

STREET IMPROVEMENTS FOR

S E L M A

E.L. OF LAURA TO W.L. OF PATTIE

PROJECT NO.

472-76-245-80669-000-000-001

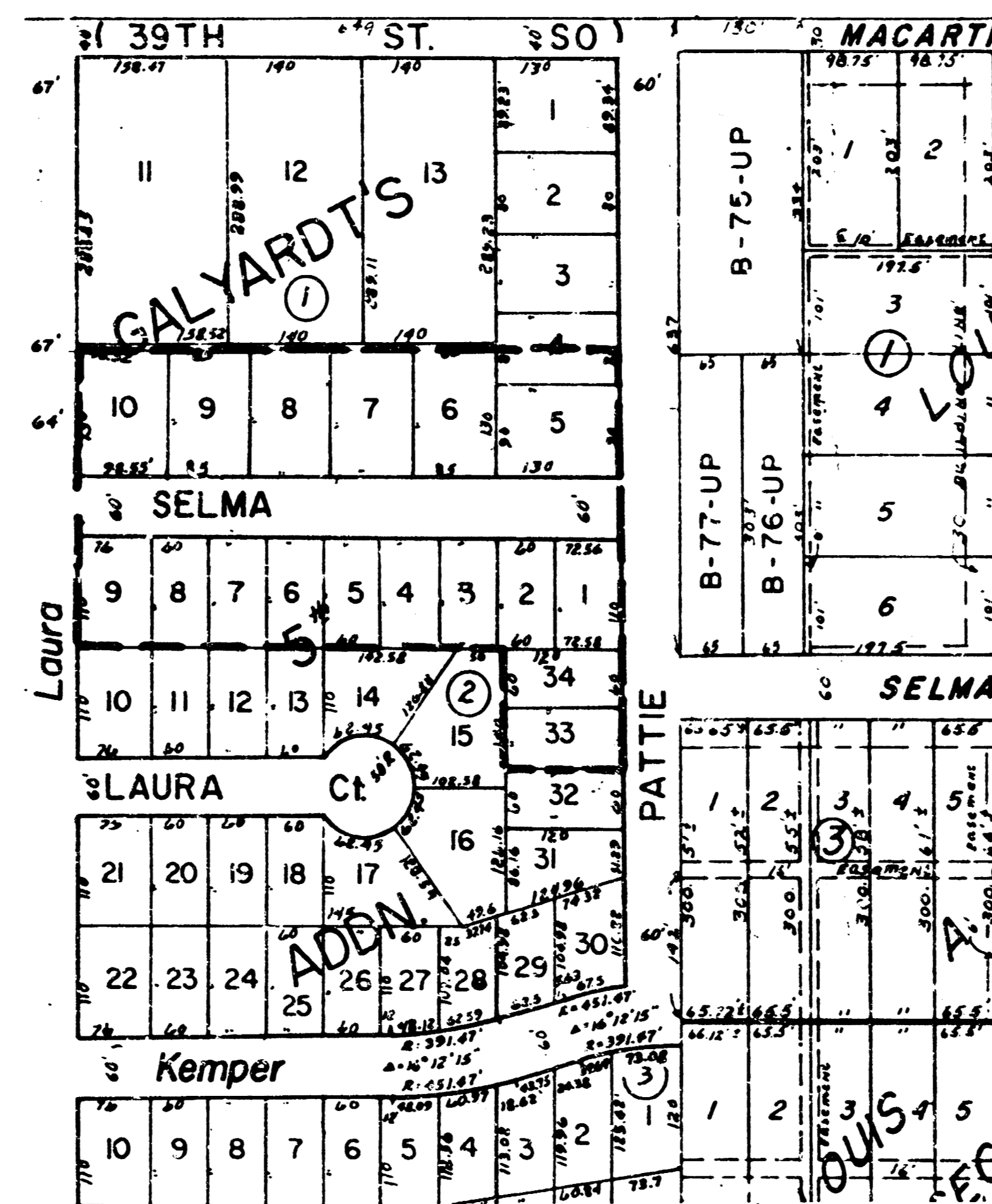
GENERAL NOTES

MAILBOXES WITHIN THE LIMITS OF THE PROJECT SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AS APPROVED BY THE ENGINEER. CONTRACTOR WILL BE REQUIRED TO MAKE SATISFACTORY PROVISIONS FOR MAIL DELIVERY TO PROPERTIES AFFECTED BY THIS PROJECT DURING ITS CONSTRUCTION.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, AND ETCETERA ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO OR DURING CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION, AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

A saw cut of at least one-half the depth of existing surface courses or one-fourth the depth of the existing total pavement thickness shall be provided at locations where proposed construction abuts an existing surface course or pavement for which partial removal of that surface or pavement is required. Sawed joints to facilitate removal within three (3) feet of existing joints will not be permitted and for such instances the limits of removal shall extend to the existing joint. Such saw cuts will not be paid for directly and this cost shall be considered as subsidiary to the removal of the surface or pavement.



BENEFIT DISTRICT

INDEX TO DRAWINGS

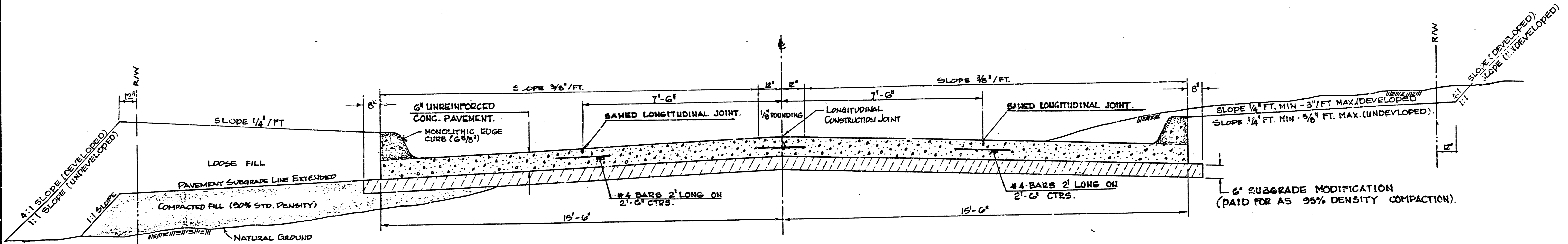
- SHEET NO.
- 1 ----- TITLE SHEET
 - 2 ----- TYPICAL SECTION
 - 3 ----- PAVING PLAN
 - 4 ----- DRIVEWAY DETAILS
 - 5 ----- CROSS SECTIONS

CITY OF WICHITA, KANSAS
MICHAEL E. LINDEBAK
CITY ENGINEER

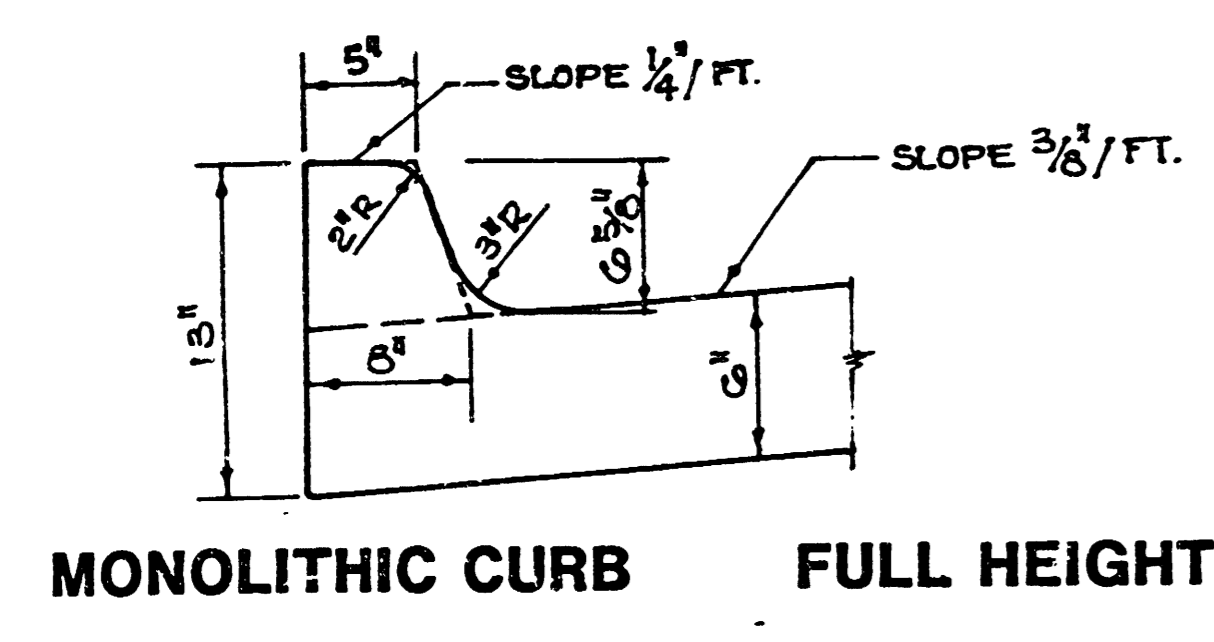
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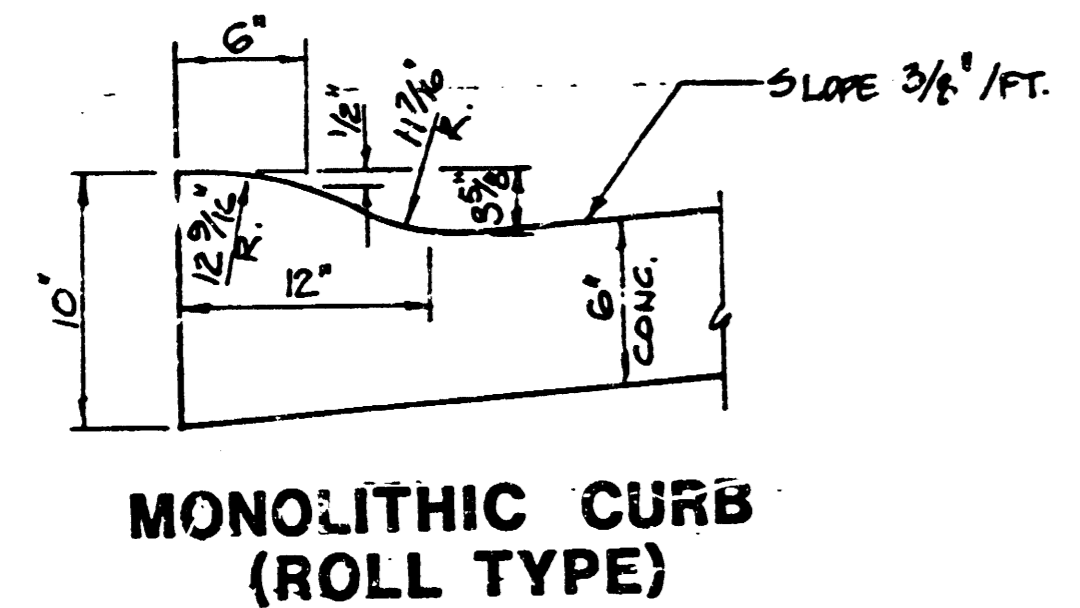
TYPICAL 3' PAVEMENT DETAILS



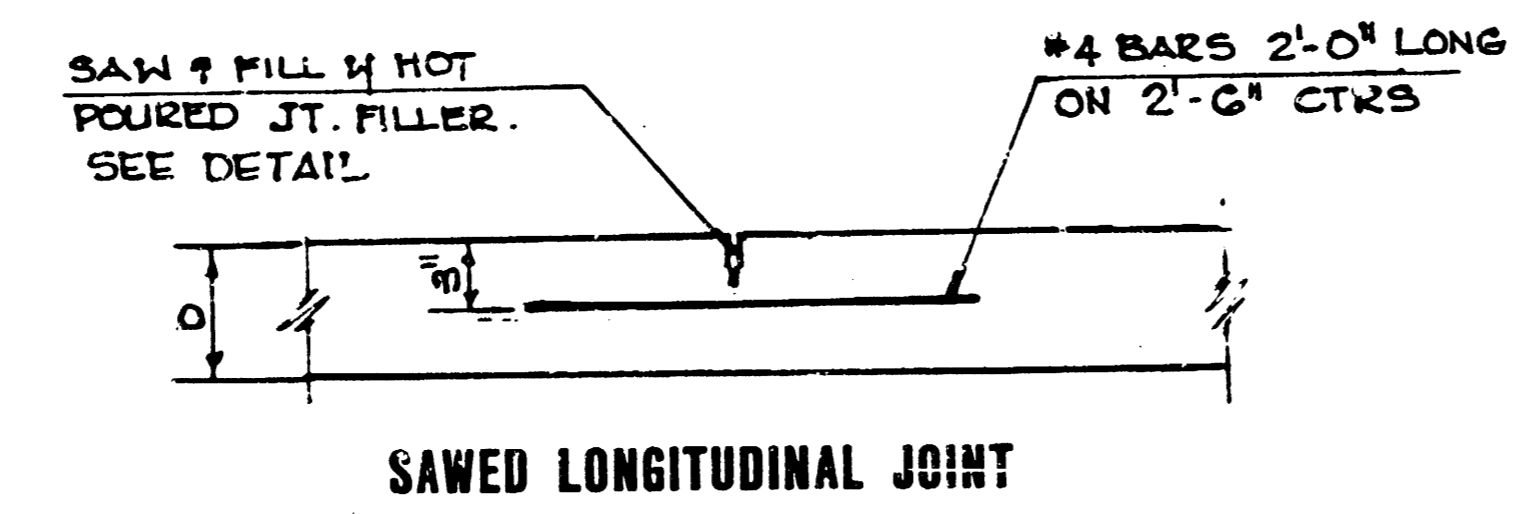
TRANSVERSE SECTION



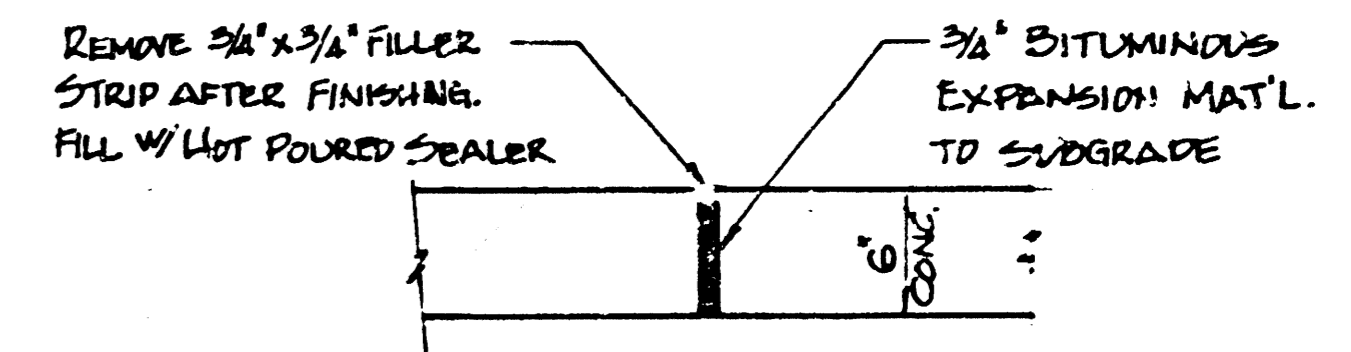
MONOLITHIC CURB FULL HEIGHT



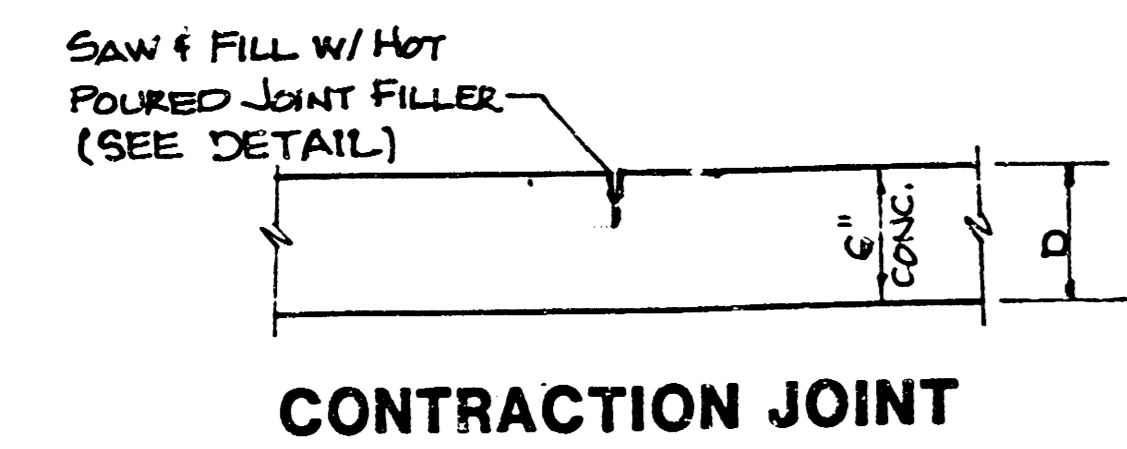
MONOLITHIC CURB (ROLL TYPE)



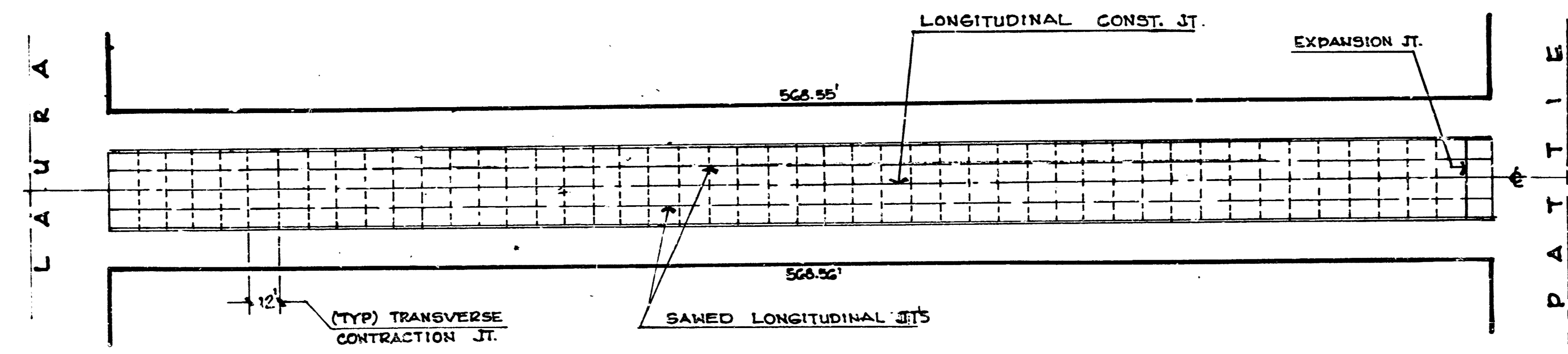
SAWED LONGITUDINAL JOINT



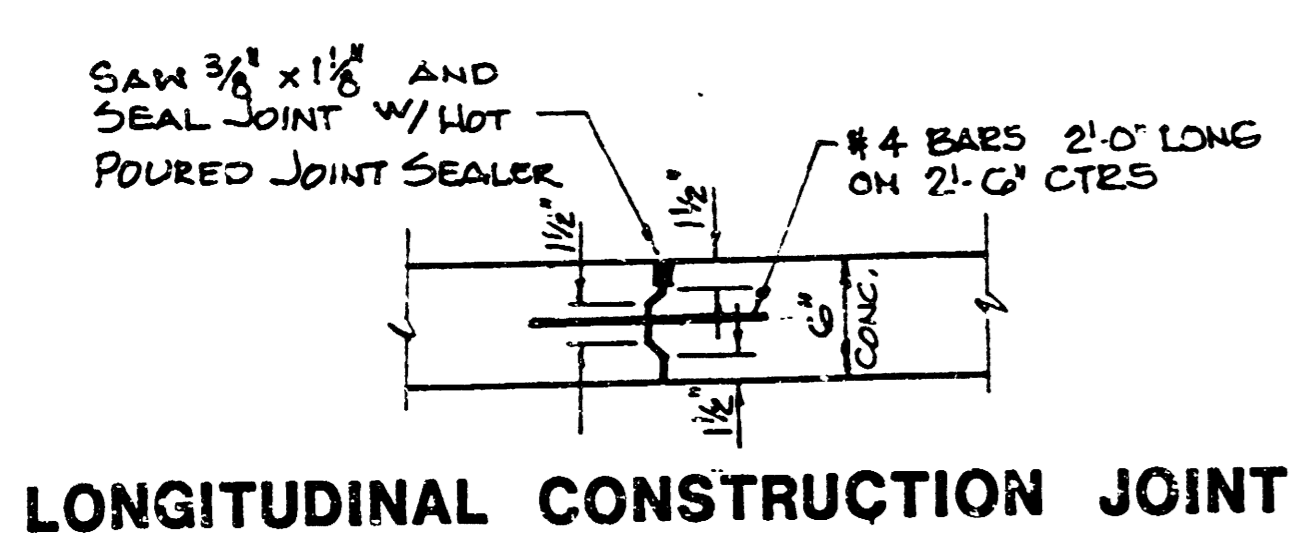
EXPANSION JOINT



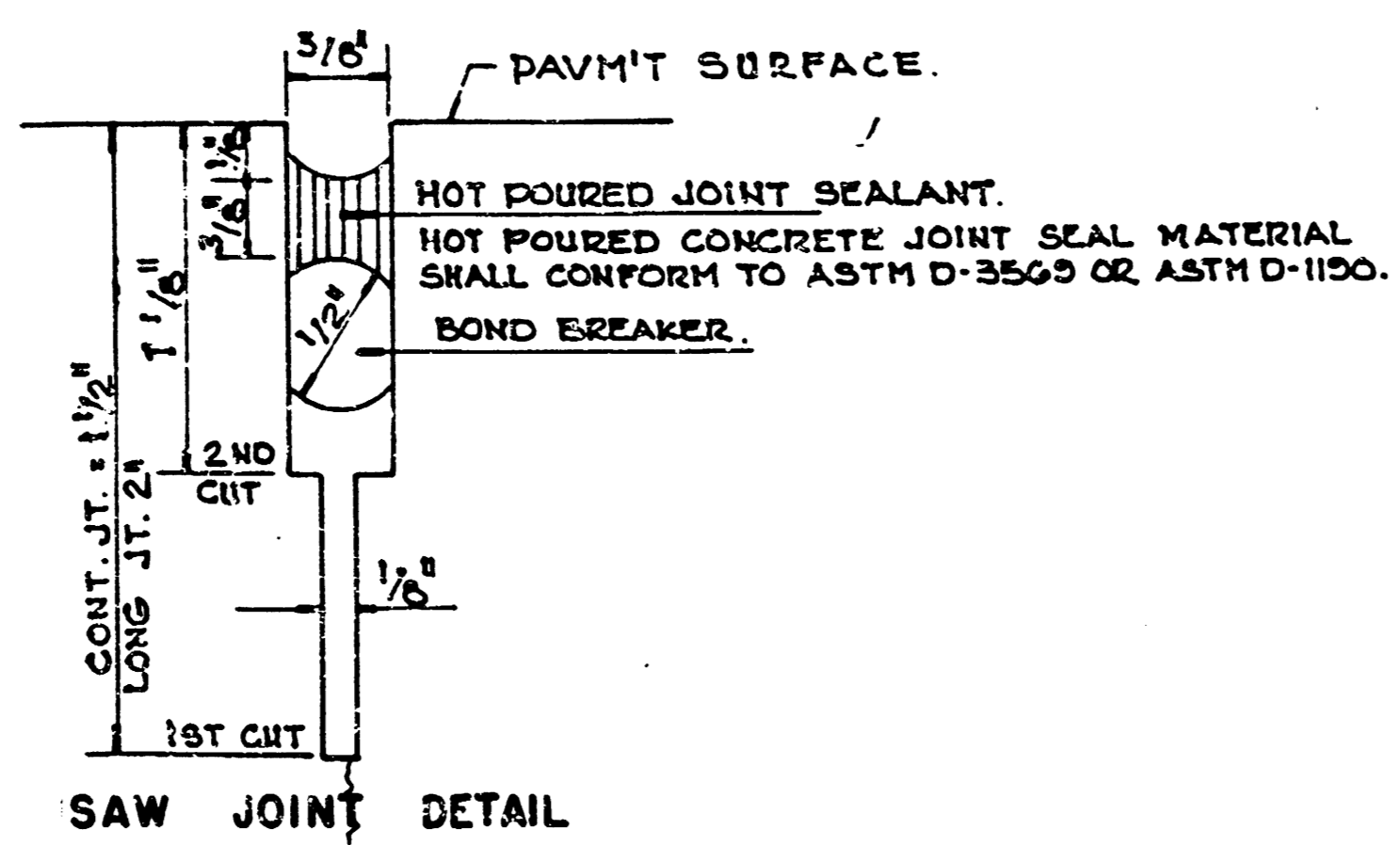
CONTRACTION JOINT



PAVEMENT JOINT LOCATIONS



LONGITUDINAL CONSTRUCTION JOINT



SAW JOINT DETAIL

6 INCH RESIDENTIAL UNREINFORCED CONCRETE PAVEMENT

CITY OF WICHITA, KANSAS

Project number 472-76-245-80669-000-000-001

TYPICAL SECTION & JOINT DETAILS

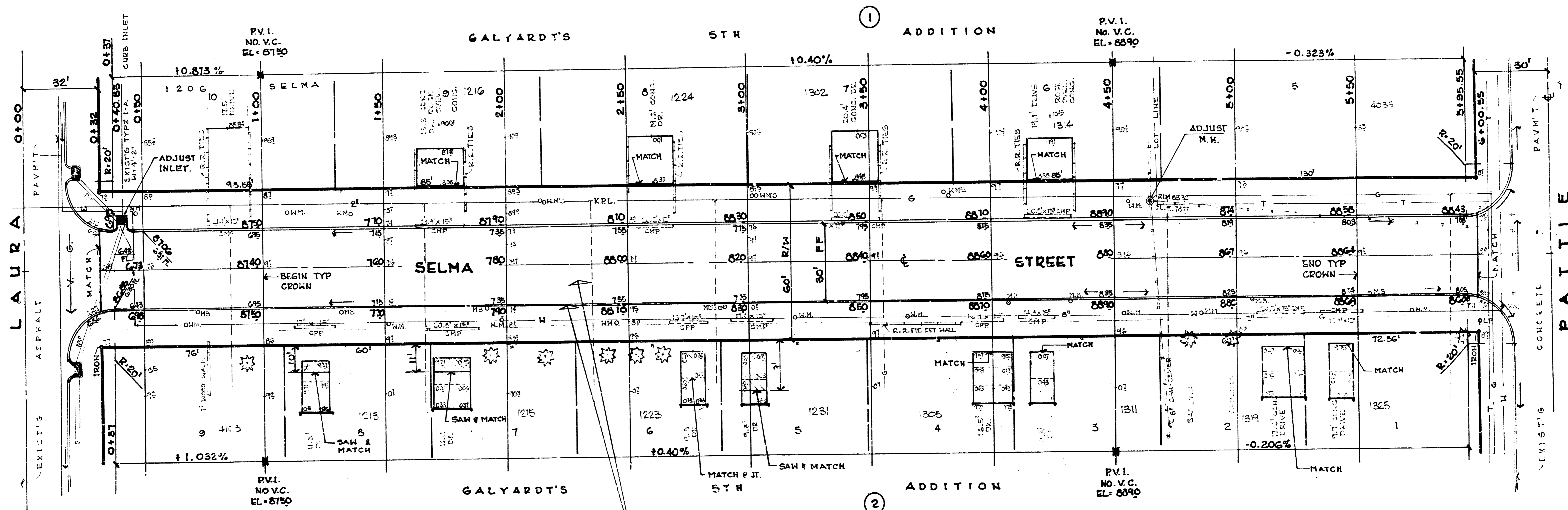
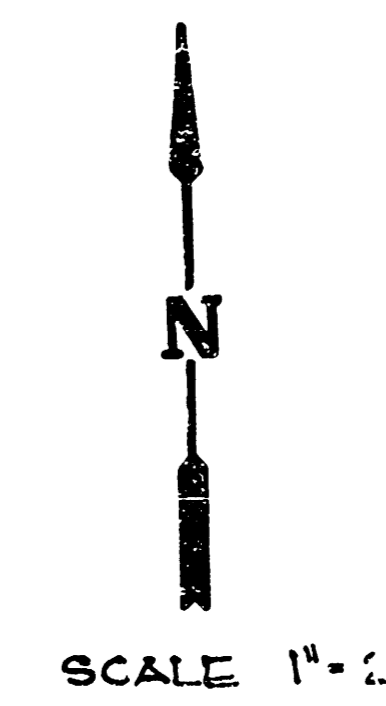
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B.M. 87.42 7' CUT ON B-CURB AT S.E. COR. OF INTERSECTION HAWAIIAN BEACH ROAD & SHADY GROVE. 100' W. OF LAURA.
 B.M. 87.11 0' CUT ON B-CURB AT E-END DRIVEWAY IN FRONT OF 1403 SELMA.

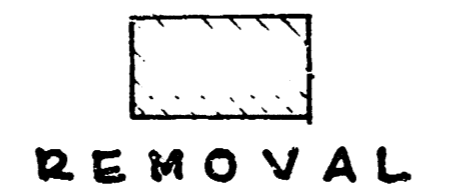
NOTE: RAILROAD TIES SHALL BE REMOVED AND SALVAGED BY THE CONTRACTOR AND GIVEN TO THE ADJACENT PROPERTY OWNER. IN THE EVENT THAT THE PROPERTY OWNER DOES NOT WANT THE RAILROAD TIES, THE CONTRACTOR SHALL DISPOSE OF SAME IN A MANNER ACCEPTABLE TO THE FIELD ENGINEER. THIS WORK TO BE DONE AT NO ADDITIONAL COST TO THE PROJECT.

NOTE: EXISTING GRAVEL TO BE REMOVED FROM OVER THE DRIVEWAYS ON THE NORTH SIDE OF SELMA AT NO EXTRA COST TO PROJECT.



CONSTRUCT 6" UNREINFORCED CONCRETE PAVEMENT W/ 6% MONOLITHIC EDGE CURB.

NOTE: DRIVE REMOVAL SHOWN WAS CALCULATED USING 0.20' WALK GRADE, AND 1 1/2" DRIVE SLOPE. FIELD ENGR. TO CONTACT PROPERTY OWNERS PRIOR TO REMOVAL FOR VERIFICATION.



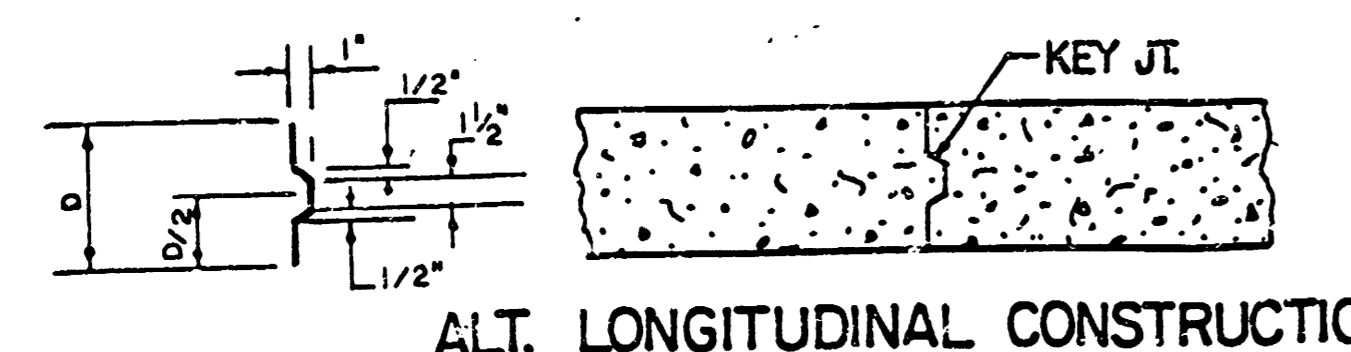
EARTHWORK QUANTITIES
 "EXCAVATION"
 1340.3 CU YDS
 110% 134.0 " "
 1474.3 CU YDS

2043 SQ YDS SUBGRADE STABILIZATION MANIPULATION

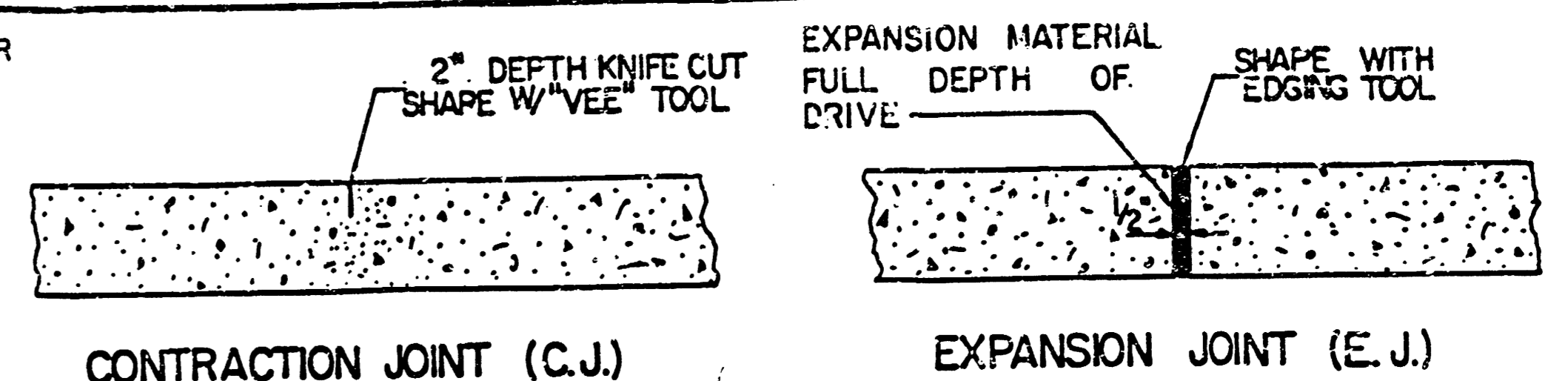
PROJECT DESCRIPTION	
SELMA STREET	3/5
E.L. LAURA TO W.L. PATTIE	
PROJECT NUMBER	
472-76-245-80669-000-000-001	

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Book 1P2-27
 14.34

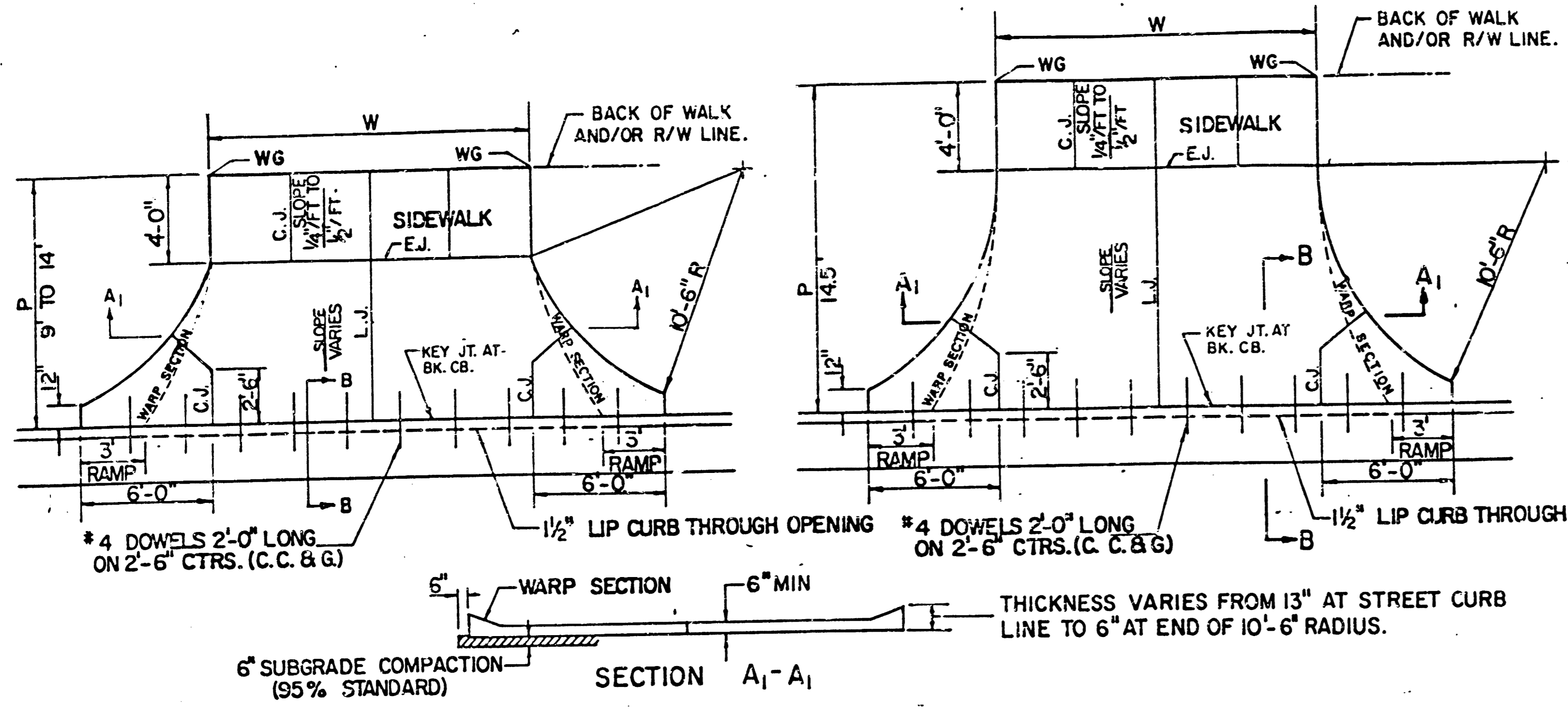


ALT. LONGITUDINAL CONSTRUCTION JOINT



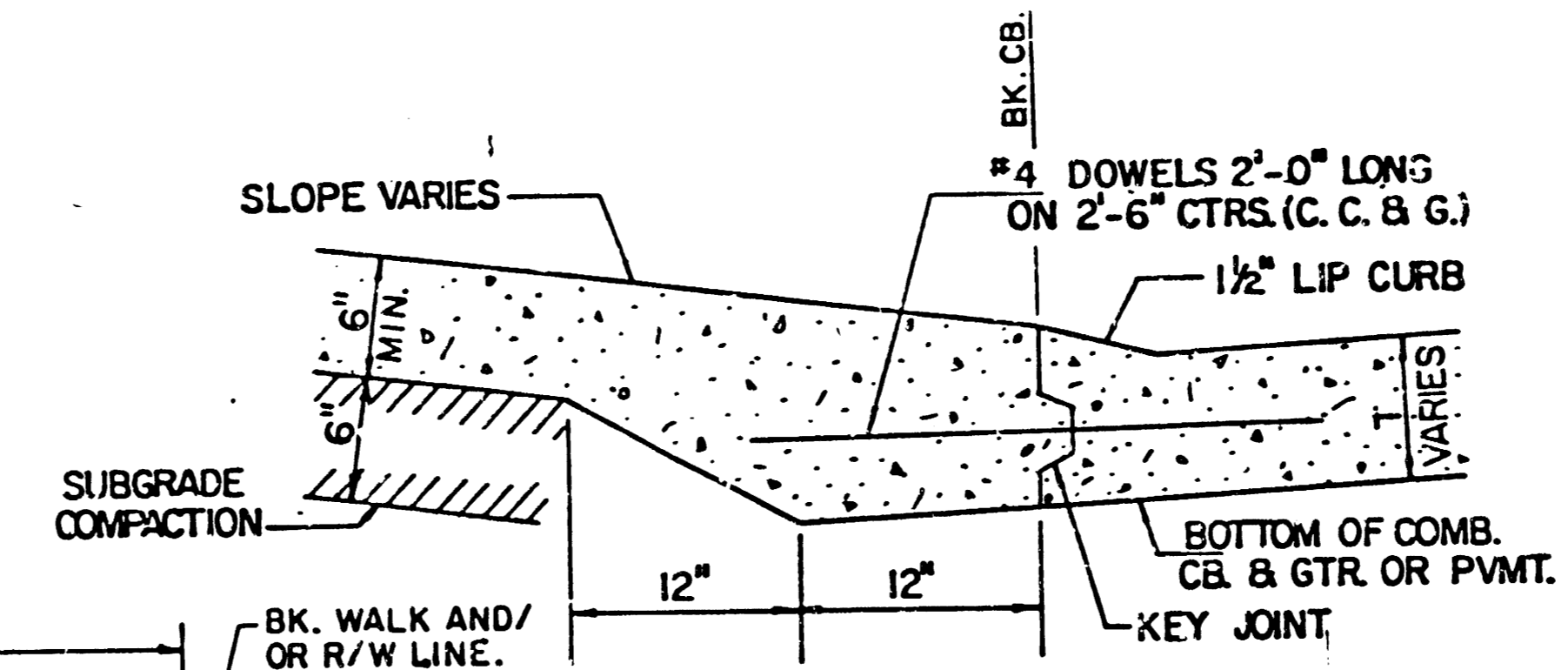
CONTRACTION JOINT (C.J.) OR LONGITUDINAL JOINT (L.J.) NO SAWN JOINTS WILL BE ALLOWED.

EXPANSION JOINT (E.J.)

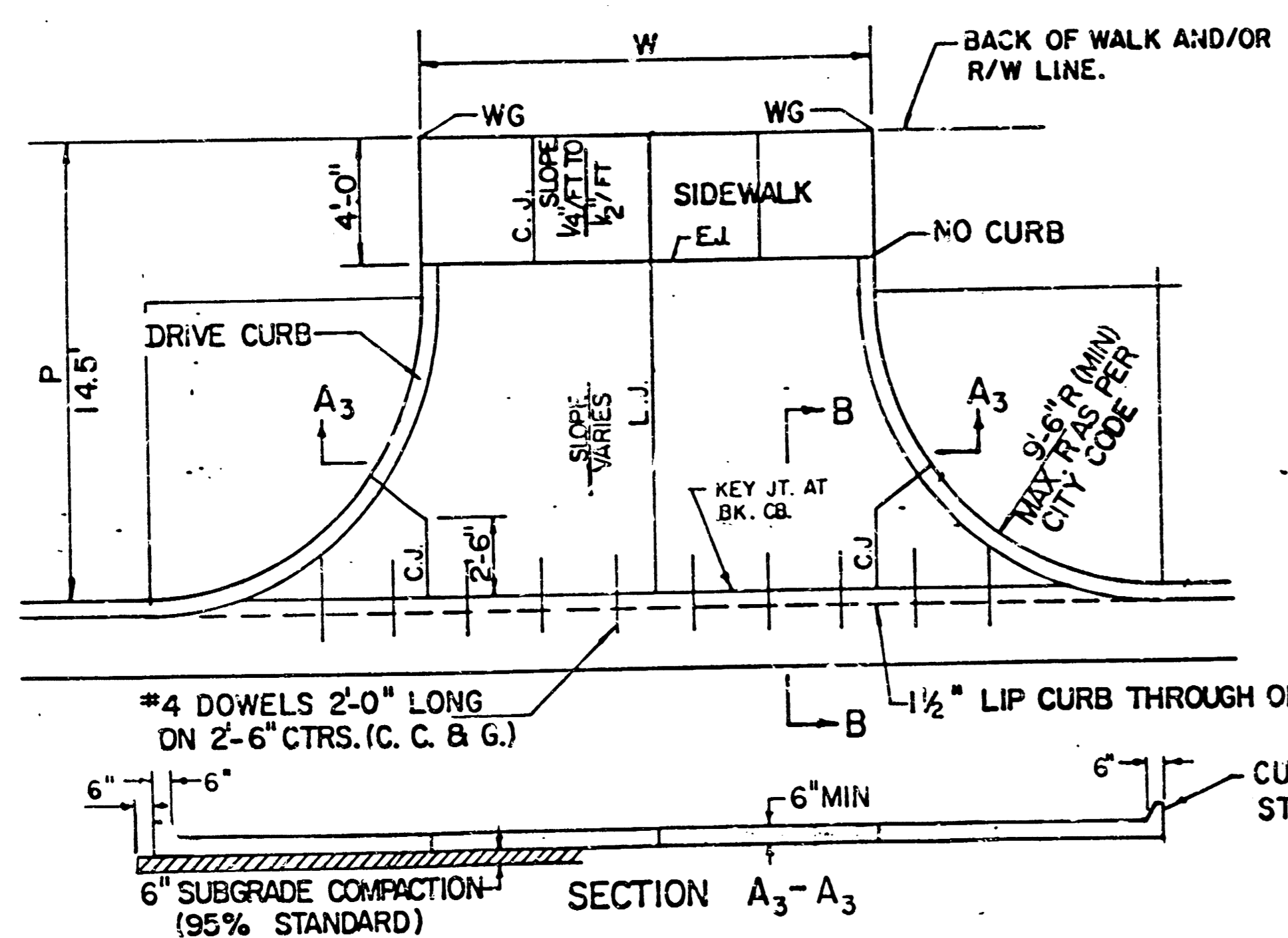


PARKING WIDTH "P"	9'	10'	11'	12'	13'	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35'	0.35'	0.40'	0.45'	0.60'	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35'	0.35'	0.40'	0.45'	0.60'	0.70'	1.04'	1.30'	1.55'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.19'	0.21'	0.23'	0.25'	0.27'	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-1.9'	-1.6'	-1.3'	-1.0'	-0.8'	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

RADIUS RAMP DRIVES (P = 9.0' & GREATER)

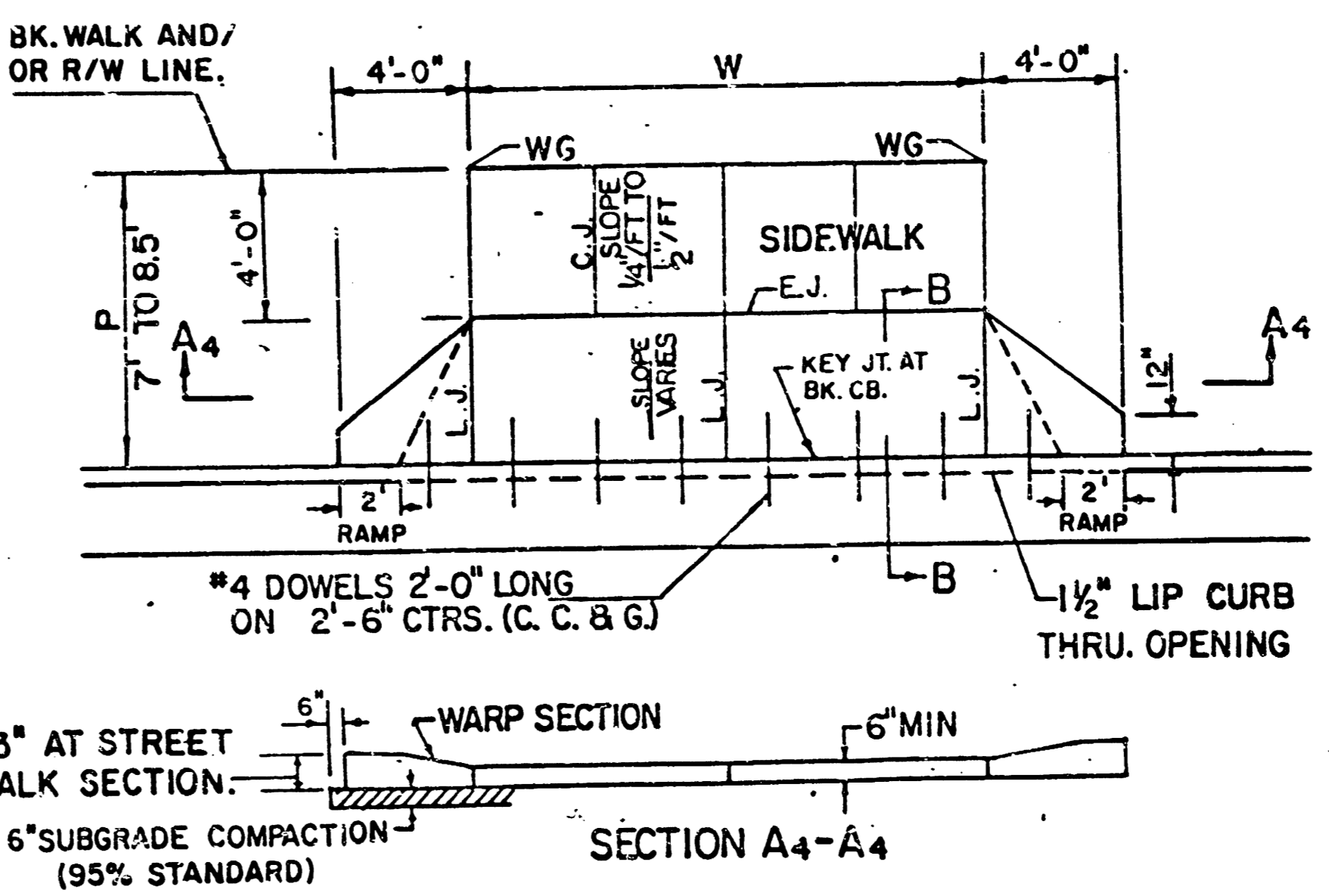


BACK OF CURB DETAIL SECTION B-B (no scale)



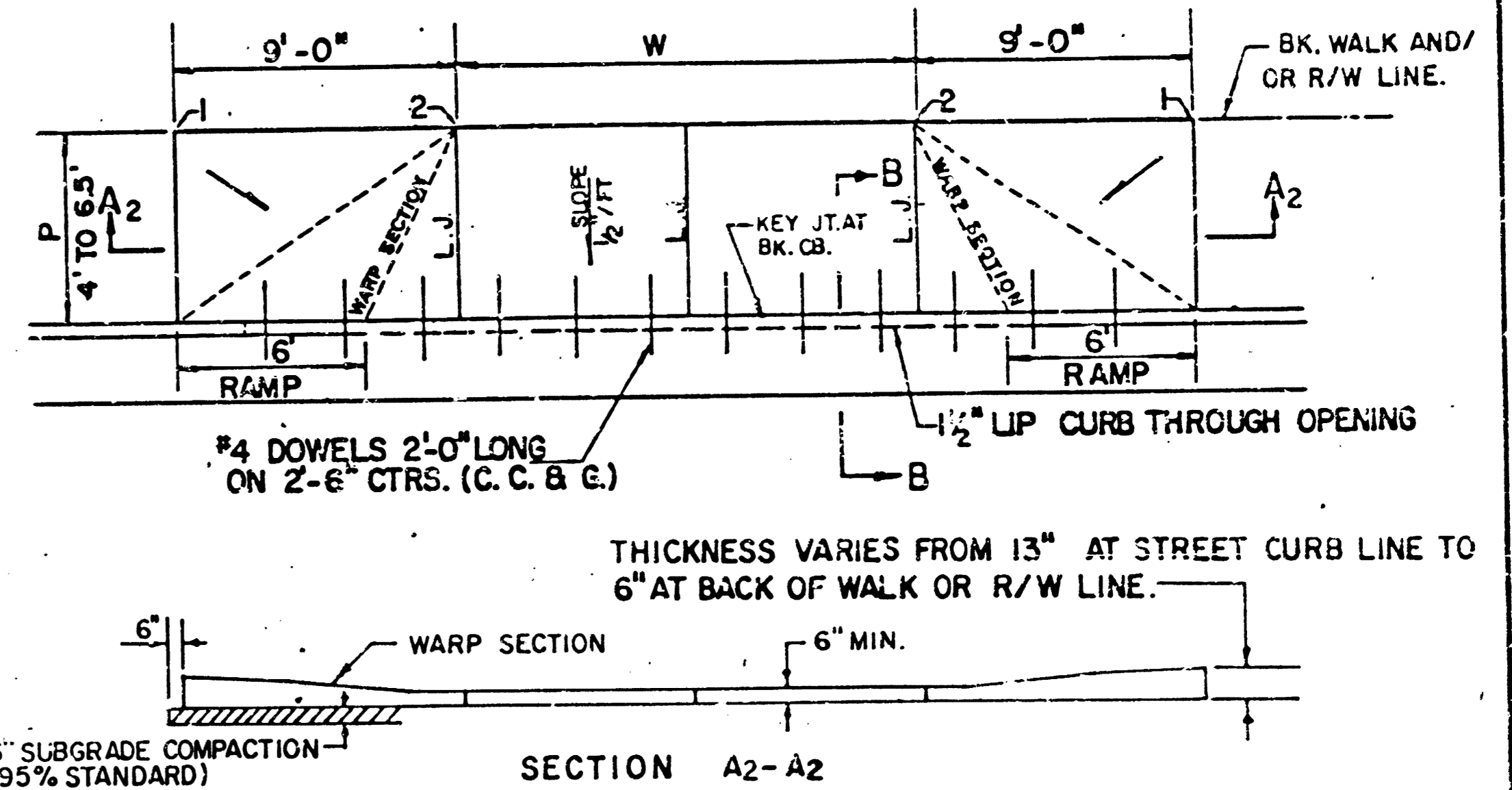
PARKING WIDTH "P"	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.80'	1.35'	1.85'	2.35'	2.85'	3.35'	3.85'	4.35'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.70'	1.04'	1.30'	1.58'	1.82'	2.08'	2.34'	2.60'
OPTIMUM MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.30'	0.42'	0.52'	0.62'	0.72'	0.82'	0.92'	1.02'
ABSOLUTE MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.00'	0.15'	0.25'	0.35'	0.45'	0.55'	0.65'

FULL RADIUS DRIVES (P = 14.5' & GREATER)



PARKING WIDTH "P"	7'	7.5'	8'	8.5'
ABSOLUTE MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.10'	0.20'	0.30'
OPTIMUM MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00'	0.10'	0.20'	0.30'
OPTIMUM MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-1.15'	-1.16'	-1.17'	-1.17'
ABSOLUTE MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-2.25'	-2.20'	-2.20'	-2.20'

FULL RAMP DRIVE (P = 7.0' TO 8.5')



PARKING WIDTH "P"	4'	4.5'	5'	5.5'	6'	6.5'
DIST. OF PT. "T" ABOVE TOP OF FULL CB.	0.08'	0.09'	0.10'	0.12'	0.13'	0.14'
DIST. OF PT. "Z" BELOW TOP OF FULL CB.	-0.26'	-0.24'	-0.22'	-0.20'	-0.18'	-0.16'

FULL RAMP DRIVE (P = 4.0' TO 6.5')

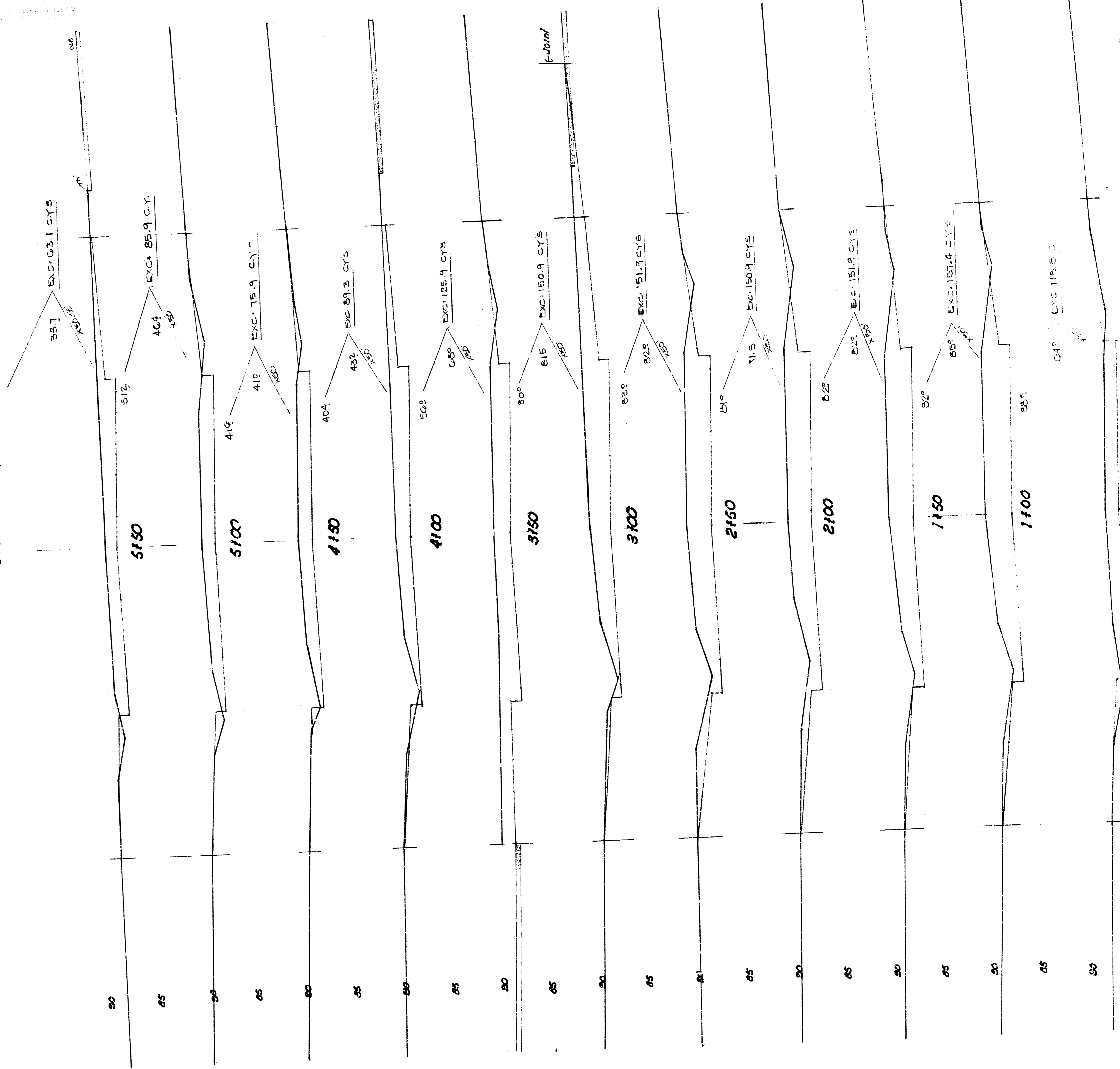
- GENERAL NOTES
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH ROLL CURB AND/OR FULL WALK.
 - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "W" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "W" DIMENSION GREATER THAN 24'.
 - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
 - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
 - DOWEL BARS SHALL BE OMITTED FROM THE KEYS CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
 - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
 - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
 - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" W-A-WA WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONFORMANCE.
 - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHEREVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THESE VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVES OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55'. VALUES SHOWN IN THE TABLES AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.

REVISED OCTOBER 1985
SCALE: 1" = 5'

STANDARD DRIVE ENTRANCES
FULL HEIGHT CURB
CITY OF WICHITA, KANSAS
PROJECT NUMBER
472-76-245-80629-000-000-001

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6100.55 MATCH EXISTING C = 10.2 FT.



5150 EXC. 83.1 C.Y.S.
 5100 EXC. 85.9 C.Y.
 4150 EXC. 76.9 C.Y.
 4100 EXC. 81.3 C.Y.
 3150 EXC. 125.9 C.Y.
 3100 EXC. 150.9 C.Y.
 2150 EXC. 151.9 C.Y.
 2100 EXC. 150.9 C.Y.
 1150 EXC. 151.9 C.Y.
 1100 EXC. 115.5 C.Y.
 0150 MATCH EXISTING C = 10.2 FT.

S.E.L.M.A.
 E.L. LAURA TO W.L. PATTIE
 Proj. No. 412-76-245-8069-000-001



SHEET TOTAL 1340.3 C.Y.S.

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