105	333,218.6228	17,559,948.9809															
				8006	332,541.7989	17,560,005.6501	8106	333,286.9044	17,559,926.7918															
				8007	332,544.2668	17,560,235.4093	8107	333,294.2189	17,559,922.3527															
D				8008	332,544.2921	17,560,240.4092	8108	333,297.4517	17,559,920.7203															
				8009	332,544.7994	17,560,287.6386																		
				8010	332,546.1249	17,560,408.4021																		
				8011	332,537.7690	17,560,561.5314																		
				8012	332,539.8643	17,560,756.6023																		
				8013	332,695.5094	17,560,908.9394																		
				8014	332,713.5083	17,560,908.7460																		
E																								

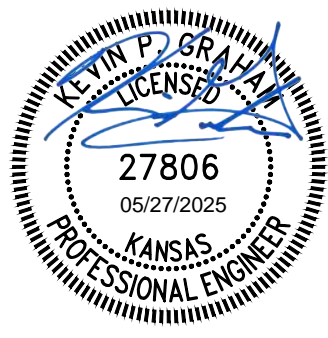
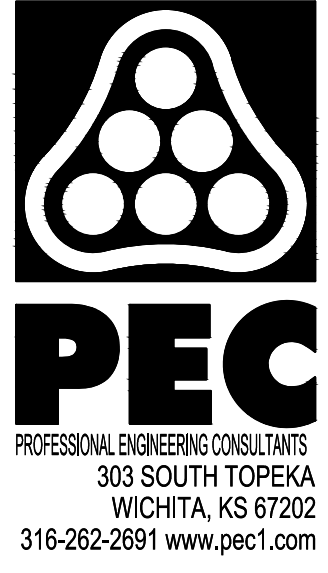
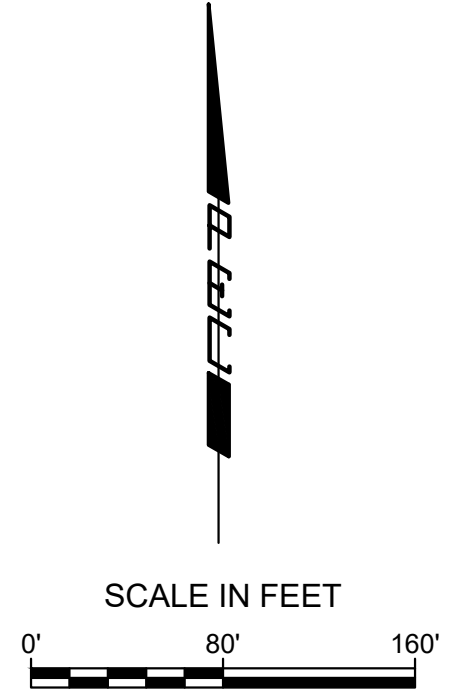
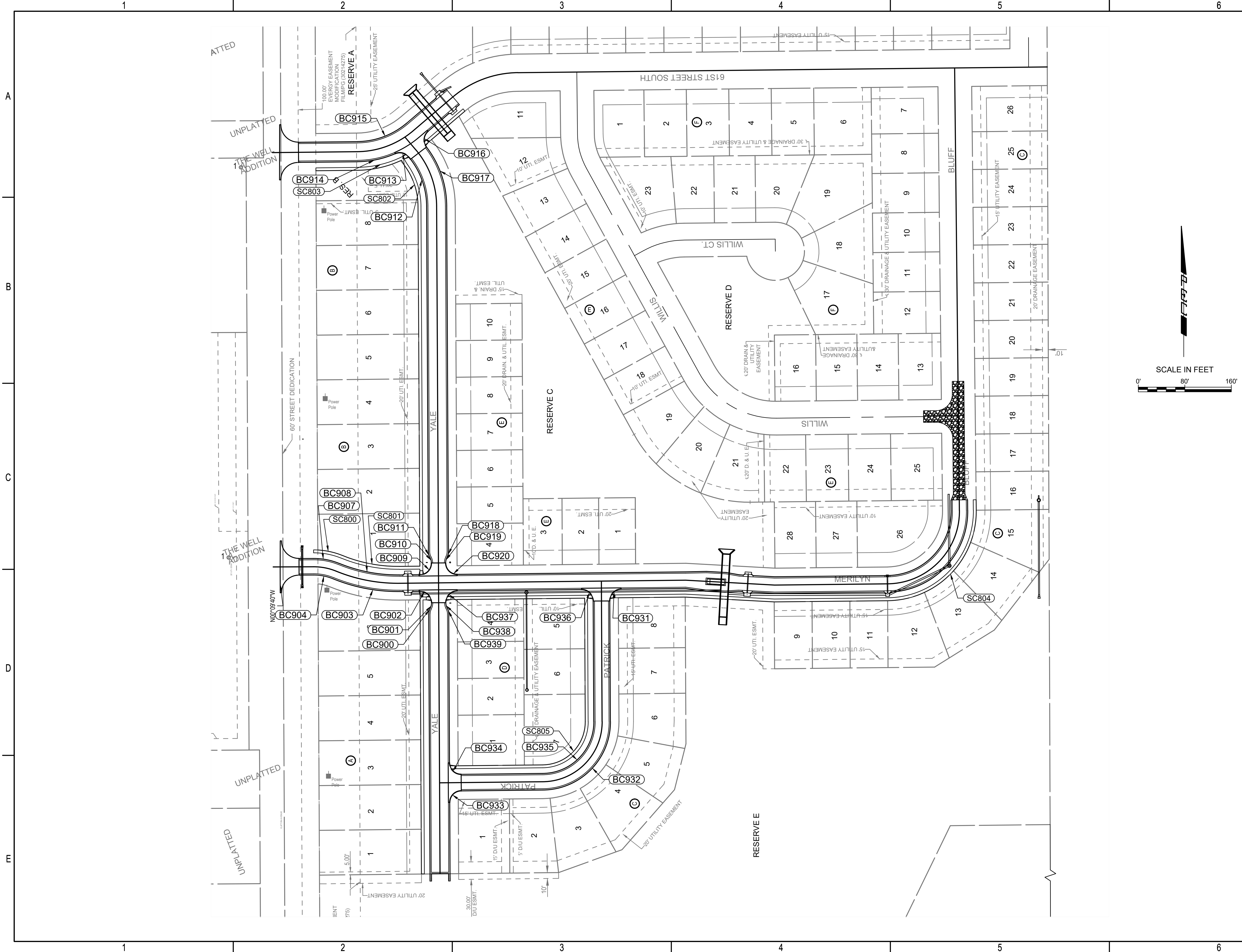


PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

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 PLOTTED 5/27/2025 9:30:48 AM BY KEVIN GRAHAM
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 GEOMETRY CURVE PLAN.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:									

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

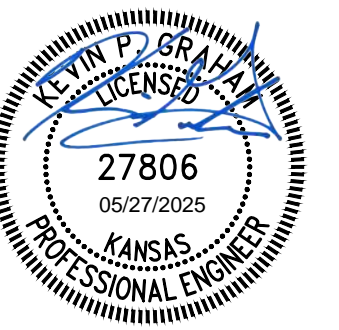
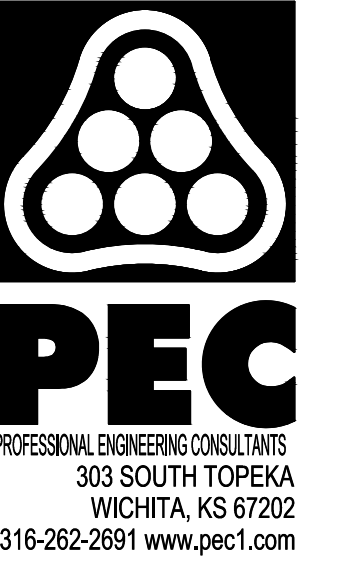
COORDINATE GEOMETRY CURVE PLAN

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 U:\WICHITA-CIVIL\2020\200605\004\2PPD3_PLANS\0301_PAVING PHASE 1\08-200605-004-CP-104 COORDINATE
 GEOMETRY CURVE TABLES.DWG

CURVE DATA								
CURVE #	DELTA	RADIUS (FT.)	T (FT.)	LENGTH (FT.)	P.C. NORTHING	P.C. EASTING	P.T. NORTHING	P.T. EASTING
BC900	028°57'18"	20.50	5.29	10.36	332,526.8736	17,559,969.5567	332,516.9362	17,559,967.0447
BC901	028°57'18"	19.50	5.03	9.85	332,526.8736	17,559,969.5567	332,536.3262	17,559,971.9462
BC902	090°19'28"	19.50	19.61	30.74	332,536.3262	17,559,971.9462	332,555.7261	17,559,952.2371
BC903	022°55'27"	294.40	59.69	117.79	332,577.3605	17,559,802.3966	332,555.3509	17,559,917.3126
BC904	022°35'41"	85.48	17.08	33.71	332,577.3605	17,559,802.3966	332,583.7552	17,559,769.5206
BC907	022°35'41"	114.48	22.87	45.15	332,604.1907	17,559,813.4029	332,612.7548	17,559,769.3737
BC908	022°55'27"	265.40	53.81	106.19	332,604.1907	17,559,813.4029	332,584.3493	17,559,916.9990
BC909	089°40'32"	19.50	19.39	30.52	332,584.7285	17,559,952.3106	332,604.3264	17,559,971.6009
BC910	028°57'18"	19.50	5.03	9.85	332,604.6888	17,559,971.5991	332,614.1167	17,559,969.1136
BC911	028°57'18"	20.50	5.29	10.36	332,624.0281	17,559,966.5008	332,614.1167	17,559,969.1136
BC912	037°20'54"	132.50	44.78	86.37	333,218.6965	17,559,963.4807	333,298.9398	17,559,935.9053
BC913	073°31'19"	19.50	14.57	25.02	333,303.0824	17,559,932.5759	333,308.4524	17,559,909.8614
BC914	022°50'39"	167.50	33.84	66.78	333,294.6167	17,559,844.9782	333,308.4524	17,559,909.8614
BC915	039°50'32"	132.50	48.02	92.14	333,329.6147	17,559,844.6023	333,361.2897	17,559,929.1566
BC916	087°10'54"	19.50	18.56	29.67	333,347.0596	17,559,966.4090	333,320.3139	17,559,963.6208
BC917	037°20'54"	167.50	56.61	109.19	333,218.8742	17,559,998.4802	333,320.3139	17,559,963.6208
BC918	028°57'18"	20.50	5.29	10.36	332,614.2684	17,559,998.9883	332,624.2058	17,560,001.5003
BC919	028°57'18"	19.50	5.03	9.85	332,614.2684	17,559,998.9883	332,604.8158	17,559,996.5987
BC920	090°19'28"	19.50	19.61	30.74	332,604.8158	17,559,996.5987	332,585.4159	17,560,016.3079
BC931	089°40'32"	19.50	19.39	30.52	332,559.4423	17,560,298.2226	332,539.8444	17,560,278.9323

CURVE DATA								
CURVE #	DELTA	RADIUS (FT.)	T (FT.)	LENGTH (FT.)	P.C. NORTHING	P.C. EASTING	P.T. NORTHING	P.T. EASTING
BC932	089°40'32"	117.50	116.84	183.90	332,213.9781	17,560,163.7510	332,332.0680	17,560,279.9875
BC933	089°40'32"	19.50	19.39	30.52	332,212.4661	17,560,022.9812	332,192.8682	17,560,003.6909
BC934	090°19'28"	19.50	19.61	30.74	332,260.8684	17,560,003.3456	332,241.4685	17,560,023.0548
BC935	089°40'32"	88.50	88.00	138.51	332,242.9764	17,560,163.4396	332,331.9207	17,560,250.9879
BC936	090°19'28"	19.50	19.61	30.74	332,539.3121	17,560,249.9347	332,558.7119	17,560,230.2255
BC937	089°40'32"	19.50	19.39	30.52	332,556.4135	17,560,016.2344	332,536.8155	17,559,996.9441
BC938	028°57'18"	19.50	5.03	9.85	332,536.4532	17,559,996.9459	332,527.0253	17,559,999.4313
BC939	028°57'18"	20.50	5.29	10.36	332,517.1139	17,560,002.0442	332,527.0253	17,559,999.4313

CURVE DATA								
CURVE #	DELTA	RADIUS (FT.)	T (FT.)	LENGTH (FT.)	P.C. NORTHING	P.C. EASTING	P.T. NORTHING	P.T. EASTING
SC800	022°35'41"	128.98	25.77	50.86	332,617.6059	17,559,818.9061	332,627.2546	17,559,769.3002
SC801	022°55'27"	250.90	50.87	100.38	332,617.6059	17,559,818.9061	332,598.8484	17,559,916.8422
SC802	035°25'22"	118.00	37.68	72.95	333,218.6228	17,559,948.9809	333,286.9044	17,559,926.7918
SC803	026°10'34"	177.00	41.15	80.86	333,285.1172	17,559,845.0802	333,304.1074	17,559,922.9609
SC804	090°00'00"	154.00	154.00	241.90	332,539.8643	17,560,756.6023	332,695.5094	17,560,908.9394
SC805	089°40'32"	74.00	73.58	115.82	332,257.4756	17,560,163.2838	332,331.8471	17,560,236.4881



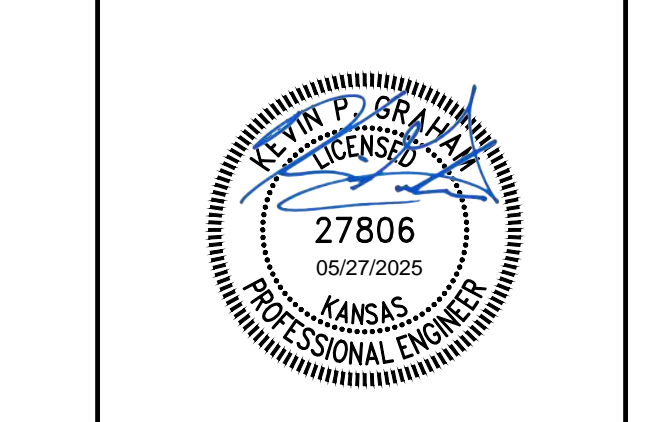
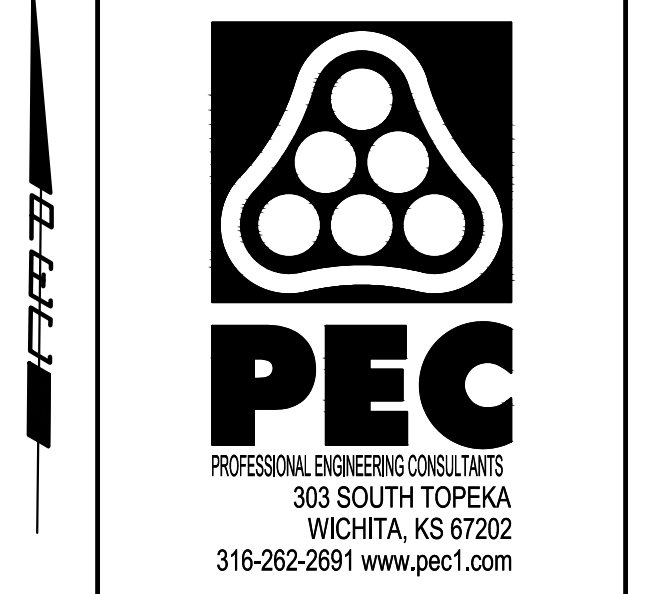
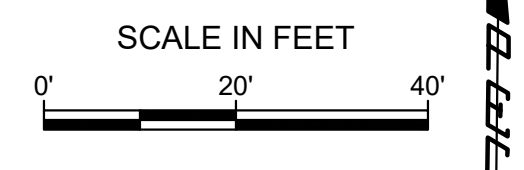
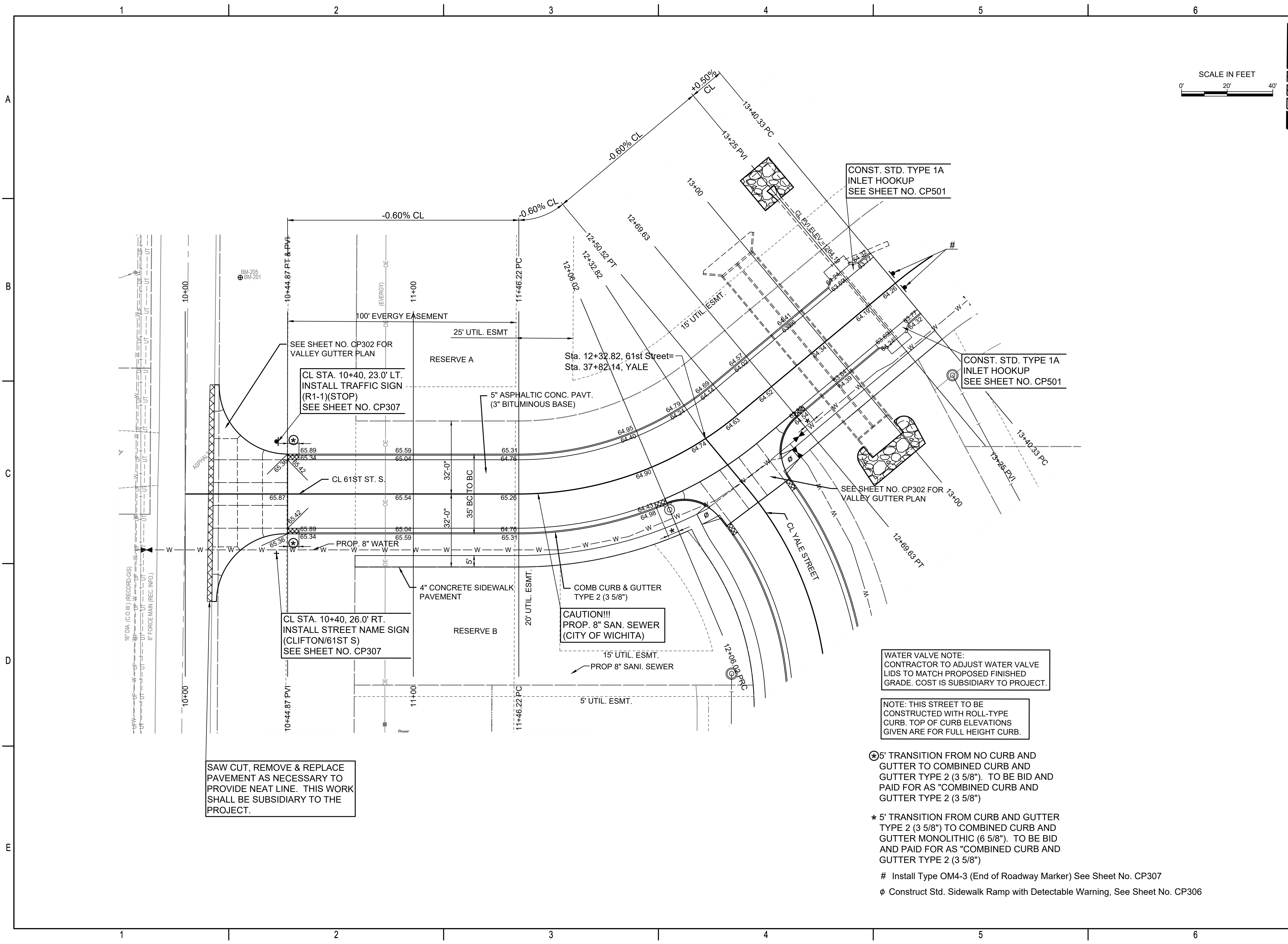
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

COORDINATE GEOMETRY CURVE TABLES

SAVED 12/24/2024 9:14:21 AM BY KEVIN.GRAHAM
 PLOTTED 5/27/2025 9:31:37 AM BY KEVIN.GRAHAM
 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\09-200605-004-CP201 61ST STREET SOUTH.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:			

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

61ST STREET SOUTH PLAN

CP201
09 OF 71

SAW CUT, REMOVE & REPLACE PAVEMENT AS NECESSARY TO PROVIDE NEAT LINE. THIS WORK SHALL BE SUBSIDIARY TO THE PROJECT.

WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE LIDS TO MATCH PROPOSED FINISHED GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.

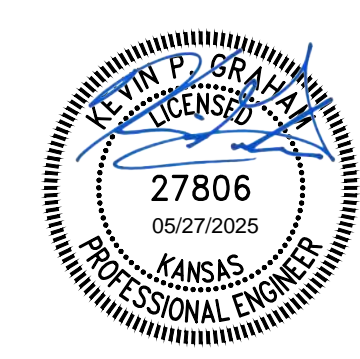
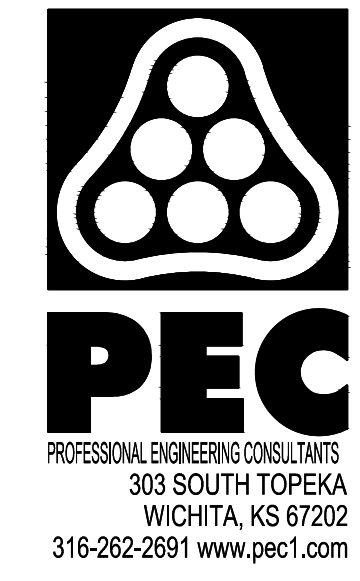
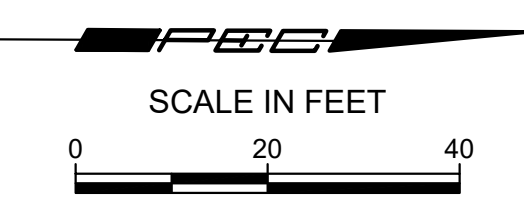
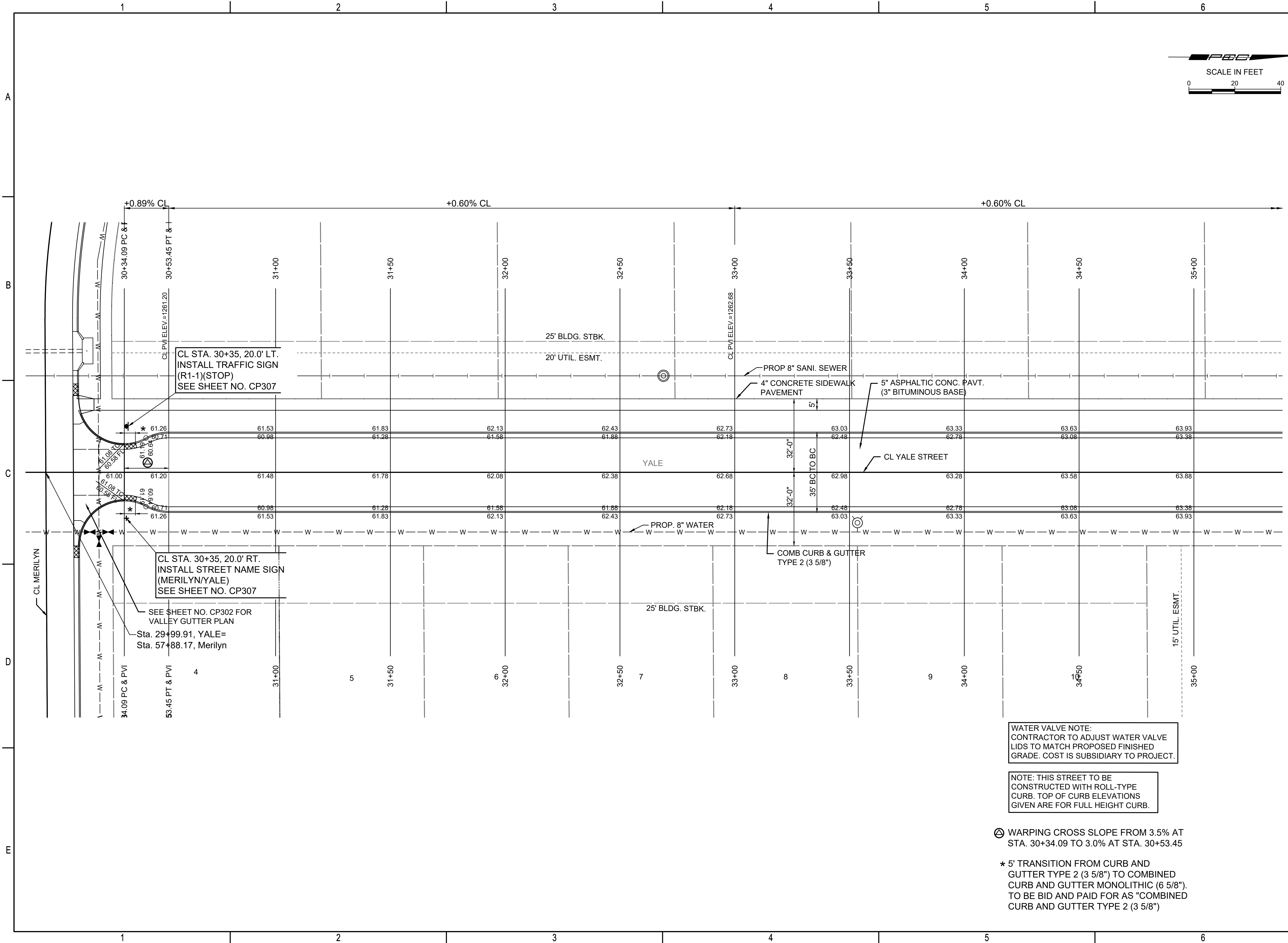
⊕ 5' TRANSITION FROM NO CURB AND GUTTER TO COMBINED CURB AND GUTTER TYPE 2 (3 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER TYPE 2 (3 5/8")

* 5' TRANSITION FROM CURB AND GUTTER TYPE 2 (3 5/8") TO COMBINED CURB AND GUTTER MONOLITHIC (6 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER TYPE 2 (3 5/8")

Install Type OM4-3 (End of Roadway Marker) See Sheet No. CP307

⊕ Construct Std. Sidewalk Ramp with Detectable Warning, See Sheet No. CP306

SAVED 12/24/2024 9:11:36 AM BY KEVIN.GRAHAM
 PLOTTED 5/27/2025 9:32:49 AM BY KEVIN.GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PLANS\0301_PAVING PHASE 1\11-200605-004-CP203 YALE STREET PLAN.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

YALE STREET PLAN

CP203
11 OF 71

WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE LIDS TO MATCH PROPOSED FINISHED GRADE. COST IS SUBSIDIARY TO PROJECT.

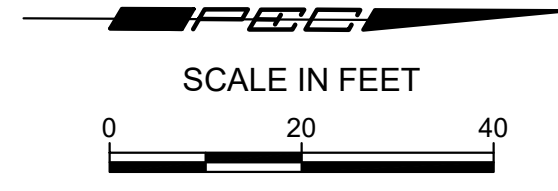
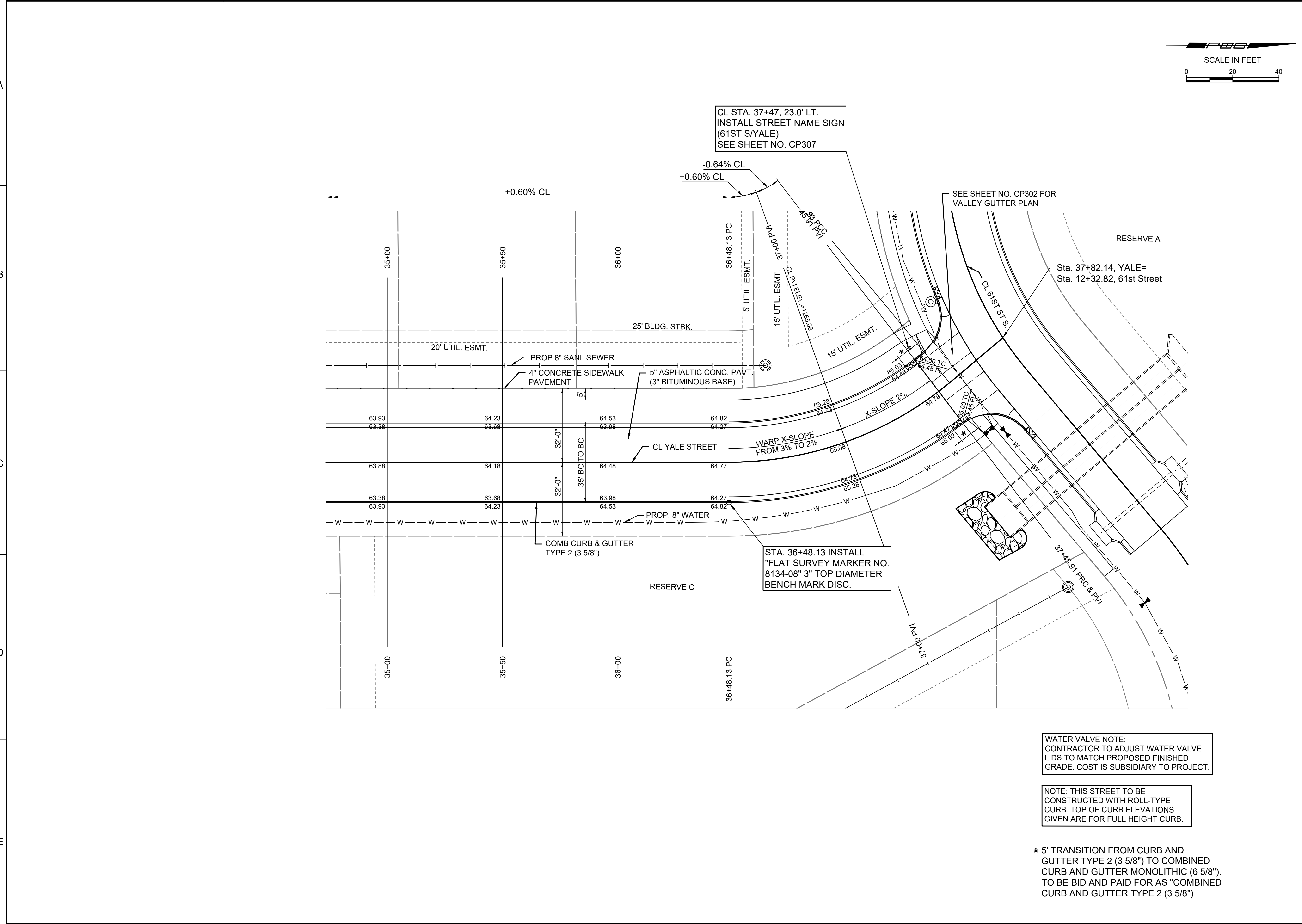
NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.

⊗ WARPING CROSS SLOPE FROM 3.5% AT STA. 30+34.09 TO 3.0% AT STA. 30+53.45

* 5' TRANSITION FROM CURB AND GUTTER TYPE 2 (3 5/8") TO COMBINED CURB AND GUTTER MONOLITHIC (6 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER TYPE 2 (3 5/8")

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SAVED 12/24/2024 9:11:30 AM BY KEVIN.GRAHAM
 PLOTTED 5/27/2025 9:33:09 AM BY KEVIN.GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PPD3_PLANS\0301_PAVING PHASE 1\12-200605-004-CP204_YALE STREET PLAN.DWG



CL STA. 37+47, 23.0' LT.
 INSTALL STREET NAME SIGN
 (61ST S/YALE)
 SEE SHEET NO. CP307

SEE SHEET NO. CP302 FOR
 VALLEY GUTTER PLAN

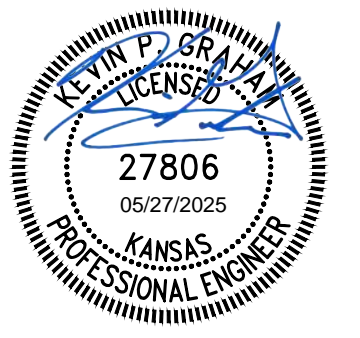
Sta. 37+82.14, YALE=
 Sta. 12+32.82, 61st Street

STA. 36+48.13 INSTALL
 "FLAT SURVEY MARKER NO.
 8134-08" 3" TOP DIAMETER
 BENCH MARK DISC.

WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE
 LIDS TO MATCH PROPOSED FINISHED
 GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE
 CONSTRUCTED WITH ROLL-TYPE
 CURB. TOP OF CURB ELEVATIONS
 GIVEN ARE FOR FULL HEIGHT CURB.

* 5' TRANSITION FROM CURB AND
 GUTTER TYPE 2 (3 5/8") TO COMBINED
 CURB AND GUTTER MONOLITHIC (6 5/8").
 TO BE BID AND PAID FOR AS "COMBINED
 CURB AND GUTTER TYPE 2 (3 5/8")



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

**SWANEY FARM ADDITION
 PHASE 1**

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

YALE STREET PLAN
CP204
 12 OF 71

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64+80 PVI
65+00
65+50
65+60.45 PC & PVI
66+00
66+50
67+00
67+50
67+74.80

CL PVI ELEV.=1262.80
CL PVI ELEV.=1262.17
CL PVI ELEV.=1263.25
CL PVI ELEV.=1263.14

+0.50% CL
-0.78% CL
+0.55% CL
-0.60% CL

CL MERILYN STREET
CL BLUFF

25' BLDG. STBK
15' UTIL. ESMT.
10' UTIL. ESMT.
20' UTIL. ESMT.
15' BLDG. STBK.
25' BLDG. STBK.

4" CONCRETE SIDEWALK
PAVEMENT
15' UTIL. ESMT.
29'-0" BC TO BC
29'-0"
PROP 8" WATER
PROP 8" SAN. SEWER
5" ASPHALTIC CONC. PAVT.
(3" BITUMINOUS BASE)

CONST. GRATED DRIVEWAY
INLET HOOKUP
SEE SHEET NO. CP503

CONST. STD. TYPE 1A
INLET HOOKUP
SEE SHEET NO. CP501

STA. 67+56.80 INSTALL
"FLAT SURVEY MARKER NO.
8134-08" 3" TOP DIAMETER
BENCH MARK DISC.

CL STA. 66+64, 22.0' RT.
INSTALL FLAT PLATE
STREET NAME SIGN
(MERILYN/BLUFF)
SEE SHEET NO. CP307

REINFORCED CRUSHED
ROCK BASE (6")

R=28.00'
▲=090°00'00"

R=28.00'
▲=090°00'00"

WATER VALVE NOTE:
CONTRACTOR TO ADJUST WATER VALVE
LIDS TO MATCH PROPOSED FINISHED
GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE
CONSTRUCTED WITH ROLL-TYPE
CURB. TOP OF CURB ELEVATIONS
GIVEN ARE FOR FULL HEIGHT CURB.

Install Type OM4-3 (End of Roadway Marker) See Sheet No. CP306

SCALE IN FEET
0 20 40

PEC
PROFESSIONAL ENGINEERING CONSULTANTS
303 SOUTH TOPEKA
WICHITA, KS 67202
316-262-2691 www.pec1.com

CITY OF
WICHITA

27806
05/27/2025
KANSAS
PROFESSIONAL ENGINEER

PAVING AND INCIDENTAL DRAINAGE
IMPROVEMENTS

SWANEY FARM ADDITION
PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS
MERILYN PLAN	
CP207	
15 OF 71	

1
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6

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64+80 PVI
65+00
65+50
65+60.45 PC & PVI
66+00
66+50
67+00
67+50
67+74.80

CL PVI ELEV.=1262.80
CL PVI ELEV.=1262.17
CL PVI ELEV.=1263.25
CL PVI ELEV.=1263.14

+0.50% CL
-0.78% CL
+0.55% CL
-0.60% CL

CL MERILYN STREET
CL BLUFF

25' BLDG. STBK
15' UTIL. ESMT.
10' UTIL. ESMT.
20' UTIL. ESMT.
15' BLDG. STBK.
25' BLDG. STBK.

4" CONCRETE SIDEWALK
PAVEMENT
15' UTIL. ESMT.
29'-0" BC TO BC
29'-0"
PROP 8" WATER
PROP 8" SAN. SEWER
5" ASPHALTIC CONC. PAVT.
(3" BITUMINOUS BASE)

CONST. GRATED DRIVEWAY
INLET HOOKUP
SEE SHEET NO. CP503

CONST. STD. TYPE 1A
INLET HOOKUP
SEE SHEET NO. CP501

STA. 67+56.80 INSTALL
"FLAT SURVEY MARKER NO.
8134-08" 3" TOP DIAMETER
BENCH MARK DISC.

CL STA. 66+64, 22.0' RT.
INSTALL FLAT PLATE
STREET NAME SIGN
(MERILYN/BLUFF)
SEE SHEET NO. CP307

REINFORCED CRUSHED
ROCK BASE (6")

R=28.00'
▲=090°00'00"

R=28.00'
▲=090°00'00"

WATER VALVE NOTE:
CONTRACTOR TO ADJUST WATER VALVE
LIDS TO MATCH PROPOSED FINISHED
GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE
CONSTRUCTED WITH ROLL-TYPE
CURB. TOP OF CURB ELEVATIONS
GIVEN ARE FOR FULL HEIGHT CURB.

Install Type OM4-3 (End of Roadway Marker) See Sheet No. CP306

SCALE IN FEET
0 20 40

PEC
PROFESSIONAL ENGINEERING CONSULTANTS
303 SOUTH TOPEKA
WICHITA, KS 67202
316-262-2691 www.pec1.com

CITY OF
WICHITA

27806
05/27/2025
KANSAS
PROFESSIONAL ENGINEER

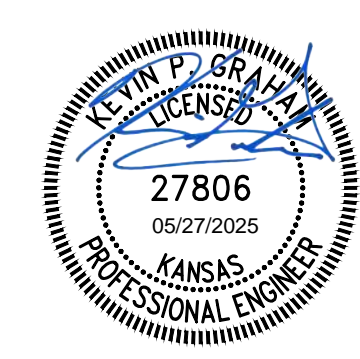
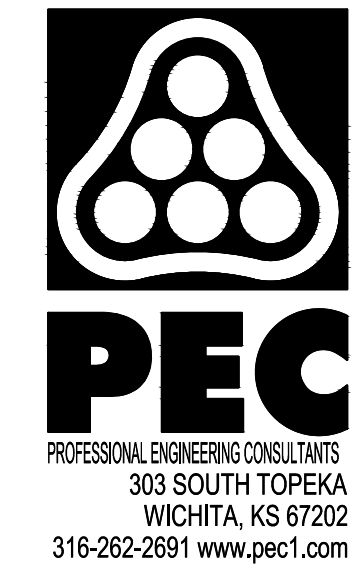
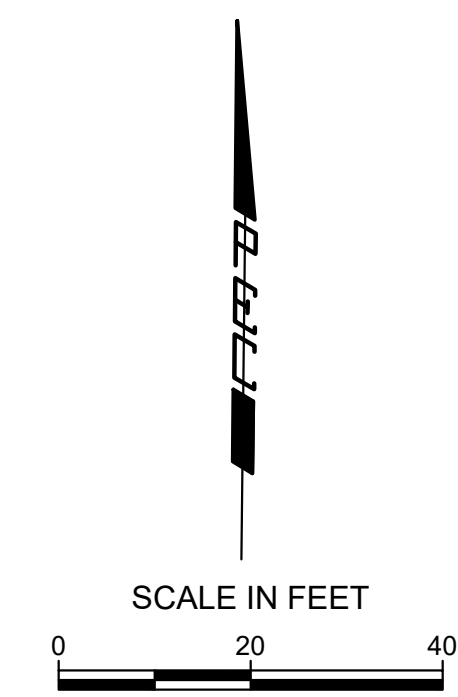
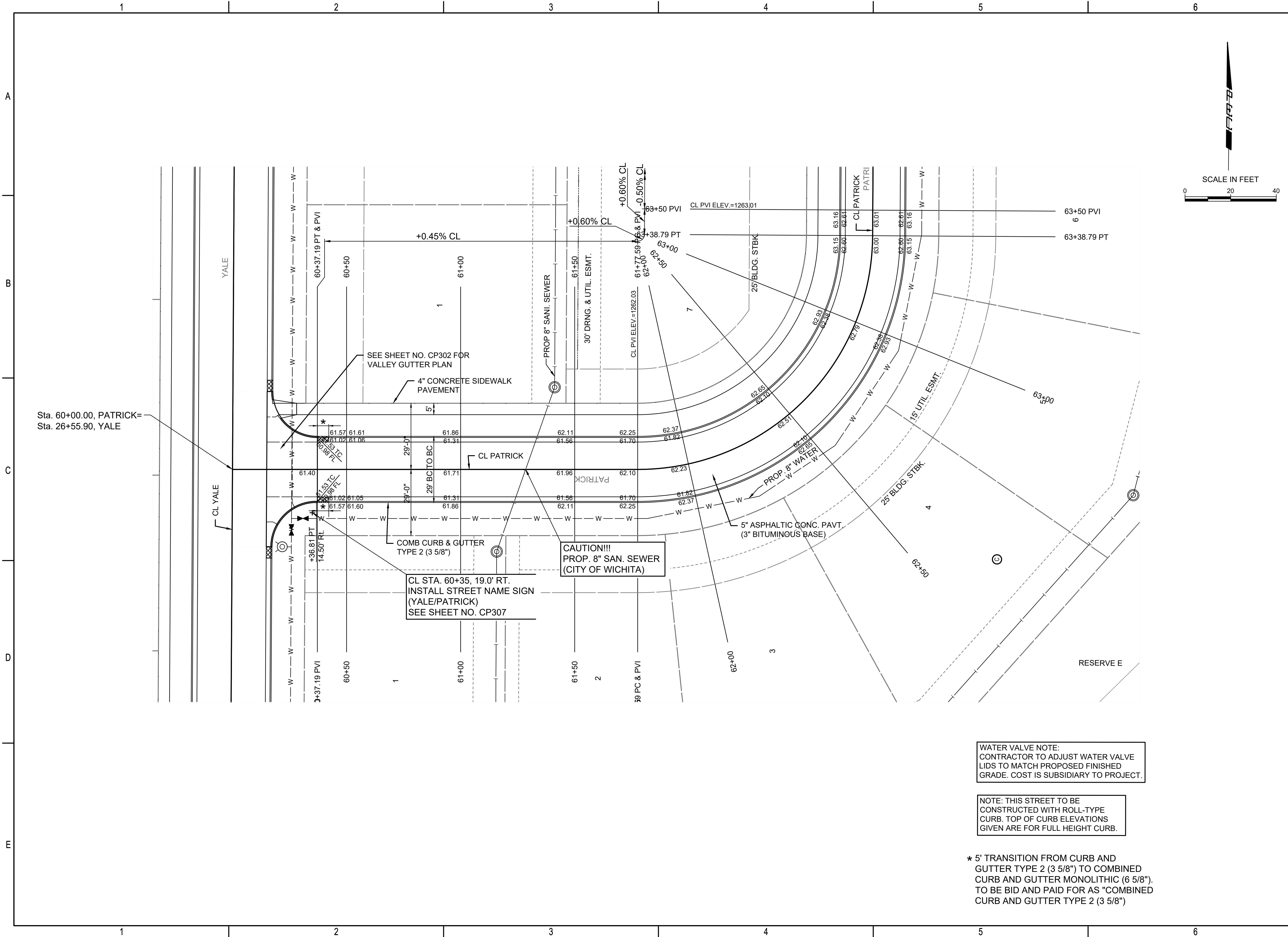
PAVING AND INCIDENTAL DRAINAGE
IMPROVEMENTS

SWANEY FARM ADDITION
PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS
MERILYN PLAN	
CP207	
15 OF 71	

SAVED 12/24/2024 9:25:59 AM BY KEVIN.GRAHAM
 PLOTTED 5/27/2025 9:35:13 AM BY KEVIN.GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PLANS\0301_PAVING PHASE 1\16-200605-004-CP208 PATRICK PLAN.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

PATRICK STREET PLAN

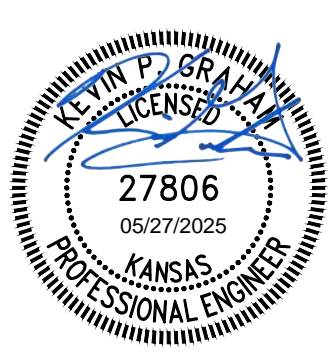
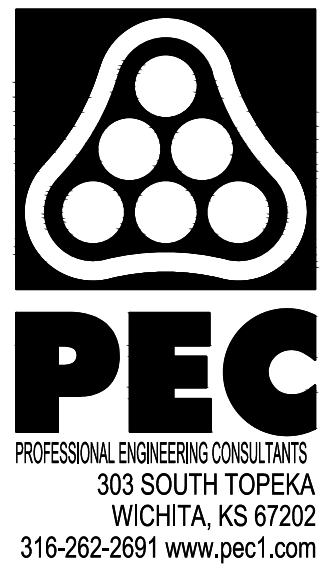
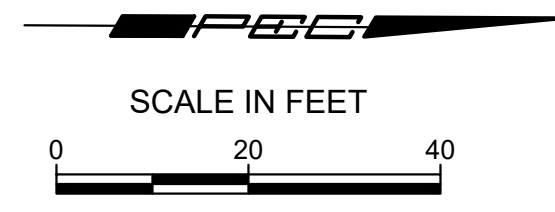
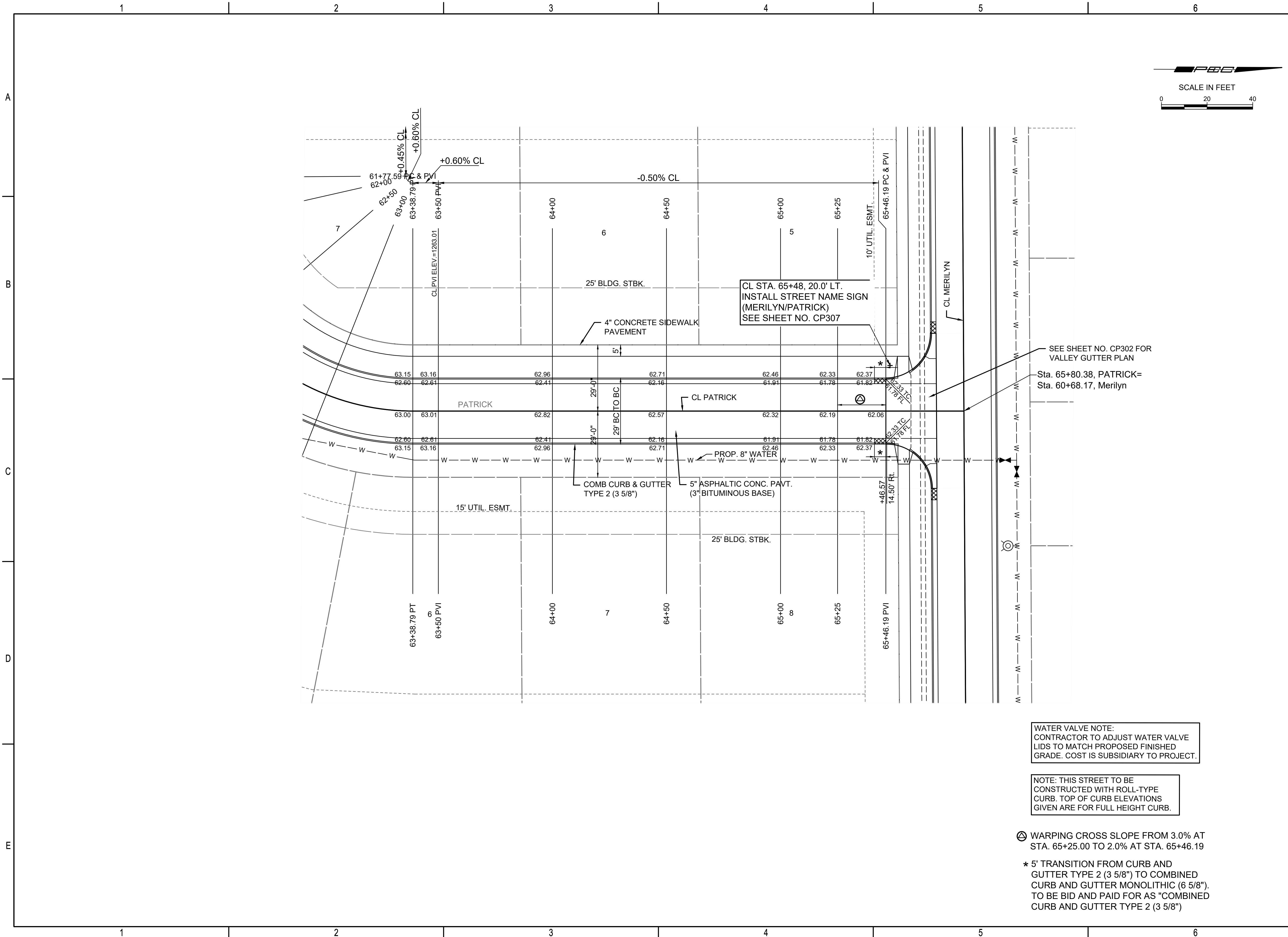
CP208
 16 OF 71

WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE LIDS TO MATCH PROPOSED FINISHED GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.

* 5' TRANSITION FROM CURB AND GUTTER TYPE 2 (3 5/8") TO COMBINED CURB AND GUTTER MONOLITHIC (6 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER TYPE 2 (3 5/8")

SAVED 12/24/2024 9:23:21 AM BY KEVIN GRAHAM
 PLOTTED 5/27/2025 9:35:45 AM BY KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PPD3_PLANS\0301_PAVING PHASE 1\17-200605-004-CP209 PATRICK PLAN.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

PATRICK STREET PLAN

CP209
17 OF 71

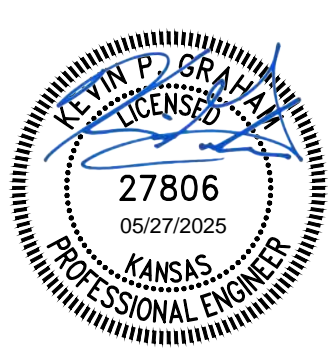
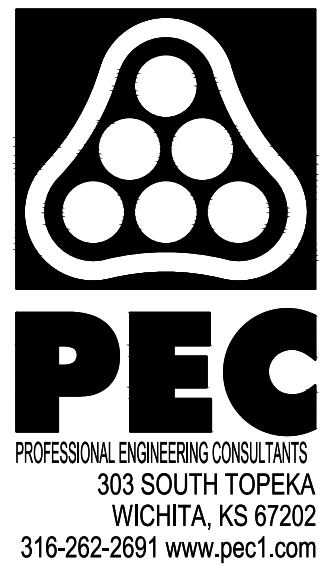
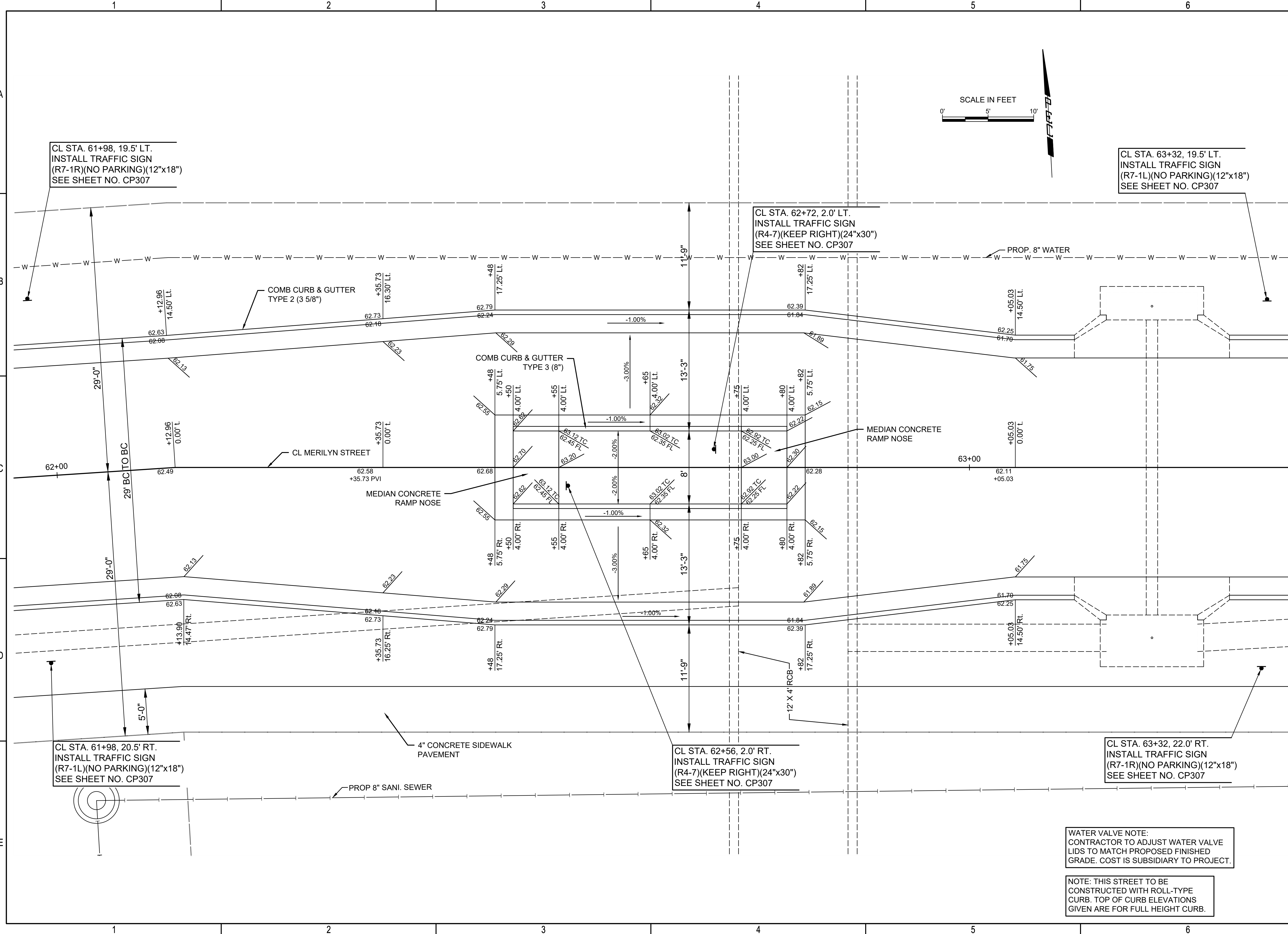
WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE LIDS TO MATCH PROPOSED FINISHED GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.

⊖ WARPING CROSS SLOPE FROM 3.0% AT STA. 65+25.00 TO 2.0% AT STA. 65+46.19

* 5' TRANSITION FROM CURB AND GUTTER TYPE 2 (3 5/8") TO COMBINED CURB AND GUTTER MONOLITHIC (6 5/8"). TO BE BID AND PAID FOR AS "COMBINED CURB AND GUTTER TYPE 2 (3 5/8")

SAVED 5/27/2025 11:23:51 AM BY KEVIN.GRAHAM
 PLOTTED 5/27/2025 11:27:58 AM BY KEVIN.GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PLANS\0301_PAVING PHASE 1\18-200605-004-CP301_MEDIAN AREA.DWG



PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

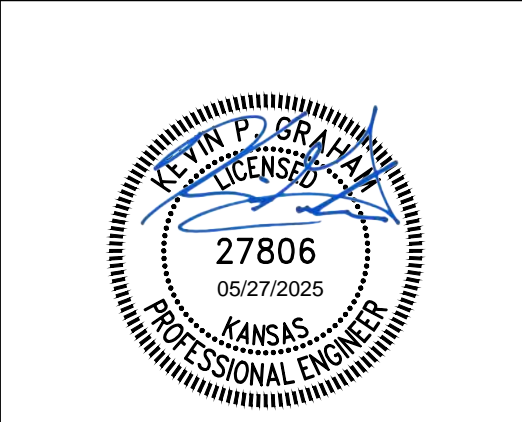
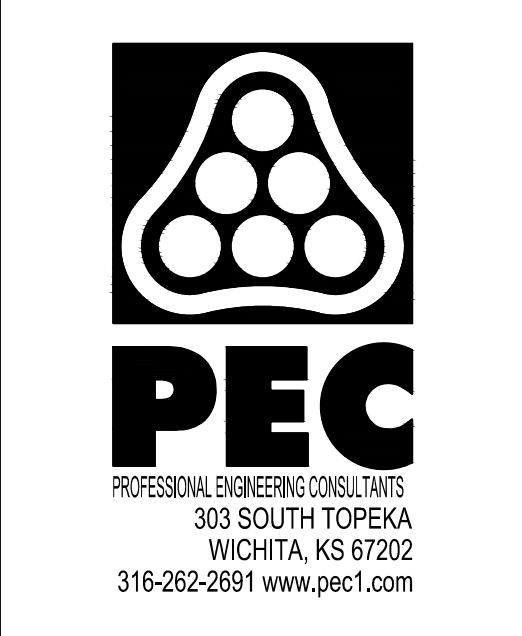
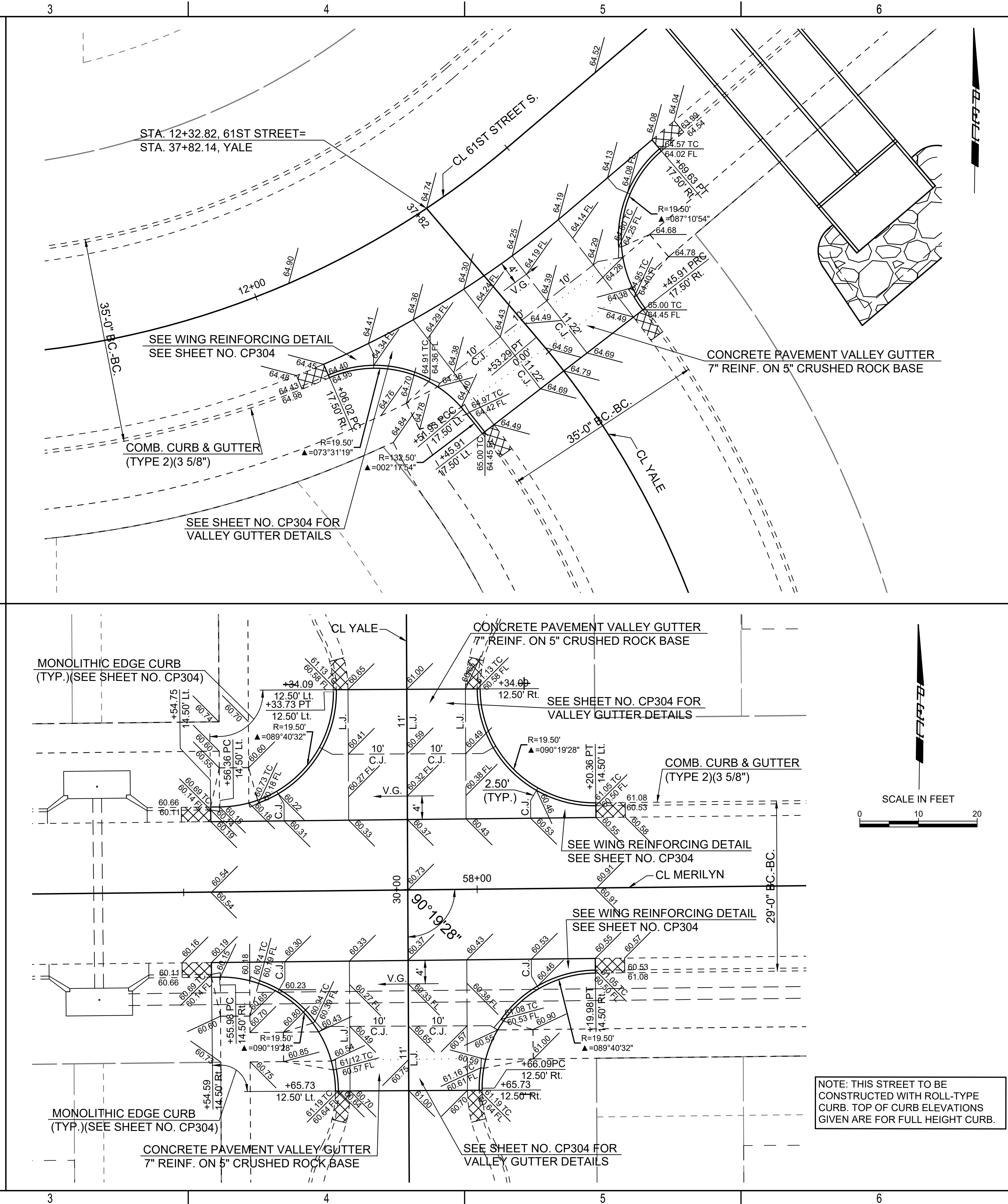
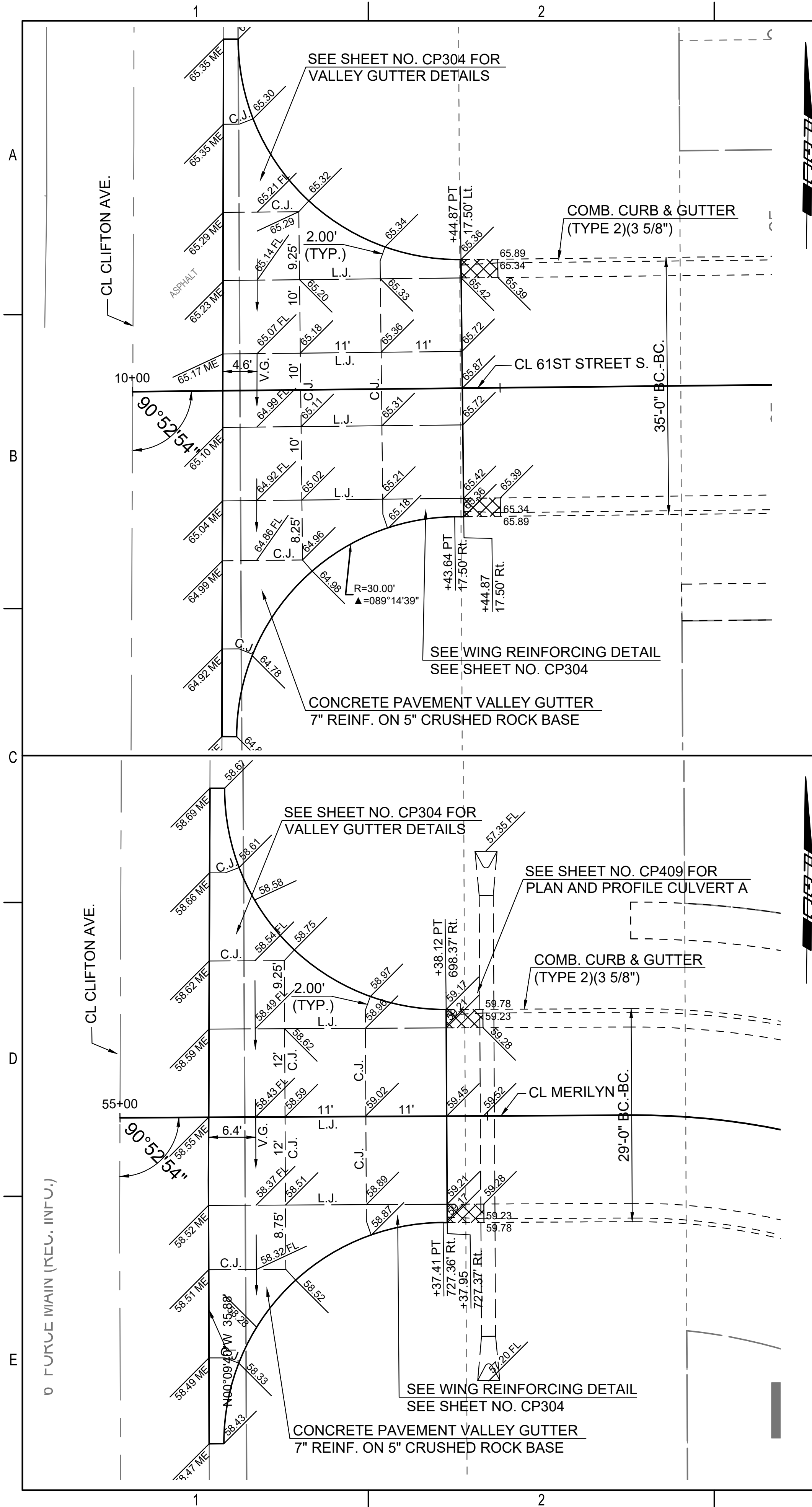
MEDIAN AREA

CP301
18 OF 71

WATER VALVE NOTE:
 CONTRACTOR TO ADJUST WATER VALVE LIDS TO MATCH PROPOSED FINISHED GRADE. COST IS SUBSIDIARY TO PROJECT.

NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.

SAVED 9/4/2024 9:01:52 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:36:42 AM BY KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\19-200605-004-CP302 VALLEY GUTTER
 PLANS.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

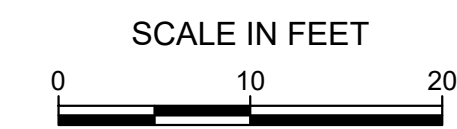
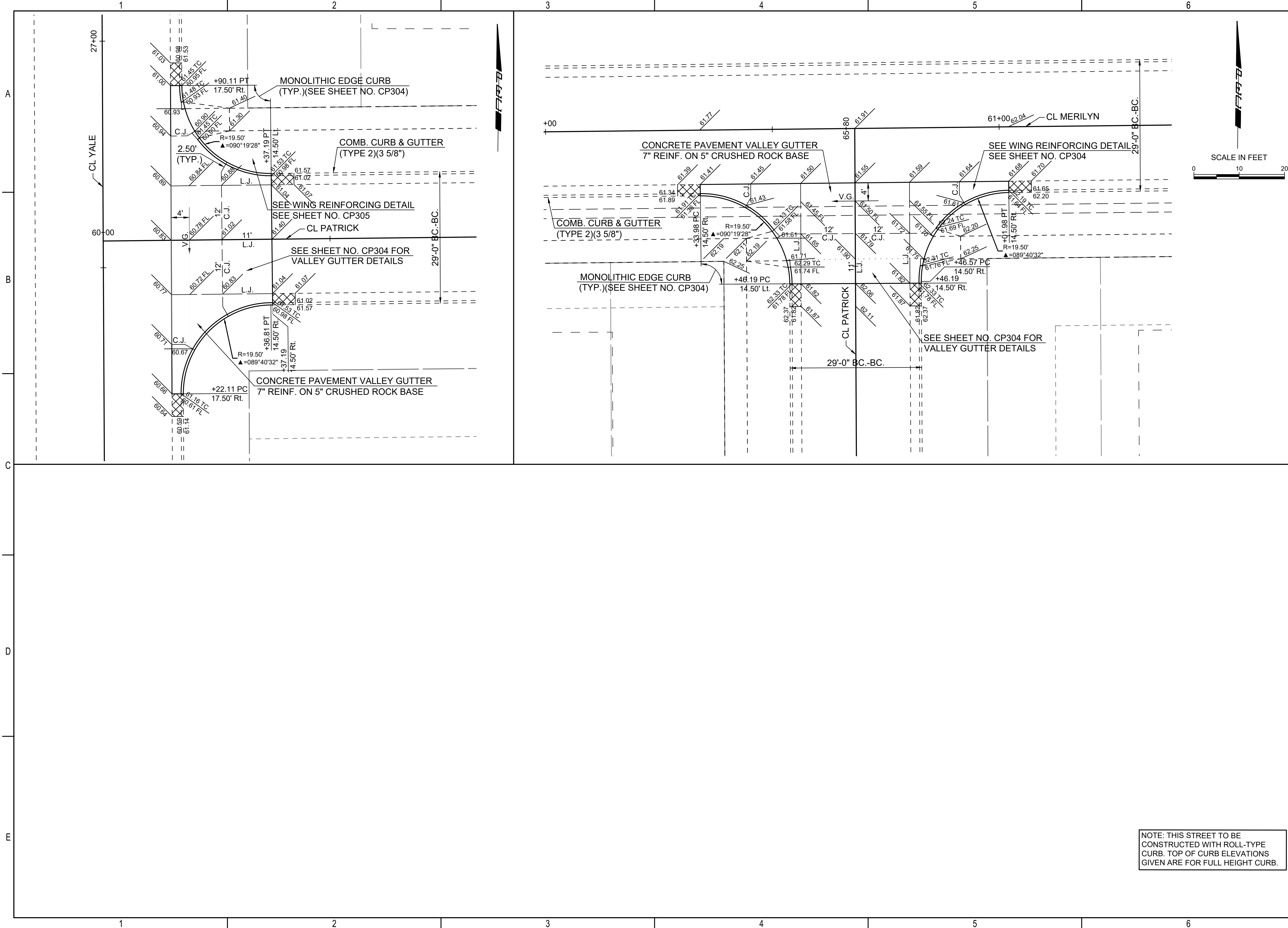
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

VALLEY GUTTER PLANS

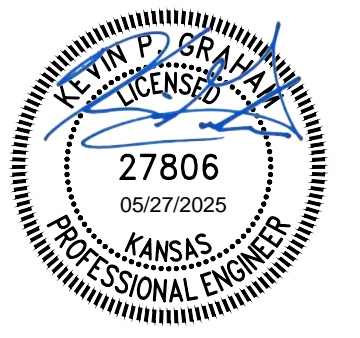
NOTE: THIS STREET TO BE CONSTRUCTED WITH ROLL-TYPE CURB. TOP OF CURB ELEVATIONS GIVEN ARE FOR FULL HEIGHT CURB.



SAVED 8/14/2024 10:24:30 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:37:03 AM BY KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\20-200605-004-CP303 VALLEY GUTTER PLANS.DWG



NOTE: THIS STREET TO BE
 CONSTRUCTED WITH ROLL-TYPE
 CURB. TOP OF CURB ELEVATIONS
 GIVEN ARE FOR FULL HEIGHT CURB.



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

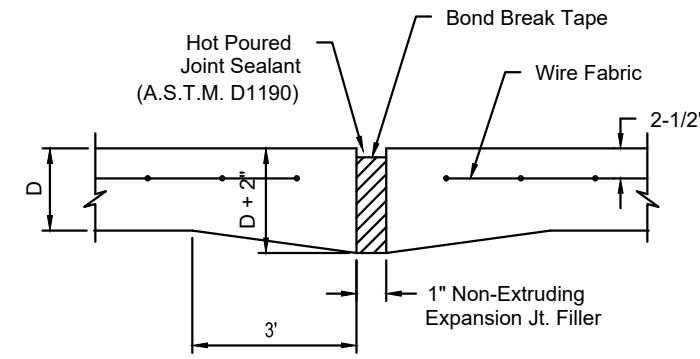
Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

VALLEY GUTTER PLANS

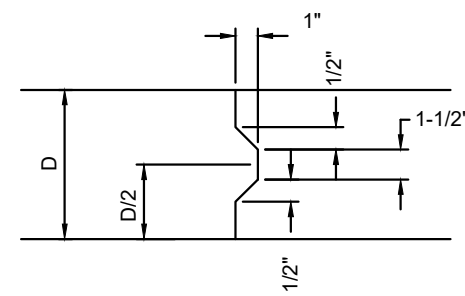
SAVED 6/12/2024 6:55:08 AM BY BILL SEIXSON
 PLOTTED 5/27/2025 9:37:10 AM BY KEVIN GRAHAM
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 DETAILS.DWG

Saved 06-12-2024 6:55:08 AM by BILL SEIXSON
 Plt. Scale: 1" = 0'-0" (1:0) 5/27/2025 9:37:10 AM by KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\21-200605-004-CP304 VALLEY GUTTER DETAILS

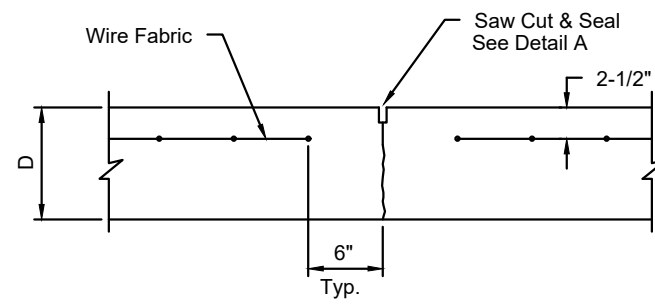


EXPANSION JOINT

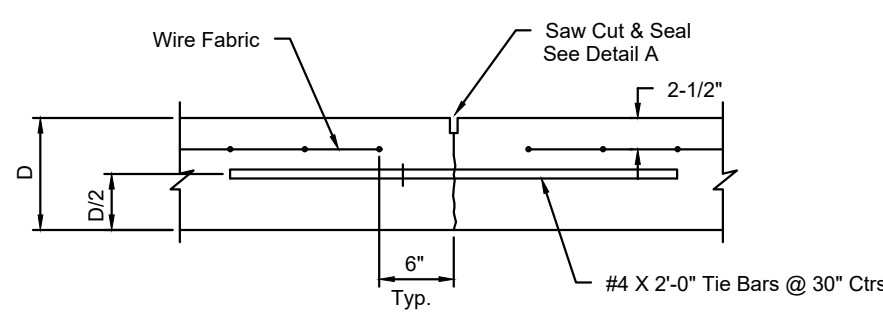
NOTE: Extra Thickness to be Subsidiary to Price of Square Yards Pavement



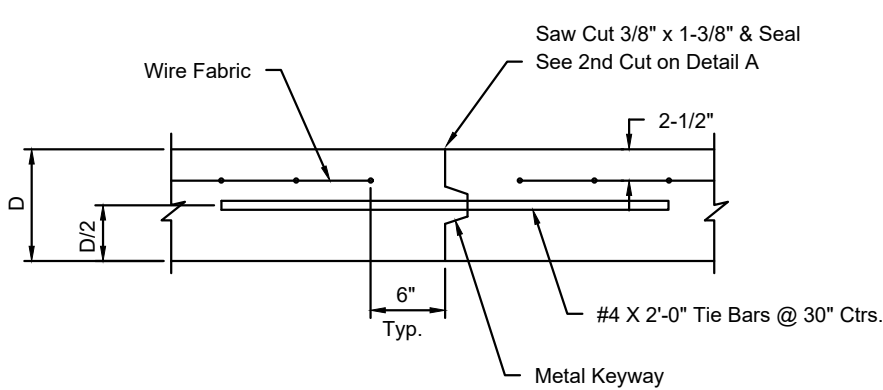
KEYWAY DETAIL



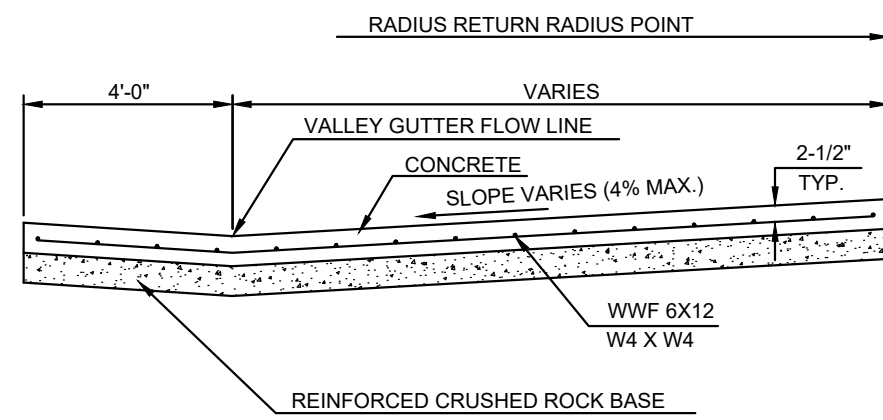
CONTRACTION JOINT DETAIL (C.J.)



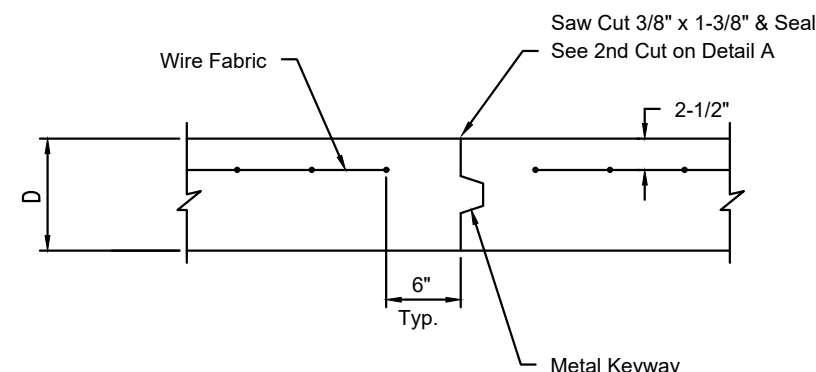
LONGITUDINAL JOINT DETAIL (L.J.)



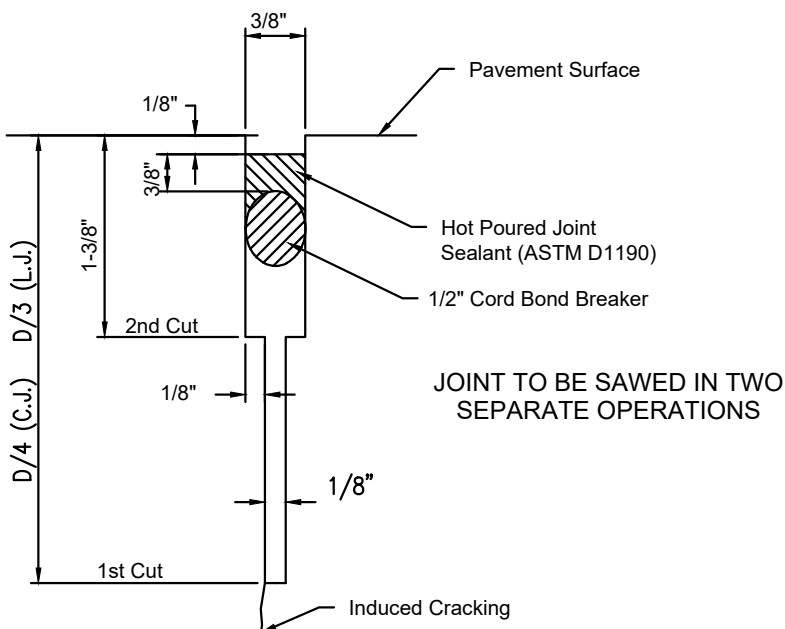
OPTIONAL LONGITUDINAL JOINT DETAIL (L.J.)



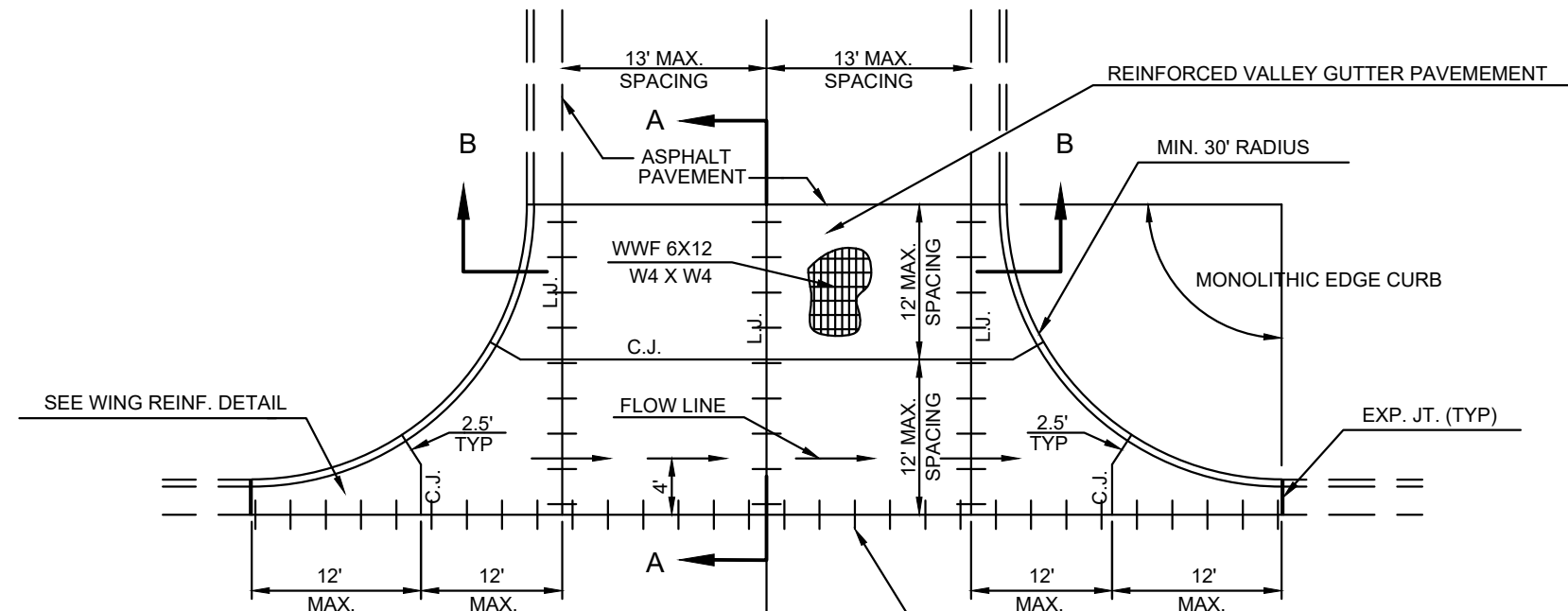
SECTION A-A



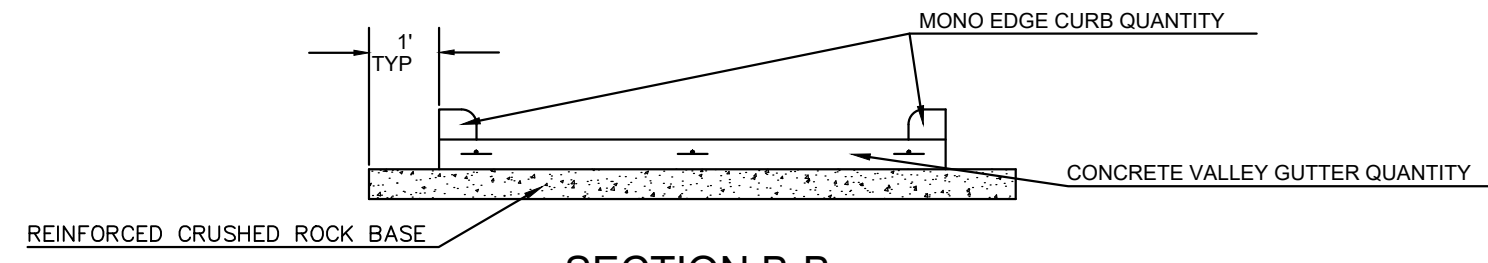
OPTIONAL CONTRACTION JOINT



SAW JOINT DETAIL (DETAIL A)

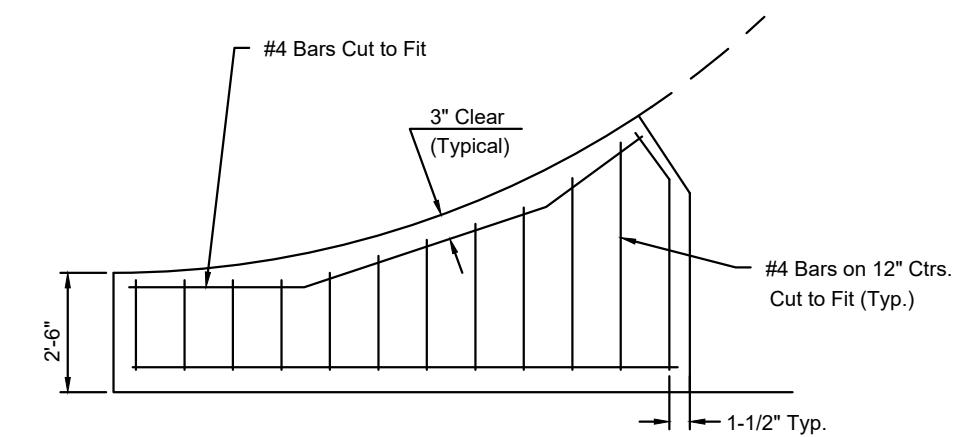


PLAN



SECTION B-B

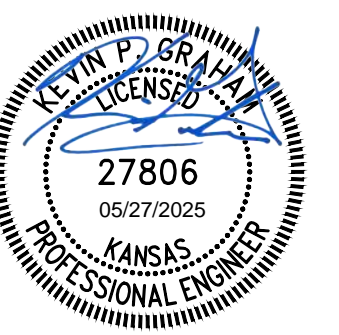
REINFORCED VALLEY GUTTER DETAIL



WING REINFORCING DETAIL

REVISION MAY 2017	SECTION B-B, ROCK EXTENDED ONE FOOT BEYOND PAVEMENT
CITY OF WICHITA	
VALLEY GUTTER DETAILS	
CITY ENGINEER GARY JANZEN, P.E.	
PROJECT NUMBER	OCA NUMBER
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501	
SHEET _ of _	

PV-109



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

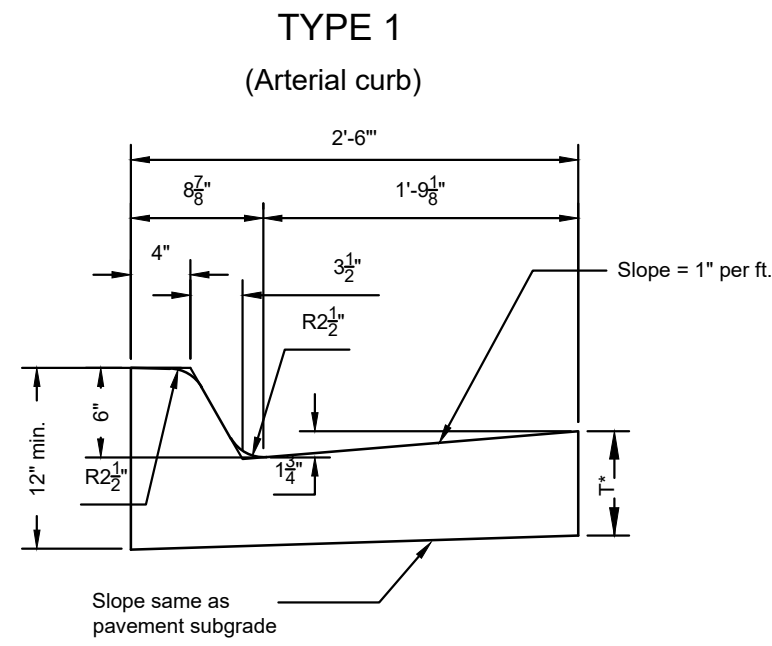
ISSUE:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

VALLEY GUTTER DETAILS

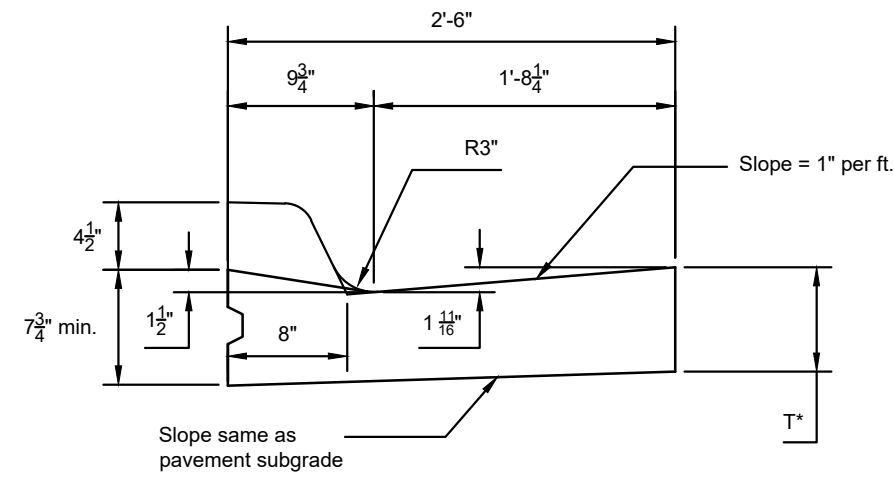
CP304
21 OF 71

SAVED 6/12/2024 6:54:45 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:37:14 AM BY KEVIN GRAHAM
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 DETAILS.DWG

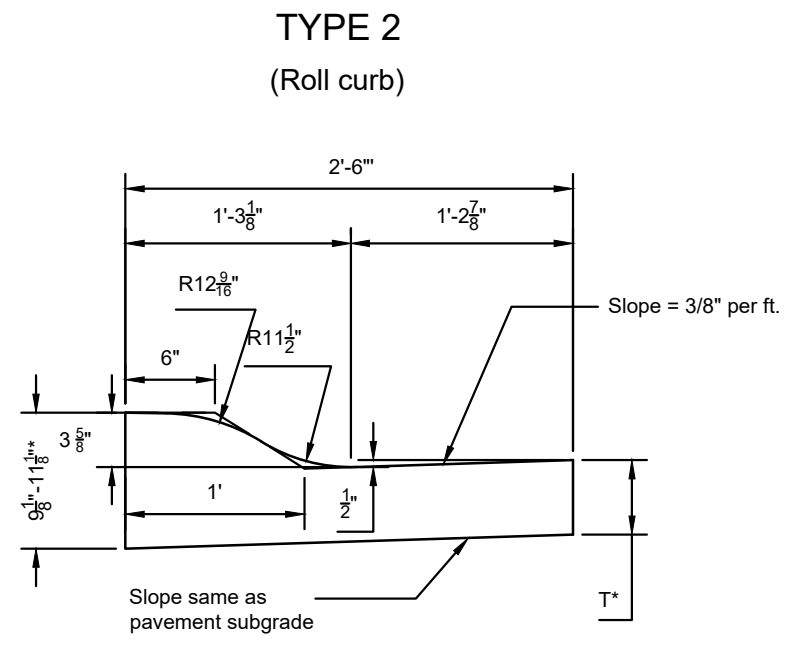
Saved 06-12-2024 6:54:45 AM by BILL SEXSON
 Plt. Scale: 1" = 6'-0" (2025 9:37:14 AM by KEVIN GRAHAM)
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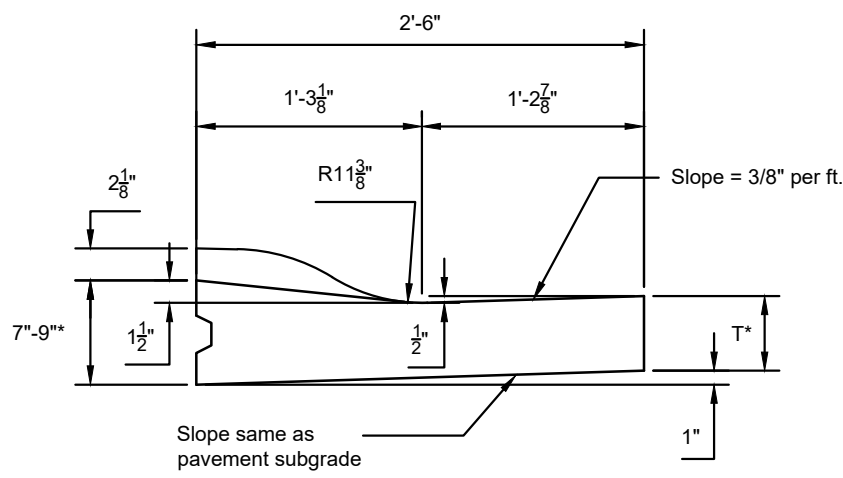
Combined Curb & Gutter (6")



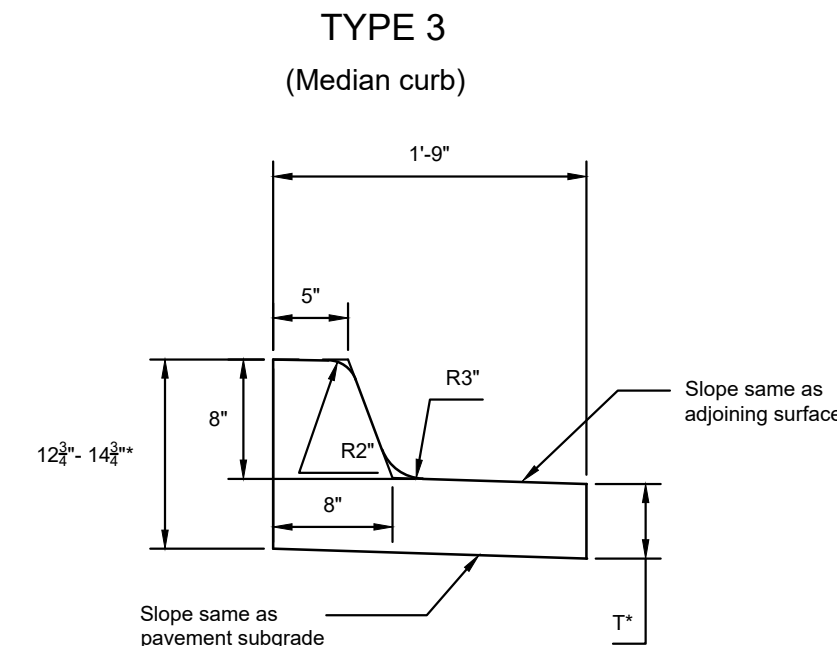
Combined Curb & Gutter (1 1/2")



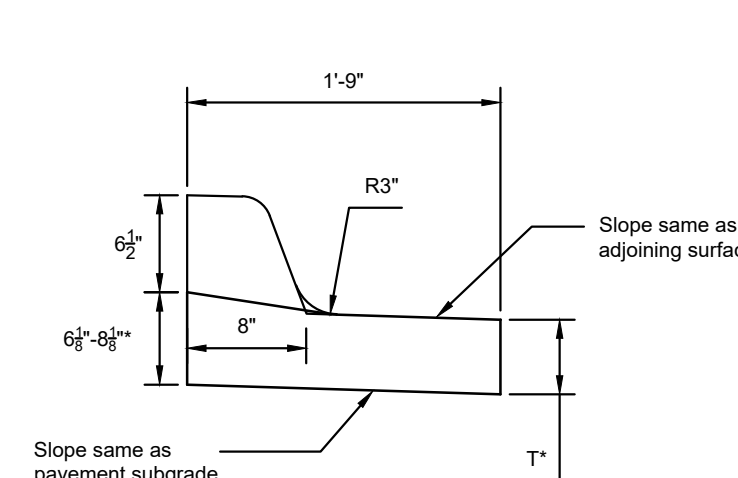
Combined Curb & Gutter (3 5/8")



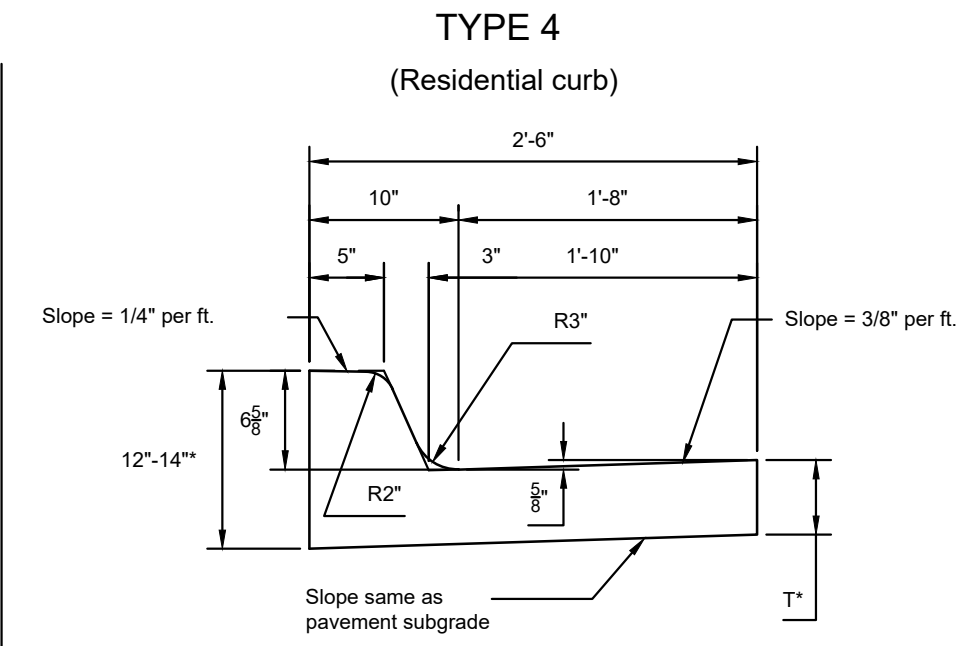
Combined Curb & Gutter (1 1/2")



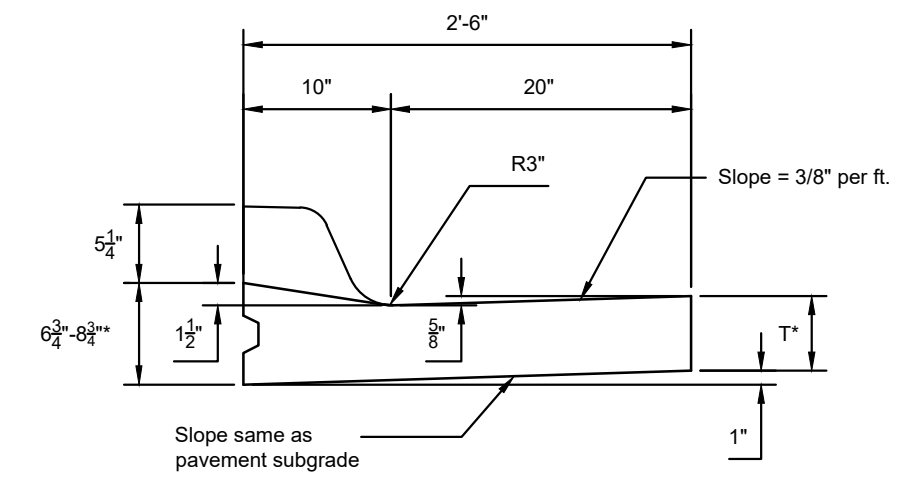
Combined Curb & Gutter (8")



Combined Curb & Gutter (1 1/2")

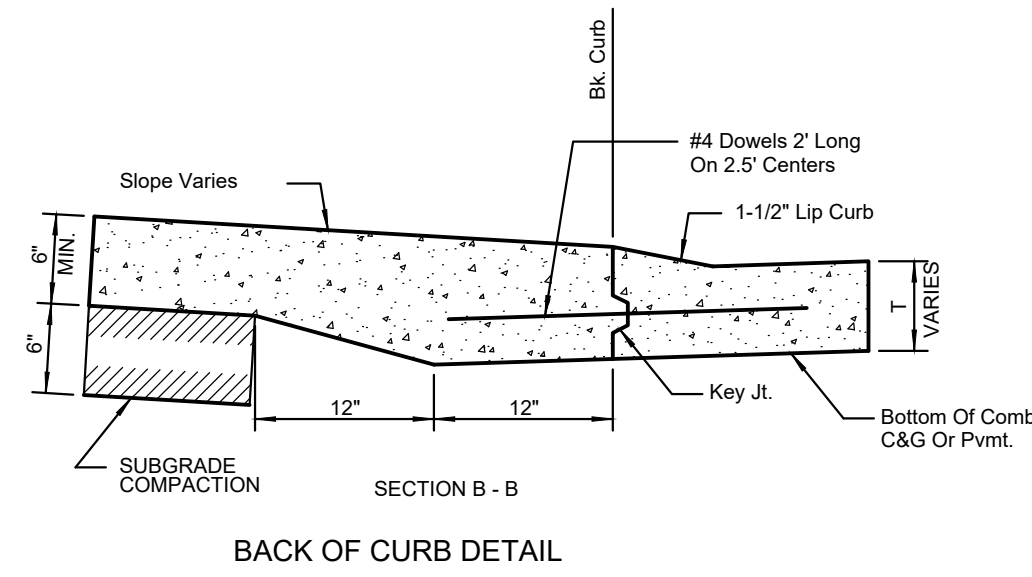


Combined Curb & Gutter (6 5/8")

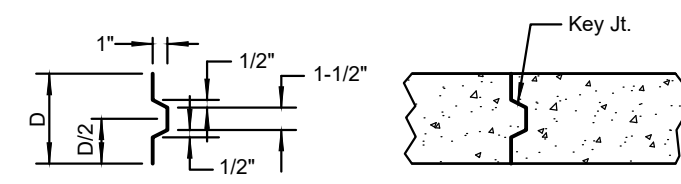


Combined Curb & Gutter (1 1/2")

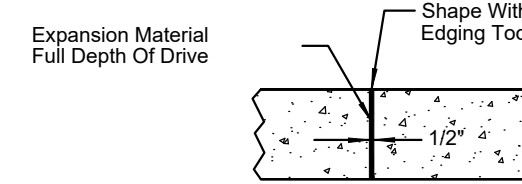
T* = Thickness of curb to adjust with pavement thickness



BACK OF CURB DETAIL



ALT. LONGITUDINAL CONSTRUCTION JOINT



EXPANSION JOINT (E.J.)

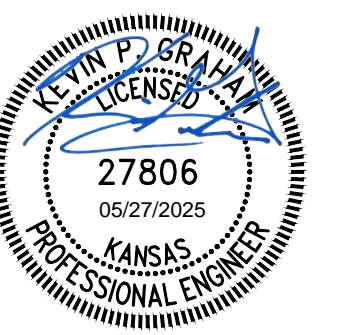
GENERAL NOTES

- Expansion (isolation) joints shall be constructed a maximum of 300' apart and at all PIs, PCs, cul-de-sac quadrants, and ends of returns.
- Contraction joints shall be constructed a minimum of 12' apart.
- Joint sealer shall be required at all joints on arterial and industrial streets and at intersections on residential streets.



CURB & GUTTER & PAVING BRICK CROSSWALK DETAILS		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET _ of _

PV-101



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

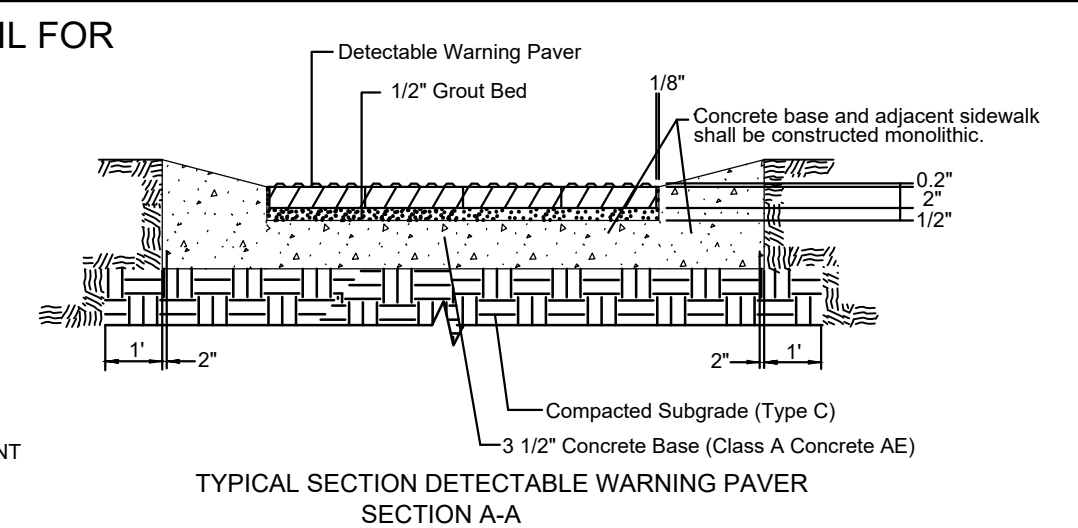
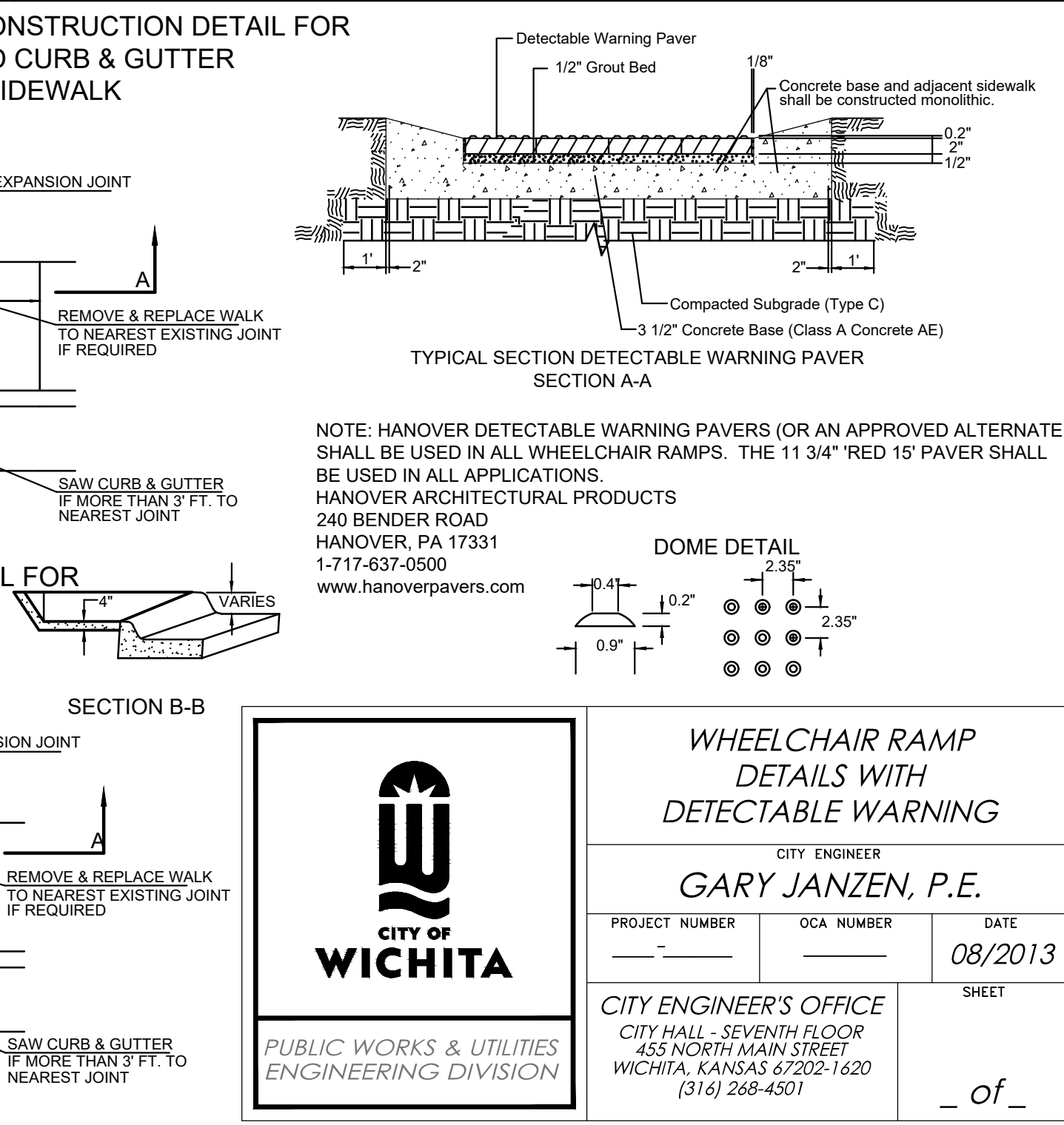
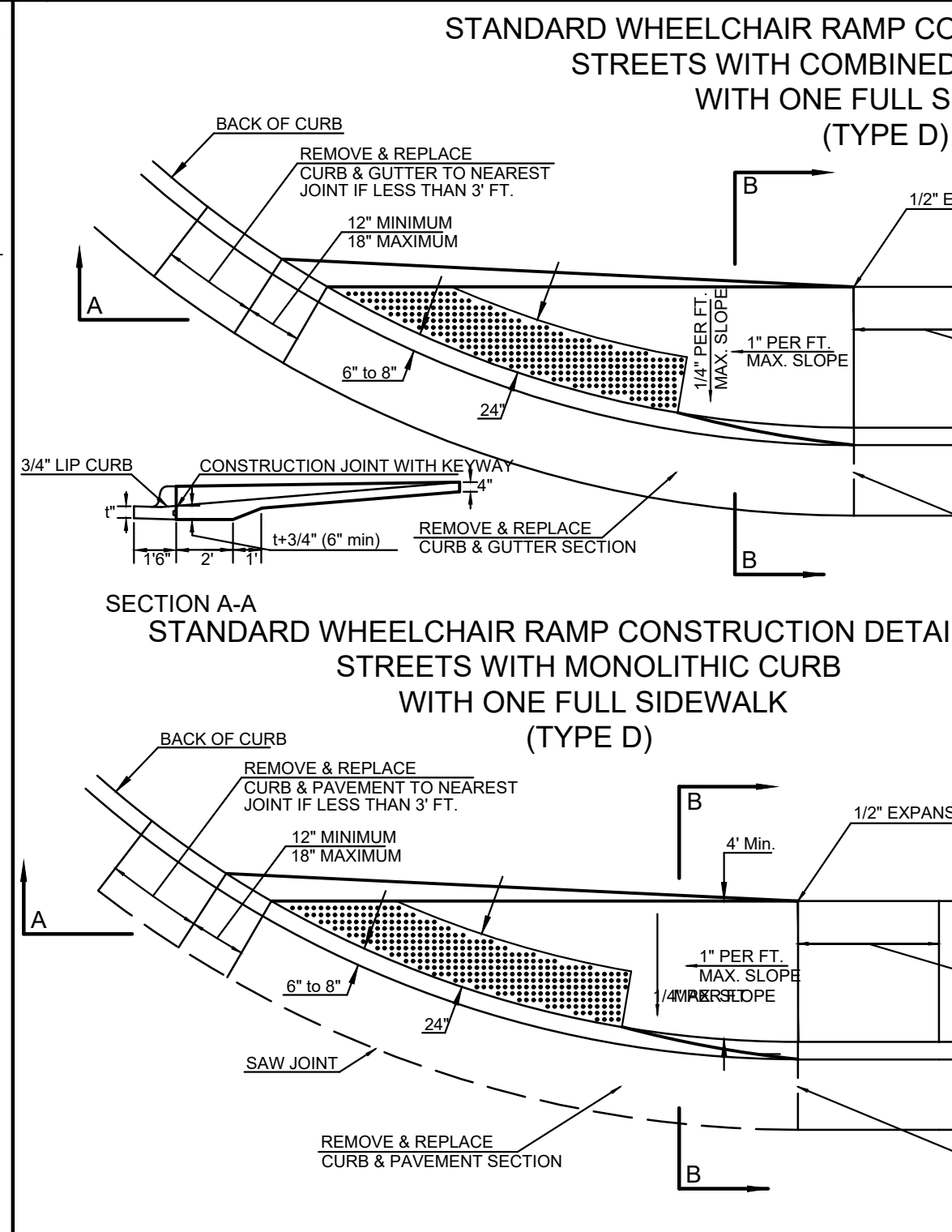
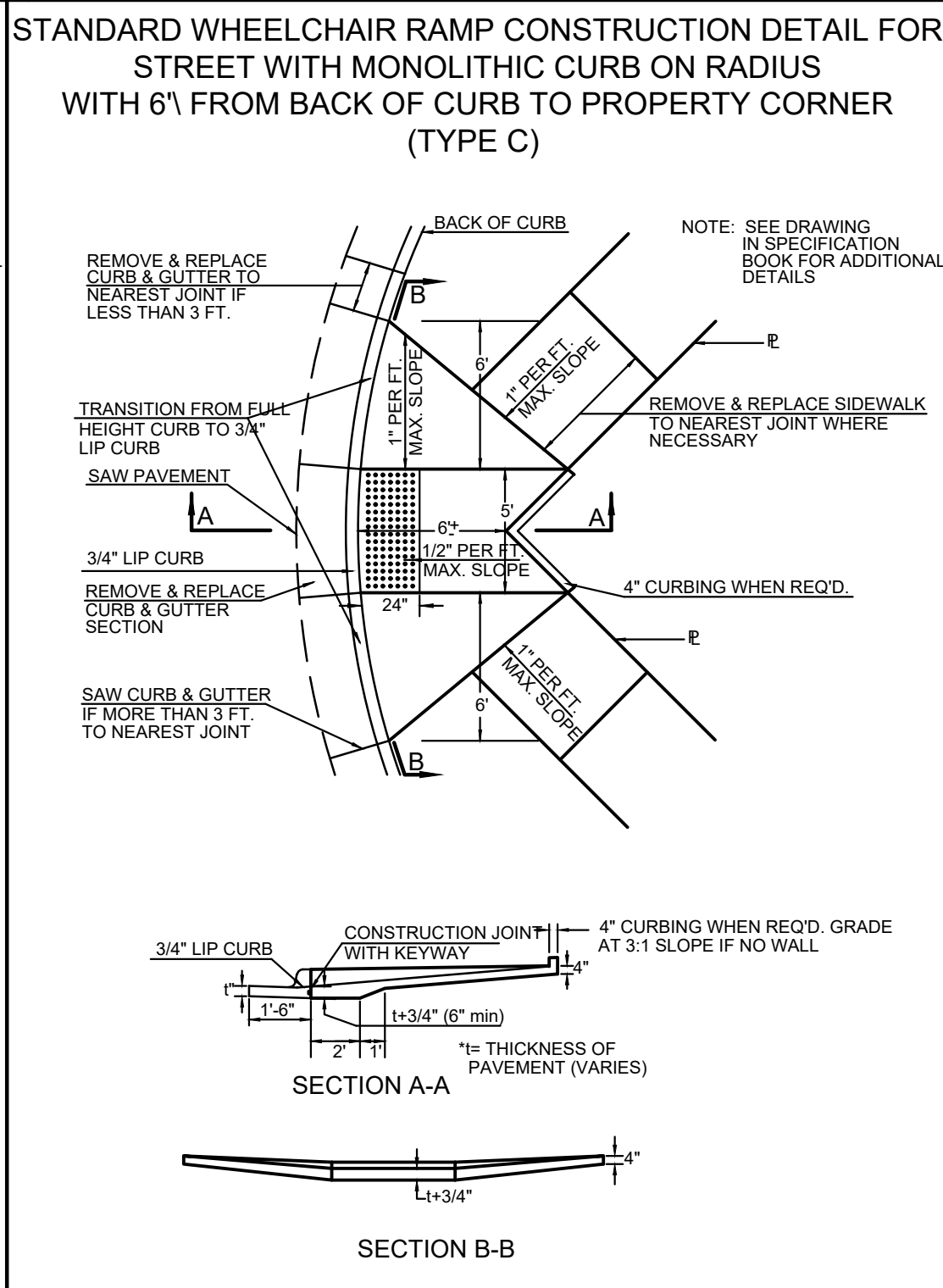
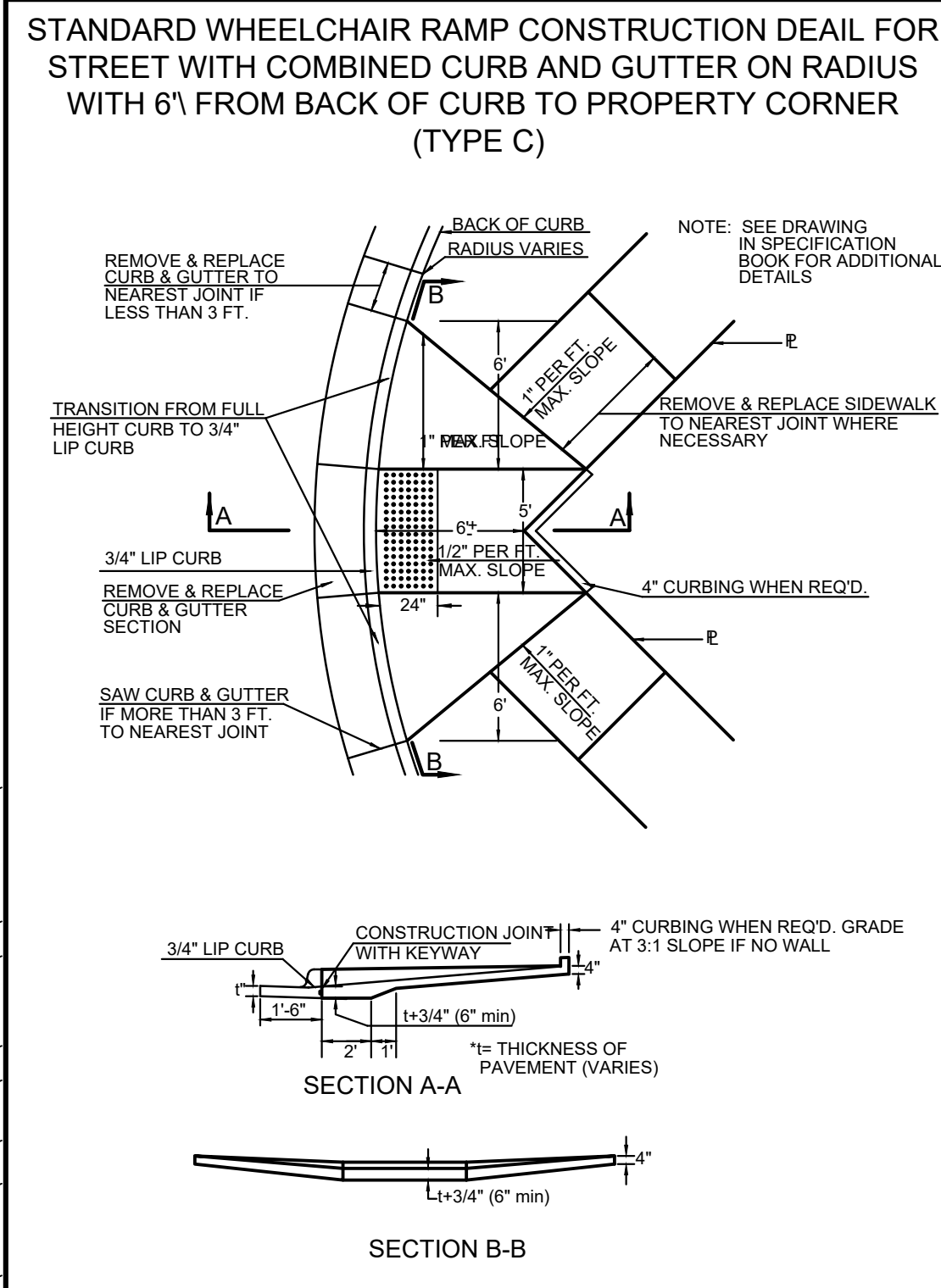
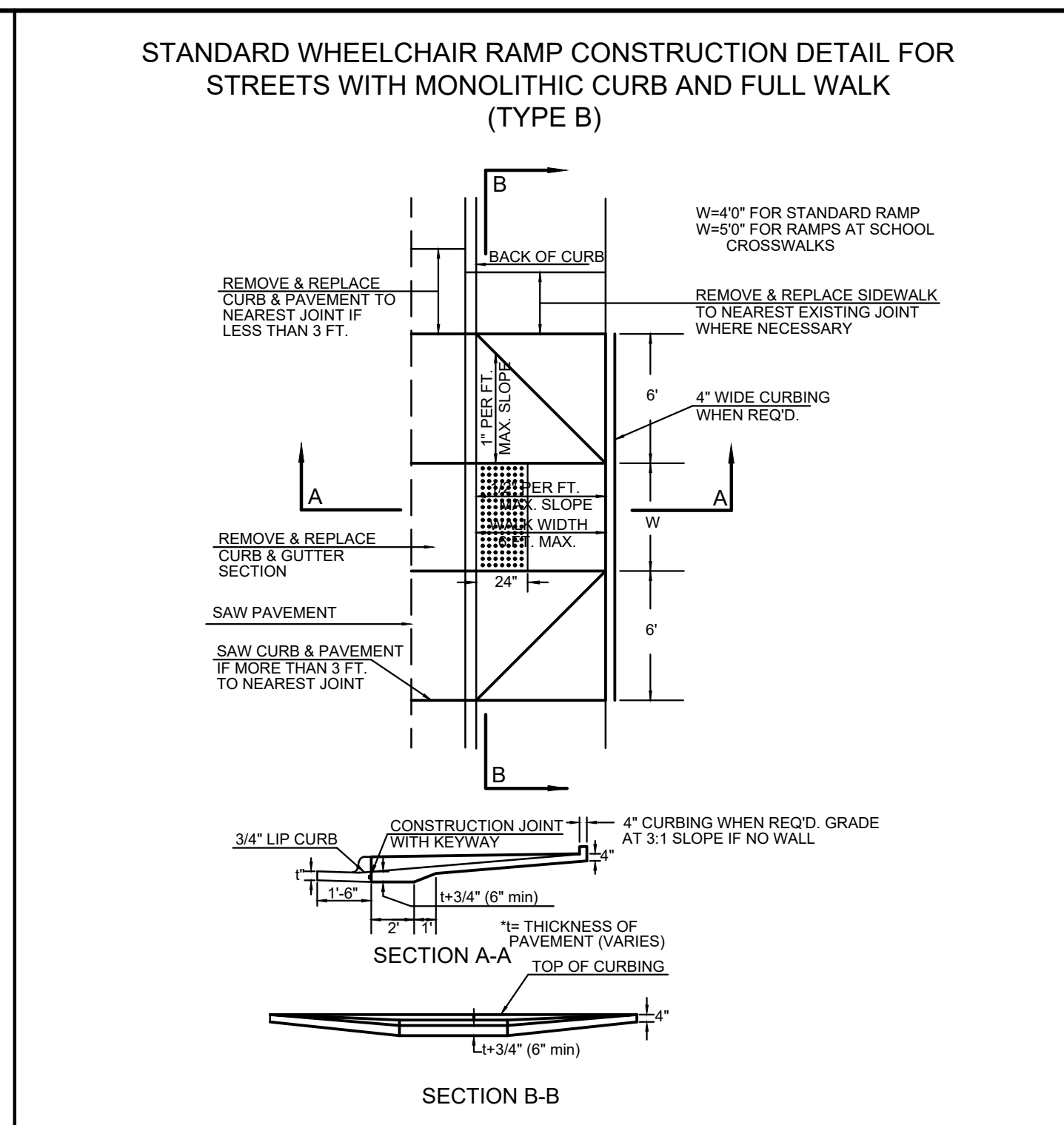
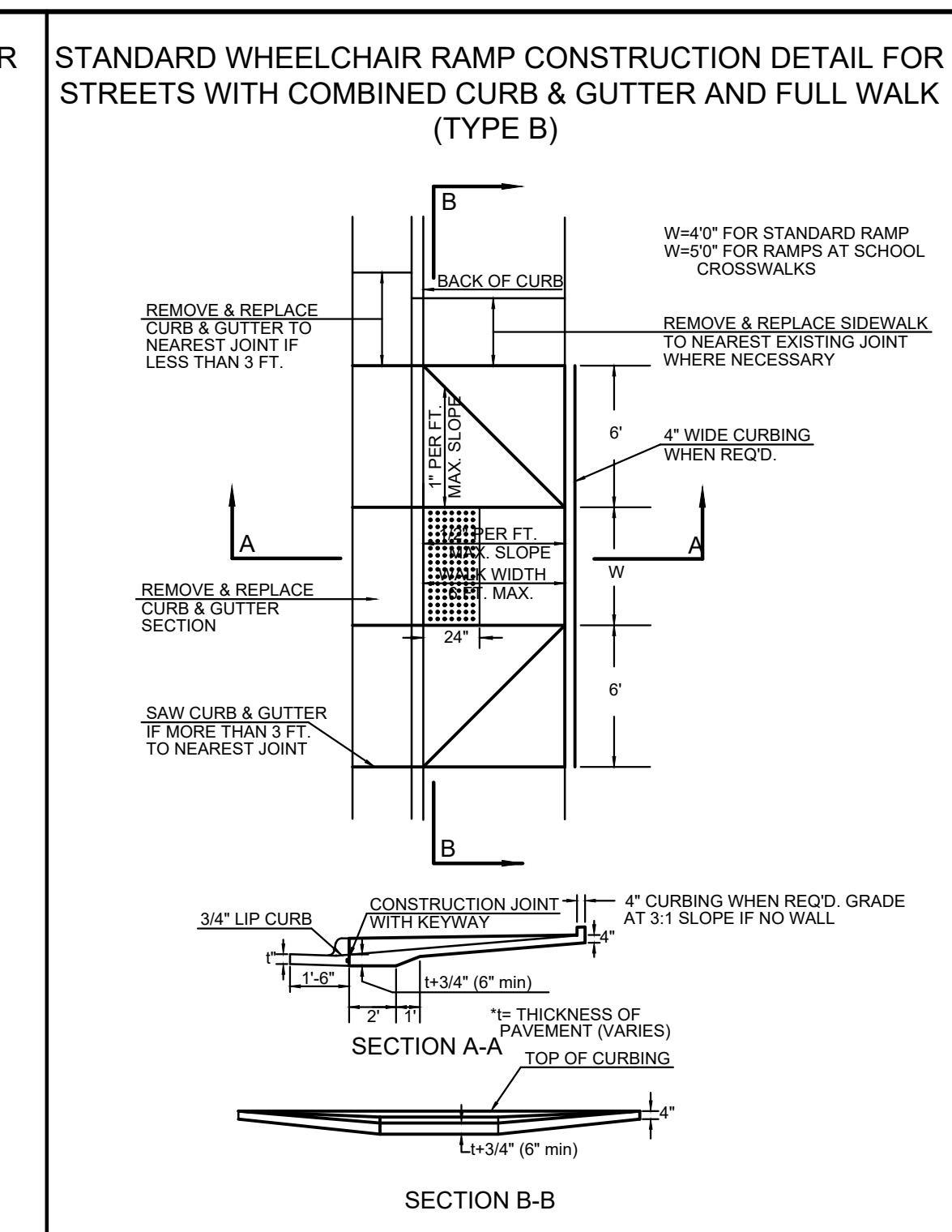
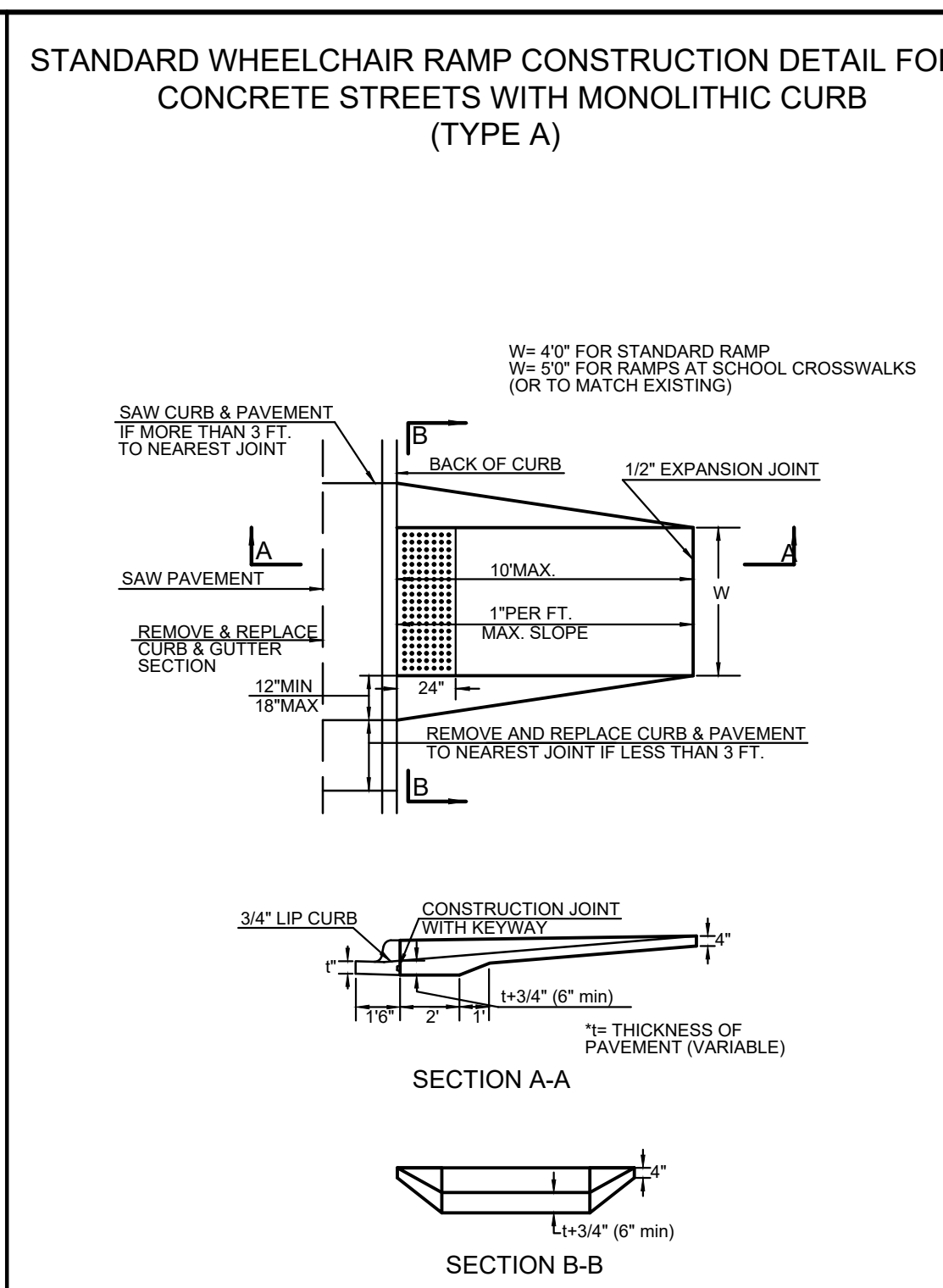
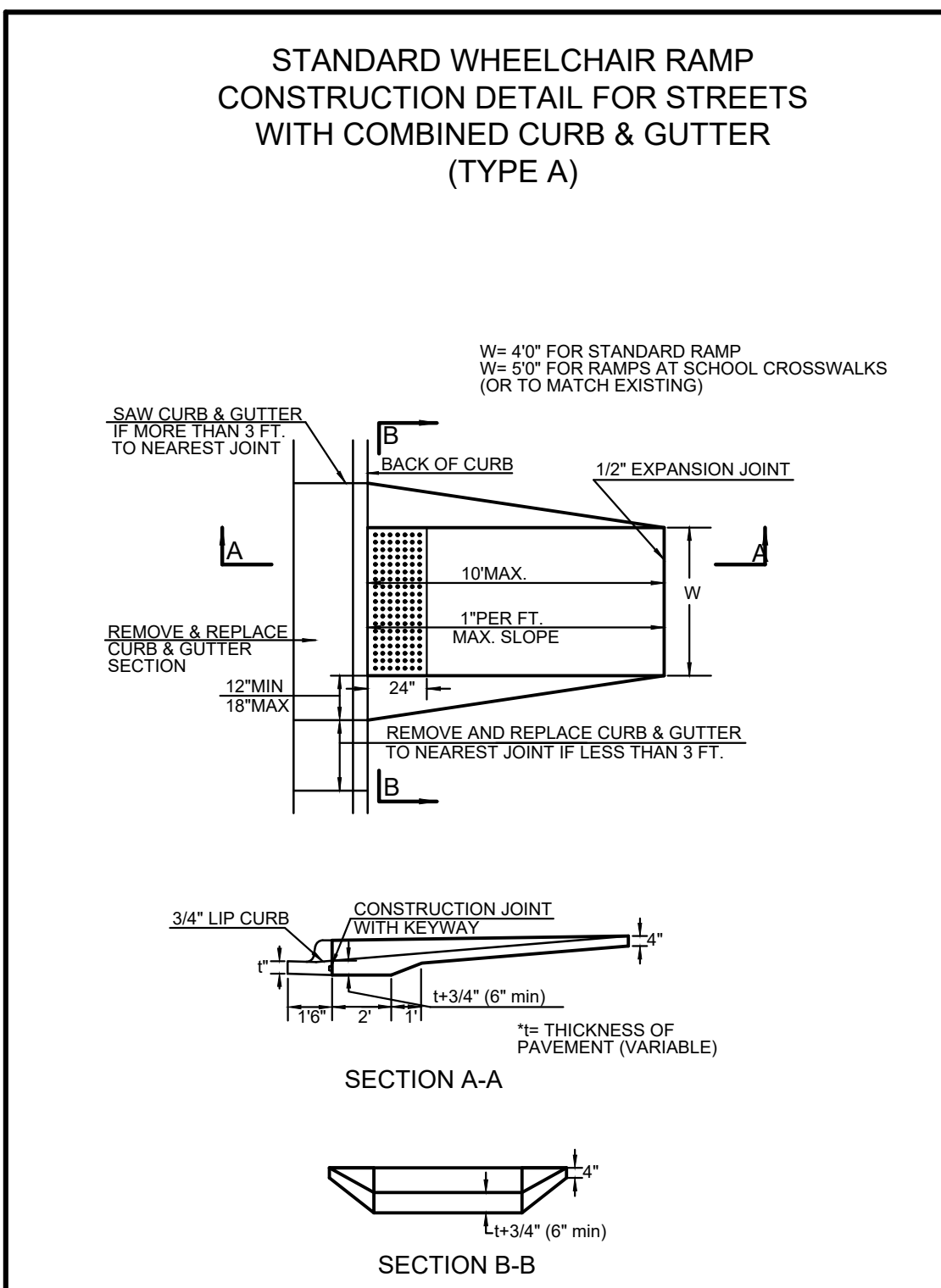
ISSUE:		
JOB NO.	200605-004	
DATE	MAY 2025	
PM	KPG	
DESIGNED BY	KPG	
DRAWN BY	BJS	
CHECKED BY	KMS	

CURB AND GUTTER DETAILS

CP305
 22 OF 71

SAVED 6/12/2024 6:54:32 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:37:20 AM BY KEVIN GRAHAM
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 DETAILS.DWG

Saved 06-12-2024 6:54:32 AM BY BILL SEXSON
 Plt. Scale: 1" = 0'-0" (AS SHOWN)
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NOTE: HANOVER DETECTABLE WARNING PAVERS (OR AN APPROVED ALTERNATE) SHALL BE USED IN ALL WHEELCHAIR RAMPS. THE 11 3/4" RED 15" PAVES SHALL BE USED IN ALL APPLICATIONS.
 HANOVER ARCHITECTURAL PRODUCTS
 240 BENDER ROAD
 HANOVER, PA 17331
 1-717-637-0500
 www.hanoverpavers.com

DOME DETAIL

2.35"

0.9"

WHEELCHAIR RAMP DETAILS WITH DETECTABLE WARNING

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER: _____ OCA NUMBER: _____ DATE: **08/2013**

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET: _____ of _____

PEC
PROFESSIONAL ENGINEERING CONSULTANTS
303 SOUTH TOPEKA
WICHITA, KS 67202
316-262-2691 www.pec1.com

CITY OF WICHITA

KEVIN P. GRAHAM
LICENSED
27806
05/27/2025
KANSAS
PROFESSIONAL ENGINEER

PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

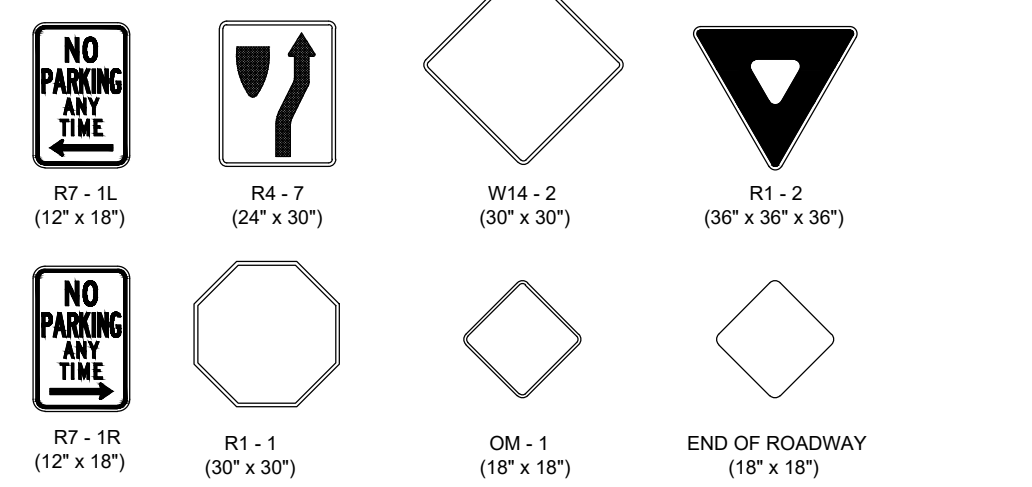
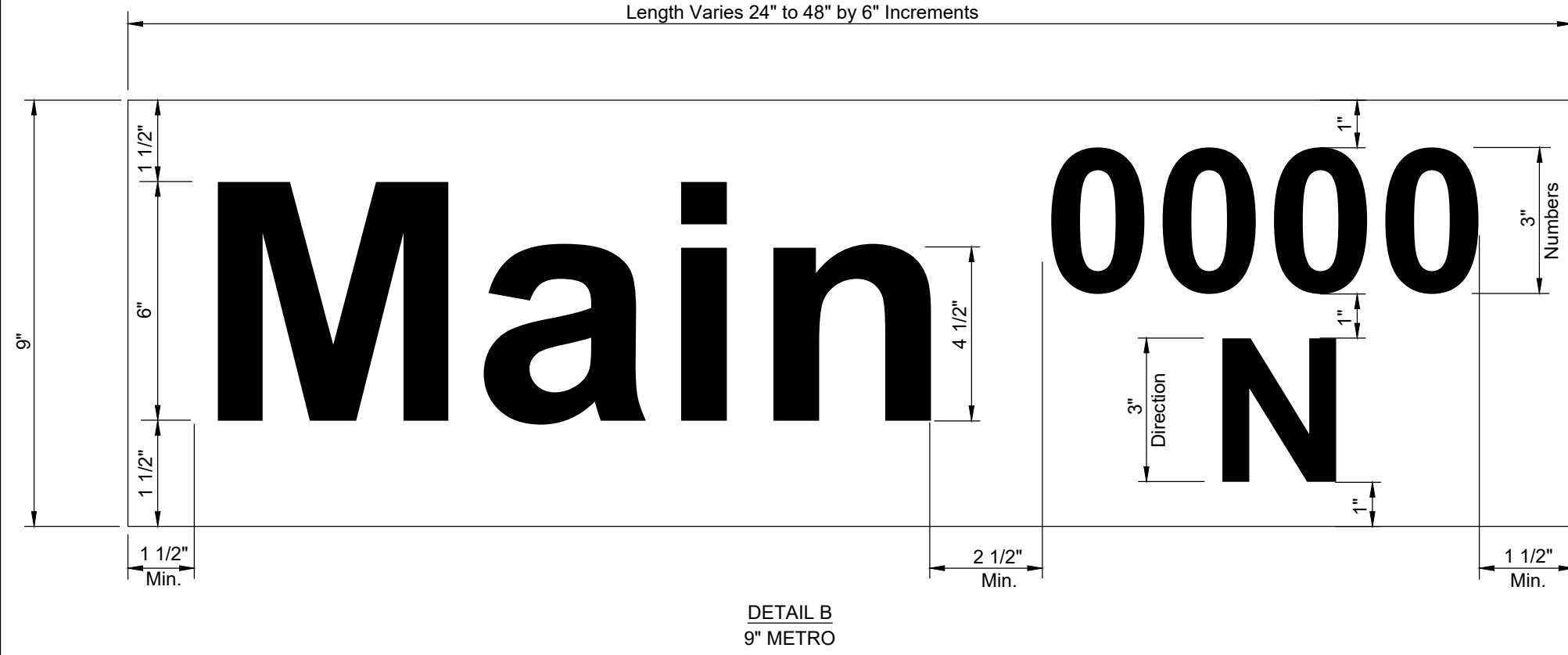
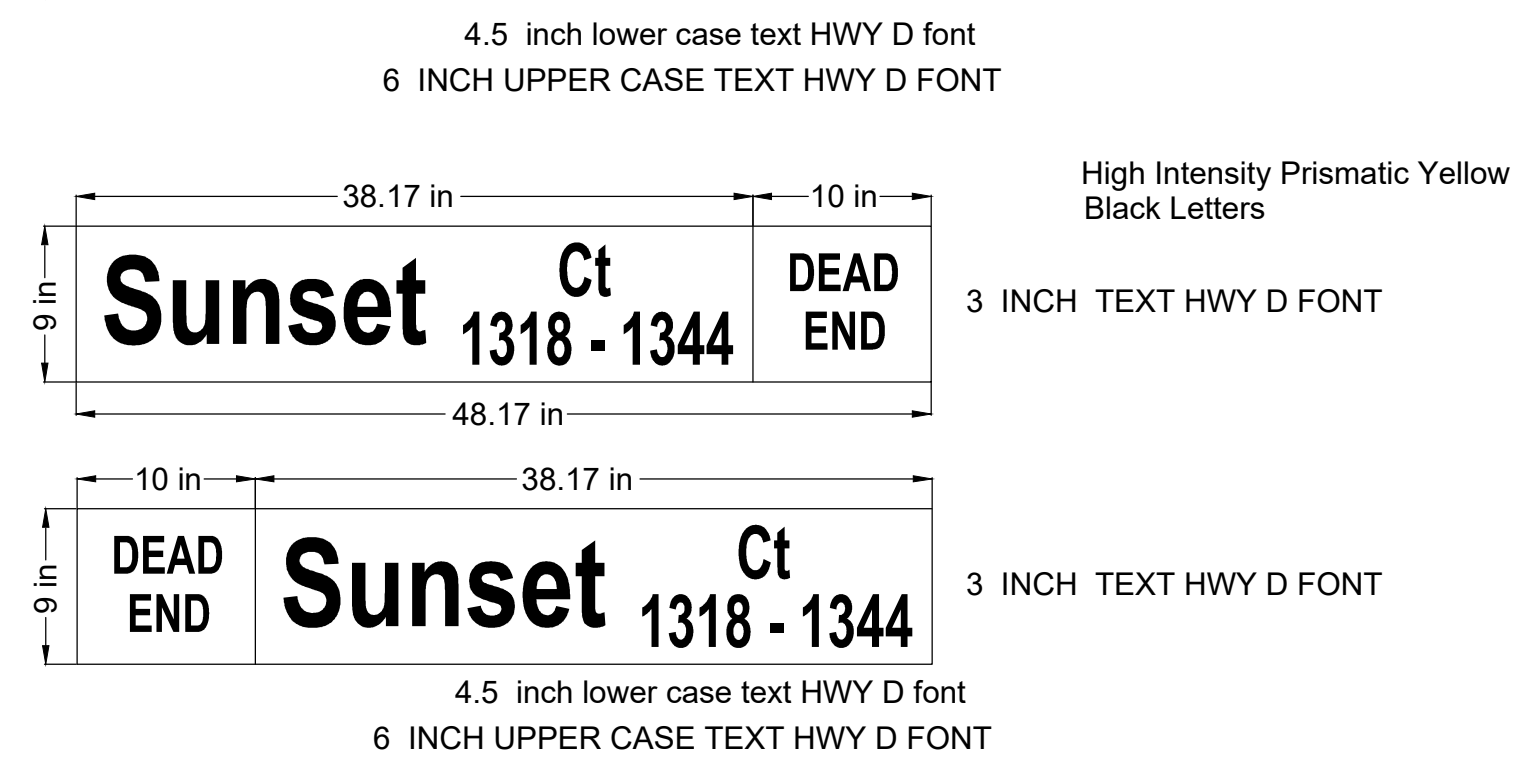
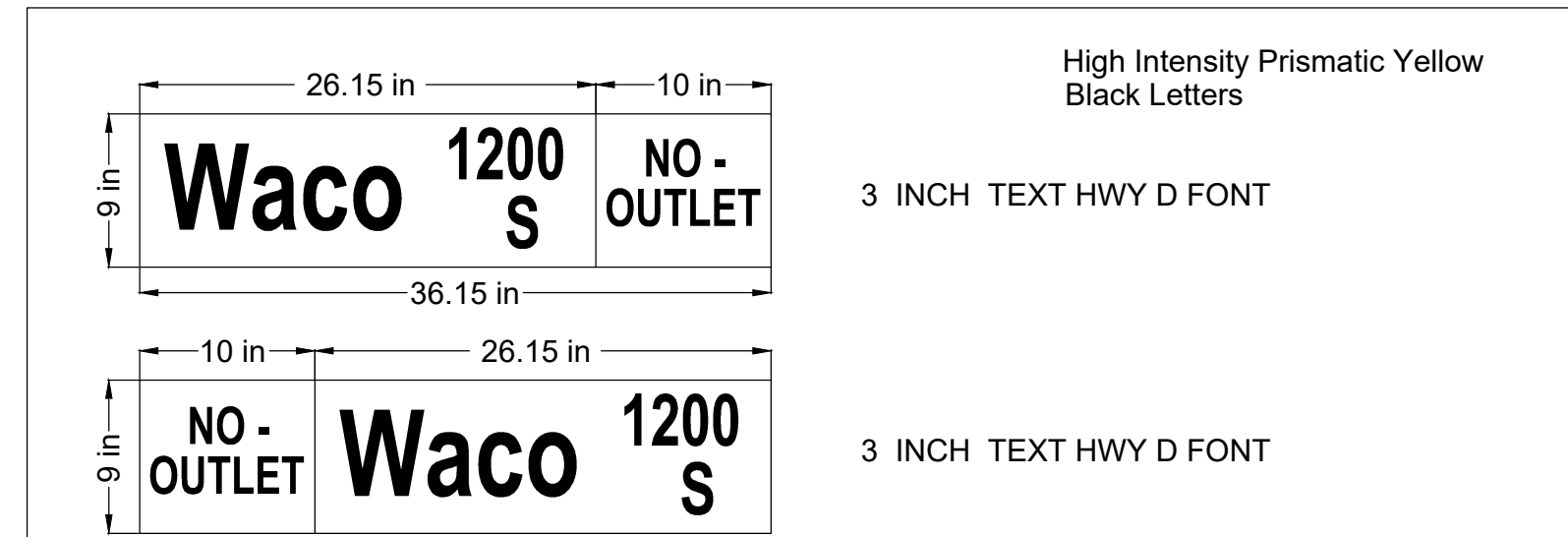
Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

WHEELCHAIR RAMP DETAILS

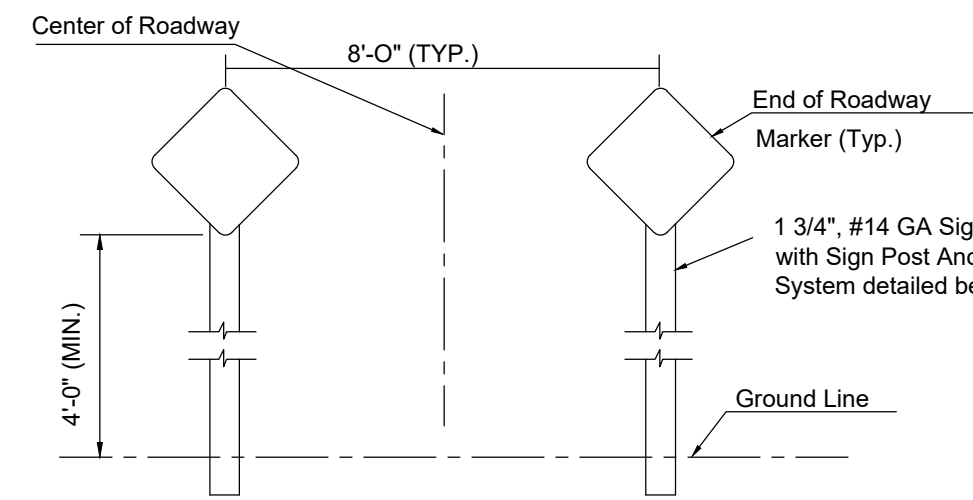
CP306
23 OF 71

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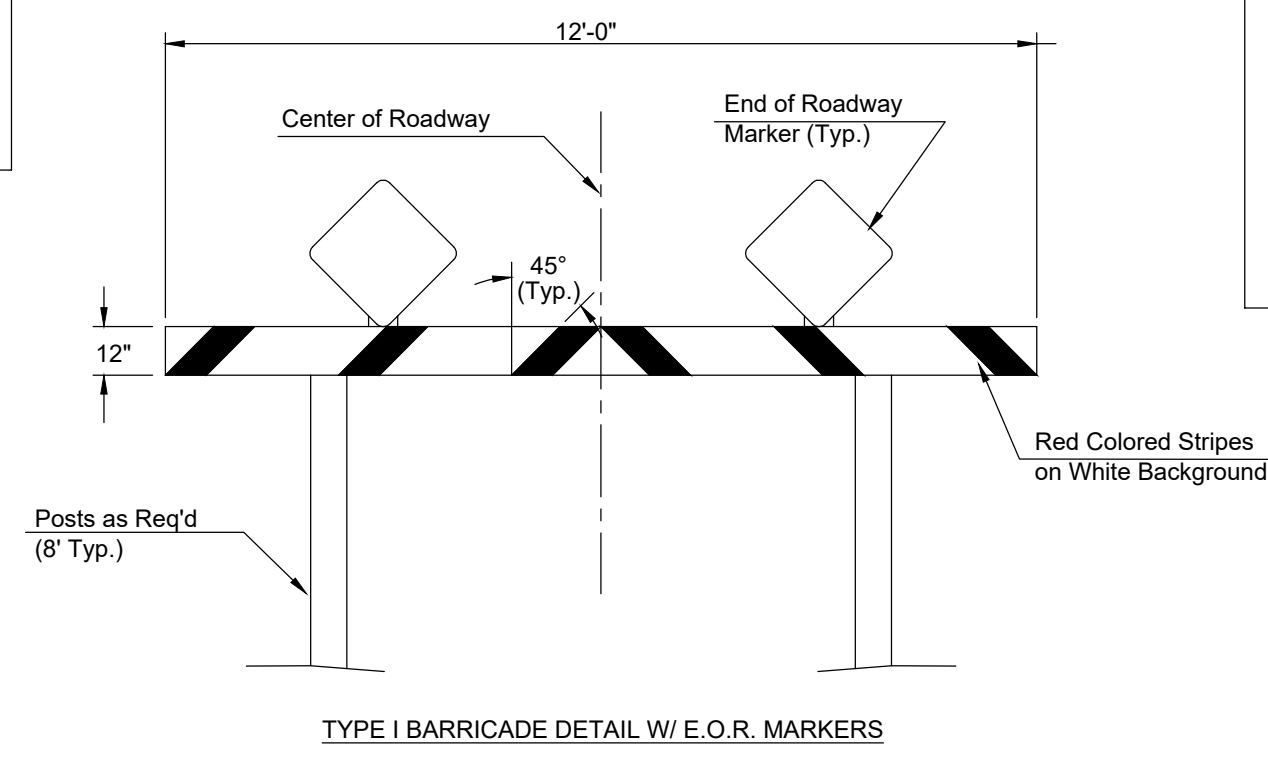
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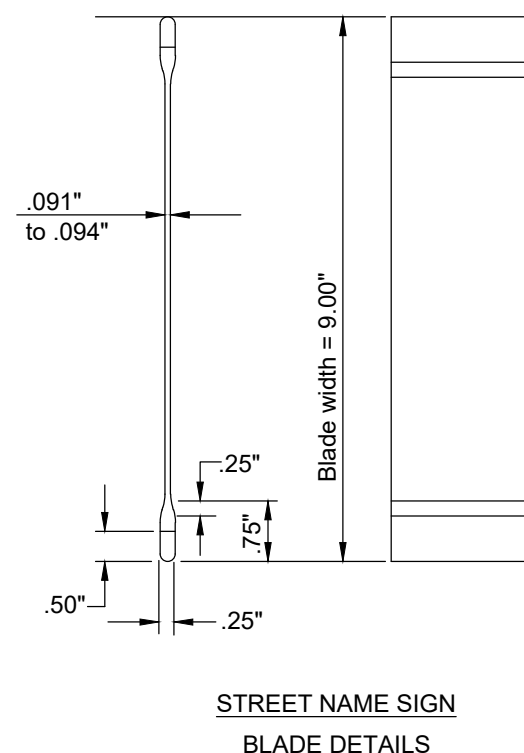
* IN NO CASE SHALL THE SPECIFICATIONS BE LESS THAN REQUIRED BY THE CURRENT MUTCD.



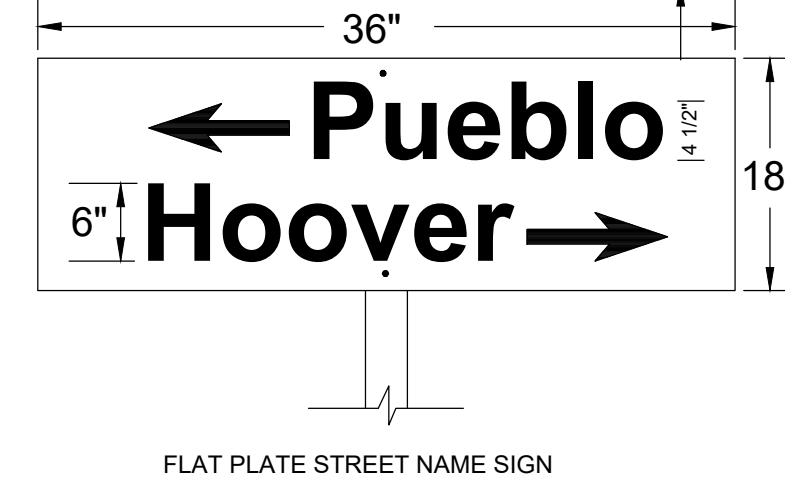
TYPICAL END OF ROADWAY SIGN MOUNTING INSTALLATION



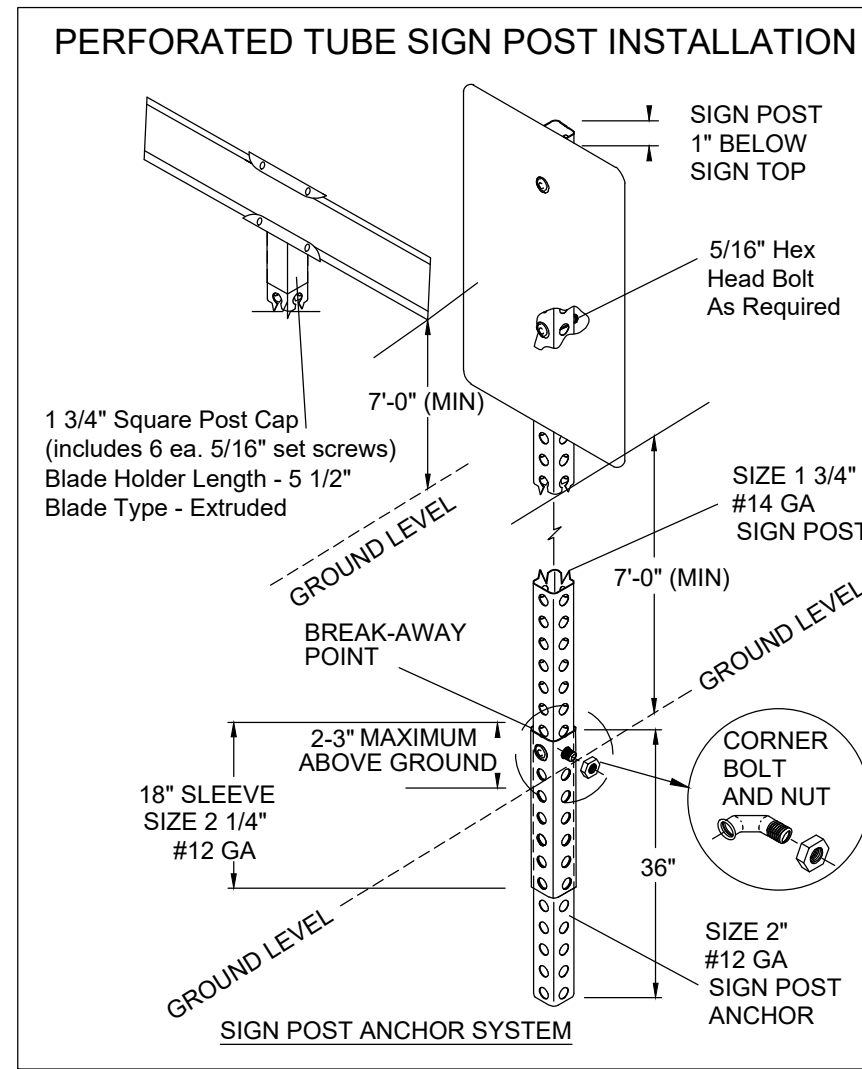
TYPE I BARRICADE DETAIL W/ E.O.R. MARKERS



STREET NAME SIGN BLADE DETAILS



FLAT PLATE STREET NAME SIGN



PERFORATED TUBE SIGN POST INSTALLATION

STATION	OFFSET	SIGN	QUANTITY*
10+40	LT.	(R1-1)(STOP)	1
10+40	RT.	STREET NAME SIGN	1
13+42	CL	(OM4-3)(END OF ROADWAY)	2
24+97.5	CL	(OM4-3)(END OF ROADWAY)	2
29+65	RT.	(R1-1)(STOP)	1
30+35	LT.	(R1-1)(STOP)	1
30+35	RT.	STREET NAME SIGN	1
37+47	LT.	STREET NAME SIGN	1
55+40	LT.	(R1-1)(STOP)	1
55+40	RT.	STREET NAME SIGN	1
66+64	RT.	FLAT PLATE (SNS)	1
69+80	CL	(OM4-3)(END OF ROADWAY)	2
60+35	RT.	STREET NAME SIGN	1
65+48	LT.	STREET NAME SIGN	1
61+98	LT.	(R7-1R)(NO PARKING)	1
61+98	RT.	(R7-1L)(NO PARKING)	1
63+32	LT.	(R7-1L)(NO PARKING)	1
63+32	RT.	(R7-1R)(NO PARKING)	1
62+56	RT.	(R4-7)(KEEP RIGHT)	1
62+72	LT.	(R4-7)(KEEP RIGHT)	1
TOTAL			23

STREET NAME	NO. BLADES REQ'D		
	9" STD.	9" METRO	FLAT PLATE
Clifton Ave #		2	
61st St S #		1	
Marilyn #		1	
61st St S	1		
Marilyn	2		
Yale	3		
Patrick	2		
Marilyn/Bluff			1

CALL TRAFFIC ENGINEERING (268-4276) BEFORE ORDERING SIGNS.

NOTE: REFERENCES BELOW TO "STANDARD SPECIFICATIONS" DENOTE "STANDARD SPECIFICATION FOR STATE ROAD AND BRIDGE CONSTRUCTION EDITION 2015" BY THE KANSAS DEPARTMENT OF TRANSPORTATION.

- FABRICATION AND INSTALLATION OF ALL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD.
- POST ANCHORS: POSTS SHALL BE ANCHORED WITH A YIELDING BASE POST SUPPORT AS DETAILED.
- POSTS FOR TRAFFIC CONTROL SIGNS: POSTS SHALL BE GALVANIZED AND CONFORM TO THE REQUIREMENTS OF SUBSECTION 1620 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT ALL POSTS SHALL WEIGH 3 LBS./FT. MINIMUM.
- POSTS FOR STREET NAME SIGNS (SNS): POSTS SHALL BE 9 FEET LONG, CONSTRUCTED FROM #14 GALVANIZED STEEL PIPE AND SHALL BE 1 3/4" SQUARE WEIGHING A MINIMUM OF 3 LBS./FT. POSTS SHALL BE POSITIONED SO THAT THE BOTTOM BLADE IS 7 FEET ABOVE GRADE.
- POSTS FOR END OF ROADWAY SIGN TO BE 8' LONG AND INSTALLED A MINIMUM OF 4' FROM ROADWAY TO BOTTOM OF SIGN.
- SIGN BLANKS FOR TRAFFIC CONTROL SIGNS: SIGN BLANKS SHALL BE FABRICATED FROM 0.080" ALUMINUM ALLOY 6063-T6 CONFORMING TO THE REQUIREMENTS OF SUBSECTION 1627 OF THE STANDARD SPECIFICATIONS.
- SIGN BLADES FOR STREET NAME SIGNS: EXTRUDED ALUMINUM BLADES SHALL BE ALUMINUM ALLOY CONFORMING TO 6063-T6 OR 6062-H38 (ASTM SPECIFICATION B221, LATEST ISSUE). BLADES SHALL HAVE AN ALODINE OR PHOSPHATE ETCHED FINISH. BLADES SHALL HAVE SQUARE CORNERS AND NO HOLES. MINIMUM BLADE LENGTH SHALL BE 24" MAXIMUM BLADE LENGTH SHALL BE 48", LENGTH VARIES BY INCREMENTS OF 6". BLADES BEARING THE STREET NAMES SHALL BE FIRMLY ATTACHED TO THE MOUNTING BRACKETS USING ALLEN-TYPE CONICAL SET SCREWS. THE BLADES SHALL BE ORIENTED PARALLEL TO THE STREET.
- MOUNTING BRACKETS FOR SIGNS: DIE-CAST ALUMINUM BRACKETS SHALL BE ALUMINUM ALLOY 6063-T6 HAVING A TENSILE STRENGTH OF 44,000 PSI. THE BRACKETS SHALL BE SMOOTHLY FINISHED FREE OF PITS, BURRS, AND FLAWS. EACH BRACKET SHALL BE TAPPED AND DRILLED FOR 5/16" ZINC-PLATED ALLEN-TYPE SET SCREWS HAVING SELF-LOCKING SAW-TOOTH ENDS.
- FASTENERS: ALL STEEL FASTENERS FOR TRAFFIC CONTROL SIGNS SHALL BE GALVANIZED AND SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1614 OF THE STANDARD SPECIFICATIONS.
- REFLECTIVE SHEETING: REFLECTIVE SHEETING SHALL BE A MINIMUM OF HIGH INTENSITY PRISMATIC.
- PROCESS INK: ALL PROCESS INK SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 2202 OF THE STANDARD SPECIFICATIONS.
- DETAILS - SNS: THE REFLECTIVE SHEETING FOR THE 9" STANDARD SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVER-WHITE #2 COPY WITH 6" UPPER CASE AND 4 1/2" LOWER CASE PRIMARY COPY AND SUFFIX COPY. BOTH SERIES "C". THE REFLECTIVE SHEETING FOR THE 9" METRO SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVERWHITE #2 COPY WITH 6" UPPER CASE AND 4 1/2" LOWER CASE PRIMARY COPY AND SUFFIX COPY. BOTH SERIES "C". THE CARDINAL DIRECTION CENTERED DIRECTLY BELOW THE BLOCK NUMBER SHALL BE AN UPPER CASE, 3" SERIES "C" LETTER. FACES TO TRIM TO A 8 1/2" WIDTH. (SEE DETAIL B.) FOR OUL-DE-SAC STREETS, A 9" METRO SIZE BLADE SHALL BE USED WITH THE BLOCK NUMBERS DISPLAYED BENEATH THE STREET NAME. IF BLOCK NUMBERS ARE NOT SHOWN ON THE PLANS THE CONTRACTOR SHALL CONTACT THE TRAFFIC ENGINEER AT 268-4501 PRIOR TO MANUFACTURING THE SIGN. SHOP DRAWINGS OF LAYOUT FOR SNS SHALL BE SUBMITTED TO THE TRAFFIC ENGINEERING DIVISION OF THE CITY OF WICHITA FOR APPROVAL PRIOR TO FABRICATION. THE FINISHED SIGNS AS SUPPLIED SHALL BE OF GOOD APPEARANCE, FREE FROM RAGGED EDGES, CRACKS SCALES OR BLISTERS AND SHALL BE CLEAN-CUT. SIGNS SHALL BE PACKED IN SUCH MANNER AS TO PREVENT DAMAGE OR DEFACEMENT DURING SHIPMENT OR STORAGE.
- PERMANENT TRAFFIC CONTROL AND SNS: PERMANENT TRAFFIC CONTROL AND SNS SHALL BE MEASURED AND PAID FOR AT THE LUMP SUM PRICE FOR SIGNING. THE PAYMENT AS SET FORTH ABOVE SHALL BE CONSIDERED FULL COMPENSATION FOR ALL EXCAVATION, BACKFILLING, POSTS, ANCHORS, FASTENERS, MATERIALS, LABOR, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK.

REVISED: December 2018 | TM | Updated Edition year and Subsection Number in Notes

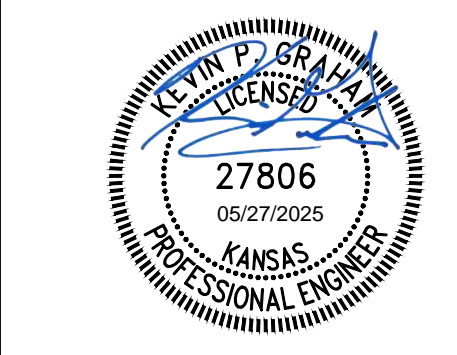
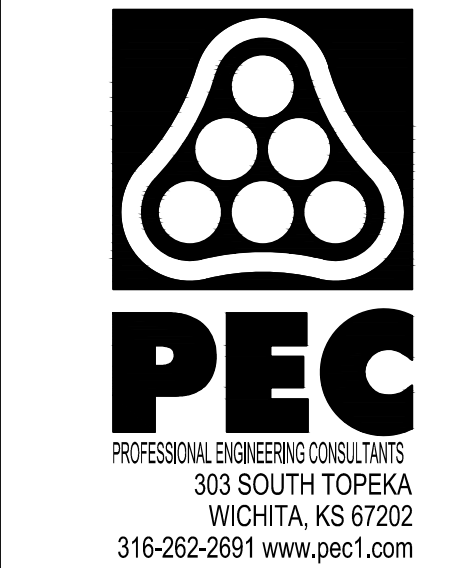
TRAFFIC SIGNAL INSTALLATION DETAIL SHEET

TRAFFIC ENGINEER
MIKE ARMOUR, P.E.

PROJECT NUMBER: 472-2024-086026 | ORG NUMBER: | DATE: | SHEET: TR-112 of

CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 455 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

PUBLIC WORKS & UTILITIES ENGINEERING DIVISION

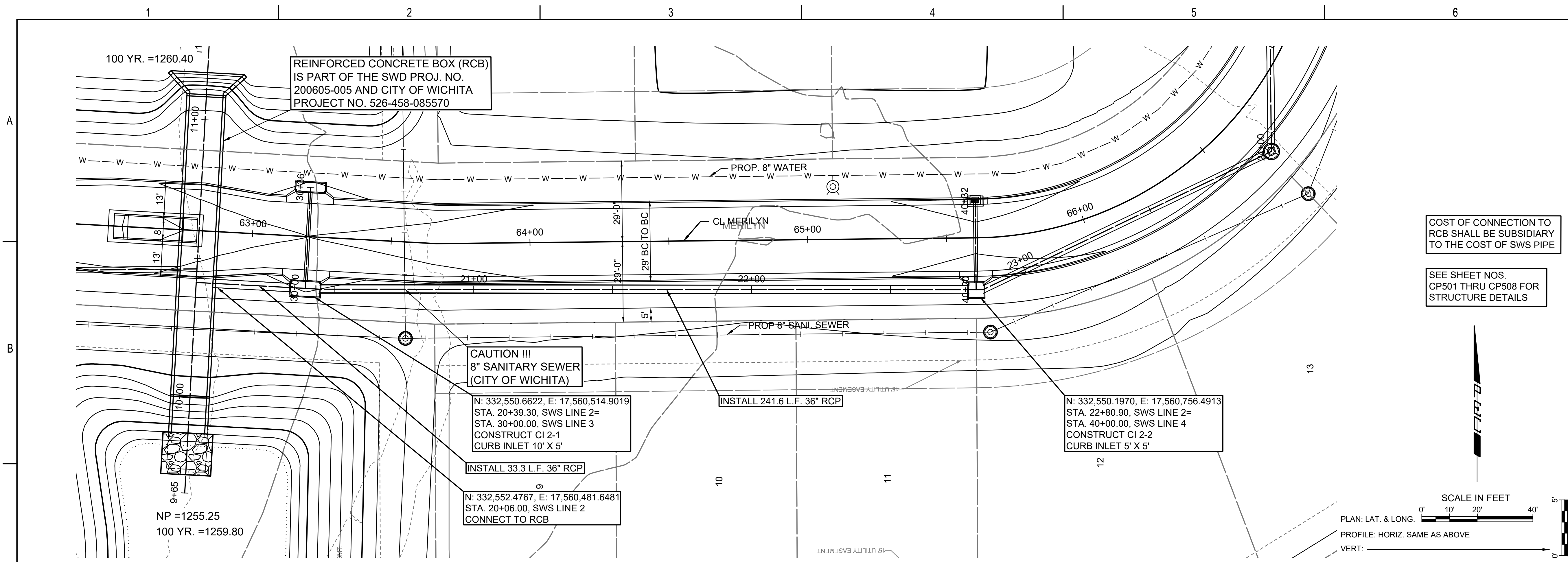


PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
 SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

ISSUE:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BSJ
CHECKED BY	KMS

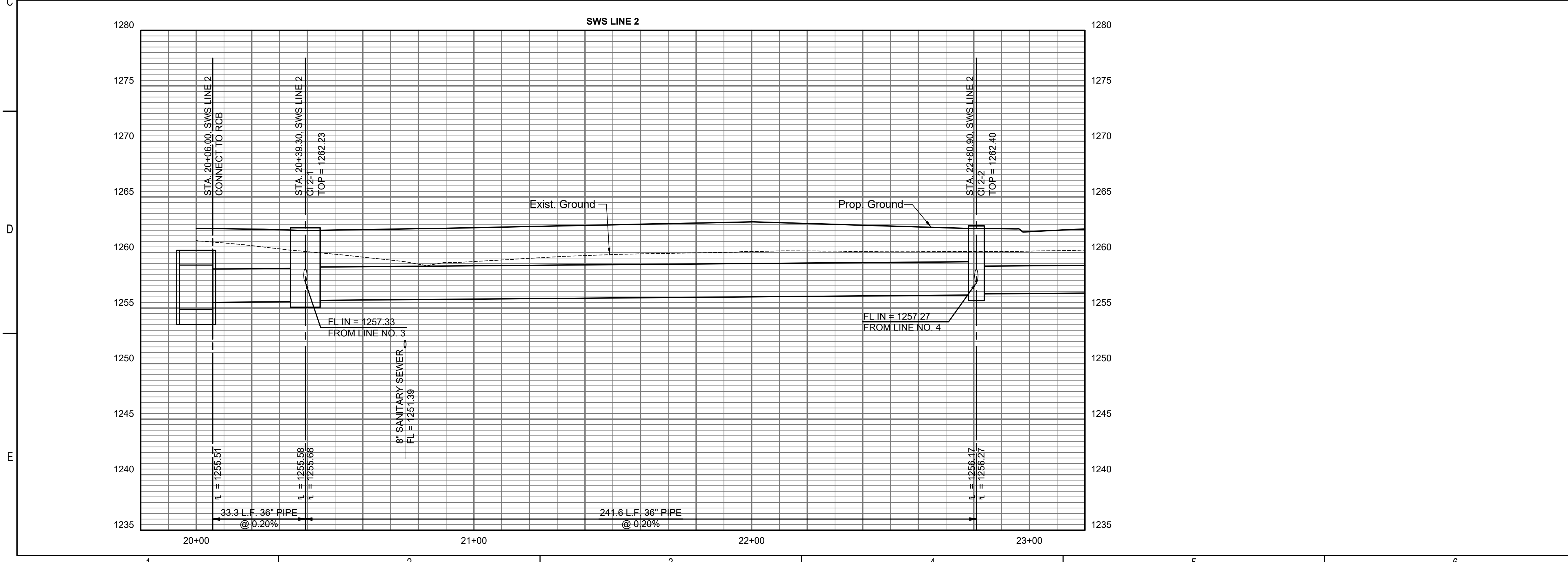
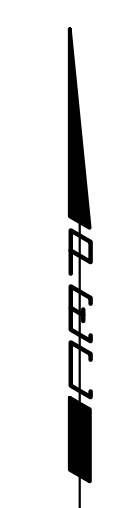
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 24 OF 71

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 LINE 2.DWG



COST OF CONNECTION TO RCB SHALL BE SUBSIDIARY TO THE COST OF SWS PIPE

SEE SHEET NOS. CP501 THRU CP508 FOR STRUCTURE DETAILS



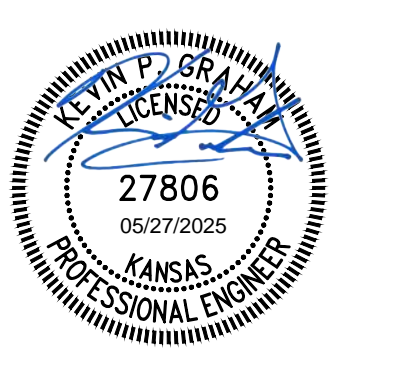
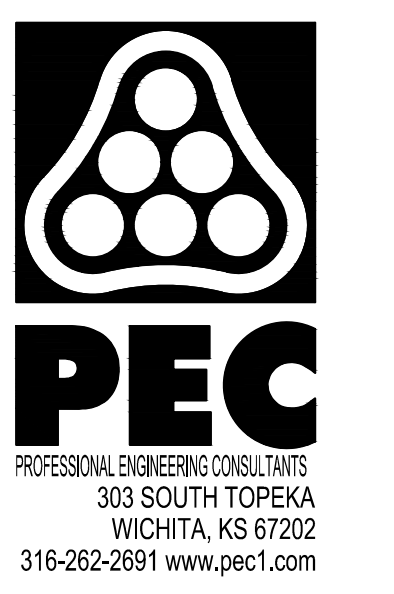
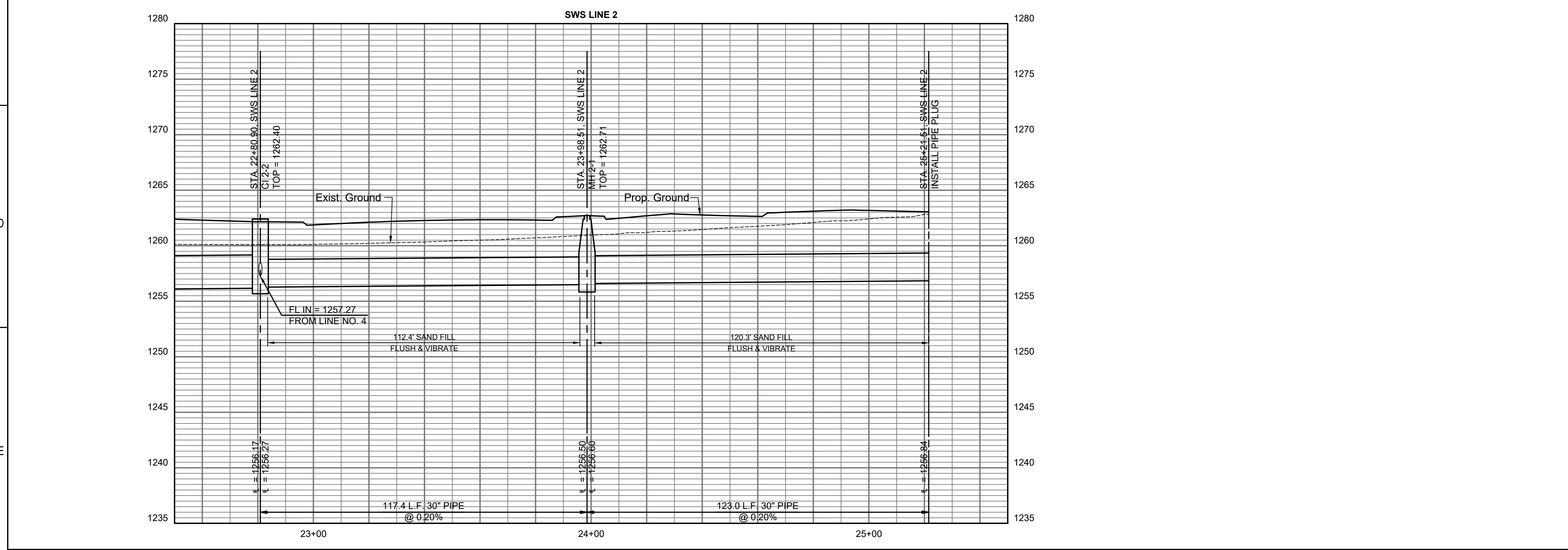
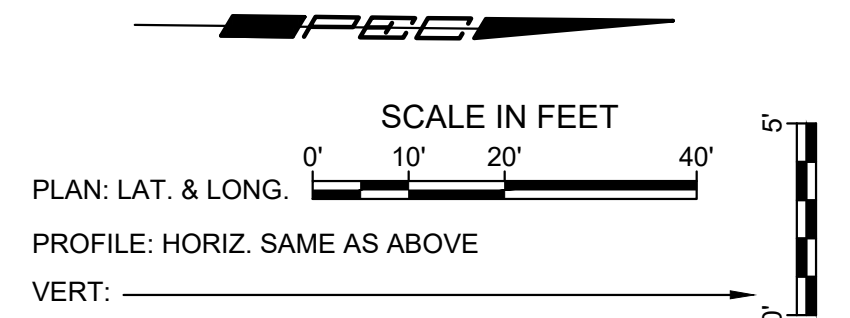
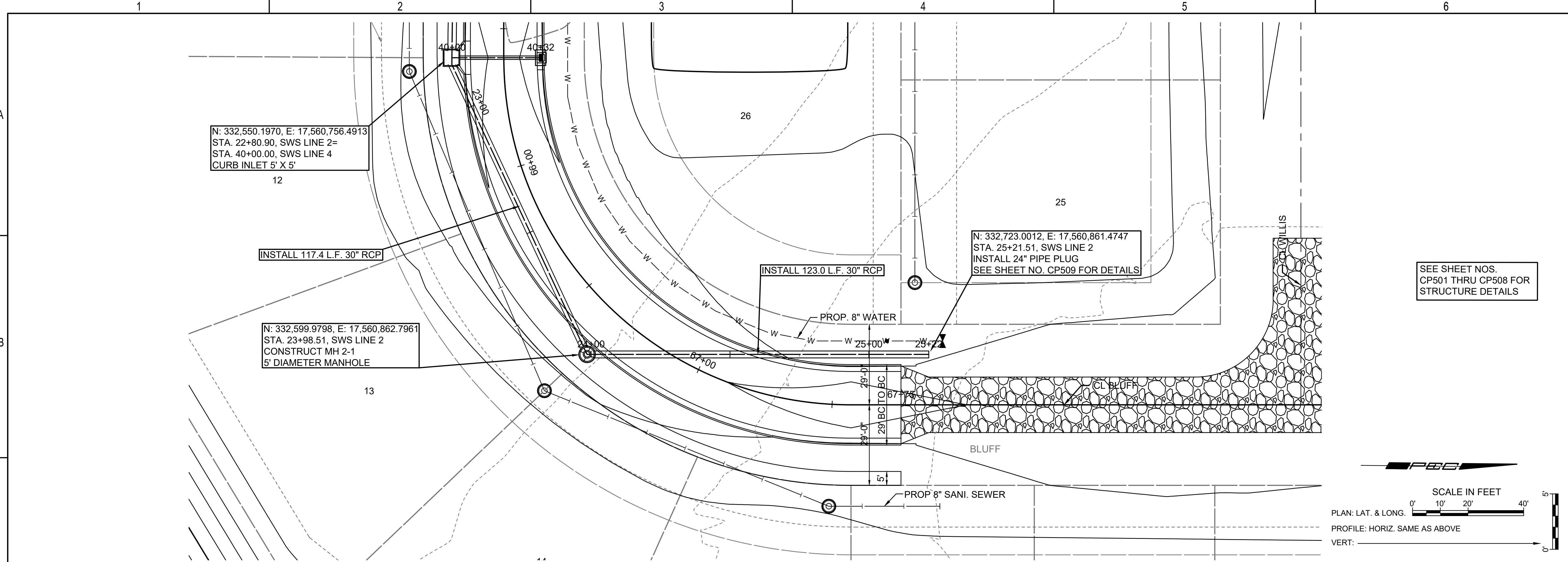
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS
PLAN AND PROFILE-SWS LINE 2	
CP401	
25 OF 71	

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 LINE 2.DWG



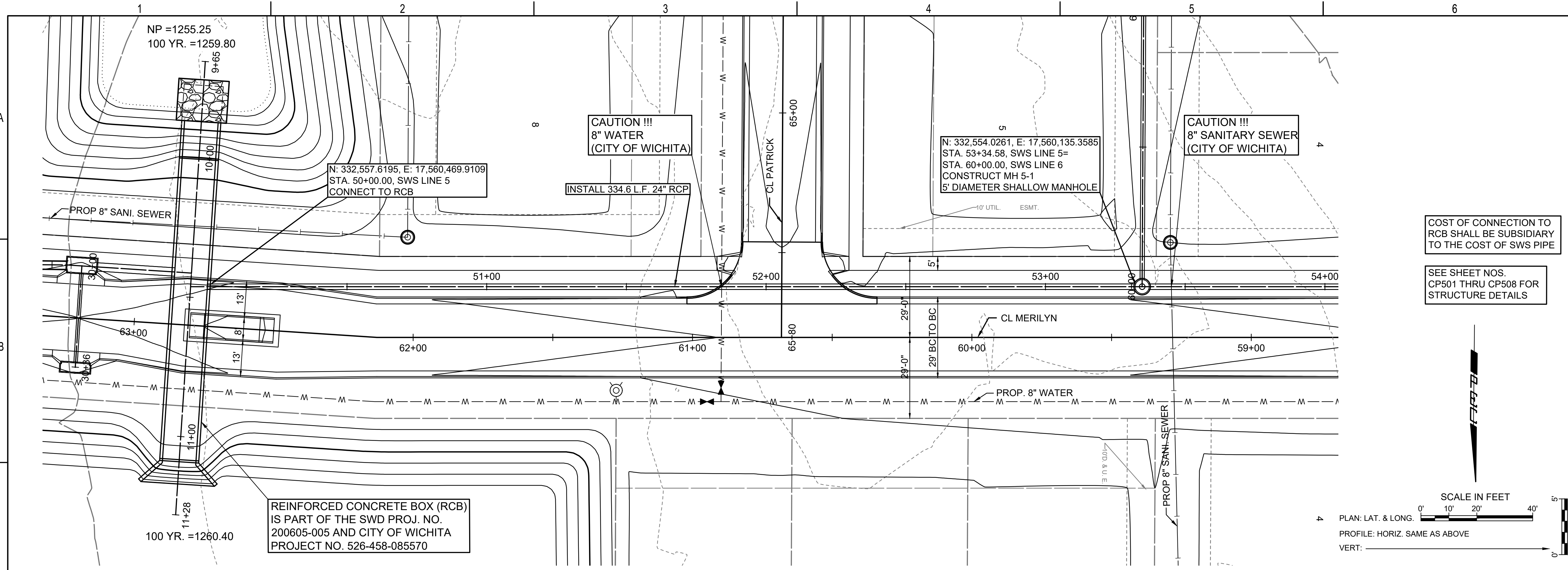
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
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CHECKED BY	KMS

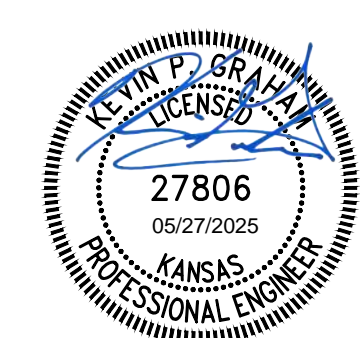
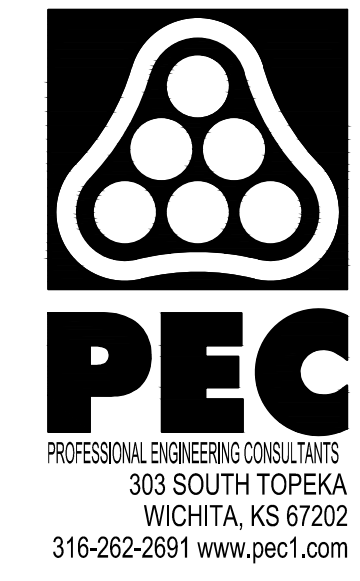
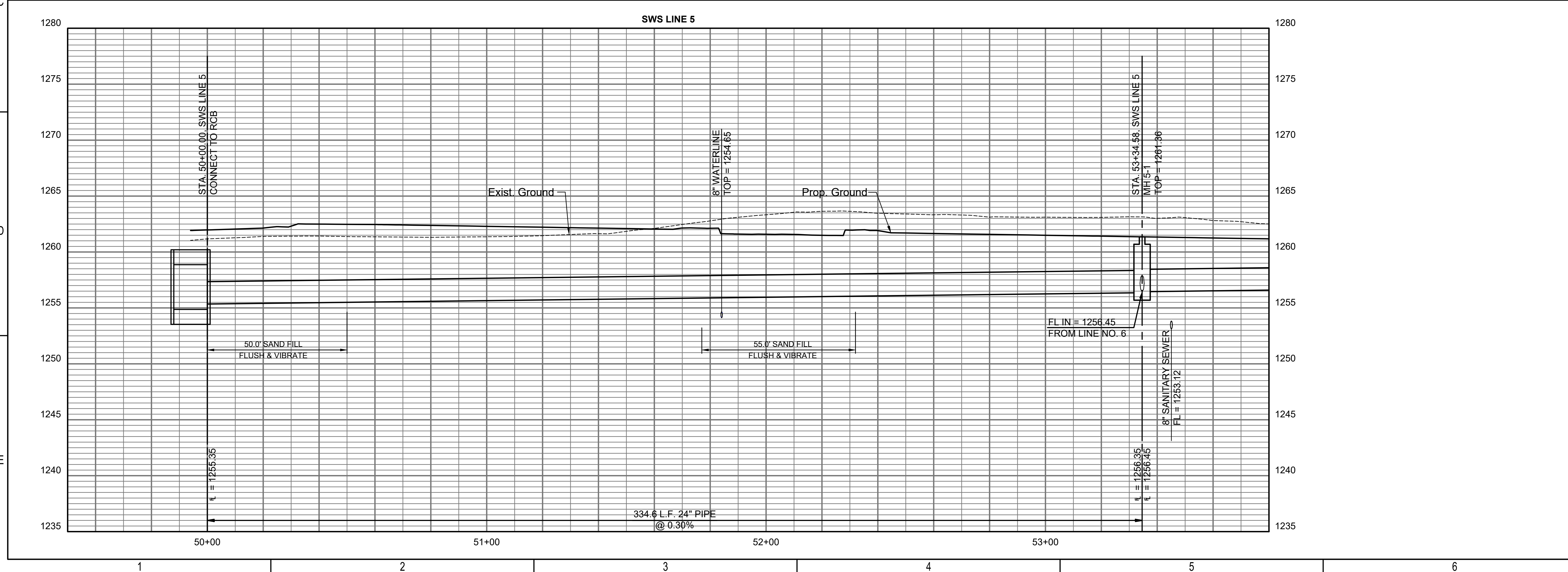
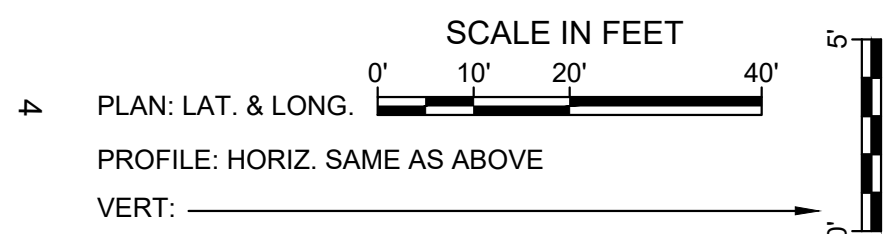
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 LINE 2
CP402
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 LINE 5.DWG



COST OF CONNECTION TO RCB SHALL BE SUBSIDIARY TO THE COST OF SWS PIPE

SEE SHEET NOS. CP501 THRU CP508 FOR STRUCTURE DETAILS



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

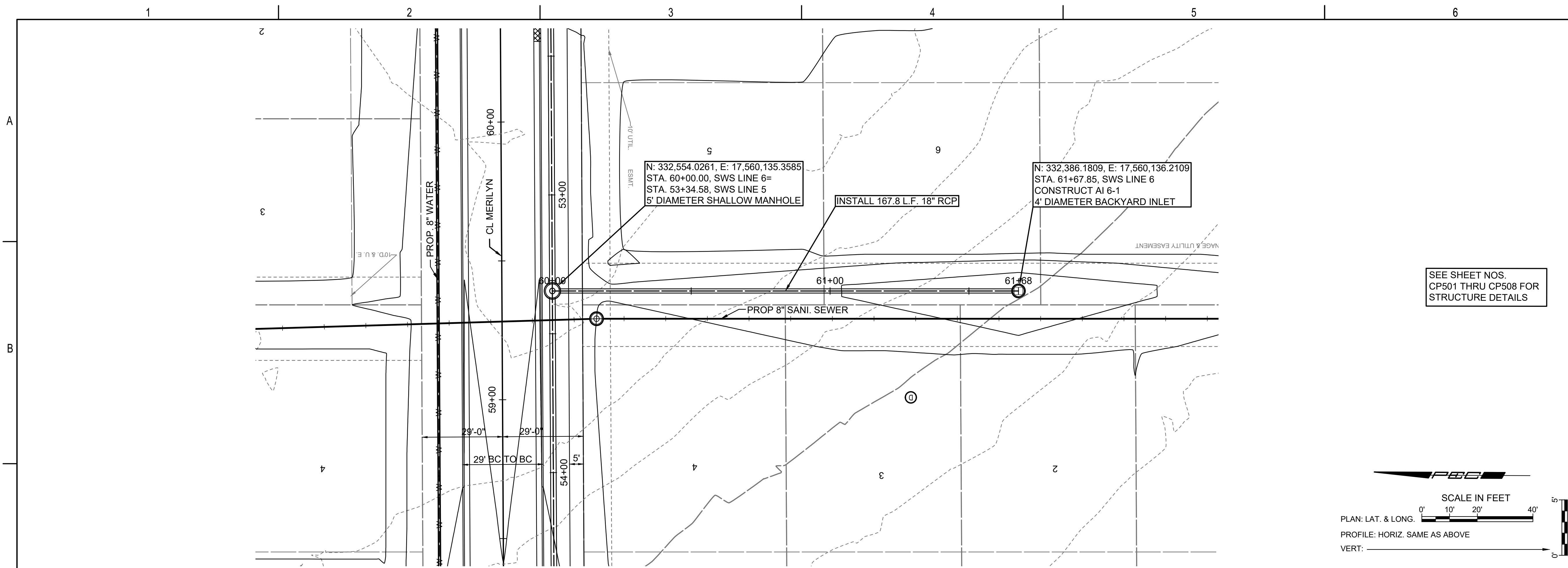
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JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
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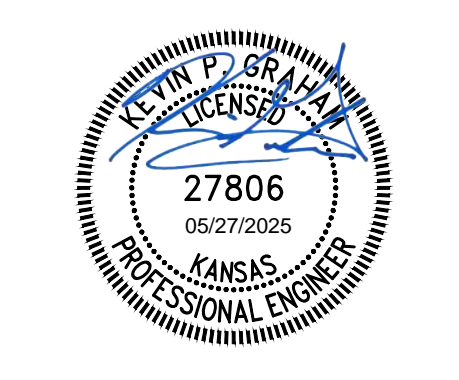
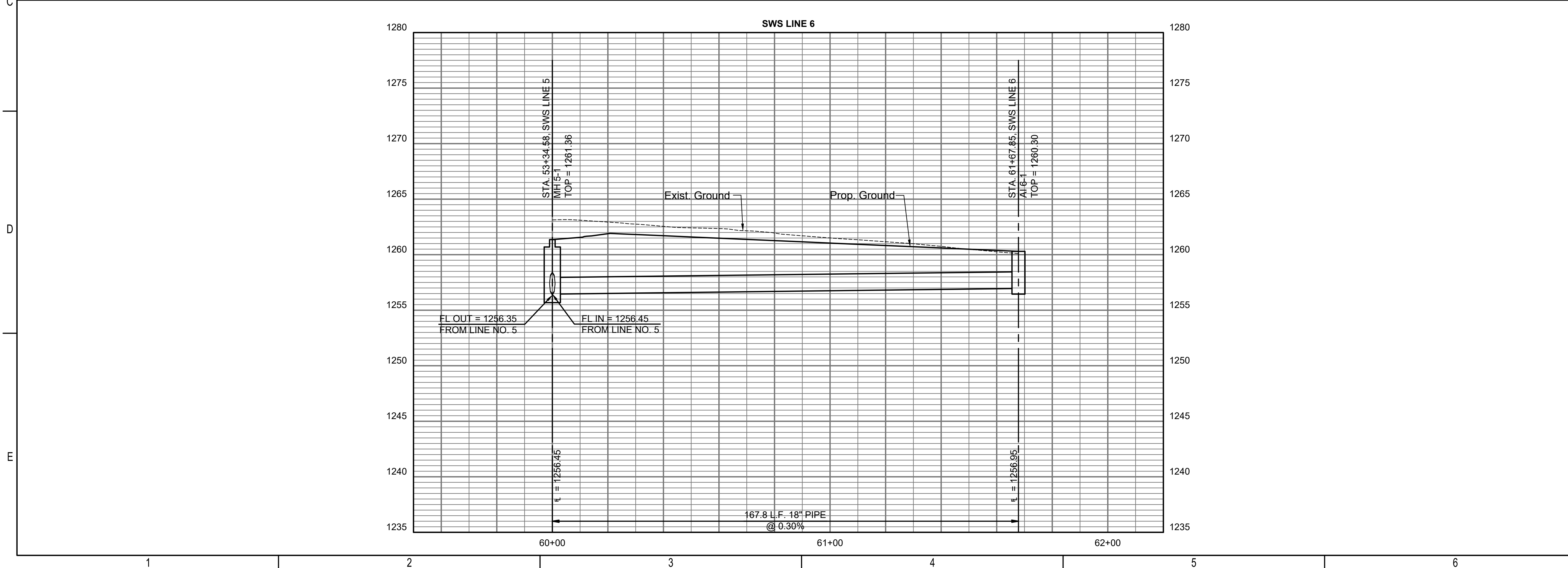
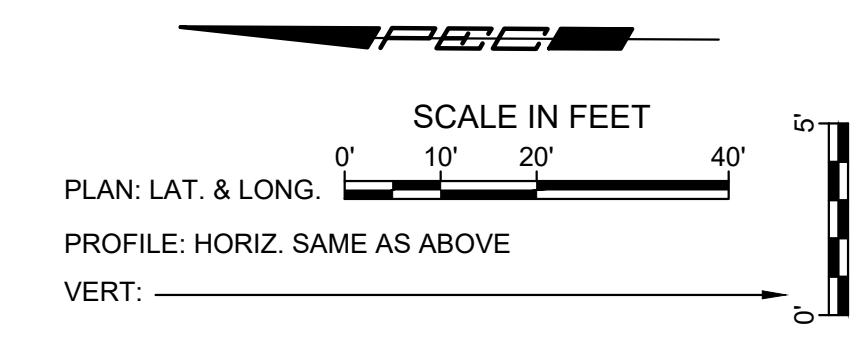
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CP404
28 OF 71

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 LINE 6.DWG



SEE SHEET NOS.
 CP501 THRU CP508 FOR
 STRUCTURE DETAILS



PAYING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

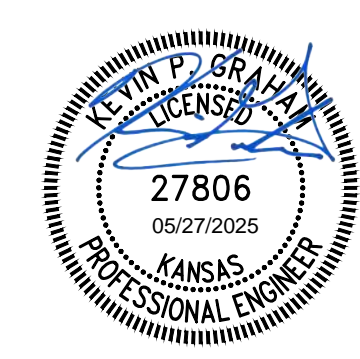
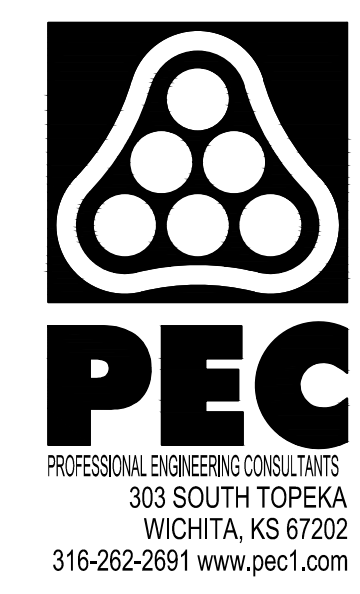
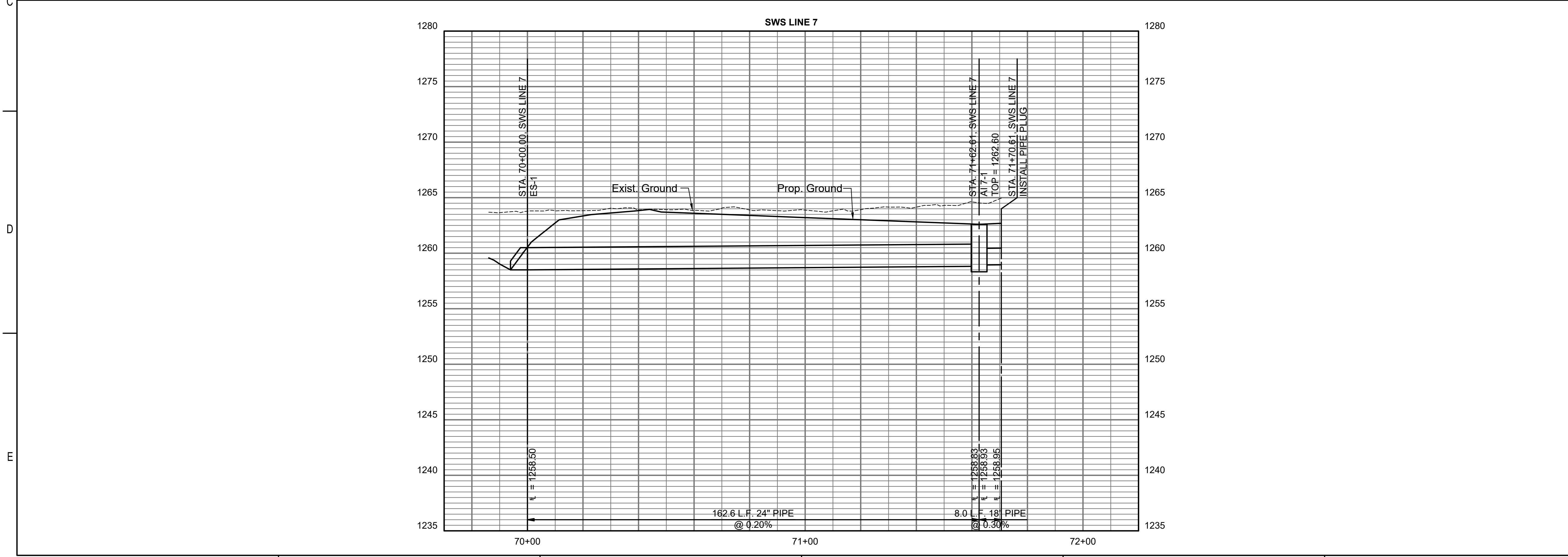
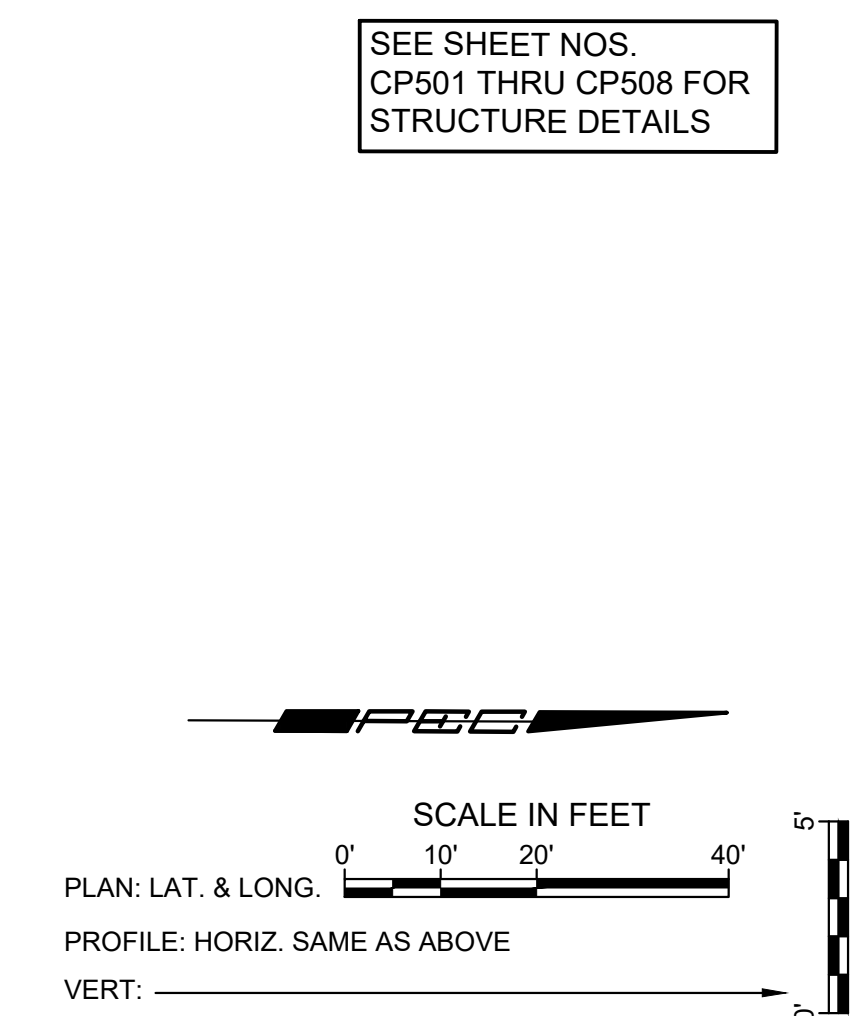
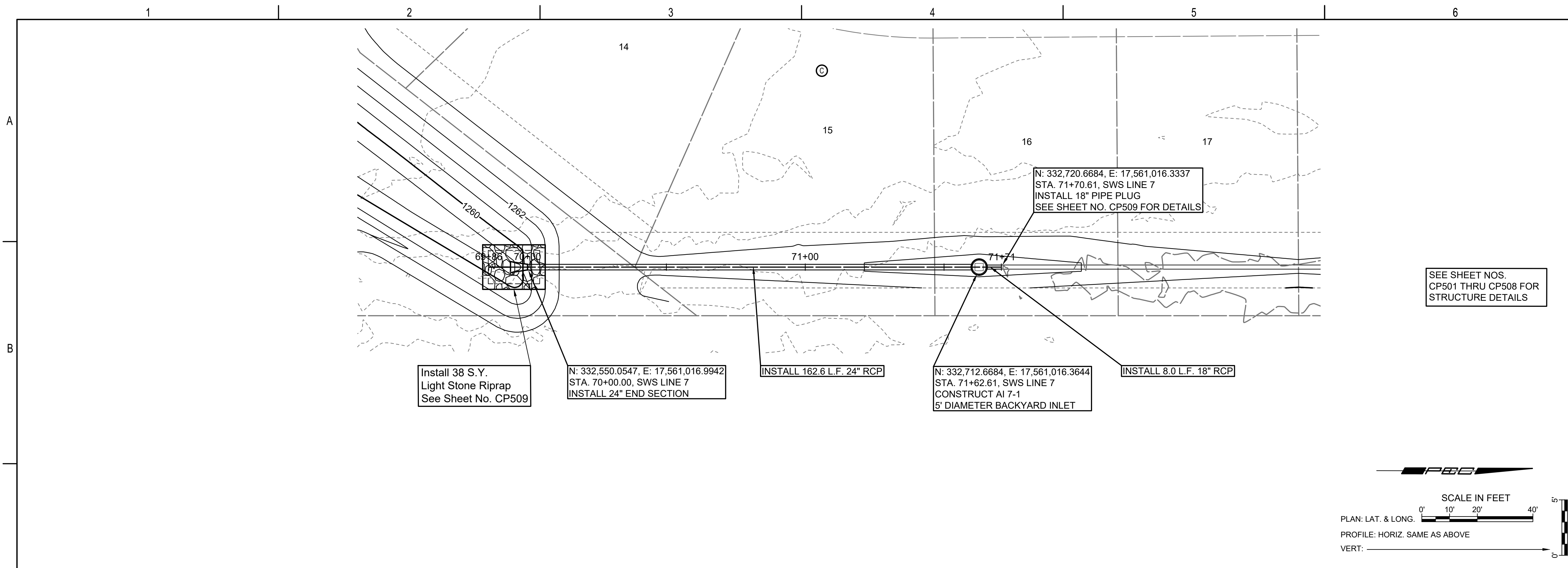
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JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
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CHECKED BY	KMS

PLAN AND PROFILE-SWS
 LINE 6

CP406
 30 OF 71

SAVED 9/3/2024 11:43:31 AM BY BILL_SEXSON
 PLOTTED 5/27/2025 9:42:55 AM BY KEVIN GRAHAM
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 LINE 7.DWG



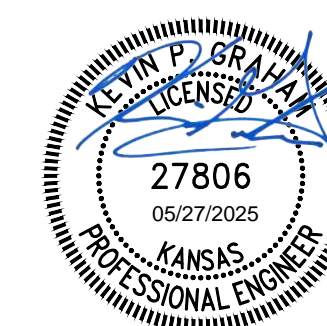
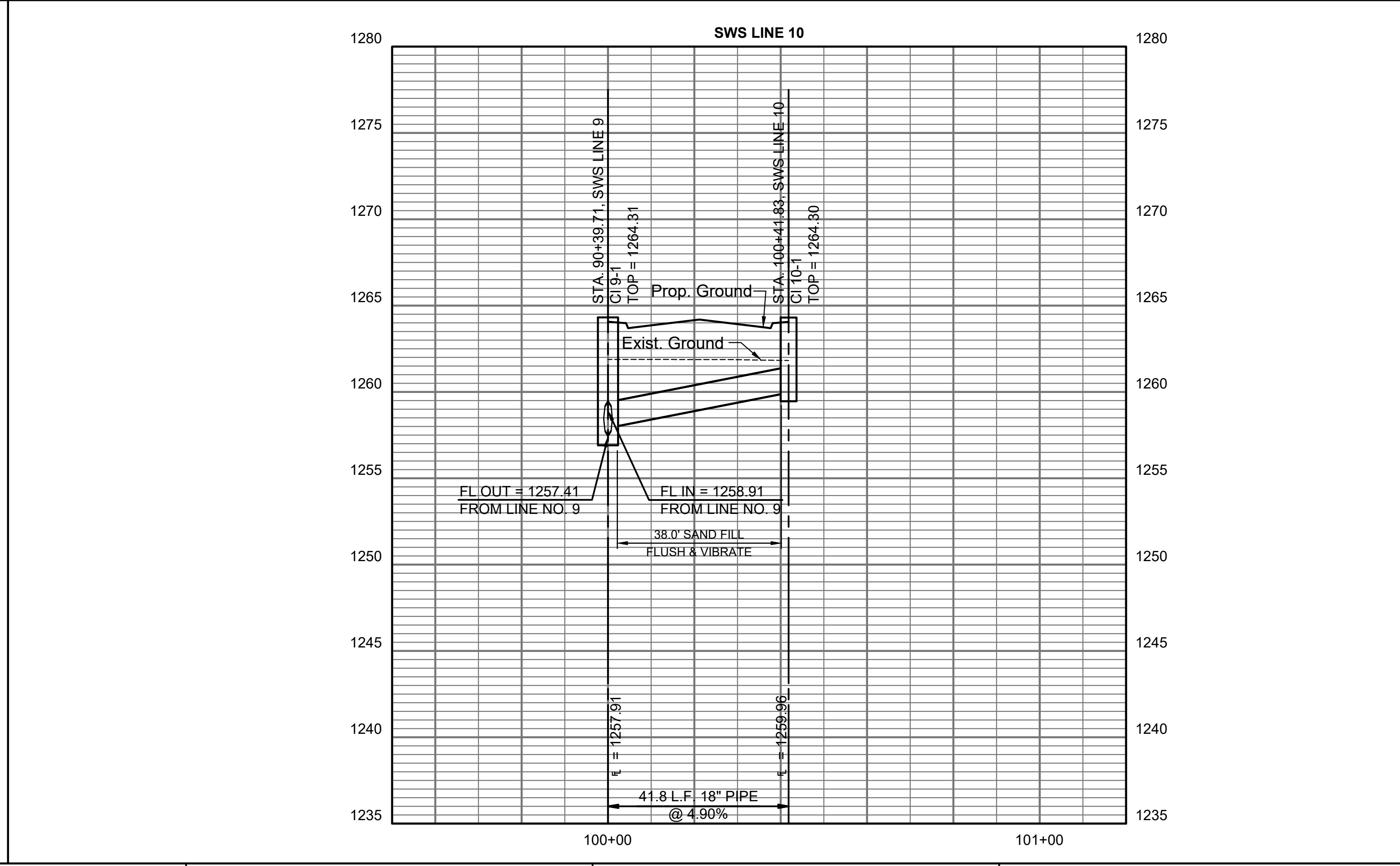
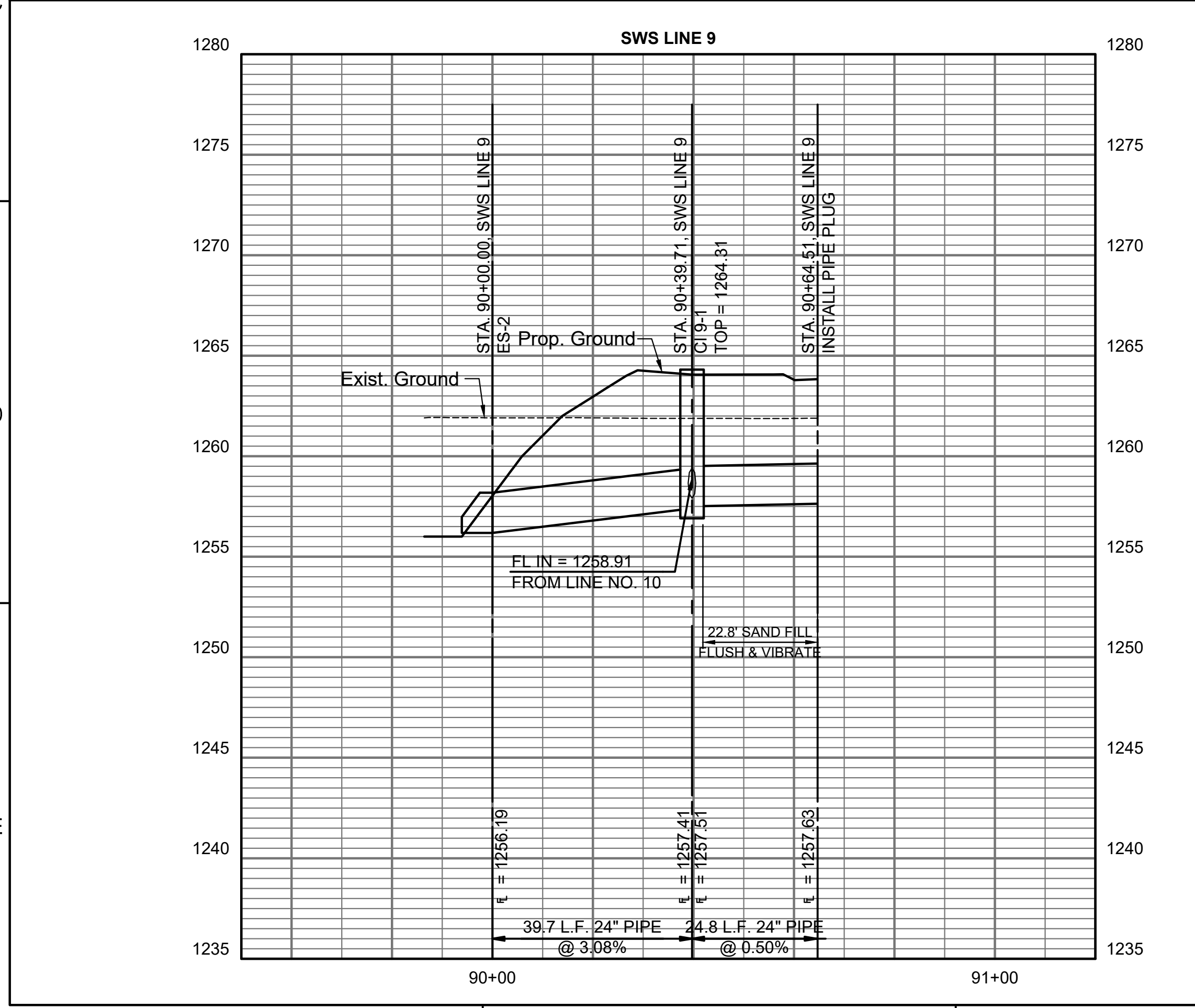
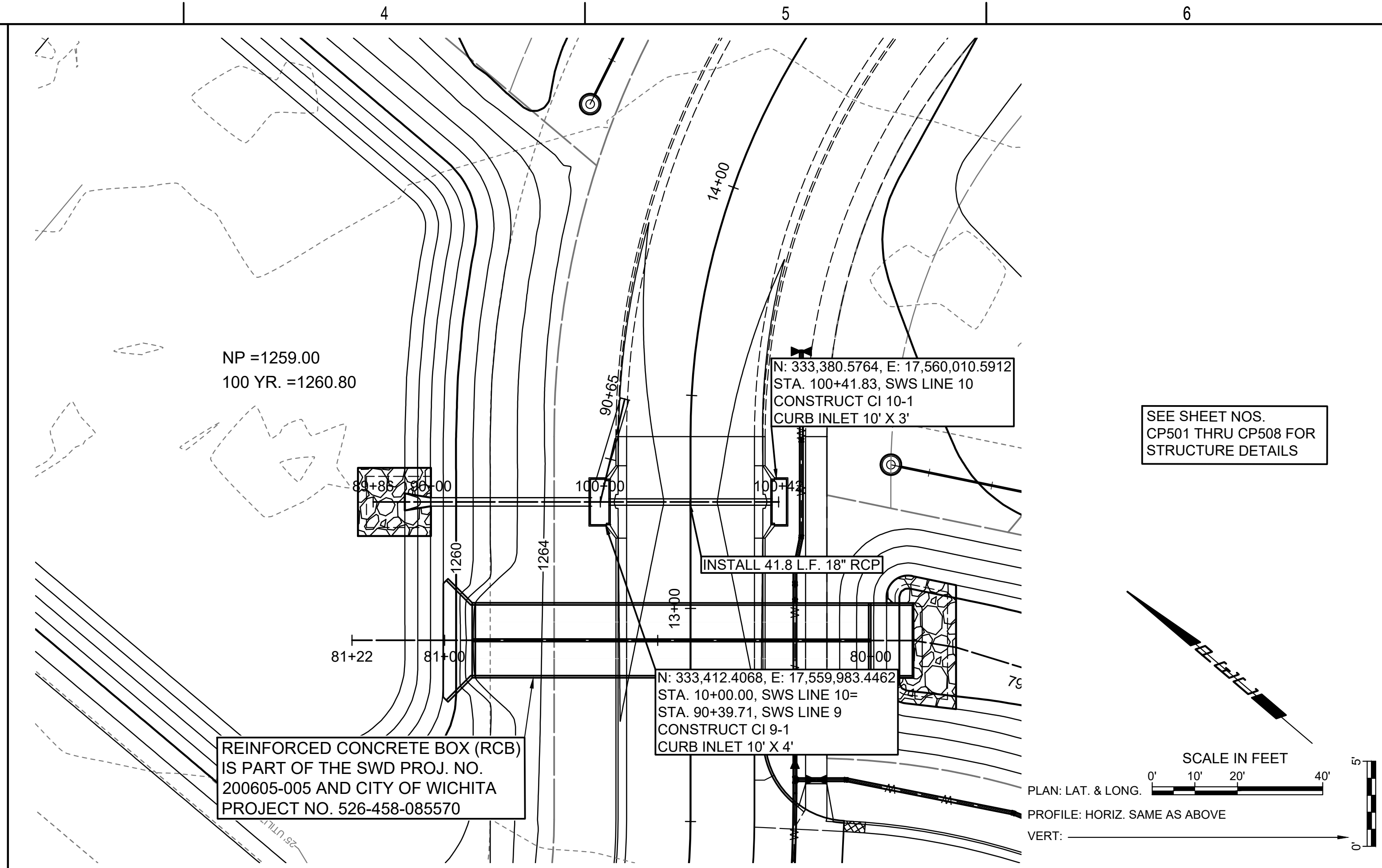
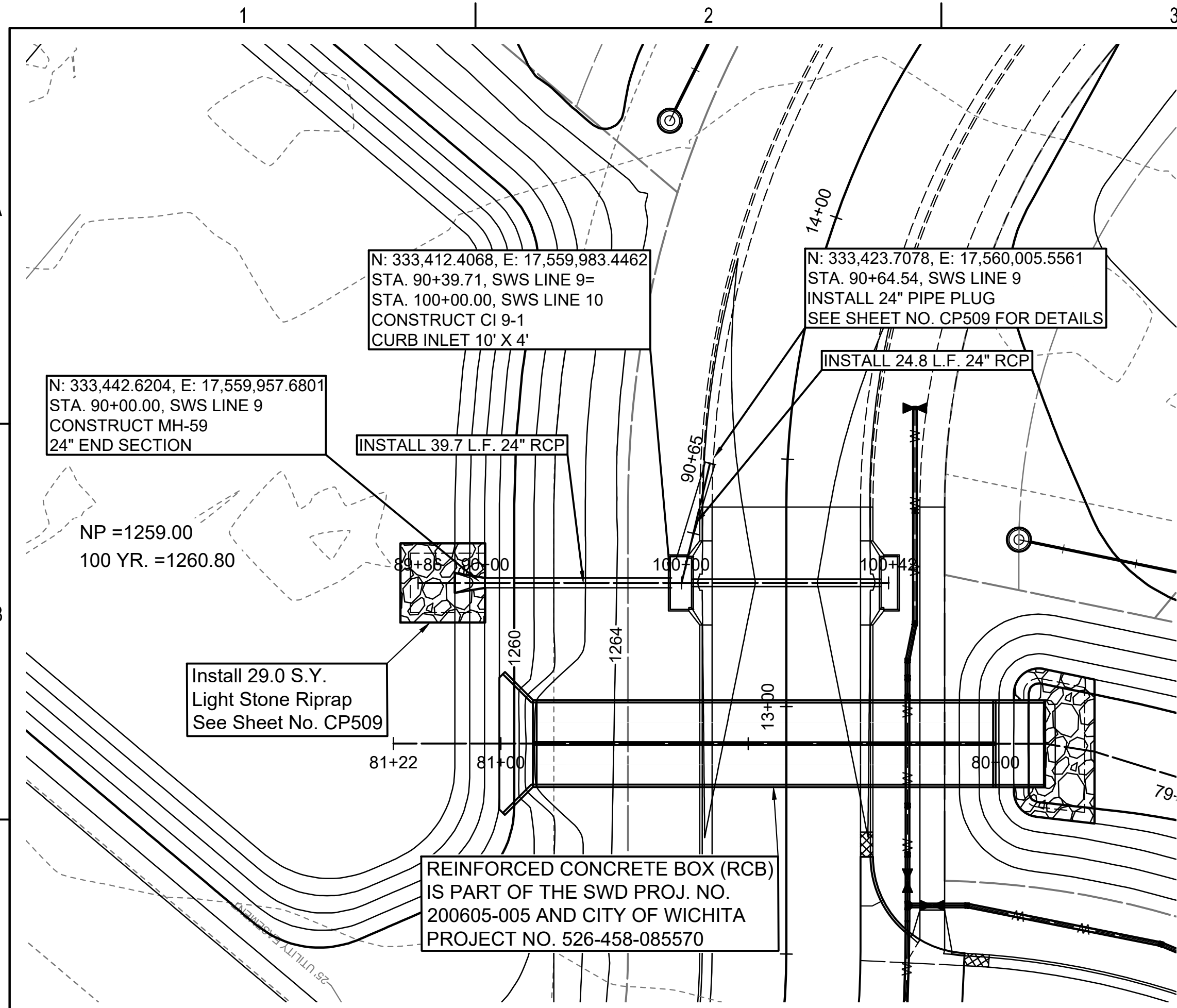
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

PLAN AND PROFILE-SWS LINE 7
CP407
 31 OF 71

SAVED 9/3/2024 11:44:55 AM BY BILL_SEXSON
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 LINES 9 AND 10.DWG



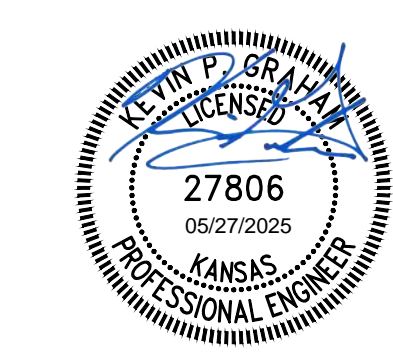
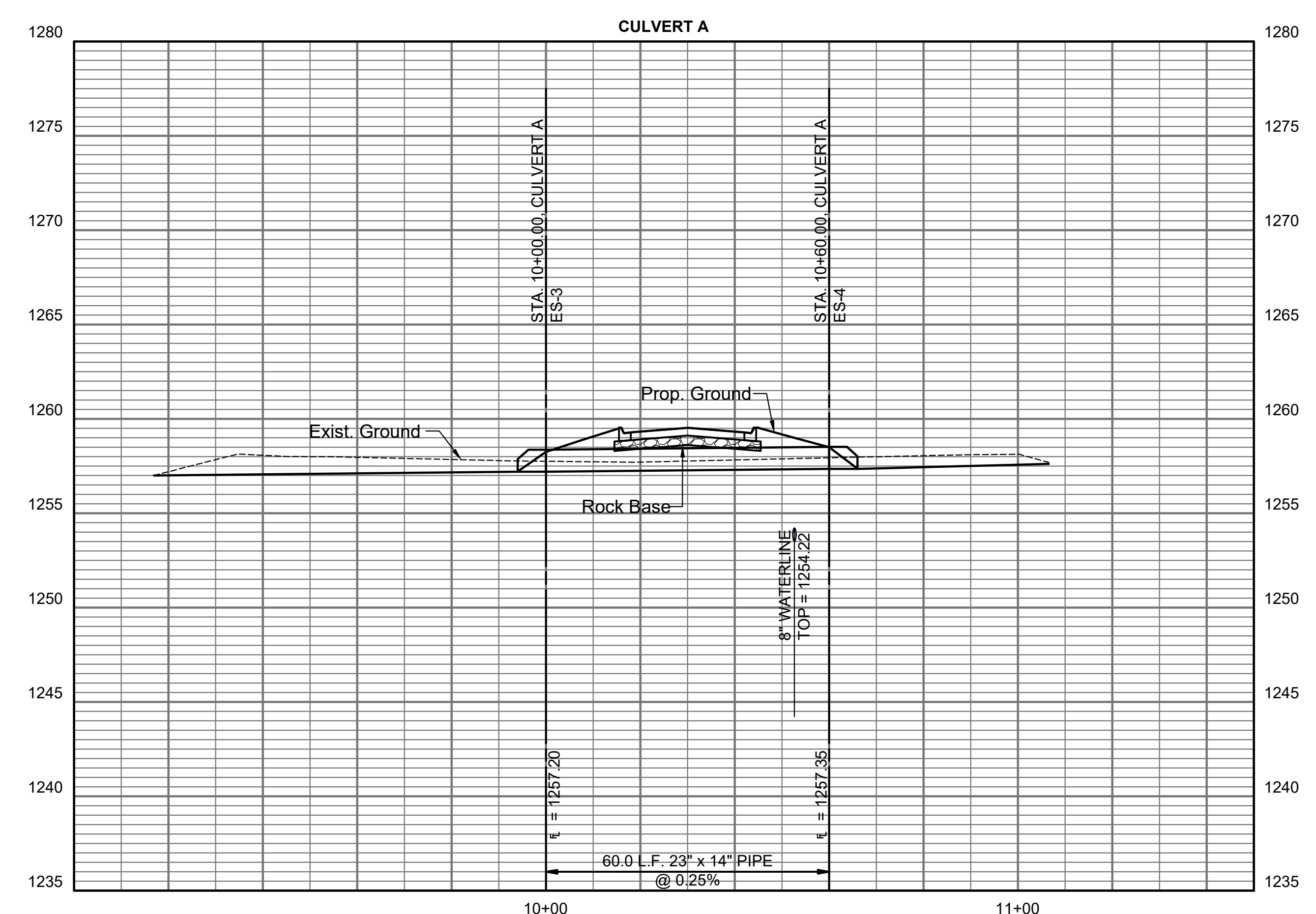
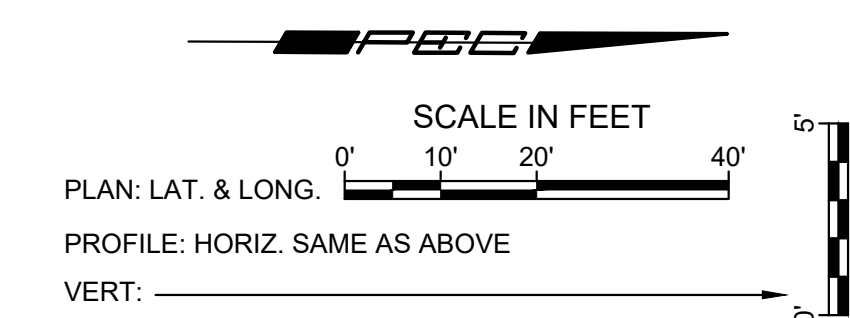
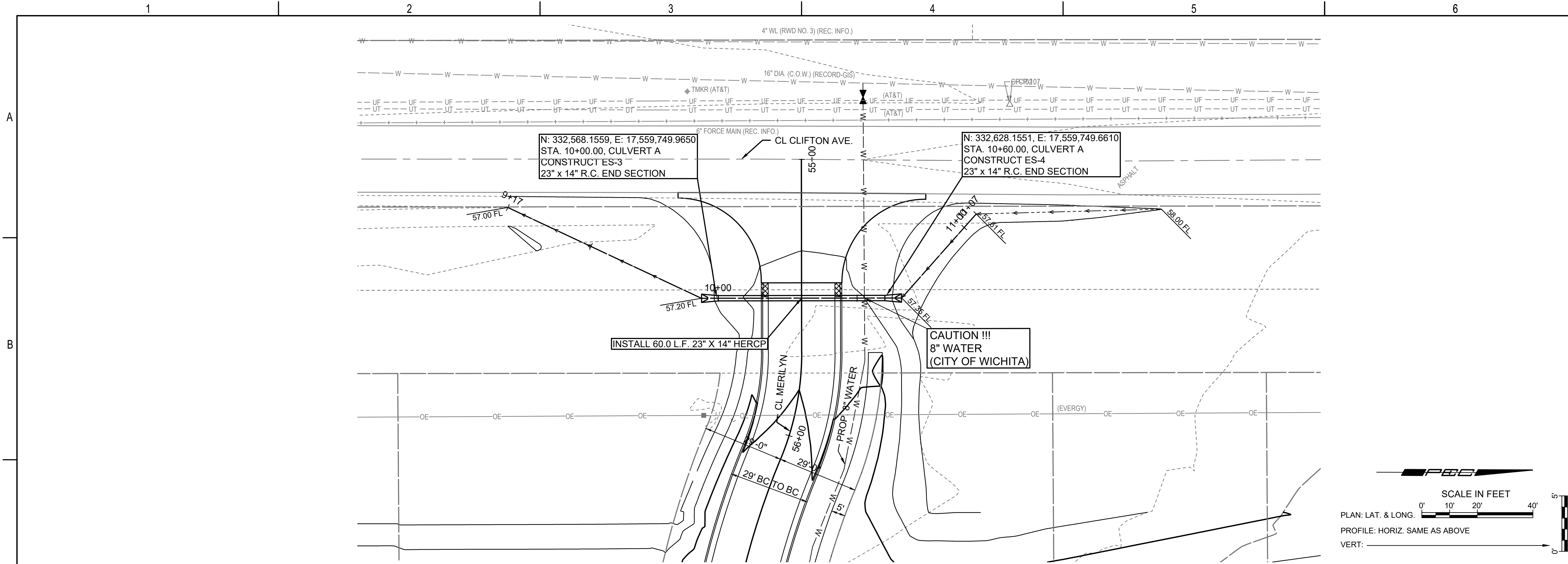
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

PLAN AND PROFILE-SWS
 LINES 9 AND 10

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 PROFILE-CULVERT A.DWG



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

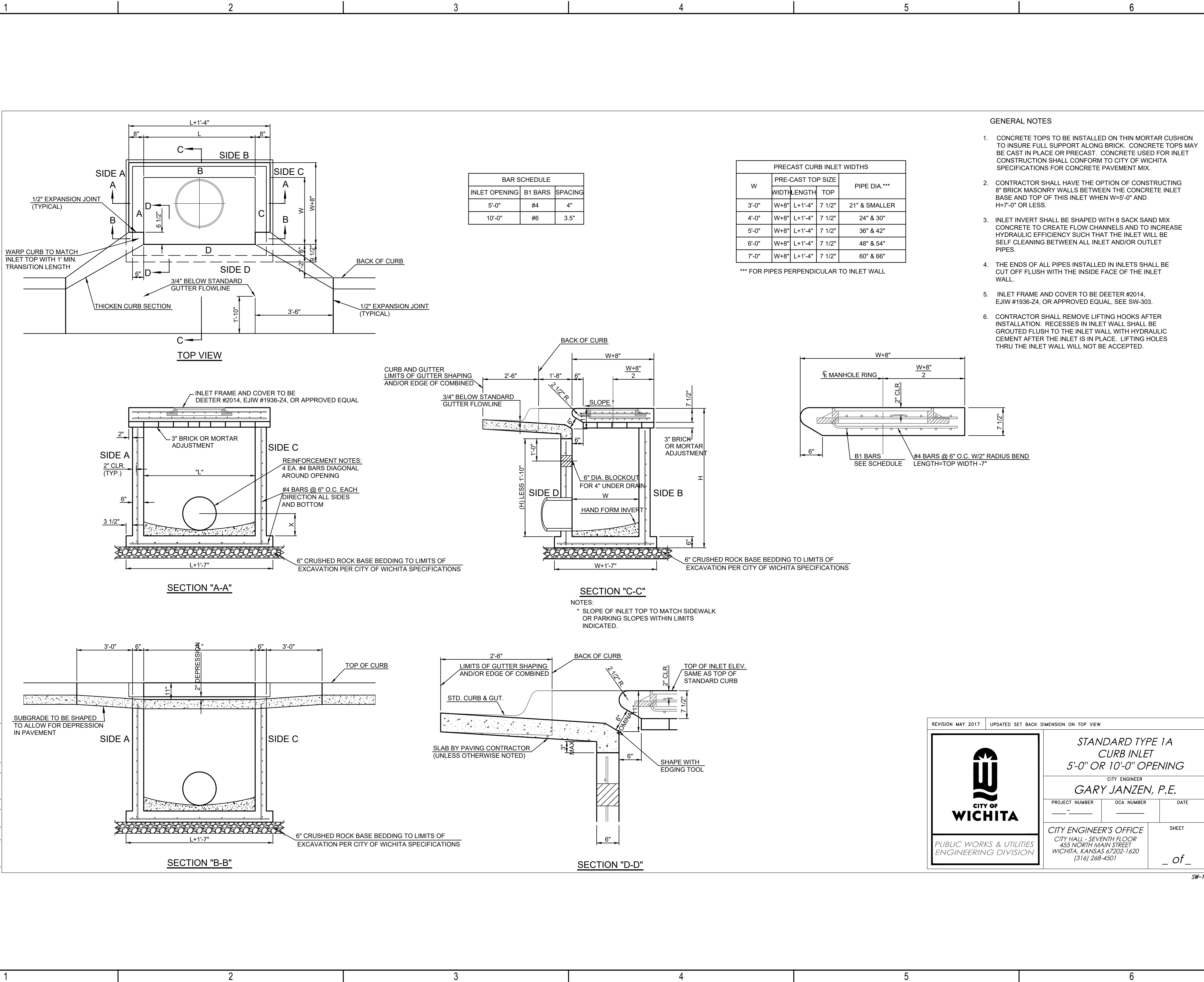
Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

PLAN AND
 PROFILE-CULVERT A
CP409
 33 OF 71

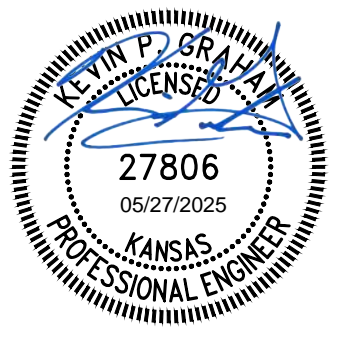
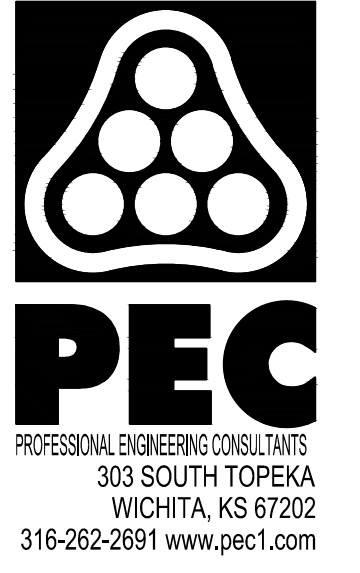
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 INLET DETAILS.DWG

Save: 08-01-2024 10:56:58 AM by BILL SEXSON
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GENERAL NOTES

- CONCRETE TOPS TO BE INSTALLED ON THIN MORTAR CUSHION TO INSURE FULL SUPPORT ALONG BRICK. CONCRETE TOPS MAY BE CAST IN PLACE OR PRECAST. CONCRETE USED FOR INLET CONSTRUCTION SHALL CONFORM TO CITY OF WICHITA SPECIFICATIONS FOR CONCRETE PAVEMENT MIX.
- CONTRACTOR SHALL HAVE THE OPTION OF CONSTRUCTING 8" BRICK MASONRY WALLS BETWEEN THE CONCRETE INLET BASE AND TOP OF THIS INLET WHEN W=5'-0" AND H=7'-0" OR LESS.
- INLET INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE INLET WILL BE SELF CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.
- THE ENDS OF ALL PIPES INSTALLED IN INLETS SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE INLET WALL.
- INLET FRAME AND COVER TO BE DEETER #2014, EJIW #1936-Z4, OR APPROVED EQUAL, SEE SW-303.
- CONTRACTOR SHALL REMOVE LIFTING HOOKS AFTER INSTALLATION. RECESSES IN INLET WALL SHALL BE GROUTED FLUSH TO THE INLET WALL WITH HYDRAULIC CEMENT AFTER THE INLET IS IN PLACE. LIFTING HOLES THRU THE INLET WALL WILL NOT BE ACCEPTED.



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

REVISION MAY 2017	UPDATED SET BACK DIMENSION ON TOP VIEW
CITY OF WICHITA	
STANDARD TYPE 1A CURB INLET 5'-0" OR 10'-0" OPENING	
CITY ENGINEER GARY JANZEN, P.E.	
PROJECT NUMBER	OCA NUMBER
DATE	
CITY ENGINEER'S OFFICE	
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4301	
SHEET _ of _	

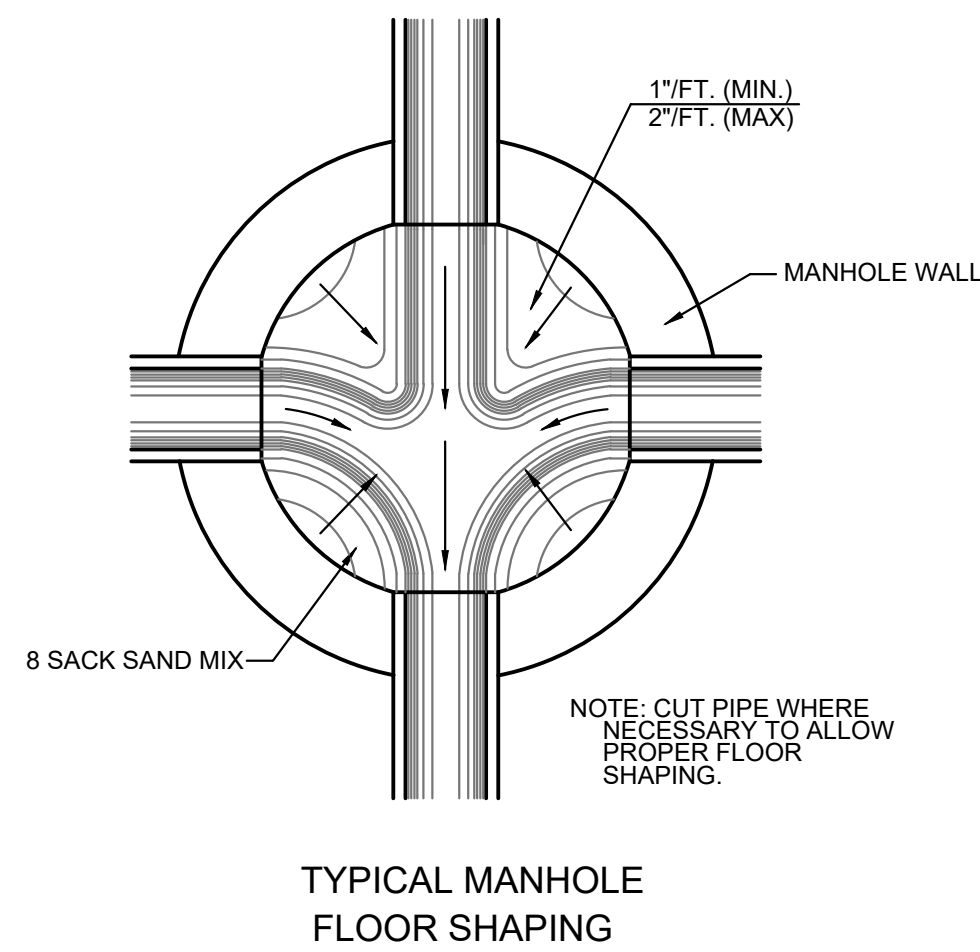
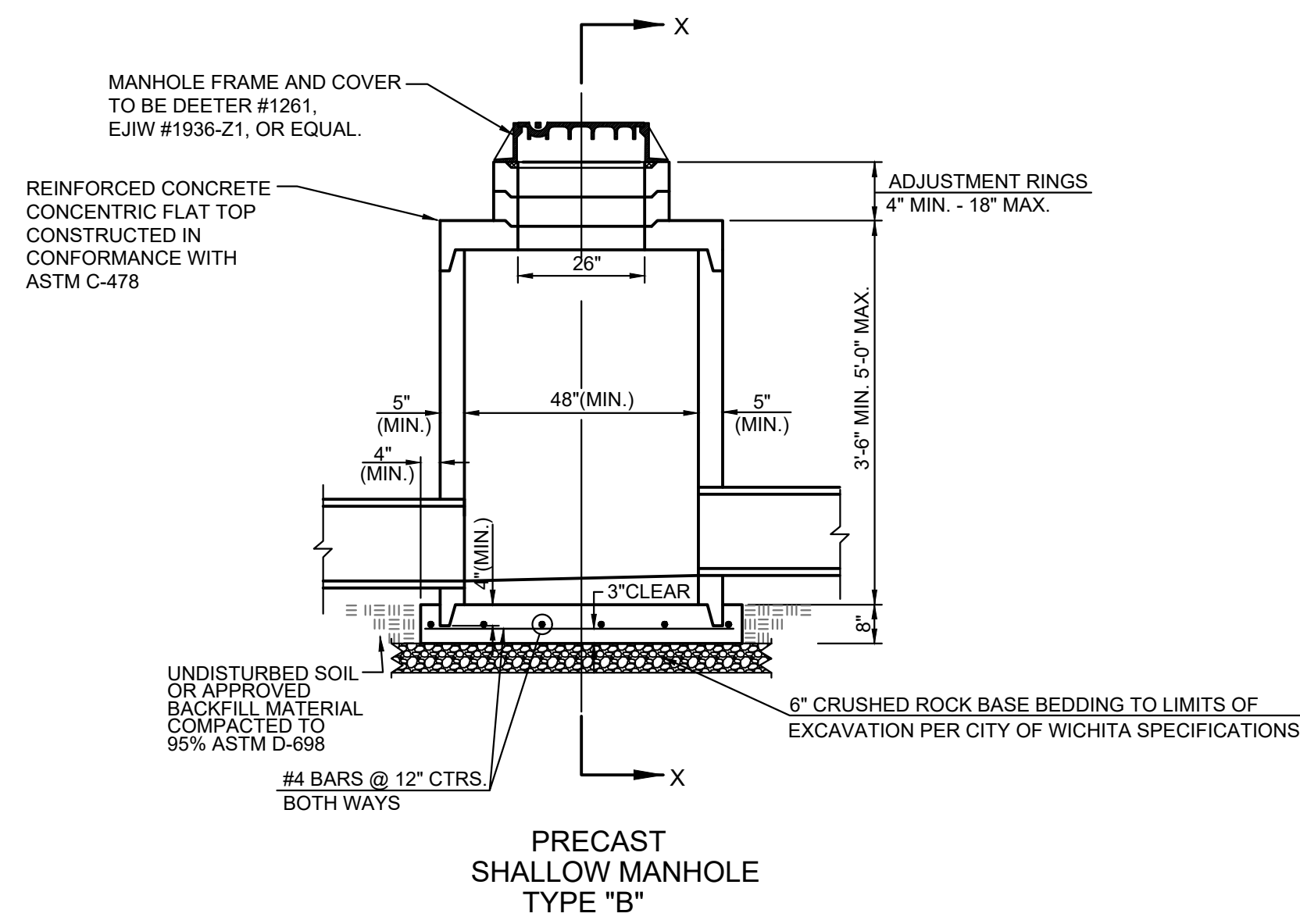
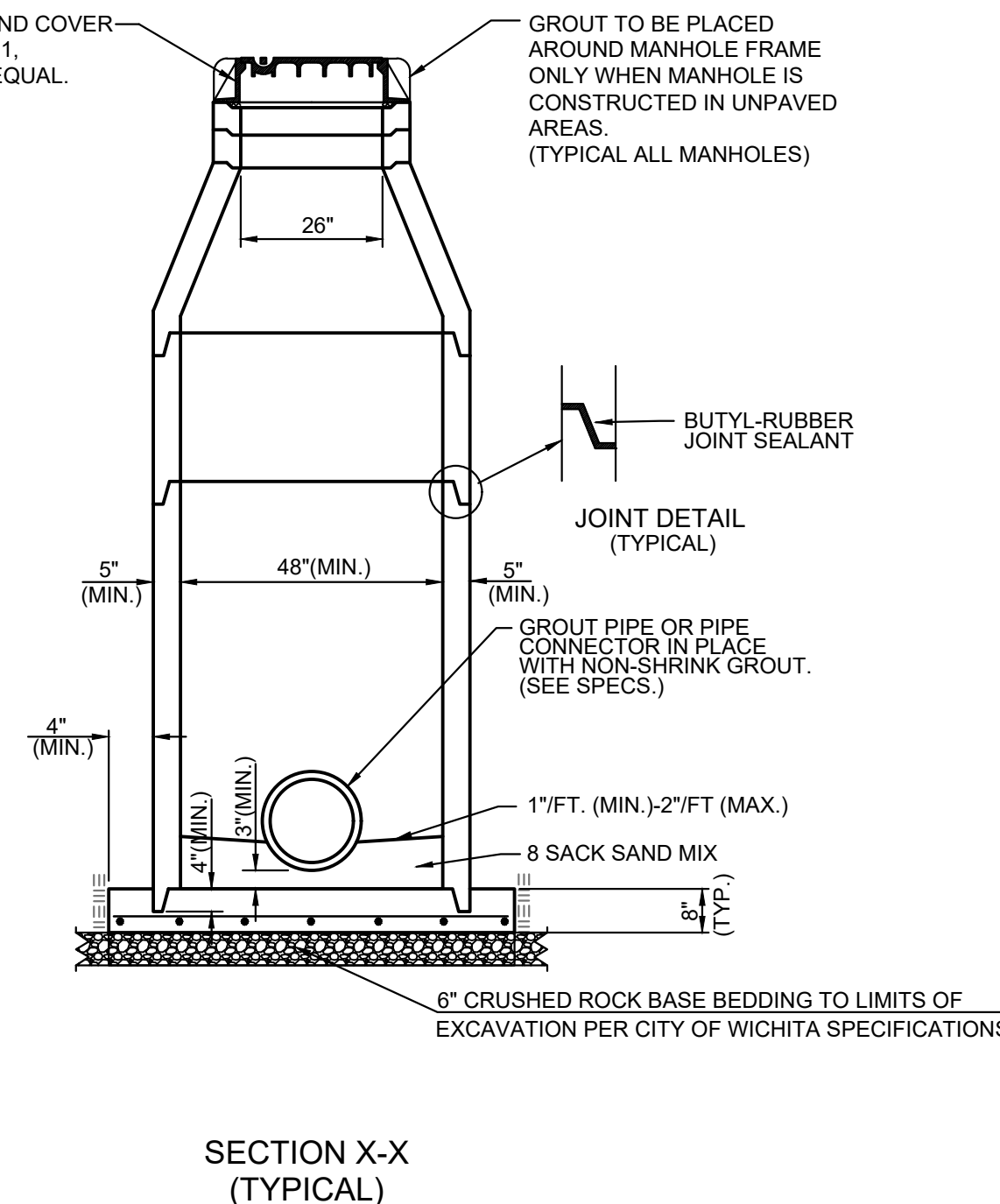
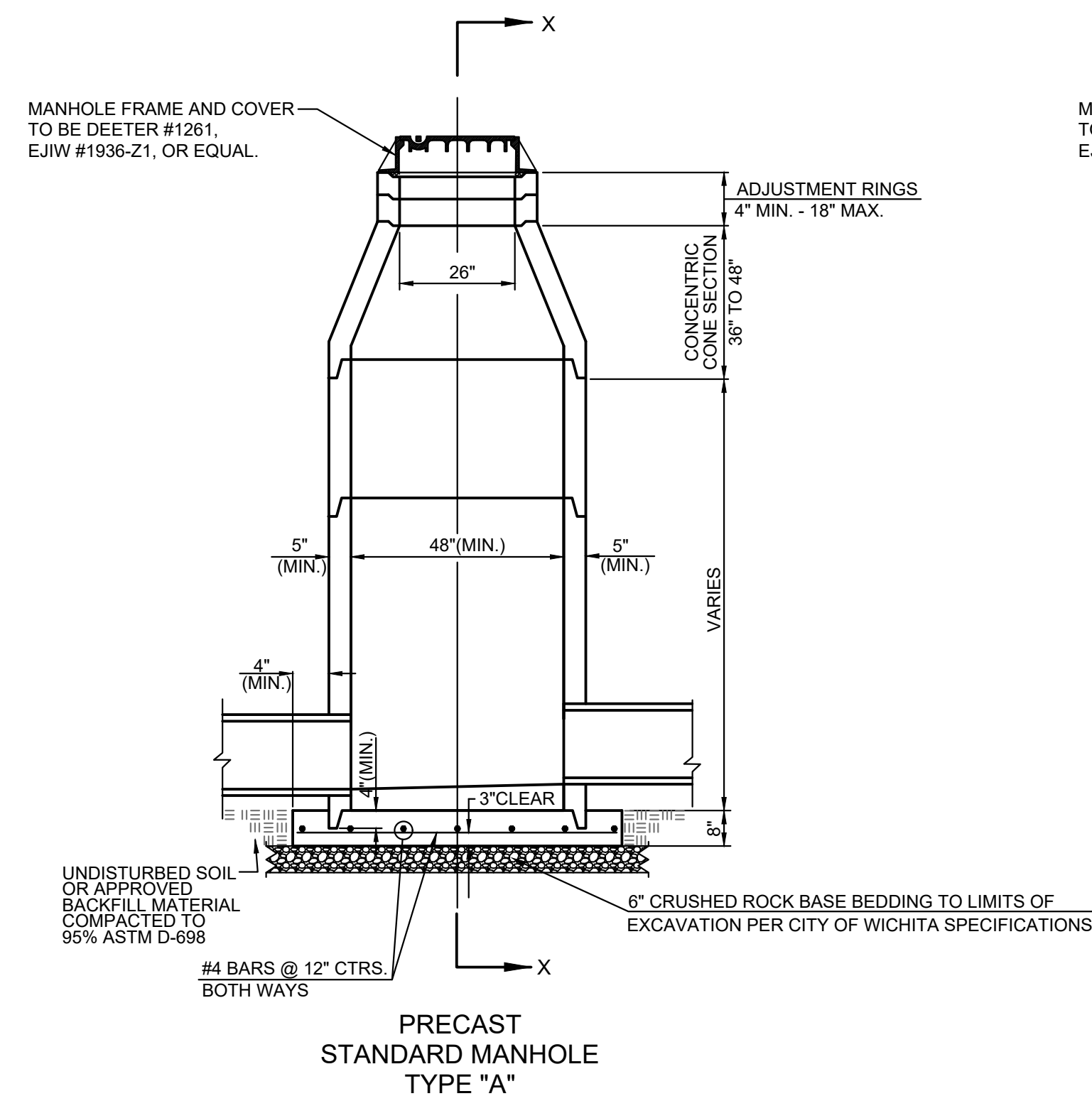
SW-103

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS
STD. TYPE 1A CURB INLET DETAILS	

CP501
34 OF 71

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 MANHOLE DETAILS.DWG

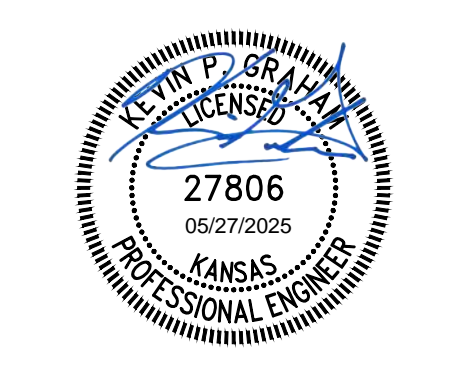
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 U:\Wichita-Civil\2020\200605\04\2PD3_PLANS\0301_PAVING PHASE 1\35-200605-004-CP502 PRECAST CONCRETE MANHOLE DETAILS



- GENERAL NOTES**
- IF, IN THE OPINION OF THE ENGINEER, THE MANHOLE SUBGRADE APPEARS UNSTABLE, THE CONTRACTOR WILL HAVE THE OPTION TO COMPACT SUBGRADE AS SHOWN OR INCREASE THE THICKNESS OF THE MANHOLE BASE AS DIRECTED BY THE ENGINEER.
 - STEEL REINFORCING WILL BE REQUIRED IN ALL MANHOLE BASES.
 - ALL MANHOLE CONSTRUCTION SHALL BE WATER TIGHT.
 - TOP OF MANHOLE FLOOR SLAB SHALL BE AT LEAST 3 INCHES BELOW THE FLOW LINE OF THE OUTLET PIPE TO INSURE SUFFICIENT MINIMUM THICKNESS OF SHAPED INVERT.
 - ALL PRECAST CONCRETE MANHOLE SECTIONS SHALL CONFORM TO THE LATEST REVISION OF ASTM C-478 AS MODIFIED BY THE SPECIFICATIONS.
 - CONCRETE USED FOR MANHOLE CONSTRUCTION SHALL CONFORM TO CITY OF WICHITA SPECIFICATIONS FOR CONCRETE PAVEMENT MIX.
 - PRECAST MANHOLES SHALL BE SET AT LEAST 4 INCHES INTO MANHOLE BASE.
 - MANHOLES WITH PIPE SIZES 24" AND LARGER SHALL HAVE 5 FOOT INSIDE DIAMETER (MIN.)
 - MANHOLES WITH PRECAST BASES MAY BE USED AT THE CONTRACTORS OPTION. THESE MANHOLES SHALL HAVE AN 8" MINIMUM BASE THICKNESS AND SHALL BE PLACED ON AN 8" MIN. CRUSHED ROCK BASE. PIPES SHALL BE ENCASED WITH CRUSHED ROCK TO AT LEAST 3 FEET FROM THE MANHOLE WALL.
 - CONTRACTOR SHALL REMOVE LIFTING HOOKS AFTER INSTALLATION. RECESSES IN MANHOLE WALL SHALL BE GROUTED FLUSH TO THE MANHOLE WALL WITH HYDRAULIC CEMENT AFTER THE MANHOLE IS IN PLACE. LIFTING HOLES THRU THE MANHOLE WALL WILL NOT BE ACCEPTED.
 - THE ENDS OF ALL PIPES IN MANHOLES SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE MANHOLE WALL.
 - MANHOLE INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE MANHOLE WILL BE SELF CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.
 - MANHOLE FRAME AND COVER TO BE DEETER #1261, EJIW #1936-Z1, OR APPROVED EQUAL, SEE SW-303.
 - FOR FLAT GRATED INLET APPLICATION, GRATE TO BE DEETER #1933, EJIW #1205 MDI, OR APPROVED EQUAL.
 - FOR BEEHIVE GRATE APPLICATION, GRATE TO BE DEETER #4495, EJIW #120545, OR APPROVED EQUAL.



REVISED: MARCH 2015		
PRECAST CONCRETE MANHOLE (STORM SEWER)		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
-	-	-
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET _ of _



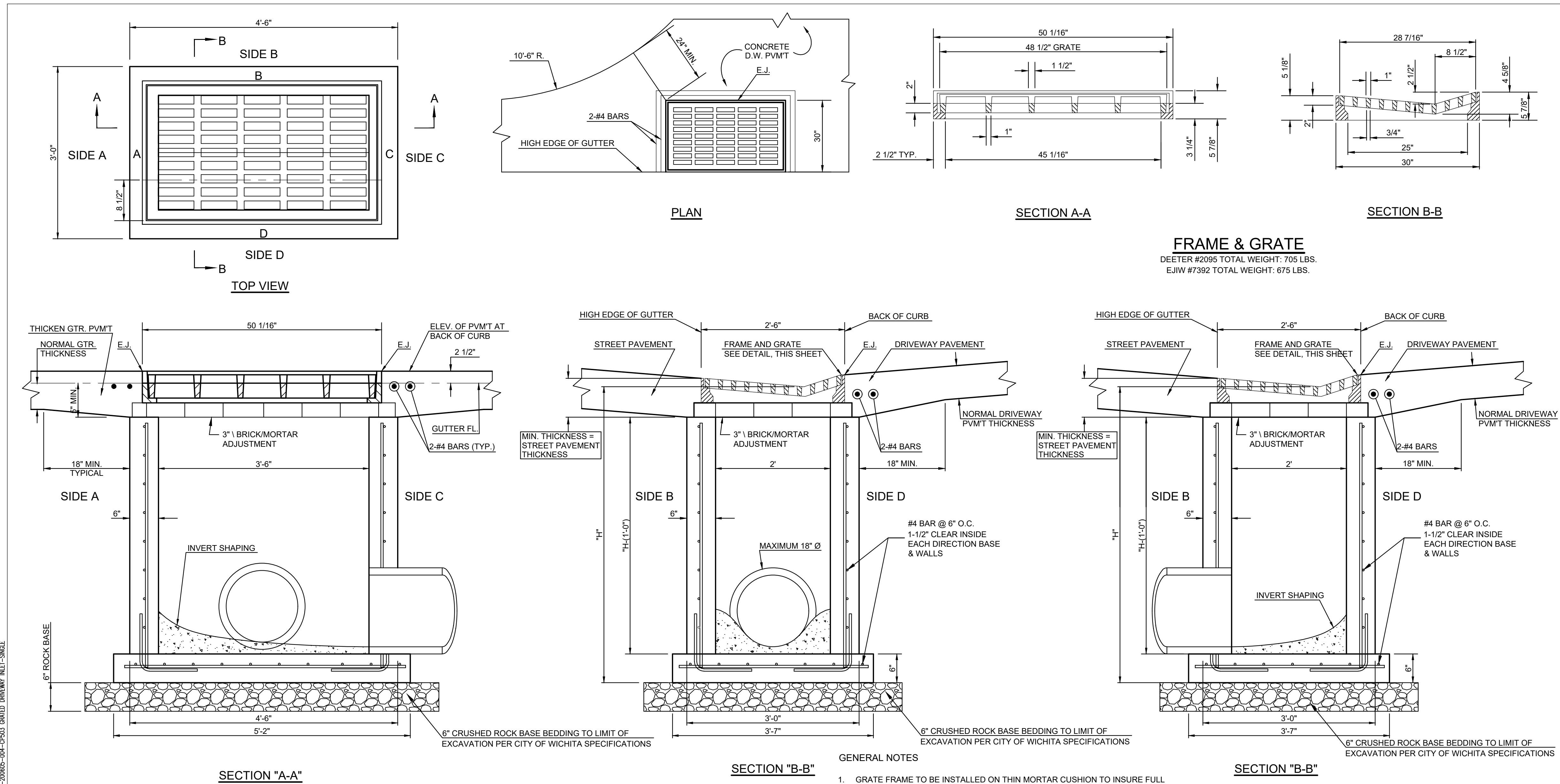
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		
JOB NO.	200605-004	
DATE	MAY 2025	
PM	KPG	
DESIGNED BY	KPG	
DRAWN BY	BJS	
CHECKED BY	KMS	

PRECAST CONCRETE MANHOLE DETAILS
CP502
 35 OF 71

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 INLET-SINGLE.DWG

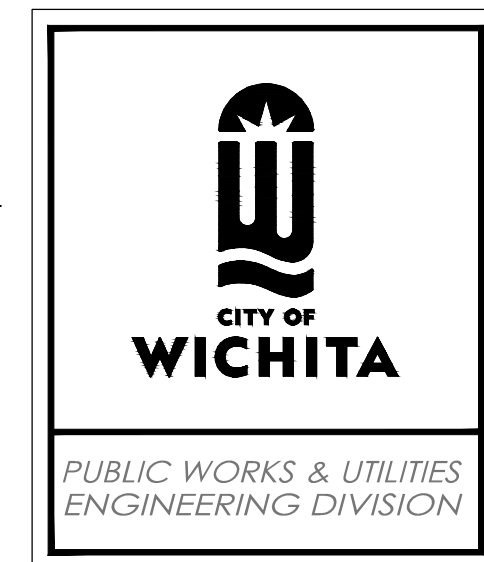
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FRAME & GRATE
 DEETER #2095 TOTAL WEIGHT: 705 LBS.
 EJIW #7392 TOTAL WEIGHT: 675 LBS.

GENERAL NOTES

1. GRATE FRAME TO BE INSTALLED ON THIN MORTAR CUSHION TO INSURE FULL SUPPORT ALONG BRICK. CONCRETE USED FOR INLET CONSTRUCTION SHALL CONFORM TO CITY OF WICHITA SPECIFICATIONS FOR CONCRETE PAVEMENT MIX.
2. CONTRACTOR SHALL HAVE THE OPTION OF CONSTRUCTING 8" BRICK MASONRY WALL BETWEEN THE CONCRETE INLET BASE AND TOP OF THIS INLET WHEN H=7'-0" OR LESS.
3. INLET INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE INLET WILL BE SELF CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.
4. THE ENDS OF ALL PIPES INSTALLED IN INLETS SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE INLET WALL.
5. INLET FRAME AND GRATE TO BE DEETER #2095, EJIW #7392, OR APPROVED EQUAL.
6. CONTRACTOR SHALL REMOVE LIFTING HOOKS AFTER INSTALLATION. RECESSES IN INLET WALL SHALL BE GROUTED FLUSH TO THE INLET WALL WITH HYDRAULIC CEMENT AFTER THE INLET IS IN PLACE. LIFTING HOLES THRU THE INLET WALL WILL NOT BE ACCEPTED.



REVISED: MARCH 2015

GRATED DRIVEWAY INLET (SINGLE)

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	OCA NUMBER	DATE

CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 455 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

SHEET
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SW-108

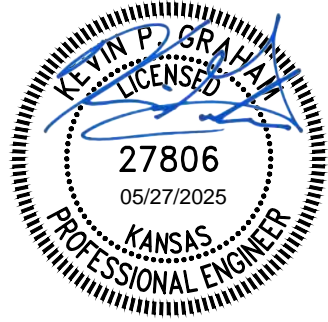
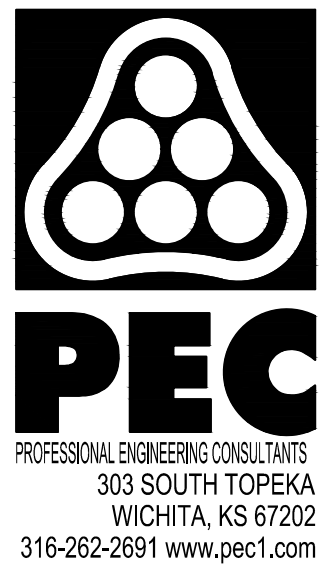
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

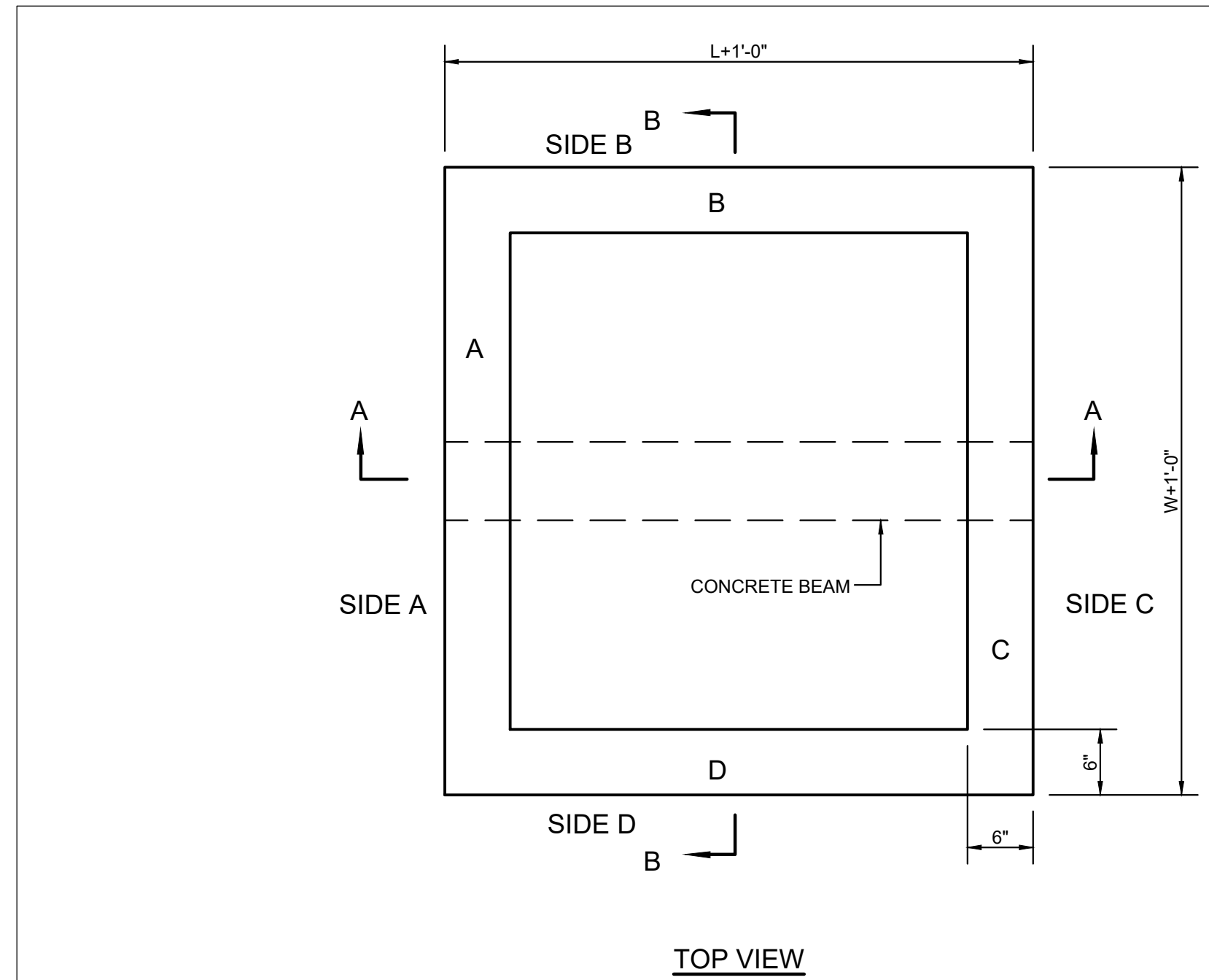
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JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BSJ
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GRATED DRIVEWAY INLET-SINGLE
CP503
 36 OF 71

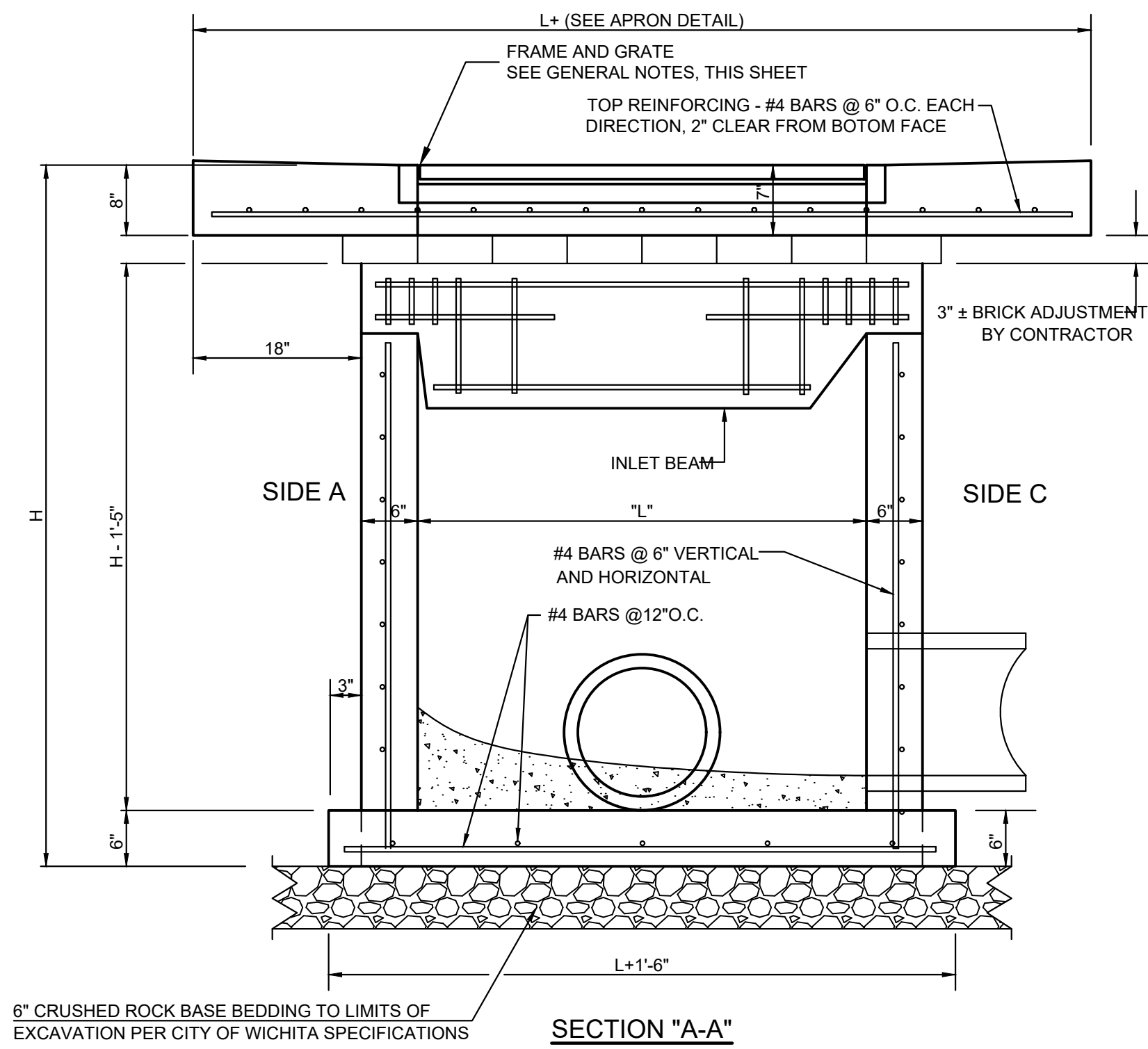


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 INLET.DWG

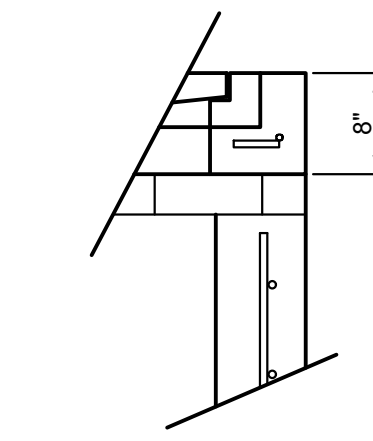
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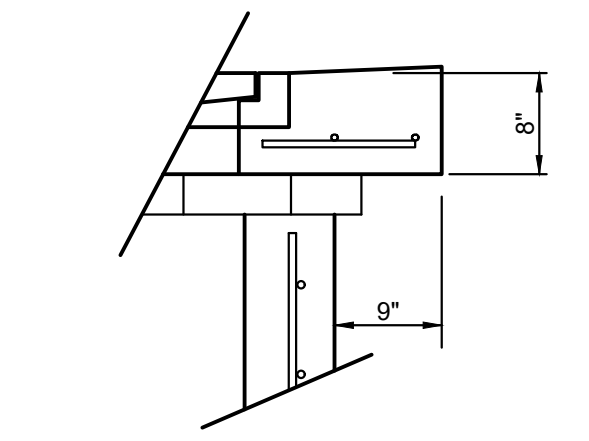
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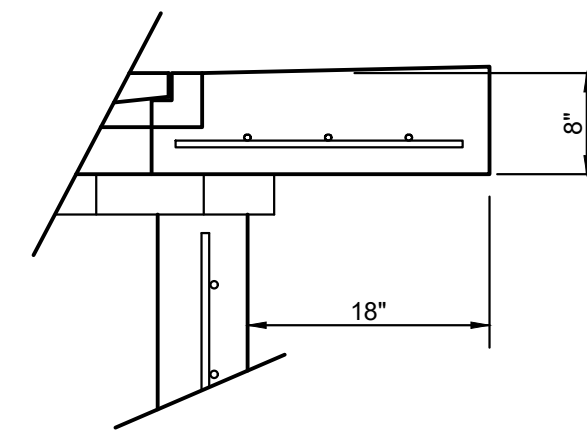
SECTION "A-A"



FLUSH STYLE TOP
NO APRON



9" APRON
* APRON TO EXTEND ON ALL 4 SIDES OF INLET.
DESIGNER TO DESIGNATE APRON SIZE.

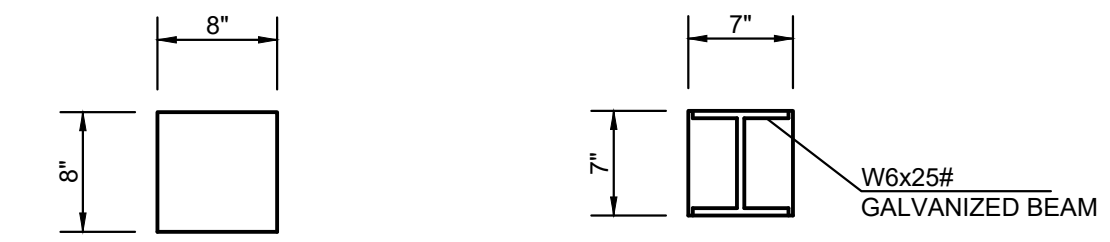


18" APRON

W=4'-4" and L=4' for DOUBLE DROP INLET

The structure(s) on this detail sheet are designed for HS-20 loading at these specific dimensions only. If larger dimensions are required, the ENGINEER shall provide a project specific structure design for approval by the City Engineer's office.

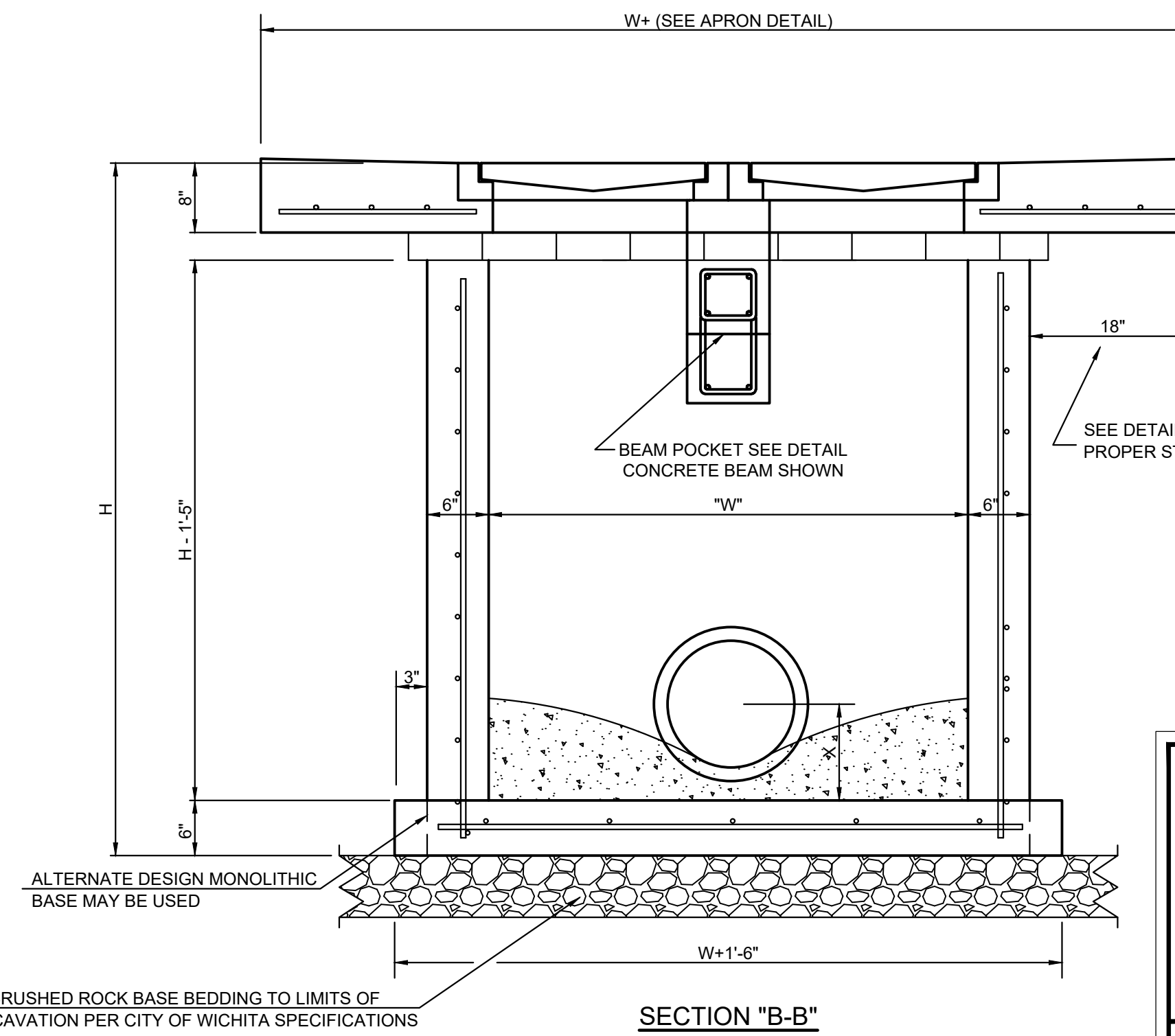
BEAM REQUIRED FOR THIS INLET



CONCRETE BEAM POCKET GALV. STEEL BEAM POCKET

GENERAL NOTES

- GRATE FRAME TO BE INSTALLED ON THIN MORTAR CUSHION TO INSURE FULL SUPPORT ALONG BRICK. CONCRETE USED FOR INLET CONSTRUCTION SHALL CONFORM TO CITY OF WICHITA SPECIFICATIONS FOR CONCRETE PAVEMENT MIX.
- INLET INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE INLET WILL BE SELF CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.
- THE ENDS OF ALL PIPES INSTALLED IN INLETS SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE INLET WALL.
- INLET FRAME AND GRATE TO BE DEETER #2433, EJIW #5391-Z1 OR APPROVED EQUAL FOR 2'x2' SINGLE DROP INLET AND DEETER #2434, EJIW #5391 Z3 OR APPROVED EQUAL FOR 2'x4' DOUBLE DROP INLET.
- CONTRACTOR SHALL REMOVE LIFTING HOOKS AFTER INSTALLATION. RECESSES IN INLET WALL SHALL BE GROUTED FLUSH TO THE INLET WALL WITH HYDRAULIC CEMENT AFTER THE INLET IS IN PLACE. LIFTING HOLES THRU THE INLET WALL WILL NOT BE ACCEPTED.

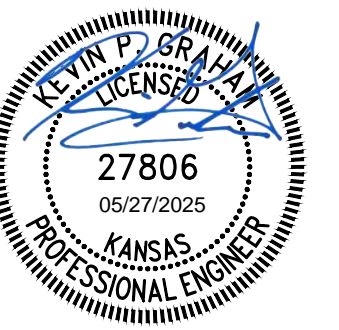


SECTION "B-B"



REVISED: MARCH 2015		
DOUBLE DOUBLE DROP INLET WITH BEAM		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE		SHEET
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		_ of _

SW-202



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

ISSUE:			

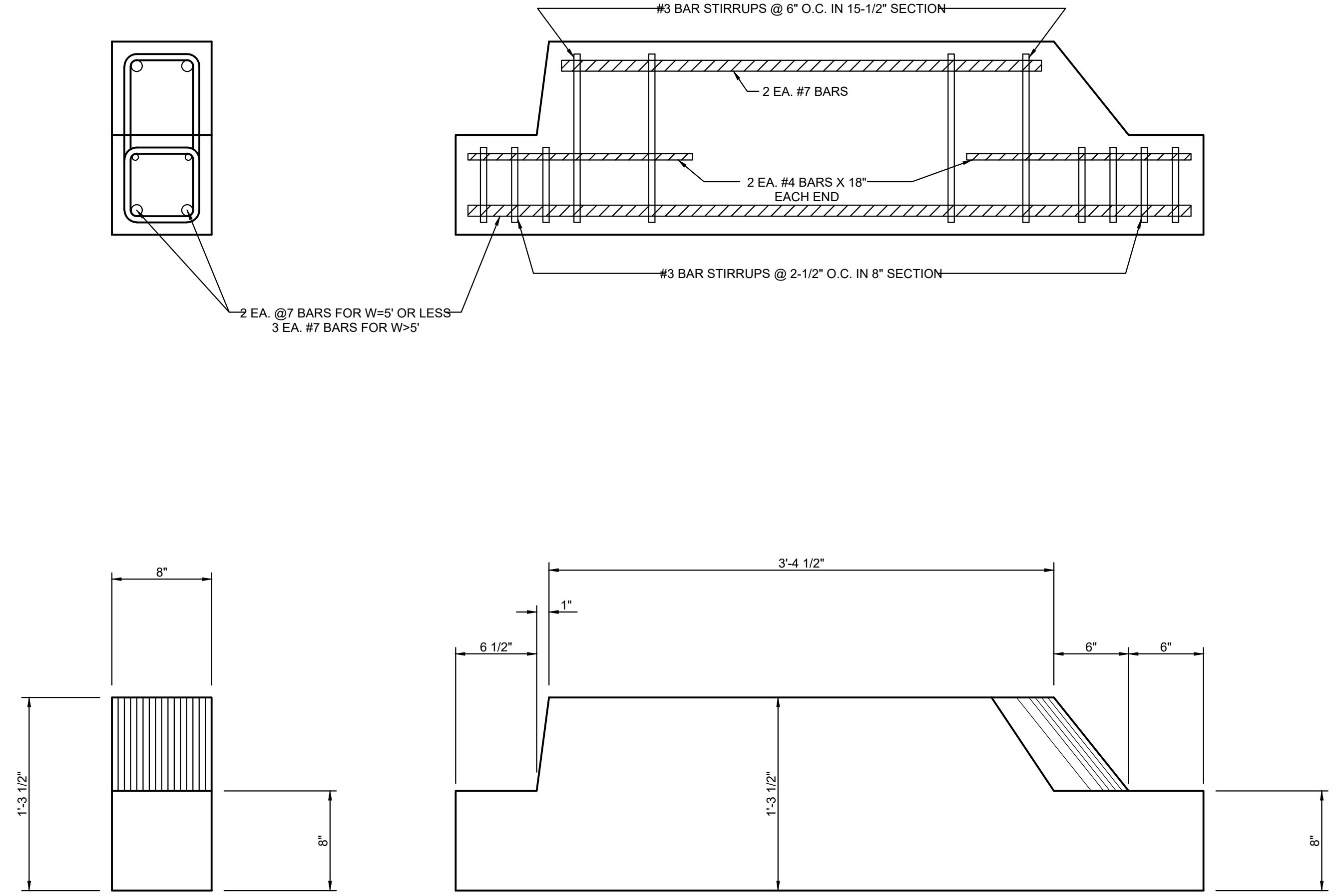
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DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
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CHECKED BY	KMS

DOUBLE DOUBLE DROP INLET

CP504
37 OF 71

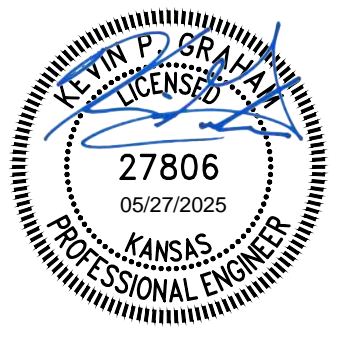
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 DETAILS.DWG

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INLET BEAM		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
---	---	11/2010
CITY ENGINEER'S OFFICE		SHEET
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		- of -

SW-110



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

**SWANEY FARM ADDITION
 PHASE 1**

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

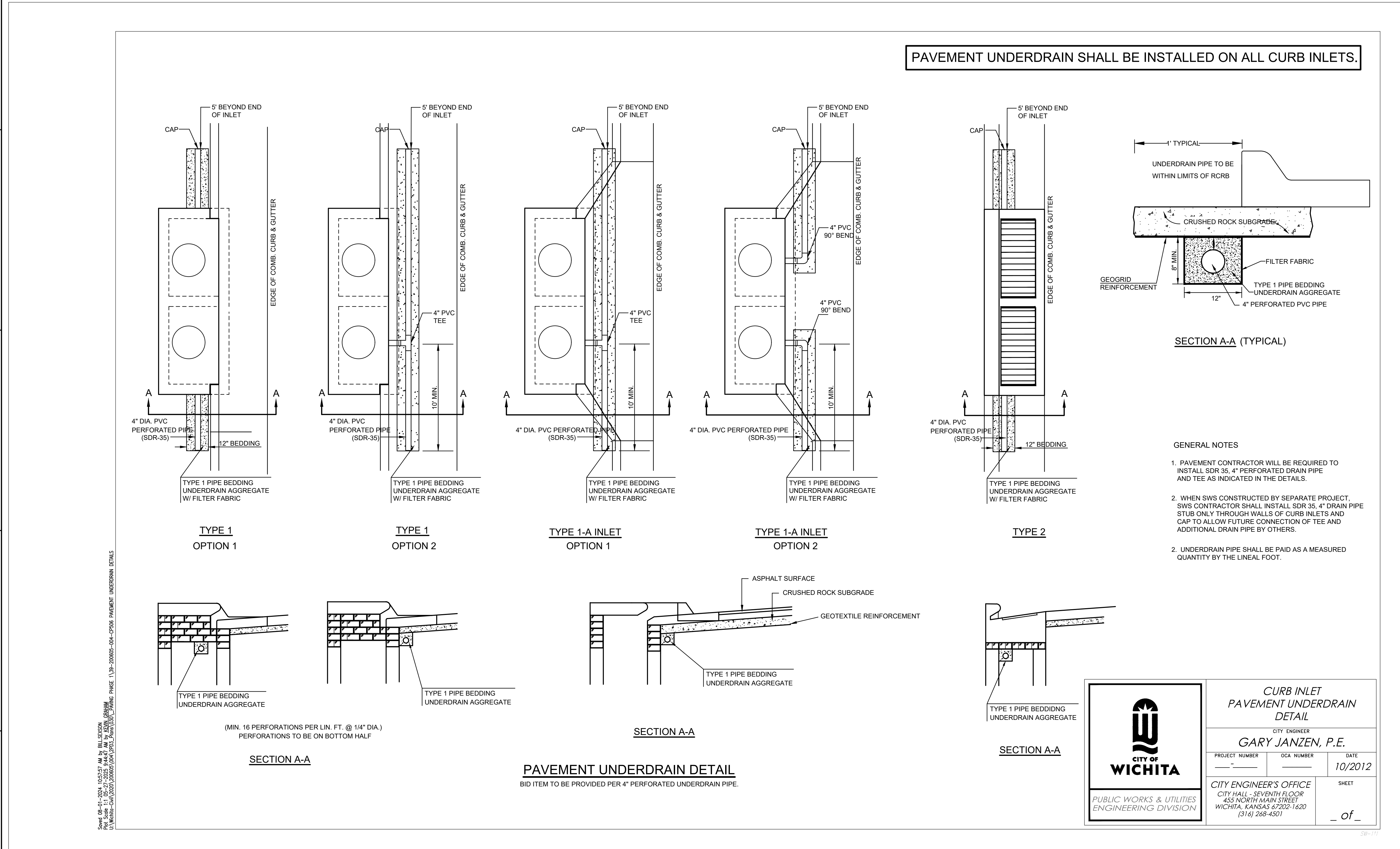
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INLET BEAM DETAILS

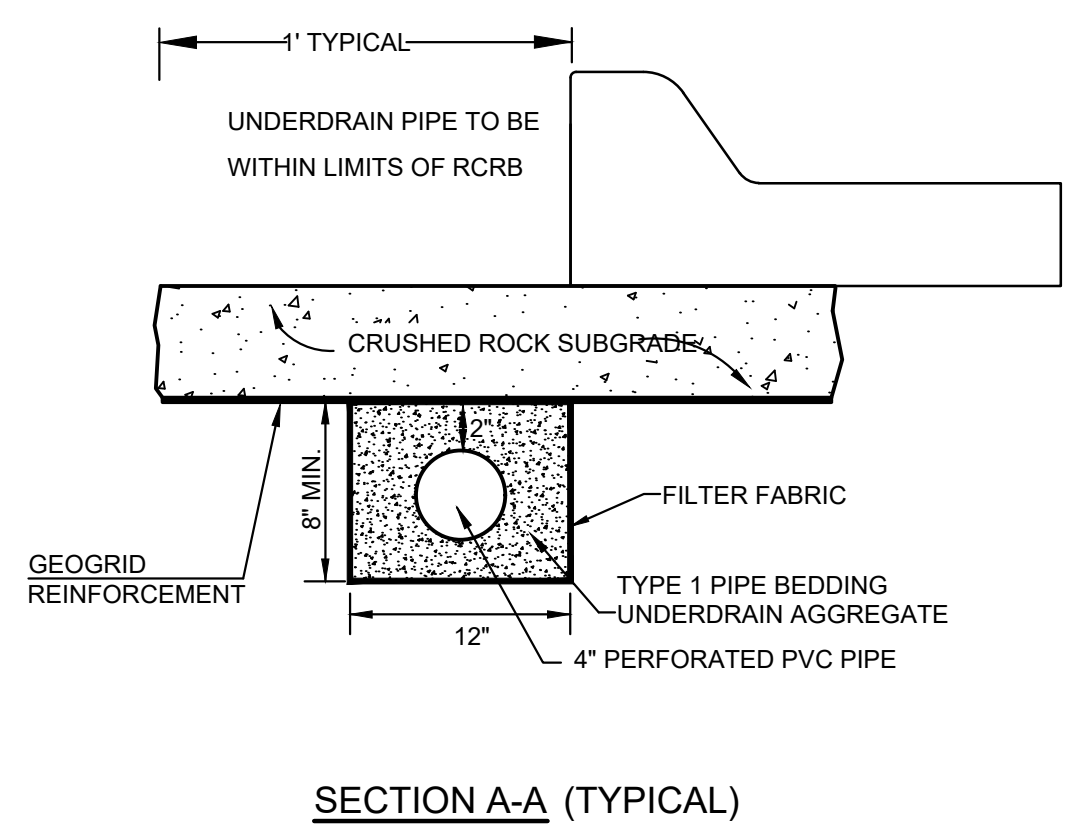
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 DETAILS.DWG

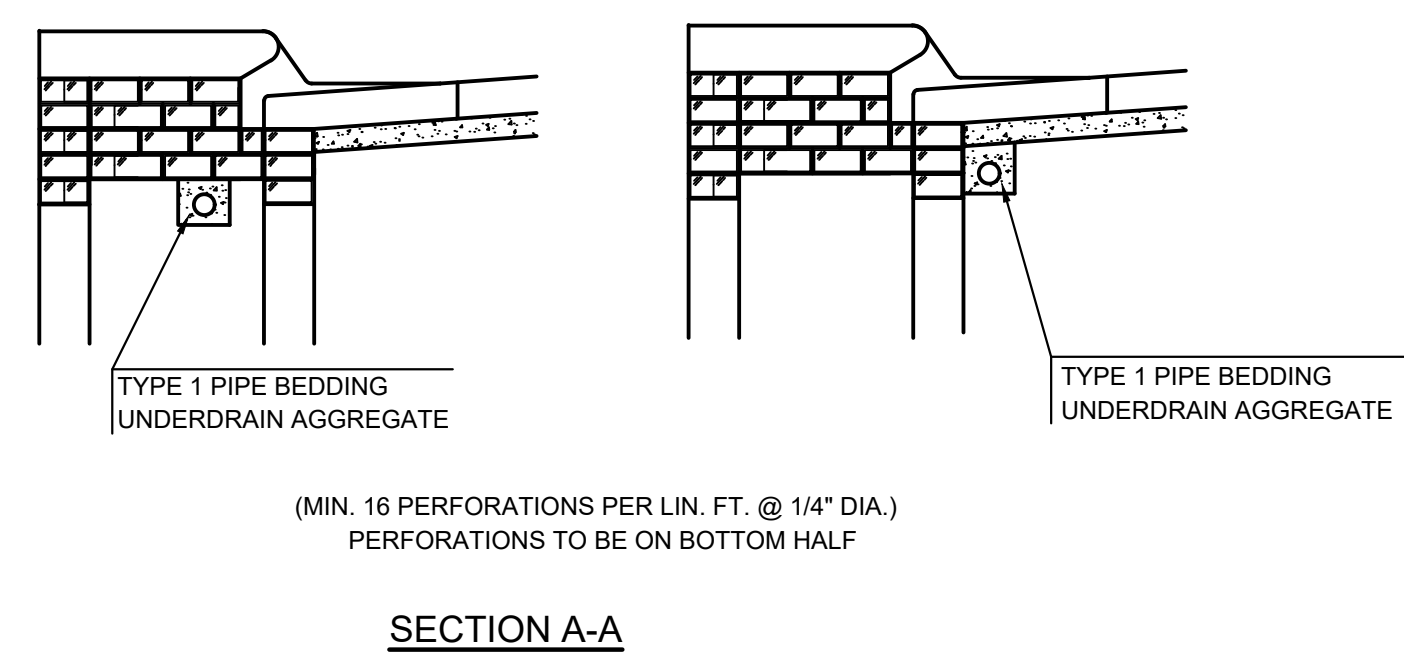
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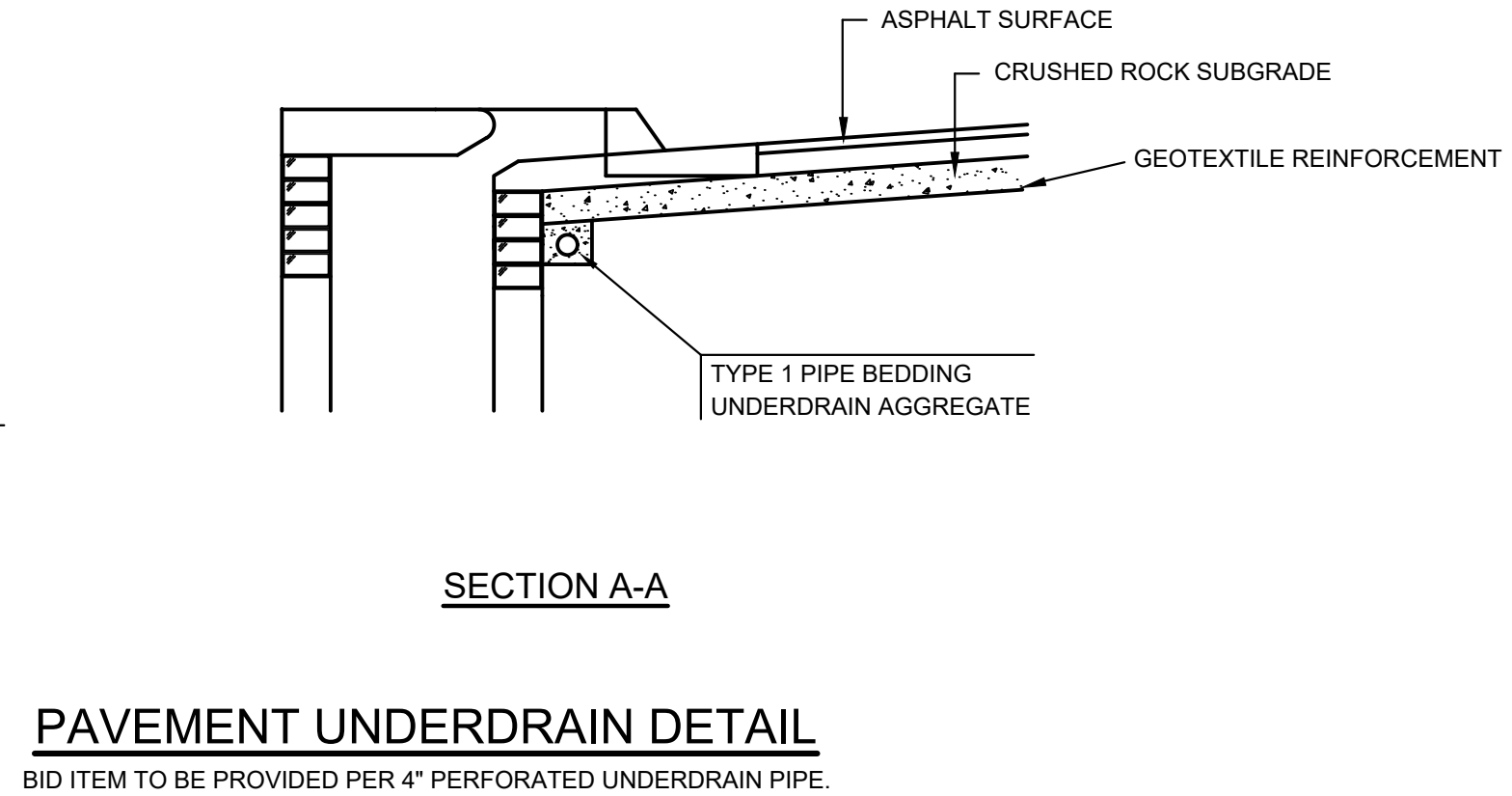
PAVEMENT UNDERDRAIN SHALL BE INSTALLED ON ALL CURB INLETS.



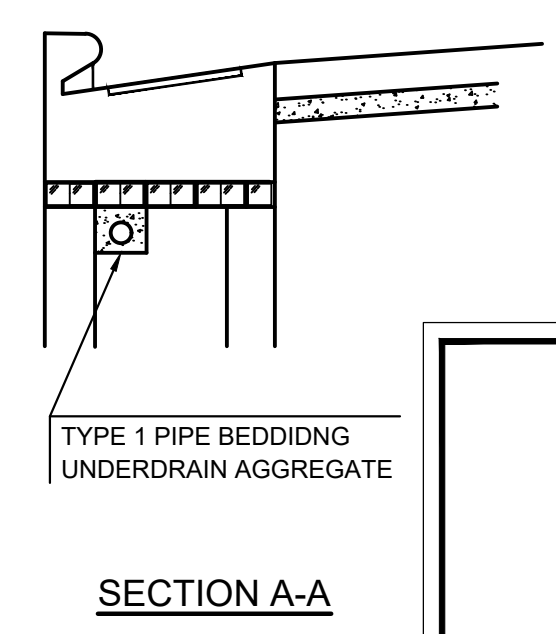
- GENERAL NOTES**
- PAVEMENT CONTRACTOR WILL BE REQUIRED TO INSTALL SDR 35, 4\" PERFORATED DRAIN PIPE AND TEE AS INDICATED IN THE DETAILS.
 - WHEN SWS CONSTRUCTED BY SEPARATE PROJECT, SWS CONTRACTOR SHALL INSTALL SDR 35, 4\" DRAIN PIPE STUB ONLY THROUGH WALLS OF CURB INLETS AND CAP TO ALLOW FUTURE CONNECTION OF TEE AND ADDITIONAL DRAIN PIPE BY OTHERS.
 - UNDERDRAIN PIPE SHALL BE PAID AS A MEASURED QUANTITY BY THE LINEAL FOOT.



(MIN. 16 PERFORATIONS PER LIN. FT. @ 1/4\" DIA.)
 PERFORATIONS TO BE ON BOTTOM HALF

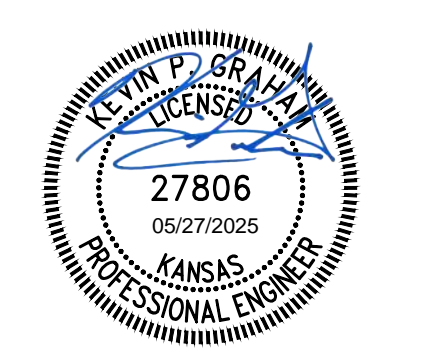


SECTION A-A
PAVEMENT UNDERDRAIN DETAIL
 BID ITEM TO BE PROVIDED PER 4\" PERFORATED UNDERDRAIN PIPE.



SECTION A-A

 PUBLIC WORKS & UTILITIES ENGINEERING DIVISION	CURB INLET PAVEMENT UNDERDRAIN DETAIL		
	CITY ENGINEER GARY JANZEN, P.E.		
	PROJECT NUMBER	OCA NUMBER	DATE
			10/2012
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4301		SHEET _ of _	



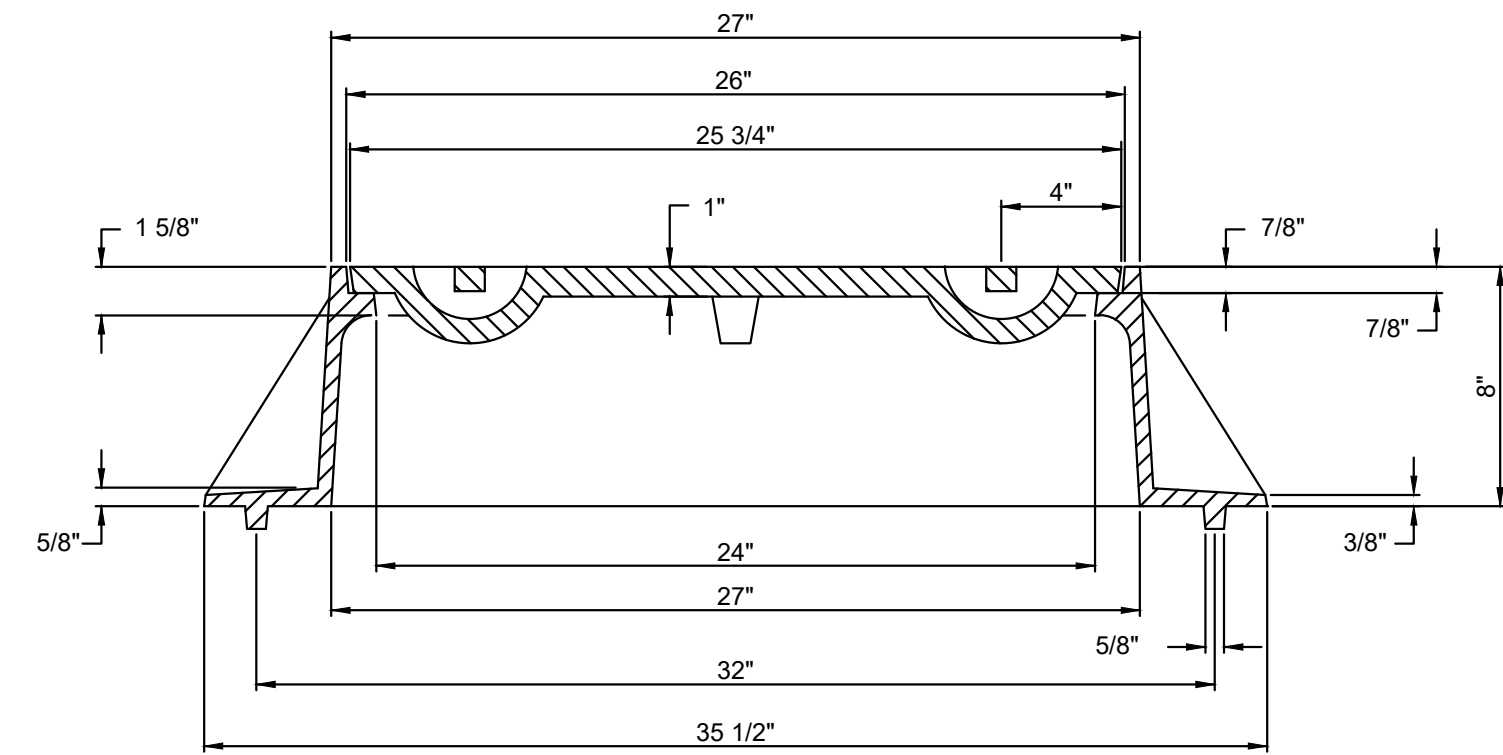
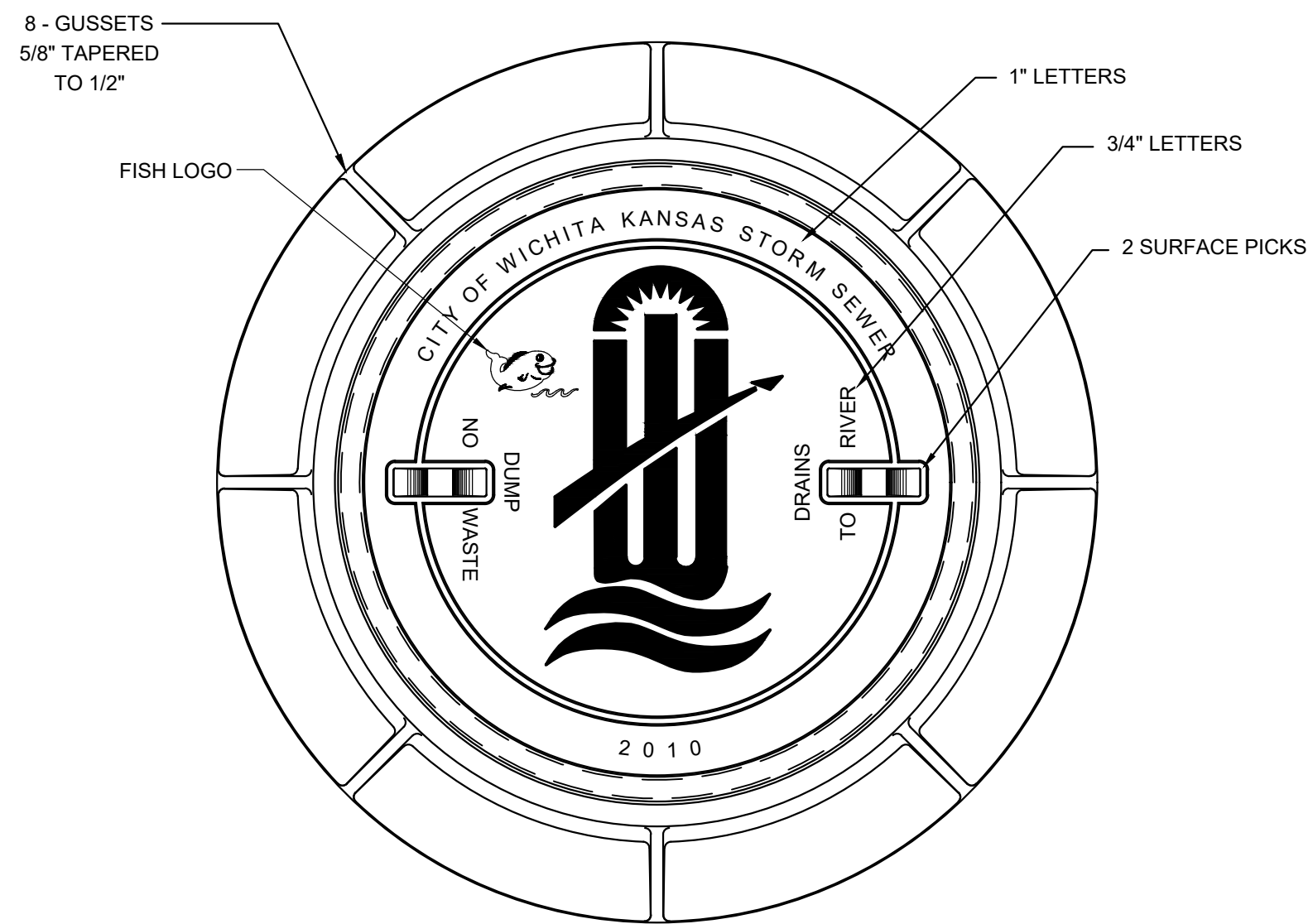
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:			
JOB NO.	200605-004		
DATE	MAY 2025		
PM	KPG		
DESIGNED BY	KPG		
DRAWN BY	BJS		
CHECKED BY	KMS		

**PAVEMENT UNDERDRAIN
 DETAILS**
CP506
 39 OF 71

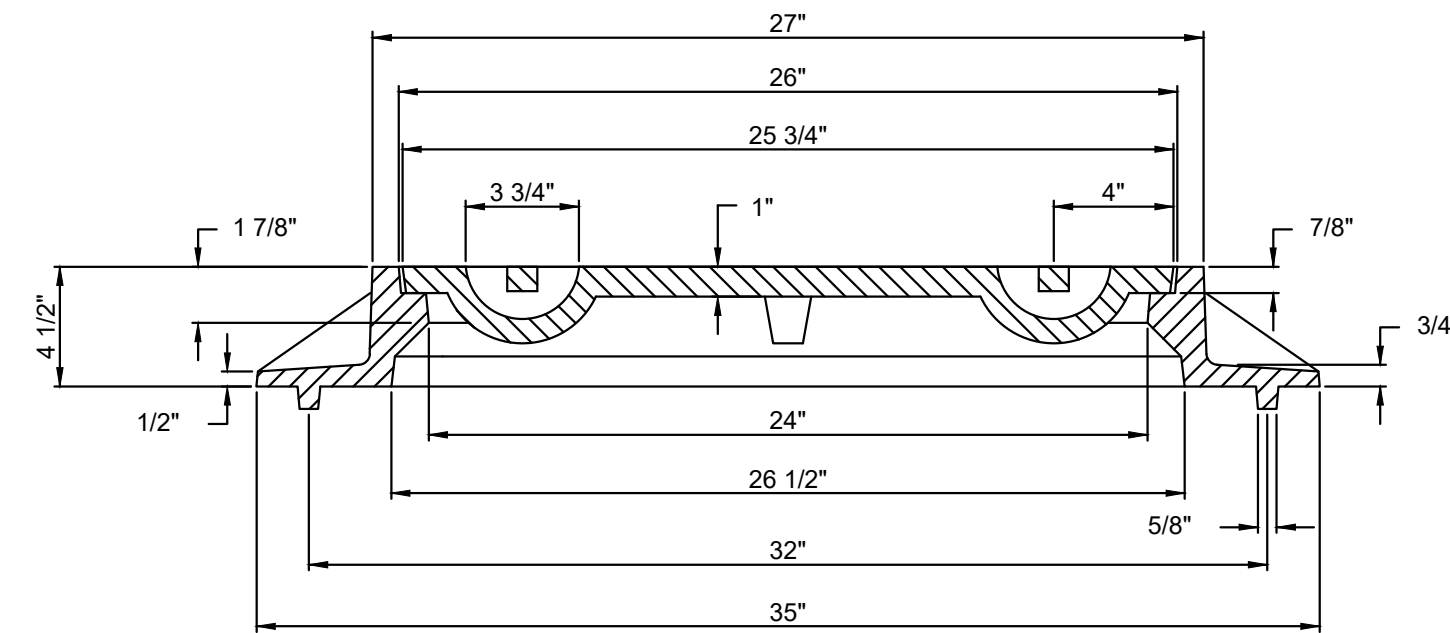
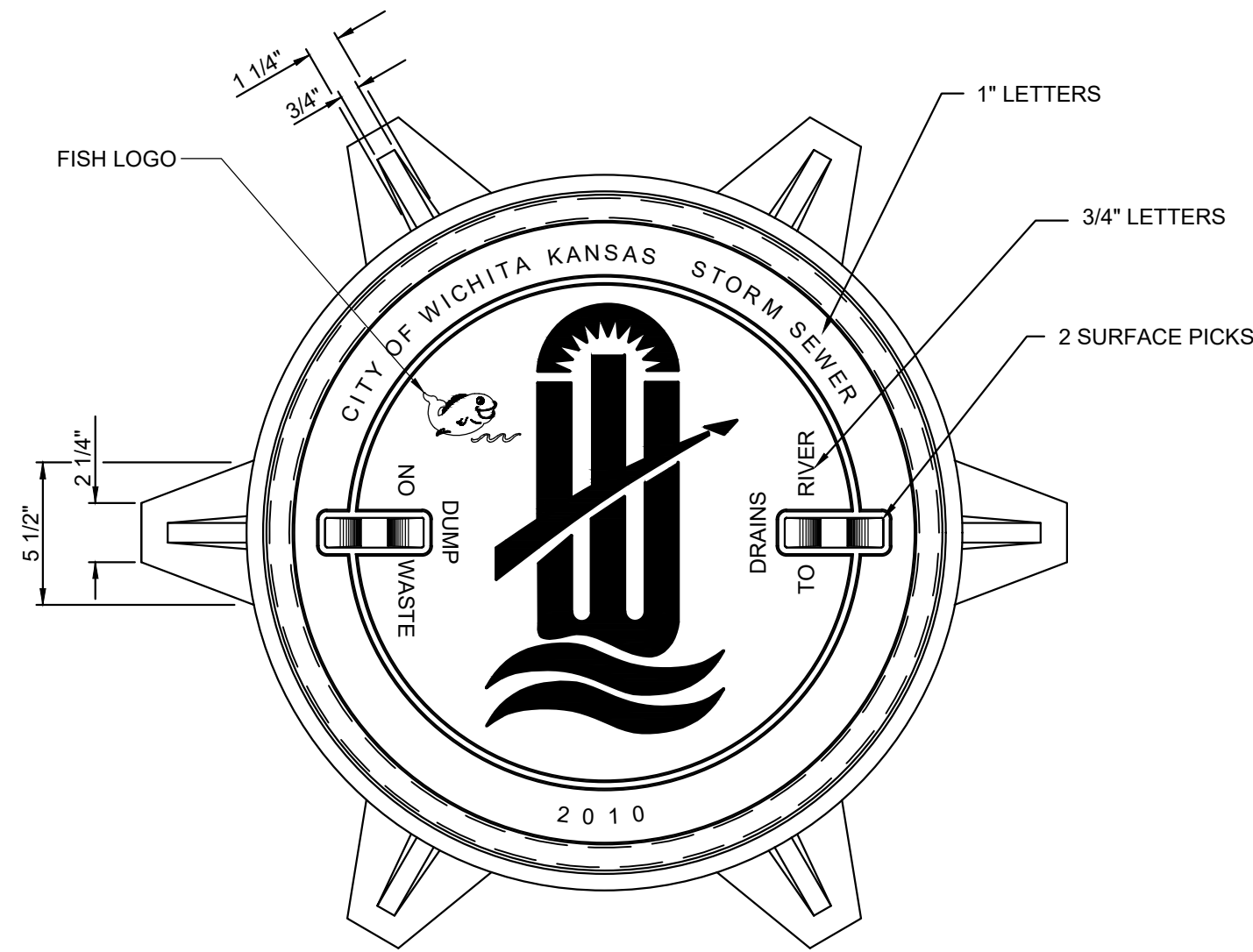
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 AND COVER DETAILS.DWG

Sheet 08-01-2024 10:58:20 AM BY BILL SEXSON
 U:\WICHITA-CIVIL\2020\200605\04\2PD3_PLANS\0301_PAVING PHASE 1\41-200605-004-CP508 MANHOLE-INLET FRAME AND COVER DETAILS



MANHOLE FRAME
 DEETER #1261 OR EJIW #1936-Z1

- NOTE:
 1. FURNISHED WITH MACHINED HORIZONTAL BEARING SURFACE.
 2. COVER TO BE DEETER #1261 OR EJIW #1936A.



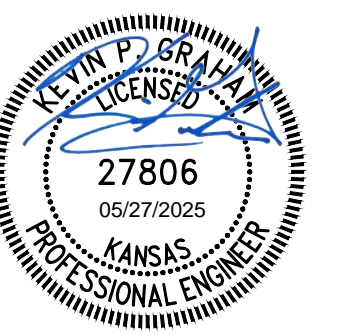
INLET FRAME
 DEETER #2014 OR EJIW #1936-Z4

- NOTE:
 1. FURNISHED WITH MACHINED HORIZONTAL BEARING SURFACES.
 2. NOT TO BE USED UNDER PAVEMENT.
 3. COVER TO BE DEETER #1261 OR EJIW #1936A.



MANHOLE/INLET FRAME AND COVER (STORM SEWER)		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE 11/2010
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET _ of _

SW-303



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

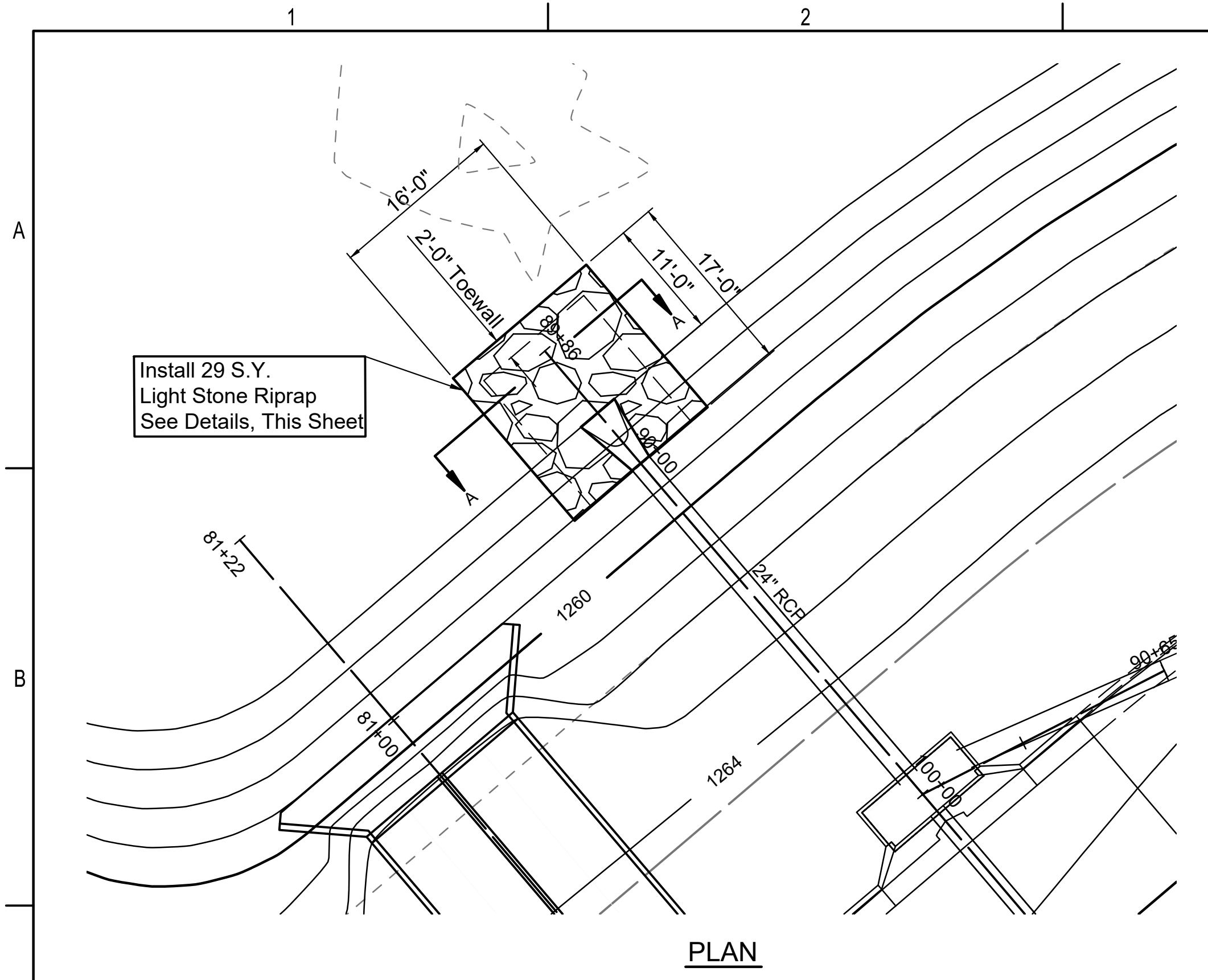
PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:			
JOB NO.	200605-004		
DATE	MAY 2025		
PM	KPG		
DESIGNED BY	KPG		
DRAWN BY	BJS		
CHECKED BY	KMS		

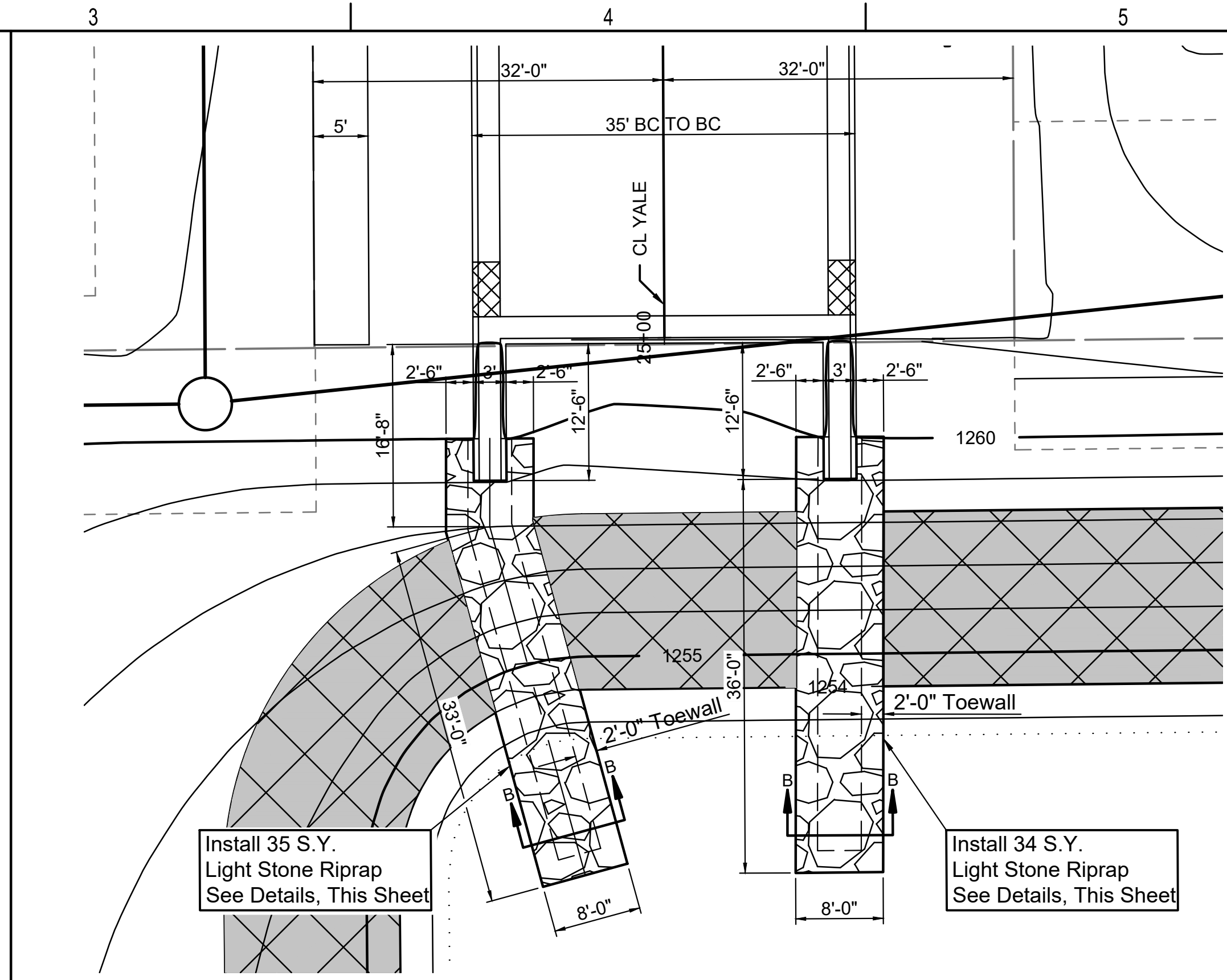
MANHOLE-INLET FRAME AND COVER DETAILS

CP508
 41 OF 71

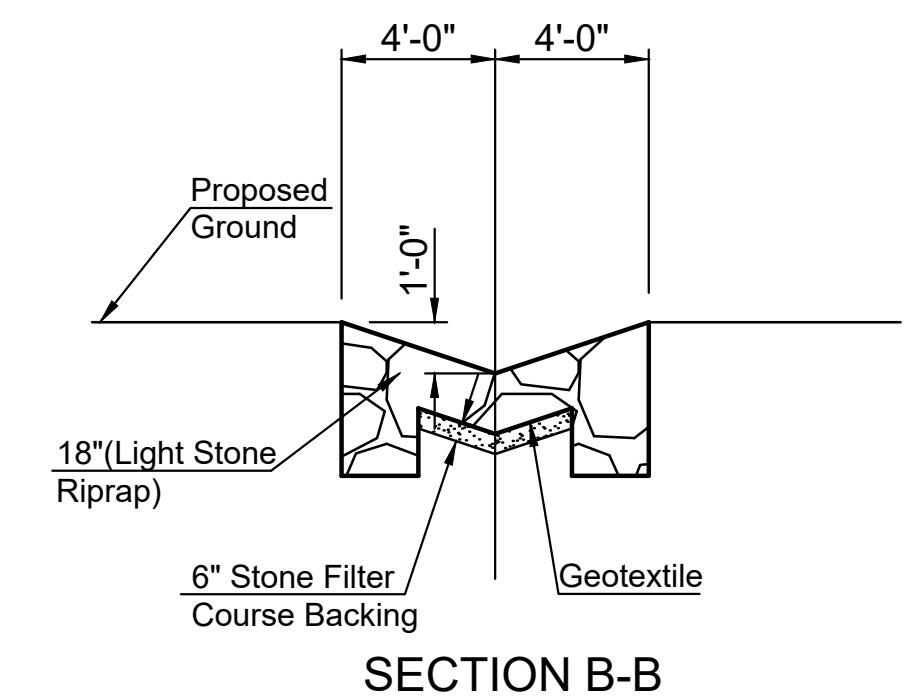
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 DETAILS.DWG



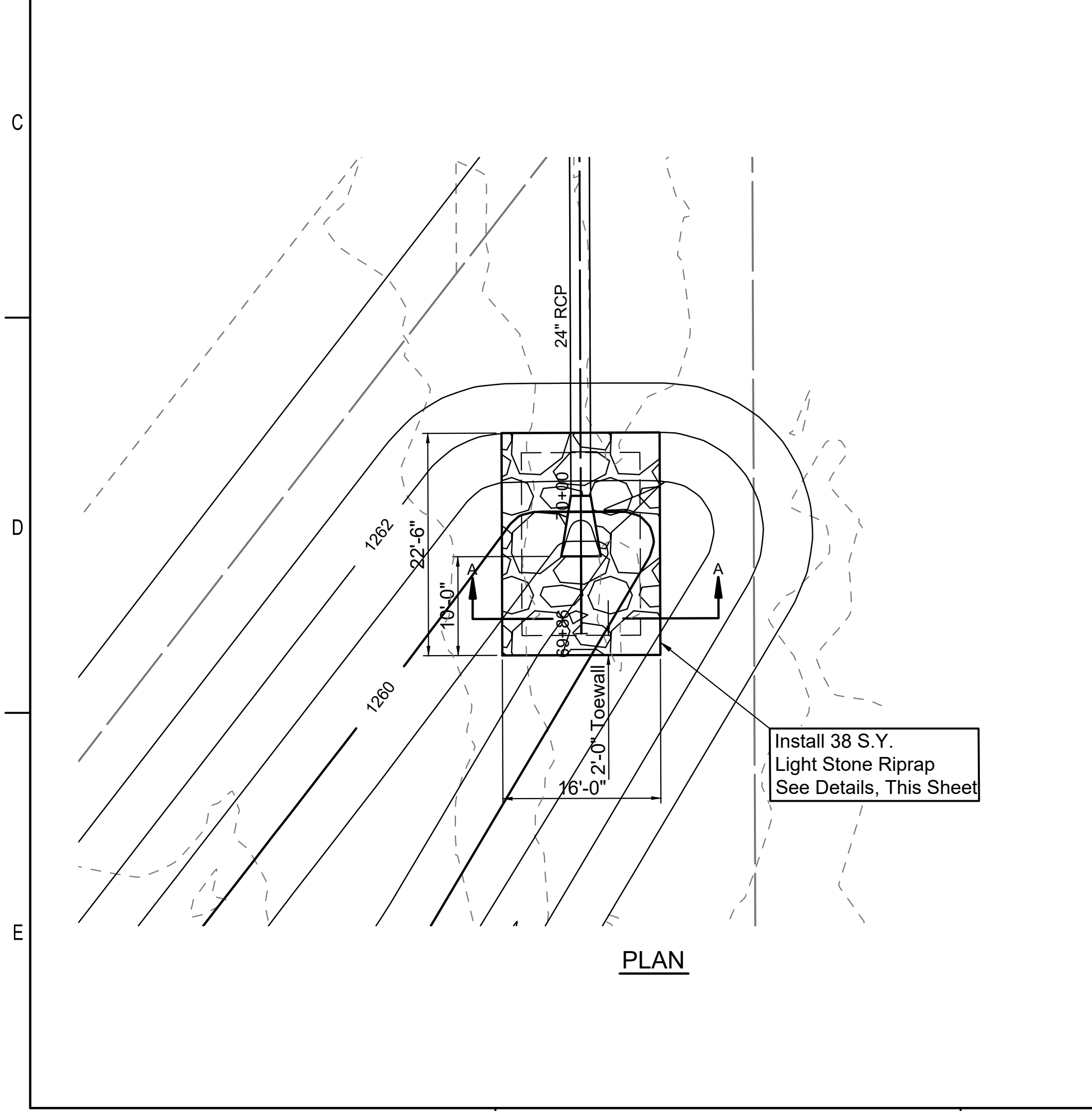
PLAN



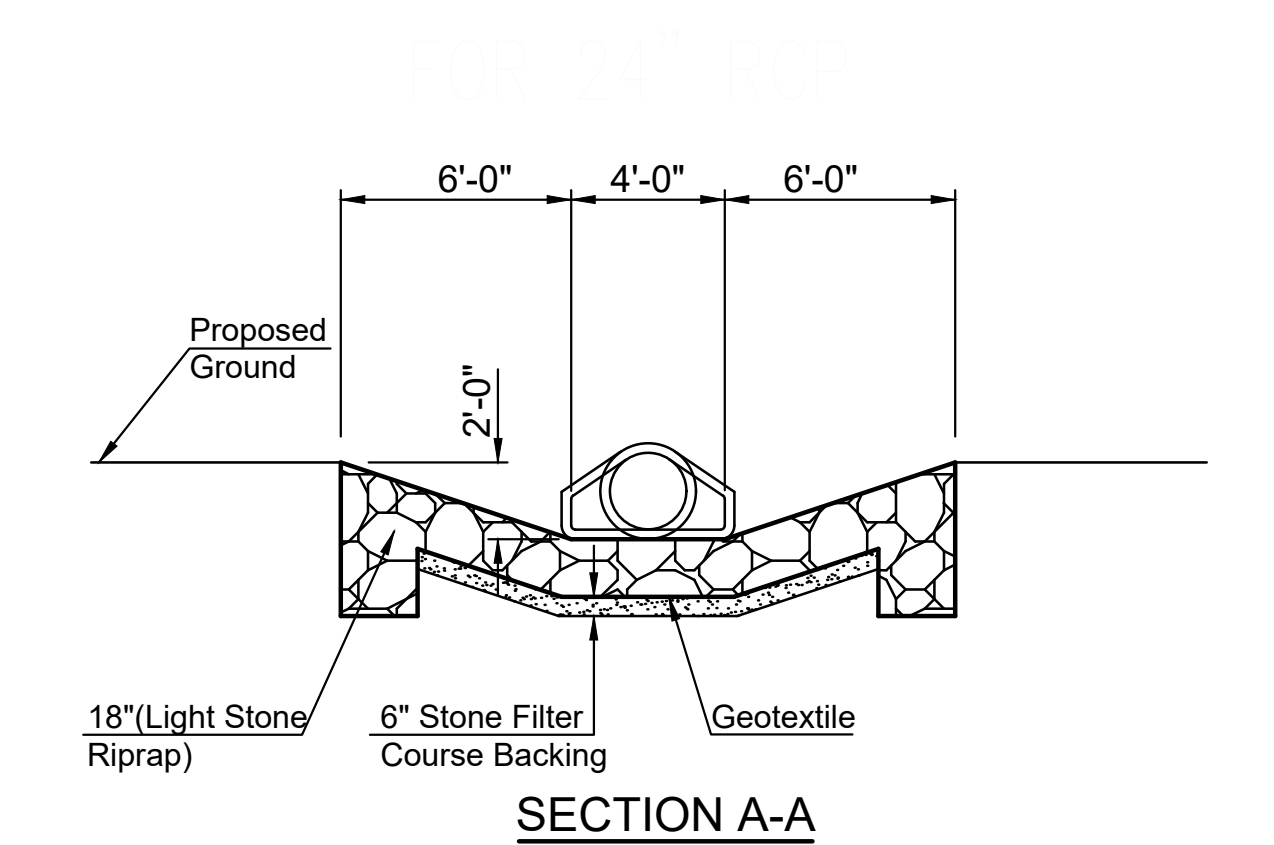
SECTION B-B



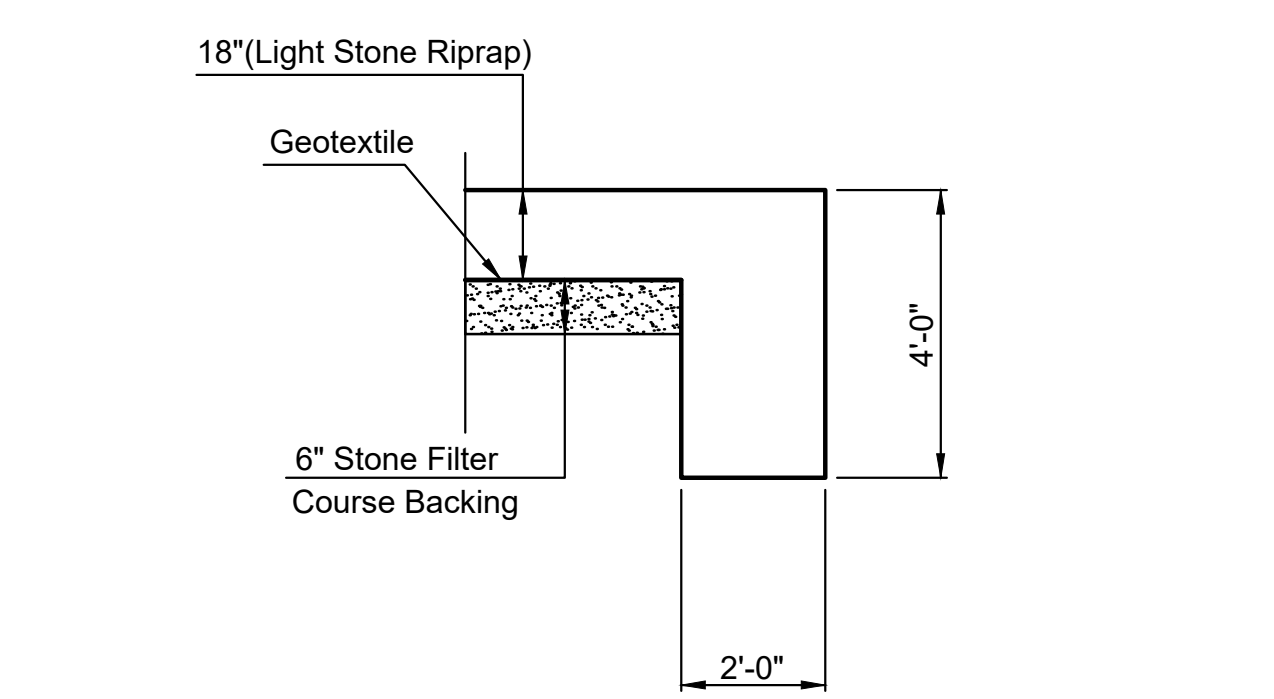
SCALE IN FEET
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PLAN



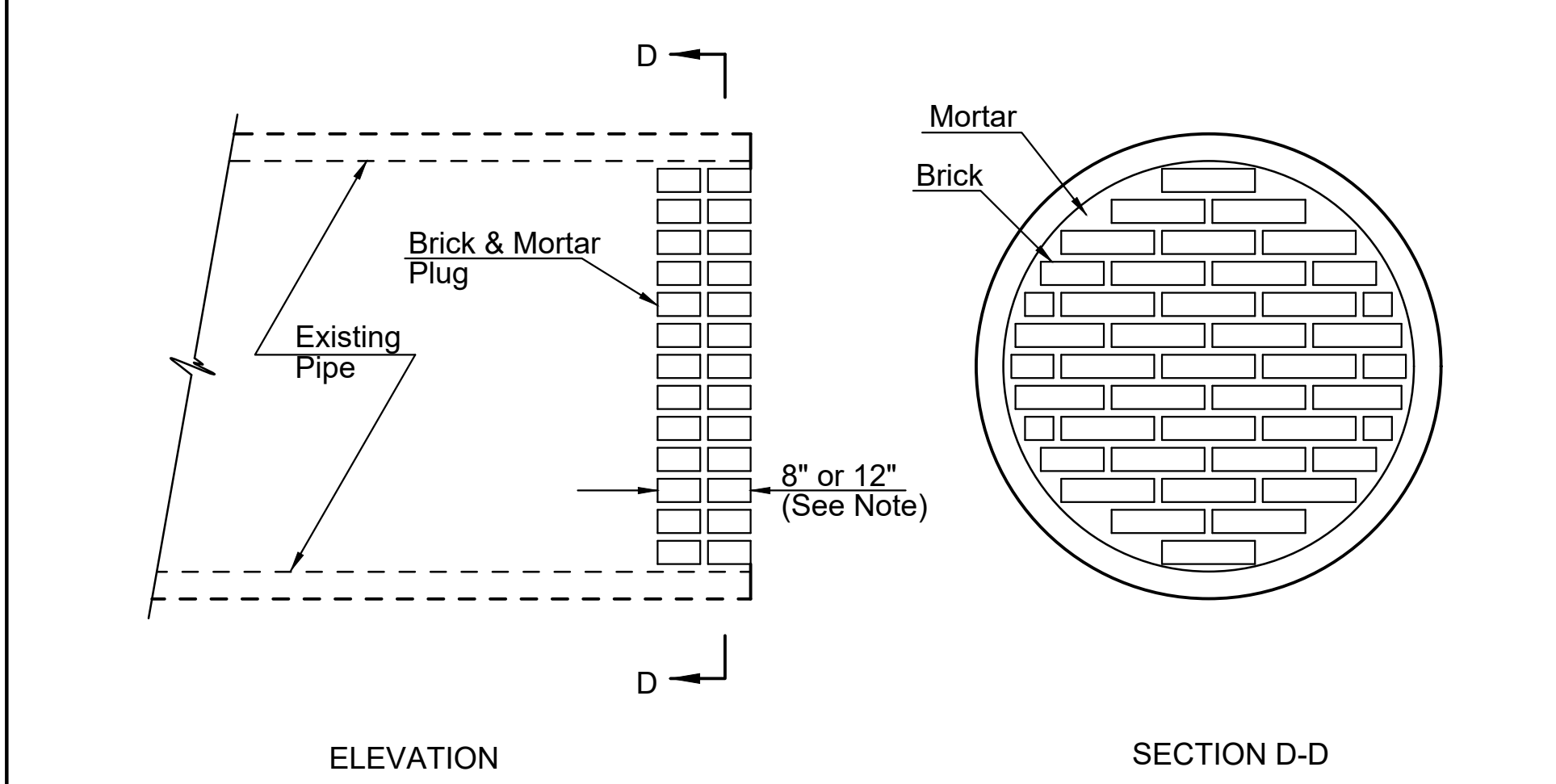
SECTION A-A



TYPICAL SECTION THRU TOEWALL
 NO SCALE

NOTES

1. ALL RIPRAP FOR THIS PROJECT SHALL BE NATURAL STONE. NEITHER BROKEN CONCRETE, FABRIC ENVELOPE, NOR PREMIXED DRY PACKAGED CONCRETE BAG ALTERNATES WILL BE ALLOWED.
2. TOEWALLS SHALL BE INSTALLED ALONG ALL EDGES OF STONE RIPRAP OR AS SHOWN ON PLANS.
3. GROUTING OF THE SURFACE OF THE RIPRAP SHALL NOT BE PERFORMED. GROUTING OF THE TOEWALLS SHALL BE PERFORMED PER SPECIFICATIONS.



PIPE PLUGGING DETAIL

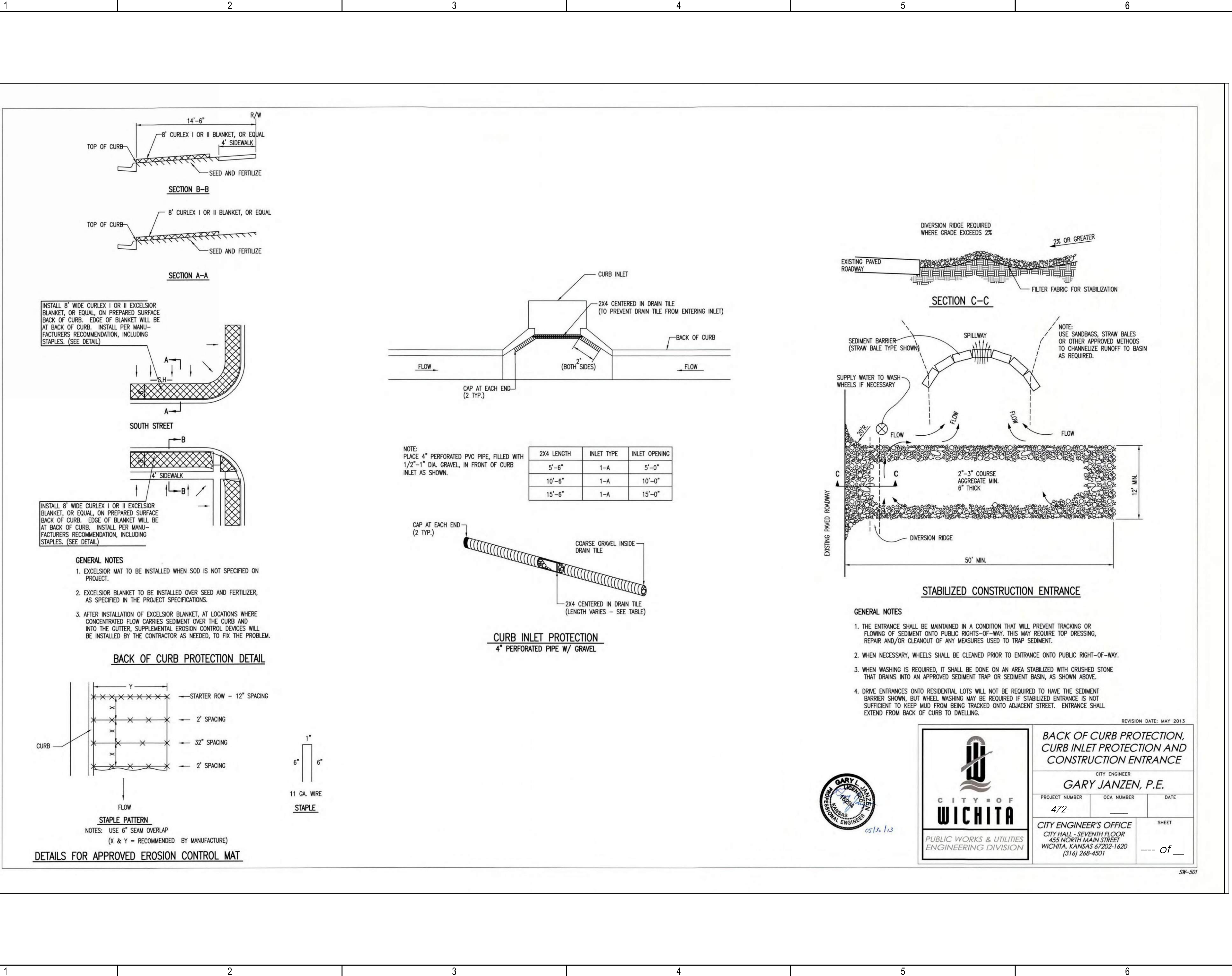
NOTE: PIPE PLUGS SHALL BE CONSTRUCTED OF BRICK MASONRY, WITH CONSTRUCTION REQUIREMENTS AND MATERIALS CONFORMING TO THE SAME REQUIREMENTS AS SPECIFIED FOR MANHOLE CONSTRUCTION ON SH. NO. 13. PIPES HAVING DIAMETERS OF 18" OR SMALLER SHALL BE PLUGGED WITH 8" THICK PLUGS. PIPES HAVING DIAMETERS LARGER THAN 18" SHALL BE PLUGGED WITH 12" THICK PLUGS. THE CONSTRUCTION OF PIPE PLUGS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED SUBSIDIARY.

PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
 SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:			
JOB NO.	200605-004		
DATE	MAY 2025		
PM	KPG		
DESIGNED BY	KPG		
DRAWN BY	BJS		
CHECKED BY	KMS		
RIPRAP AND MAT DETAILS			
CP509			
42 OF 71			

SAVED 8/1/2024 10:59:11 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:45:46 AM BY KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PAVING PHASE 1\44-200605-004-CG501 BACK OF CURB & CURB
 INLET PROT.DWG

SAVED 8/1/2024 10:59:11 AM BY BILL SEXSON
 PLOTTED 5/27/2025 9:45:46 AM BY KEVIN GRAHAM
 U:\WICHITA-CIVIL\2020\200605004\2PD3_PAVING PHASE 1\44-200605-004-CG501 BACK OF CURB & CURB
 INLET PROT.DWG



REVISION DATE: MAY 2013

BACK OF CURB PROTECTION, CURB INLET PROTECTION AND CONSTRUCTION ENTRANCE

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER: 472-
 OCA NUMBER: _____
 DATE: _____

CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 455 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

SHEET
 ---- of ----

SW-501

PEC
 PROFESSIONAL ENGINEERING CONSULTANTS
 303 SOUTH TOPEKA
 WICHITA, KS 67202
 316-262-2691 www.pec1.com

CITY OF WICHITA

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085958

PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
PINEWAY ADDITION PHASE 1

Issue: _____

JOB NO. 200605-004
 DATE MAY 2025
 PM KPG
 DESIGNED BY KPG
 DRAWN BY BJS
 CHECKED BY KMS

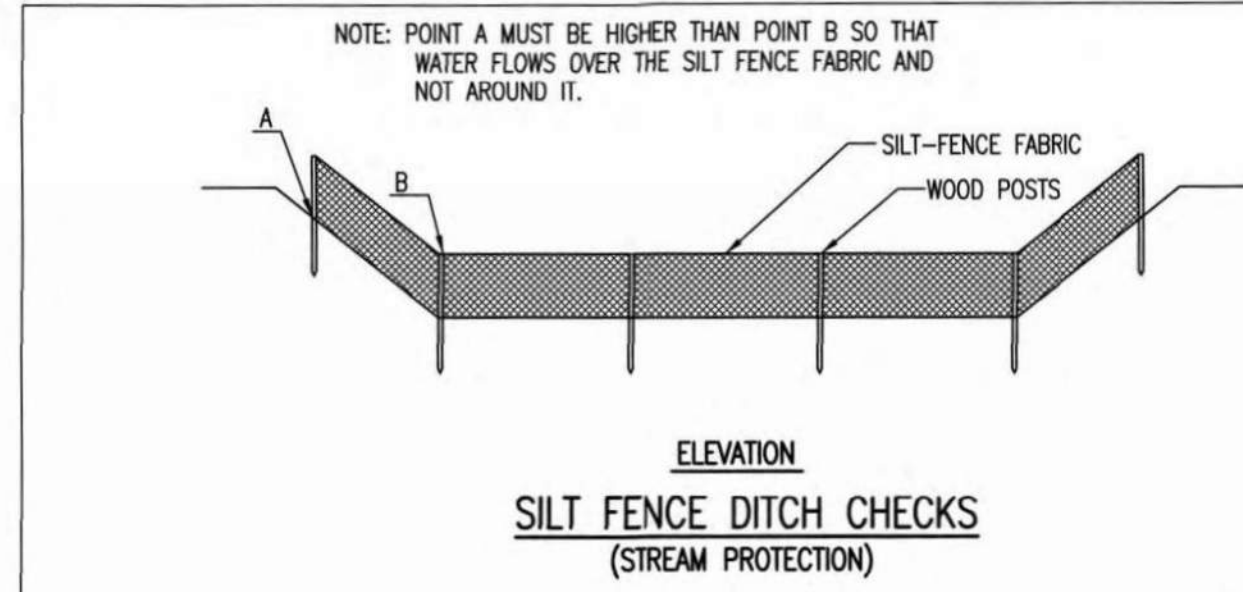
BACK OF CURB AND CURB INLET PROTECTION

CG501
 44 OF 71

Saved 8/1/2024 10:59:20 AM BY BILL SEXSON
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 U:\WICHITA-CIVIL\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\45-200605-004-CG502 SILT FENCE DITCH CHECK.DWG

1 2 3 4 5 6

A
B
C
D
E



ELEVATION
SILT FENCE DITCH CHECKS
(STREAM PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK, NOT OVER IT. SILT FENCE DITCH CHECKS OFTEN FAIL WHEN OVERTOPPED. SILT FENCE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE SILT FENCE SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE TOP OF THE LOW POINT OF THE FENCE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. SILT FENCE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. SILT FENCE SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.

THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK DITCH GRADE (%)	SPACING CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS AT LEAST 12" DEEP BY 6" WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSTREAM SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSTREAM EDGE OF THE TRENCH. LINE TWO SIDES OF THE TRENCH WITH THE FABRIC AS SHOWN ON DETAIL. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE ON THE UPSTREAM SIDE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSTREAM OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 24". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

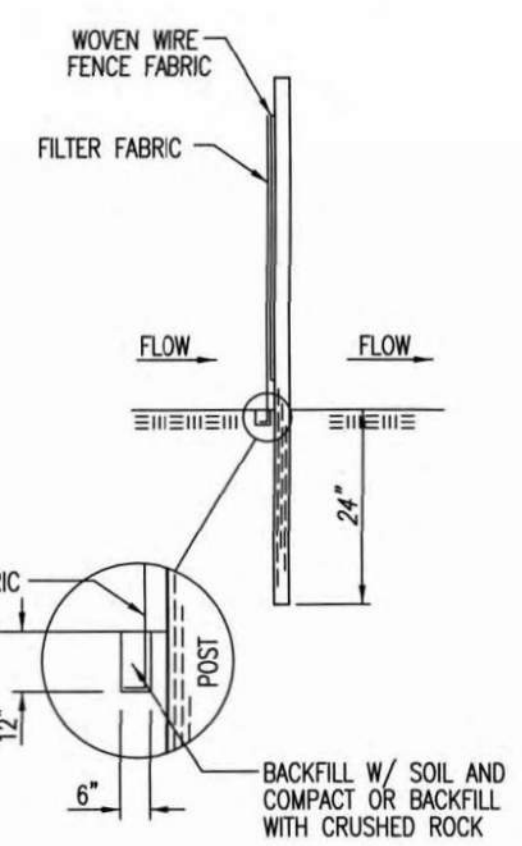
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK-NOT OVER IT. PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. SILT FENCE INSTALLATIONS QUICKLY DETERIORATE WHEN WATER OVERTOPS THEM. DO NOT PLACE SILT FENCE POSTS ON THE UPSTREAM SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE A SILT FENCE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE SILT FENCE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE LOW POINT ON THE TOP OF THE FENCE. DO NOT PLACE SILT FENCE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

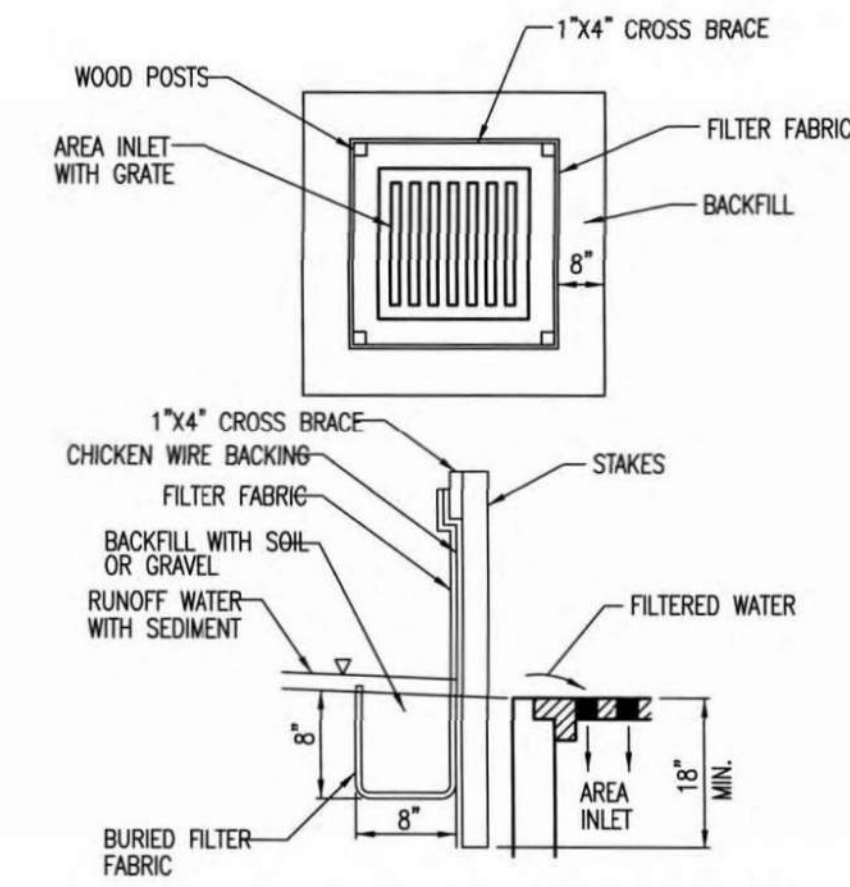
INSPECTION AND MAINTENANCE:

SILT FENCE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



ANCHOR TRENCH DETAIL



SILT FENCE BARRIERS FOR AREA INLETS
(INLET PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE WIRE OR POLYMERIC MESH BACKING USED TO HELP SUPPORT THE SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. THE MATERIAL USED TO FRAME THE TOPS OF THE POSTS SHOULD BE 1" BY 4" BOARDS. SILT FENCE FABRIC AND SUPPORT BACKING SHOULD BE ATTACHED TO THE WOODEN POSTS AND FRAME WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE A SILT FENCE DROP INLET BARRIER IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. WATER SHOULD FLOW THROUGH SILT FENCE, NOT OVER IT. SILT FENCE BARRIERS FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. WHEN USED AS A BARRIER FOR AREA INLETS, SILT FENCE FABRIC AND POSTS MUST BE SUPPORTED AT THE TOP BY A WOODEN FRAME. WHEN A SILT FENCE BARRIER FOR AREA INLETS IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 8" DEEP BY 8" WIDE. DRIVE POSTS TO A DEPTH OF AT LEAST 18" AROUND THE PERIMETER OF THE AREA INLET. THE DISTANCE BETWEEN POSTS SHOULD BE 4' OR LESS. IF THE DISTANCE BETWEEN TWO ADJACENT CORNER POSTS IS MORE THAN 4', ADD ANOTHER POST(S) BETWEEN THEM. CONNECT THE TOPS OF ALL THE POSTS WITH A WOODEN FRAME MADE OF 1" BY 4" BOARDS. USE NAILS OR SCREWS FOR FASTENING. ATTACH THE WIRE OR POLYMERIC-MESH BACKING TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC LONG ENOUGH TO WRAP AROUND THE PERIMETER OF THE AREA INLET. ADD MORE LENGTH FOR OVERLAPPING THE FABRIC JOINT. PLACE THE EDGE OF THE FABRIC IN THE TRENCH, STARTING AT THE OUTSIDE EDGE OF THE TRENCH. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. ATTACH THE SILT FENCE TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. THE JOINT SHOULD BE OVERLAPPED TO THE NEXT POST.

NOTE: WHEN A SILT FENCE BARRIER FOR AREA INLET IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

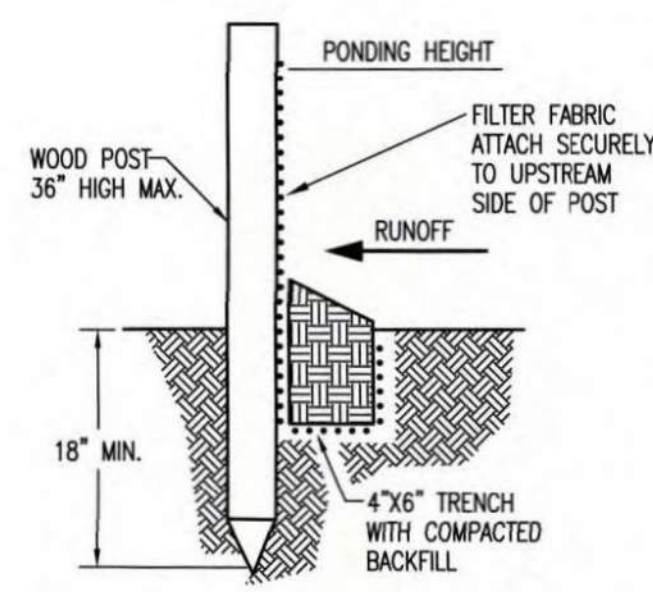
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE BARRIER FOR AREA INLET-NOT OVER IT. PLACE A SILT FENCE BARRIER FOR AREA INLET IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. SILT FENCE BARRIER FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. DO NOT PLACE POSTS ON THE OUTSIDE OF THE SILT FENCE BARRIER FOR AREA INLET. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESISTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT INSTALL SILT FENCE BARRIER FOR AREA INLETS WITHOUT FRAMING THE TOP OF THE POSTS. THE CORNER POSTS AROUND AREA INLETS ARE STRESSED IN TWO DIRECTIONS WHEREAS A NORMAL SILT FENCE IS ONLY STRESSED IN ONE DIRECTION. THIS ADDED STRESS REQUIRES MORE SUPPORT.

INSPECTION AND MAINTENANCE:

SILT FENCE BARRIER FOR AREA INLETS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE SILT FENCE?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



SILT FENCE BARRIERS

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, SILT FENCE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. SILT FENCE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 6" DEEP BY 4" WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT-FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE UPSLOPE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 18". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICABLE, DO NOT PLACE SILT FENCE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. WHEN THE FLOW CONCENTRATES, IT OVERTOPS THE BARRIER AND THE SILT FENCE SLOPE BARRIER QUICKLY DETERIORATES. DO NOT PLACE SILT-FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE SILT FENCE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT SUFFICIENTLY ANCHORED, IT WILL WASH OUT. SILT FENCE SLOPE BARRIERS MUST BE DUG INTO THE GROUND-SILT FENCE AT GROUND LEVEL DOES NOT WORK BECAUSE WATER WILL FLOW UNDERNEATH.

INSPECTION AND MAINTENANCE:

SILT FENCE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DO THE SILT FENCES SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

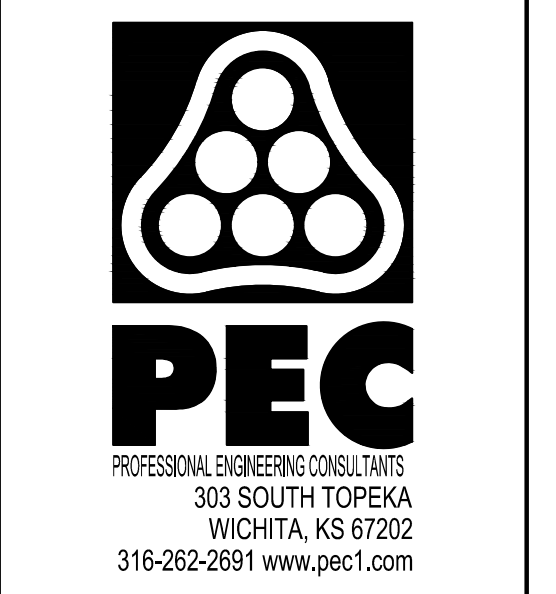
REVISION DATE: MAY 2013



SILT FENCE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER 472-2024-086026	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501	SHEET 2 of	



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

PINENEY ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086958

Issue:		
JOB NO.	200605-004	
DATE	MAY 2025	
PM	KPG	
DESIGNED BY	KPG	
DRAWN BY	BJS	
CHECKED BY	KMS	

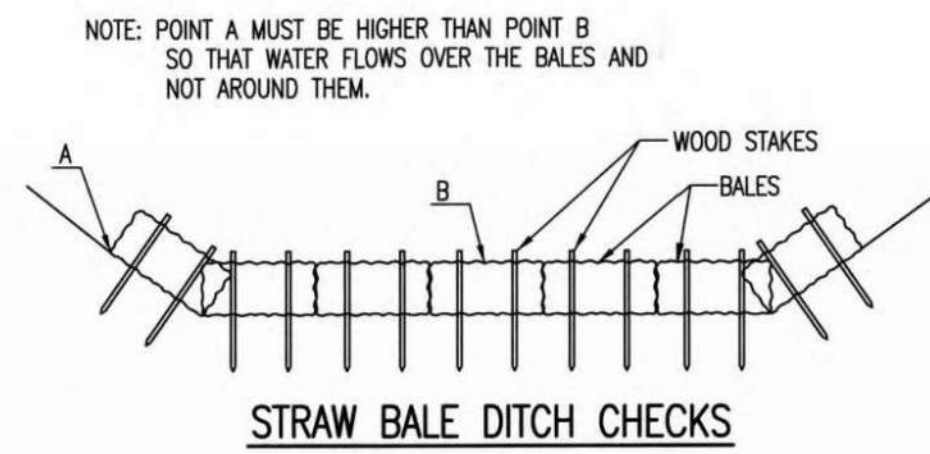
SILT FENCE DITCH CHECK

CG502
45 OF 71

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 PLOTTED 5/27/2025 9:45:55 AM BY KEVIN GRAHAM
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Scale: 08-01-2024 10:59:35 AM by BILL SEXSON
 Plot Scale: 1:1 05-27-2025 9:45:55 AM by KEVIN GRAHAM
 U:\Wichita-Civil\2020\200605\04\2PD3_PLANS\0301_PAVING PHASE 1\46-200604-004-CG503 STRAW BALE DITCH CHECK



STRAW BALE DITCH CHECKS

MATERIAL SPECIFICATION:
 BALE DITCH CHECKS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG.
 OPTIONAL: THE DOWNSTREAM SCOUR APRON SHOULD BE CONSTRUCTED OF A DOUBLE-NETTED STRAW EROSION-CONTROL BLANKET AT LEAST 6' WIDE.
 OPTIONAL: THE METAL LANDSCAPE STAPLES USED TO ANCHOR THE EROSION-CONTROL BLANKET SHOULD BE AT LEAST 8" LONG.

PLACEMENT:
 BALE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE DITCH CHECK SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK.
 STRAW BALE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD.
 BALES SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.
 THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH GRADE (%)	CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH—IT WILL BE USED LATER.
 OPTIONAL: ON THE DOWNSTREAM SIDE OF THE TRENCH, ROLL OUT A LENGTH OF EROSION-CONTROL BLANKET (SCOUR APRON) EQUAL TO THE LENGTH OF THE TRENCH. PLACE THE UPSTREAM EDGE OF THE EROSION-CONTROL BLANKET ALONG THE BOTTOM UPSTREAM EDGE OF THE TRENCH. THE EROSION CONTROL BLANKET SHOULD BE ANCHORED IN THE TRENCH WITH ONE ROW OF 8" LANDSCAPE STAPLES PLACED ON 18" CENTERS. THE REMAINDER OF THE EROSION-CONTROL BLANKET (THE PORTION THAT IS NOT LYING IN THE TRENCH) WILL SERVE AS THE DOWNSTREAM SCOUR APRON. THIS SECTION OF THE BLANKET SHOULD BE ANCHORED TO THE GROUND WITH 8" LANDSCAPE STAPLES PLACED AROUND THE PERIMETER OF THE BLANKET ON 18" CENTERS. THE REMAINDER OF THE BLANKET SHOULD BE ANCHORED USING TWO EVENLY SPACED ROWS OF 8" LANDSCAPE STAPLES ON 18" CENTERS PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH.
 PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.
 ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSTREAM SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP AND EXTEND UPSTREAM NO MORE THAN 24".

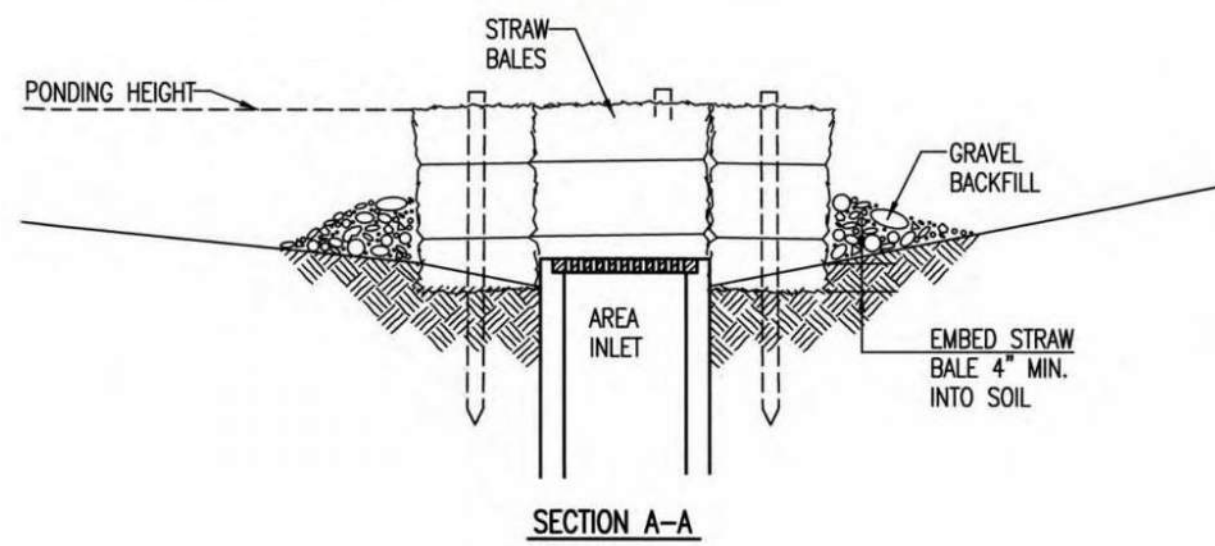
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

DO NOT PLACE A BALE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW.
 DO NOT PLACE BALE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW.
 FOLLOW PRESCRIBED DITCH-CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS.
 DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE.
 DO NOT PLACE BALE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.
 BALE DITCH CHECKS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE CHECK.

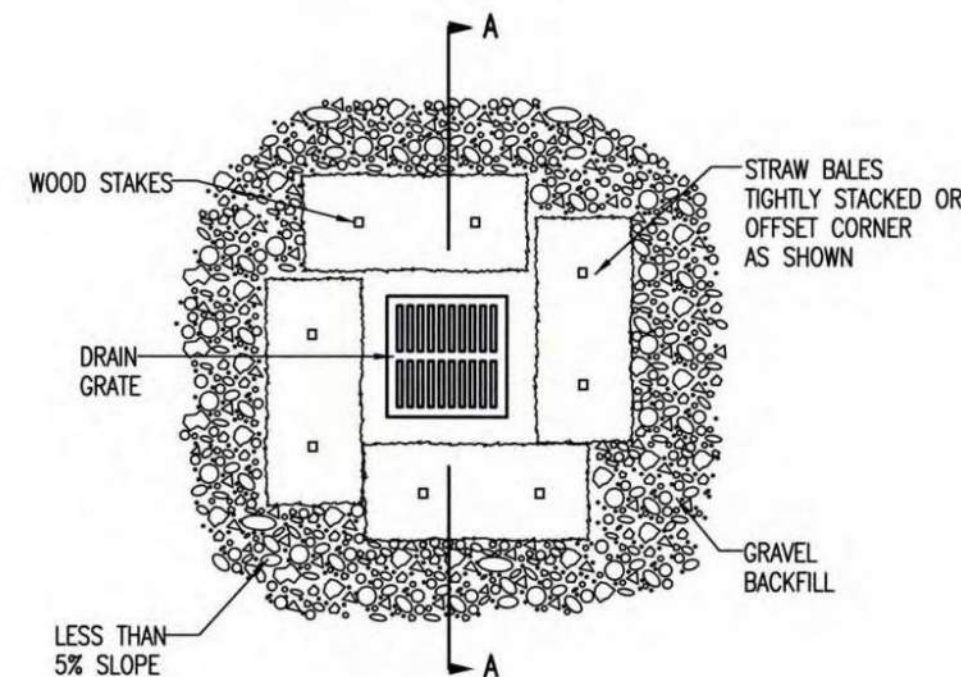
INSPECTION AND MAINTENANCE:

BALE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES AND/OR SCOUR APRONS (OPTIONAL) DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



SECTION A-A



STRAW BALE BARRIERS FOR AREA INLETS (INLET PROTECTION)

MATERIAL SPECIFICATION:

BALE AREA INLET BARRIERS SHOULD BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG.
 TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

BALE AREA INLET BARRIERS SHOULD BE PLACED DIRECTLY AROUND THE PERIMETER OF A DROP INLET. WHEN A BALE AREA INLET BARRIER IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 4" DEEP BY A BALE'S WIDTH WIDE.
 PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. SOME BALES MAY NEED TO BE SHORTENED TO FIT INTO THE TRENCH AROUND THE AREA INLET. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.
 ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE RECEIVING SIDE OF THE BARRIER AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.
 NOTE: WHEN A BALE AREA INLET BARRIER IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

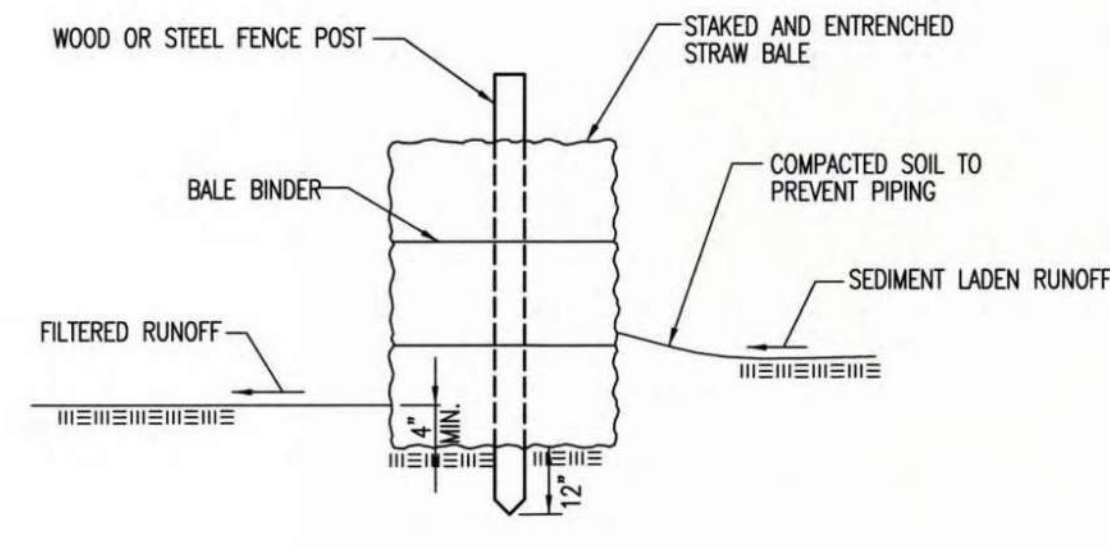
LIST OF COMMON PLACEMENT INSTALLATION MISTAKES TO AVOID:

BALES SHOULD BE PLACED DIRECTLY AGAINST THE PERIMETER OF THE AREA INLET. THIS ALLOWS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR. BALE AREA INLET BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE AREA INLET BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE AREA INLET BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



STRAW BALE BARRIERS

MATERIAL SPECIFICATION:

BALE SLOPE BARRIERS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG.
 TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT.
 WHEN PRACTICABLE, BALE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW.
 BALE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE.
 PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.
 ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSLOPE SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICAL, DO NOT PLACE BALE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. CONCENTRATED FLOW OVER A SLOPE BARRIER CREATES A SCOUR HOLE ON THE DOWNSLOPE SIDE OF THE BARRIER. THE SCOUR HOLE EVENTUALLY UNDERMINES THE BALES AND THE BARRIER FAILS.
 DO NOT PLACE BALE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.
 BALE SLOPE BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?





CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

REVISION DATE: MAY 2013

STRAW BALE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER: 472-2024-086026
OCA NUMBER: _____
DATE: _____

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET: _____ of _____

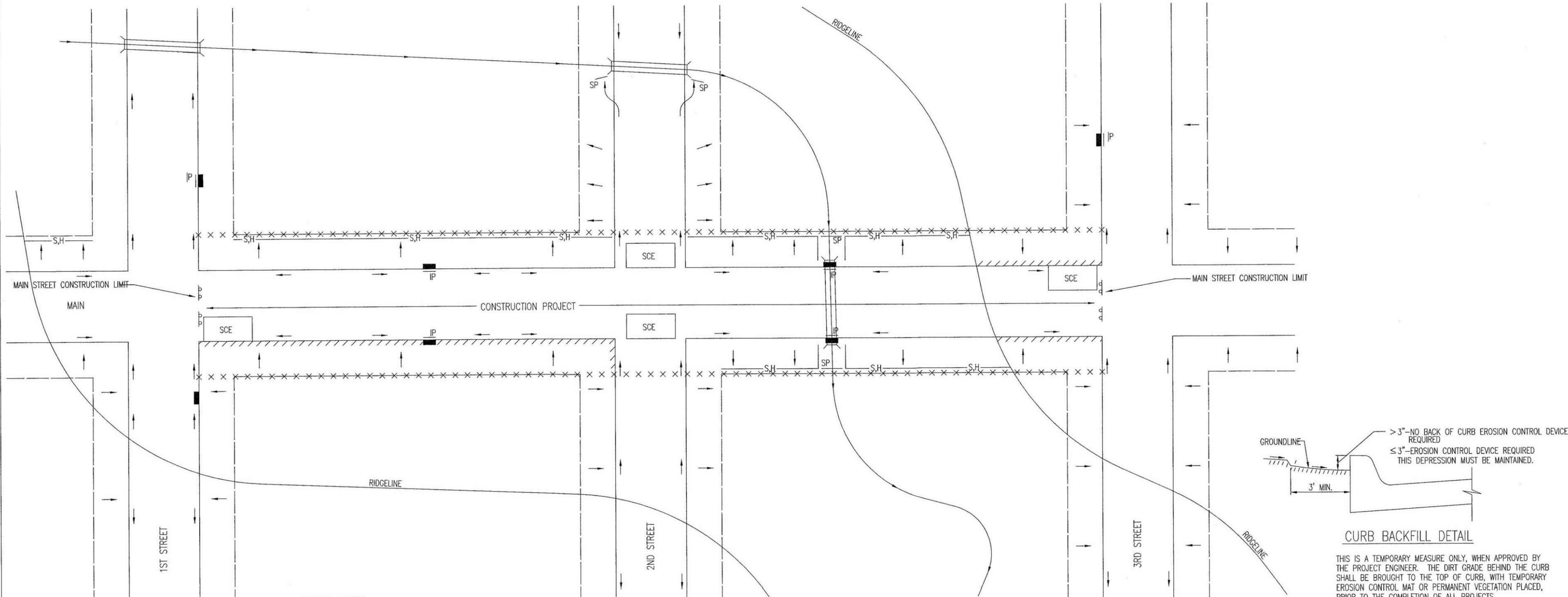
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
 PHASE 1
 PINENEY ADDITION
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086958

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

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GENERAL NOTES

- THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPES OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
- EROSION CONTROL DEVICES MUST BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PROCESS AND UNTIL THE DISTURBED EARTH IS RESTABILIZED.
- IF THE PROJECT WILL DISTURB 1 ACRE OR MORE, A FEDERAL/STATE NPDES STORMWATER PERMIT IS REQUIRED. A DETAILED STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED TO BE THE MINIMUM TO BE SHOWN IN THE POLLUTION PREVENTION PLAN.
- FOR PROJECTS DISTURBING LESS THAN 1 ACRE, CONTRACTORS ARE ENCOURAGED TO PREPARE STORMWATER POLLUTION PREVENTION PLANS PRIOR TO CONSTRUCTION. EROSION CONTROL DEVICES MUST BE USED ON ALL PROJECTS.
- FAILURE TO USE AND MAINTAIN EROSION CONTROL DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE CONTRACTOR TO THE PENALTIES PROVIDED FOR THEREIN.
- THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT DEVICE OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED AS LONG AS THEY ARE EFFECTIVE AND MAINTAINED.

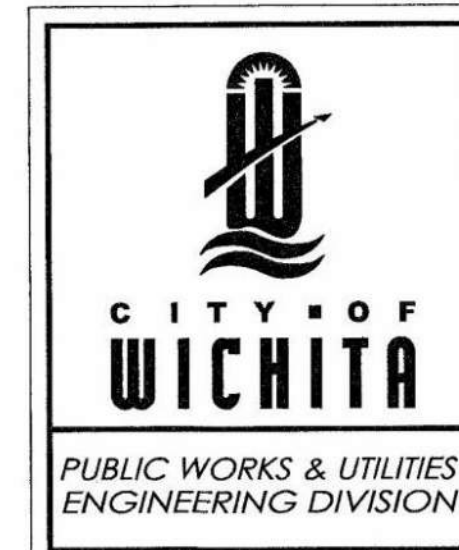


LEGEND

- R-O-W LIMITS
- DRAINAGE FLOW PATH
- × × × × R/W LIMIT WITHIN CONSTRUCTION LIMIT
- STORM WATER INLETS
- IP INLET PROTECTION
- S.H— SILT FENCE OR HAY BALE BARRIER
- SP— STREAM PROTECTION
- SCE STABILIZED CONSTRUCTION ENTRANCE
- /// BACK OF CURB PROTECTION

GENERAL NOTES

- THE INTENT OF ALL EROSION CONTROL DEVICES IS TO KEEP ALL SEDIMENT CONFINED TO THE CONSTRUCTION SITE, AND OUT OF ALL UNDERGROUND PIPES, DITCHES, LAKES, AND OTHER DRAINAGE FACILITIES, AND OFF OF STREETS.
- THE POINT OF COMPLIANCE IS GENERALLY THE RIGHT-OF-WAY LINES WITHIN THE LIMITS OF CONSTRUCTION.
- EROSION CONTROL DEVICES WILL BE REQUIRED AT ALL POINTS ALONG THE PROJECT WHERE DISTURBED EARTH CAN DRAIN ONTO PRIVATE PROPERTY.
- INLET PROTECTION DEVICES WILL BE REQUIRED WHEREVER WATER CAN DRAIN OFF THE PROJECT SITE INTO AN INLET, INCLUDING ANY SIDE STREET INLETS.
- EROSION CONTROL DEVICES SHALL BE INSTALLED AT CREEK CROSSINGS SO AS TO PREVENT SEDIMENT FROM ENTERING THEREIN.
- STABILIZED CONSTRUCTION ENTRANCES SHALL BE PROVIDED, AS NEEDED, TO PREVENT MUD FROM TRACKING ONTO STREETS NOT UNDER CONSTRUCTION AND ON STREETS WITHIN THE PROJECT LIMITS IF TRAFFIC IS BEING MAINTAINED THROUGH THE PROJECT.
- ANY MUD TRACKED ONTO STREETS MUST BE REMOVED AT THE END OF EACH WORK DAY.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE EROSION CONTROL DEVICES BACK OF CURB, WHENEVER WATER CAN DRAIN OVER CURB, TO KEEP ERODED SOIL OUT OF THE GUTTERLINES, IN ACCORDANCE WITH THE FOLLOWING:
 - THE DEVICE REQUIRED WILL BE APPROVED EROSION CONTROL MAT LISTED ON THE CITY'S APPROVED MATERIAL LIST. SAID BLANKET SHALL BE PLACED OVER THE APPROPRIATE SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS. (SEE SOIL EROSION BMPs - BACK OF CURB SEDIMENT BARRIER DETAILS)
 - THIS DEVICE SHALL BE INSTALLED IMMEDIATELY WHENEVER THE CURB IS BACKFILLED TO WITHIN 3" OF THE TOP OF CURB. (SEE CURB BACKFILL DETAIL)
 - ADDITIONALLY, OTHER EROSION CONTROL DEVICES (HAY BALES, SILT FENCE, ETC.) WILL BE INSTALLED AT LOCATIONS WHERE CONCENTRATED FLOW RESULTING IN SEDIMENT OVERRUNNING THE MAT.
 - SHOULD THE PROJECT PLANS SPECIFY THAT THE RIGHT-OF-WAY IS TO BE SOODED, THE EXCELSIOR MAT WILL NOT BE REQUIRED SO LONG AS THE SOD IS PLACED WITHIN 48 HOURS AFTER CURB BACKFILL REACHES A HEIGHT OF 3" OR LESS FROM TOP OF CURB. (SEE CURB BACKFILL DETAIL)

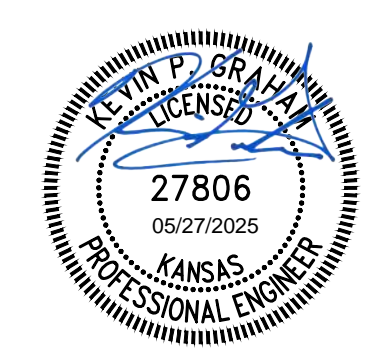
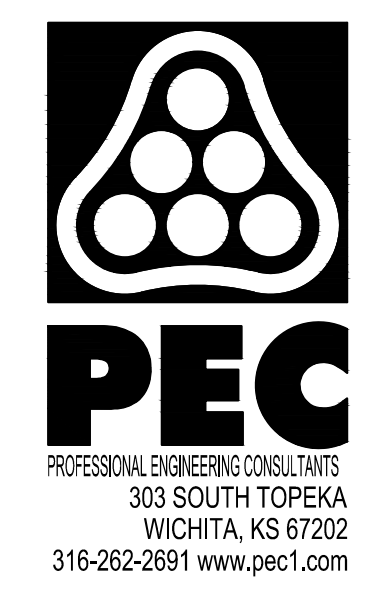


STREET IMPROVEMENT PROJECTS	
CITY ENGINEER GARY JANZEN, P.E.	
PROJECT NUMBER 472-2024-086026	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501	
SHEET 4	

PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
PINEWAY ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086958

Issue:	
JOB NO.	200605-004
DATE	MAY 2025
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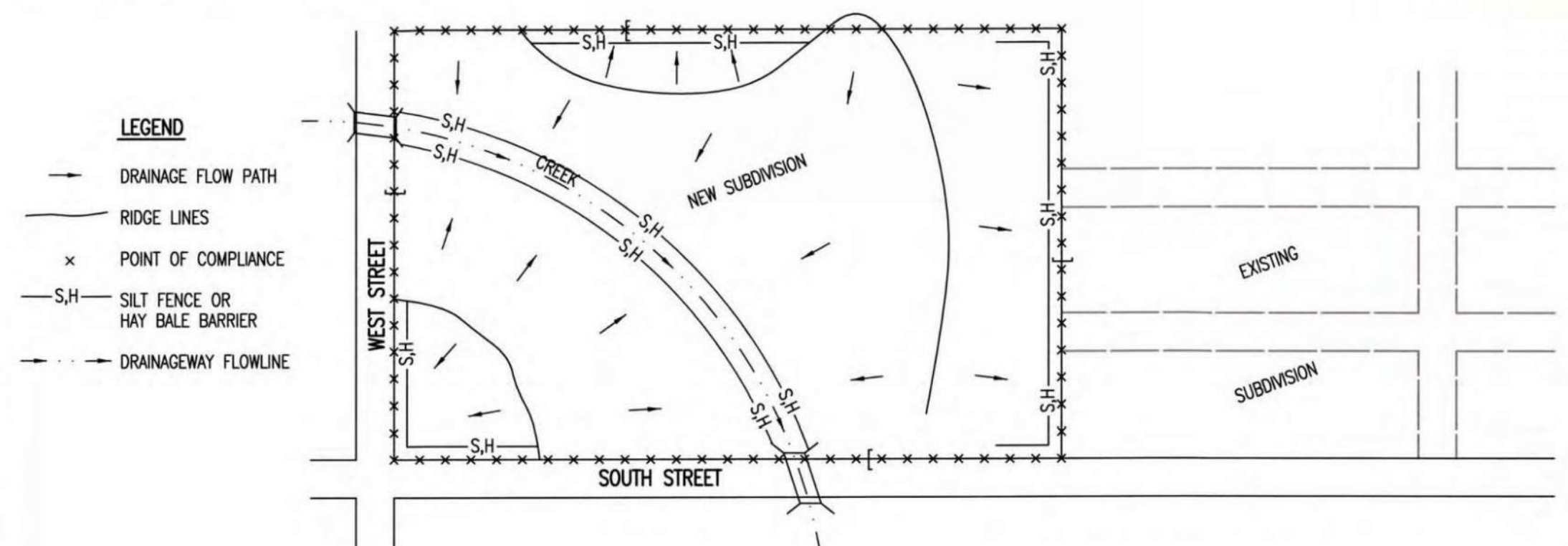
STREET IMPROVEMENT PROJECTS
CG504
 47 OF 71



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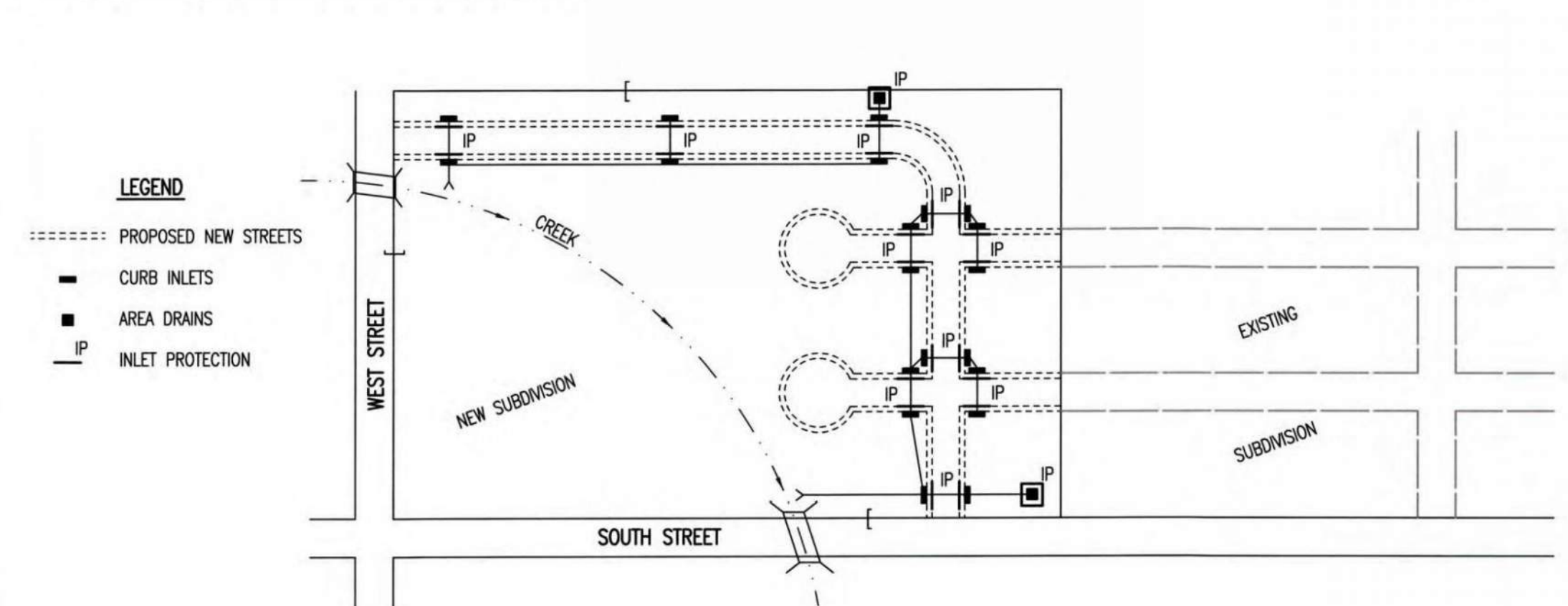
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 U:\Wichita-Civil\2020\200605\004\2PD3_PLANS\0301_PAVING PHASE 1\48-200604-004-CG505 SUBDIVISION DEVELOPMENT PROCESS

PHASE 1 - INITIAL EARTHWORK AND UTILITIES (EXCEPT STORM SEWER)



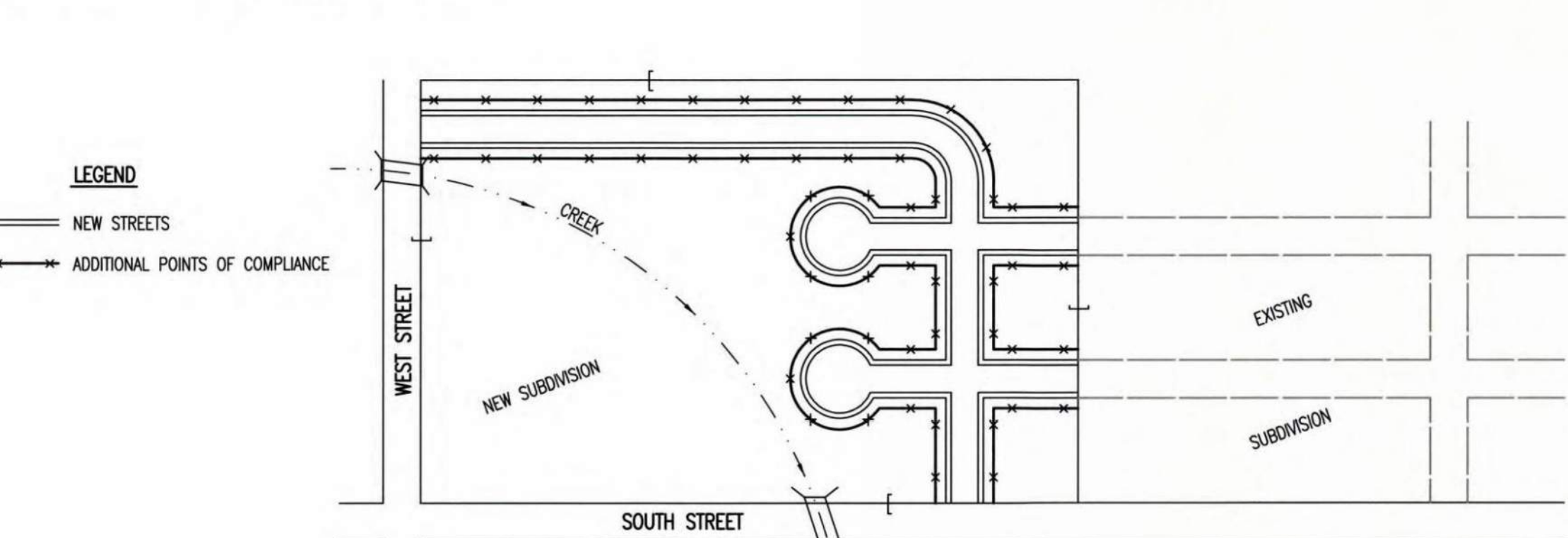
- LEGEND**
- DRAINAGE FLOW PATH
 - RIDGE LINES
 - x POINT OF COMPLIANCE
 - S.H- SILT FENCE OR HAY BALE BARRIER
 - - - DRAINAGEWAY FLOWLINE
- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, THE POINTS OF COMPLIANCE ARE THE PERIMETER BOUNDARIES AND ANY DRAINAGE WAYS OR STORM SEWERS DRAINING THROUGH OR FROM THE SITE. SHOULD LAKES BE CONSTRUCTED WITHIN THE SUBDIVISION THAT WILL DISCHARGE DURING STORMS, THEY ARE ALSO A POINT OF COMPLIANCE.
 - HAY BALES OR SILT FENCE MUST BE CONSTRUCTED ALONG THE PROPERTY LINE WHERE ON SITE WATER CAN DRAIN OFF THE PROPERTY. THESE EROSION CONTROL DEVICES WILL ALSO BE INSTALLED ALONG ANY DRAINAGE DITCH OR LAKE THAT CAN DISCHARGE.
 - SHOULD SILT OR SEDIMENT ENTER THE DITCHES OR STREETS ON THE ADJACENT BOUNDARY STREETS, APPROPRIATE EROSION CONTROL DEVICES WILL BE PLACED WITHIN THE SUBDIVISION TO PREVENT THIS.
 - ANY MUD TRACKED ONTO ADJACENT STREETS WILL BE REMOVED WITHIN 48 HOURS OR BY FRIDAY AT 6:00 PM, WHICHEVER IS EARLIER.
 - CONTRACTORS WORKING WITHIN THE SITE WILL NOT BE REQUIRED TO USE INDIVIDUAL EROSION CONTROL DEVICES AS LONG AS THOSE SPECIFIED ABOVE ARE IN PLACE AND EFFECTIVE. CONTRACTORS WORKING ON THE BOUNDARY LINE STREETS OR ON ADJACENT PROPERTIES TO EXTEND UTILITIES ARE EXPECTED TO USE EROSION CONTROL DEVICES AT THEIR WORK LOCATIONS, AS NEEDED.
 - UTILIZE STABILIZED CONSTRUCTION ENTRANCE AT ENTRANCE AND EXIT ONTO ANY EXISTING PUBLIC STREETS.
 - IF THE INITIAL EARTH WORK AND UTILITIES ARE DONE AS PART OF A PUBLIC IMPROVEMENT PROJECT, THESE EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS SPECIFIED IN THE INDIVIDUAL PROJECT CONTRACTS. THE CONTRACTOR WILL MAINTAIN THE DEVICES UNTIL COMPLETION OF THE CONTRACT, AT WHICH TIME THE DEVELOPER WILL ASSUME MAINTENANCE RESPONSIBILITIES. IF THESE CONTRACTS ARE NOT PUBLIC IMPROVEMENT PROJECTS, THE DEVELOPER WILL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THESE DEVICES.
 - WITHIN 14 DAYS OF COMPLETION OF EARTHWORK ACTIVITIES IN ANY GIVEN AREA, THAT AREA SHALL BE TEMPORARILY OR PERMANENTLY SEEDED AND MULCHED.

PHASE 2 - INSTALLATION OF STORM SEWER



- LEGEND**
- - - PROPOSED NEW STREETS
 - CURB INLETS
 - AREA DRAINS
 - IP INLET PROTECTION
- DURING THIS PHASE OF SUBDIVISION DEVELOPMENT, ALL EROSION CONTROL DEVICES REQUIRED IN PHASE 1 SHALL REMAIN IN PLACE AND BE MAINTAINED.
 - AS NEW STORM SEWERS, WITH INLETS, ARE INSTALLED, THE STORM SEWERS MUST NOW BE PROTECTED SO ALL NEW INLETS BECOME POINTS OF COMPLIANCE.
 - AREA DRAINS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, HAY BALE OR SILT FENCE PROTECTION WILL BE INSTALLED AROUND THEM.
 - CURB OPENING INLETS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, INLET PROTECTION DEVICES MUST BE INSTALLED. IF WATER CANNOT FLOW INTO CURB INLETS UNTIL STREET CONSTRUCTION IS COMPLETE, THEN STREET CONTRACTOR WILL INSTALL INLET PROTECTION. SEE PHASE 3 - STREET CONSTRUCTION.
 - THE STORM SEWER CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING THESE DEVICES.
 - THE SUBDIVISION DEVELOPER WILL MAINTAIN THESE EROSION CONTROL DEVICES ONCE INSTALLED.
 - ALL DISTURBED GROUND WILL BE FINAL GRADED AND TEMPORARILY OR PERMANENTLY SEEDED WITHIN 14 DAYS IF COMPLETION OF WORK IN ANY GIVEN PART OF THE SUBDIVISION.
 - ONCE ALL DISTURBED GROUND DRAINING TO AN INLET HAS BEEN RESTABILIZED WITH GRASS OR SOO, THE SUBDIVISION DEVELOPER WILL BE RESPONSIBLE FOR PERMANENTLY REMOVING THE INLET PROTECTION.

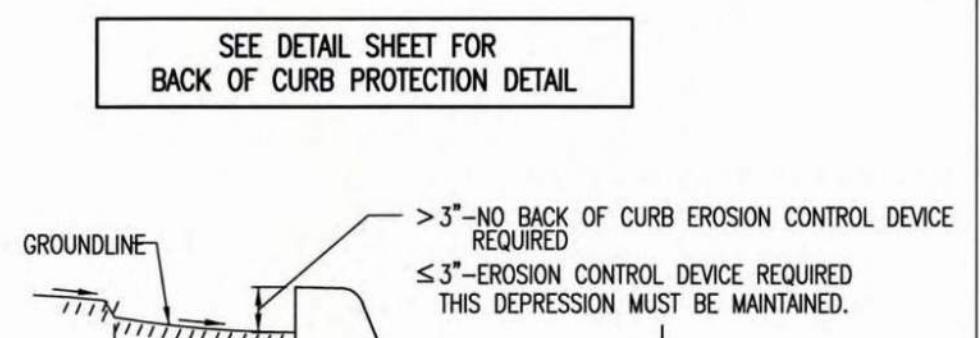
PHASE 3 - STREET CONSTRUCTION



- LEGEND**
- == NEW STREETS
 - ADDITIONAL POINTS OF COMPLIANCE
- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, NEW STREETS ARE INSTALLED. ALL EROSION CONTROL DEVICES INSTALLED DURING PHASE 1 AND 2 MUST STILL BE MAINTAINED. THE POINT OF COMPLIANCE NOW SHIFTS TO THE BACK OF CURB ALONG EACH STREET.
 - CURB OPENING INLET PROTECTION:
 - SUMP AREAS - INLET PROTECTION SHALL BE PROVIDED WHEN STREET SUBGRADE WORK IS COMPLETED.
 - NON-SUMP LOCATIONS - PROVIDE INLET PROTECTION AS SOON AS BASE COURSE ASPHALT IS INSTALLED, BEFORE THE SURFACE COURSE LIFT.
 - EROSION CONTROL DEVICES WILL BE REQUIRED BACK OF CURB WHEREVER WATER CAN FLOW OVER THE CURB AND THE CURB HAS BEEN BACKFILLED TO WITHIN 3" OR LESS OF THE TOP OF CURB (SEE CURB BACKFILL DETAIL). FOR CURBS NOT YET ENTIRELY BACKFILLED (3" OR MORE BELOW TOP OF CURB), ADDITIONAL DEVICES WILL BE REQUIRED AT POINTS WHERE WATER BREAKS OVER CURB WHICH COULD RESULT IN THE PLACEMENT OF SEDIMENT IN THE GUTTER.
 - SEE DETAIL SHEET FOR BACK OF CURB PROTECTION.
 - THE BACK OF CURB PROTECTION SPECIFIED ON THIS PLAN MAY HAVE TO BE SUPPLEMENTED WITH HAY BALE OR SILT FENCE EROSION CONTROL DEVICES AT LOCATIONS WHERE CONCENTRATED FLOW RESULTS IN SEDIMENT BEING CARRIED OVER THE EXCELSDOR MATS.
 - THE STREET CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING BACK OF CURB EROSION CONTROL DEVICES.
 - THE INDIVIDUAL LOT OWNERS WILL BE RESPONSIBLE FOR MAINTAINING THE BACK OF CURB EROSION CONTROL DEVICES IN FRONT OF THEIR LOTS UNTIL SUCH TIME AS ADJACENT DISTURBED EARTH IS STABILIZED WITH GRASS OR SOO.

GENERAL NOTES

- THE INTENT OF ALL EROSION CONTROL DEVICES IS TO PREVENT ERODED SOIL FROM ENTERING DITCHES, STORM SEWERS, LAKES, STREETS OR ANY OTHER OTHER DRAINAGE FEATURE.
- THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPE OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
- EROSION CONTROL DEVICES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS TO REMAIN EFFECTIVE. MAINTENANCE SHALL BE AS INDICATED ON SOIL EROSION BMP'S DETAIL SHEETS.
- PERSONS DESTROYING EROSION CONTROL DEVICES SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING THEM OR INSTALLING SUITABLE REPLACEMENT DEVICES.
- THE DEVELOPMENT OF ANY SUBDIVISION THAT DISTURBS 1 ACRE OR MORE WILL REQUIRE A FEDERAL/STATE NPDES STORMWATER PERMIT. THE PREPARATION OF A STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. EROSION CONTROL DEVICES ARE REQUIRED. THE DETAILS SHOWN ON THIS SHEET ARE THE MINIMUM STANDARDS TO BE SHOWN ON POLLUTION PREVENTION PLANS.
- FOR SUBDIVISIONS SMALLER THAN 1 ACRE, SOIL EROSION DEVICES ARE REQUIRED. ALSO, DEVELOPERS AND CONTRACTORS ARE ENCOURAGED TO DEVELOP POLLUTION PREVENTION PLANS FOR EACH PROJECT PRIOR TO CONSTRUCTION.
- FAILURE TO USE AND MAINTAIN SOIL EROSION DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE SUBDIVISION DEVELOPER AND CONTRACTORS TO THE PENALTIES PROVIDED THEREIN.
- THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE DEVICES OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED SO LONG AS THEY ARE EFFECTIVE AND MAINTAINED.
- A STABILIZED EARTH SURFACE IS DEFINED AS ONE THAT IS HARD SURFACED WITH CONCRETE, ASPHALT, OR THE LIKE, OR ONE ON WHICH 70% OF THE GRASS HAS GERMINATED ON THE ENTIRE SURFACE.



CURB BACKFILL DETAIL (STREET CONSTRUCTION ONLY)

THIS IS A TEMPORARY MEASURE ONLY. WHEN APPROVED BY THE PROJECT ENGINEER, THE DIRT GRADE BEHIND THE CURB SHALL BE BROUGHT TO THE TOP OF CURB, WITH TEMPORARY EROSION CONTROL MAT OR PERMANENT VEGETATION PLACED, PRIOR TO THE COMPLETION OF ALL PROJECTS.

CITY OF WICHITA

PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

SUBDIVISION DEVELOPMENT PROCESS

CITY ENGINEER
GARY JANZEN, P.E.

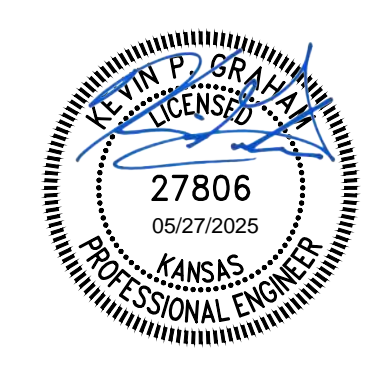
PROJECT NUMBER	OCA NUMBER	DATE
472-2024-086026		

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET
5 of 5



SW-505



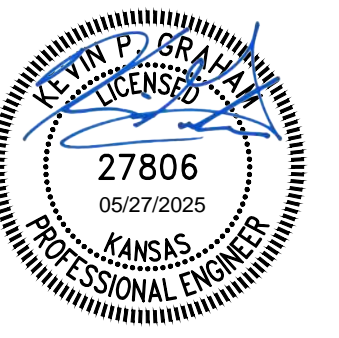
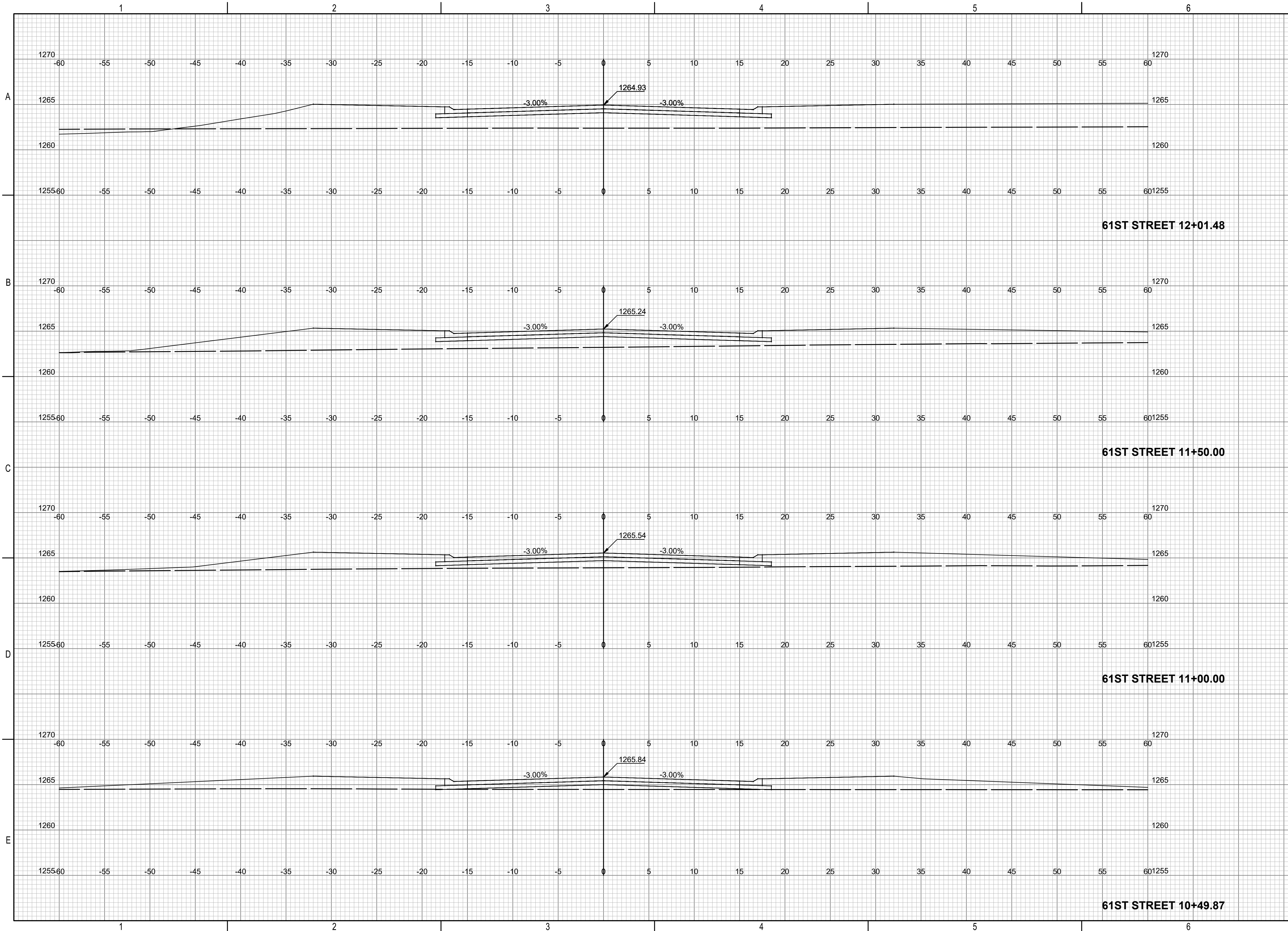
PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
PINEWAY ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086958

Issue:									

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

SUBDIVISION DEVELOPMENT PROCESS

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 ST SOUTH.DWG



PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

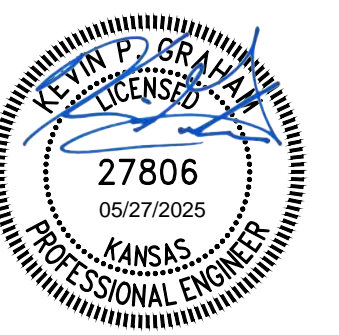
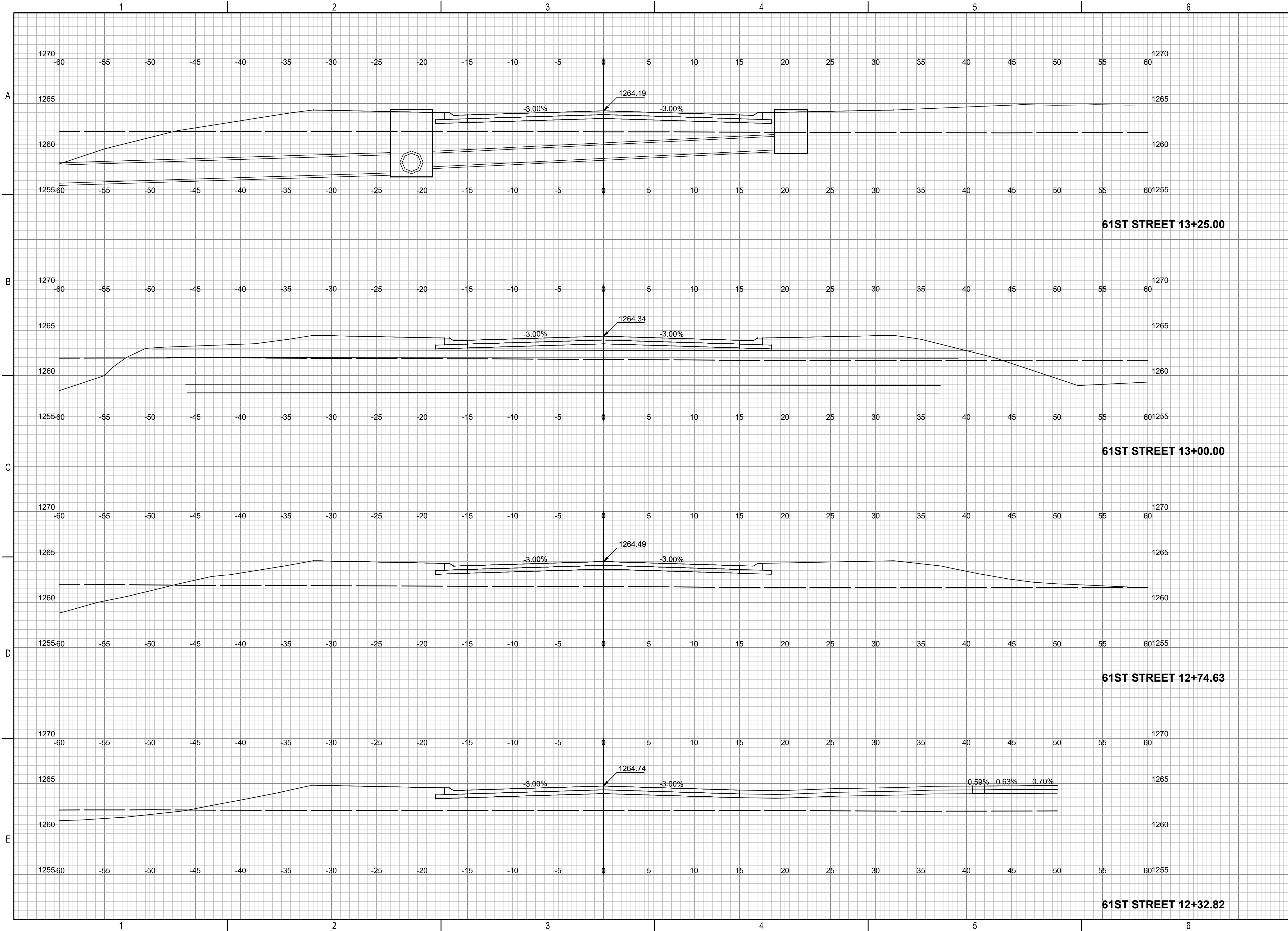
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JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-61ST ST
 SOUTH

CX601
 49 OF 71

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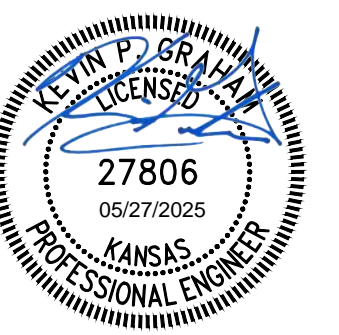
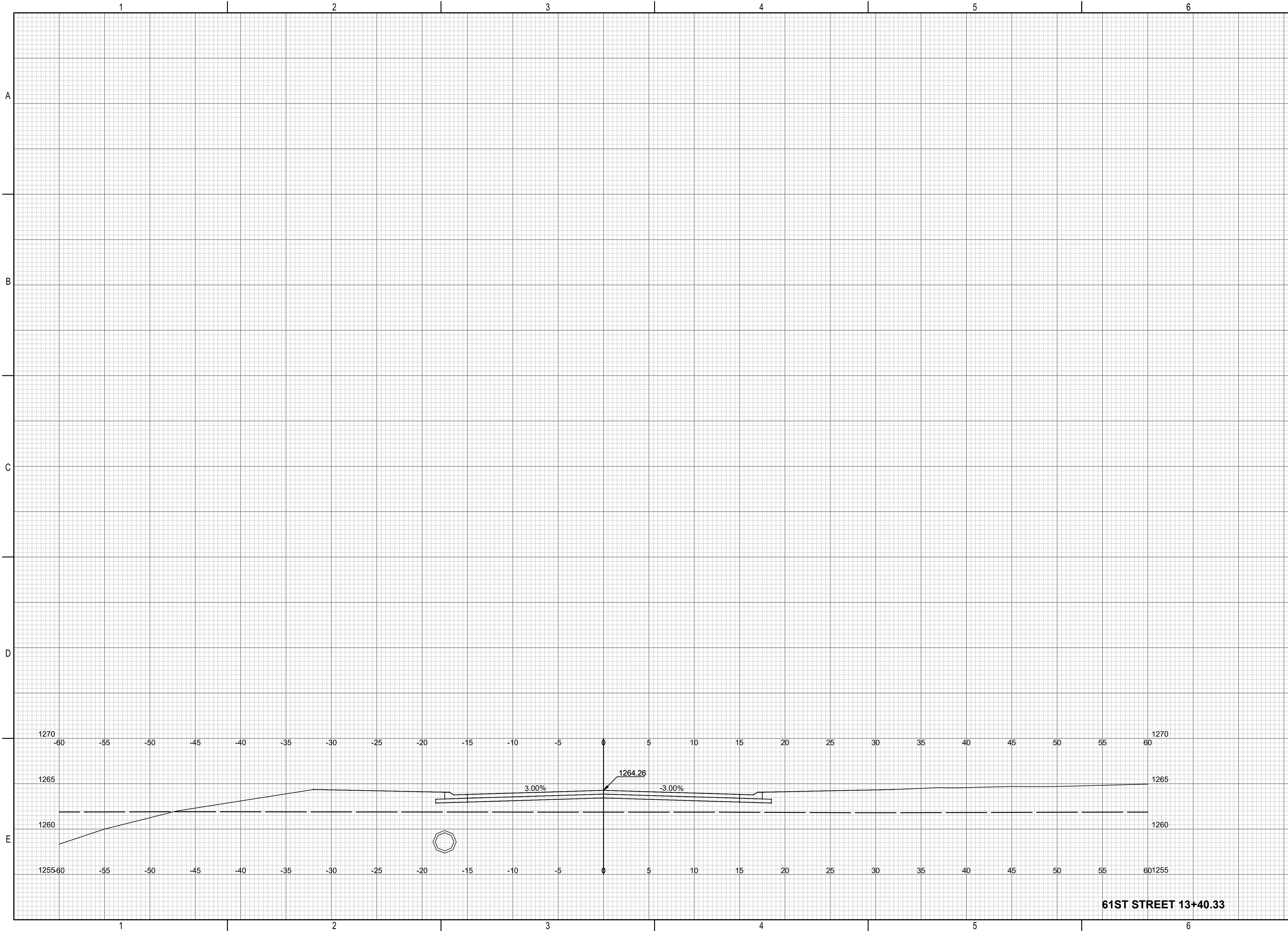
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 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-61ST ST SOUTH

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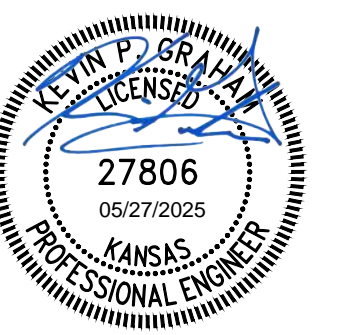
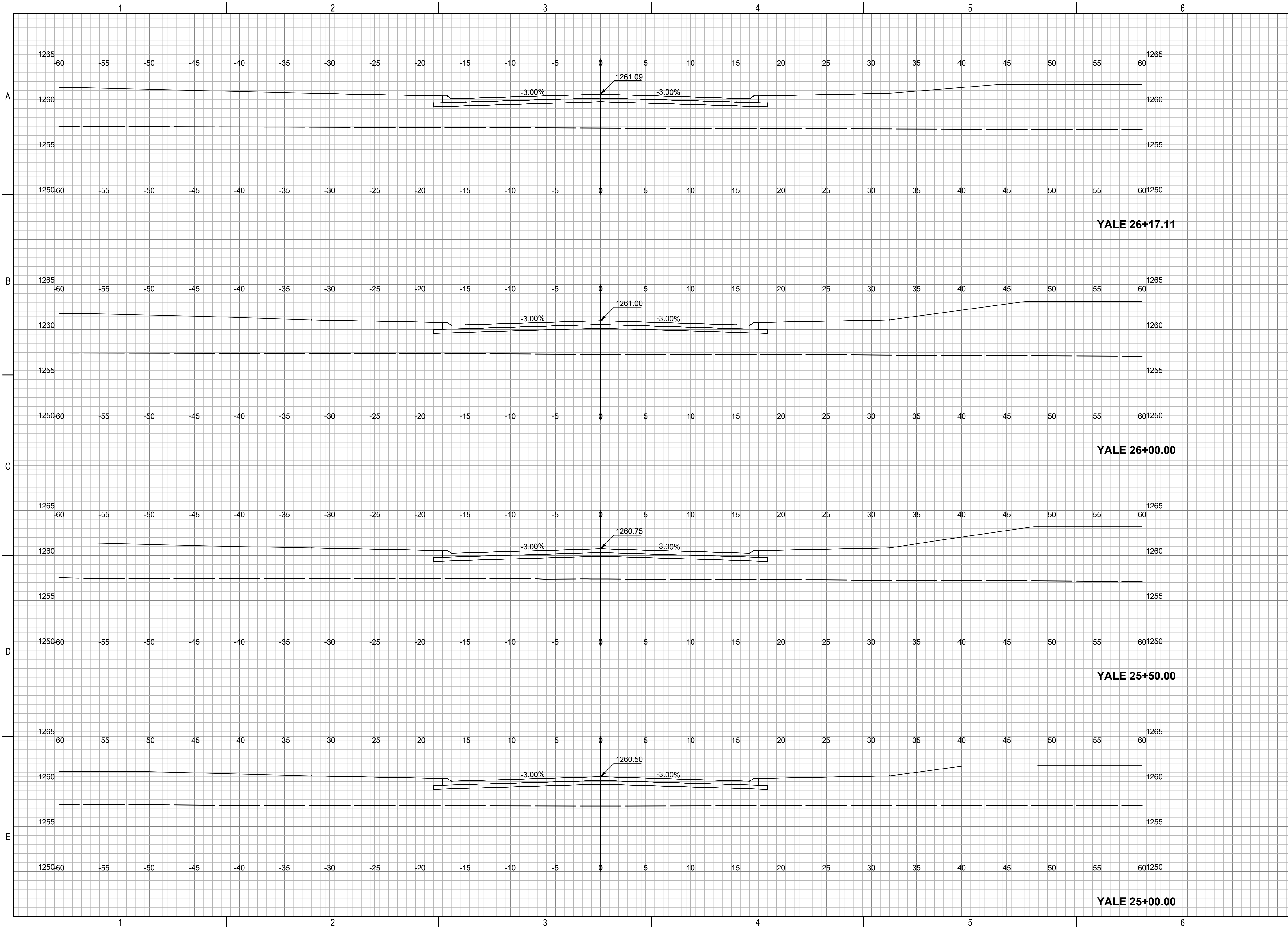
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 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
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CROSS SECTIONS-61ST ST SOUTH

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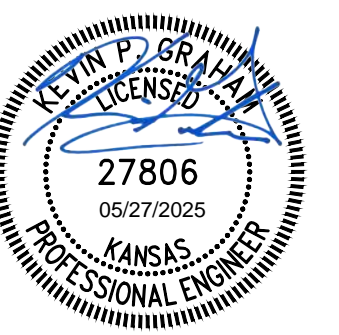
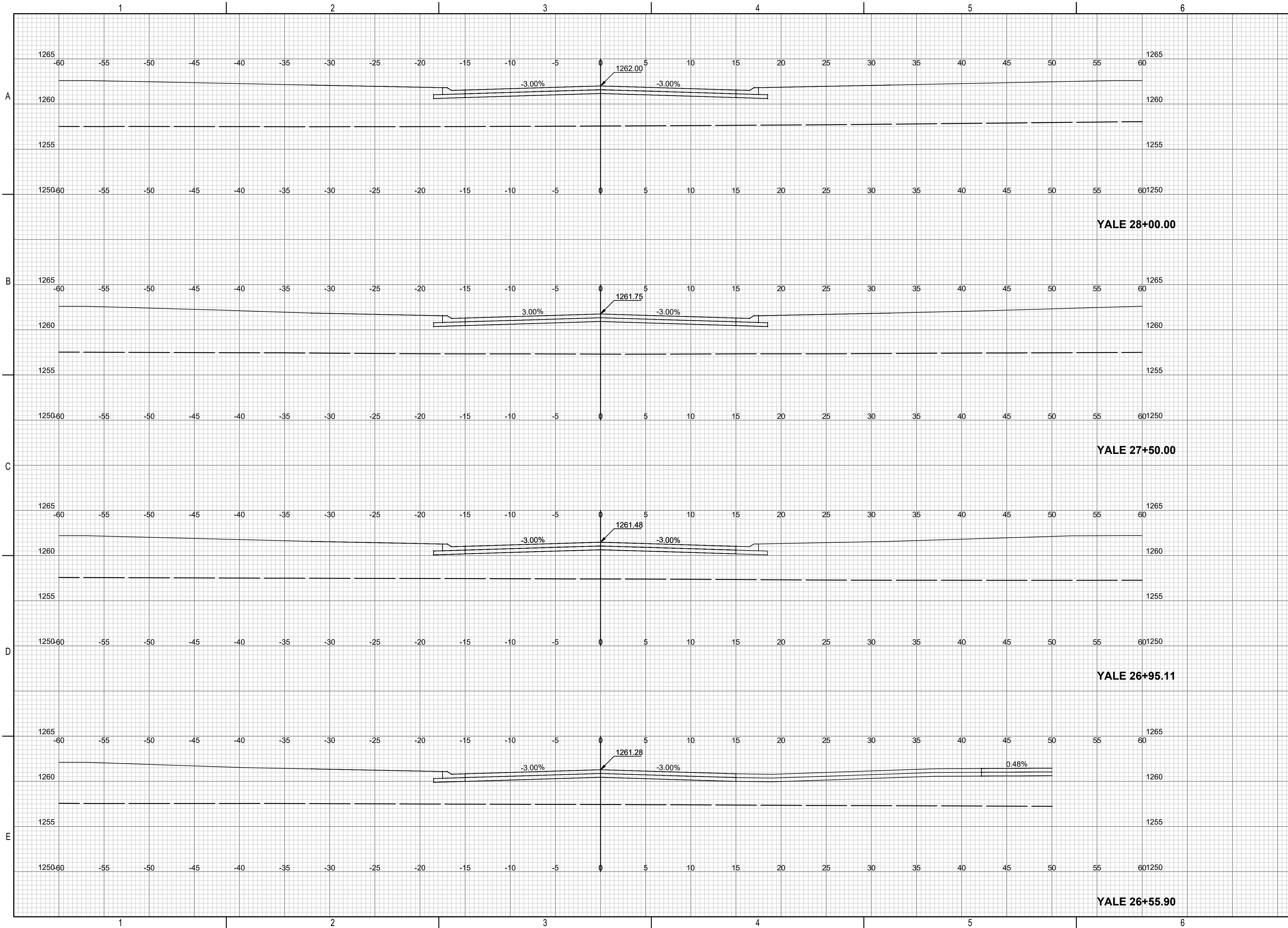
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 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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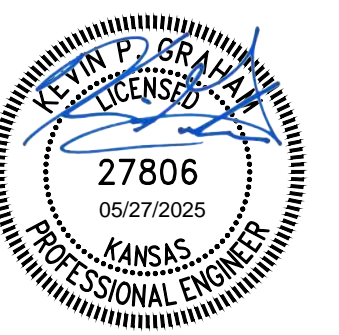
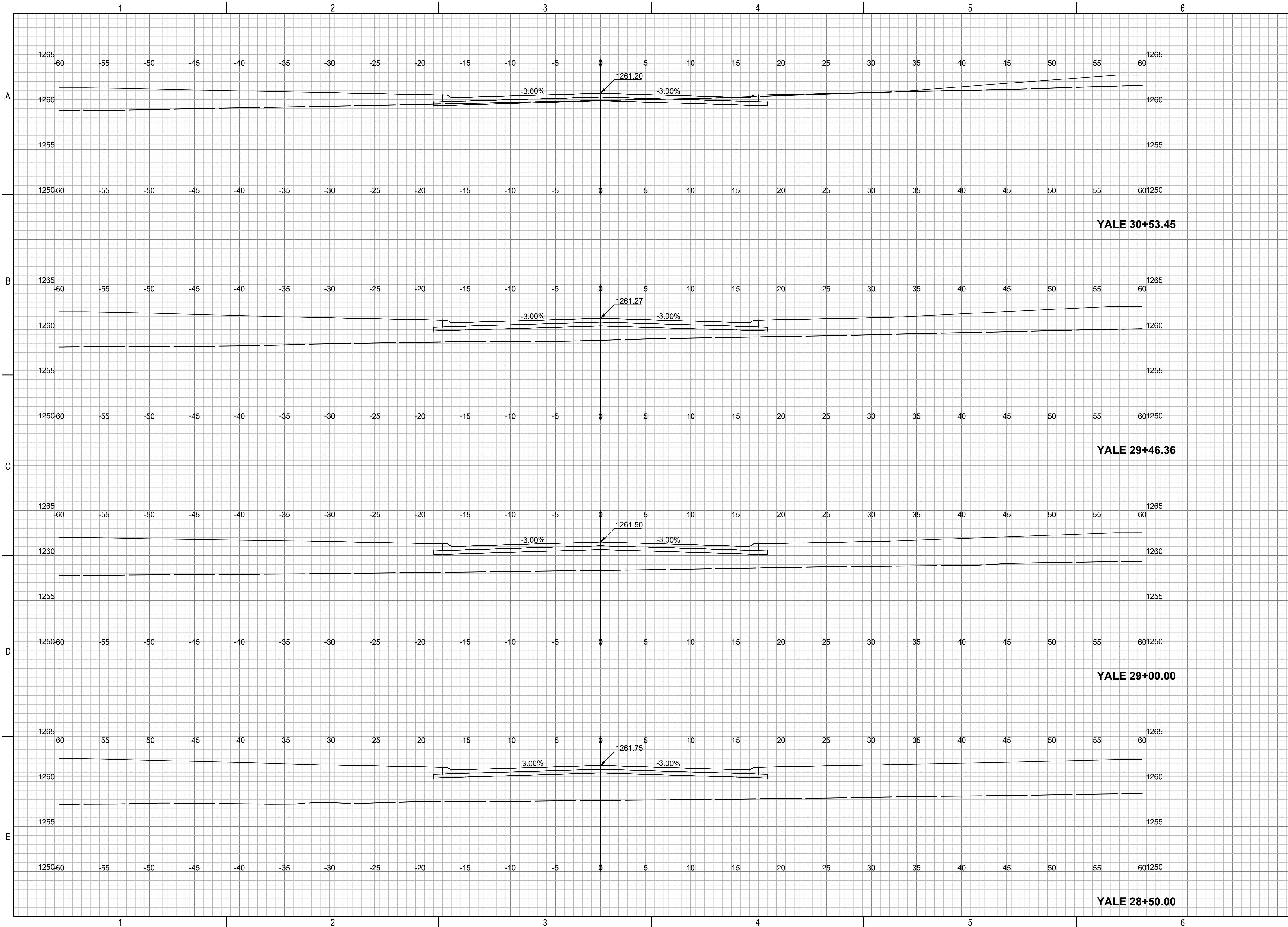
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 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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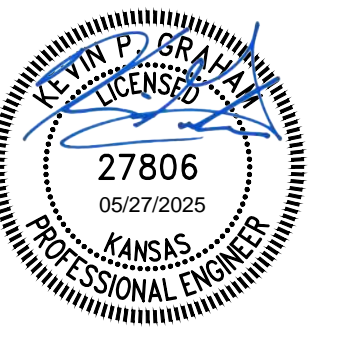
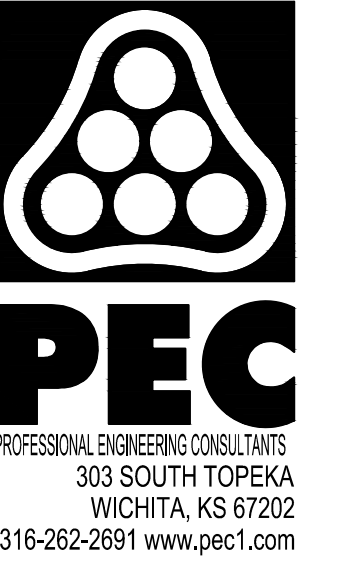
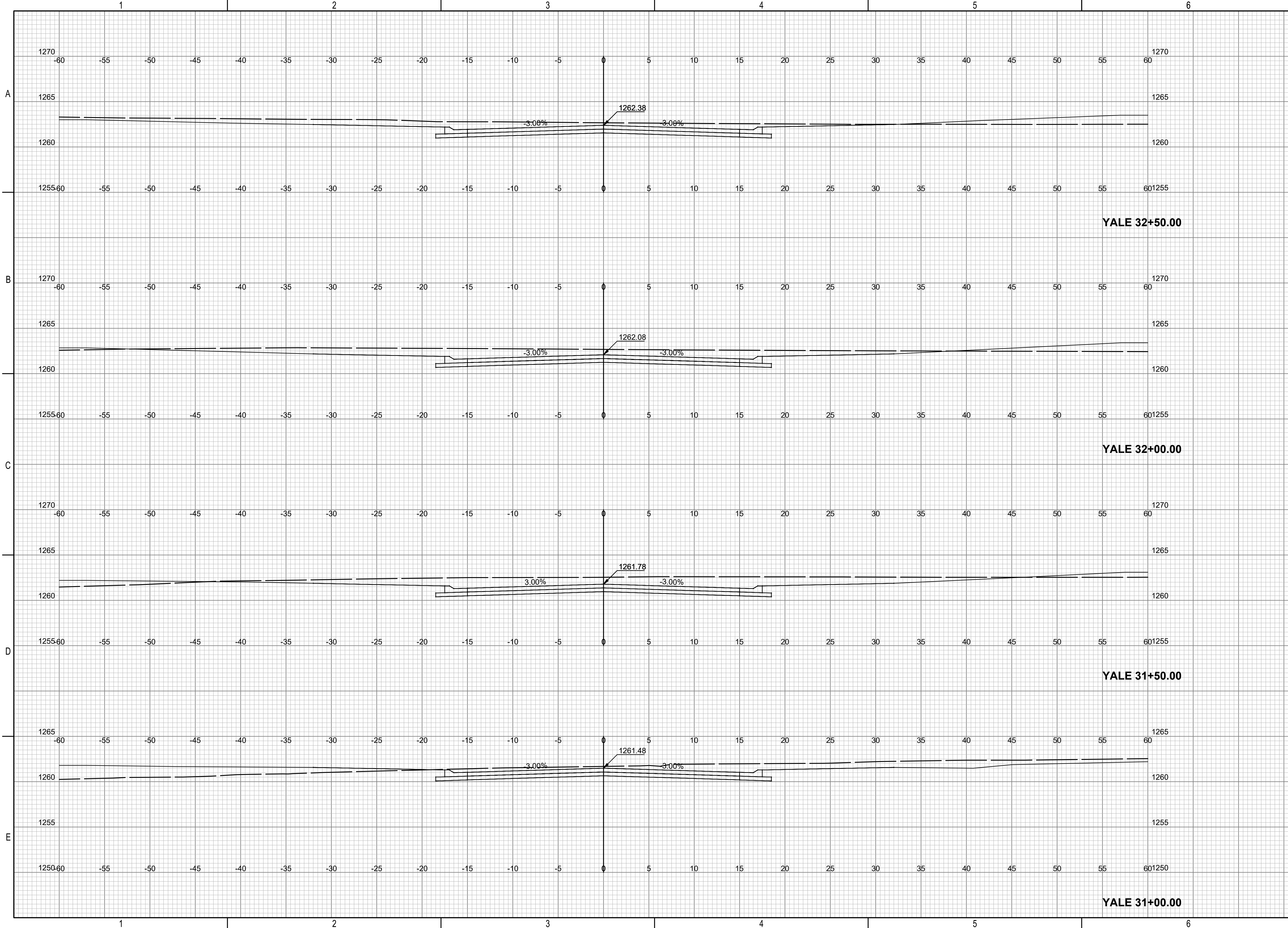
PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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PAYING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS

SWANEY FARM ADDITION
 PHASE 1

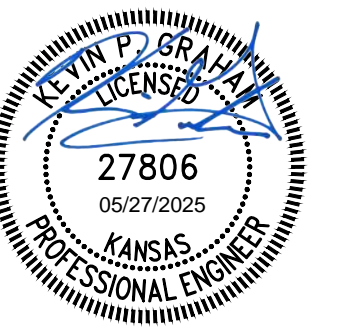
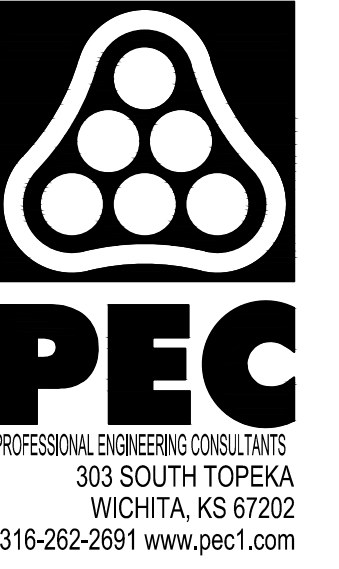
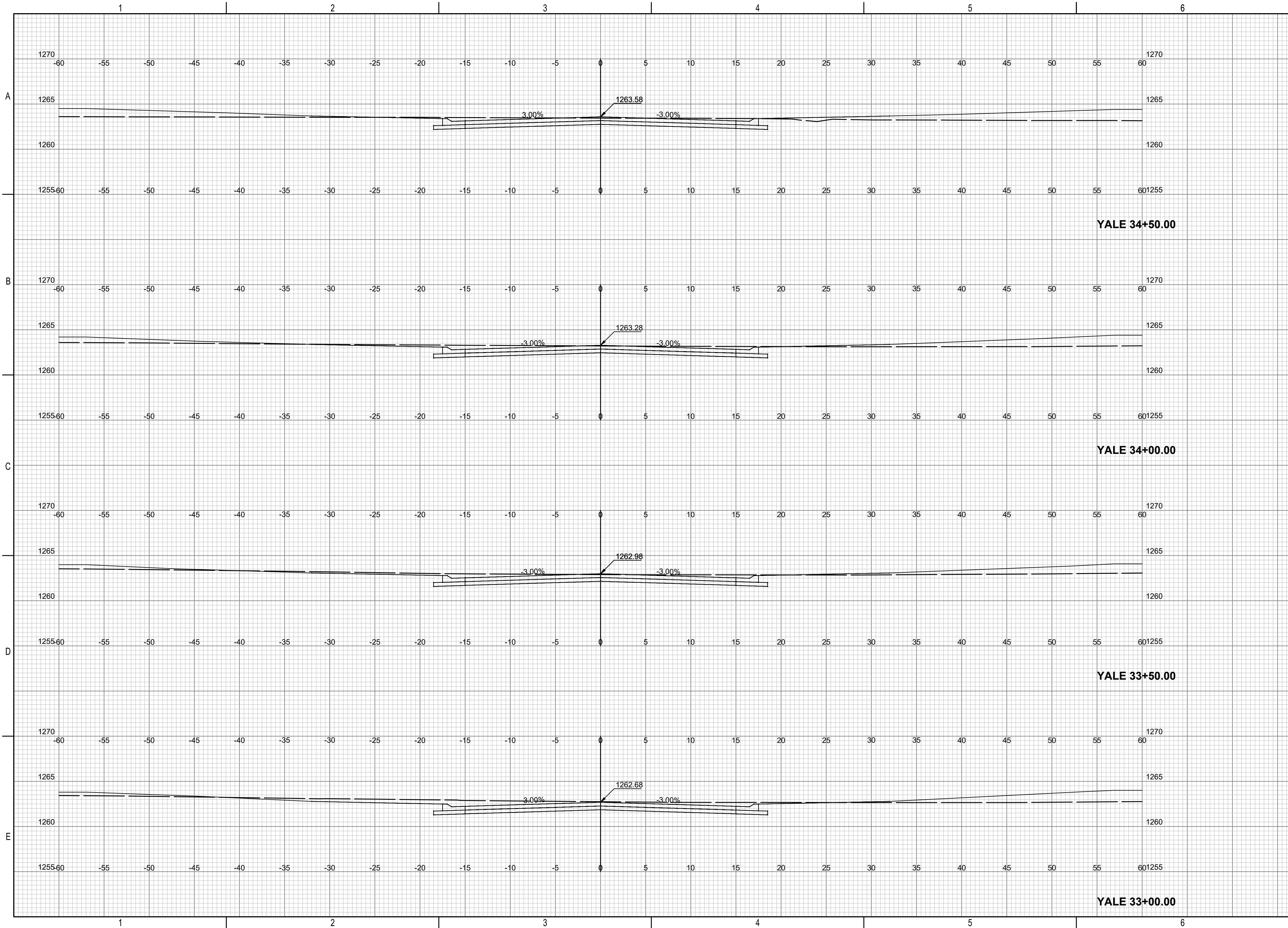
PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

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DATE	MAY 2025
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DESIGNED BY	KPG
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CROSS SECTIONS-YALE

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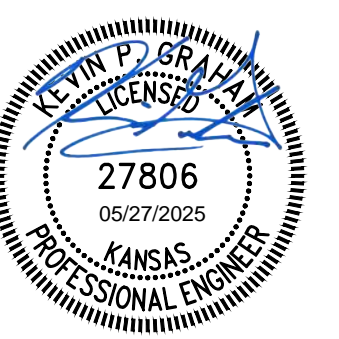
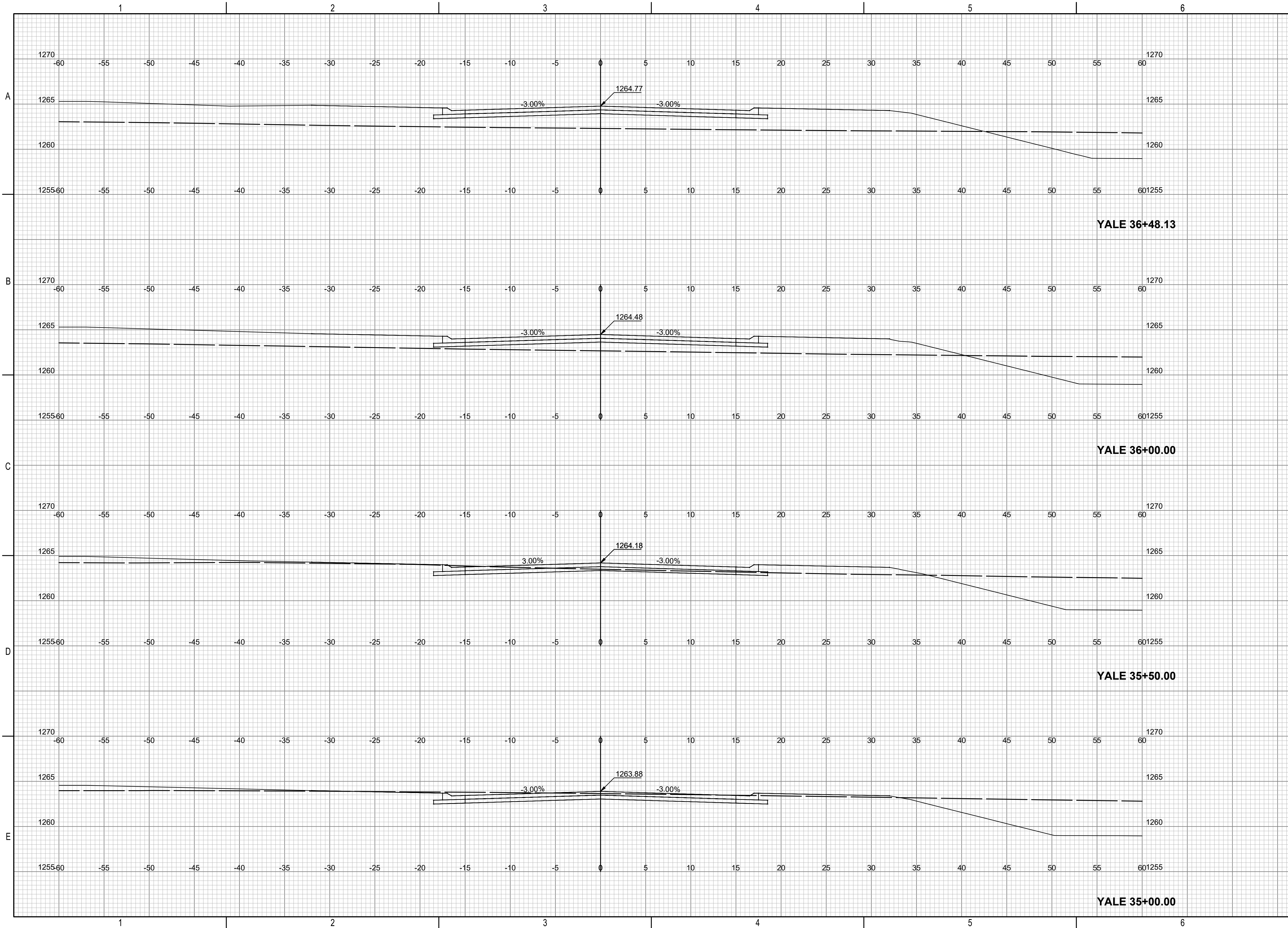
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
 SWANEY FARM ADDITION
 PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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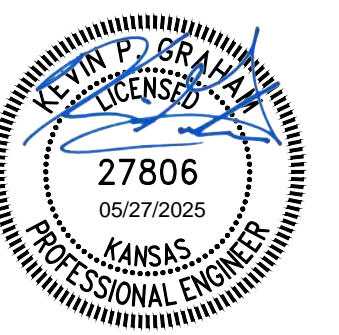
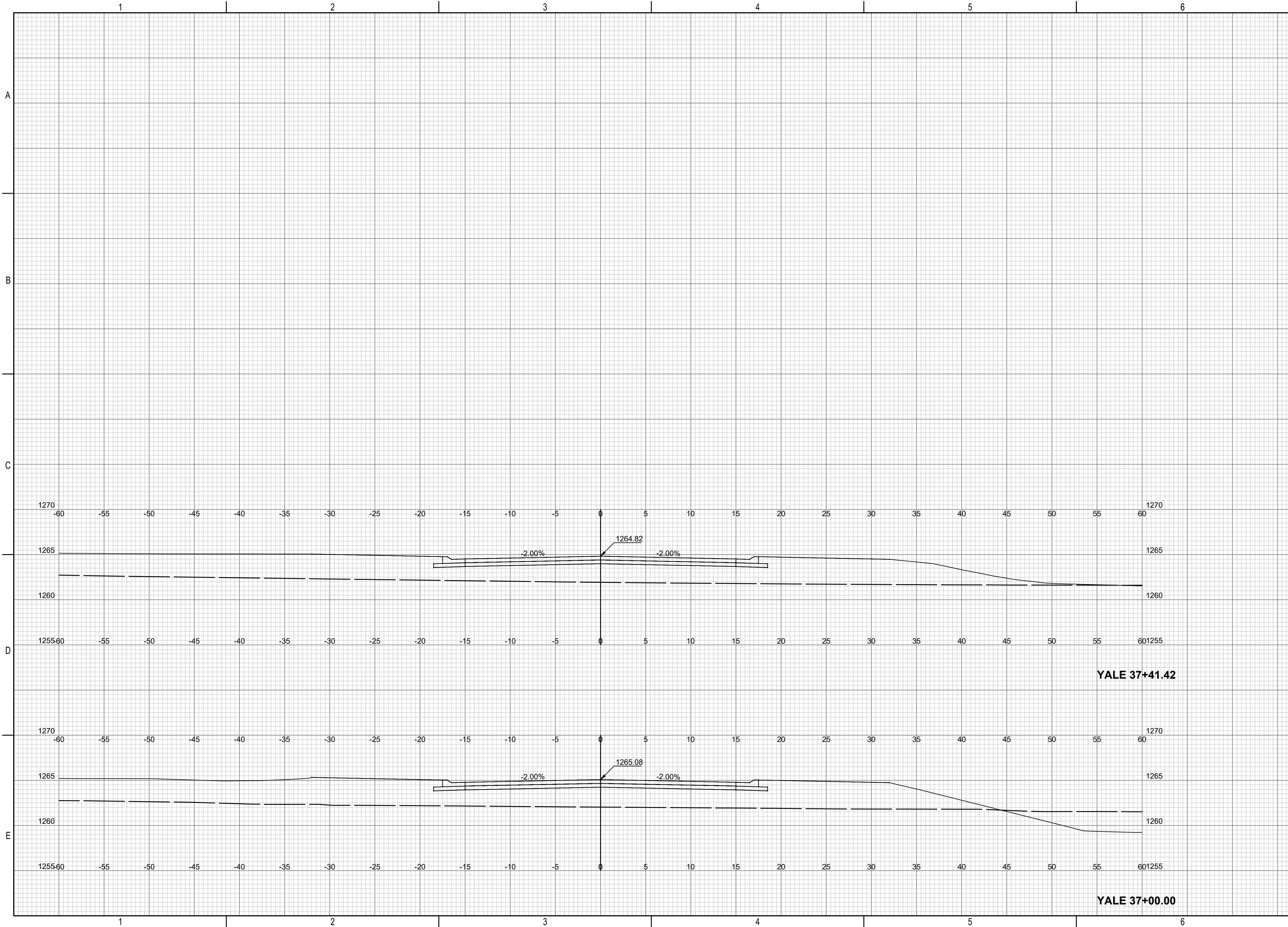
PAYING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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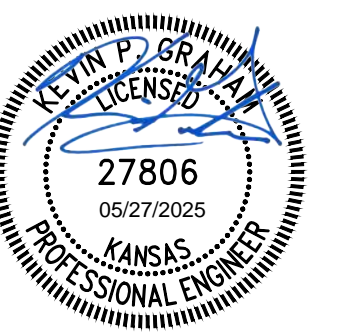
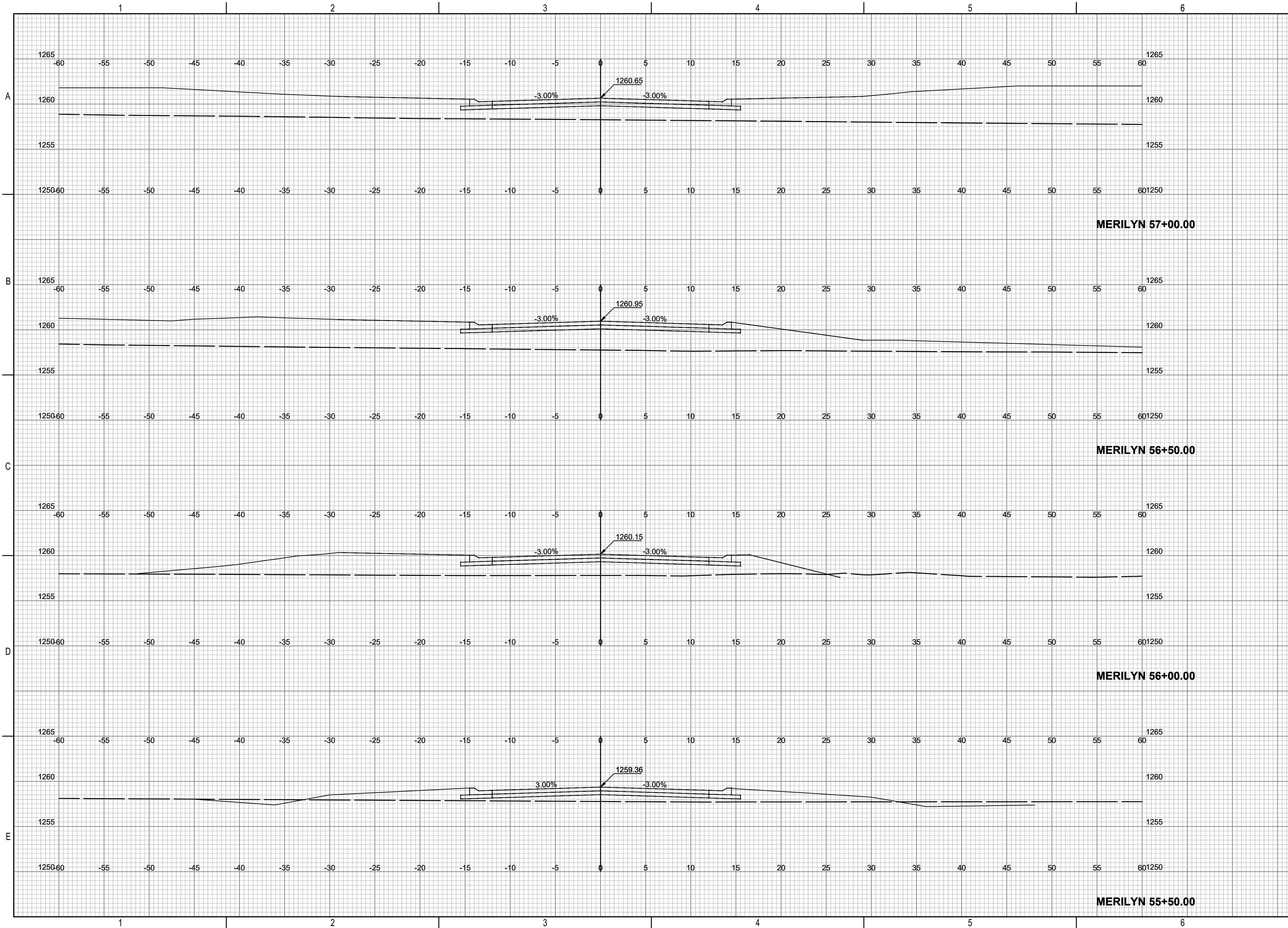
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-YALE

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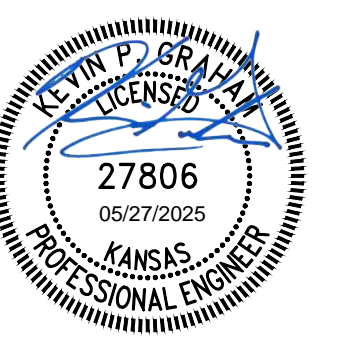
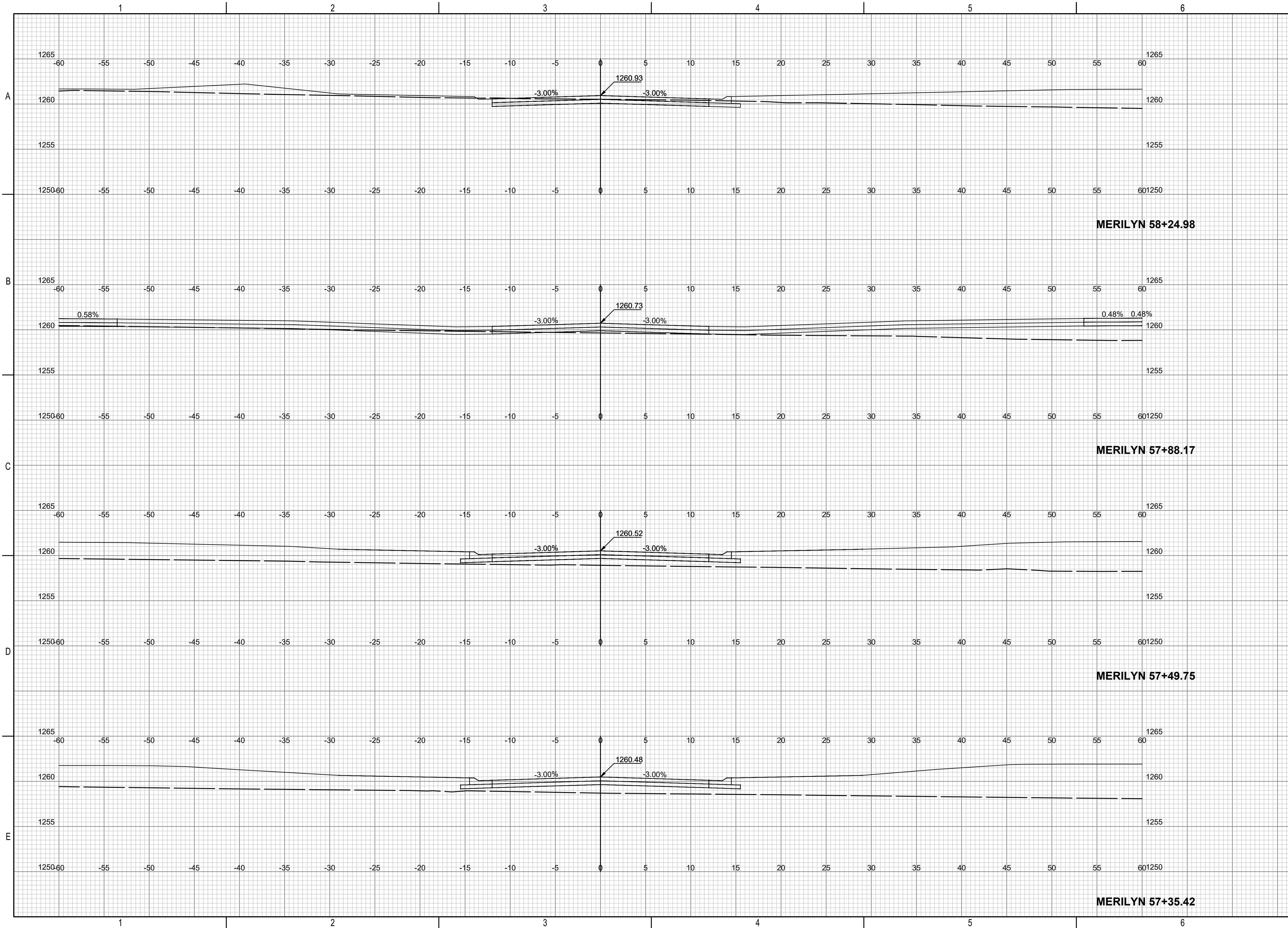
PAYING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

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DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

SAVED 8/1/2024 11:09:15 AM BY BILL_SEXSON
 PLOTTED 5/27/2025 9:51:17 AM BY KEVIN GRAHAM
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 SECTIONS-MERILYN.DWG



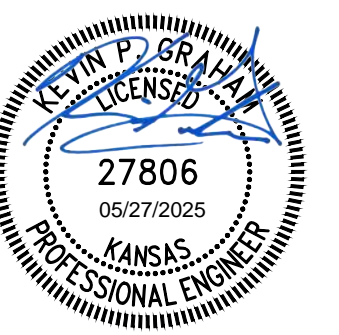
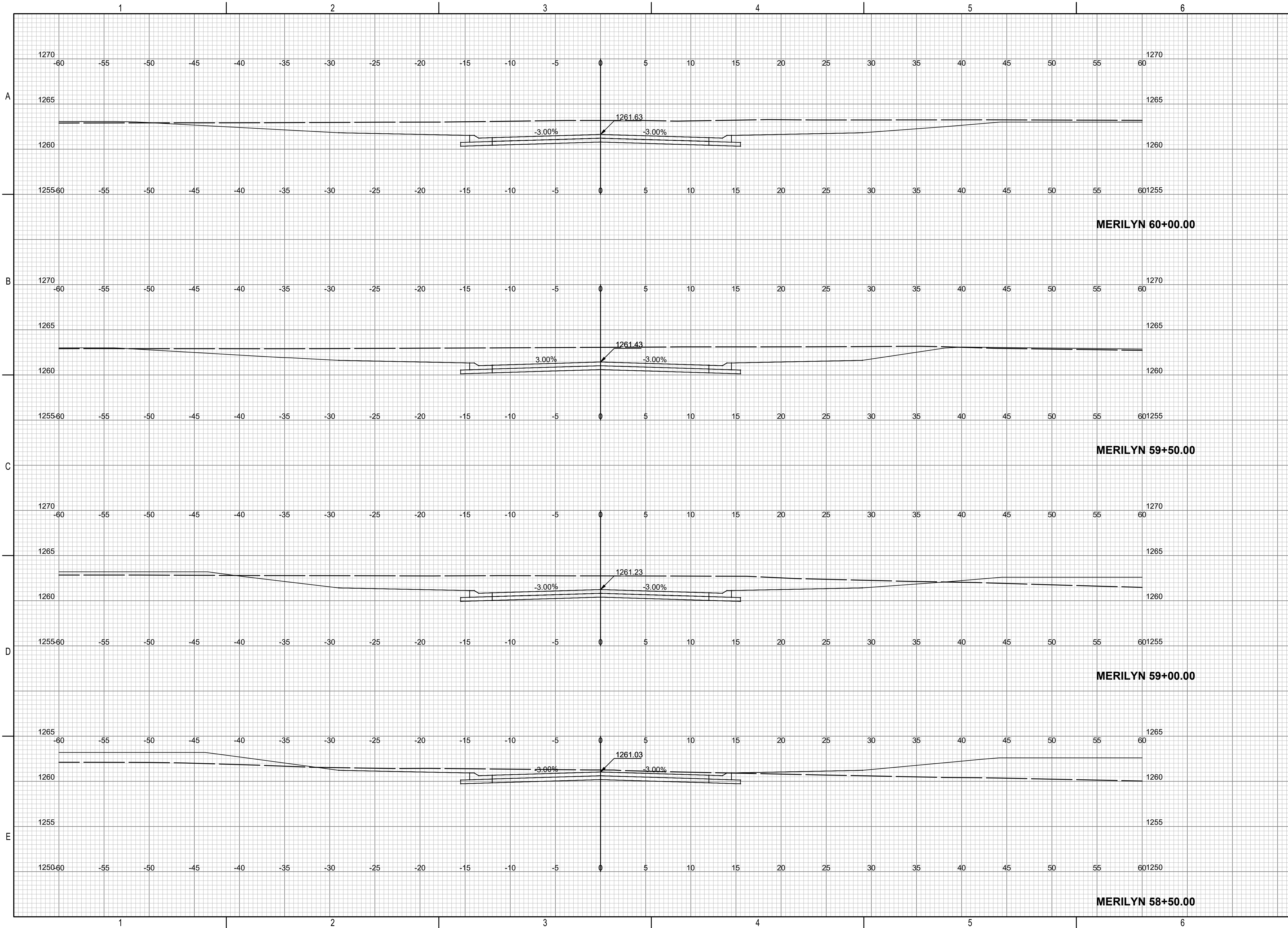
PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

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 PLOTTED 5/27/2025 9:51:35 AM BY KEVIN GRAHAM
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 SECTIONS-MERILYN.DWG



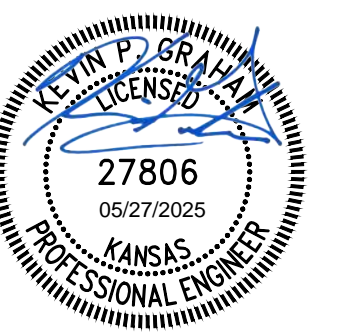
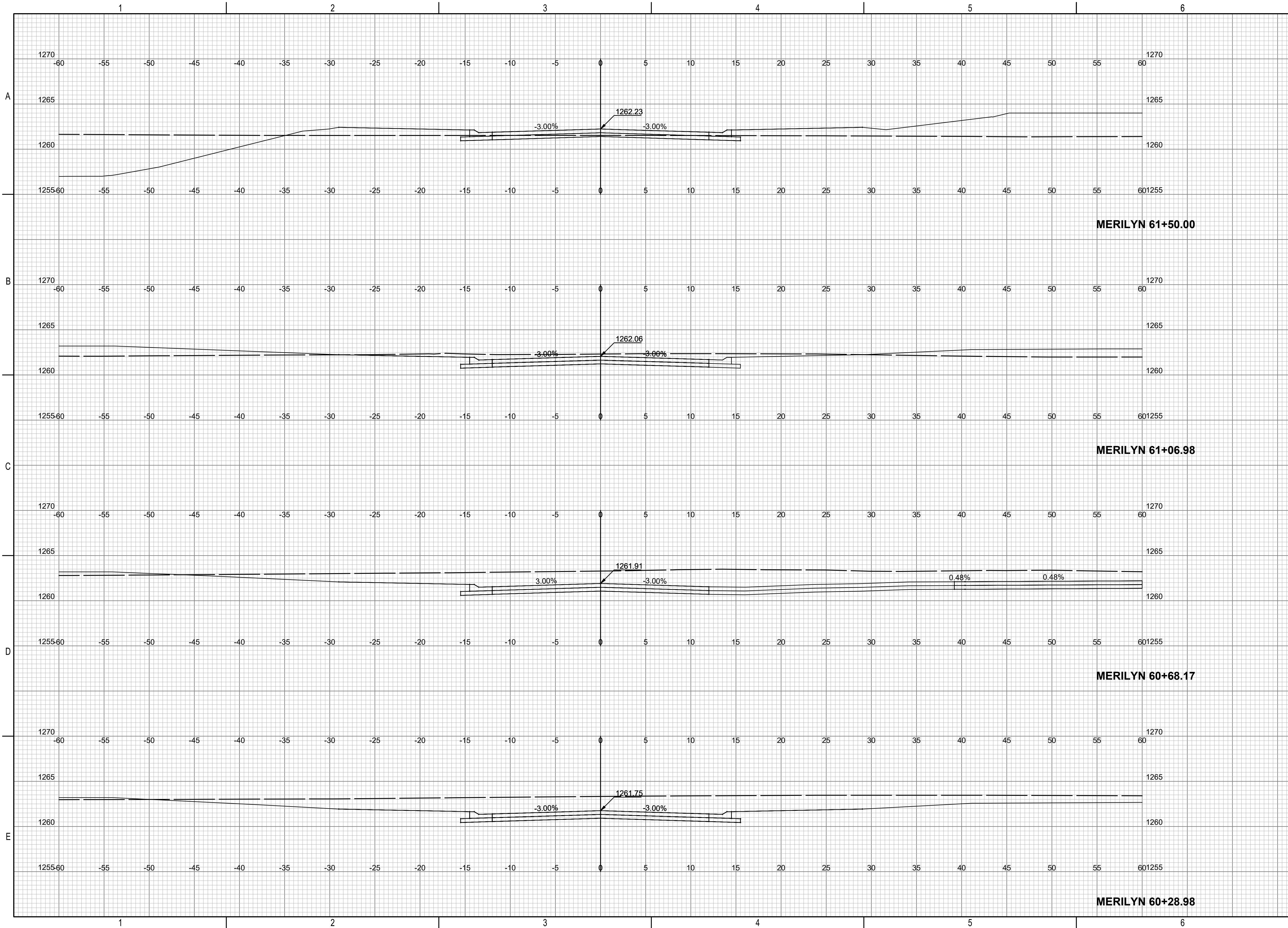
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

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 SECTIONS-MERILYN.DWG



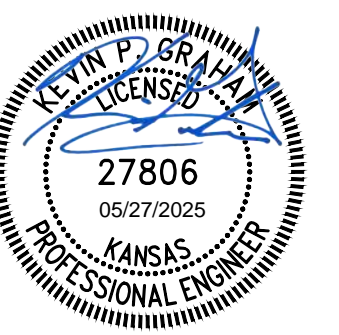
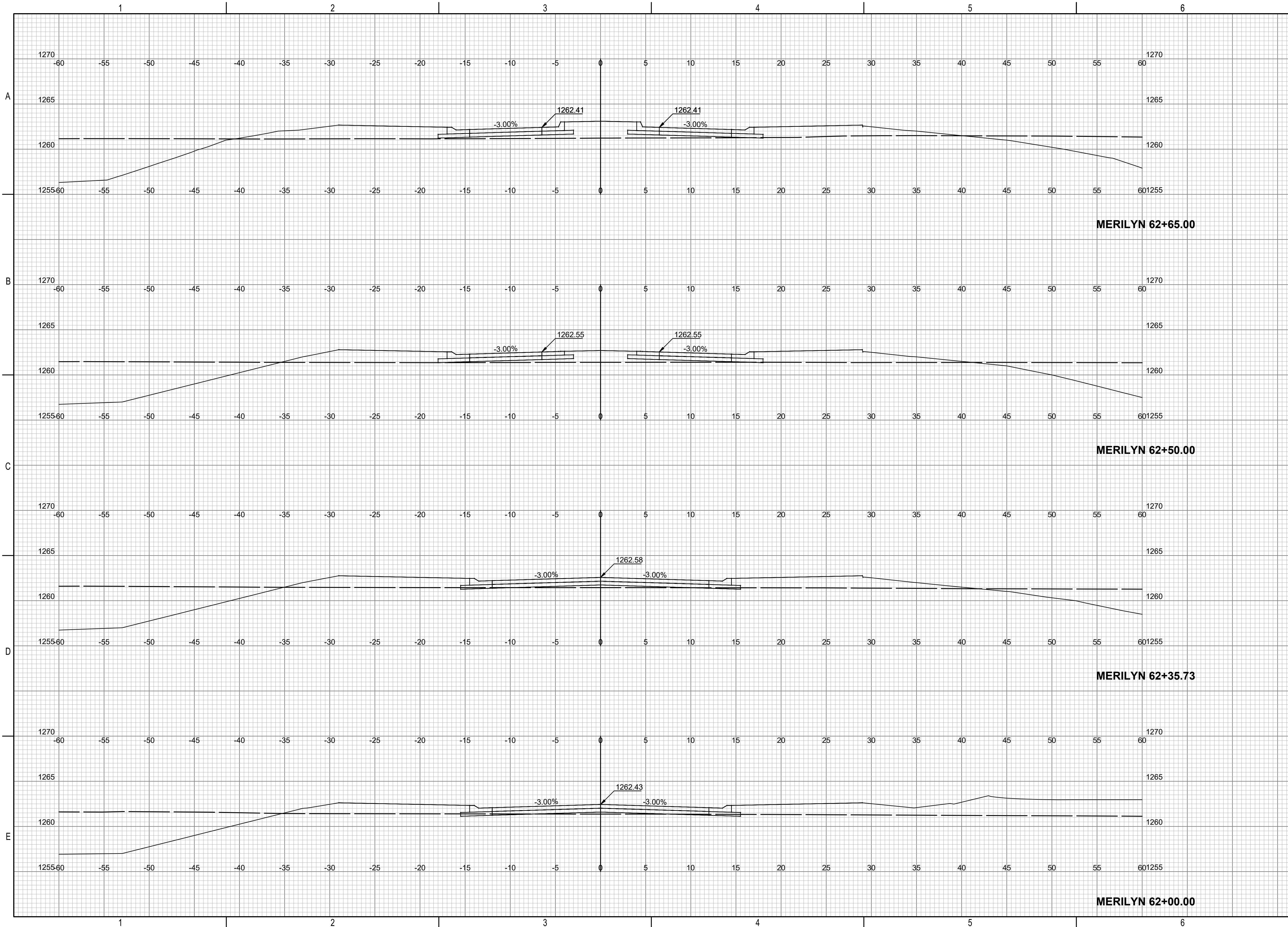
PAYING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

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 SECTIONS-MERILYN.DWG



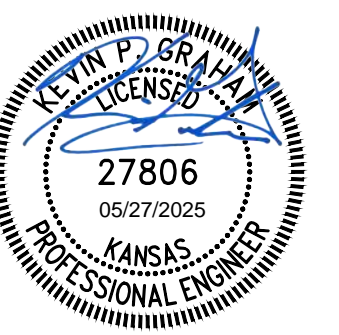
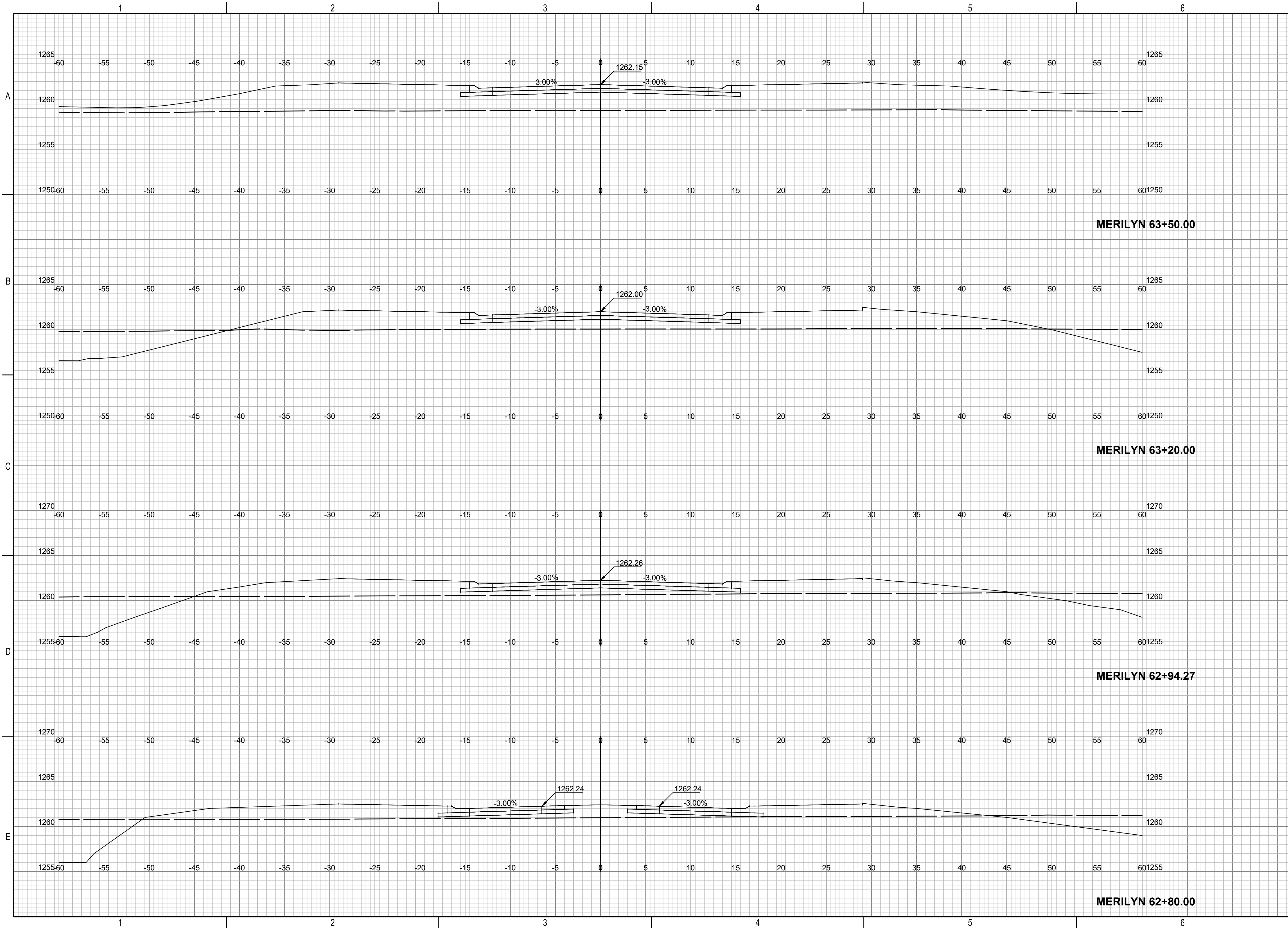
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

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 SECTIONS-MERILYN.DWG



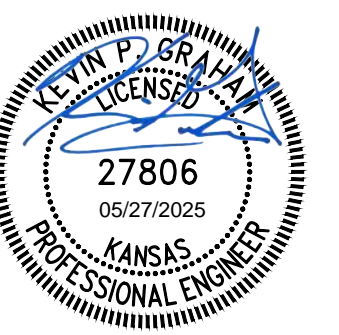
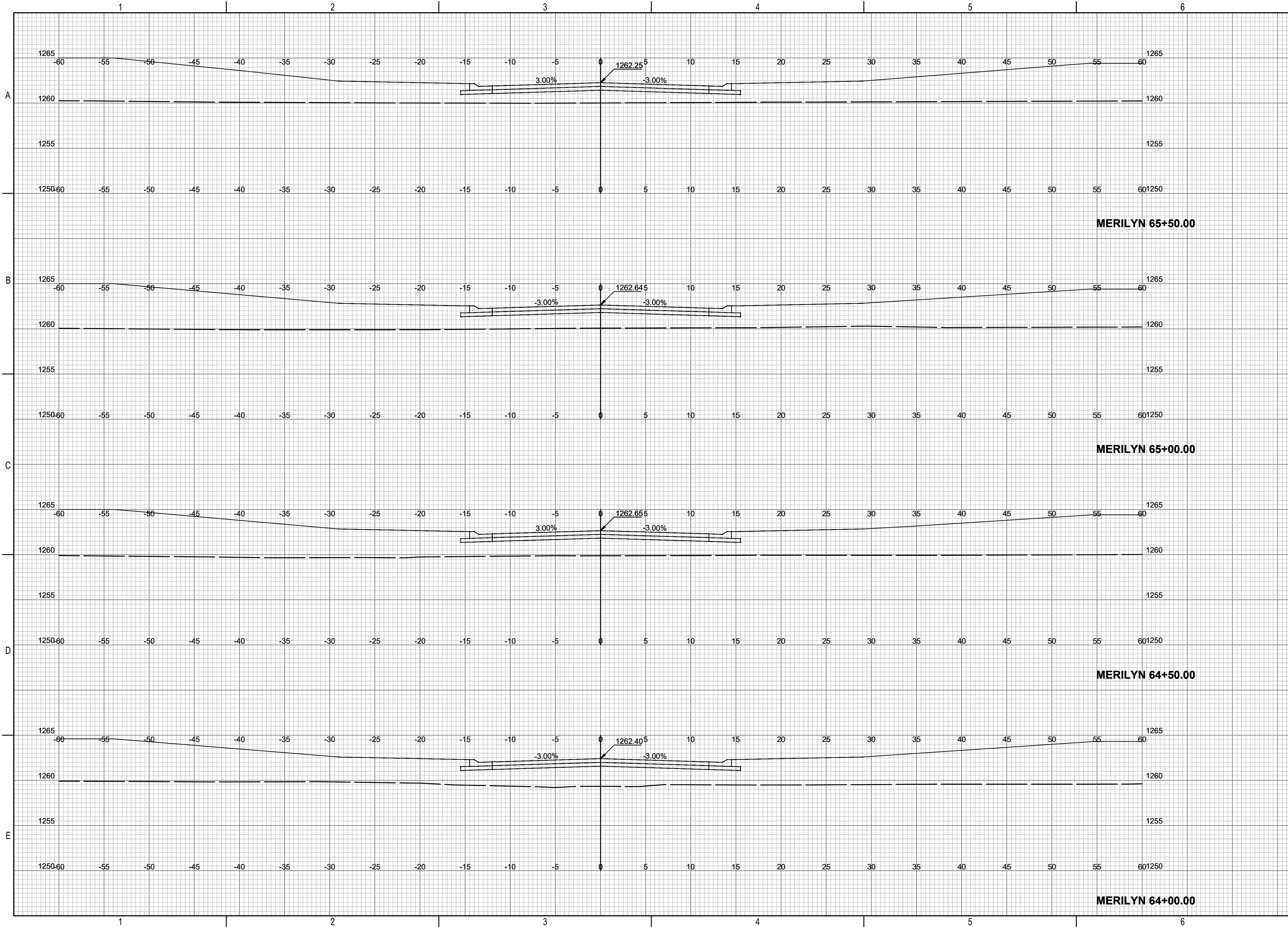
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

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 SECTIONS-MERILYN.DWG



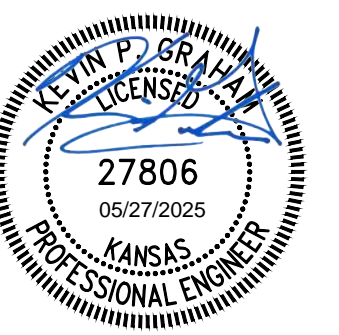
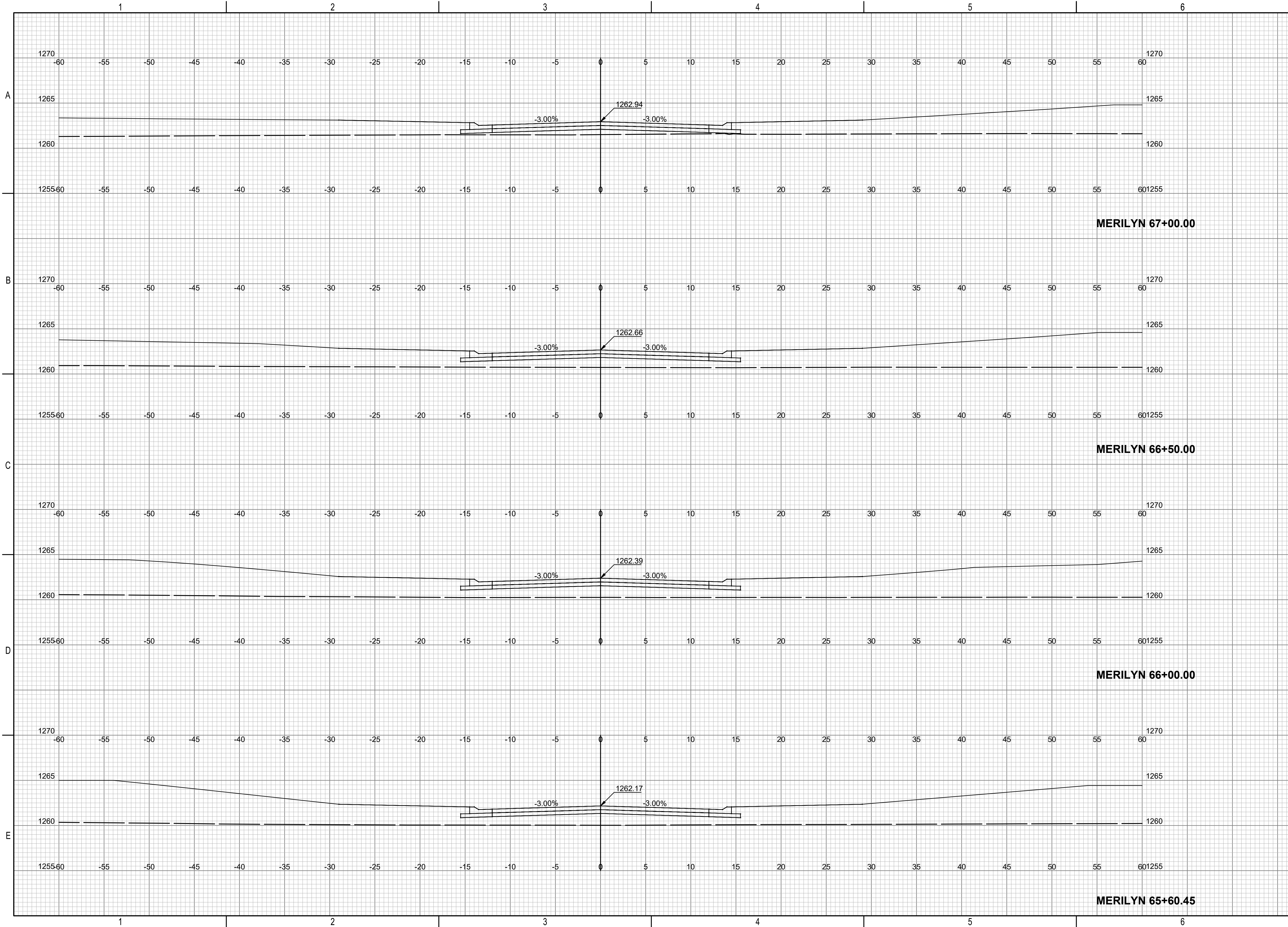
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
 SWANEY FARM ADDITION
 PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

SAVED 8/1/2024 11:10:57 AM BY BILL_SEXSON
 PLOTTED 5/27/2025 9:53:57 AM BY KEVIN GRAHAM
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 SECTIONS-MERILYN.DWG



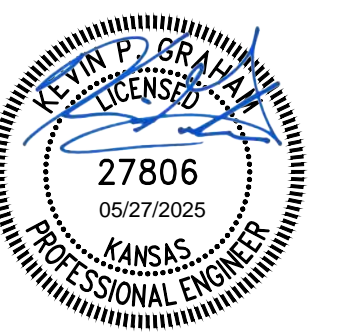
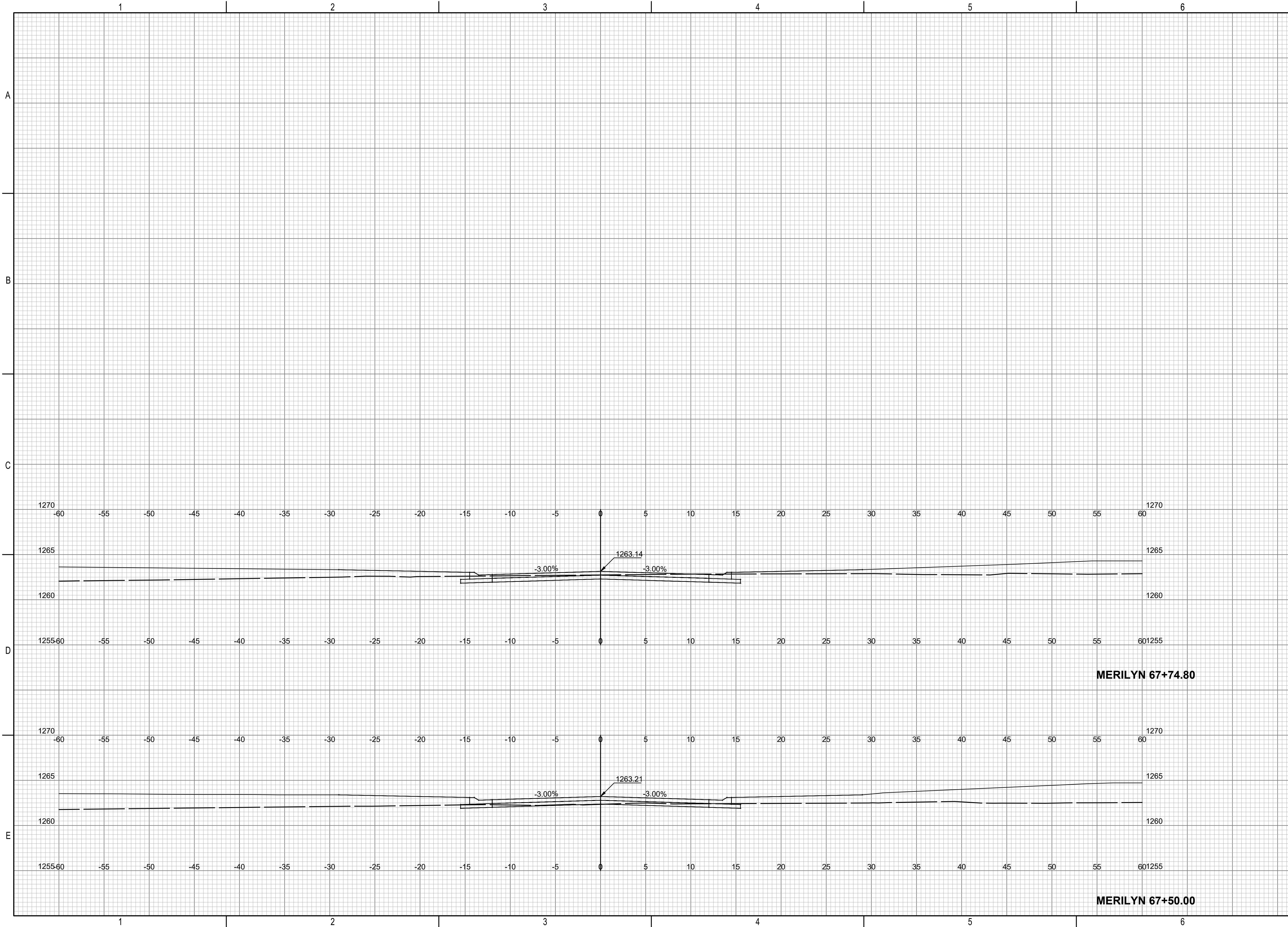
PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

SAVED 8/1/2024 11:11:00 AM BY BILL_SEXSON
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 SECTIONS-MERILYN.DWG



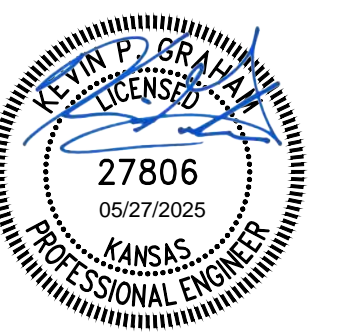
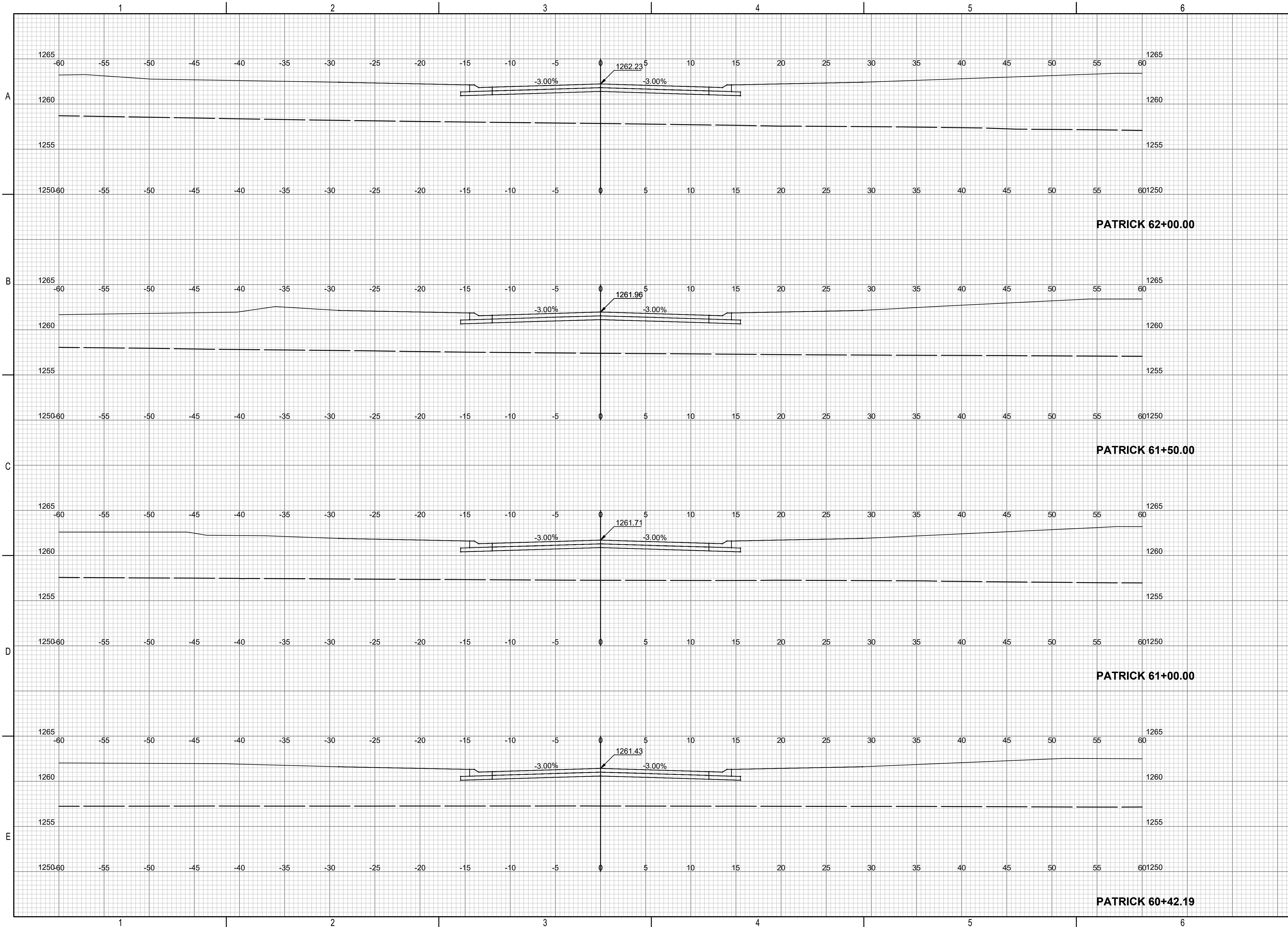
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
**SWANEY FARM ADDITION
 PHASE 1**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-MERILYN

SAVED 8/1/2024 11:11:10 AM BY BILL_SEXSON
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 SECTIONS-PATRICK.DWG



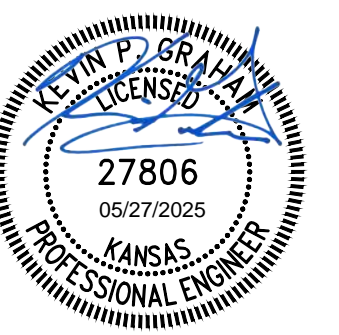
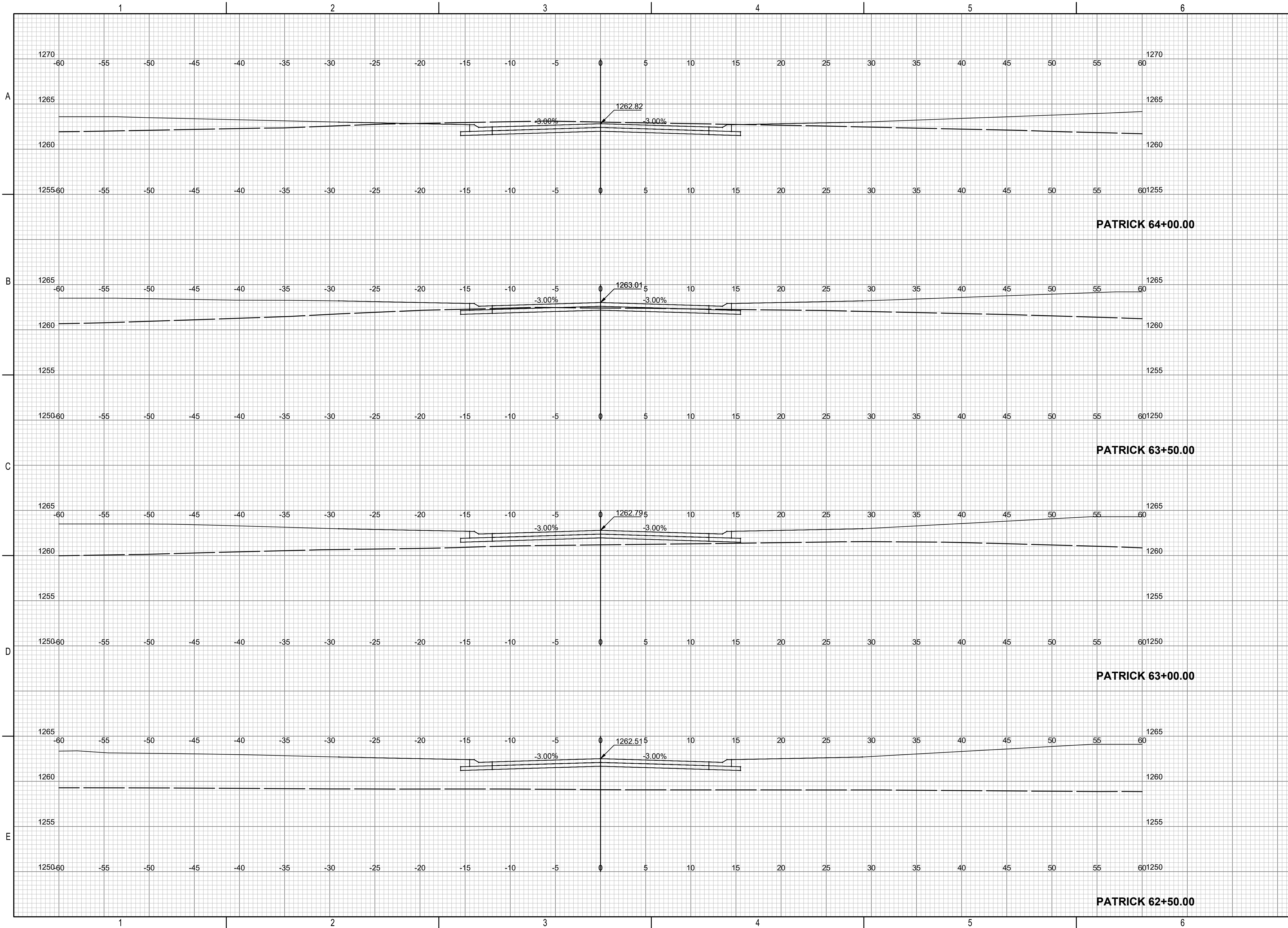
PAYING AND INCIDENTAL DRAINAGE IMPROVEMENTS
SWANEY FARM ADDITION PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:	

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-PATRICK

SAVED 8/1/2024 11:11:26 AM BY BILL_SEXSON
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 SECTIONS-PATRICK.DWG



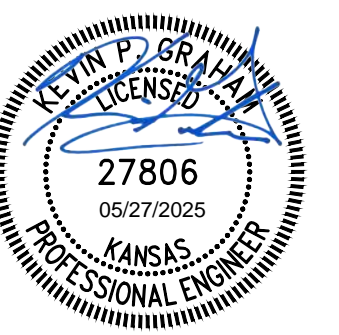
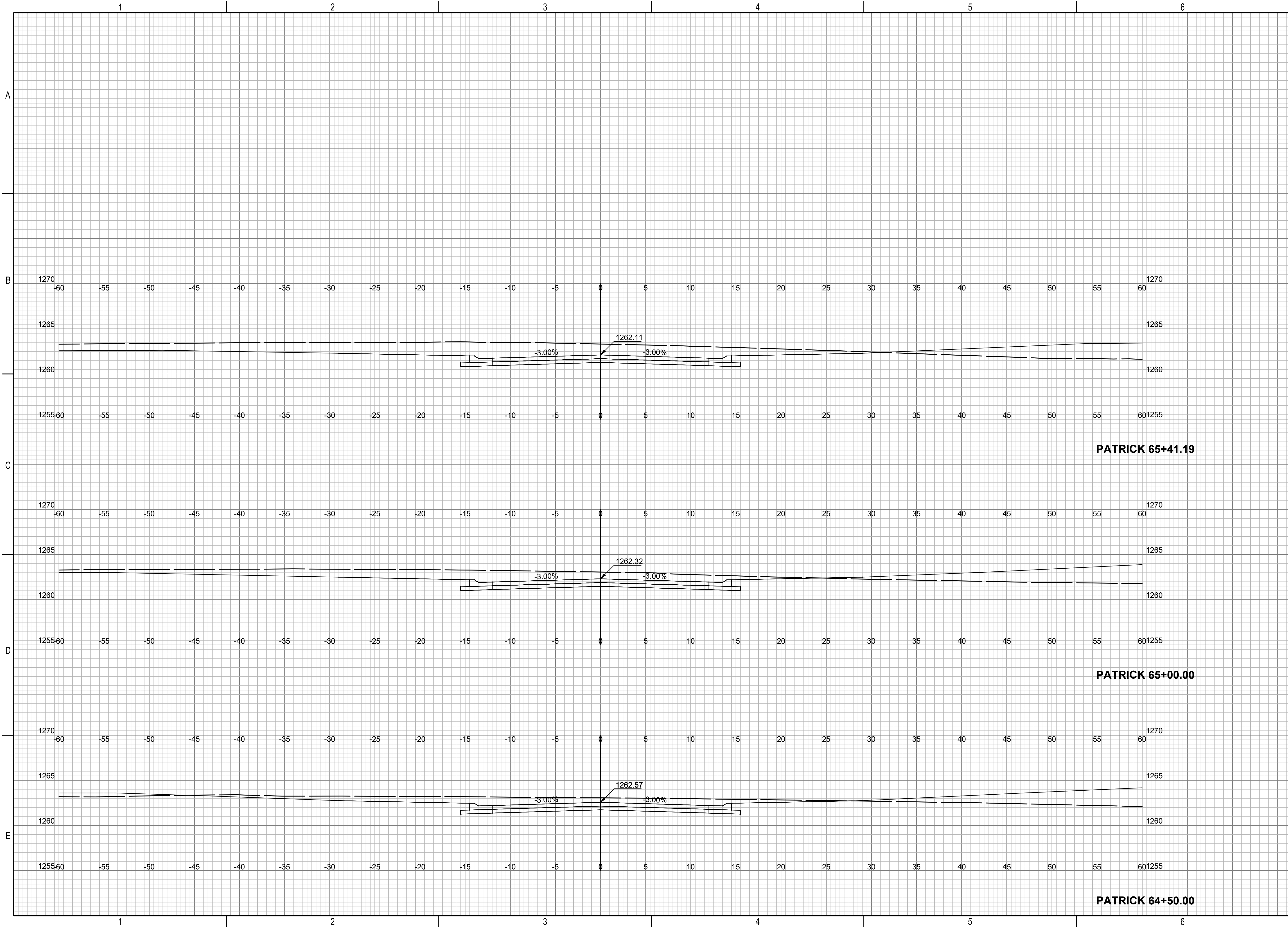
PAVING AND INCIDENTAL DRAINAGE
 IMPROVEMENTS
 SWANEY FARM ADDITION
 PHASE 1
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-PATRICK

SAVED 8/1/2024 11:16:36 AM BY BILL_SEXSON
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 SECTIONS-PATRICK.DWG



PAVING AND INCIDENTAL DRAINAGE IMPROVEMENTS

SWANEY FARM ADDITION PHASE 1

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-086026

Issue:		

JOB NO.	200605-004
DATE	MAY 2025
PM	KPG
DESIGNED BY	KPG
DRAWN BY	BJS
CHECKED BY	KMS

CROSS SECTIONS-PATRICK

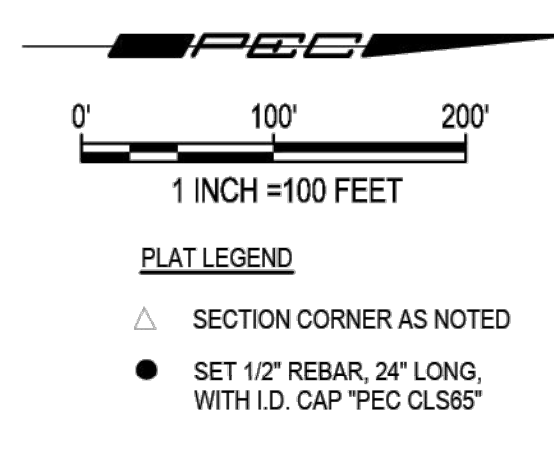
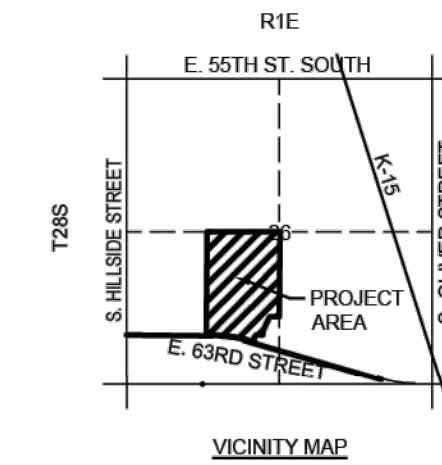
SWANEY FARM ADDITION

CITY OF WICHITA, SEDGWICK COUNTY, KANSAS

FINAL PLAT

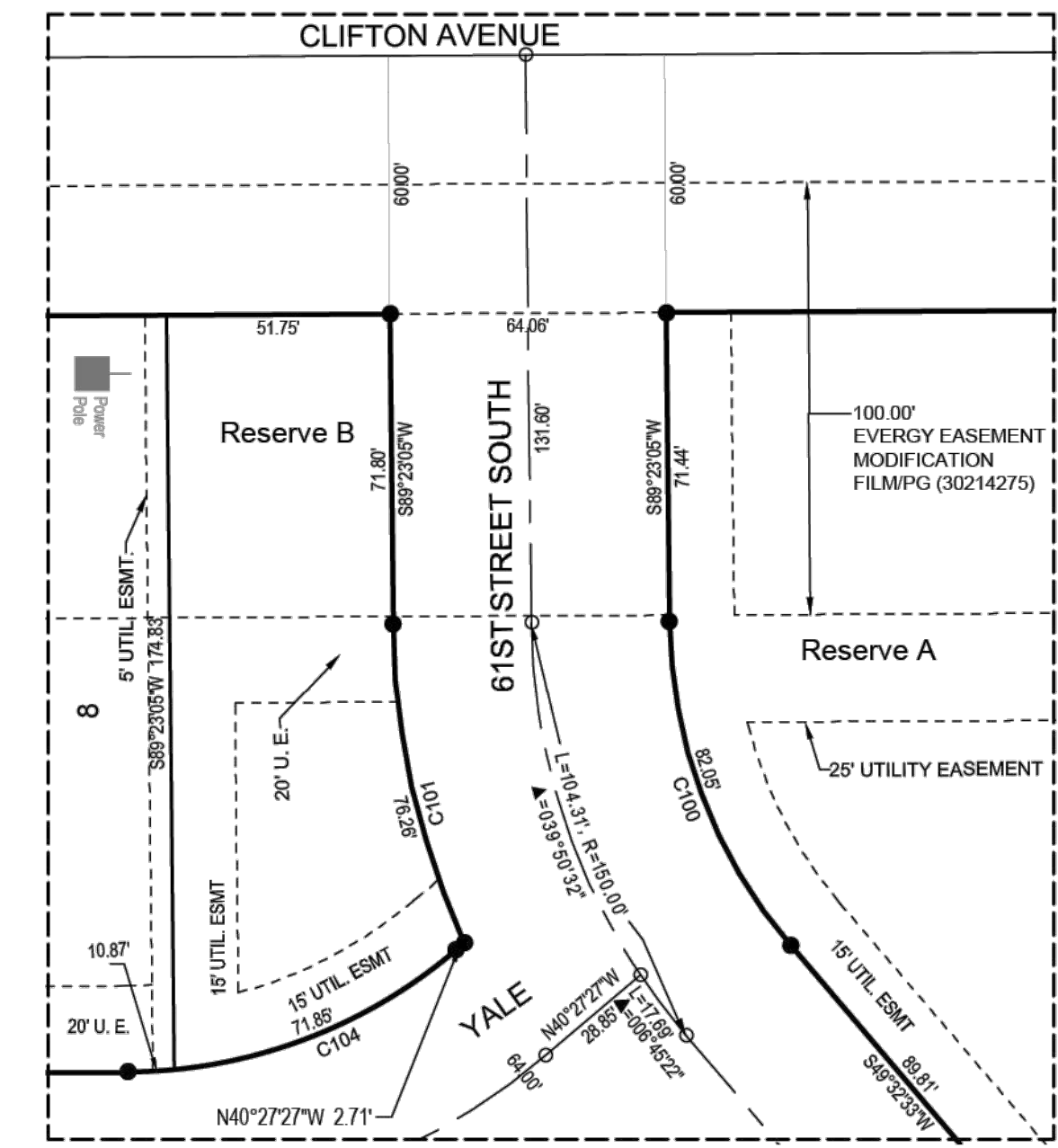
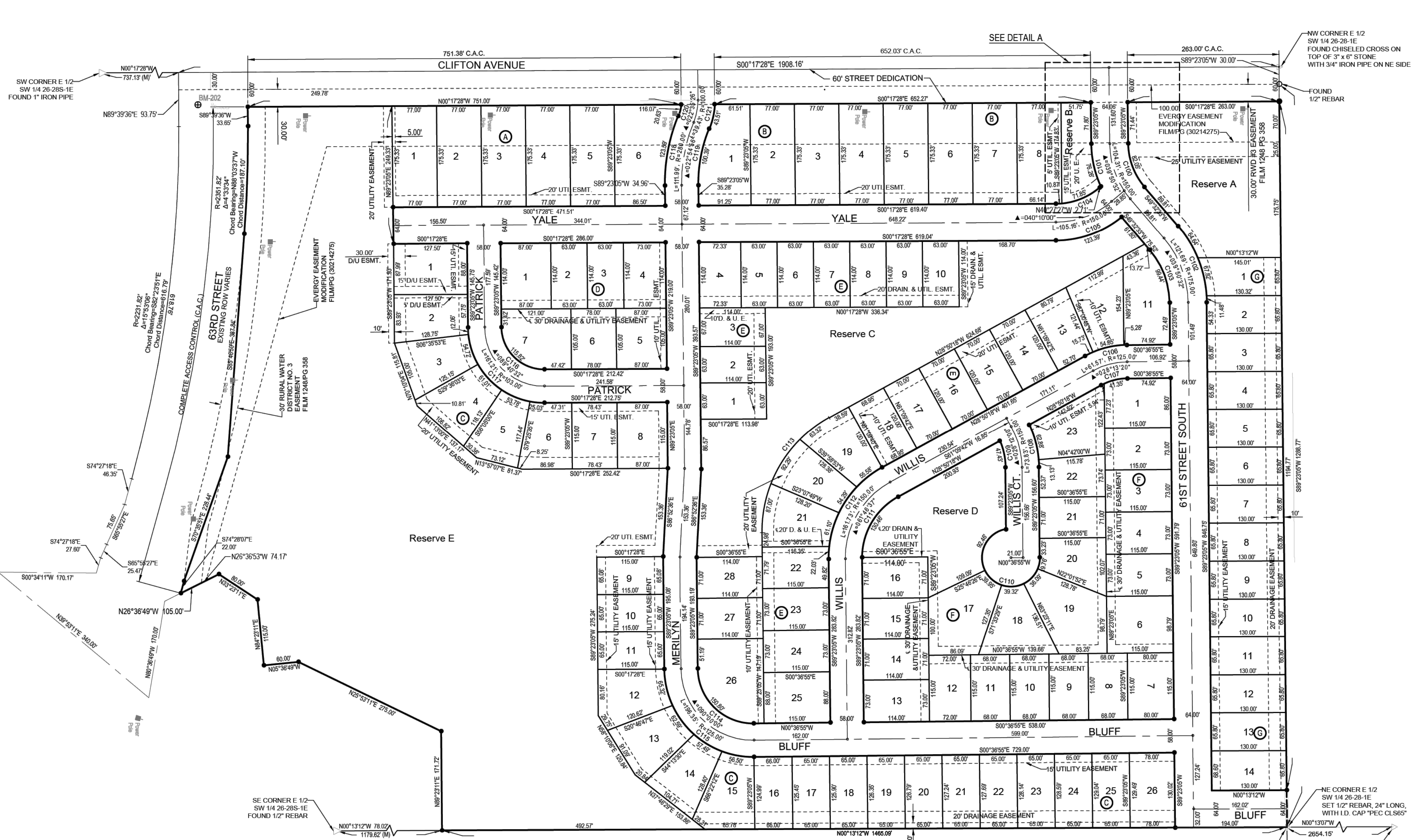
MINIMUM PAD TABLE

LOCATION	MINIMUM OPENING (BFE + 2)
BLOCK A, LOTS 1-6	1261.6
BLOCK B, LOTS 1-3	1261.6
BLOCK C, LOTS 1-4	1261.6
BLOCK C, LOTS 5-7	1262.8
BLOCK C, LOTS 9-11	1263.5
BLOCK C, LOTS 12-14	1263.8
BLOCK D, LOTS 1-4, 7	1261.6
BLOCK E, LOTS 11-12	1264.9
BLOCK E, LOTS 13-14	1264.7
BLOCK E, LOT 15	1264.4
BLOCK E, LOT 16	1264.4
BLOCK E, LOT 17	1263.8
BLOCK E, LOT 18	1263.8
BLOCK E, LOT 19	1263.7
BLOCK E, LOT 20	1263.6
BLOCK E, LOTS 21-23	1263.5
BLOCK F, LOT 1	1264.9
BLOCK F, LOTS 15-16	1264.7
BLOCK F, LOTS 17-18	1264.8
BLOCK G, LOTS 1-3	1265.0



BENCH MARK

BENCHMARK 202 - CHISELED X ON CURB STORM DRAIN ON THE SE SIDE OF CLIFTON AND 63RD.
 ELEV. = 1259.36 (NAVD88)
 THENCE N00°17'28\"/>



CURVE TABLE

CURVE NO.	RADIUS	DELTA	LENGTH	CHORD LENGTH	CHORD BEARING
C100	118.00	39°50'32"	82.05	80.41'	N69° 27' 48\"/>



SAVER 2/23/2024 2:26:23 PM BY BILL SEKSON
 PLOTTED 2/23/2024 2:26:44 PM BY BILL SEKSON
 U:\WICHITA-CIVIL\2020\0605\01\PECDRAWINGS\PLAT\200605-001 SWANEY FARM ADDITION-2-22-2023.DWG