

PRIVATE STREET IMPROVEMENTS
to serve
BAY COUNTRY 2ND ADDITION

Project Number
129 PPP 607879

CITY OF WICHITA, KANSAS
Michael E. Lindebak, P.E. City Engineer
OCTOBER, 2001

GENERAL NOTES:

1. Contractor is required to provide notice to utility companies a minimum of twenty-four (24) hours prior to any excavation, as follows:

Kansas One-Call 887-2470

The Contractor must notify the following in case of an emergency:

Cox Communications 262-0661
Kansas Gas Service 383-8600
K.G.E. 383-8600
Peoples Gas Company 1-800-303-0752
Southwestern Bell Telephone Company 1-800-344-7233
City of Wichita Water Department 268-4908
City of Wichita Traffic Engineering 268-4446

2. Utility service lines, poles, valve boxes, meters, and etcetera are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the Contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction.

3. Trees and shrubs in public right-of-way which are in direct conflict with proposed new construction shall be removed by the Contractor with the Engineer's approval. Trees and shrubs which are not in direct conflict with proposed new construction shall be saved and protected from damage.

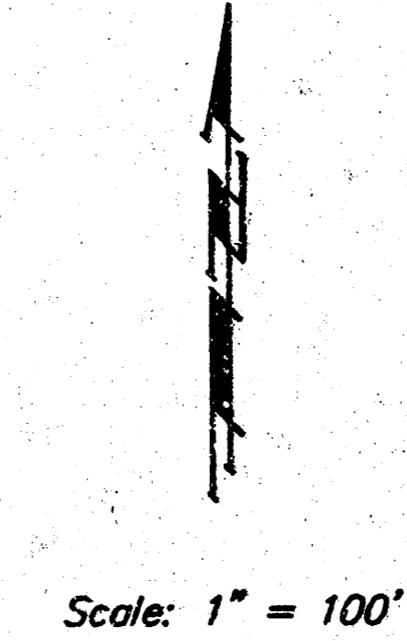
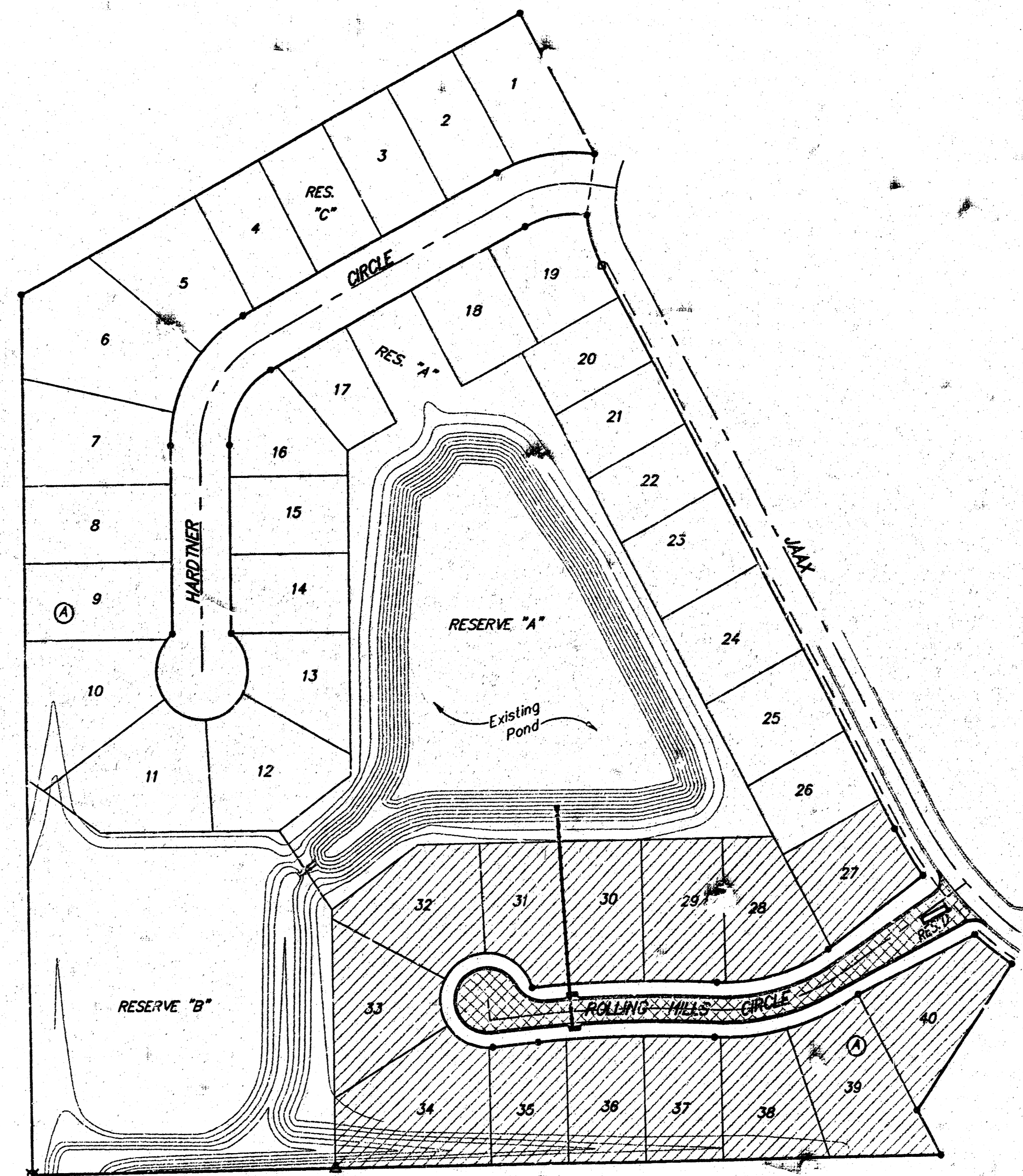
4. All areas disturbed by construction shall be seeded at 300 lbs./acre with Rye Grass immediately following construction in that area. Contractor shall prepare ground per City specifications.

5. Rubble from the removal of miscellaneous structures and excess excavation which is to be wasted shall be disposed of on sites to be provided by the Contractor. These sites shall be approved by the Engineer as to suitability, appearance and site location. Locations that, in the opinion of the Engineer, will leave an unsightly appearance will not be approved. All disposal sites must be approved by the Kansas Department of Health and Environment. Material either stockpiled or disposed of in a flood plain would require a Kansas State Board of Agriculture permit. Any material dumped in waters of the United States or wetlands is subject to U.S. Corps. of Engineers permitting regulations. Any material buried or stockpiled beyond approved construction limits would require additional archaeological investigations unless buried in a previously approved borrow location.

6. The Contractor shall be responsible for preserving property irons. The Contractor will be required to re-establish any property irons which are damaged or destroyed by his construction operations. Such irons shall be re-established by a licensed land surveyor in accordance with state law.

7. A saw cut of at least one-half the depth of existing surface courses of one-fourth the depth of the existing total pavement thickness shall be provided at locations where proposed construction abuts an existing surface course or pavement for which partial removal of that surface or pavement is required. Sawed joint to facilitate removal within three (3) feet of existing joints will not be permitted and for such instances the limits of removal shall extend to the existing joint. Such saw cuts will not be paid for directly and this cost shall be considered as subsidiary to the removal of the surface or pavement.

8. All excess excavation shall remain on site and be stockpiled at a location determined by the engineer.



BENCHMARKS:

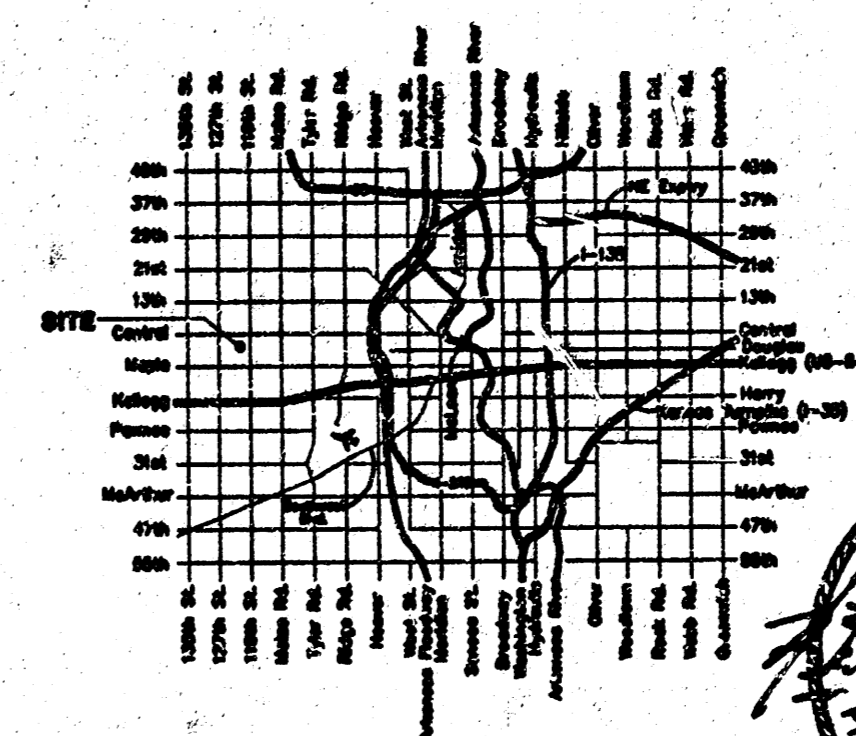
"B" Cut - Top of Curb, 14.5'±
South of the S.W. Corner of Lot 37,
Block 4, Bay Country.

Elev. = 142.48 (City Datum)

Sheet Index

Title Sheet	1
29" Pavement Detail	2
Sign Detail	3
Rolling Hills Cir.	4-5
Soil Erosion BMPs	6-7
Plat	8

Benefit District:
Proposed Streets:



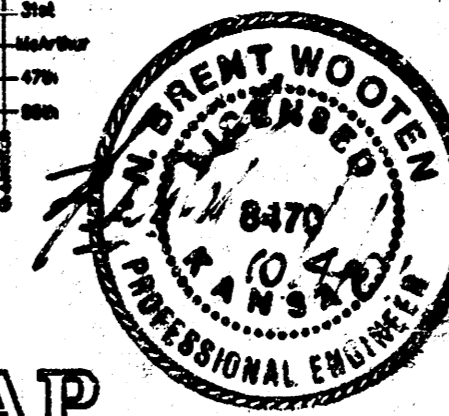
VICINITY MAP

APPROVED AS NOTED
BY CITY ENGINEER OF WICHITA

Paving VRH 10/5/01

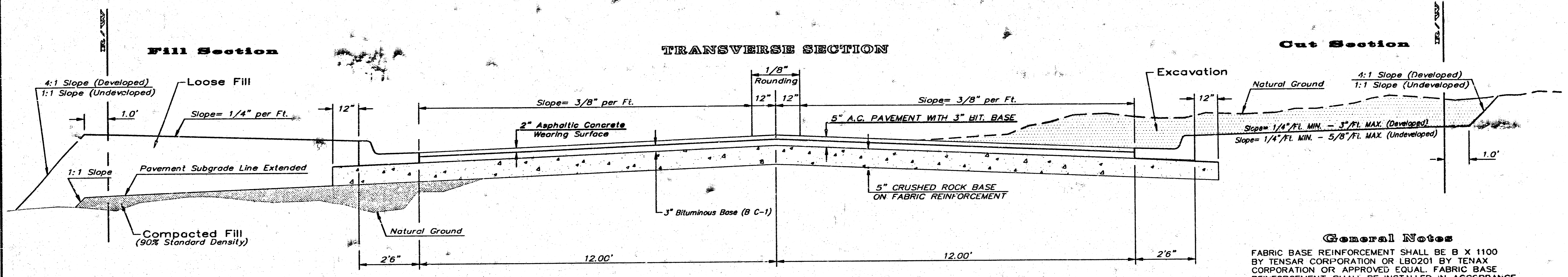
NOTE TO CONTRACTORS

Inspection and testing for this project is to be provided by a Licensed Consulting Engineering Firm under contract with the Owner/Developer. Said inspection to be in accordance with the City of Wichita standard construction engineering practices and certified by a Licensed Professional Engineer. No work shall be performed in dedicated easements or public right-of-way by the Contractor without such inspection nor shall any work be commenced without written authorization by the City Engineer. All Construction and Materials shall comply with the City of Wichita Specifications and Standards (on file and available in the City Engineer's Office).



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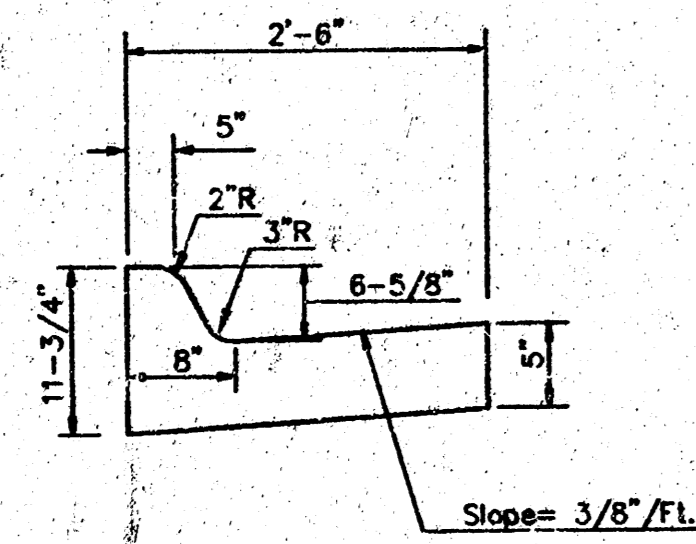
TYPICAL 29' B-B PAVEMENT DETAILS



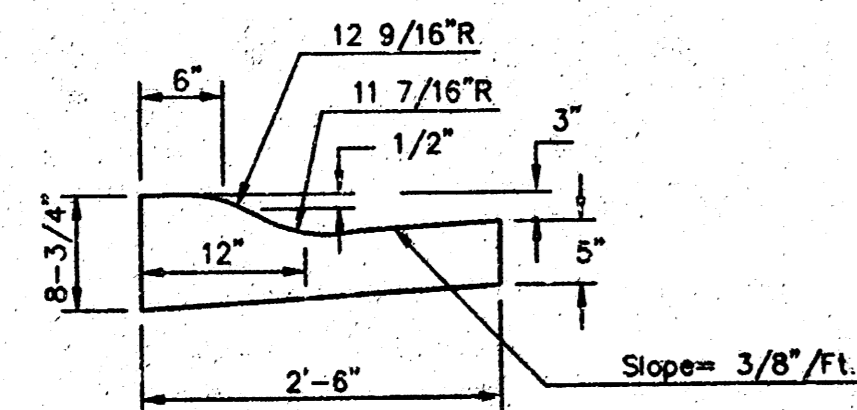
CRUSHED ROCK GRADATION REQUIREMENTS PERCENT OF AGGREGATE RETAINED

2-1/2"	0
3/4"	20 - 60
#4	50 - 80
#40	80 - 94
#200	90 - 98

ROCK QUALITY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY THE KDOT 1990 EDITION STANDARD SPECIFICATION SUBSECTION 1102 FOR DURABILITY CLASS I.

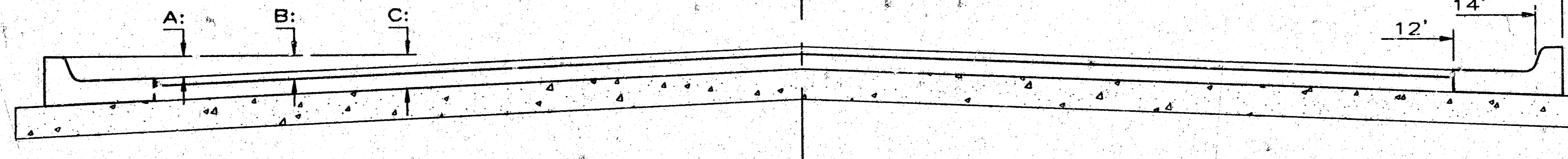


COMBINED CURB & GUTTER

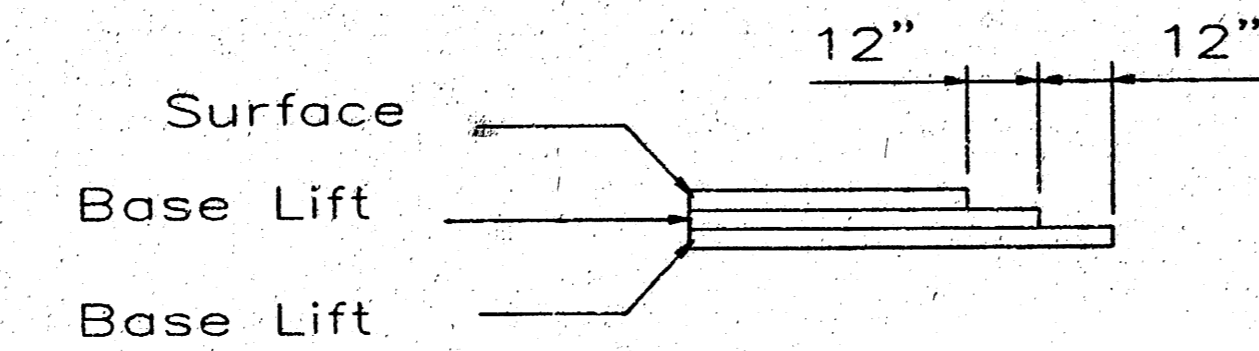


COMBINED ROLL TYPE CURB & GUTTER

ELEVATION OFFSETS

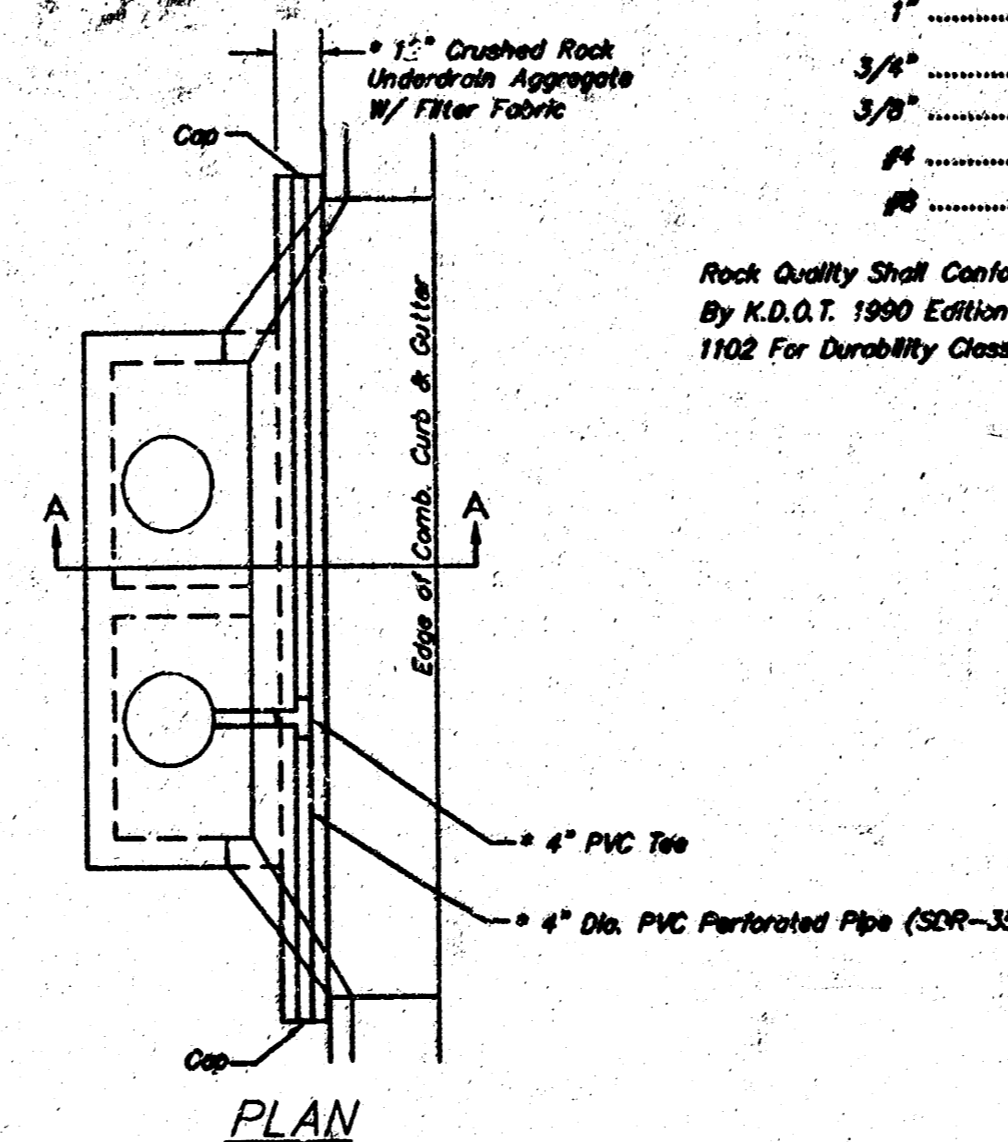


	DISTANCE FROM CENTERLINE (L.T. & RT.)										
	0'	2'	4'	6'	8'	10'	12'	14'	14.5'	15.5'	
A: Top of Curbs to Top of Surface Lift	0.13	0.18	0.24	0.30	0.36	0.43	0.49	—	—	—	—
B: Top of Curbs to Top of Upper Base Lift	0.30	0.35	0.41	0.47	0.53	0.60	0.66	—	—	—	—
C: Top of Curbs to Top of C.R. Subgrade	0.55	0.60	0.66	0.72	0.78	0.85	0.91	0.97	0.98	1.01	



TRANSVERSE CONSTRUCTION JOINTS

Transverse construction joints shall be constructed in flexible base pavements at locations where pavement joints existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).



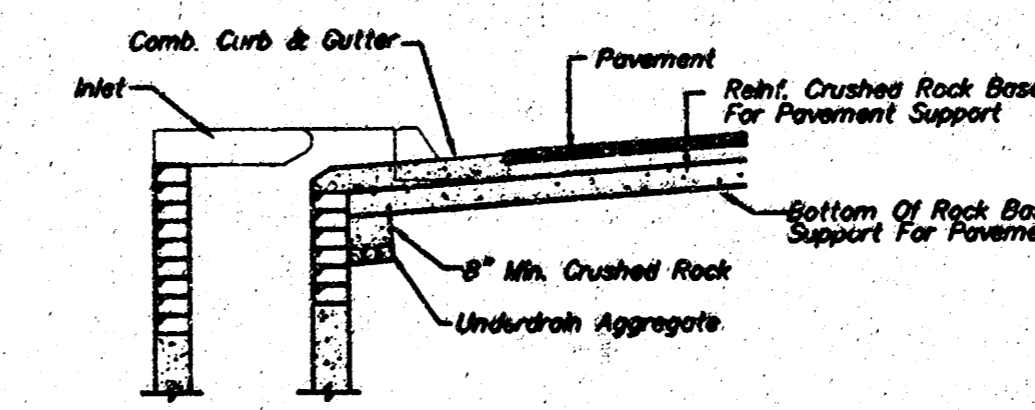
PAVEMENT UNDERDRAIN DETAIL
NOT TO SCALE

UNDERDRAIN AGGREGATE Percent of Aggregate Retained

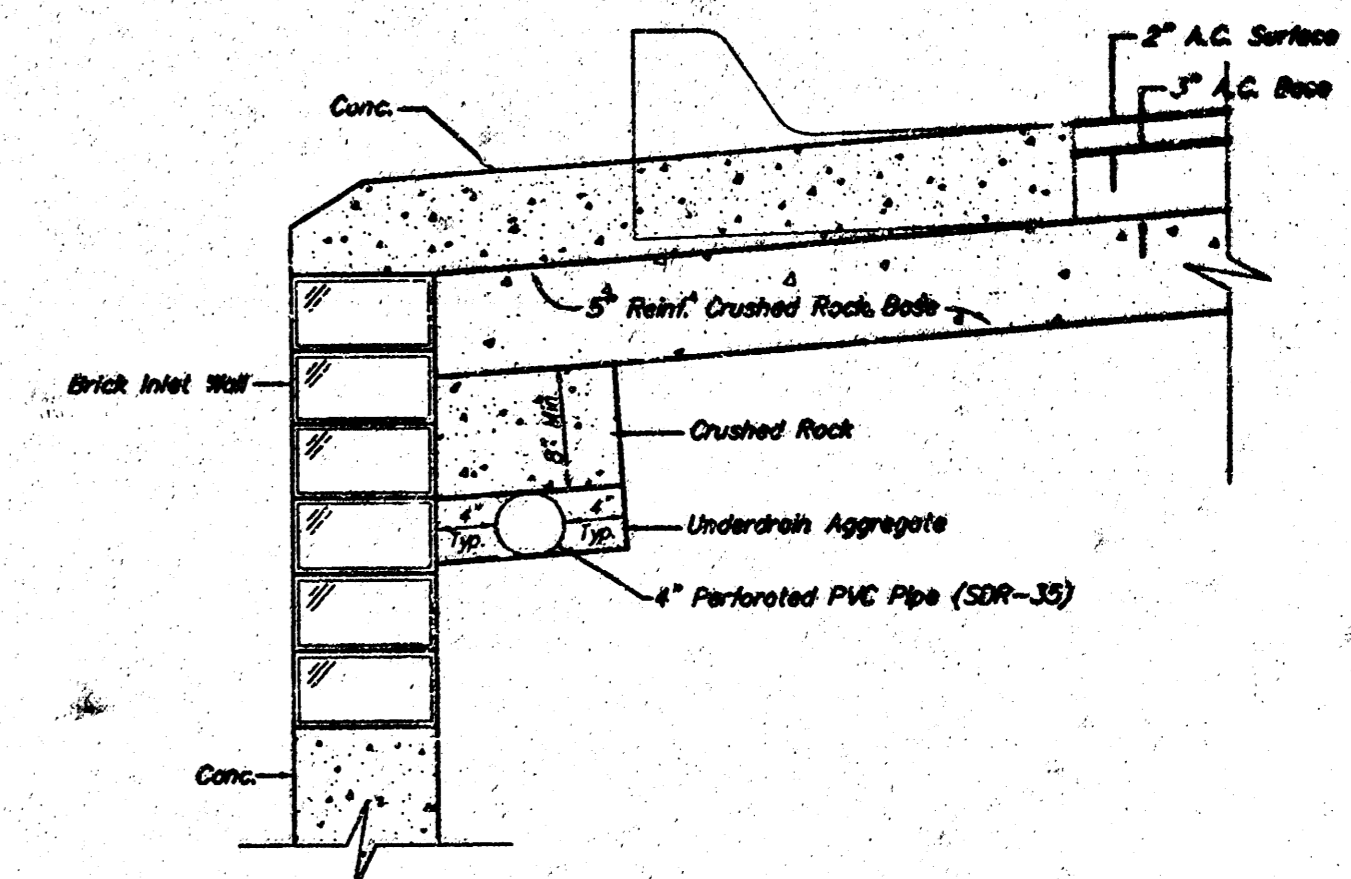
1"	0
3/4"	0 to 10
3/8"	45 to 80
#4	90 to 100
#8	95 to 100

Rock Quality Shall Conform To The Requirements Specified By KDOT 1990 Edition Standard Specification Subsection 1102 For Durability Class I.

NOTE: Place 4" PVC Perforated Pipe at all drainage sump locations. Cost of Underdrain System to be incidental to the Reinforced Crushed Rock Subgrade. Inlet Type May Vary From That Shown.



SECTION A-A
(Min. 16 Perforations Per Lh. Ft. @ 1/4" Dia.)
Perforations To Be on Bottom Half



TRENCH DRAIN DETAIL FOR RES. STREETS
NOT TO SCALE

General Notes

FABRIC BASE REINFORCEMENT SHALL BE B X 1100 BY TENAX CORPORATION OR LBO201 BY TENAX CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

5 INCH Residential Asphaltic Concrete
Pavement w/ Crushed Rock Base on Fabric Reinforcement
City of Wichita, Kansas

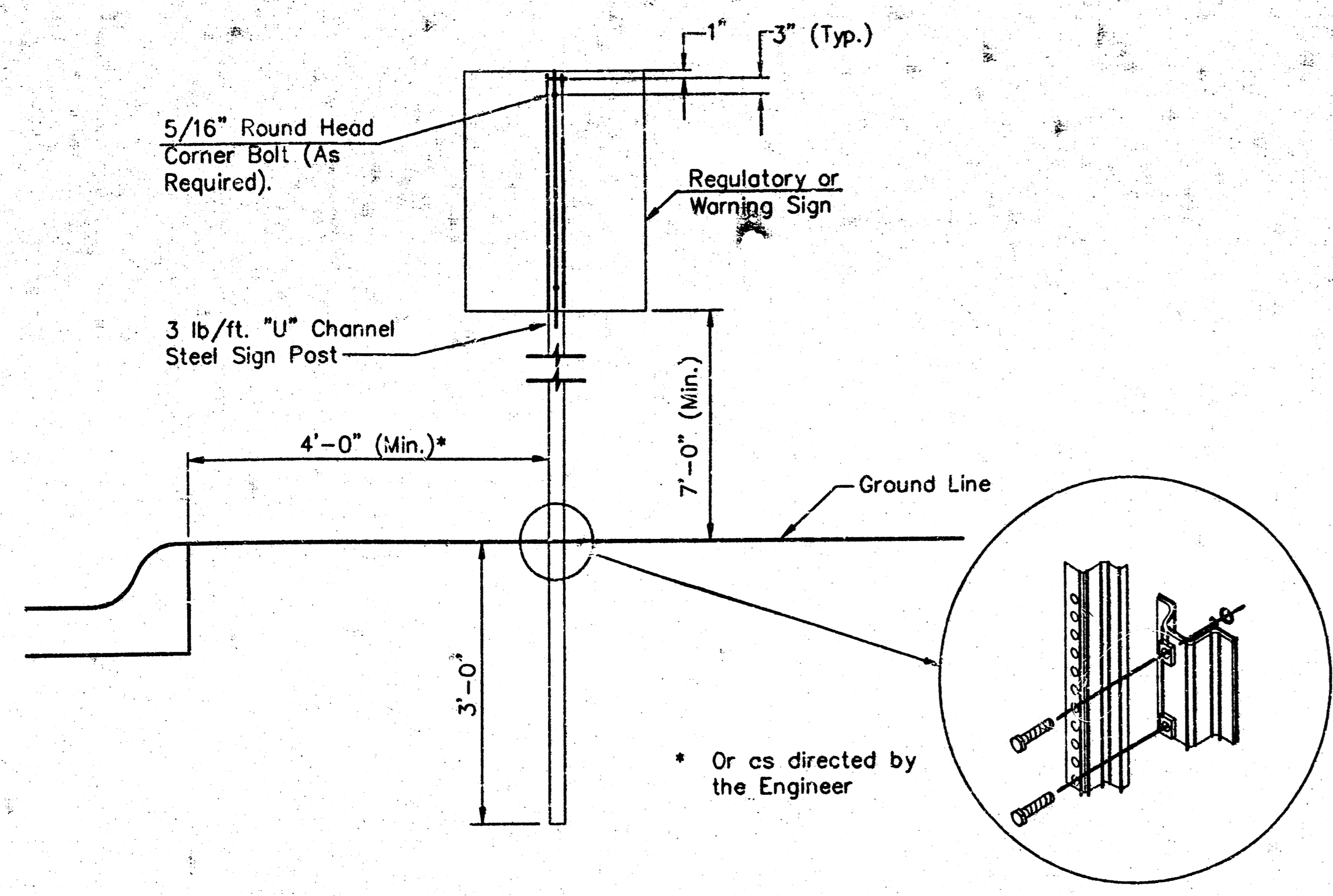
BAUGHMAN COMPANY P.A.
ENGINEERING, SURVEYING, & PLANNING
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PROJECT NUMBER
129 PPP 607879

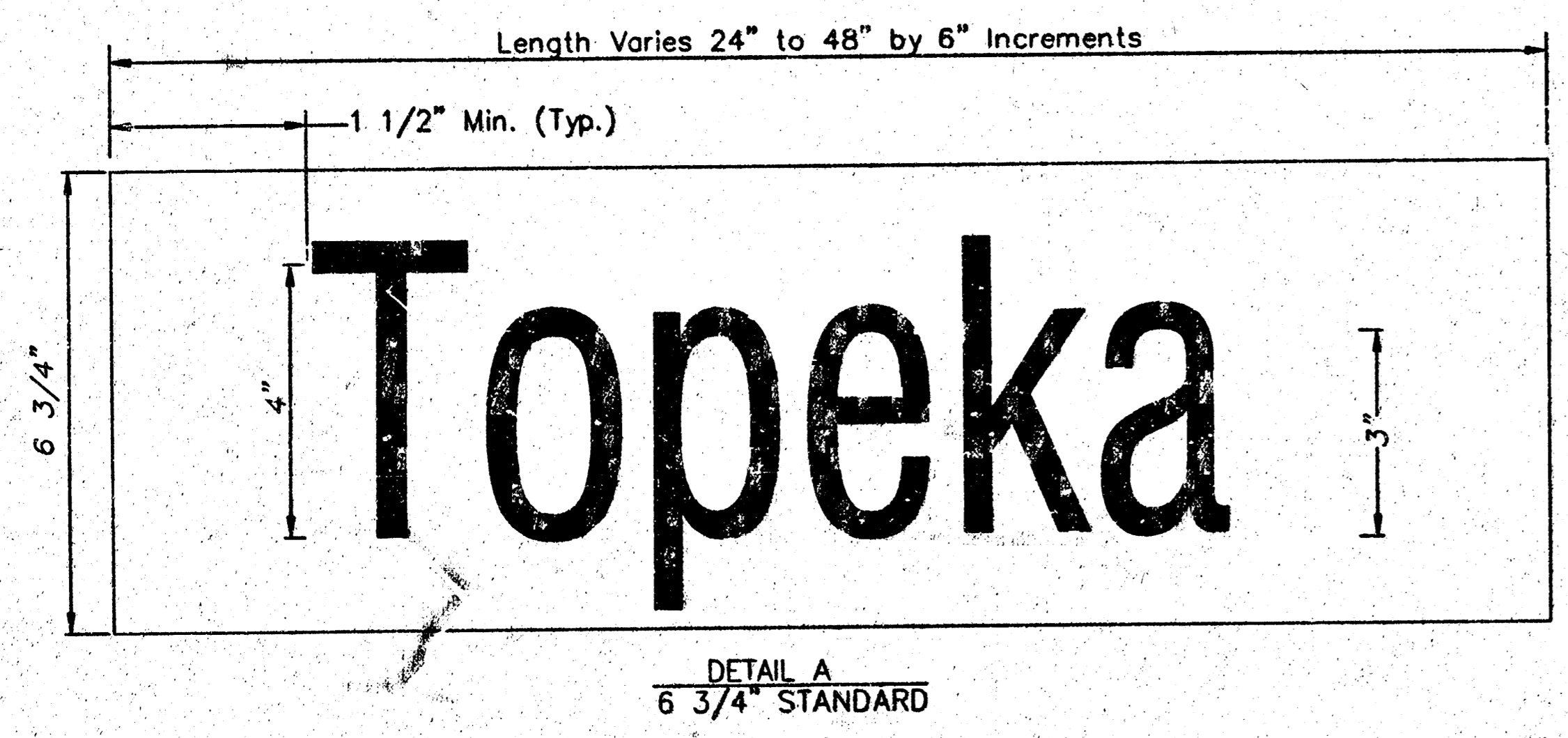
DESIGN C.O.W.	DRAWN Staff	APPROVED	DATE	SCALE
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SHEET
OF 2

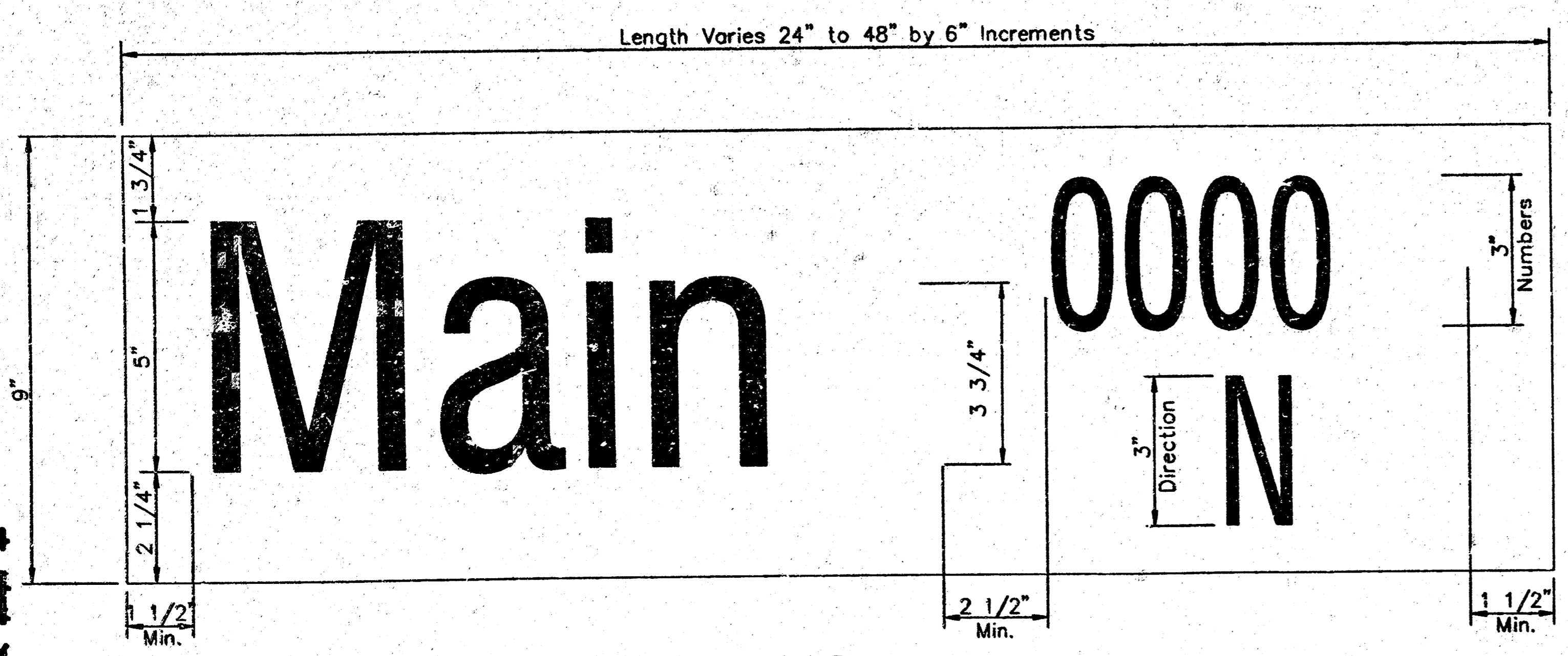
FHWA REG NO.	STATE	PROJECT NO.	YEAR	SHEET NO.	SHEETS
7	KANSAS				



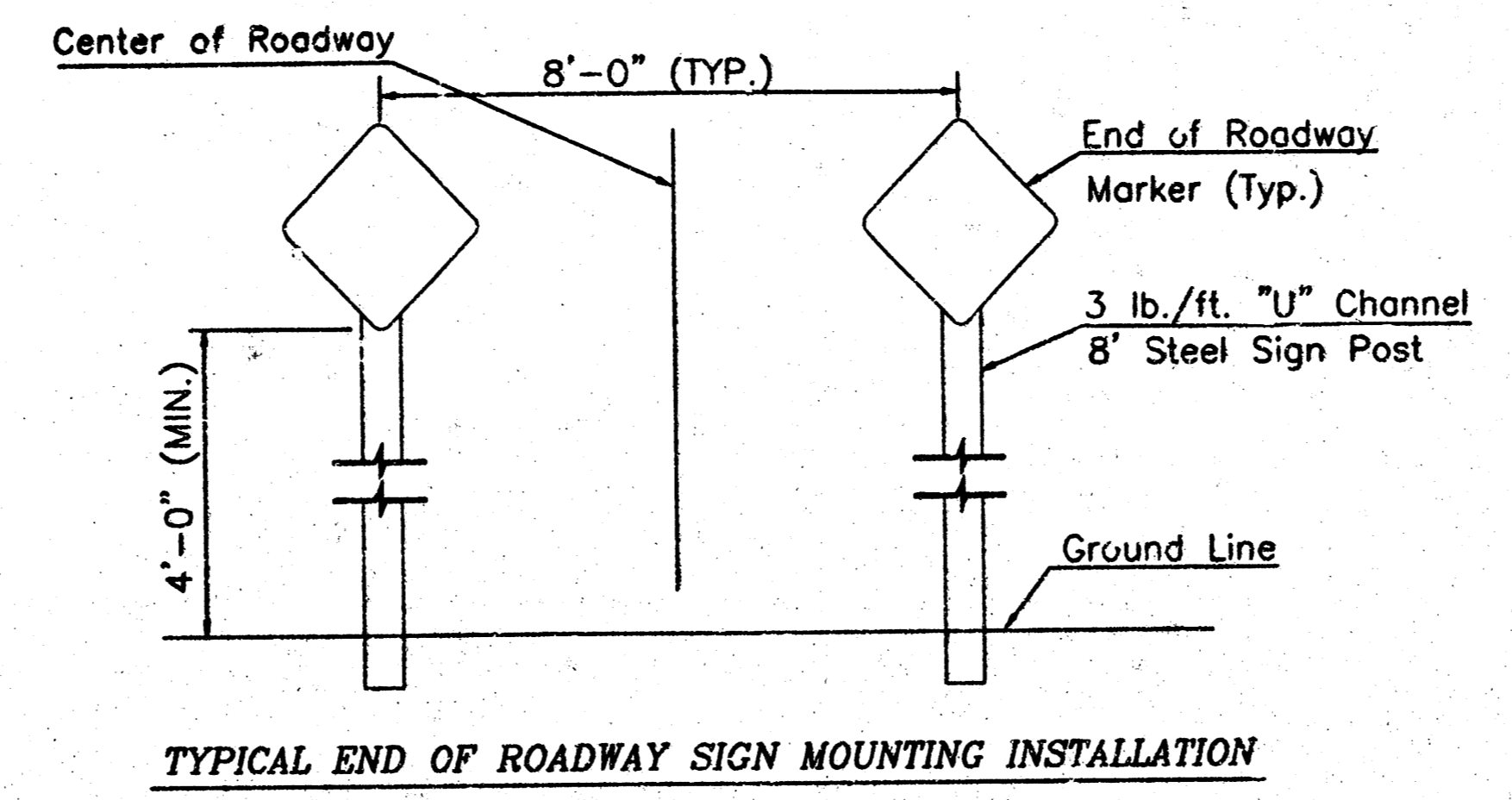
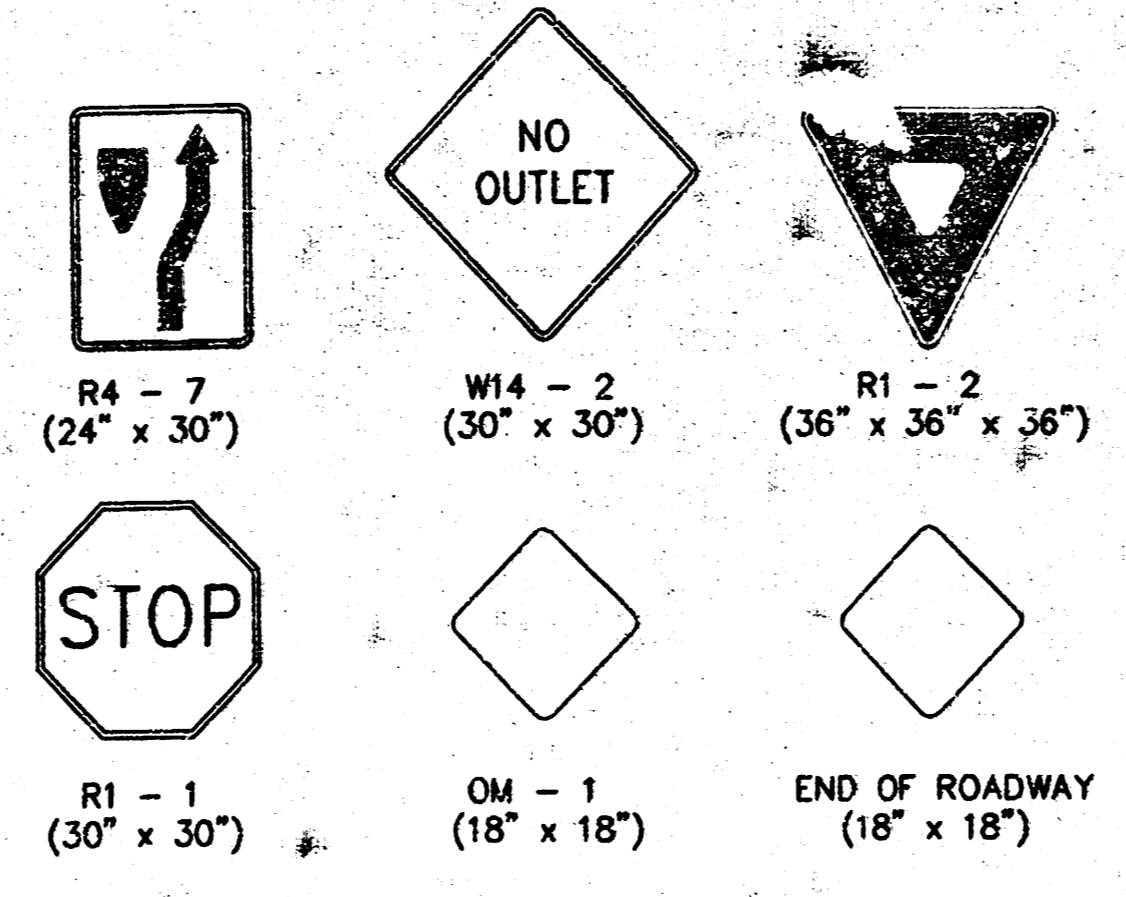
TYPICAL TRAFFIC CONTROL SIGN MOUNTING INSTALLATION
CURB AND GUTTER SECTION



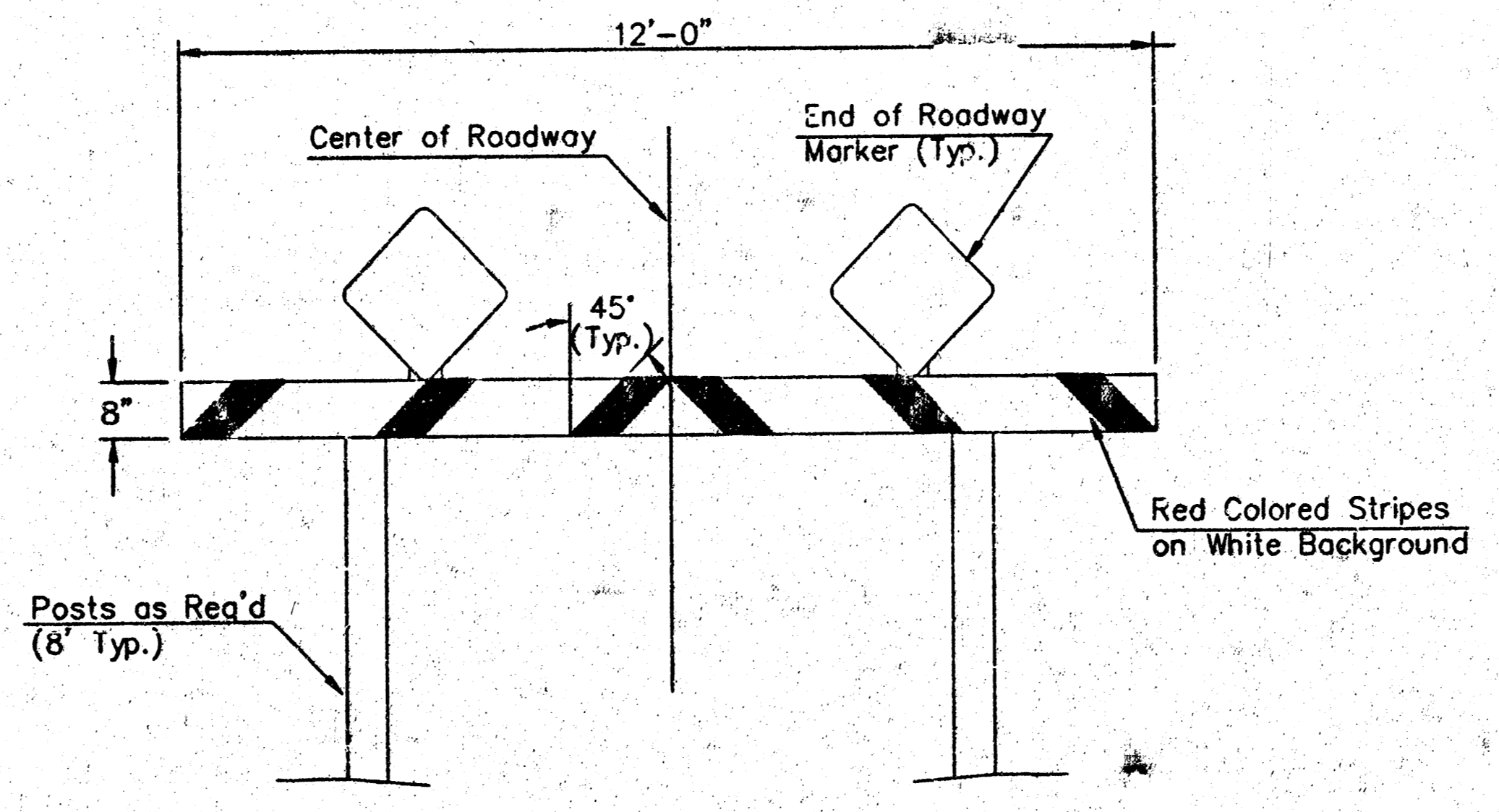
DETAIL A
6 3/4" STANDARD



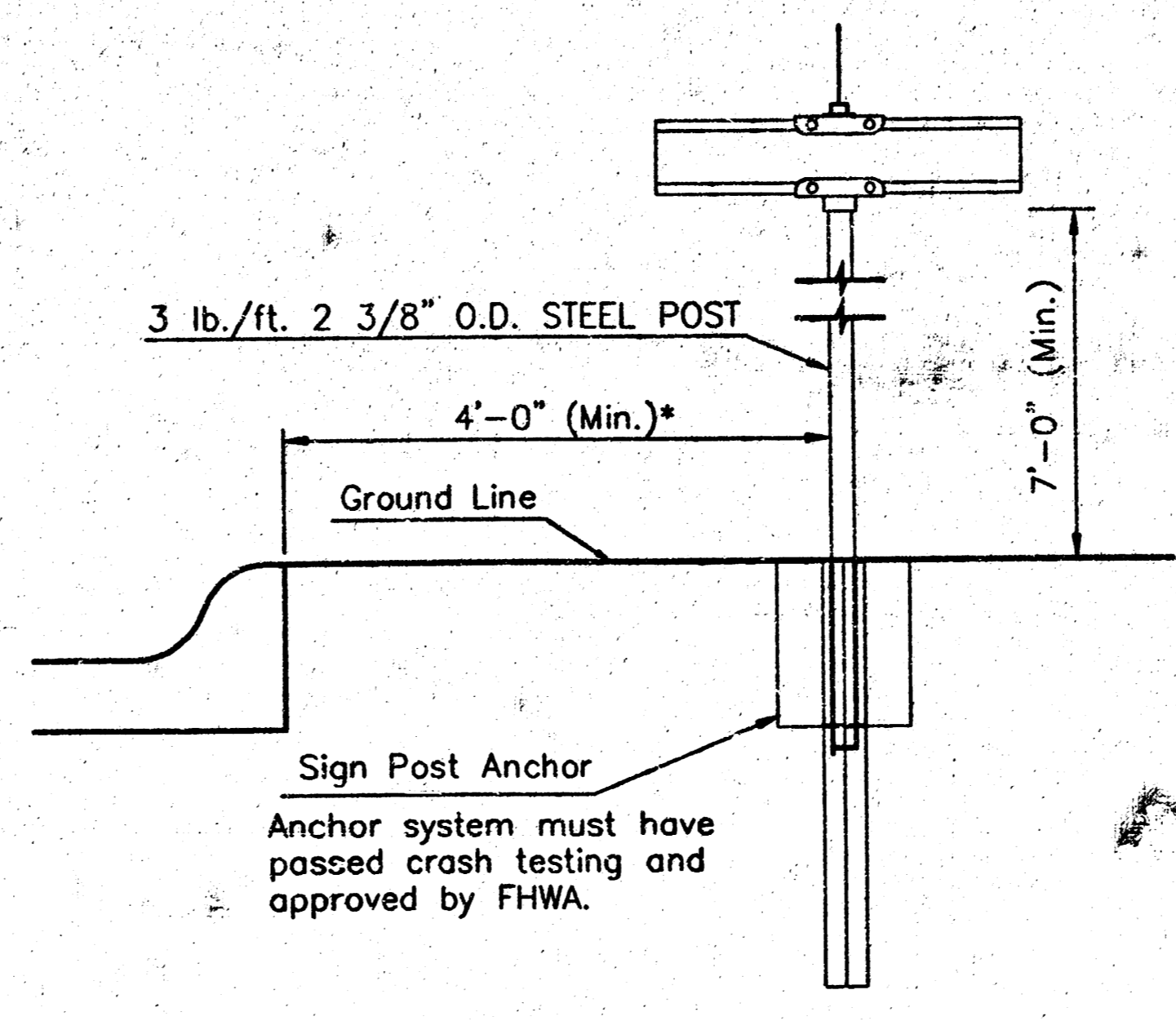
DETAIL B
9" METRO



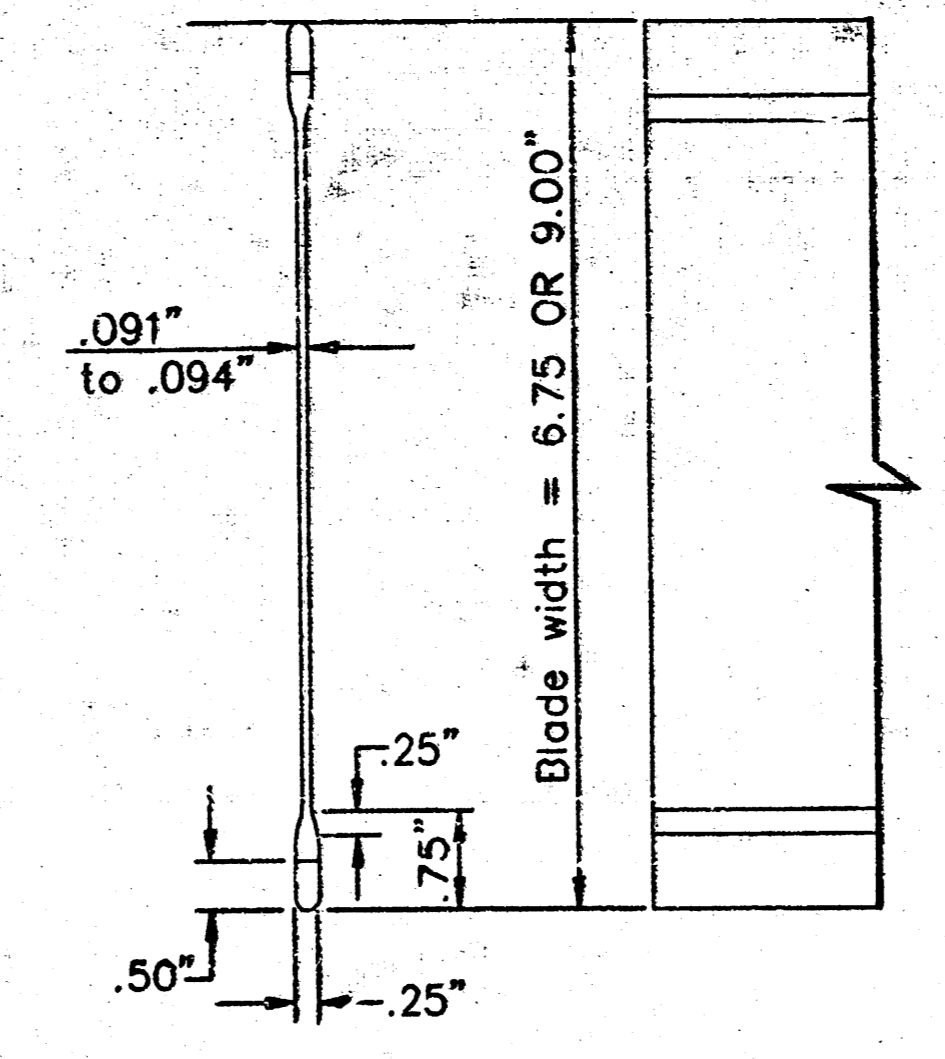
TYPICAL END OF ROADWAY SIGN MOUNTING INSTALLATION



TYPE I BARRICADE DETAIL W/ E.O.R. MARKER



TYPICAL STREET NAME SIGN MOUNTING INSTALLATION
CURB AND GUTTER SECTION



STREET NAME SIGN
BLADE DETAILS

STATION	OFFSET	SIGN	QUANTITY*
5+23	24' LL	SNS	1
5+33	43' RL	SNS	1
4+97.63	5.94' RL	R4-7 & OM-1	1
5+20.42	8.77' RL	R4-7 & OM-1	1
TOTAL			1

* FOR INFORMATION ONLY

STREET NAME	NO. BLADES REQ'D 6 3/4" STD.	9" METRO
ROLLING HILLS	1	
JAAX	1	
ROLLING HILLS DR 12601 - 12629		1

NOTE: REFERENCES BELOW TO "STANDARD SPECIFICATIONS" DENOTE "STANDARD SPECIFICATION FOR STATE ROAD AND BRIDGE CONSTRUCTION EDIFROM 1990" BY THE KANSAS DEPARTMENT OF TRANSPORTATION.

- POST ANCHORS: POSTS SHALL BE ANCHORED WITH A WELDING BASE POST SUPPORT AS DETAILED.
- POSTS FOR TRAFFIC CONTROL SIGNS: POSTS SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1620 OF THE STANDARD SPECIFICATIONS EXCEPT THAT ALL POSTS SHALL WEIGH 3 LBS./FOOT MINIMUM.
- POSTS FOR STREET NAME SIGNS (SNS): POSTS SHALL BE 9 FEET LONG, CONSTRUCTED FROM 2 3/8" O.D. GALVANIZED STEEL PIPE WEIGHING A MINIMUM OF 3 LBS./FOOT. POSTS SHALL BE POSITIONED SO THAT THE BOTTOM BLADE IS 7 FEET ABOVE GRADE.
- POSTS FOR END OF ROADWAY SIGN TO BE 8' LONG AND INSTALLED A MINIMUM OF 4' FROM ROADWAY TO BOTTOM OF SIGN.
- SIGN BLANKS FOR TRAFFIC CONTROL SIGNS: SIGN BLANKS SHALL BE FABRICATED FROM 0.080" ALUMINUM ALLOY 6063-T6 CONFORMING TO THE REQUIREMENTS OF SUBSECTION 1626 OF THE STANDARD SPECIFICATIONS.
- SIGN BLADES FOR STREET NAME SIGNS: EXTRUDED ALUMINUM BLADES SHALL BE ALUMINUM ALLOY CONFORMING TO 6063-T6 OR 5052-H38 (ASTM SPECIFICATION B221, LATEST ISSUE). BLADES SHALL HAVE AN ALODINE OR PHOSPHATE ETCHED FINISH. BLADES SHALL HAVE SQUARE CORNERS AND NO HOLES.
MINIMUM BLADE LENGTH SHALL BE 24". MAXIMUM BLADE LENGTH SHALL BE 48". LENGTH VARIES BY INCREMENTS OF 6".
BLADES BEARING THE STREET NAMES SHALL BE FIRMLY ATTACHED TO THE MOUNTING BRACKETS USING ALLEN-TYPE SET SCREWS. THE BLADES SHALL BE ORIENTED PARALLEL TO THE STREET.
- MOUNTING BRACKETS FOR SIGNS: DIE-CAST ALUMINUM BRACKETS SHALL BE ALUMINUM ALLOY 360 HAVING A TENSILE STRENGTH OF 44,000 PSI. THE BRACKETS SHALL BE SMOOTHLY FINISHED FREE OF PITS, BURRS, AND FLAWS. EACH BRACKET SHALL BE TAPPED AND DRILLED FOR 5/16" ZINC-PLATED ALLEN-TYPE SET SCREWS HAVING SELF-LOCKING SAW-TOOTH ENDS.
- FASTENERS: ALL STEEL FASTENERS FOR TRAFFIC CONTROL SIGNS SHALL BE GALVANIZED AND SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1614 OF THE STANDARD SPECIFICATIONS.
- REFLECTIVE SHEETING: REFLECTIVE SHEETING SHALL BE TYPE II - HIGH PERFORMANCE CLASS HA IN ACCORDANCE WITH SUBSECTION 2201 OF THE STANDARD SPECIFICATIONS.
- PROCESS INK: ALL PROCESS INK SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 2202 OF THE STANDARD SPECIFICATIONS.
- DETAILS: REGULATORY AND WARNING SIGNS SHALL CONFORM TO THE DETAILS IN "STANDARD HIGHWAY SIGNS", FHWA, 1979.
- DETAILS - SNS: THE REFLECTIVE SHEETING FOR THE 6 3/4" STANDARD SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVER-WHITE #2 COPY WITH 4" UPPER CASE AND LOWER CASE PRIMARY COPY AND SUFFIX COPY, BOTH SERIES "C". FACES TO TRIM TO A 6 1/4". (SEE DETAIL A.)
THE REFLECTIVE SHEETING FOR THE 9" METRO SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVERWHITE #2 COPY WITH 5" UPPER CASE AND LOWER CASE PRIMARY COPY AND SUFFIX COPY, BOTH SERIES "C". THE CARDINAL DIRECTION CENTERED DIRECTLY BELOW THE BLOCK NUMBER SHALL BE AN UPPER CASE, 3" SERIES "C" LETTER. FACES TO TRIM TO A 8 1/2" WIDTH. (SEE DETAIL B.)
FOR CUL-DE-SAC STREETS, A 9" METRO SIZE BLADE SHALL BE USED WITH THE HOUSE NUMBERS DISPLAYED BENEATH THE STREET NAME. LETTERING TO BE THE SAME AS FOR THE 6 3/4" SIZE BLADE, EXCEPT THAT THE HOUSE NUMBER INFORMATION SHALL BE 4" SERIES "C".
SHOP DRAWINGS OF LAYOUT FOR SNS SHALL BE SUBMITTED TO THE TRAFFIC ENGINEERING DIVISION OF THE CITY OF WICHITA FOR APPROVAL PRIOR TO FABRICATION. THE FINISHED SIGNS AS SUPPLIED SHALL BE OF GOOD APPEARANCE, FREE FROM RAGGED EDGES, CRACKS SCALES OR BLISTERS AND SHALL BE CLEAN-CUT. SIGNS SHALL BE PACKED IN SUCH MANNER AS TO PREVENT DAMAGE OR DEFACTION DURING SHIPMENT OR STORAGE.
- PERMANENT TRAFFIC CONTROL AND SNS: PERMANENT TRAFFIC CONTROL AND SNS SHALL BE MEASURED AND PAID FOR AT THE LUMP SUM PRICE FOR SIGNING. THE PAYMENT AS SET FORTH ABOVE SHALL BE CONSIDERED FULL COMPENSATION FOR ALL EXCAVATION, BACKFILLING, POSTS, ANCHORS, FASTENERS, MATERIALS, LABOR, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK.

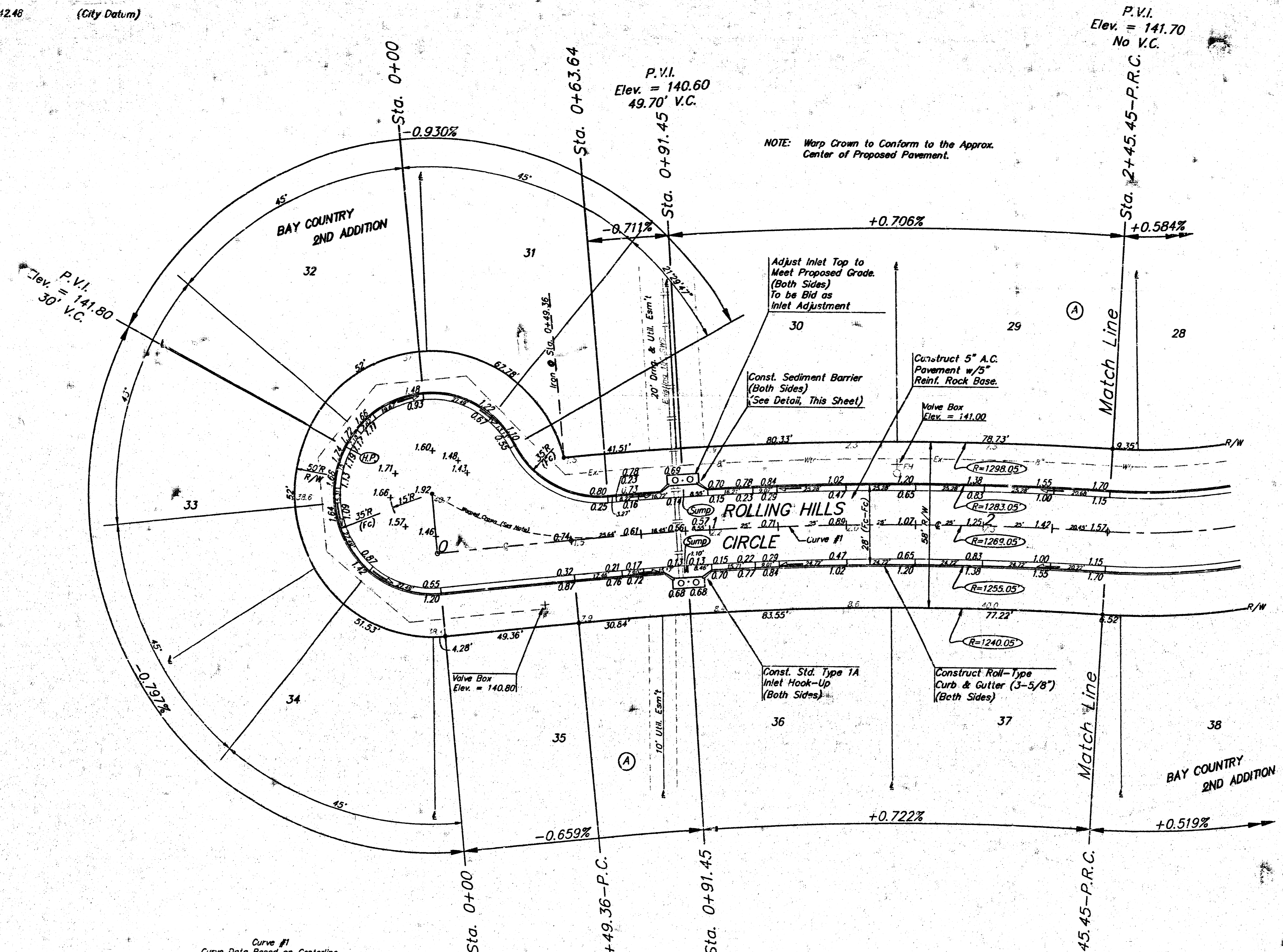
SCALE: NONE	APPROVED BY:	DATE: JUNE '93
DRAWN BY: TM		REVISED: JUNE '97
CITY OF WICHITA DEPARTMENT OF PUBLIC WORKS TRAFFIC ENGINEERING SECTION		

BENCHMARK

"C" Cut - Top of Curb, 14.5'±
South of the S.W. Corner of Lot 37,
Block 4, Bay Country.

Elev. = 142.48 (City Datum)

SCALE: 1" = 20'
• = Iron



NOTE: Warp Crown to Conform to the Approx. Center of Proposed Pavement.

Adjust Inlet Top to Meet Proposed Grade. (Both Sides) To be Bid as Inlet Adjustment

Const. Sediment Barrier (Both Sides) (See Detail, This Sheet)

Construct 5" A.C. Pavement w/5" Reinf. Rock Base.

Valve Box Elev. = 141.00

Const. Std. Type 1A Inlet Hook-Up (Both Sides)

Construct Roll-Type Curb & Gutter (3-5/8") (Both Sides)

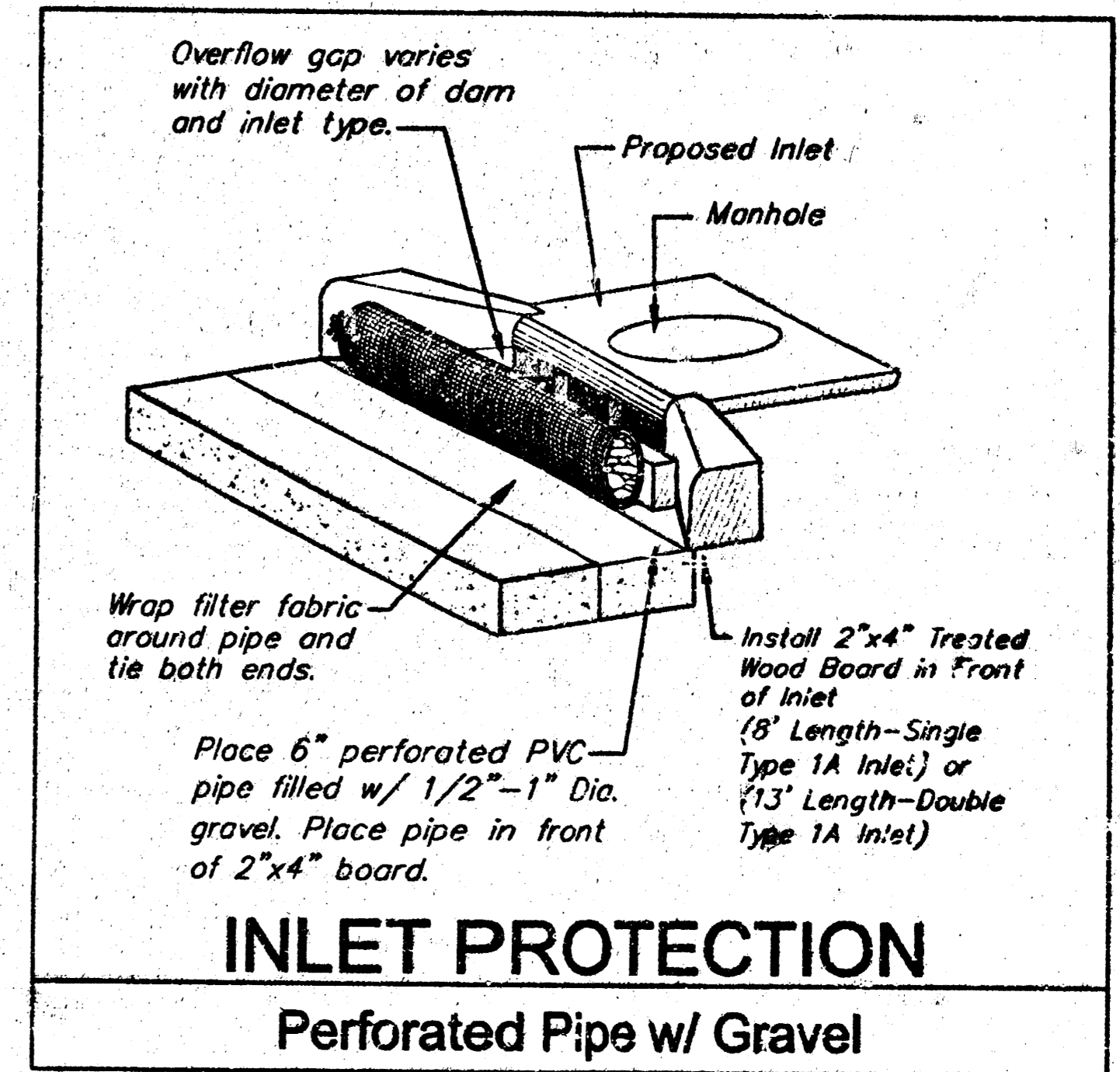
P.V.I. Elev. = 140.60
48.34' V.C.

P.V.I. Elev. = 141.70
No V.C.

Roll type curb & gutter to be constructed on the pavement on this sheet. Top of curb elevation are given for full height curb.

Curve #1
Curve Data Based on Centerline

Station	Arc	FACE CHORD LENGTHS	Defl	T Defl
B' Lt.	B' Rt.			
0+49.36	-	-	0°00'00"	0°00'00"
0+63.64	14.28'	14.53'	14.03'	0°19'20"
0+75.00	11.36'	11.56'	11.16'	0°15'24"
1+00.00	25.00'	25.43'	24.57'	0°33'51"
1+25.00	25.00'	25.43'	24.57'	0°33'52"
1+50.00	25.00'	25.43'	24.57'	0°33'52"
1+75.00	25.00'	25.43'	24.57'	0°33'51"
2+00.00	25.00'	25.43'	24.57'	0°33'52"
2+25.00	25.00'	25.43'	24.57'	0°33'52"
2+45.45	20.45'	20.80'	20.08'	0°27'42"



BAY COUNTRY 2ND ADDITION
ROLLING HILLS CIR
STA. 0+00 TO STA. 2+45.45

BAUGHMAN COMPANY P.A.
ENGINEERING, SURVEYING, & PLANNING
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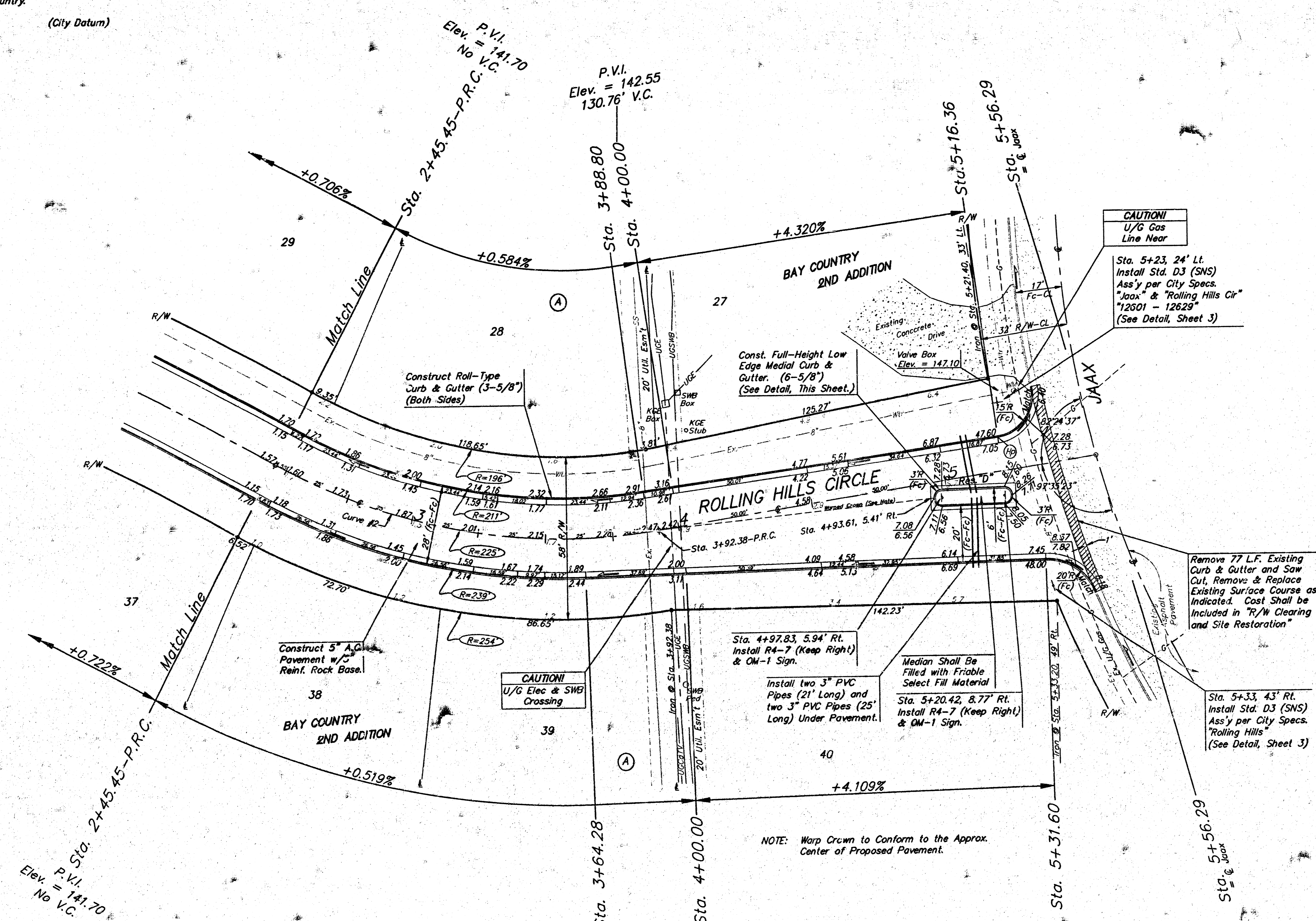
PROJECT NUMBER: 129 PPP 607879

DESIGN: BLP/AEG	DRAWN: JAK	APPROVED:	DATE: 10/5/07	SCALE: NOTED
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SHEET 4 OF 8

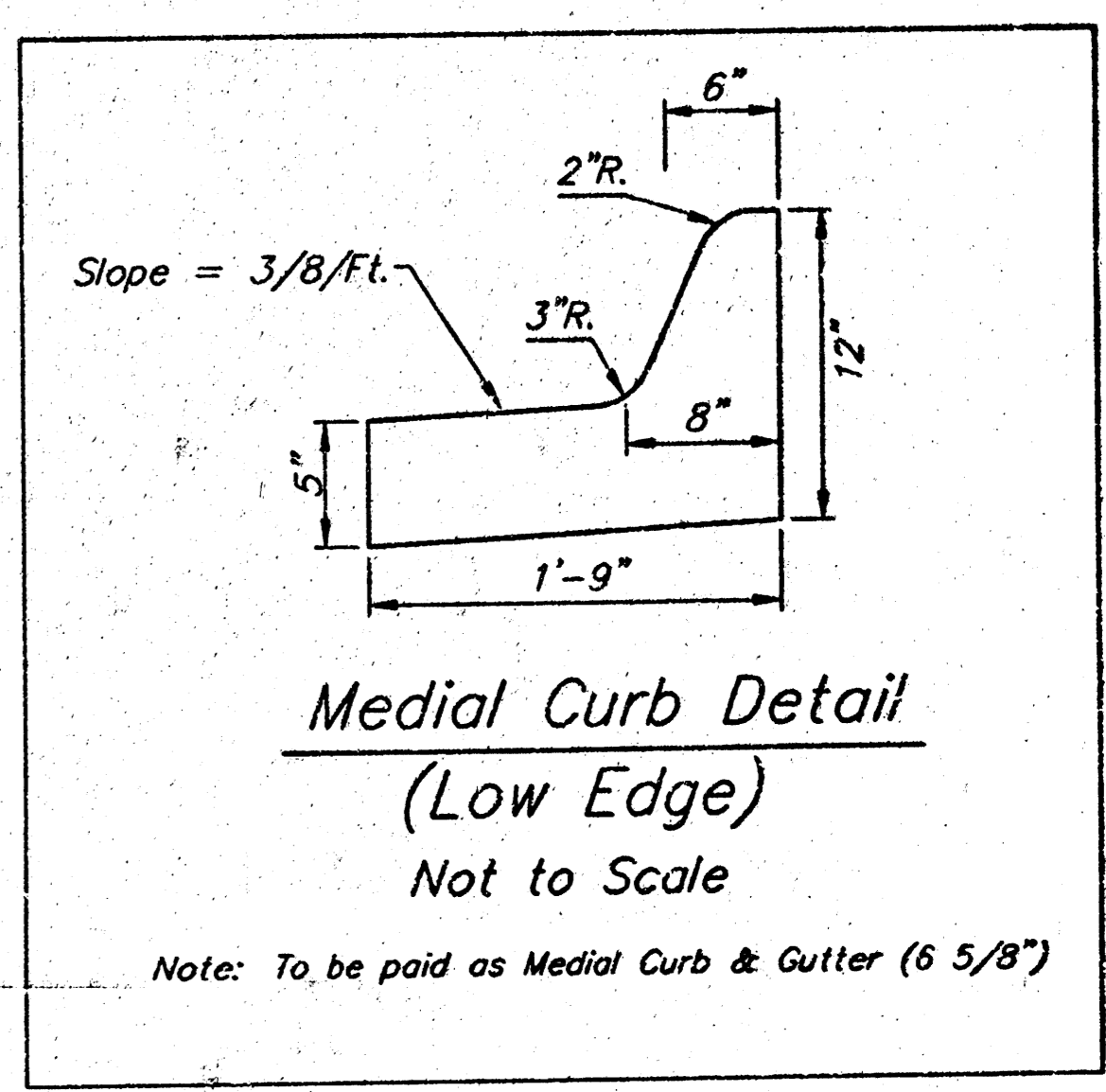
BENCHMARK
 "B" Cut - Top of Curb, 14.5'
 S.W. of the S.W. Corner of Lot 37,
 Block 4, Bay Country.
 Elev. = 142.48 (City Datum)

SCALE: 1" = 20'
 • = Iron



Curve #2
 Curve Data Based on Centerline
 Rad. = 225' Delta = 37° 25' 01" Tangent = 76.20'
 Arc = 146.93' L.C. = 144.34' Del/Ft. = 7.63975 Min.

Station	Arc	FACE CHORD LENGTHS		Defl	T Defl
		B' Lt.	B' Rt.		
2+45.45	4.55'	4.71'	5.00'	0°00'00"	0°00'00"
2+50.00	4.55'	4.71'	5.00'	0°34'46"	0°34'46"
2+75.00	25.00'	22.54'	27.43'	3°10'59"	3°43'45"
3+00.00	25.00'	22.54'	27.43'	3°11'00"	6°56'45"
3+25.00	25.00'	22.54'	27.43'	3°11'00"	10°07'45"
3+50.00	25.00'	22.54'	27.43'	3°10'59"	13°18'44"
3+64.28	14.28'	12.88'	15.57'	1°49'06"	15°0'50"
3+75.00	10.72'	9.67'	11.97'	1°21'54"	16°29'44"
3+88.80	13.50'	12.45'	15.15'	1°45'25"	18°15'09"
3+92.38	3.58'	3.23'	3.93'	0°27'22"	18°42'31"

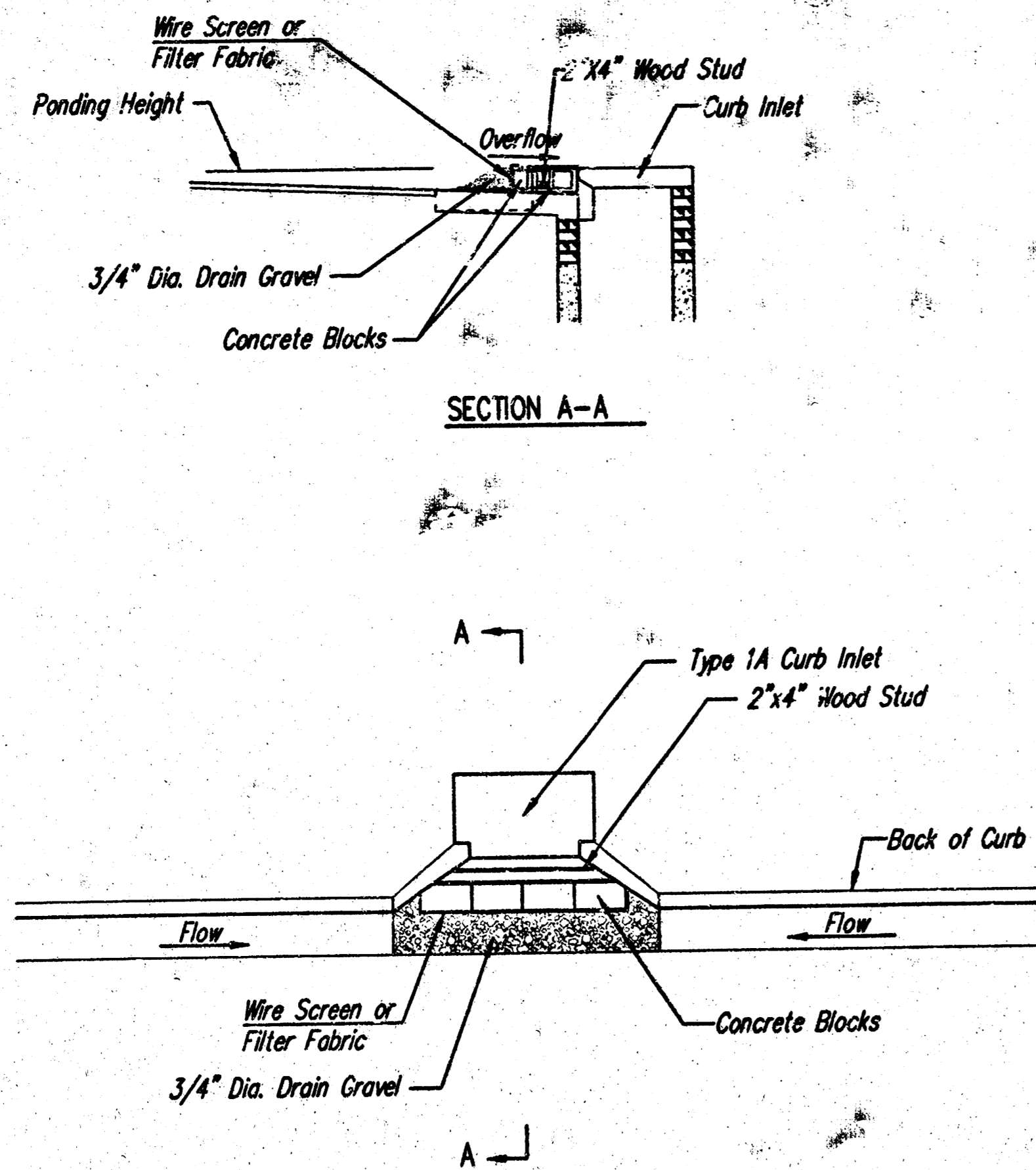


BAY COUNTRY 2ND ADDITION
ROLLING HILLS CIR
 STA. 2+45.45 TO STA. 5+56.29

BAUGHMAN COMPANY P.A.
 ENGINEERING, SURVEYING, & PLANNING
 316-262-7271 • 315 ELLIS • WICHITA, KANSAS 67211

PROJECT NUMBER: 129 PPP 60789
 SHEET: 5 OF 8
 DATE: 10/5/01
 SCALE: NOTED

Roll type curb & gutter to be constructed on the pavement on this sheet.
 Top of curb elevation are given for full height curb.



CURB INLET GRAVEL FILTERS
(INLET PROTECTION-RESIDENTIAL STREETS ONLY)

NOTE: Other types of curb inlet protection may be approved by the city so long as equal protection is provided.

A gravel inlet filter shall be installed at sump locations on residential streets. This type of protection is not to be used on arterial or collector streets at any time that it would pose an undue traffic hazard.

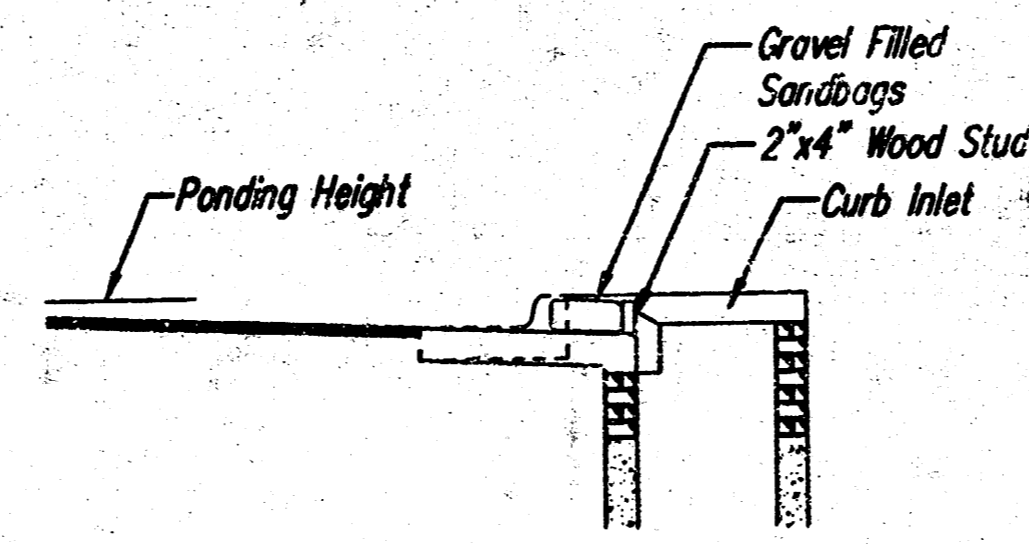
Instructions for Installing:

- STEP 1: Place concrete blocks around the inlet as shown on drawing. Insert 2x4 board as shown.
- STEP 2: Wrap 1/2" mesh wire screen around the concrete blocks.
- STEP 3: Place 1" to 1-1/2" diameter rock around the blocks and wire screen. Be sure the rock extends down from the top of the concrete block.
- STEP 4: To prevent damage to vehicles, signs warning drivers about the structures may be necessary. An alternative installation is the use of gravel bags supported by a 2x4" board to prevent collapsing.

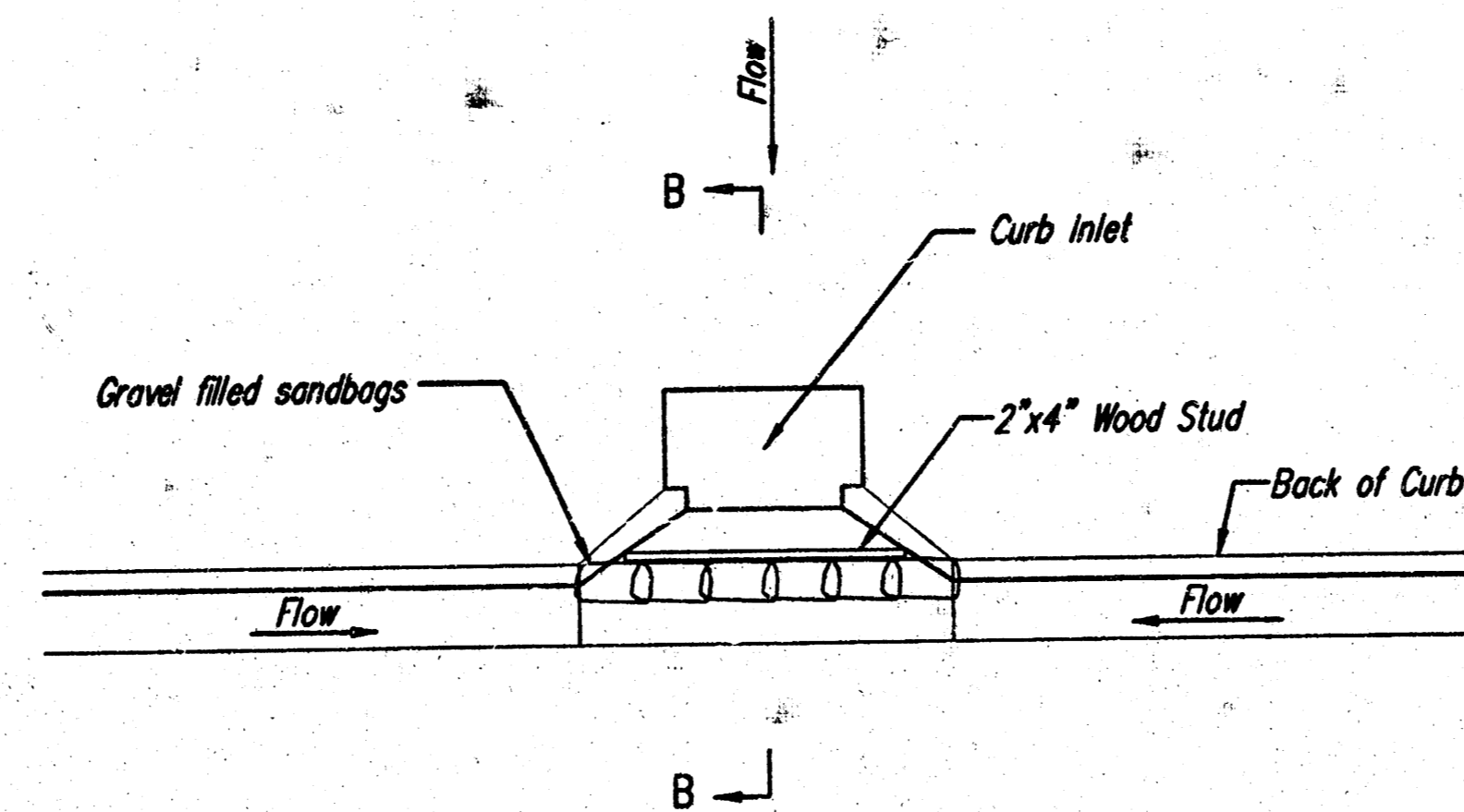
Use of rock with diameters smaller than 1" in the bag may result in clogging of pores and reduce the amount of water flowing into an inlet.

Maintenance:

All curb inlet gravel filters shall be inspected and repaired after each runoff event. Sediment deposits are to be removed once material is within 8 cm (3 inches) of the top of any block. Periodically, the gravel shall be raked to increase infiltration and filtering of runoff waters. Accumulated sediment is to be removed immediately from roads and streets.



SECTION B-B

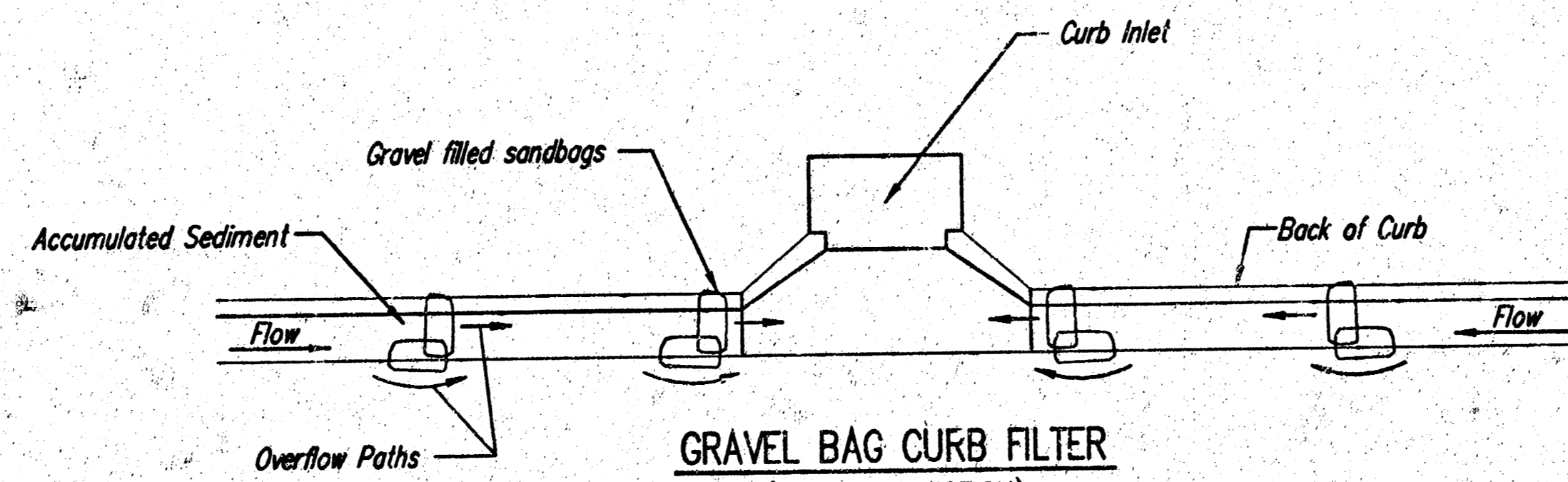


Diversion ridge required where grade exceeds 2%

SECTION C-C

CURB INLET SANDBAG FILTERS
(INLET PROTECTION)

NOTE: Other types of curb inlet protection may be approved by the City so long as equal protection is provided.



GRAVEL BAG CURB FILTER
(INLET PROTECTION)

NOTE: Place two or more sets of bags in a manner that results in maximum support. The flow line bag must be lower than top of curb.

CURB SEDIMENT TRAPS:

When inlets are located on streets having a grade (i.e., sump conditions do not exist), installing gravel (or sand) bags in the gutter flow line to create small sediment traps can be considered. Gravel bags are recommended over sand bags to allow for drainage.

If the spacing between bags becomes too large, little sediment may be trapped. Spacing of bags should be completed using the table or graph that illustrates placement distances based upon street slope. When installed in the gutter, bag tops must be lower than the sidewalk.

Spacing:

Gravel bags are to be placed according to street grades using the following table or graph that appears below.

GRADE (%)	SPACING (FEET)
0.5	75
1.0	45
2.0	18
3.0	12
4.0	9
5.0	6

Maintenance:

Collected sediment shall be removed after every runoff event. Bags that are destroyed by vehicular traffic or through natural deterioration are to be immediately replaced.

NOTES:

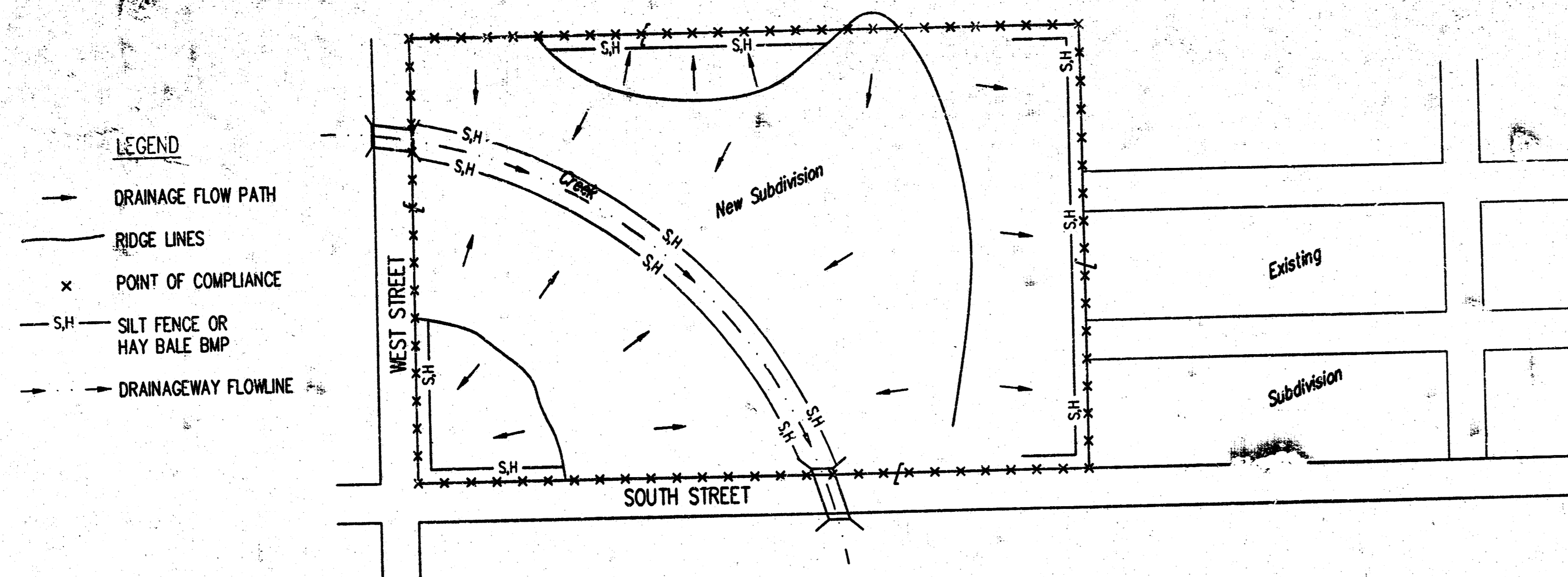
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN, AS SHOWN ABOVE.
4. DRIVE ENTRANCES ONTO RESIDENTIAL LOTS WILL NOT BE REQUIRED TO HAVE THE SEDIMENT BARRIER SHOWN, BUT WHEEL WASHING MAY BE REQUIRED IF STABILIZED ENTRANCE IS NOT SUFFICIENT TO KEEP MUD FROM BEING TRACKED ONTO ADJACENT STREET. ENTRANCE SHALL EXTEND FROM BACK OF CURB TO DWELLING.

**SOIL EROSION
BMP DETAILS**

CHRISTOPHER M. CARRIER, P.E.
STORM WATER ENGINEER

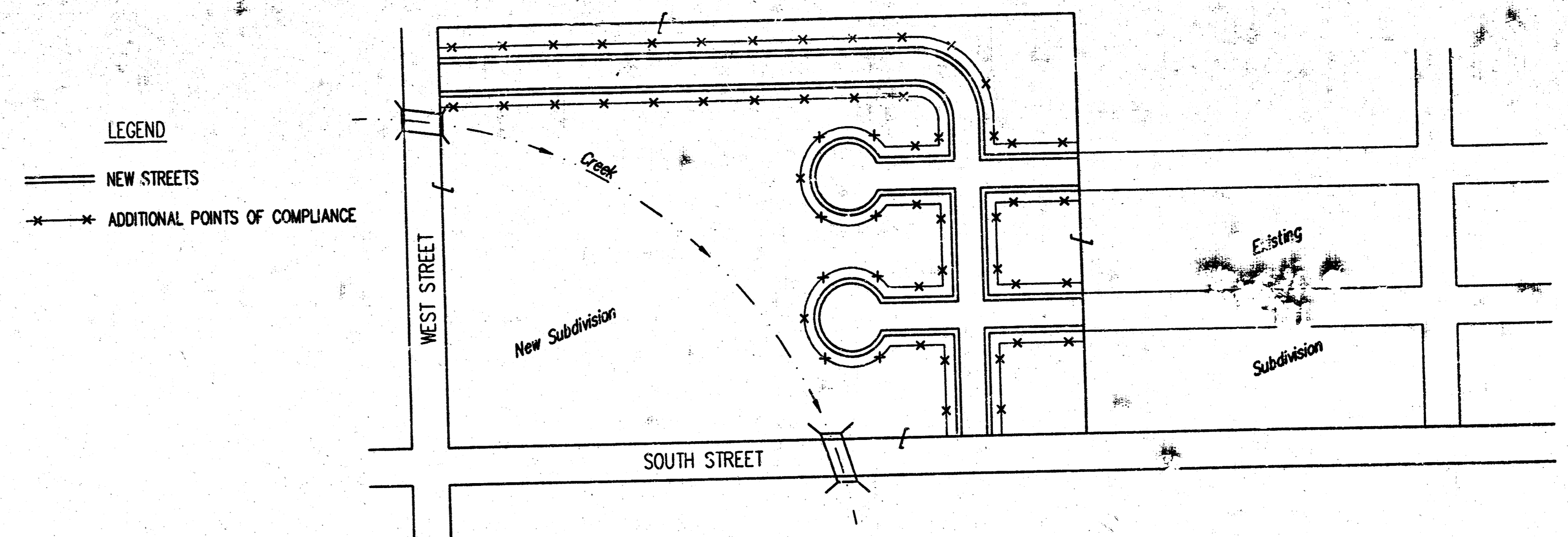
PROJECT NUMBER	CCA NO.
129 PPP 607879	-----
DATE	MAY 2001
SHEET 6 OF 8	

PHASE 1 - INITIAL EARTHWORK AND UTILITIES (EXCEPT STORM SEWER)



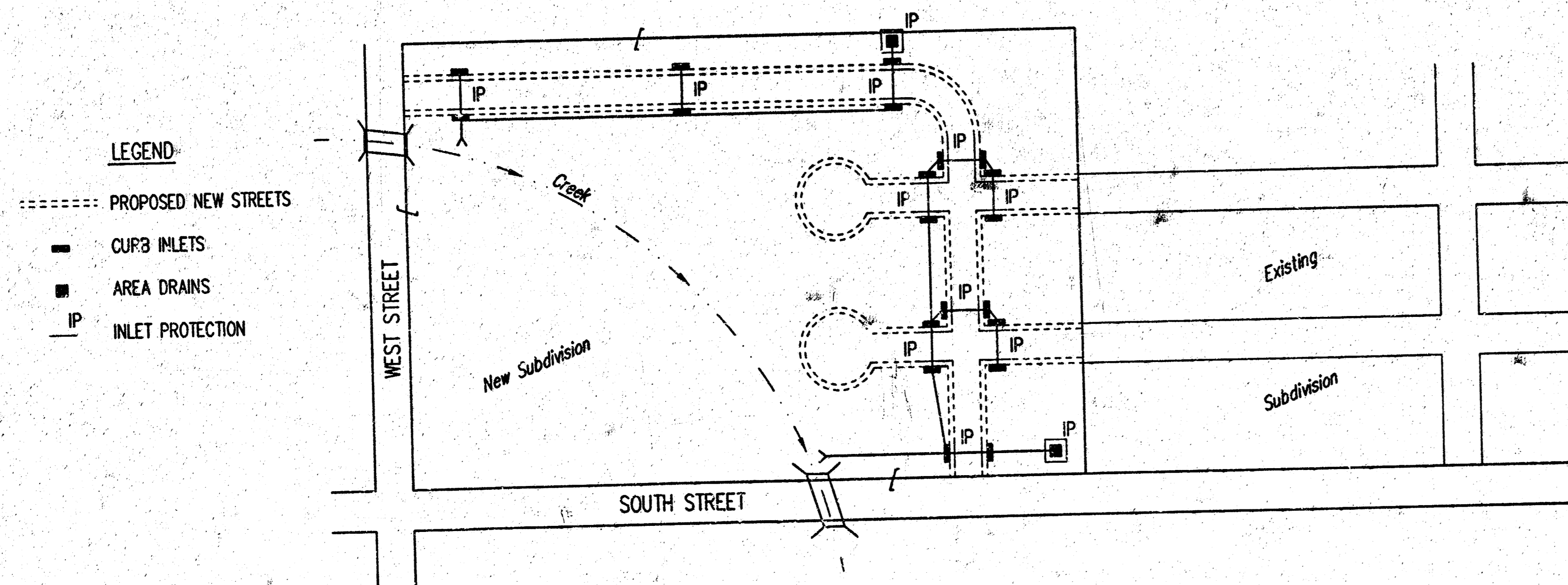
- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, THE POINTS OF COMPLIANCE ARE THE PERIMETER BOUNDARIES AND ANY DRAINAGE WAYS OR STORM SEWERS DRAINING THROUGH OR FROM THE SITE. SHOULD LAKES BE CONSTRUCTED WITHIN THE SUBDIVISION THAT WILL DISCHARGE DURING STORMS, THEY ARE ALSO A POINT OF COMPLIANCE.
- HAYBALES OR SILT FENCE MUST BE CONSTRUCTED ALONG THE PROPERTY LINE WHERE ON SITE WATER CAN DRAIN OFF THE PROPERTY. THESE BMP'S WILL ALSO BE INSTALLED ALONG ANY DRAINAGE DITCH OR LAKE THAT CAN DISCHARGE.
- SHOULD SILT OR SEDIMENT ENTER THE DITCHES OR GUTTERLINES ON THE ADJACENT BOUNDARY STREETS, APPROPRIATE BMP'S WILL BE PLACED WITHIN THE SUBDIVISION TO PREVENT THIS.
- ANY MUD TRACKED ONTO ADJACENT STREETS WILL BE REMOVED AT THE END OF EACH WORK DAY.
- CONTRACTORS WORKING WITHIN THE SITE WILL NOT BE REQUIRED TO USE INDIVIDUAL BMP'S AS LONG AS THOSE SPECIFIED ABOVE ARE IN PLACE AND EFFECTIVE. CONTRACTORS WORKING ON THE BOUNDARY LINE STREETS OR ON ADJACENT PROPERTIES TO EXTEND UTILITIES ARE EXPECTED TO USE BMP'S AT THEIR WORK LOCATIONS, AS NEEDED.
- UTILIZE STABILIZED CONSTRUCTION ENTRANCE AT ENTRANCE AND EXIT ONTO ANY EXISTING PUBLIC STREETS.
- THE SUBDIVISION DEVELOPER (OWNER) SHALL INSTALL AND MAINTAIN THE ON-SITE BMP'S.

PHASE 3 - STREET CONSTRUCTION



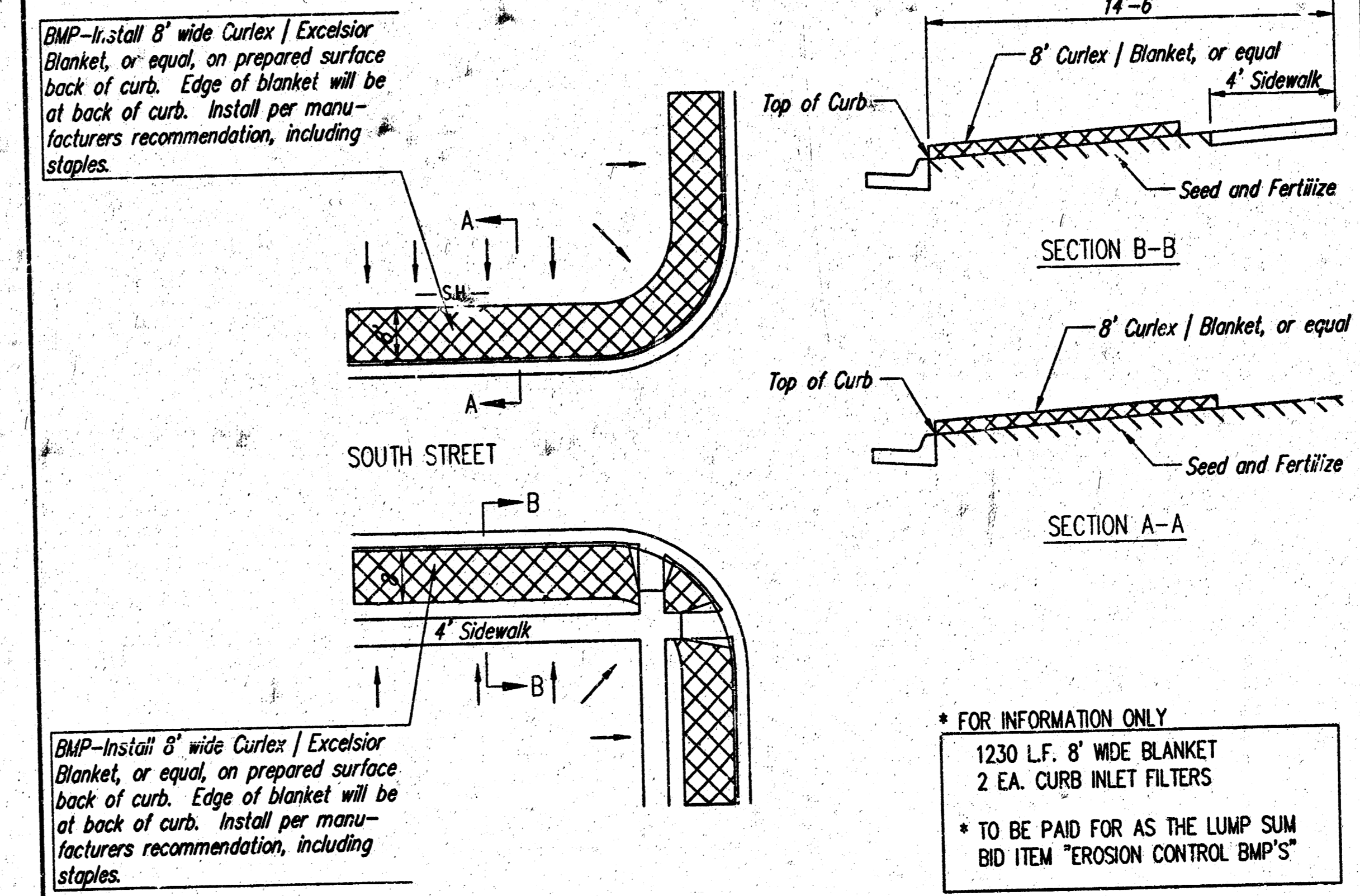
- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, NEW STREETS ARE INSTALLED. ALL BMP'S INSTALLED DURING PHASE 1 AND 2 MUST STILL BE MAINTAINED. THE POINT OF COMPLIANCE NOW SHIFTS TO THE BACK OF CURB ALONG EACH STREET.
- CURB OPENING INLET PROTECTION:
 - SUMP AREAS - INLET PROTECTION SHALL BE PROVIDED WHEN STREET SUBGRADE WORK IS COMPLETED.
 - NON-SUMP LOCATIONS - PROVIDE INLET PROTECTION AS SOON AS BASE COURSE ASPHALT IS INSTALLED, BEFORE THE SURFACE COURSE LIFT.
- BMP'S WILL BE REQUIRED BACK OF CURB WHEREVER WATER CAN FLOW OVER THE CURB AND THE CURB HAS BEEN BACKFILLED TO WITHIN 3" OR LESS OF THE TOP OF CURB (SEE CURB BACKFILL DETAIL). FOR CURBS NOT YET ENTIRELY BACKFILLED (3" OR MORE BELOW TOP OF CURB), BMP'S WILL BE REQUIRED AT POINTS WHERE WATER BREAKS OVER CURB WHICH COULD RESULT IN THE PLACEMENT OF SEDIMENT IN THE GUTTER.
- SEE DETAIL THIS SHEET ON BACK OF CURB PROTECTION.
- THE BACK OF CURB PROTECTION SPECIFIED ON THIS PLAN MAY HAVE TO BE SUPPLEMENTED WITH HAYBALE OR SILT FENCE BMP'S AT LOCATIONS WHERE CONCENTRATED FLOW RESULTS IN SEDIMENT BEING CARRIED OVER THE EXCELSIOR MATS.
- THE STREET CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING BACK OF CURB BMP'S.
- THE INDIVIDUAL LOT OWNERS WILL BE RESPONSIBLE FOR MAINTAINING THE BACK OF CURB BMP'S IN FRONT OF THEIR LOTS UNTIL SUCH TIME AS ADJACENT DISTURBED EARTH IS STABILIZED WITH GRASS OR SOD.

PHASE 2 - INSTALLATION OF STORM SEWER

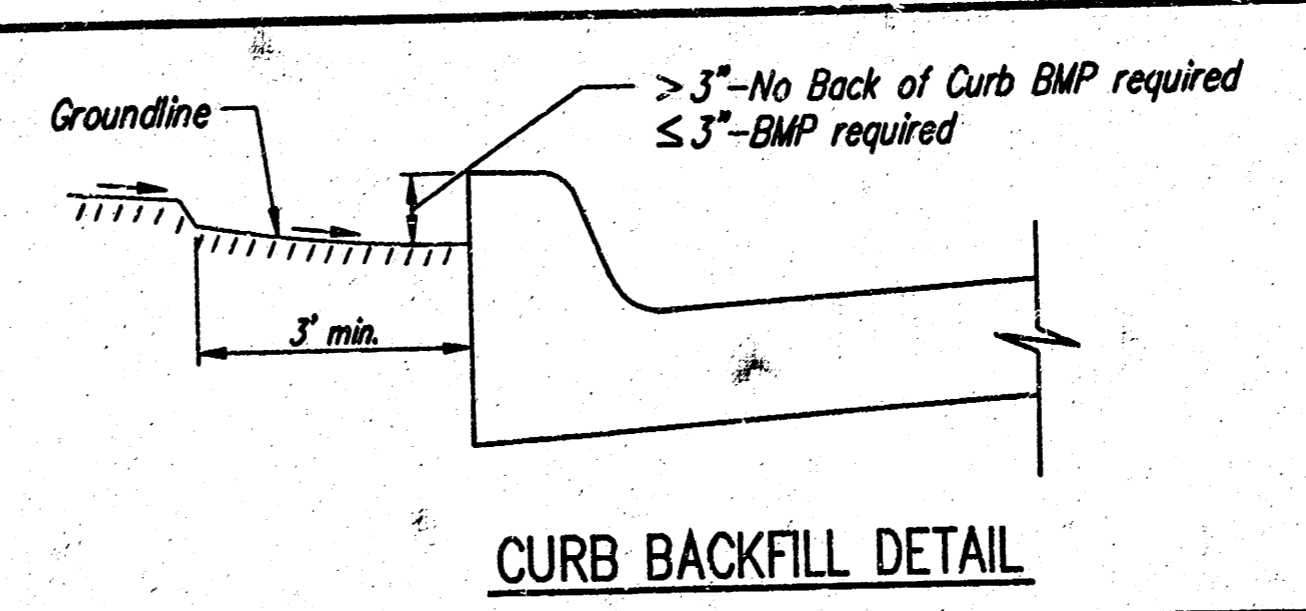


- DURING THIS PHASE OF SUBDIVISION DEVELOPMENT, ALL BMP'S REQUIRED IN PHASE 1 SHALL REMAIN IN PLACE AND BE MAINTAINED.
- AS NEW STORM SEWERS, WITH INLETS, ARE INSTALLED, THE STORM SEWERS MUST NOW BE PROTECTED SO ALL NEW INLETS BECOME POINTS OF COMPLIANCE.
- AREA DRAINS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, HAYBALE OR SILT FENCE PROTECTION WILL BE INSTALLED AROUND THEM.
- CURB OPENING INLETS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, INLET PROTECTION BMP'S MUST BE INSTALLED. SEE PHASE 3 - STREET CONSTRUCTION.
- THE STORM SEWER CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING THESE BMP'S. IF WATER CANNOT FLOW INTO CURB INLETS UNTIL STREET CONSTRUCTION IS COMPLETE, THEN STREET CONTRACTOR WILL INSTALL INLET PROTECTION.
- THE SUBDIVISION DEVELOPER WILL MAINTAIN THESE BMP'S ONCE INSTALLED.
- ONCE ALL DISTURBED GROUND DRAINING TO AN INLET HAS BEEN RESTABILIZED WITH GRASS OR SOD, THE SUBDIVISION DEVELOPER WILL BE RESPONSIBLE FOR PERMANENTLY REMOVING THE INLET PROTECTION.

- GENERAL NOTES:**
- THE INTENT OF ALL BEST MANAGEMENT PRACTICES (B.M.P.'S) IS TO PREVENT ERODED SOIL FROM ENTERING DITCHES, STORM SEWERS, OR ANY OTHER DRAINAGE FEATURE.
 - THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPE OF BMP'S WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
 - BMP'S SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS TO REMAIN EFFECTIVE. MAINTENANCE SHALL BE AS INDICATED ON THE BMP DETAIL SHEETS.
 - PERSONS DESTROYING BMP'S SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING THEM OR INSTALLING SUITABLE REPLACEMENT BMP'S.
 - THE DEVELOPMENT OF ANY SUBDIVISION THAT DISTURBS 5 ACRES OR MORE WILL REQUIRE A FEDERAL/STATE NPDES STORMWATER PERMIT. THE PREPARATION OF A STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. EROSION CONTROL BMP'S ARE REQUIRED. THE DETAILS SHOWN ON THIS SHEET ARE THE MINIMUM STANDARDS TO BE SHOWN ON POLLUTION PREVENTION PLAN.
 - FOR SUBDIVISIONS SMALLER THAN 5 ACRES, SOIL EROSION BMP'S ARE REQUIRED. ALSO, DEVELOPERS AND CONTRACTORS ARE ENCOURAGED TO DEVELOP POLLUTION PREVENTION PLANS FOR EACH PROJECT PRIOR TO CONSTRUCTION.
 - FAILURE TO USE AND MAINTAIN BMP'S IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE SUBDIVISION DEVELOPER AND CONTRACTORS TO THE PENALTIES PROVIDED THEREIN.
 - THE APPLICATION OF BMP'S SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT BMP OTHER THAN THAT SHOWN. BMP'S, OTHER THAN THOSE SHOWN, MAY BE UTILIZED SO LONG AS THEY ARE EFFECTIVE AND MAINTAINED.
 - A STABILIZED EARTH SURFACE IS DEFINED AS ONE THAT IS HARD SURFACED WITH CONCRETE, ASPHALT, OR THE LIKE, OR ONE ON WHICH 70% OF THE GRASS HAS GERMINATED ON THE ENTIRE SURFACE.



BACK OF CURB PROTECTION DETAIL



CITY OF WICHITA

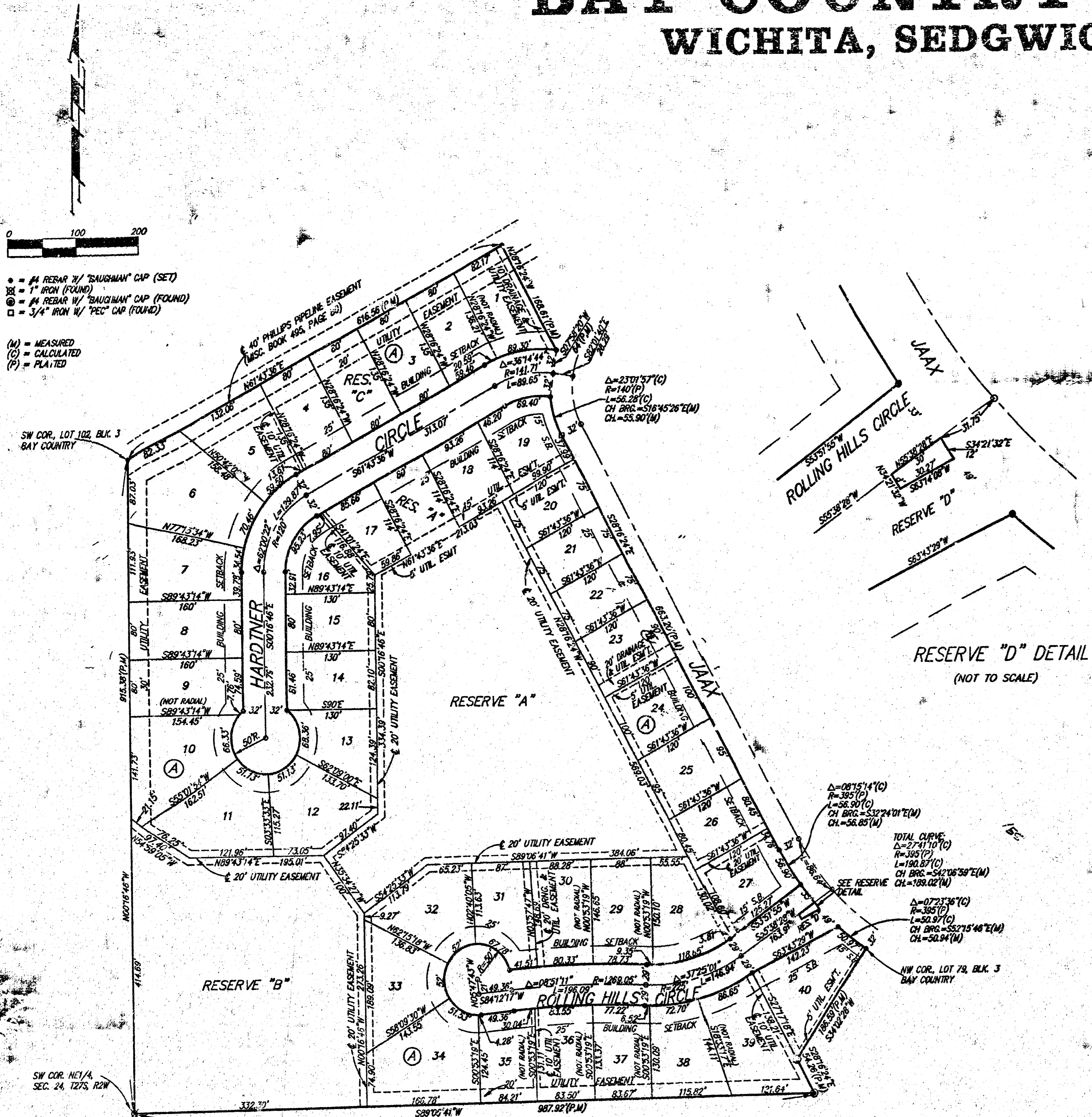
SOIL EROSION BMP'S SUBDIVISION DEVELOPMENT PROCESS

CHRISTOPHER M. CARRIER, P.E.
STORM WATER ENGINEER

PROJECT NUMBER: 129 PPP 607879
DATE: MAY 2001

OCA NO. _____
SHEET 7 OF 8

BAY COUNTRY 2ND ADDITION WICHITA, SEDGWICK COUNTY, KANSAS



- = 1/4 REBAR 3/4 "BAUGHMAN" CAP (SET)
- = 1" IRON (FOUND)
- = 1/4 REBAR 1/2 "BAUGHMAN" CAP (FOUND)
- = 3/4" IRON 1/2 "REC" CAP (FOUND)
- (M) = MEASURED
- (C) = CALCULATED
- (P) = PLATTED

RESERVE "D" DETAIL
(NOT TO SCALE)

MINIMUM BUILDING PAD ELEVATION FOR LOWEST OPENING TO THE STRUCTURES			
LGES	BLOCK	CITY DATUM	MSL
2-6	A	148.6	1338.0
7-39	A	140.6	1328.0

BENCHMARK:
"T" CUT - TOP OF CURB, 14.5' SOUTH OF THE SW COR. OF LOT 37, BLOCK 4, BAY COUNTRY.
ELEV. = 142.48 CITY DATUM
1328.89 M.S.L.

State of Kansas) SS We, Baughman Company, P.A., Surveyors in Sedgwick County) do hereby certify that we have surveyed and platted "BAY COUNTRY 2ND ADDITION", Wichita, Sedgwick County, Kansas, and that the accompanying plat is a true and correct exhibit of the property surveyed, described as follows: A replat of Lots 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, and 78, Block 3, TOGETHER with all of Reserve "C" and together with that part of Reserve "D" lying between said Lots 56 and 57, in said Block 3, and together with Hardner Circle, all in Bay Country, an Addition to Wichita, Sedgwick County, Kansas.

All being situated in the NE1/4 of Sec. 24, Twp. 27-S, R-2-W of the 6th P.M., Sedgwick County, Kansas.
Existing public easements and dedications being vacated by virtue of K.S.A. 12-312(b).

Baughman Company, P.A.

Michael G. Conroy
Michael G. Conroy
Surveyor



Know all men by these presents that we, the undersigned, have caused the land in the surveyors certificate to be platted into Lots, a Block, Streets, and Reserves, to be known as BAY COUNTRY 2ND ADDITION, Wichita, Sedgwick County, Kansas. The utility easements are hereby granted as indicated for the construction and maintenance of all public utilities. The drainage and utility easements are hereby granted as indicated for drainage purposes and for the construction and maintenance of all public utilities. The streets are hereby dedicated to and for the use of the public. Reserve "A" is hereby reserved for open space, drainage purposes, gazebos, ponds, sidewalks, landscaping and utilities as confined to easements. Reserve "B" is hereby reserved for drainage, landscaping, irrigation systems, pedestrian improvements, and utilities as confined to easements. Reserve "C" is hereby reserved for drainage purposes and utilities as confined to easements. Reserve "D" is hereby reserved for street purposes, landscaping, entry monuments, and utilities. Reserves "A", "B", "C", and "D" shall be owned and maintained by the homeowners association for the addition. The Minimum Building Pad Elevations for the lowest opening to the structures shall be as indicated on the face of the plat.

I.T.J. Investments, Inc.

Irma N. Jacoby
Irma N. Jacoby, President

Bob Cook Homes, L.L.C.

D.K. Cook
D.K. Cook, member

William Arthur Starnes
Linda Lee Starnes

State of Kansas) SS The foregoing instrument acknowledged before me, this 27th day of OCTOBER, 1998, by Irma N. Jacoby, President of I.T.J. Investments, Inc., on behalf of the corporation.

JUDITH M. TERHUNE
Notary Public - State of Kansas
My App't. Exp. 11-7-2001

State of Kansas) SS The foregoing instrument acknowledged before me, this 27th day of OCTOBER, 1998, by D.K. COOK, MEMBER of Bob Cook Homes, L.L.C., on behalf of the company.

JUDITH M. TERHUNE
Notary Public - State of Kansas
My App't. Exp. 11-7-2001

State of Kansas) SS The foregoing instrument acknowledged before me, this 29th day of OCTOBER, 1998, by William Arthur Starnes and Linda Lee Starnes, husband and wife.

JUDITH M. TERHUNE
Notary Public - State of Kansas
My App't. Exp. 11-7-2001

We, the undersigned, holders of a mortgage on the above described property, do hereby consent to this plat of "BAY COUNTRY 2ND ADDITION", Wichita, Sedgwick County, Kansas.

Emprise Bank
Gary K. Brown
V.P.

State of Kansas) SS The foregoing instrument acknowledged before me, this 30th day of OCTOBER, 1998, by GARY K. BROWN, V.P. PRESIDENT of Emprise Bank, on behalf of the bank.

JUDITH M. TERHUNE
Notary Public - State of Kansas
My App't. Exp. 11-7-2001

This plat of "BAY COUNTRY 2ND ADDITION", Wichita, Sedgwick County, Kansas has been submitted to and approved by the Wichita-Sedgwick County Metropolitan Area Planning Commission, Wichita, Kansas.
Dated this 12th day of MARCH, 1998.
Wichita-Sedgwick County Metropolitan Area Planning Commission



Richard E. Lopez
Richard E. Lopez, Chairman

Martin S. Kraut
Martin S. Kraut, Secretary

This plat approved and all dedications shown hereon accepted by the City Council of the City of Wichita, Kansas, this 13th day of JULY, 1998.



Bob Knight
Bob Knight, Mayor

Pat Burnett
Pat Burnett, City Clerk

Entered on transfer record this 4th day of August, 1998.



James Atford
James Atford, County Clerk

State of Kansas) SS This is to certify that this plat has been filed for record in the office of the Register of Deeds, this 2nd day of August, 1998 at 9:52 o'clock A.M. and is duly recorded.

Bill Meek
Bill Meek, Register of Deeds

#1818161



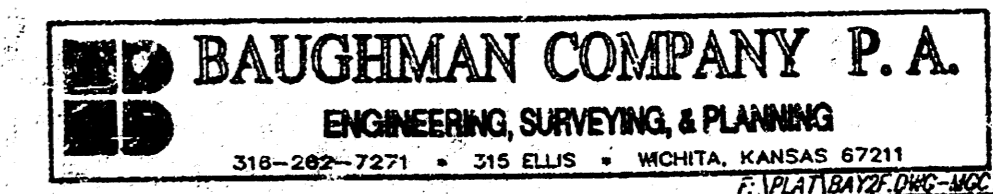
Linda Kizzire
Linda Kizzire, Deputy

We, the undersigned, holders of a mortgage on the above described property, do hereby consent to this plat of "BAY COUNTRY 2ND ADDITION", Wichita, Sedgwick County, Kansas.
Southwest National Bank

Robert E. Hoopes
Robert E. Hoopes, (Name)

State of Kansas) SS The foregoing instrument acknowledged before me, this 6th day of NOVEMBER, 1998, by ROBERT E. HOOPES, PRESIDENT of Southwest National Bank, on behalf of the bank.

JUDITH M. TERHUNE
Notary Public - State of Kansas
My App't. Exp. 11-7-2001



20.00
ck