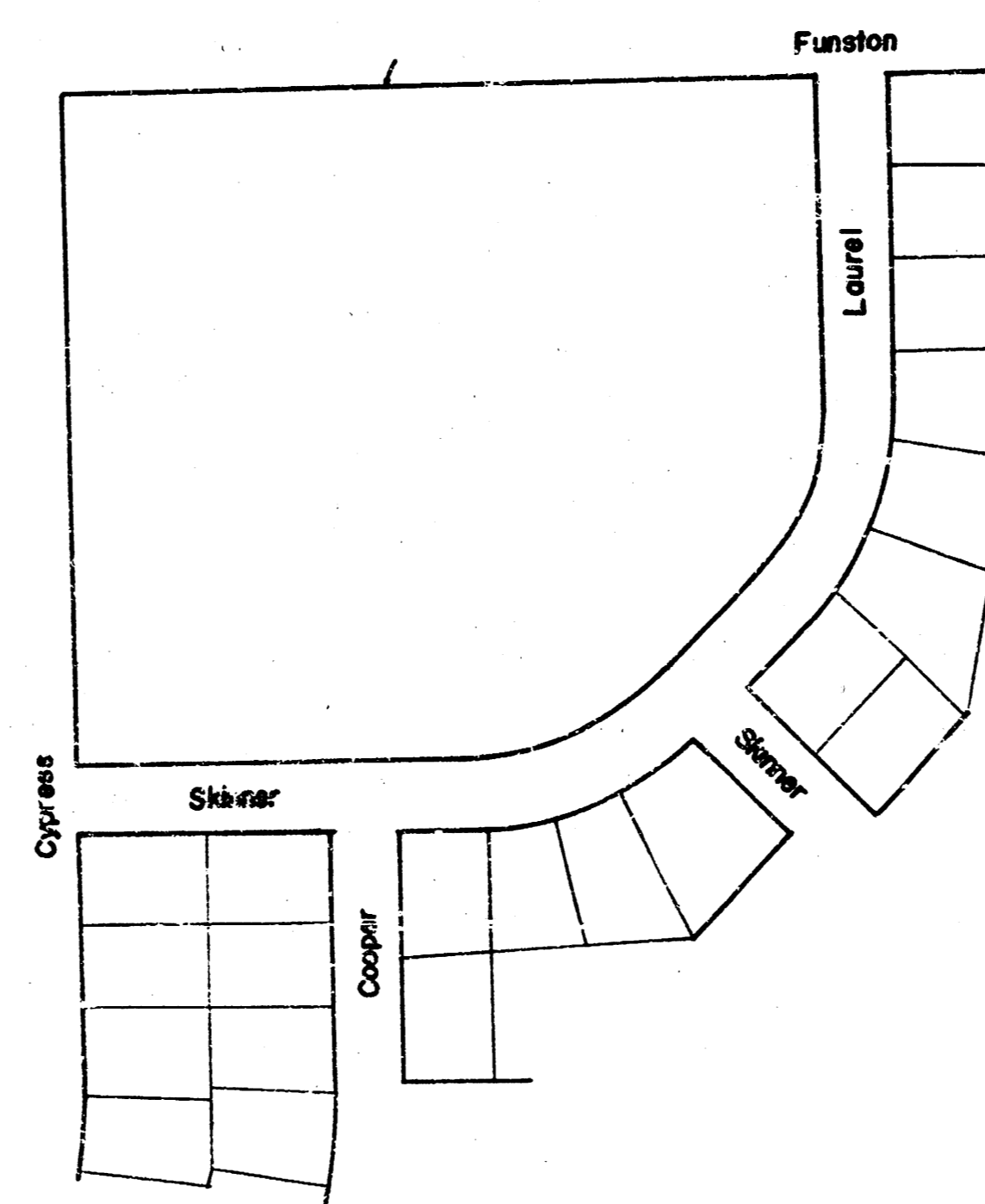
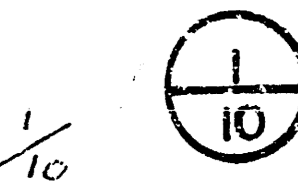
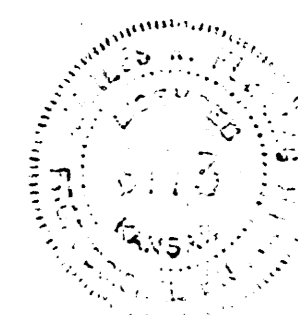
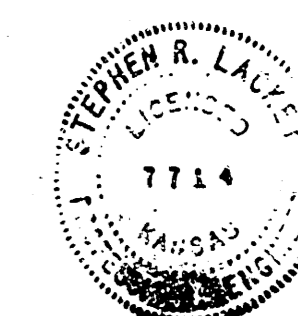


Skinner and Laurel  
from EL Cypress to SL Funston

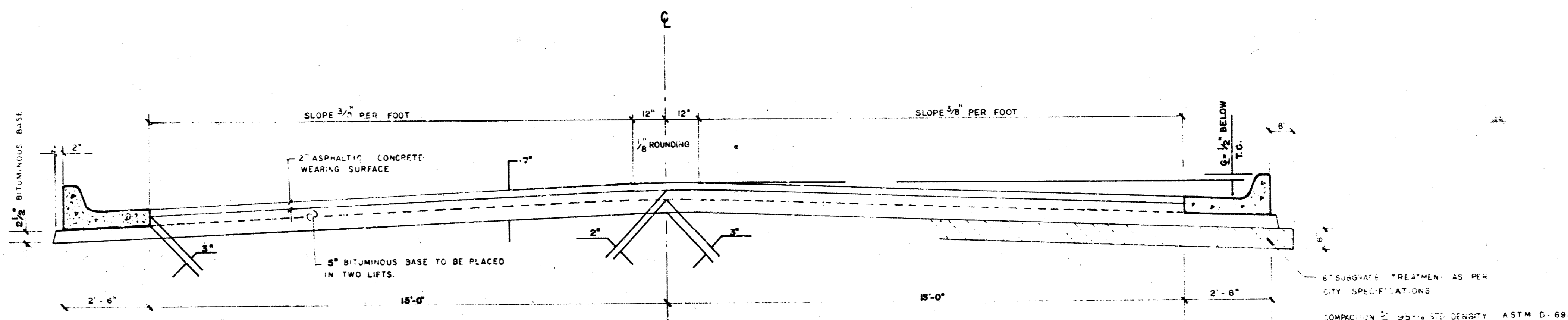
Skinner  
from SL Laurel to Southeasterly line of  
lot 1, blk 3 and Southeasterly line of lot 1  
blk 2, Cedar Ridge 1st Add.



Department of Engineering  
City of Wichita, Kansas  
R.W.Bruggeman Director Engineering / City Engineer  
Proj. no. 472-76-245-80998-000-000-001  
Date \_\_\_\_\_



1/10



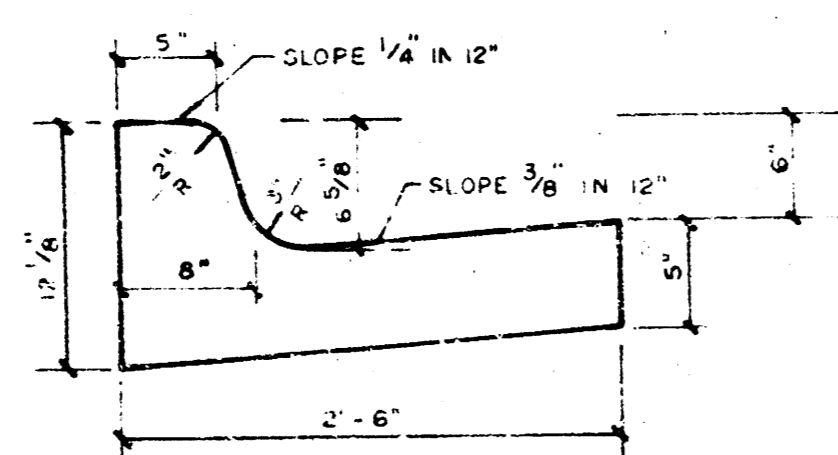
**TYPICAL SECTION**

**35' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE**

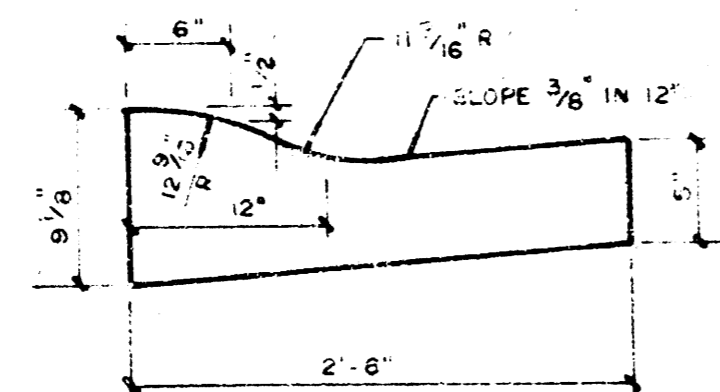
A TACK COAT OF EMULSIFIED ASPH (SS-1M or CSS-1M) SHALL BE APPLIED AT AN APPROXIMATE RATE 0.05 GALLONS PER SQ YD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT. BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND PLACED SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE IN THE TOP LIFT.

THE A.C. PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQ YDS 7" A.C. PAVEMENT (6" BITUMINOUS BASE). THE BITUMINOUS BASE UNDER THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQ YDS 2 1/2" BITUMINOUS BASE.

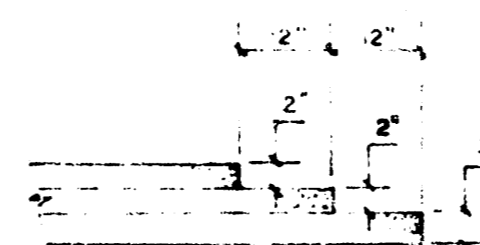
COMBINED CURB & GUTTER



ROLL TYPE CURB & GUTTER



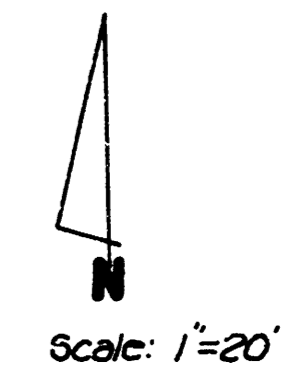
**DETAIL OF TRANSVERSE CONSTRUCTION JOINTS**



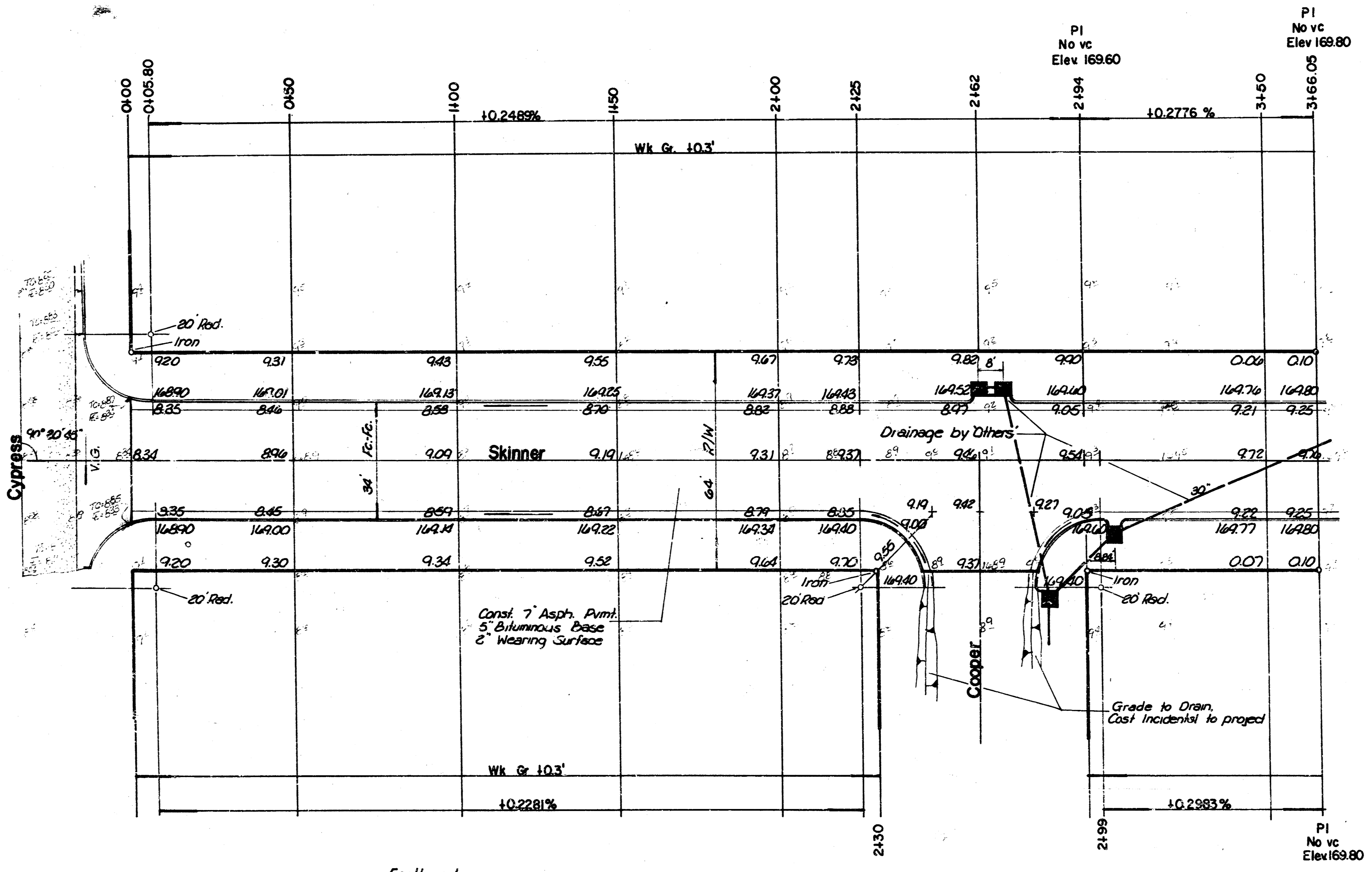
TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF A.C. PAVEMENT.

CITY OF WICHITA, KANSAS

BM 169.52 R.R. spike SW face HPL North side Mt. Vernon, 30' East of Cypress.  
 BM 170.06 '□' top S. curb Funston, 5' E. of WL Cypress.  
 BL is  $\frac{1}{2}$  R.O.W.



Simon, O.K. 4/4  
 Plan Review  
 Ext. Checked



THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY AND THE CONTRACTOR SHALL BE RESPONSIBLE TO RE-ESTABLISH ANY PROPERTY LINES WHICH ARE DAMAGED OR DESTROYED BY THE CONTRACTOR'S OPERATIONS. THIS SHALL BE RE-ESTABLISHED BY A LICENSED SURVEYOR OR LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

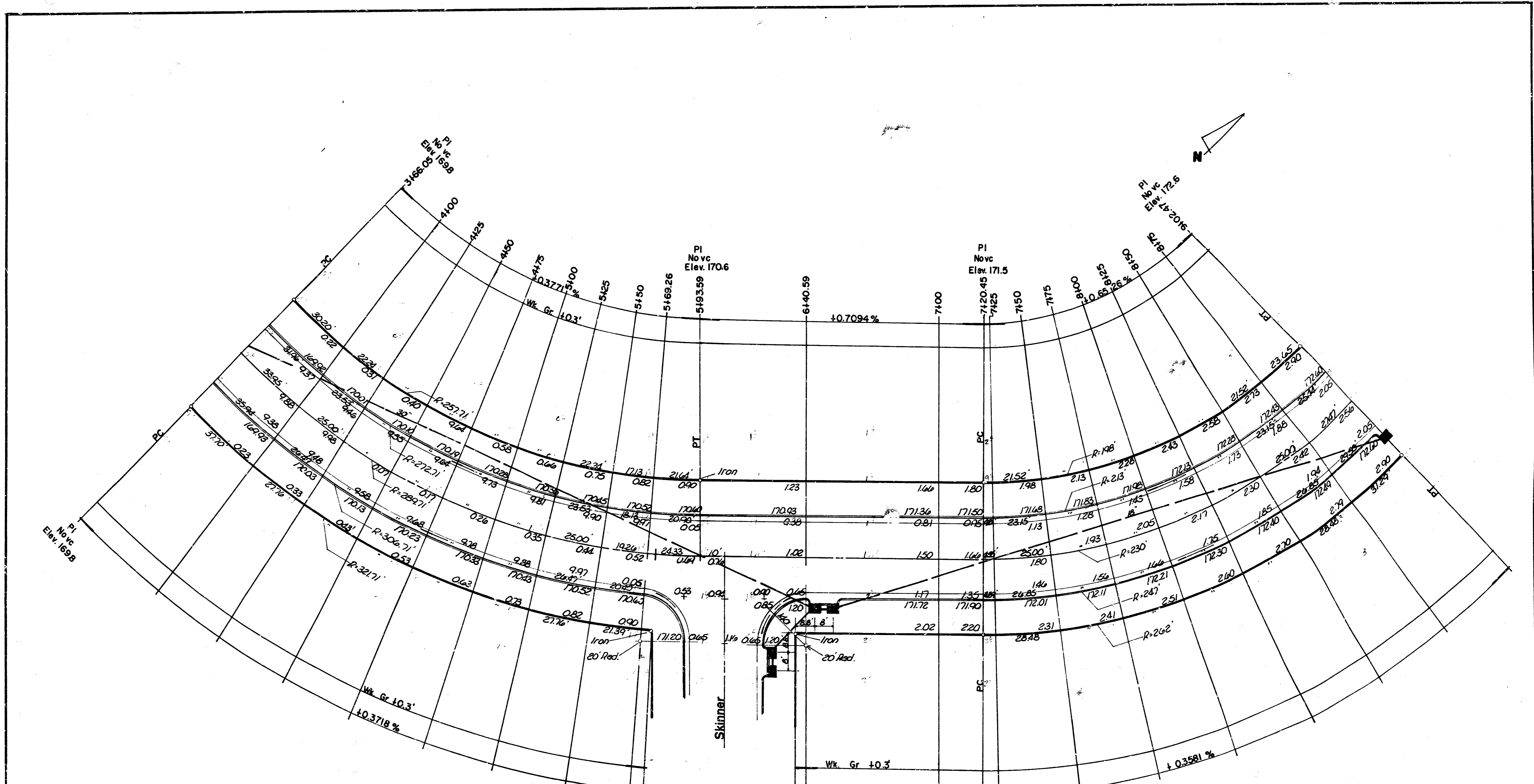
	City	Earthwork	Property
Excavation		202.8 cy	2584.1 cy
10%		20.3 cy	258.4 cy
Total		223.1 cy	2842.5 cy
Manipulation			534.1 sy

- 18.6 S.Y. Concrete Footing
- 13.2 S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)
- 13.2 S.Y. 2 1/2" Bituminous Coat
- 13.2 S.Y. 2 1/2" Bituminous Coat
- 10.16 C.Y. Excavation
- 10.16 C.Y. Concrete
- 10.16 C.Y. Concrete
- 10.16 C.Y. Concrete
- 22.5 S.Y. Manipulation
- 2.5 Tons Line of Center
- S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)

**SUB-GRADE**  
 TYPE OF SUB-GRADE TREATMENT SHALL BE DETERMINED BY THE FIELD ENGINEER. SUB-GRADE TREATMENT MAY CONSIST OF ANY TREATMENT, CEMENT TREATMENT, SAND, OR GRAVEL, OR ANY COMBINATION THEREOF.

Skinner and Laurel  
 from EL Cypress to SL Funston

5685 sq. ft. of drives to be const.



$\Delta = 45'00''$  P = 289.71 T = 20.00 L = 227.54 I.C. = 221.73

CURVE DATA BASED ON  $\Delta$  RAD. = 22.30

STA	ARC	CHORD LENGTH	DEFLECTION	TOTAL DEFLECTION
3166.00				
4100	33.95	31.02	34.88	3'21.24'
125	25.00	22.84	27.16	2'38.20'
175				8'18.04'
5100				10'41.04'
125				15'43.04'
150	25.00	22.84	27.16	2'38.20'
169.26	19.26	17.60	20.92	1'54.15'
193.54	24.33	22.23	24.21	2'30.00'

Defl. min/n = 5.933022765

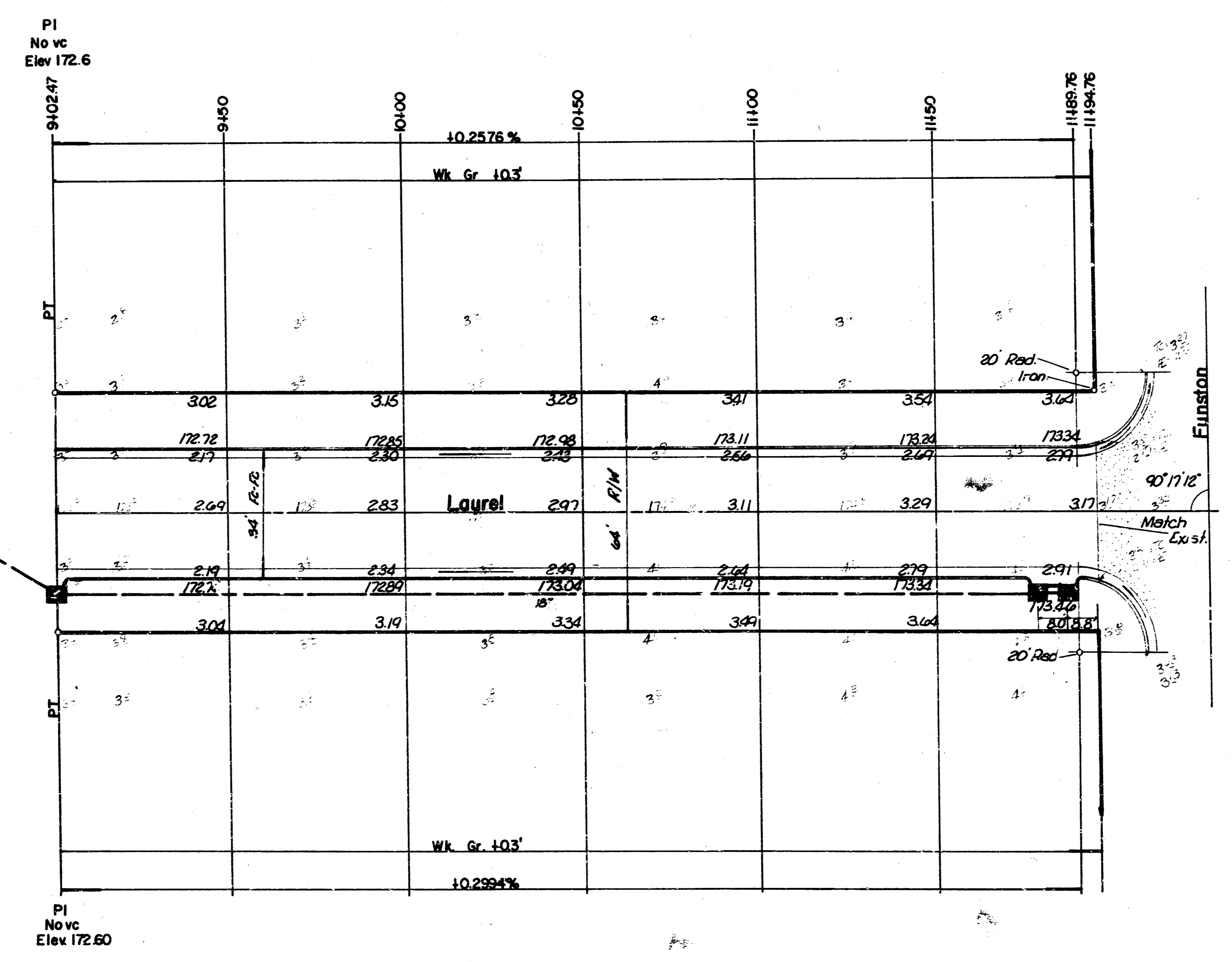
- DEFLECTION CURVES
- 184 S.V. Concrete Pavement
  - 132 S.V. Asphaltic Conc. Pavement (5" Blumous Base)
  - 424 S.V. Bituminous Base
  - LF. Concrete S&G
  - LF. Bituminous
  - S.F. 4" x 6" x 12" Ramp
  - S.F. 4" x 6" x 12" Ramp
  - 1014 C.V. Exc.
  - 128 C.V. Comp. 10" x 10"
  - 2015 Lbr. Reinforcing Steel
  - 22 S.V. Manhole
  - 22 S.V. Time or Cent.
  - S.V. V.G. Concrete & Asphalt to Concrete Base

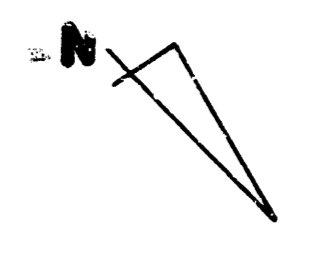
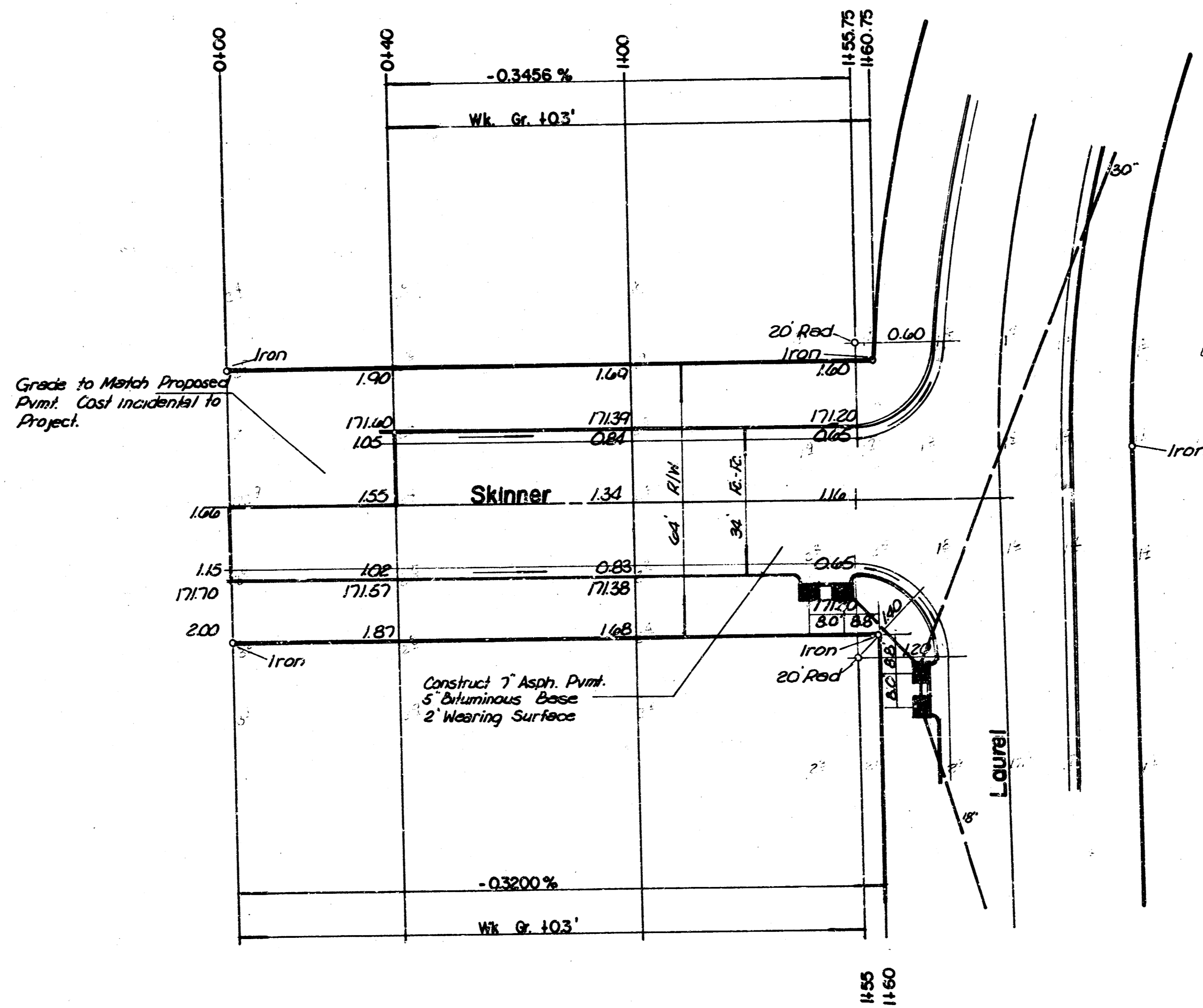
$\Delta = 62'00''$  P = 230 T = 96.08 L = 420.2 I.C. = 173.1

CURVE DATA BASED ON  $\Delta$  RAD. = 22.015

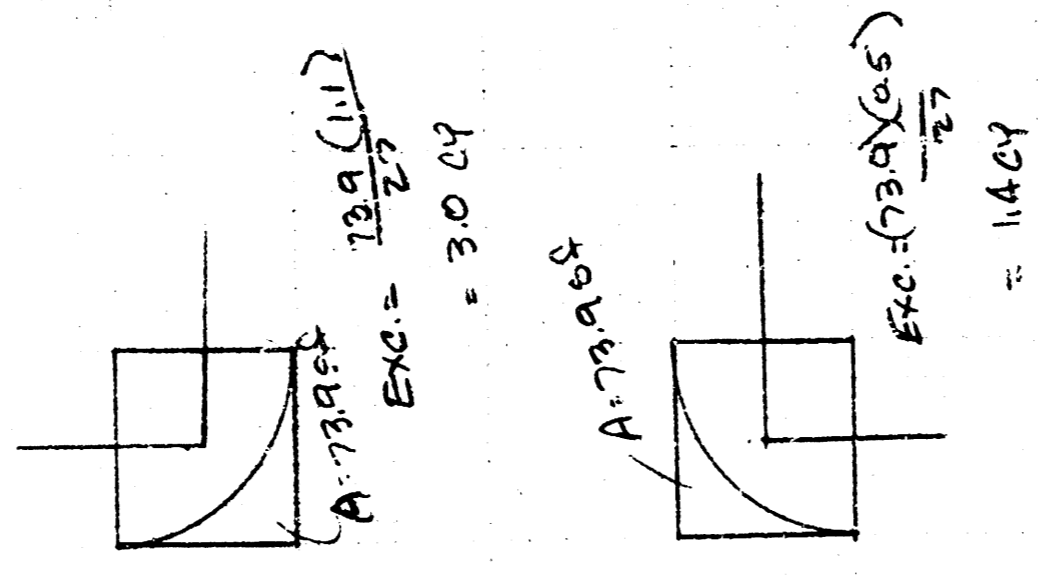
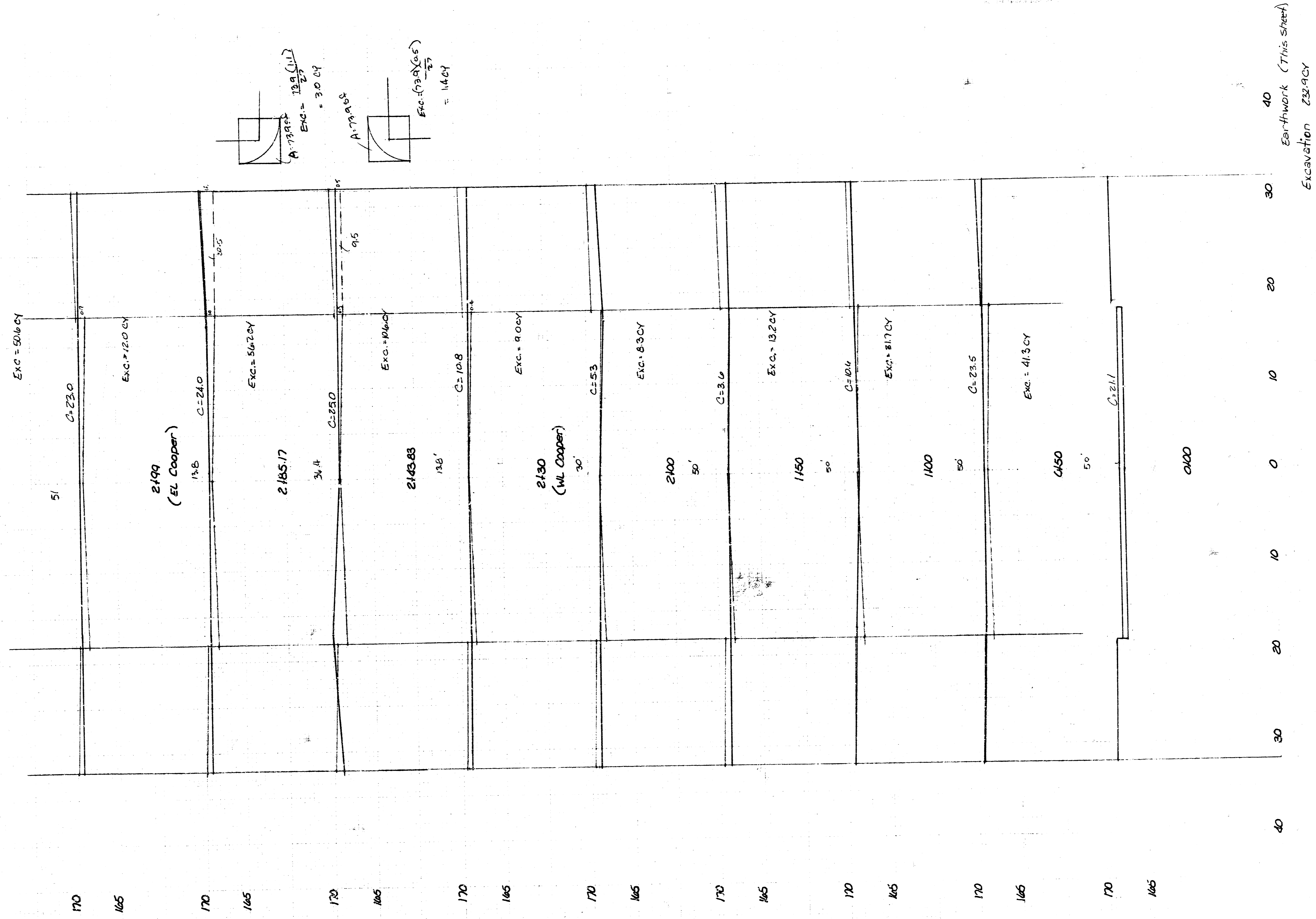
STA	ARC	CHORD LENGTH	DEFLECTION	TOTAL DEFLECTION
7120.45				
125	4.55	4.04	5.05	0'34.01'
150	25.00	22.28	27.72	3'06.50'
175				4'47.41'
8100				9'54.31'
125				13'01.21'
150				16'08.11'
175	25.00	22.28	27.72	3'06.50'
9120.47	27.47	24.48	30.46	3'25.17'

Defl. min/n = 7.47365217

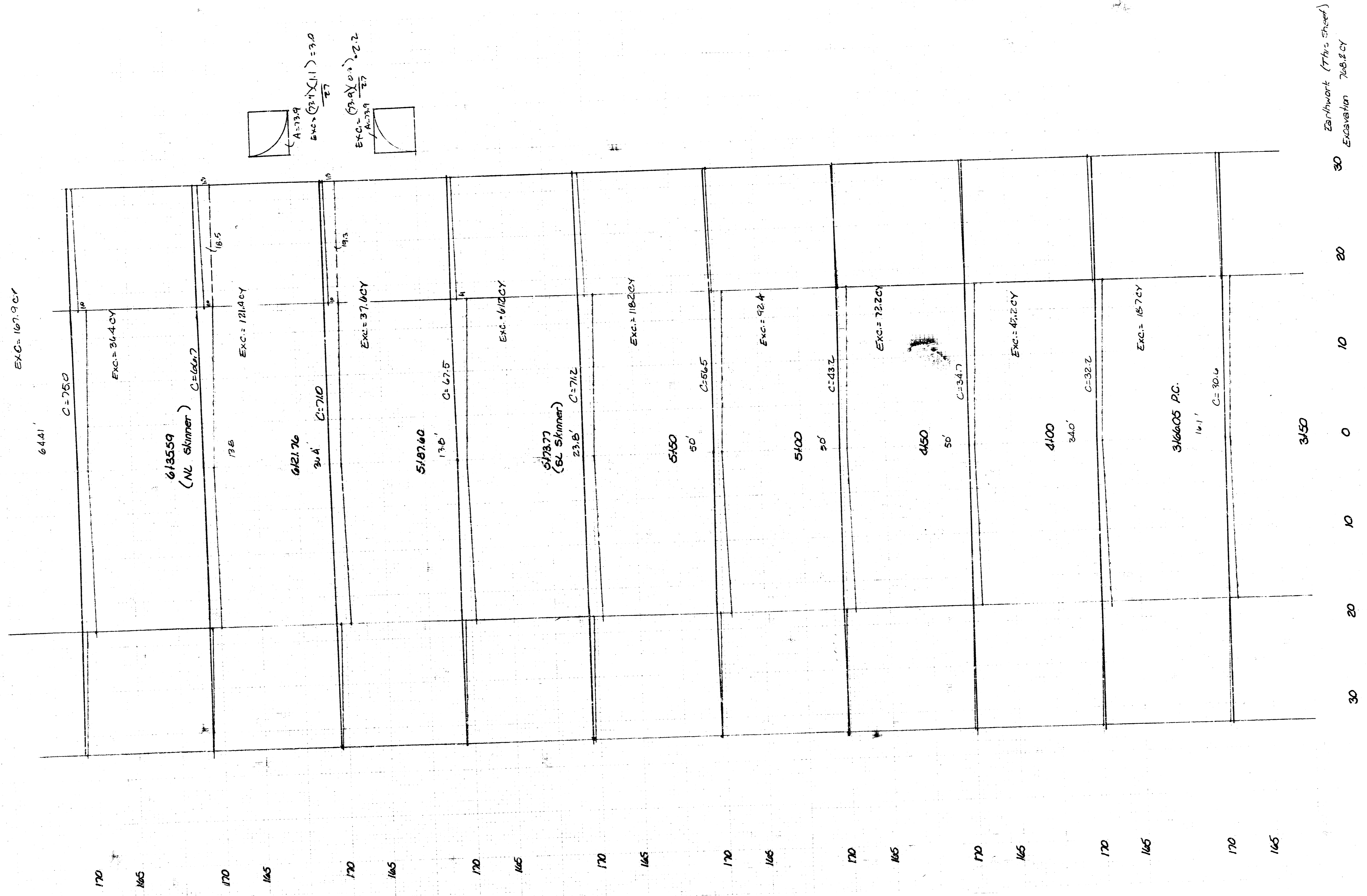




Skinner  
 from S.L. of Laurel to Southeasterly line of lot 1 blk 3  
 and Southeasterly line of lot 1 blk 2 Cedar Ridge 1st Addn.

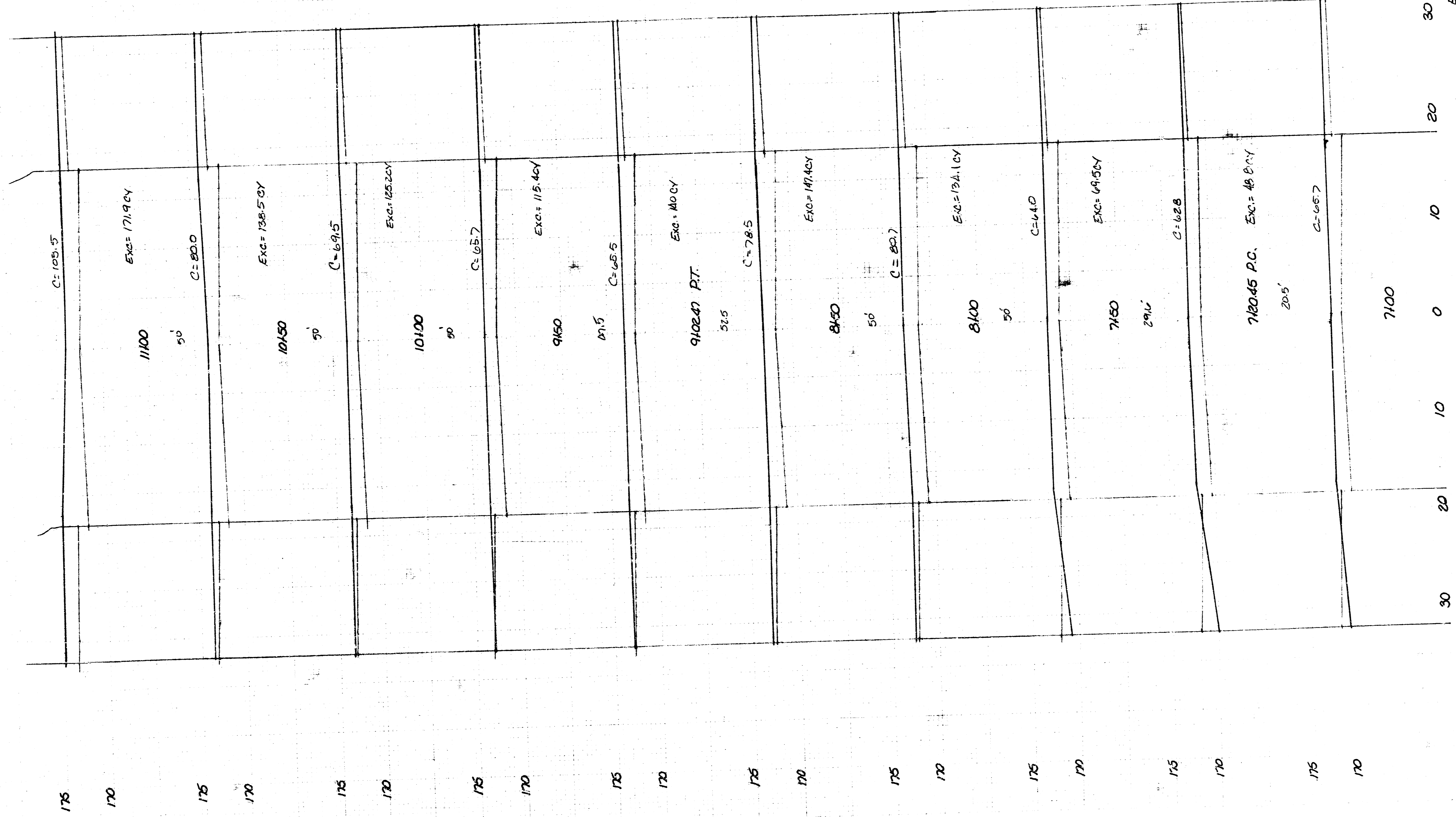


40  
Earthwork (This sheet)  
Excavation 232.9CY



Earlhwark (This Street)  
Excavation 706.8 CY

Exc. = 186.7CY

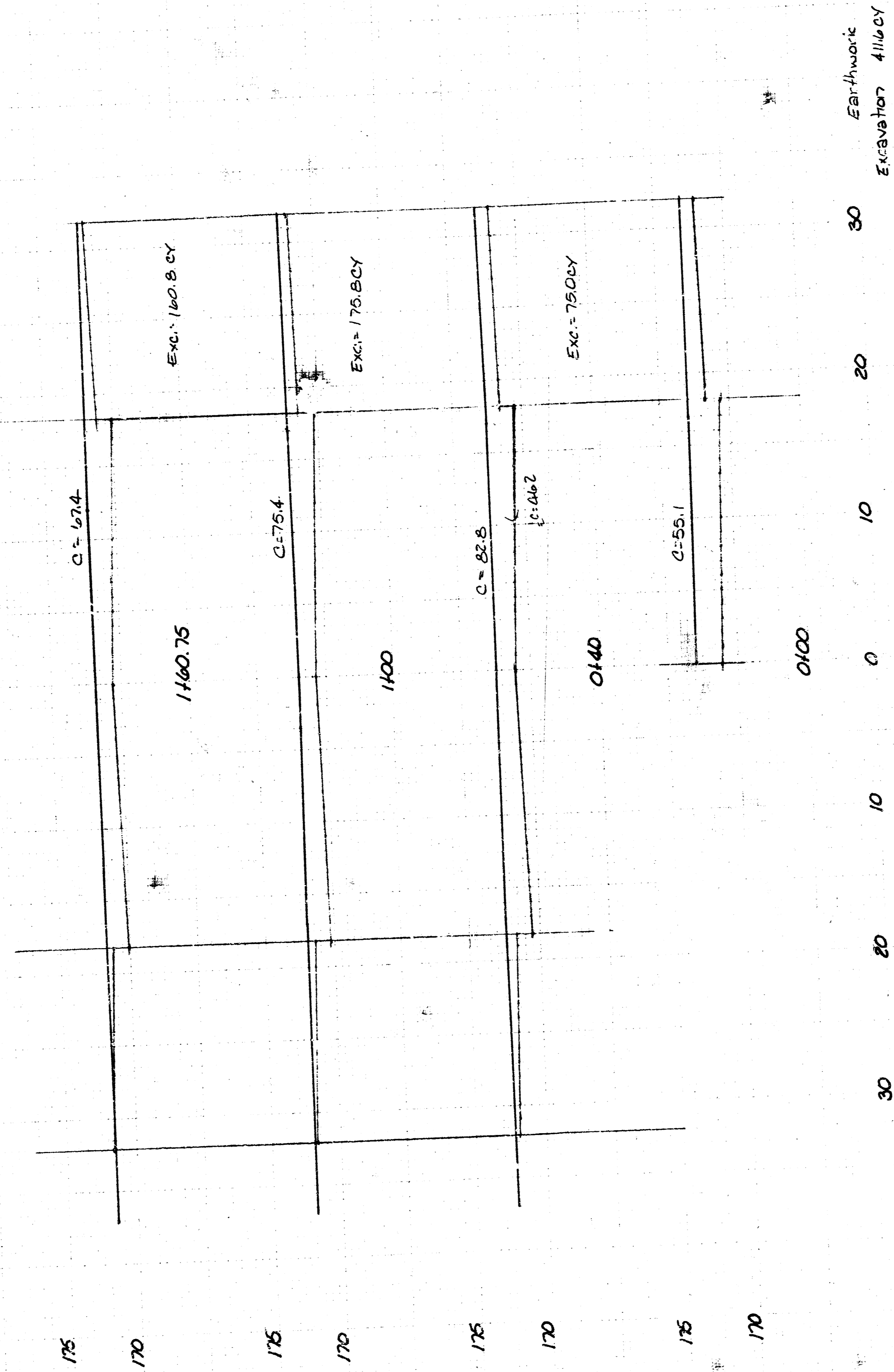


Excavation 1277.5CY  
Earthwork (This Sheet)

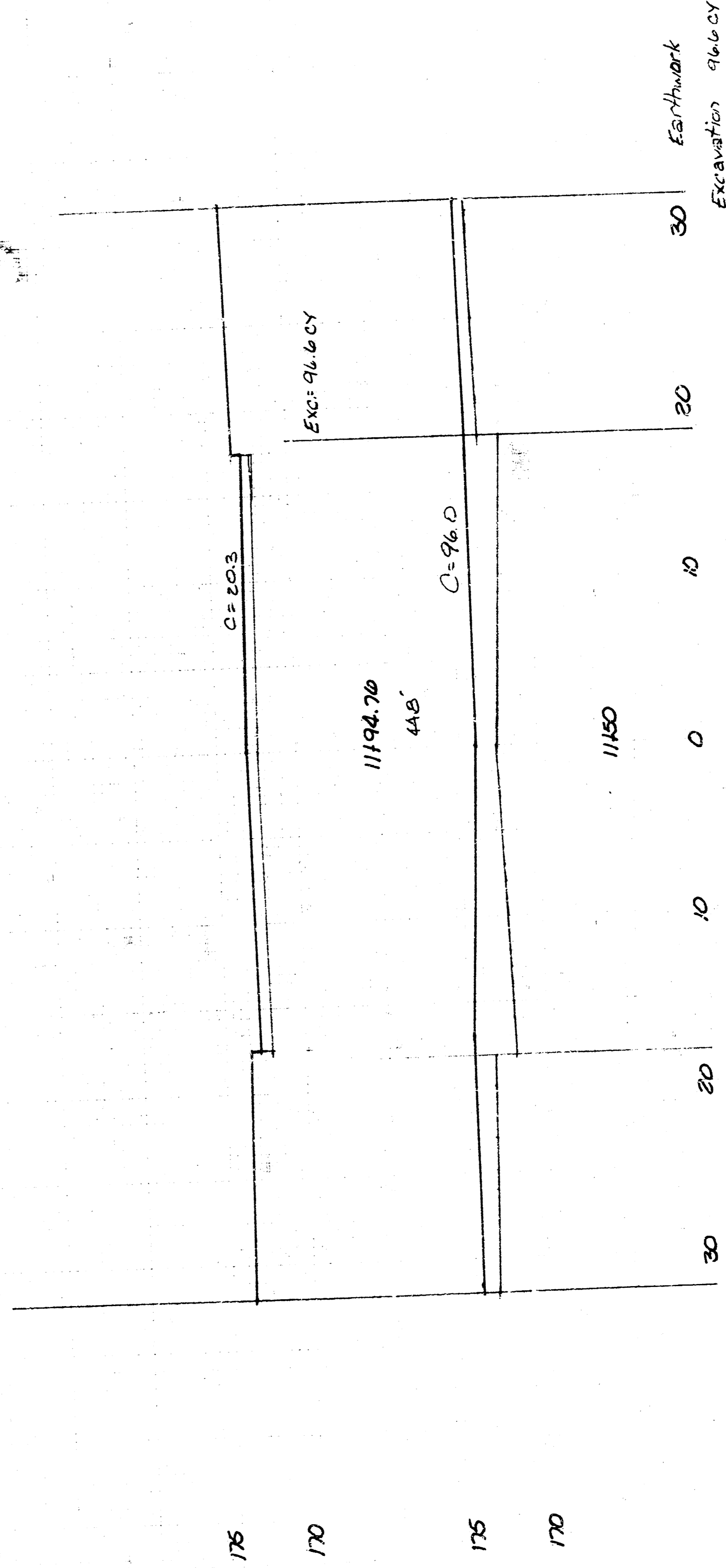
CHARLES W. BARNES CO.  
100 N. 1st Street, St. Louis, Mo.

9/10

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Skinner  
 SL Laurel to SE line of lot 1, blk 3  
 and SE line of lot 1 blk 2 Cedar Ridge lot.



10 4 4 4 10 % 10