

1
14

CITY OF WICHITA, KANSAS

MICHAEL E. LINDEBAK, CITY ENGINEER

STREET IMPROVEMENTS

BROADMOOR - N.L. MAGILL TO S.L. POLO
 POSTOAK/MAGILL/DOREEN - S.L. LOT 9, BLK. 2 & S.L. LOT 8, BLK. 8
 TO S.L. LOT 1, BLK. 8 & S.L. LOT 5, BLK. 10

(FAIRFIELD ESTATES)

CITY OF WICHITA PROJECT NO. 472-76-245-81527-000-000-001

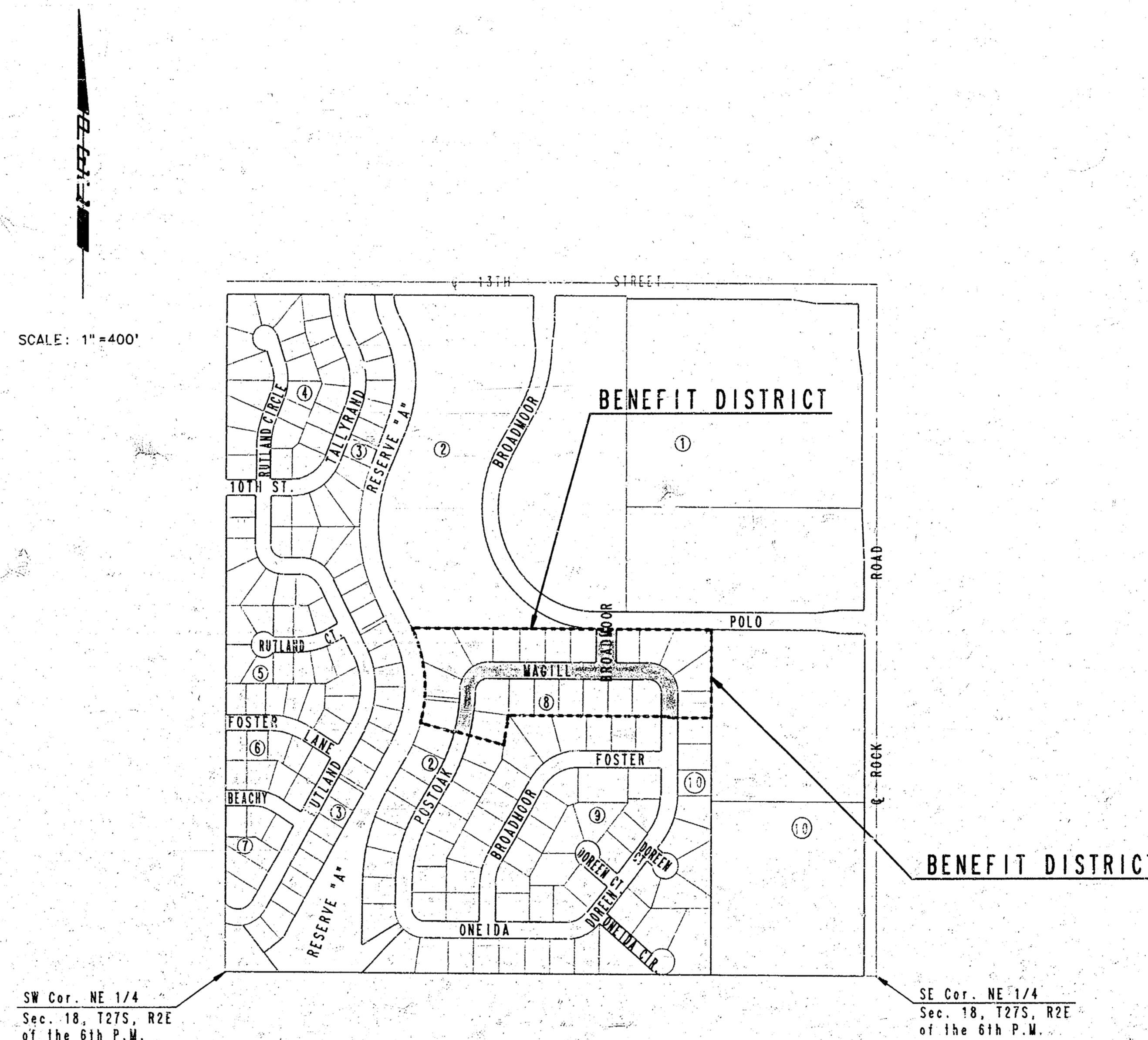
- INDEX OF SHEETS
1. TITLE SHEET
 - 2.-3. PLAT
 4. TYPICAL 35' PAVEMENT DETAILS
 5. BROADMOOR TYPICAL SECTION & MEDIAN DETAILS
 6. POSTOAK/MAGILL PAVING PLAN - STA. 37+19.35 TO STA. 43+00
 7. MAGILL/DOREEN PAVING PLAN - STA. 43+00 TO STA. 48+96.79
 8. BROADMOOR PAVING PLAN - STA. 50+00 TO STA. 51+95
 9. STANDARD DRIVE ENTRANCES
 10. WASTE DISPOSAL PLAN
 - 11-14. STREET CROSS-SECTIONS

PROJECT SURVEY CONTROL

| | |
|-------------------|---|
| VERTICAL DATUM: | CITY OF WICHITA DATUM |
| DATUM BENCH MARK: | R. R. SPIKE IN NE CORNER WOOD FENCE AT SOUTH SIDE OF KILLARNY PLACE AND ROCK ROAD. ELEV. = 176.71 |
| BENCH MARK #21: | R. R. SPIKE IN NE FACE 20' ELM 30' E OF NW CORNER LOT 18, BLOCK 10, FAIRFIELD ESTATES. ELEV. = 187.63 |
| BENCH MARK #22: | R. R. SPIKE IN E FACE 15' COTTONWOOD E SIDE OF GRAVEL DRIVE ALONG H-S FENCE. ELEV. = 192.64 |
| BENCH MARK #25: | CHIS. "0" ON TOP OF CURB E SIDE OF ROCK ROAD NEAR 4TH HLP SOUTH OF 13TH STREET. ELEV. = 182.86 |
| BENCH MARK #26: | CHIS. "0" ON TOP OF CURB E SIDE OF ROCK ROAD NEAR 6TH HLP SOUTH OF 13TH STREET. ELEV. = 181.30 |

EARTHWORK

| | |
|----------------|----------------|
| EXCAVATION | |
| X-SECTIONS | 2,411 CU. YDS. |
| 10% | 241 CU. YDS. |
| TOTAL | 2,652 CU. YDS. |
| COMPACTED FILL | |
| X-SECTIONS | 45 CU. YDS. |
| 10% | 5 CU. YDS. |
| TOTAL | 50 CU. YDS. |
| LOOSE FILL | |
| X-SECTIONS | 598 CU. YDS. |
| 10% | 60 CU. YDS. |
| TOTAL | 658 CU. YDS. |
| COMPACTED FILL | |
| X-SECTIONS | 82 CU. YDS. |
| 10% | 8 CU. YDS. |
| TOTAL | 90 CU. YDS. |



GENERAL NOTES

~~THE CONTRACT UNIT PRICE FOR "CLEARING RIGHT-OF-WAY" SHALL BE FULL COMPENSATION FOR REMOVAL AND DISPOSAL OF ALL ITEMS ENCOUNTERED WITHIN THE PROPOSED CONSTRUCTION. THIS PRICE SHALL INCLUDE, BUT NOT LIMITED TO, THE REMOVAL AND DISPOSAL OF TREES, POSTS, POLES, CONCRETE FOOTINGS AND FOUNDATIONS, UNDERGROUND UTILITY PIPING, COLVERTS, TREES AND~~

UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, AND ETCETERA ARE TO BE ADJUSTED OR REMOVED AS NECESSARY BY OTHERS PRIOR TO OR DURING CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

CONTRACTOR SHALL SATISFY HIMSELF OF SUB-SURFACE CONDITIONS PRIOR TO CONSTRUCTION.

RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND RUBBLE WHICH MAY BE ENCOUNTERED DURING EXCAVATION OPERATIONS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR OFF-SITE.

ALL EXCESS EXCAVATED MATERIAL FROM THIS PROJECT SHALL BE WASTED AS "LOOSE FILL" ON SITE (SEE SHEET NO. 10). NO EXCESS MATERIAL SHALL BE PLACED WITHIN STREET RIGHTS-OF-WAY. WASTE MATERIAL SHALL BE BLADED SMOOTH AND SLOPED TO DRAIN. THIS WORK SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

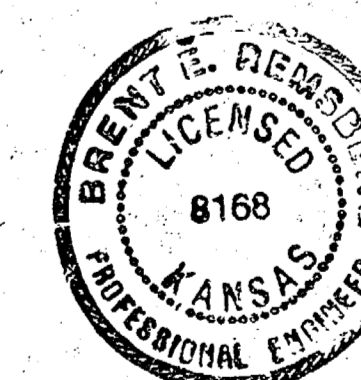
LIMITS OF EARTHWORK SHALL WATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO WATCH THE EXISTING GROUND SURFACE.

THE CONTRACTOR WILL BE PERMITTED TO BID ONLY ONE OF THE ALTERNATE TYPES OF SUBGRADE TREATMENT. THE TYPE BID BY THE SUCCESSFUL BIDDER WILL BE THE TYPE OF SUBGRADE TREATMENT USED TO CONSTRUCT THE PROJECT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

A SAW CUT AT LEAST ONE-HALF THE DEPTH OF EXISTING SURFACE COURSES OR ONE-FOURTH THE DEPTH OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ADJUTS AN EXISTING SURFACE COURSE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THAT SURFACE OR PAVEMENT IS REQUIRED. SAWED JOINT TO FACILITATE REMOVAL WITHIN THREE (3) FEET OF EXISTING JOINTS WILL NOT BE PERMITTED AND FOR SUCH INSTANCES THE LIMITS OF REMOVAL SHALL EXTEND TO THE EXISTING JOINT. SUCH SAW CUTS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE REMOVAL OF THE SURFACE OR PAVEMENT.

APRIL, 1986
 PLANS PREPARED BY
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

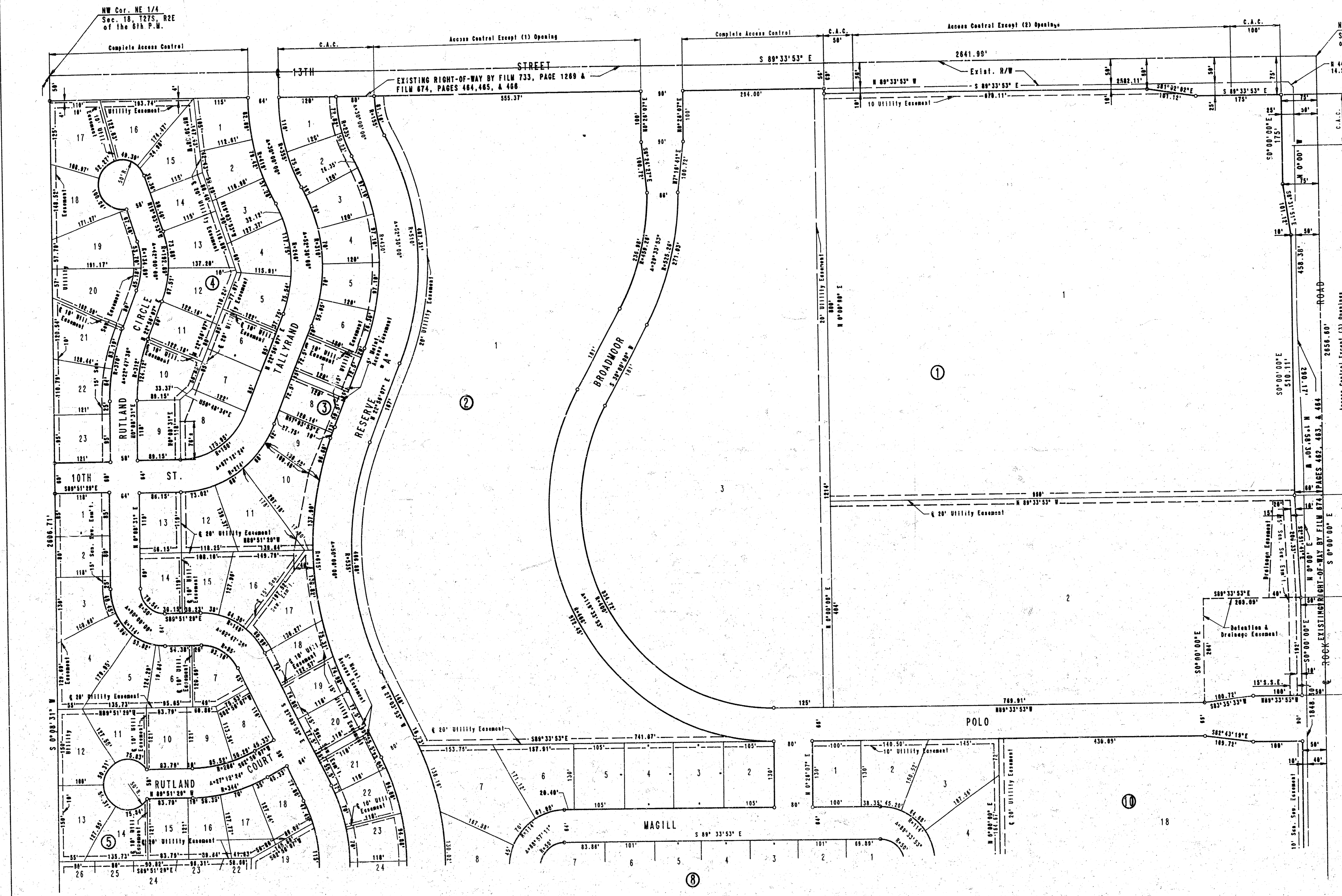


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| PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------------------|-----------|--------------|
| 472-75-245-81527-000-000-001 | 2 | 14 |

FAIRFIELD ESTATES

WICHITA, SEDGWICK COUNTY, KANSAS



NE Cor. NE 1/4
Sec. 18, T27S, R2E
of the 6th P.M.

SCALE: 1" = 100'
 ○ = IRON SET
 C.A.C. = COMPLETE ACCESS CONTROL
 B.M. - CHIS. "0" S. END CURB RETURN
 WEST SIDE ROCK ROAD AT E. 1/4 COR.
 SEC. 18, T27S, R2E,
 ELEV. 179.18 CITY DATUM
 BUILDING SETBACKS-SEE SHEET NO. 3

PLAT

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

Designed by CSB
 Drawn by DEP

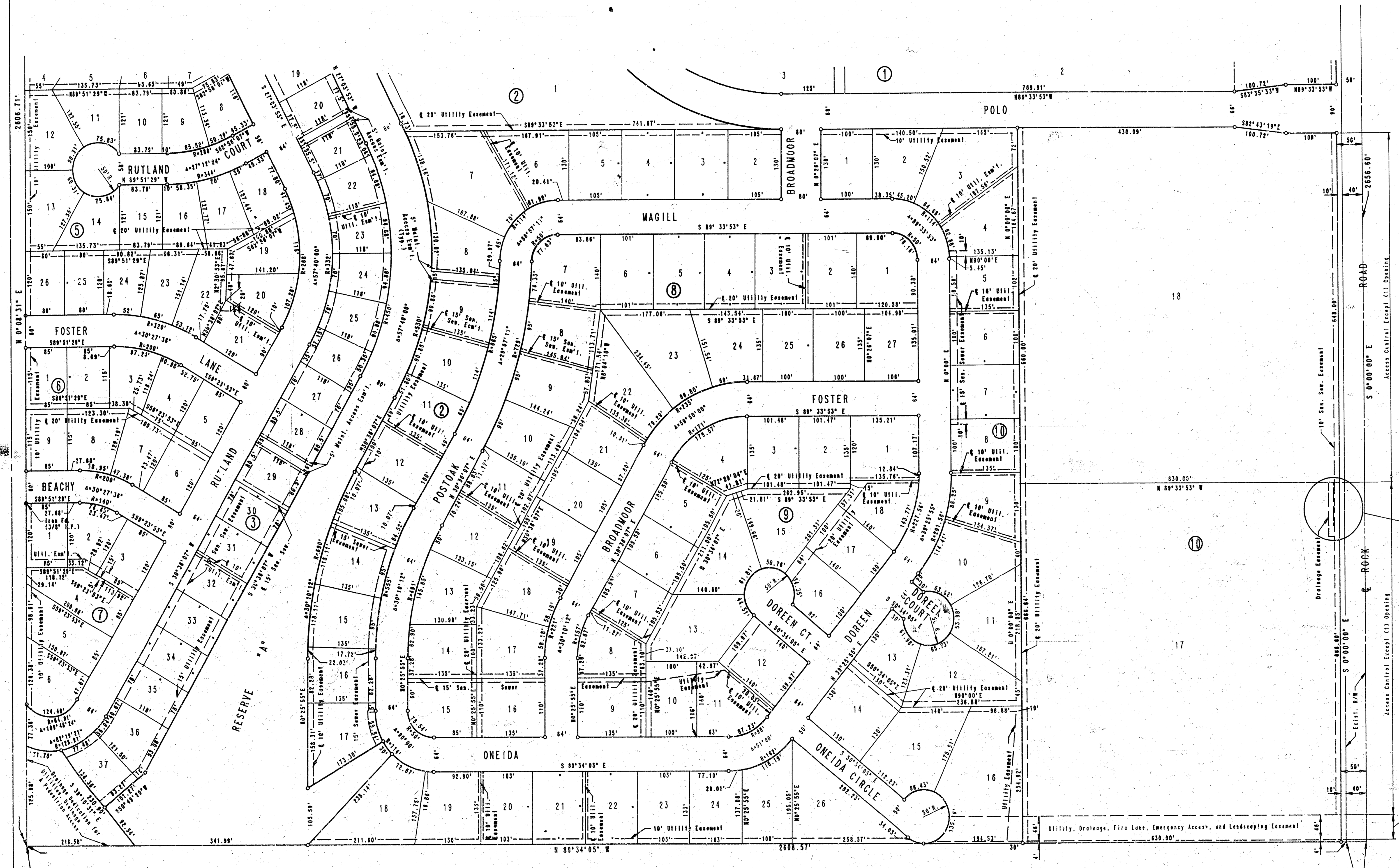
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 Date APRIL, 1986 Job No. 85592-7

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| PROJECT NO. | SHEET NO. | TOTAL SHEETS |
|------------------------------|-----------|--------------|
| 472-76-245-81527-000-000-001 | 3 | 14 |

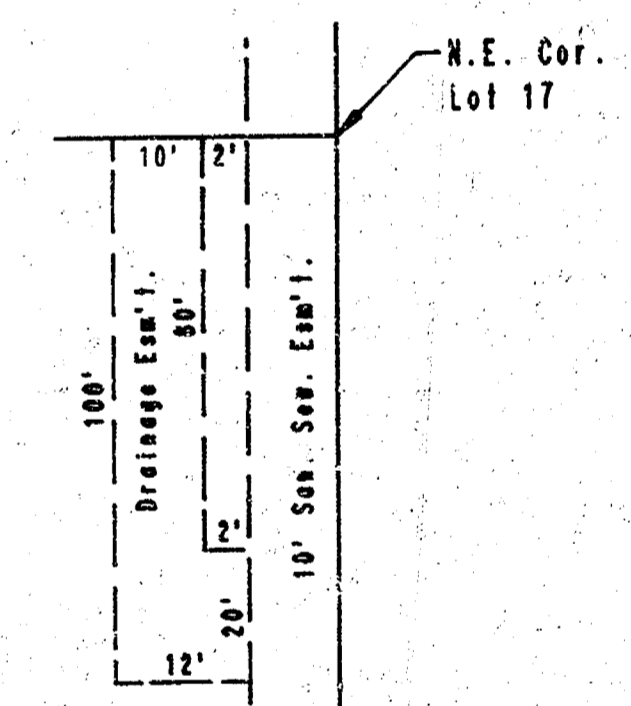
FAIRFIELD ESTATES

WICHITA, SEDGWICK COUNTY, KANSAS



SCALE: 1" = 100'
 C.A.C. - COMPLETE ACCESS CONTROL
 SEE SHEET NO. 1 FOR BENCH MARK

BUILDING SETBACKS AS PER THE REQUIREMENTS OF THE FAIRFIELD COMMERCIAL C.U.P. (OP-144), FAIRFIELD RESIDENTIAL C.U.P. (OP-145), AND FAIRFIELD ESTATES C.U.P. (OP-143) ON FILE WITH THE WICHITA SEDGWICK COUNTY METROPOLITAN AREA PLANNING DEPARTMENT.



SW Cor. NE 1/4
 Sec. 18, T27S, R2E
 of the 6th P.M.

Point of Beginning
 SE Cor. NE1/4
 Sec. 18, T27S, R2E
 of the 6th P.M.

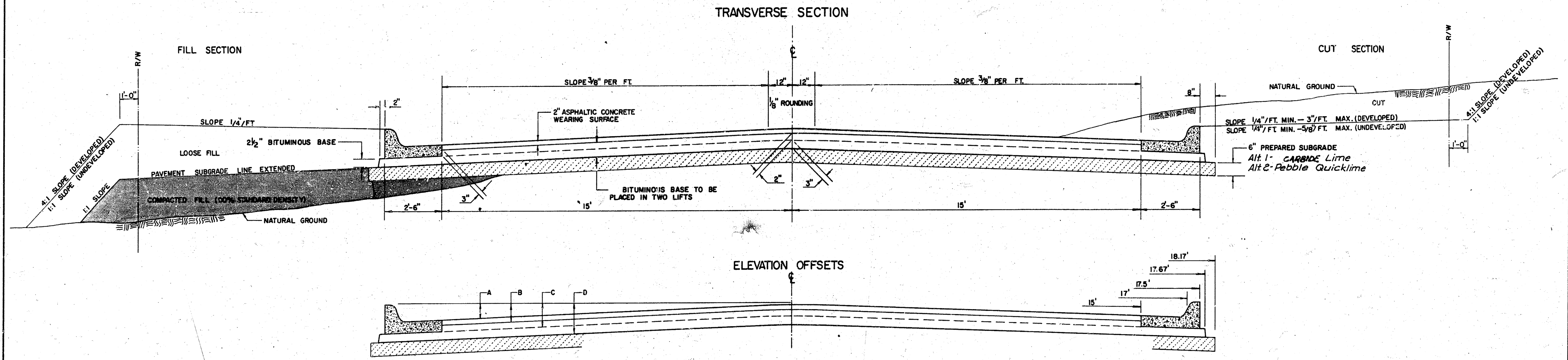
PLAT

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

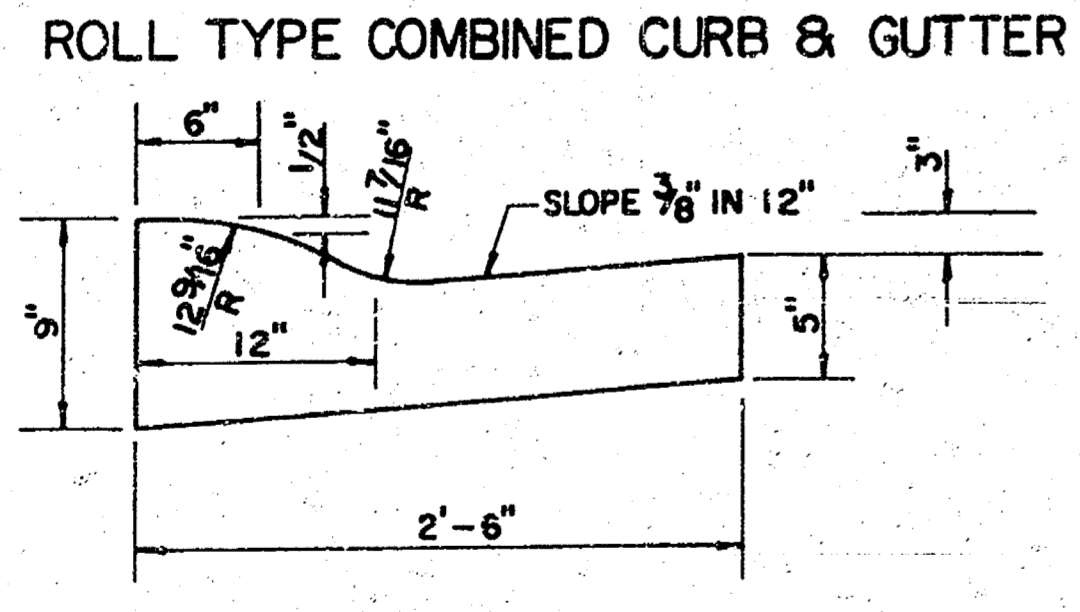
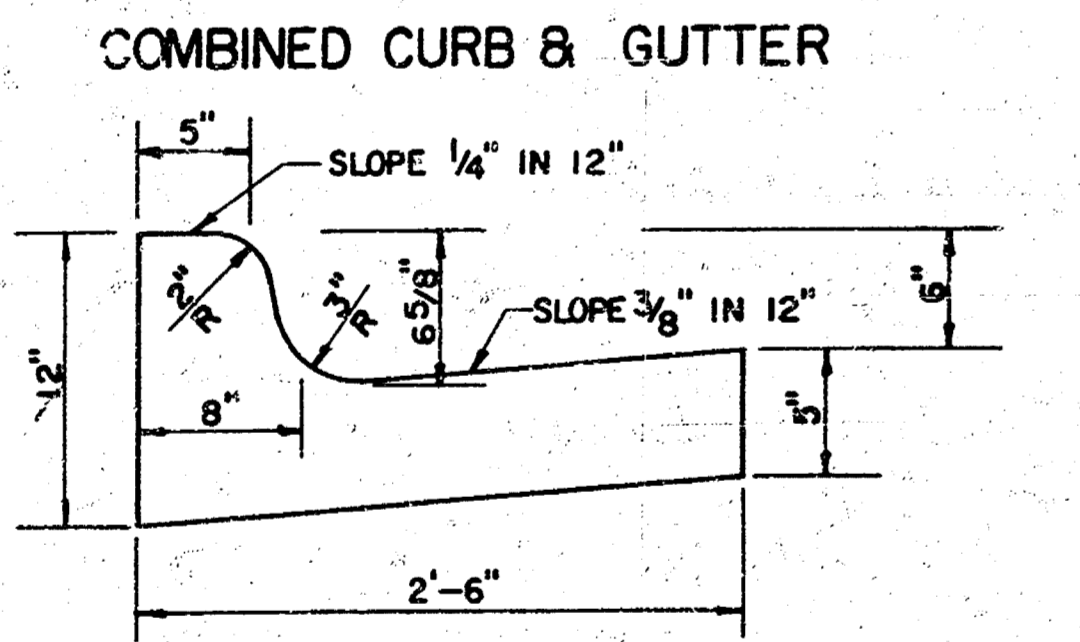
| | | | |
|-------------|-----|------------|-----------------------------|
| Designed by | CSB | Checked by | 3/ |
| Drawn by | DCP | Date | APRIL, 1988 Job No. 85592-7 |

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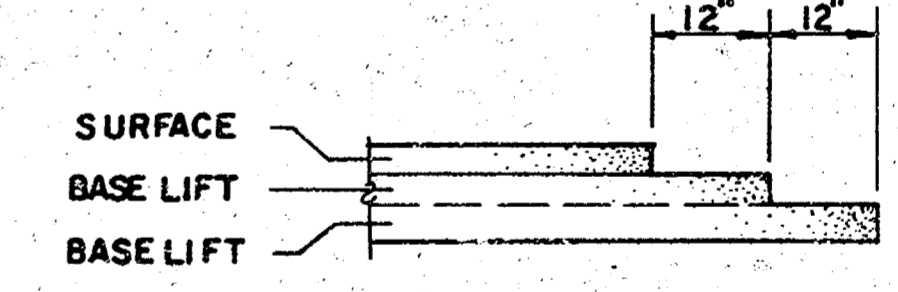
TYPICAL 35' PAVEMENT DETAILS



| | DISTANCE FROM CENTERLINE (LT. & RT.) | | | | | | | | | | | | |
|---|--------------------------------------|------|------|------|------|------|------|------|------|------|-------|--------|--------|
| | 0' | 2' | 4' | 6' | 8.5' | 10' | 12' | 14' | 15' | 17' | 17.5' | 17.67' | 18.17' |
| A: TOP OF CURBS TO TOP OF SURFACE LIFT | 0.04 | 0.08 | 0.14 | 0.21 | 0.29 | 0.33 | 0.39 | 0.46 | 0.49 | — | — | — | — |
| B: TOP OF CURBS TO TOP OF UPPER BASE LIFT | 0.21 | 0.25 | 0.31 | 0.37 | 0.45 | 0.50 | 0.56 | 0.62 | 0.65 | — | — | — | — |
| C: TOP OF CURBS TO TOP OF LOWER BASE LIFT | 0.37 | 0.43 | 0.50 | 0.57 | 0.67 | 0.72 | 0.79 | 0.87 | 0.90 | 0.98 | 1.00 | 1.00 | — |
| D: TOP OF CURBS TO TOP OF SUBGRADE | 0.62 | 0.67 | 0.74 | 0.81 | 0.90 | 0.95 | 1.02 | 1.08 | 1.12 | 1.19 | 1.21 | 1.21 | 1.23 |



TRANSVERSE CONSTRUCTION JOINTS



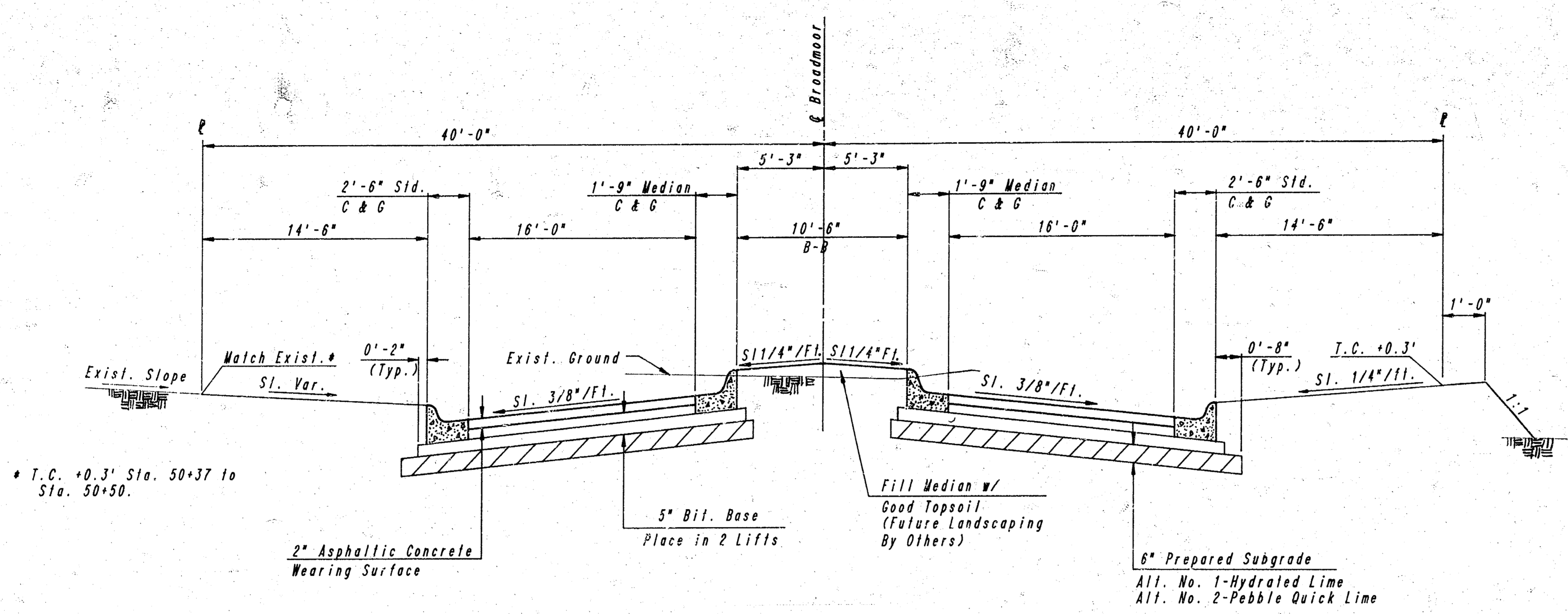
TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

GENERAL NOTES

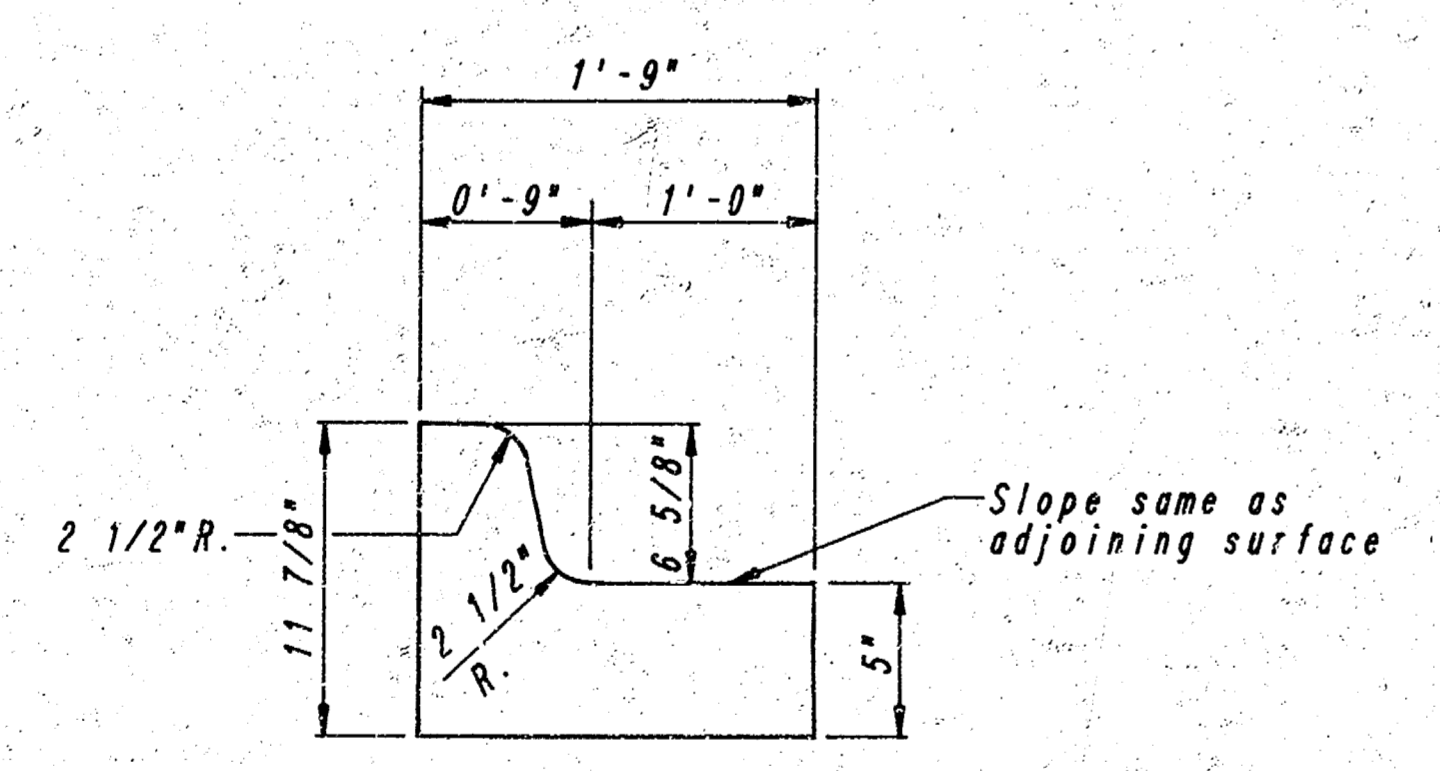
- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
 CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 472-79-245-81527-000-000-001

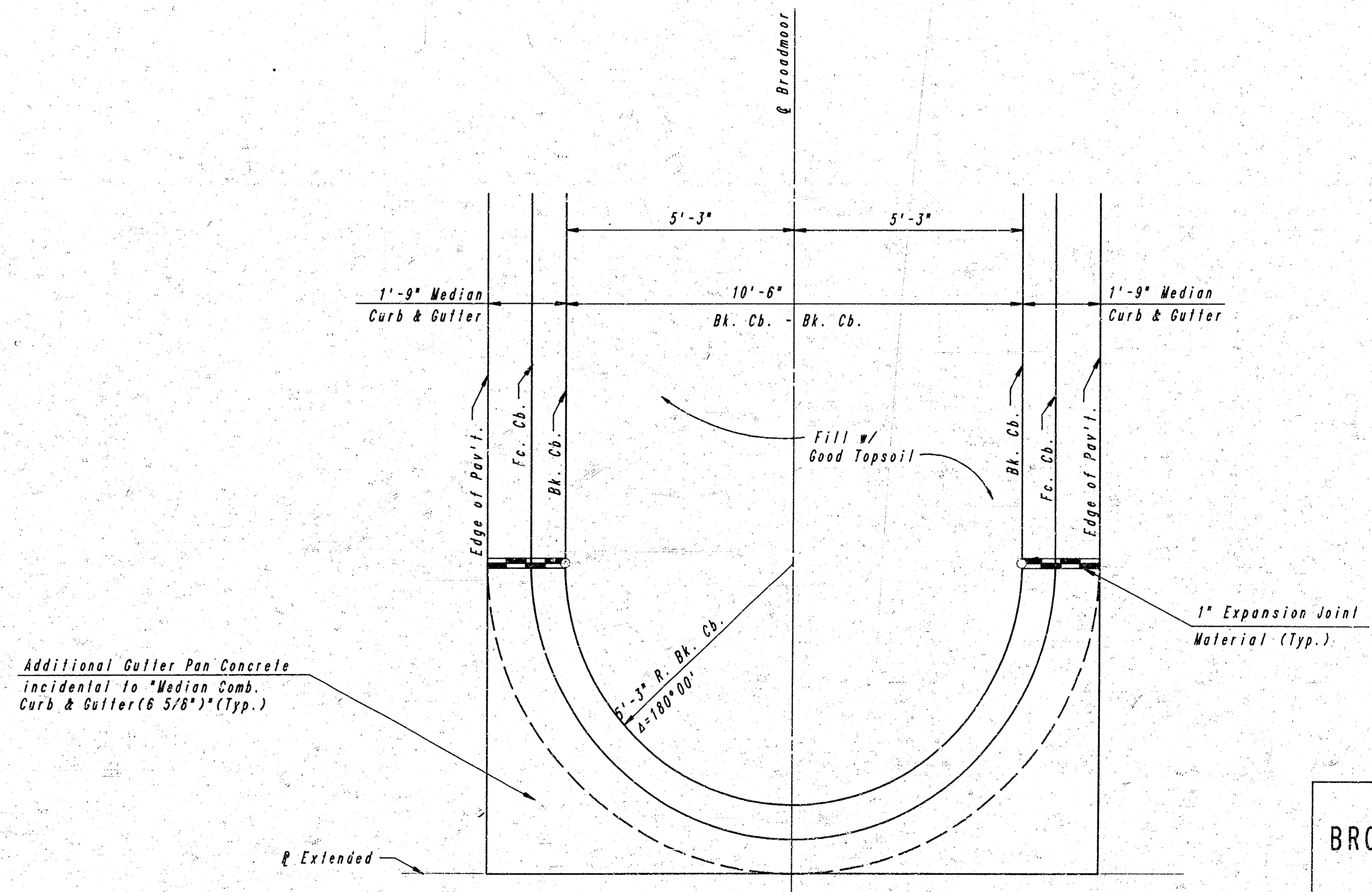
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TYPICAL SECTION
Sta. 50+38.75 to Sta. 51+55.25 Broadmoor



MEDIAN CURB & GUTTER DETAIL



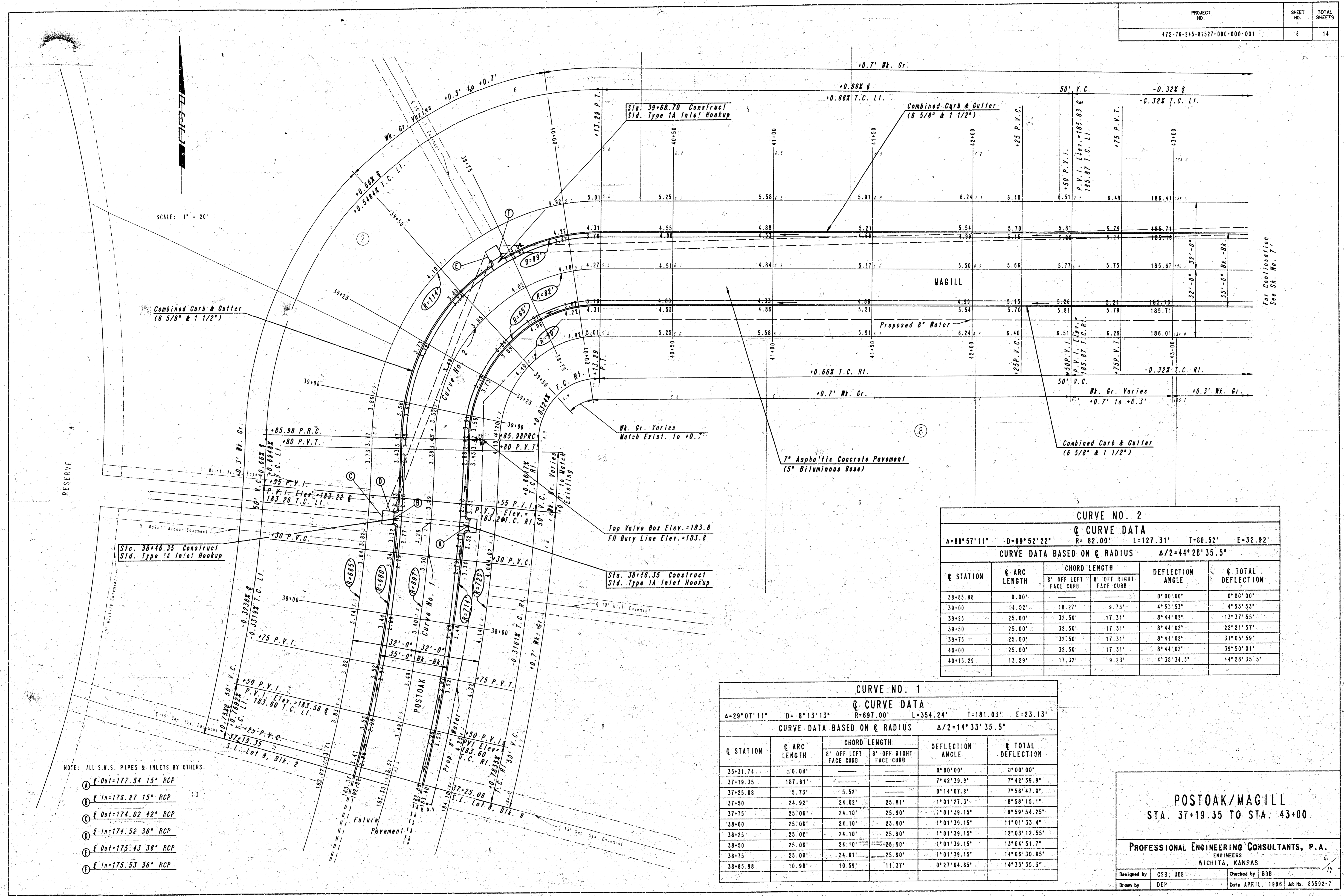
MEDIAN NOSE DETAIL
Broadmoor @ Polo & Broadmoor @ Magill

BROADMOOR TYPICAL SECTIONS & MEDIAN DETAILS

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

| | |
|----------------------|----------------------------------|
| Designed by CSB, BDB | Checked by BDB |
| Drawn by DEP | Date APRIL, 1986 Job No. 85592-7 |

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SCALE: 1" = 20'

RESERVE "A"

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS.

- A Out=177.54 15" RCP
- B In=176.27 15" RCP
- C Out=174.02 42" RCP
- D In=174.52 36" RCP
- E Out=175.43 36" RCP
- F In=175.53 36" RCP

CURVE NO. 2
Q CURVE DATA
 $\Delta=88^{\circ}57'11''$ $D=69^{\circ}52'22''$ $R=82.00'$ $L=127.31'$ $T=80.52'$ $E=32.92'$
CURVE DATA BASED ON Q RADIUS $\Delta/2=44^{\circ}28'35.5''$

| Q STATION | Q ARC LENGTH | CHORD LENGTH | | DEFLECTION ANGLE | Q TOTAL DEFLECTION |
|-----------|--------------|-----------------------|------------------------|------------------|--------------------|
| | | 8' OFF LEFT FACE CURB | 8' OFF RIGHT FACE CURB | | |
| 38+85.98 | 0.00' | | | 0°00'00" | 0°00'00" |
| 39+00 | 14.02' | 18.27' | 9.73' | 4°53'53" | 4°53'53" |
| 39+25 | 25.00' | 32.50' | 17.31' | 8°44'02" | 13°37'55" |
| 39+50 | 25.00' | 32.50' | 17.31' | 8°44'02" | 22°21'57" |
| 39+75 | 25.00' | 32.50' | 17.31' | 8°44'02" | 31°05'59" |
| 40+00 | 25.00' | 32.50' | 17.31' | 8°44'02" | 39°50'01" |
| 40+13.29 | 13.29' | 17.32' | 9.23' | 4°38'34.5" | 44°28'35.5" |

CURVE NO. 1
Q CURVE DATA
 $\Delta=29^{\circ}07'11''$ $D=8^{\circ}13'13''$ $R=697.00'$ $L=354.24'$ $T=181.03'$ $E=23.13'$
CURVE DATA BASED ON Q RADIUS $\Delta/2=14^{\circ}33'35.5''$

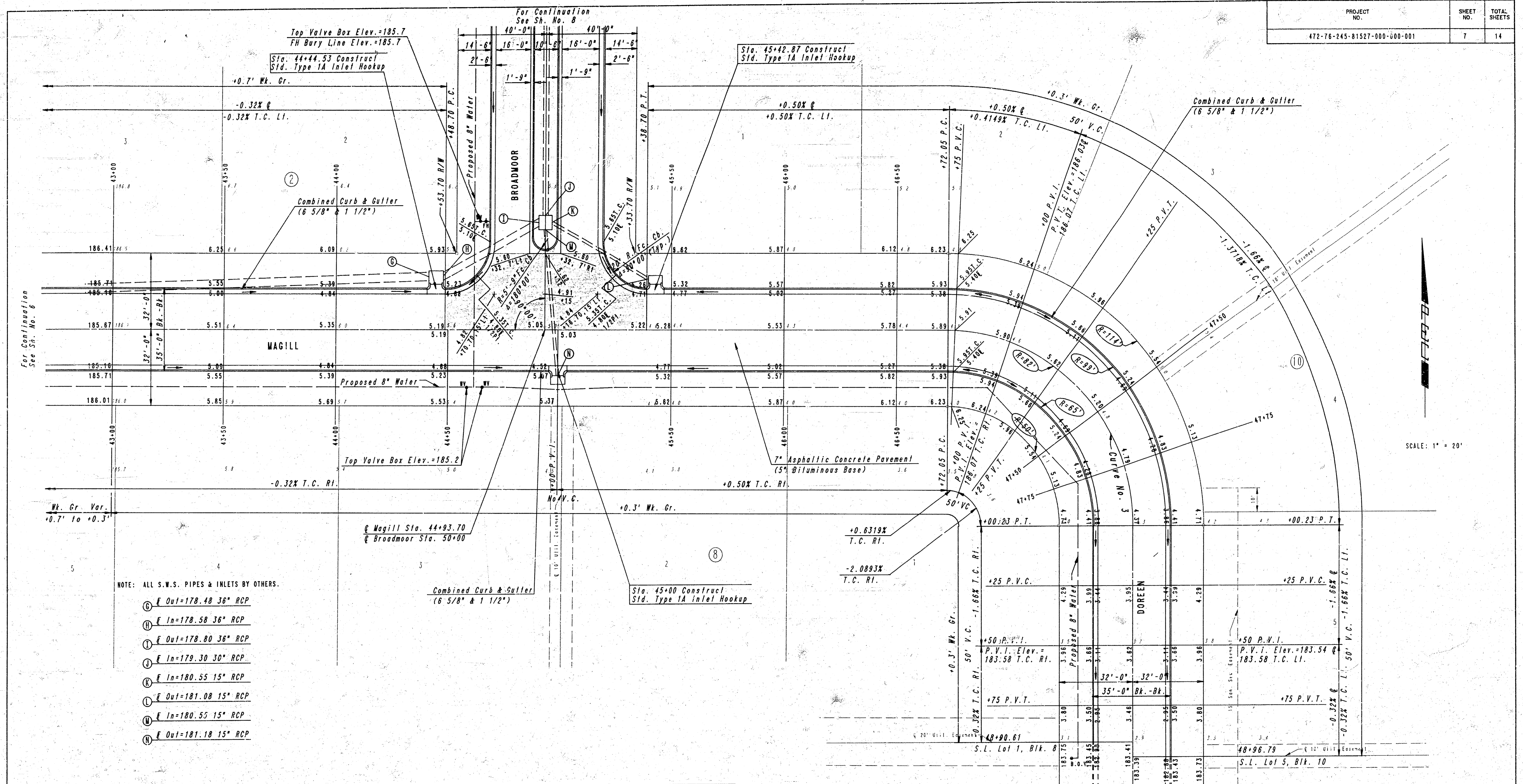
| Q STATION | Q ARC LENGTH | CHORD LENGTH | | DEFLECTION ANGLE | Q TOTAL DEFLECTION |
|-----------|--------------|-----------------------|------------------------|------------------|--------------------|
| | | 8' OFF LEFT FACE CURB | 8' OFF RIGHT FACE CURB | | |
| 35+31.74 | 0.00' | | | 0°00'00" | 0°00'00" |
| 37+19.35 | 187.61' | | | 7°42'39.9" | 7°42'39.9" |
| 37+25.08 | 5.73' | 5.52' | | 0°14'07.5" | 7°56'47.8" |
| 37+50 | 24.92' | 24.02' | 25.81' | 1°01'27.3" | 8°58'15.1" |
| 37+75 | 25.00' | 24.10' | 25.90' | 1°01'39.15" | 9°59'54.25" |
| 38+00 | 25.00' | 24.10' | 25.90' | 1°01'39.15" | 11°01'33.4" |
| 38+25 | 25.00' | 24.10' | 25.90' | 1°01'39.15" | 12°03'12.55" |
| 38+50 | 25.00' | 24.10' | 25.90' | 1°01'39.15" | 13°04'51.7" |
| 38+75 | 25.00' | 24.01' | 25.90' | 1°01'39.15" | 14°06'30.85" |
| 38+85.98 | 10.98' | 10.59' | 11.37' | 0°27'04.65" | 14°33'35.5" |

POSTOAK/MAGILL
STA. 37+19.35 TO STA. 43+00

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by CSB, BDB Checked by BDB
Drawn by DEP Date APRIL, 1986 Job No. 85592-7

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- NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS.
- Ⓒ F Out=178.48 36" RCP
 - Ⓓ F In=178.58 36" RCP
 - Ⓔ F Out=178.80 36" RCP
 - Ⓕ F In=179.30 30" RCP
 - Ⓖ F In=180.55 15" RCP
 - Ⓗ F Out=181.08 15" RCP
 - Ⓜ F In=180.55 15" RCP
 - Ⓝ F Out=181.18 15" RCP

| INTERSECTION QUANTITIES | |
|-------------------------|---|
| — S.Y. — | Unreinforced Concrete Pavement |
| 259.78 S.Y. | 7" Asphaltic Conc. Pavement (5" Bituminous Base) |
| 16.25 S.Y. | 2 1/2" Bituminous Base |
| 52.72 L.F. | Combined Curb & Gutter (6 5/8" & 1 1/2") |
| — | L.F. Monolithic Edge Curb (—) |
| — | Eq. Standard Wheelchair Ramp Construction |
| — | S.F. 4" Sidewalk Concrete Pavement |
| 278.78 S.Y. | Subgrade Stabilization Manipulation |
| 4.18 Tons | Hydrated Lime Subgrade Stabilization |
| 2.09 Tons | Pulver Quicklime Subgrade Stabilization |
| — | Tons Cement Subgrade Stabilization |
| — | Tons Fly Ash Subgrade Stabilization |
| — | Tons Preheater Fines Subgrade Stabilization |
| — | S.Y. Reinf. V.C. — Concrete & — Asphaltic Concrete Base |

| CURVE NO. 3 | | | | | |
|--|--------------|-----------------------|------------------------|------------------|--------------------|
| @ CURVE DATA | | | | | |
| Δ=89°33'53" | | D=69°52'22" | | E=33.53' | |
| CURVE DATA BASED ON @ RADIUS Δ/2=44°46'56.5" | | | | | |
| @ STATION | @ ARC LENGTH | CHORD LENGTH | | DEFLECTION ANGLE | @ TOTAL DEFLECTION |
| | | 8' OFF LEFT FACE CURB | 8' OFF RIGHT FACE CURB | | |
| 46+72.05 | 0.00 | — | — | 0°00'00" | 0°00'00" |
| 46+75 | 2.95' | 3.84' | 1.05' | 1°01'51" | 1°01'51" |
| 47+00 | 25.00' | 32.50' | 17.31' | 8°44'03" | 9°45'54" |
| 47+25 | 25.00' | 32.50' | 17.31' | 8°44'03" | 18°29'57" |
| 47+50 | 25.00' | 32.50' | 17.31' | 8°44'03" | 27°14'00" |
| 47+75 | 25.00' | 32.50' | 17.31' | 8°44'03" | 35°58'03" |
| 48+00.23 | 25.23' | 32.79' | 17.47' | 8°48'53.5" | 44°46'56.5" |

MAGILL/DOREEN
STA. 43+00 TO STA. 48+96.79

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

| | | | |
|-------------|----------|------------|-----------------------------|
| Designed by | CSB, BDB | Checked by | BDB |
| Drawn by | DEP | Date | APRIL, 1986 Job No. 85592-7 |

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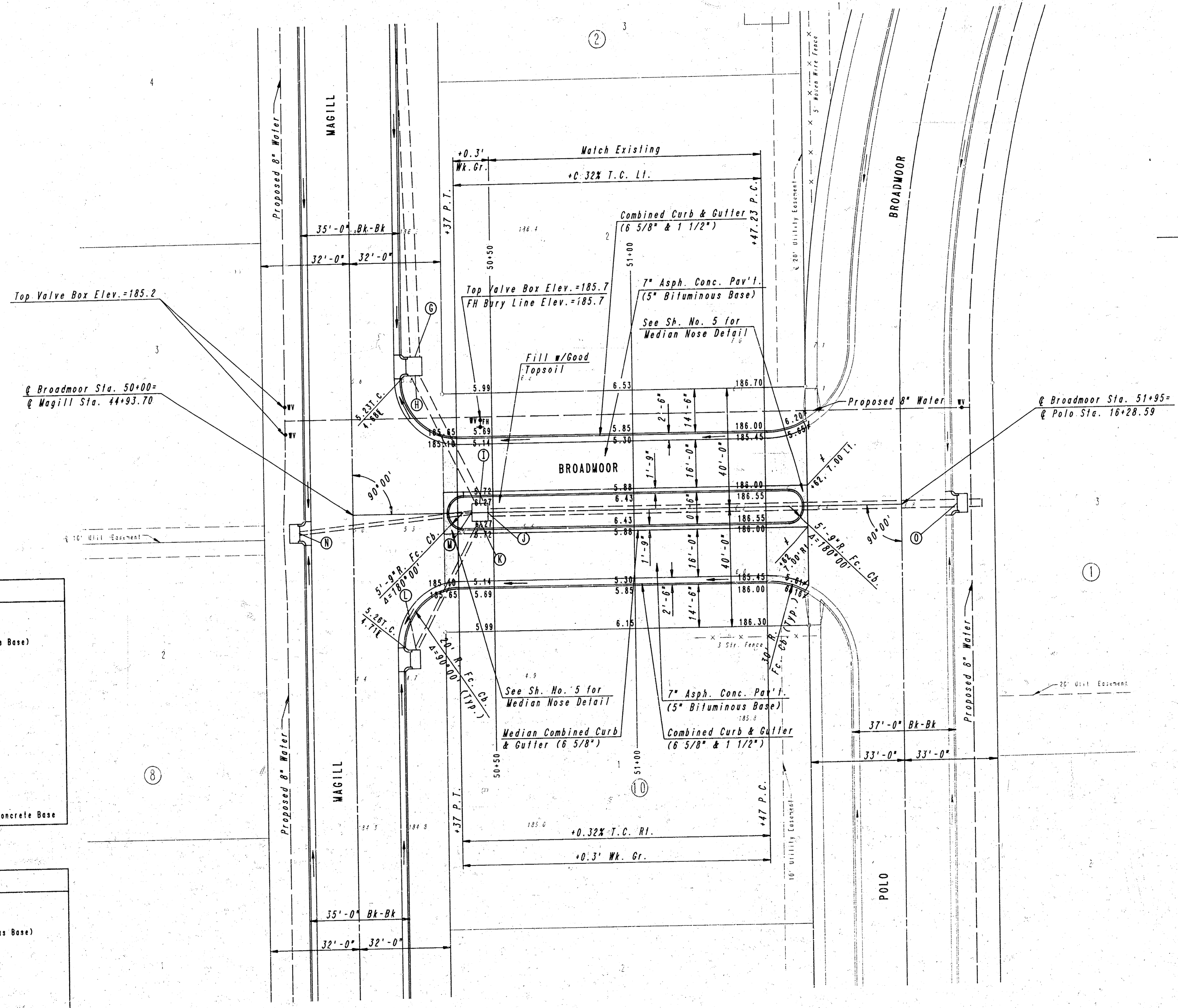
For Continuation
See Sh. Nos. 6 & 7

NOTE: ALL S.W.S. PIPES & INLETS BY OTHERS.

- ⑥ Out=178.48 36" RCP
- ⑦ In=178.58 36" RCP
- ⑧ Out=178.80 36" RCP
- ⑨ In=179.30 30" RCP
- ⑩ In=180.55 15" RCP
- ⑪ Out=181.08 15" RCP
- ⑫ In=180.55 15" RCP
- ⑬ Out=181.18 15" RCP
- ⑭ Out=180.85 30" RCP

| BROADMOOR & MAGILL | |
|-------------------------|---|
| INTERSECTION QUANTITIES | |
| — | S.Y. —' Unreinforced Concrete Pavement |
| 17.99 | S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base) |
| 9.37 | S.Y. 2 1/2" Bituminous Base |
| 10.11 | L.F. Combined Curb & Gutter (6 5/8" & 1 1/2") |
| 18.06 | L.F. Median Combined Curb & Gutter (6 5/8") |
| — | Ea. Standard Wheelchair Ramp Construction |
| — | S.F. 4" Sidewalk Concrete Pavement |
| 28.72 | S.Y. Subgrade Stabilization Manipulation |
| 0.43 | Tons Hydrated Lime Subgrade Stabilization |
| 0.22 | Tons Pebble Quicklime Subgrade Stabilization |
| — | Tons Cement Subgrade Stabilization |
| — | Tons Fly Ash Subgrade Stabilization |
| — | Tons Preheater Fines Subgrade Stabilization |
| — | S.Y. Reinf. V.G. —' Concrete & —' Asphaltic Concrete Base |

| BROADMOOR & POLO | |
|-------------------------|---|
| INTERSECTION QUANTITIES | |
| — | S.Y. —' Unreinforced Concrete Pavement |
| 57.37 | S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base) |
| 18.66 | S.Y. 2 1/2" Bituminous Base |
| 29.27 | L.F. Combined Curb & Gutter (6 5/8" & 1 1/2") |
| 34.06 | L.F. Median Combined Curb & Gutter (6 5/8") |
| — | Ea. Standard Wheelchair Ramp Construction |
| — | S.F. 4" Sidewalk Concrete Pavement |
| 79.34 | S.Y. Subgrade Stabilization Manipulation |
| 1.19 | Tons Hydrated Lime Subgrade Stabilization |
| 0.60 | Tons Pebble Quicklime Subgrade Stabilization |
| — | Tons Cement Subgrade Stabilization |
| — | Tons Fly Ash Subgrade Stabilization |
| — | Tons Preheater Fines Subgrade Stabilization |
| — | S.Y. Reinf. V.G. —' Concrete & —' Asphaltic Concrete Base |



SCALE: 1" = 20'

Match Exist. Pavement Elevation

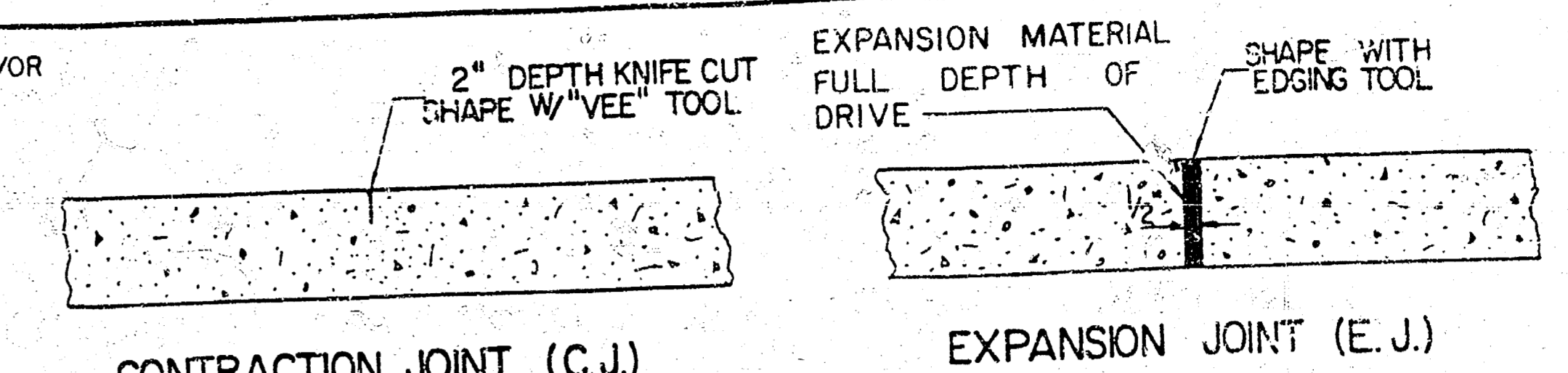
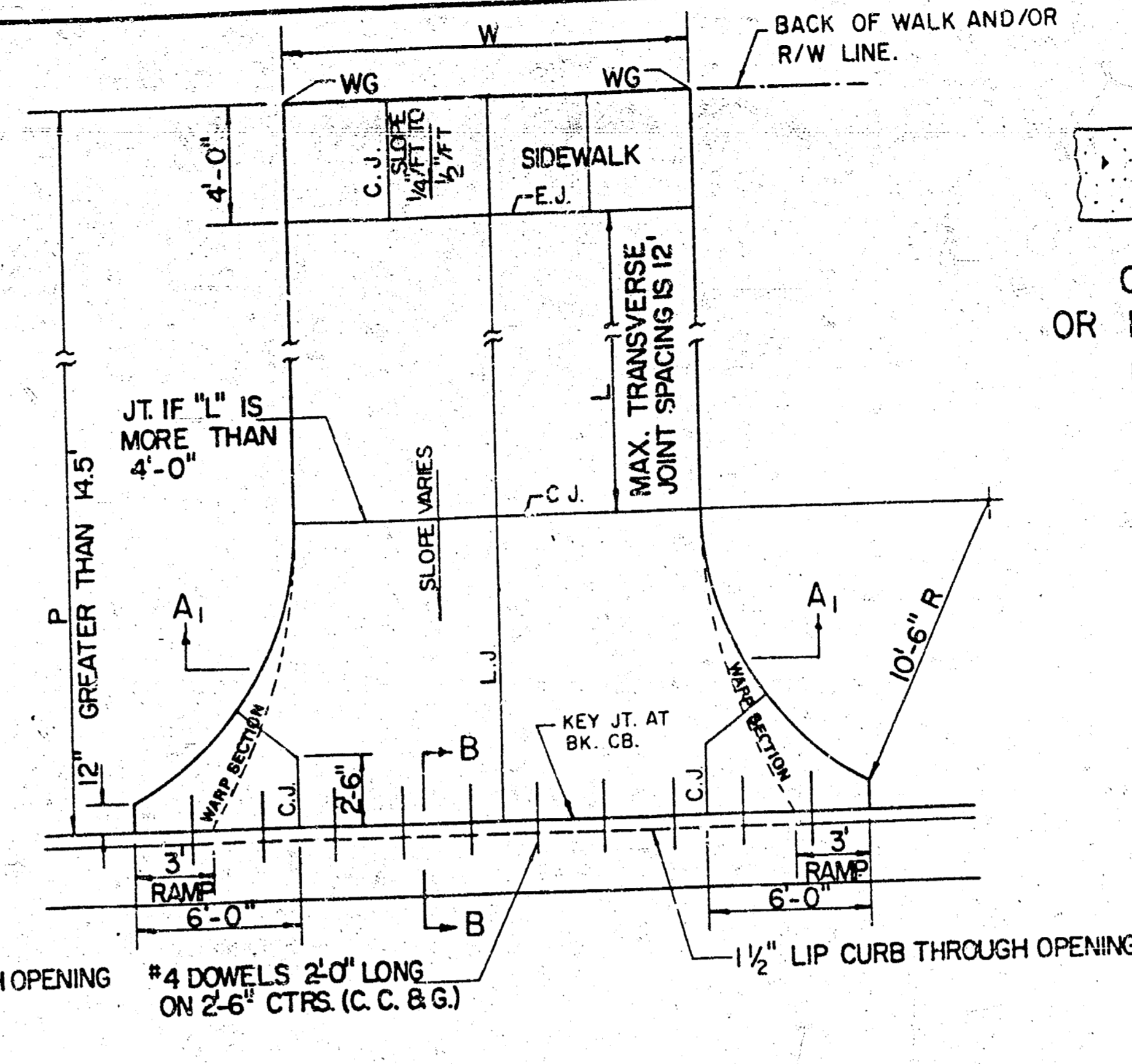
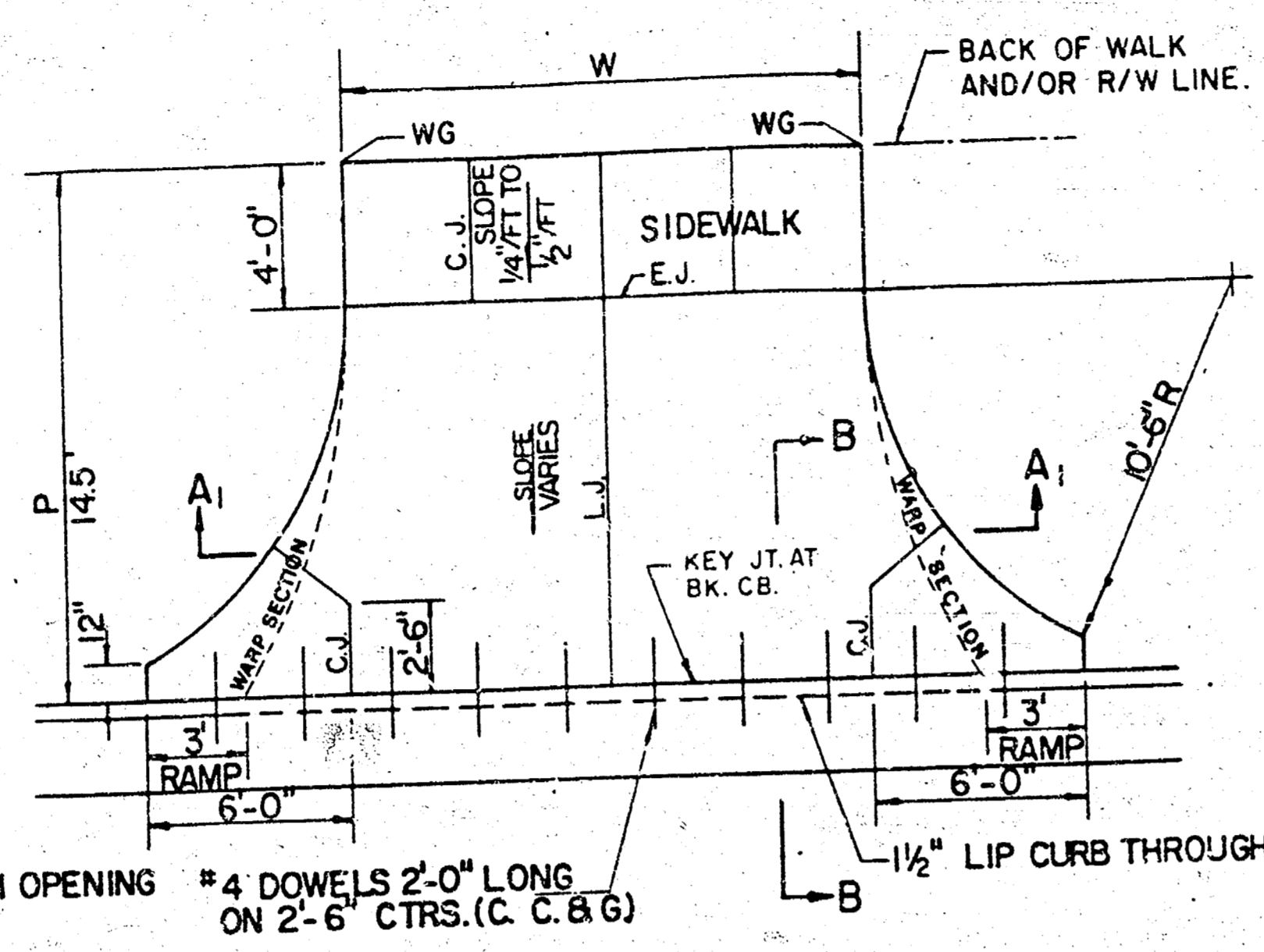
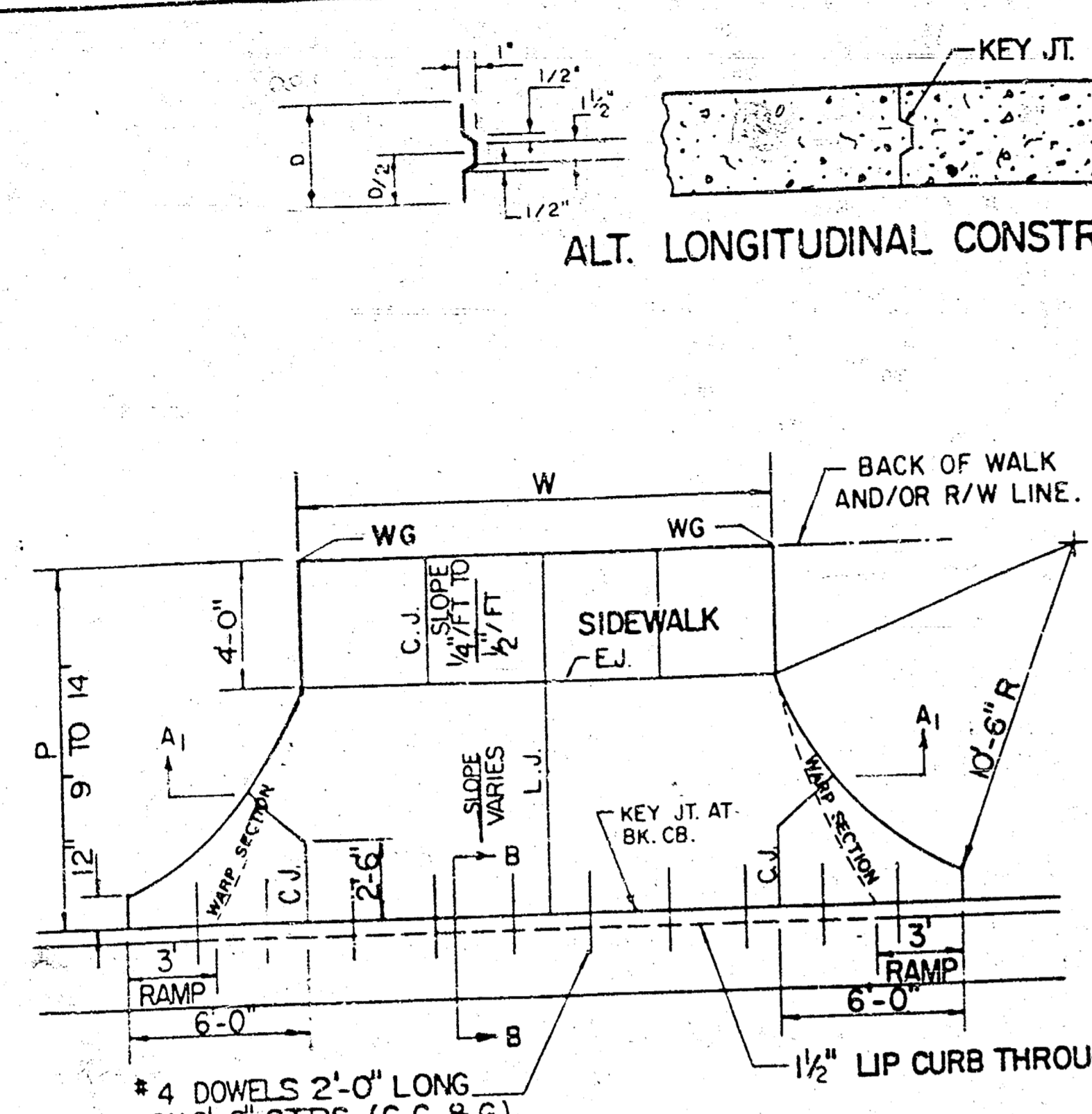
For Continuation
See Sh. No. 7

BROADMOOR
STA. 50+00 TO STA. 51+95

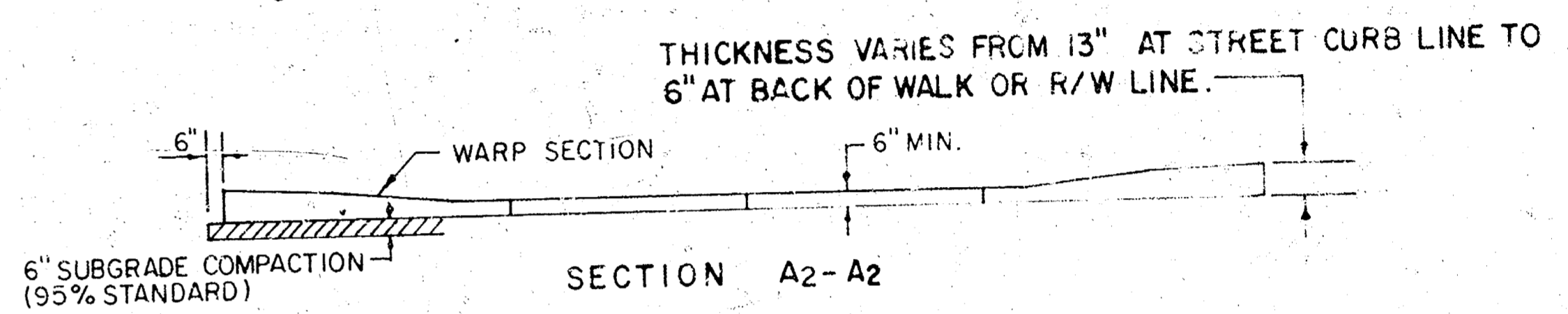
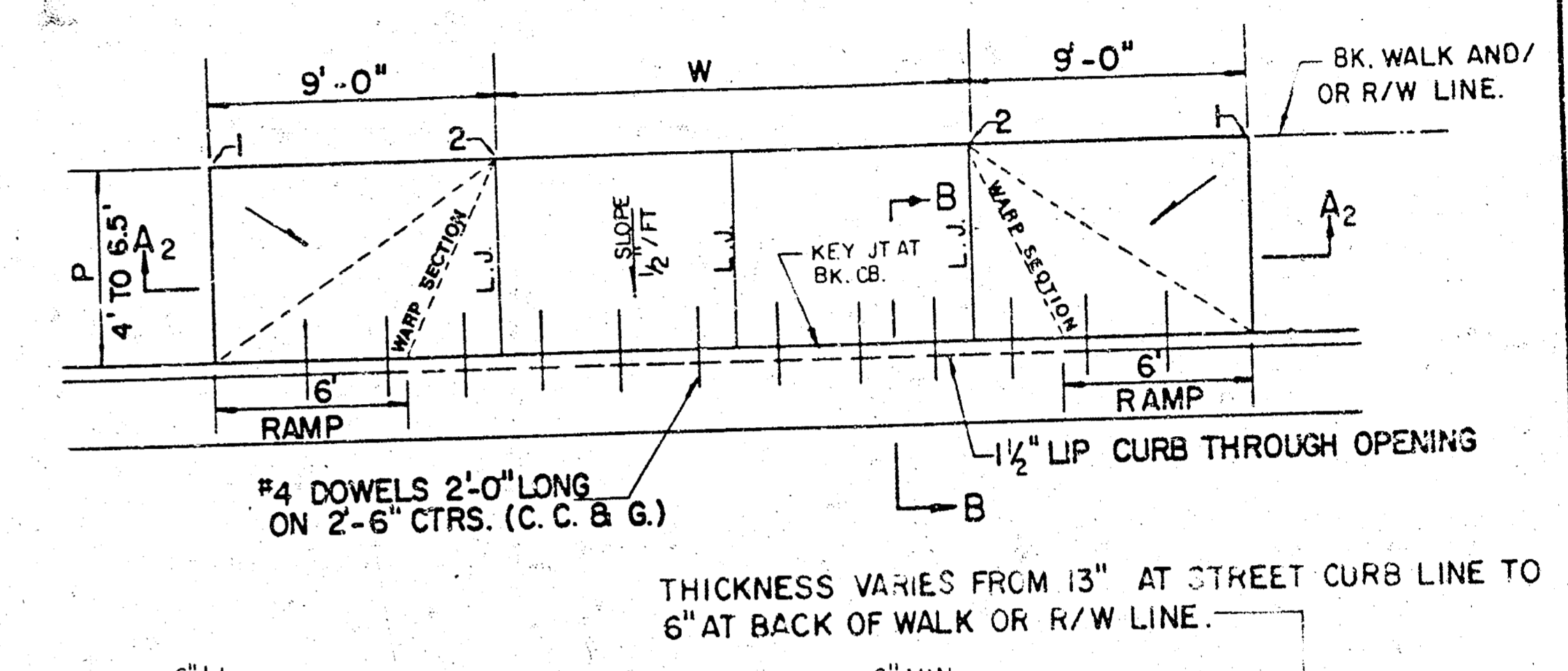
PROFESSIONAL ENGINEERING CONSULTANTS, P. A.
ENGINEERS
WICHITA, KANSAS

| | | | |
|-------------|----------|------------|-------------|
| Designed by | CSB, BDB | Checked by | BDB |
| Drawn by | DEP | Date | APRIL, 1986 |
| | | Job No. | 85592-7 |

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AVAILABLE COPY.....

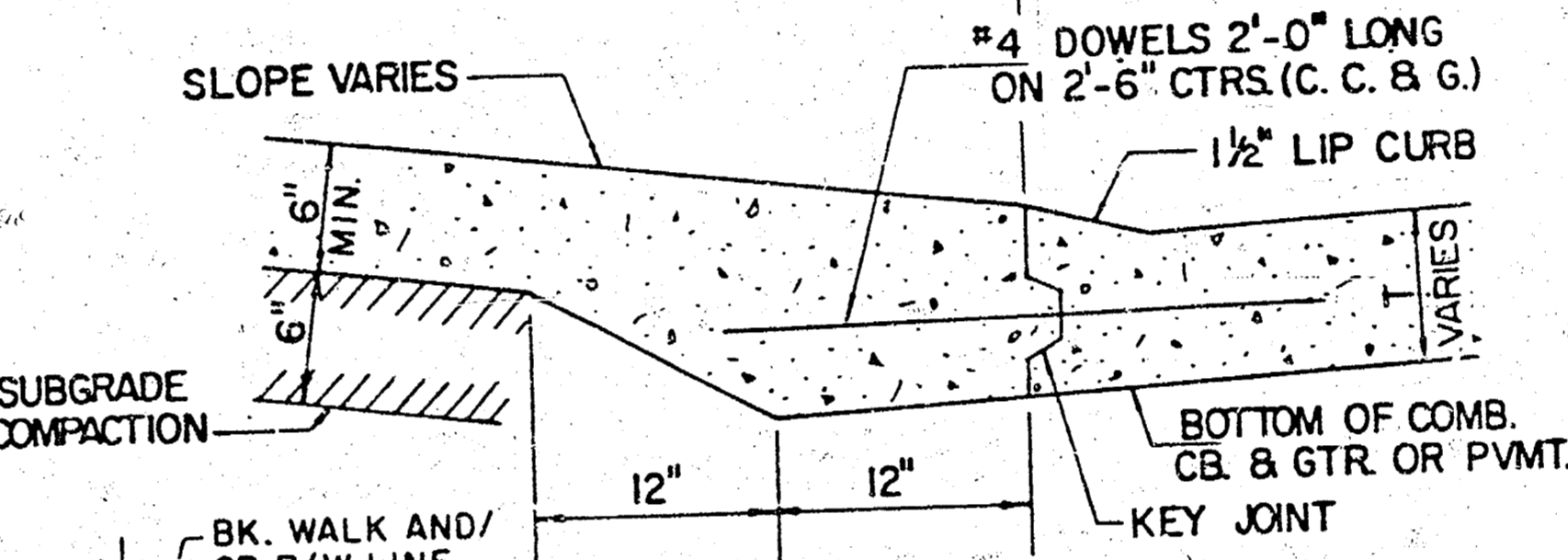


CONTRACTION JOINT (C.J.) OR LONGITUDINAL JOINT (L.J.) NO SAWN JOINTS WILL BE ALLOWED.

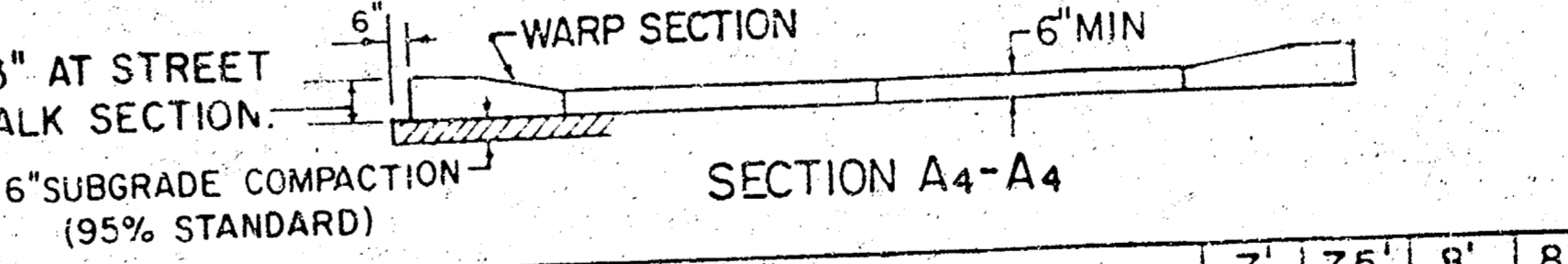
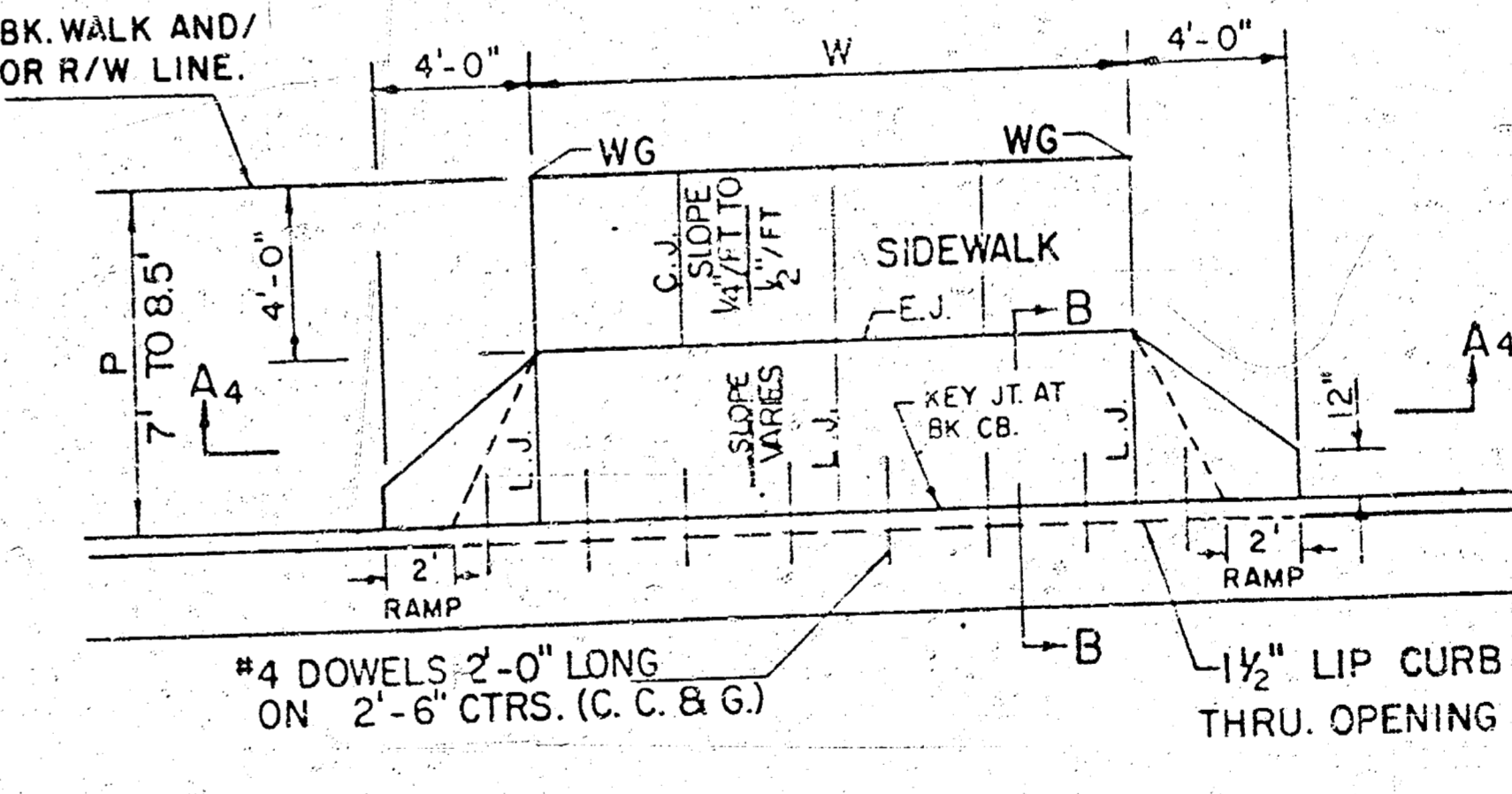
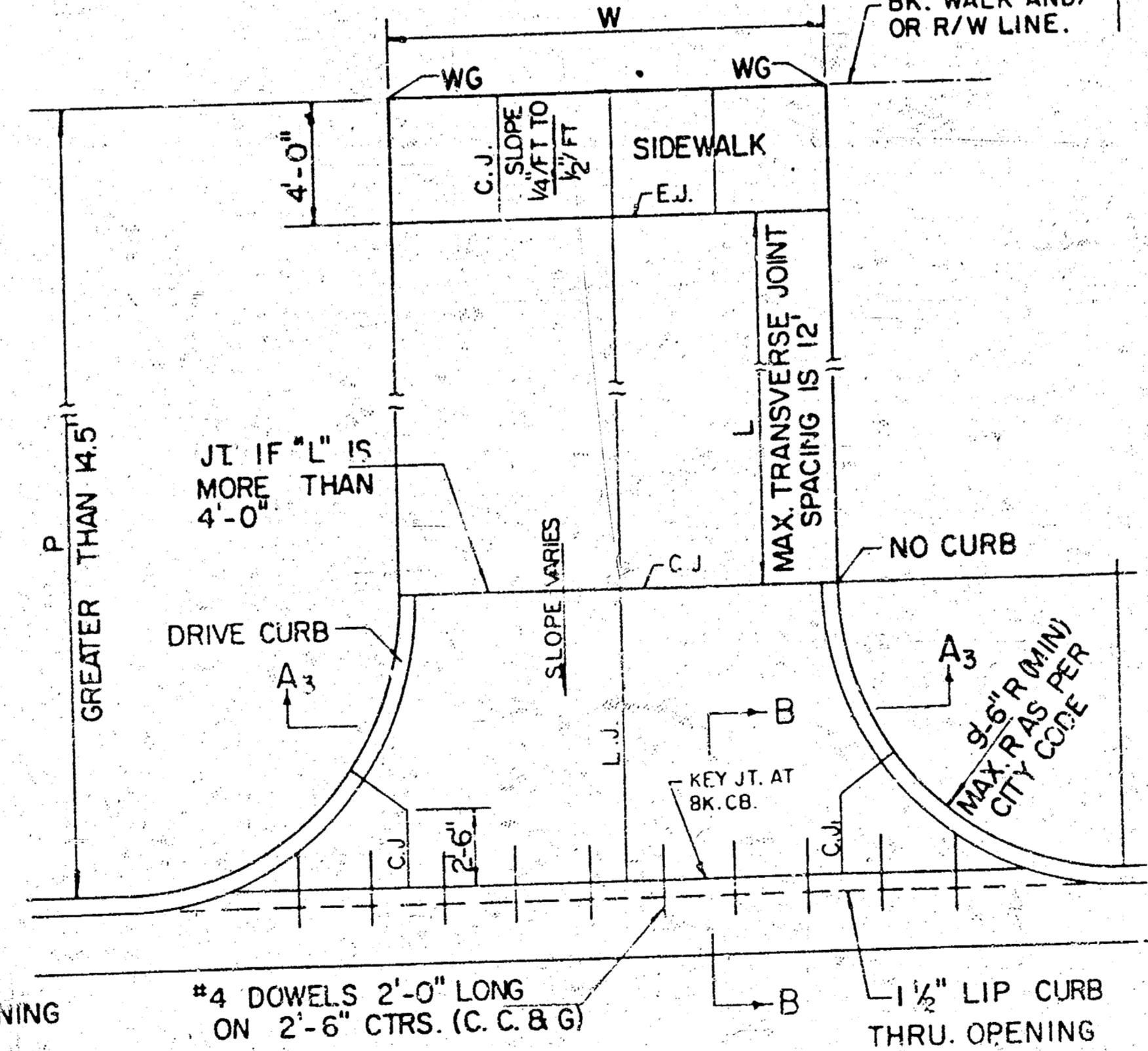
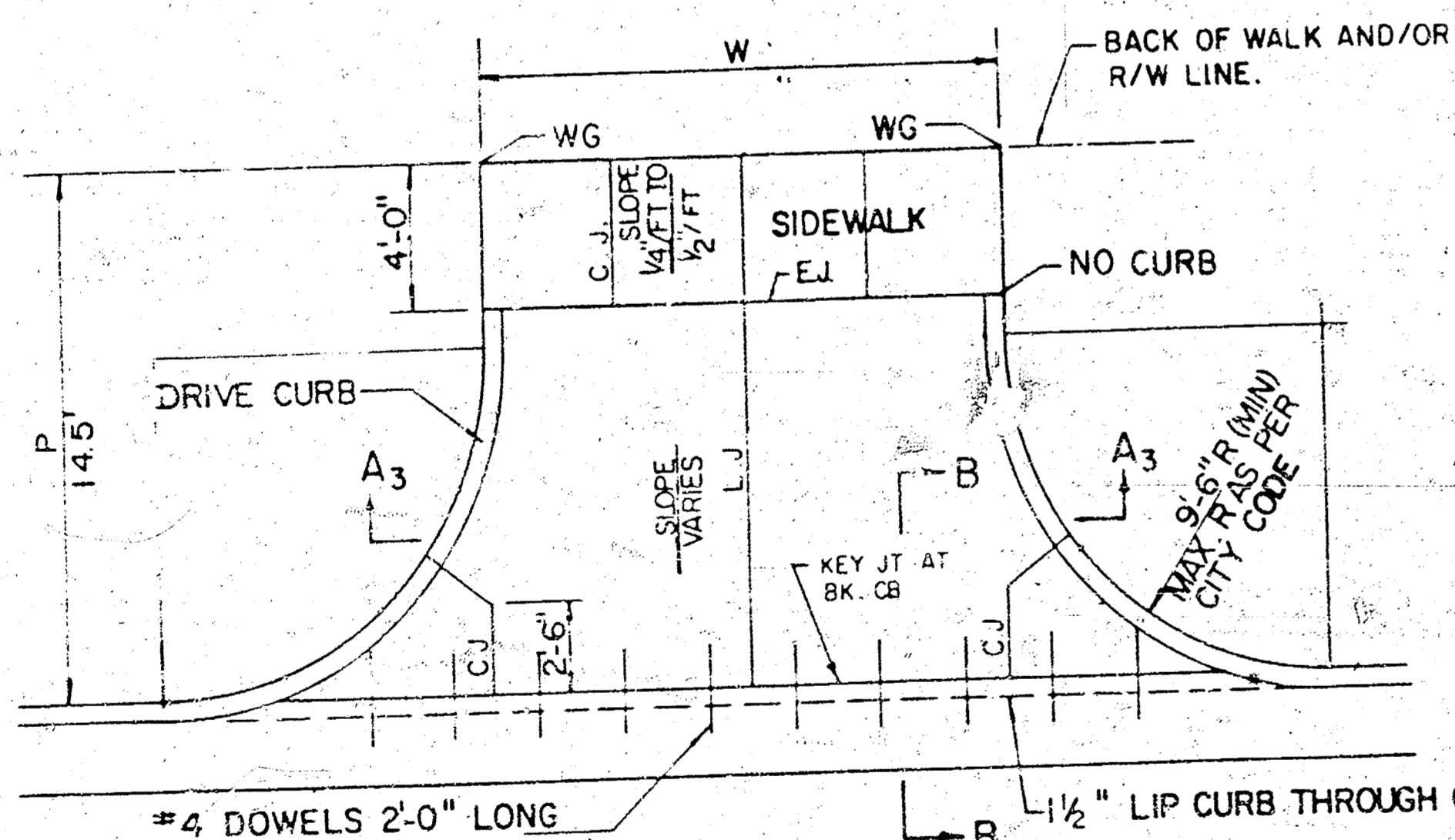


| PARKING WIDTH "P" | | 9' | 10' | 11' | 12' | 13' | 14.5' | 20' | 25' | 30' | 35' | 40' | 45' | 50' |
|-------------------|--|--------|--------|--------|--------|--------|-------|-------|-------|-------|-------|-------|-------|-------|
| ABSOLUTE | MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB | 0.35' | 0.35' | 0.40' | 0.45' | 0.60' | 0.60' | 1.35' | 1.85' | 2.35' | 2.85' | 3.35' | 3.85' | 4.35' |
| OPTIMUM | MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB | 0.35' | 0.35' | 0.40' | 0.45' | 0.60' | 0.70' | 1.04' | 1.30' | 1.56' | 1.82' | 2.08' | 2.34' | 2.60' |
| OPTIMUM | MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB | 0.19' | 0.21' | 0.23' | 0.25' | 0.27' | 0.30' | 0.42' | 0.52' | 0.62' | 0.72' | 0.82' | 0.92' | 1.02' |
| ABSOLUTE | MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB | -0.19' | -0.16' | -0.13' | -0.10' | -0.06' | 0.00' | 0.00' | 0.15' | 0.25' | 0.35' | 0.45' | 0.55' | 0.65' |

RADIUS RAMP DRIVES (P = 9.0' & GREATER)



BACK OF CURB DETAIL SECTION B-B (no scale)



| PARKING WIDTH "P" | | 14.5' | 20' | 25' | 30' | 35' | 40' | 45' | 50' |
|-------------------|---|-------|-------|-------|-------|-------|-------|-------|-------|
| ABSOLUTE | MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.80' | 1.35' | 1.85' | 2.35' | 2.85' | 3.35' | 3.85' | 4.35' |
| OPTIMUM | MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.70' | 1.04' | 1.30' | 1.56' | 1.82' | 2.08' | 2.34' | 2.60' |
| OPTIMUM | MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.30' | 0.42' | 0.52' | 0.62' | 0.72' | 0.82' | 0.92' | 1.02' |
| ABSOLUTE | MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.00' | 0.00' | 0.15' | 0.25' | 0.35' | 0.45' | 0.55' | 0.65' |

FULL RADIUS DRIVES (P = 14.5' & GREATER)

| PARKING WIDTH "P" | | 7' | 7.5' | 8' | 8.5' |
|-------------------|---|--------|--------|--------|--------|
| ABSOLUTE | MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.00' | 0.10' | 0.20' | 0.30' |
| OPTIMUM | MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CURB | 0.00' | 0.10' | 0.20' | 0.30' |
| OPTIMUM | MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CURB | -0.15' | -0.16' | -0.17' | -0.17' |
| ABSOLUTE | MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CURB | -0.25' | -0.20' | -0.20' | -0.20' |

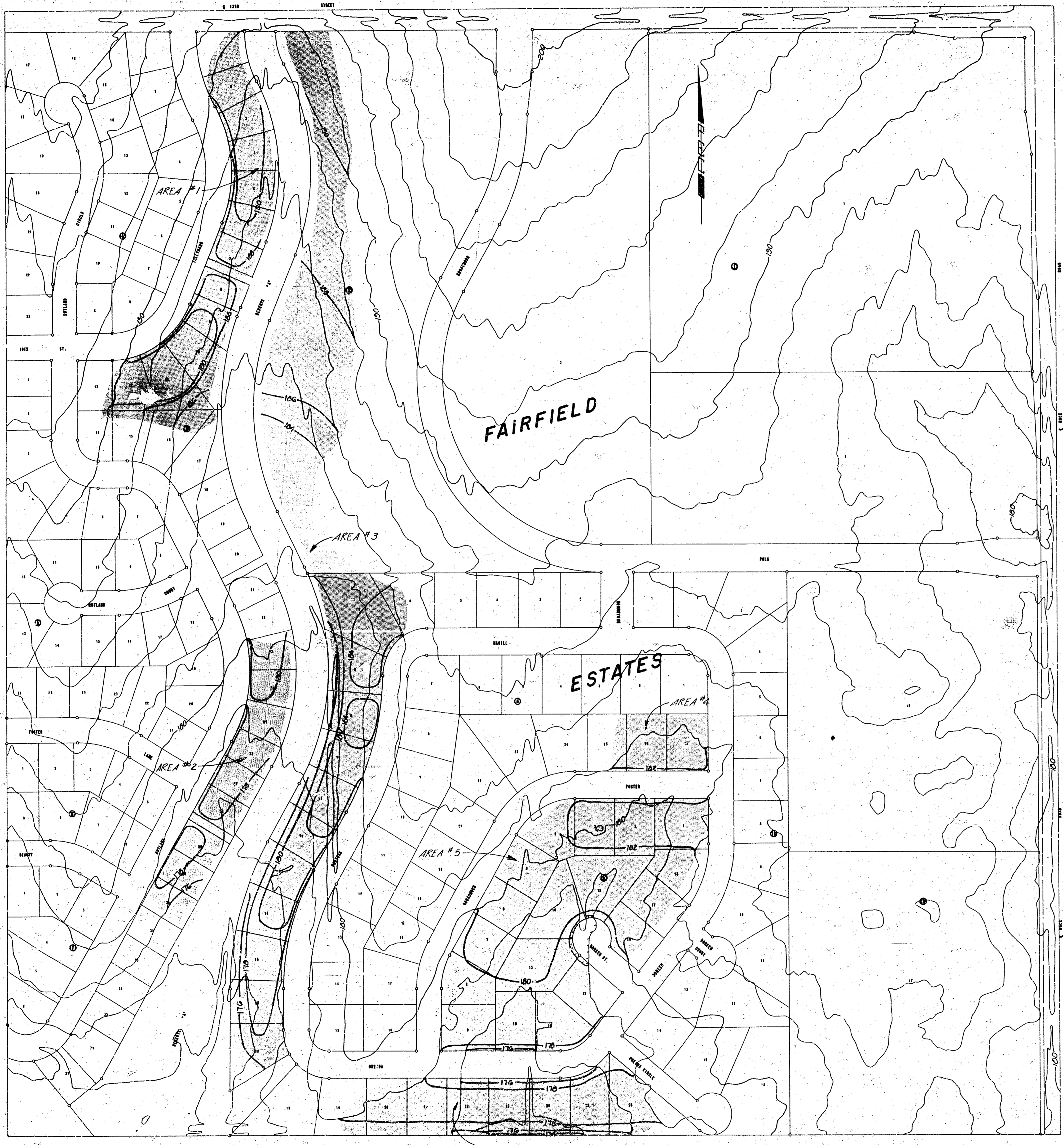
FULL RAMP DRIVE (P = 7.0' TO 8.5')

- GENERAL NOTES
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH ROLL CURB AND/OR FULL WALK.
 - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "P" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "P" DIMENSION GREATER THAN 24'.
 - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
 - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
 - DOWEL BARS SHALL BE OMITTED FROM THE KEYED CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
 - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
 - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
 - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" W#4 WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONFORMANCE.
 - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE BUTTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.

REVISED OCTOBER 1985
SCALE: 1" = 5'

STANDARD DRIVE ENTRANCES
FULL HEIGHT CURB
CITY OF WICHITA, KANSAS
PROJECT NUMBER
472-70-245-81527-000-000-001

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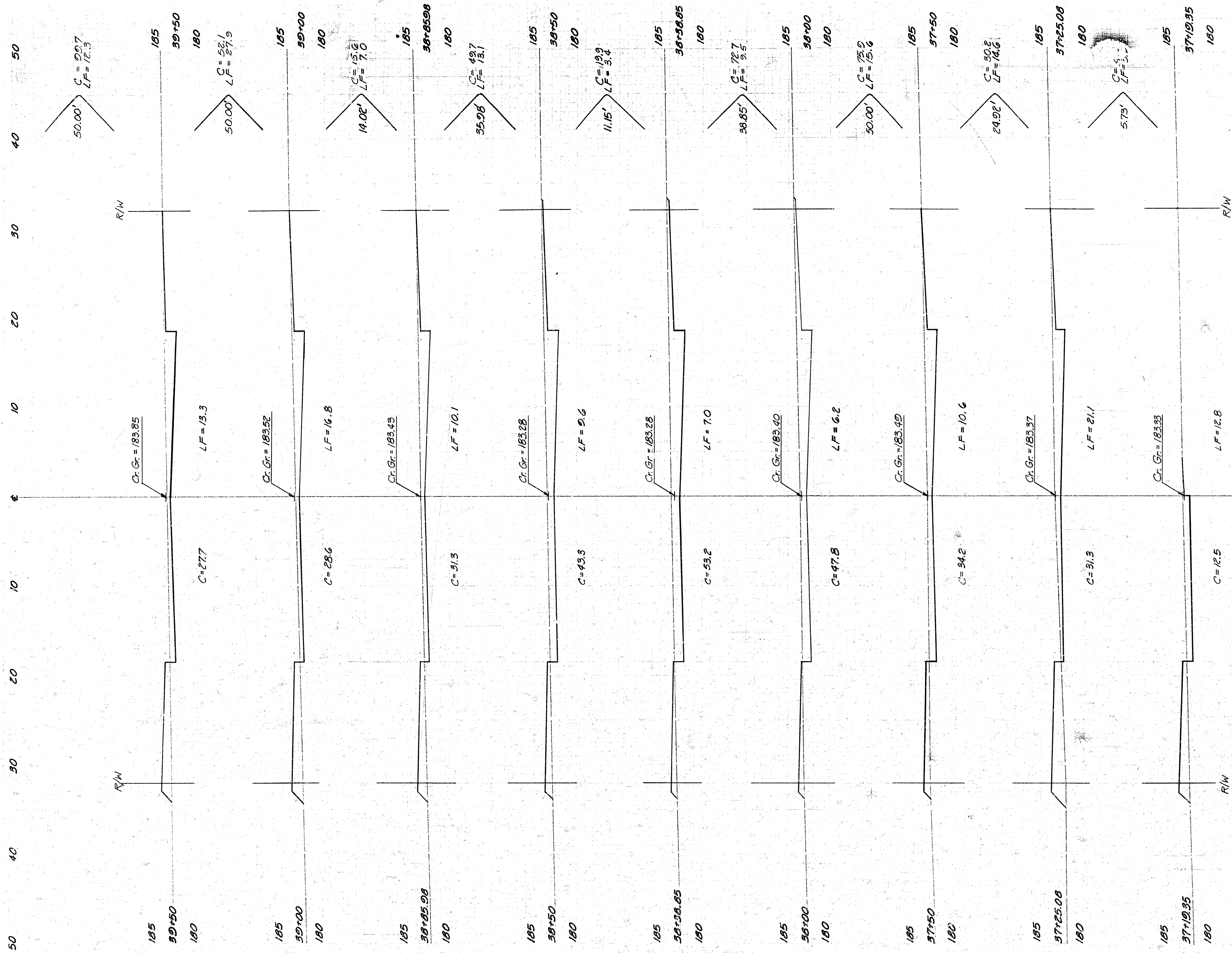
- NOTES**
- No fill shall be placed on Street Rights-of-Way.
 - Blade waste material smooth and grade to drain.
 - Contour Interval = 2'
 - Contractor shall coordinate wasting of excess materials with other contractors that may be on site.
 - Excess excavated material from this project shall be wasted within the areas shown hereon. Contractor shall waste excess material in the following sequence:
 - Area #4
 - Area #5
 - Area #6
 It is anticipated that only Waste Areas #4 & #5 will be utilized on this project.
 - All staking, inspection, testing and approval of the disposal of excess excavated material on private property, as shown hereon, shall be the sole responsibility of the landowner. Contact Mr. John Engel at Kansas General Properties, Inc. Ph. No. 681-2171.

EARTHWORK SUMMARY

| | | |
|--|----------------|-----------------------|
| Excavation: | Cross Sections | 2,411 Cu. Yds. |
| | 10% | 241 Cu. Yds. |
| | Total | 2,652 Cu. Yds. |
| Compacted Fill: | Cross Sections | 45 Cu. Yds. |
| | 10% | 5 Cu. Yds. |
| | Total | 50 Cu. Yds. |
| Manipulated Fill: | Cross Sections | 82 Cu. Yds. |
| | 10% | 8 Cu. Yds. |
| | Total | 90 Cu. Yds. |
| Loose Fill: | Cross Sections | 598 Cu. Yds. |
| | 10% | 60 Cu. Yds. |
| | Total | 658 Cu. Yds. |
| Approx. Excess Material = (20% Shrinkage Assumed for Compacted & Manipulated Fill) | | 1,826 Cu. Yds. |

| | | | |
|--|-------------------|----------|------|
| No. | Revision | By | Date |
| WASTE DISPOSAL PLAN | | | |
| (FOR EXCESS EXCAVATED MATERIAL) | | | |
| FOR INFORMATION ONLY | | | |
| PROFESSIONAL ENGINEERING CONSULTANTS, P.A. | | | |
| ENGINEERS WICHITA, KANSAS | | | |
| Designed by: C.S.B. | Job No. 235927 | 10 of 14 | |
| Drawn by: V.J.K. | Date: April, 1986 | | |

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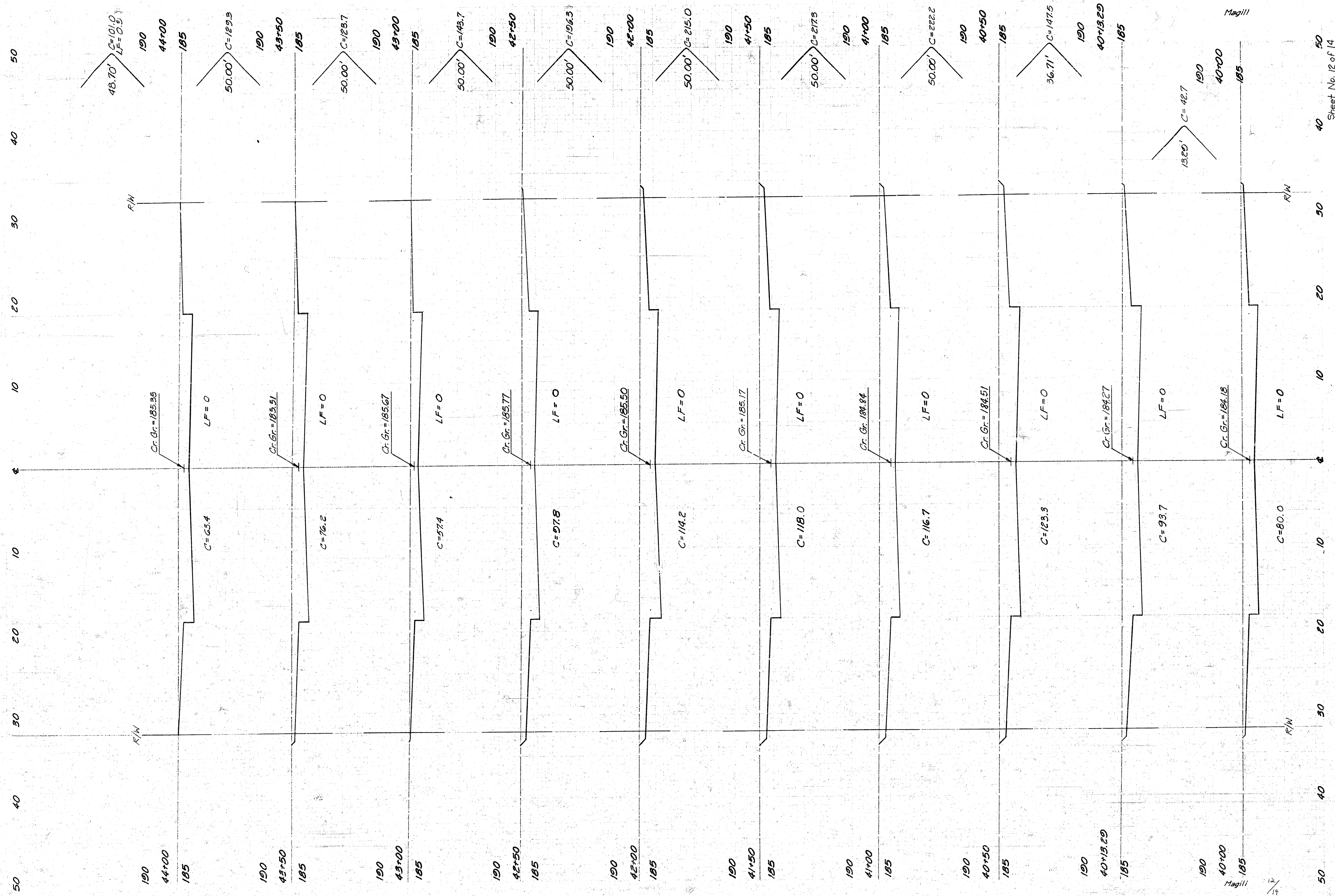
Postoak & Magill

185
37+00
180

Postoak & Magill

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Sheet No. 11 of 14



48.70' C=101.0
LF=0.5
190
44+00
185

50.00' C=129.3
190
43+50
185

50.00' C=123.7
190
43+00
185

50.00' C=143.7
190
42+50
185

50.00' C=126.3
190
42+00
185

50.00' C=215.0
190
41+50
185

50.00' C=217.3
190
41+00
185

50.00' C=222.2
190
40+50
185

36.71' C=147.5
190
40+13.29
185

1380' C=42.7
190
40+00
185

R/W

R/W

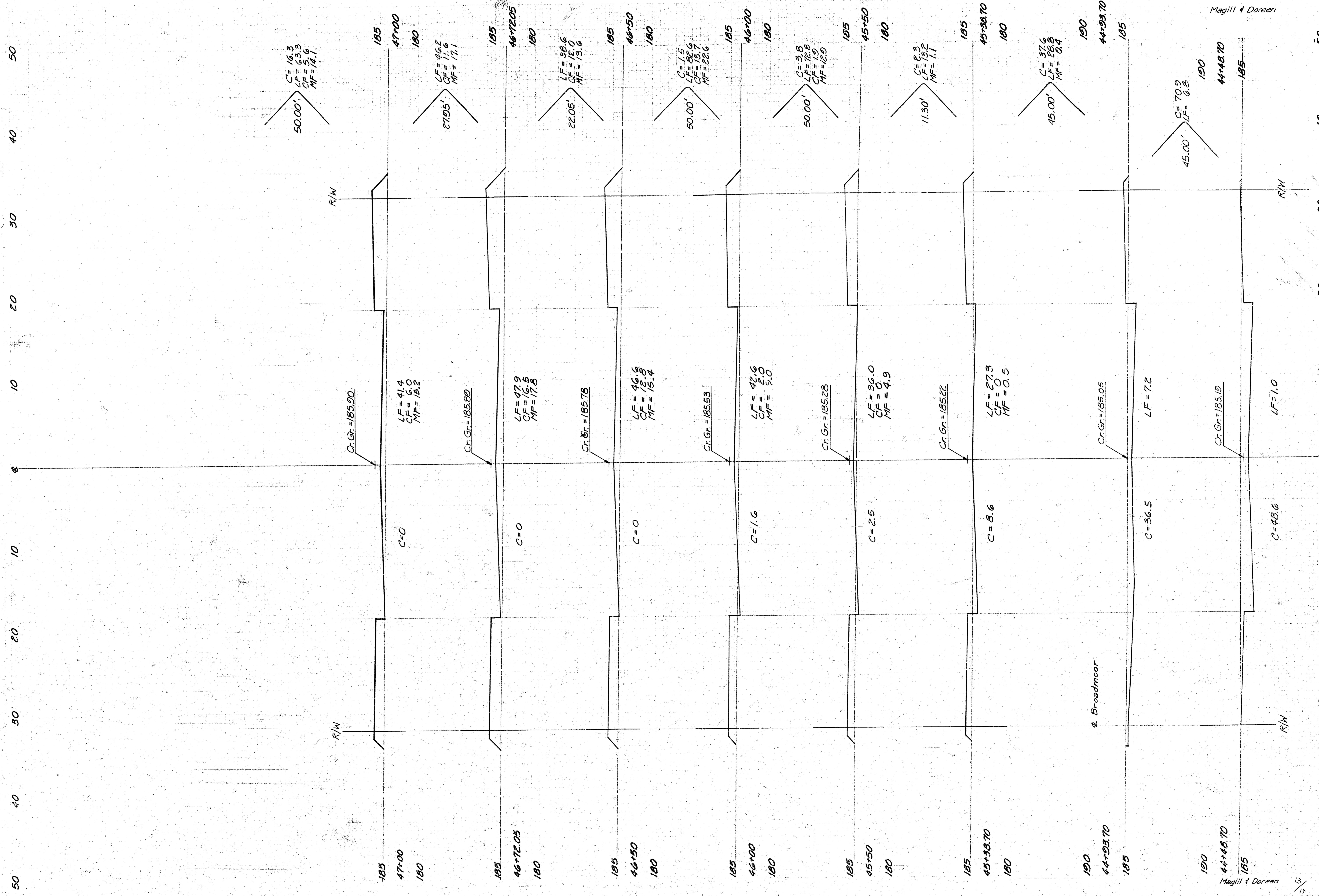
R/W

R/W

Magill

Magill

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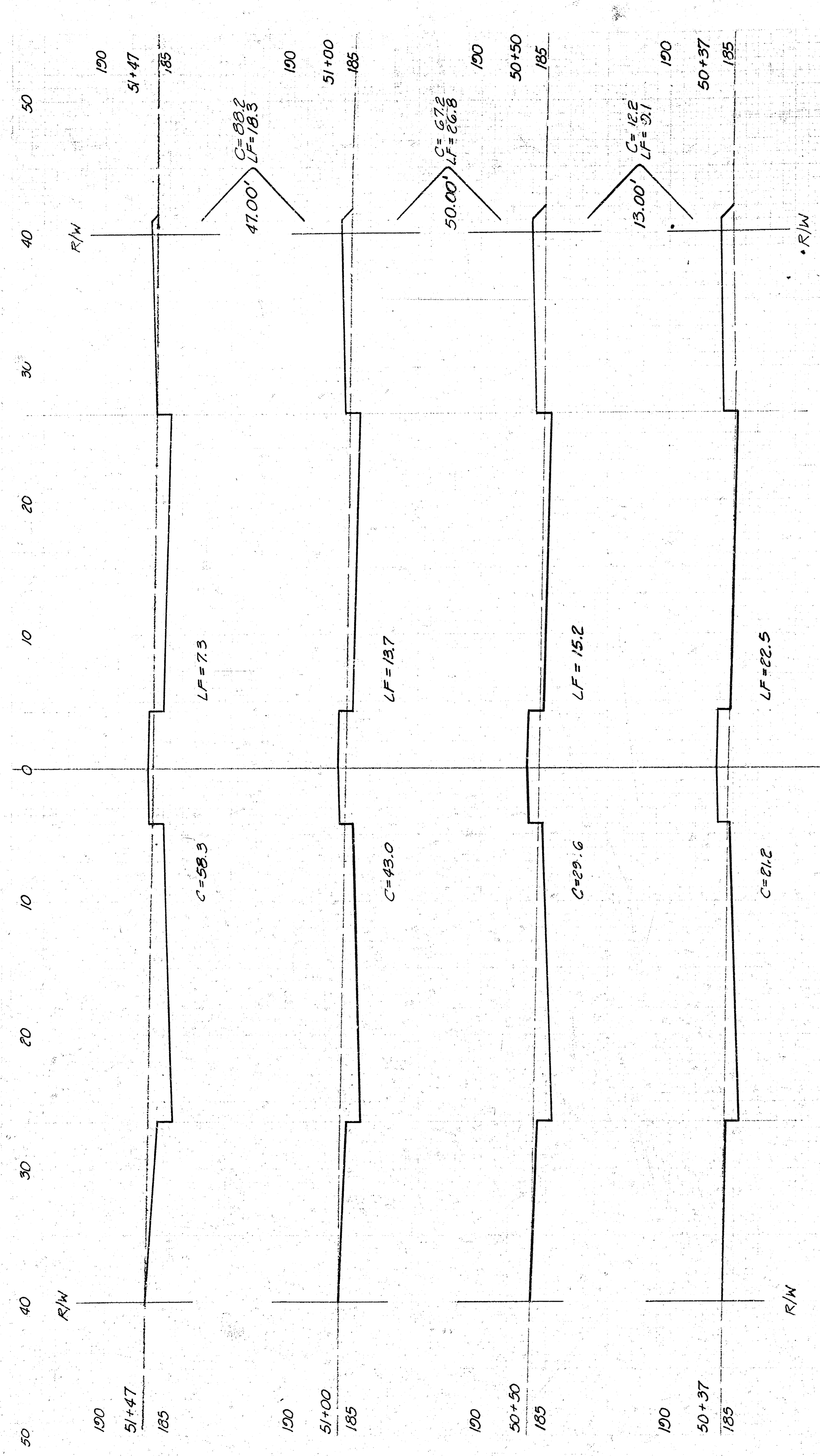
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Sheet No. 13 of 14

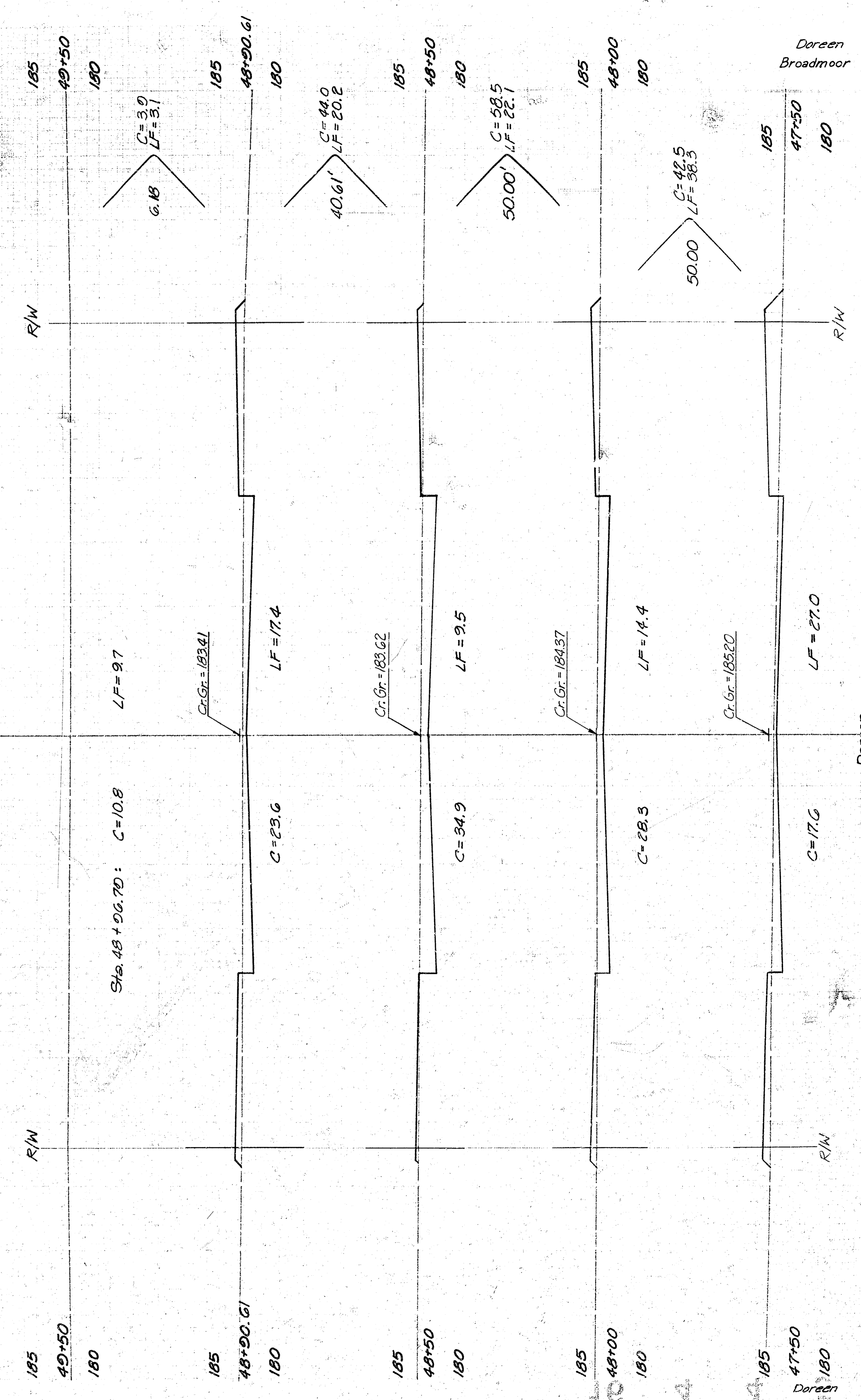
Magill & Doreen

13/14

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Broadmoor



Doreen Broadmoor

Doreen

Doreen

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