

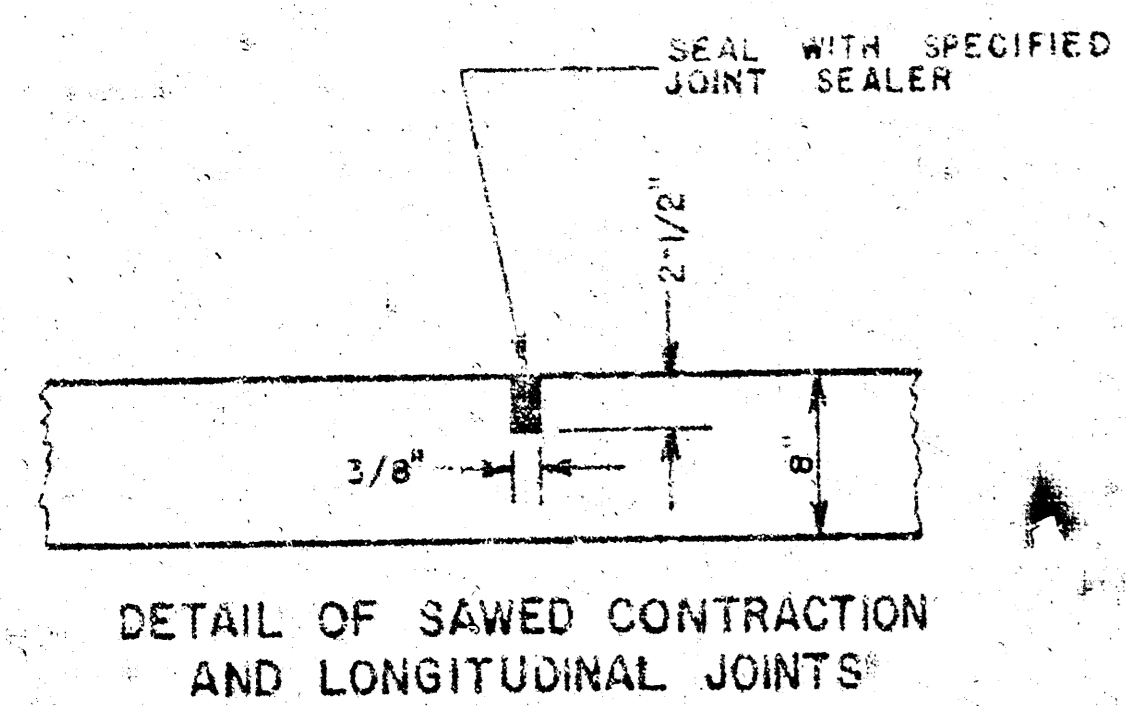
IMPROVEMENT OF TULSA STREET
E.L. HYDRAULIC AVE. TO POINT 300' EAST

PROJ. NO. DBKW 705048
CITY OF WICHITA, KANSAS
R. W. LINN, CITY ENGINEER

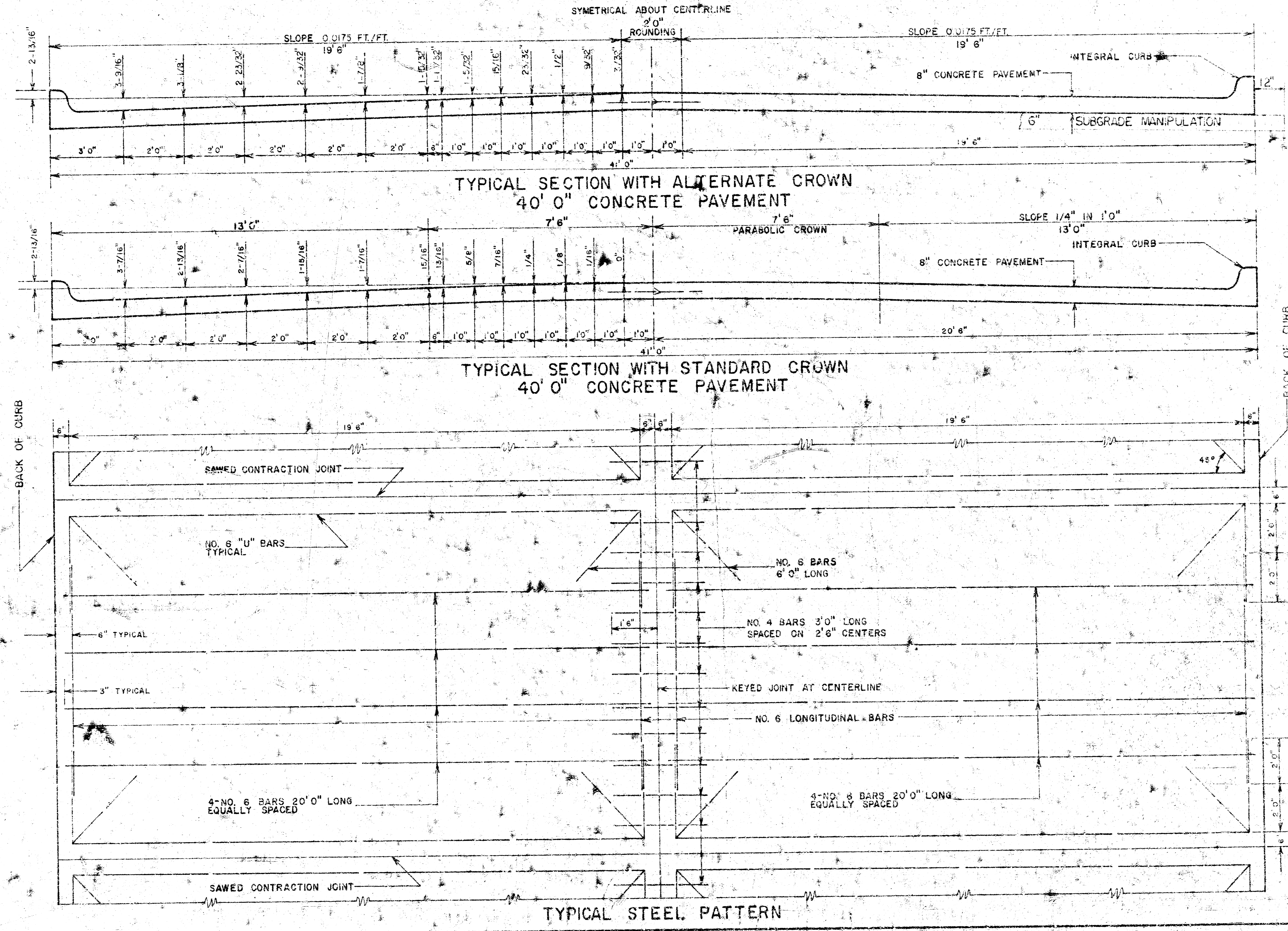
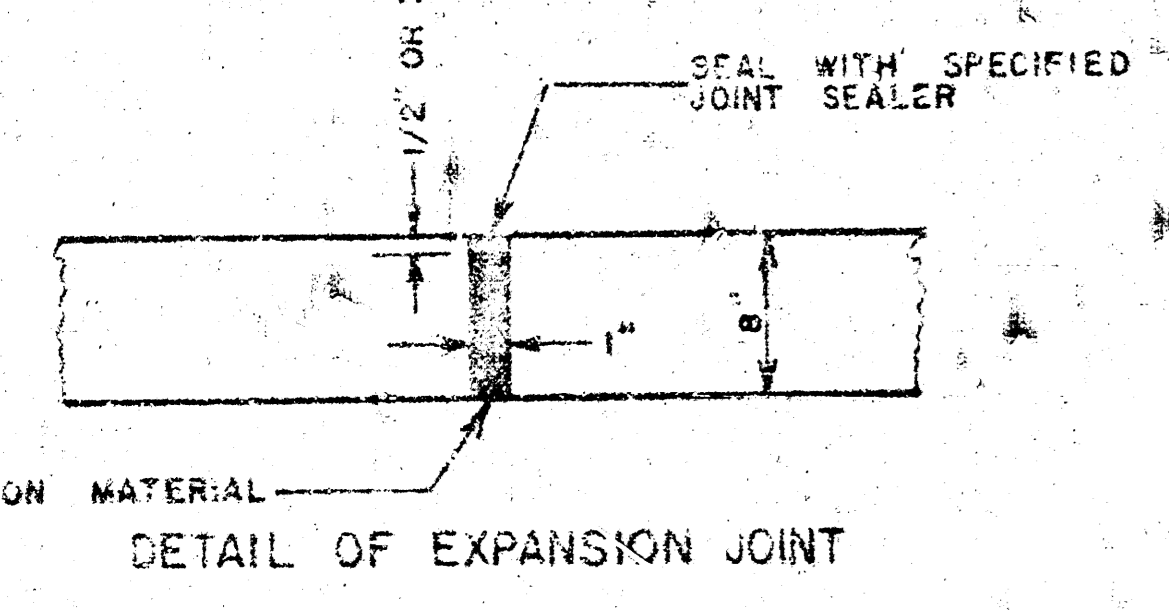
CITY OF WICHITA
APPROVED: _____
Date: 11-13-75
Engineering Div: best

NOTE TO CONTRACTOR
This project will be constructed under the supervision of the CITY ENGINEER and conforming to the SPECIFICATIONS of the CITY OF WICHITA. The CONTRACTOR will reimburse the City of Wichita for all cost of Engineering, Plans and Inspection.

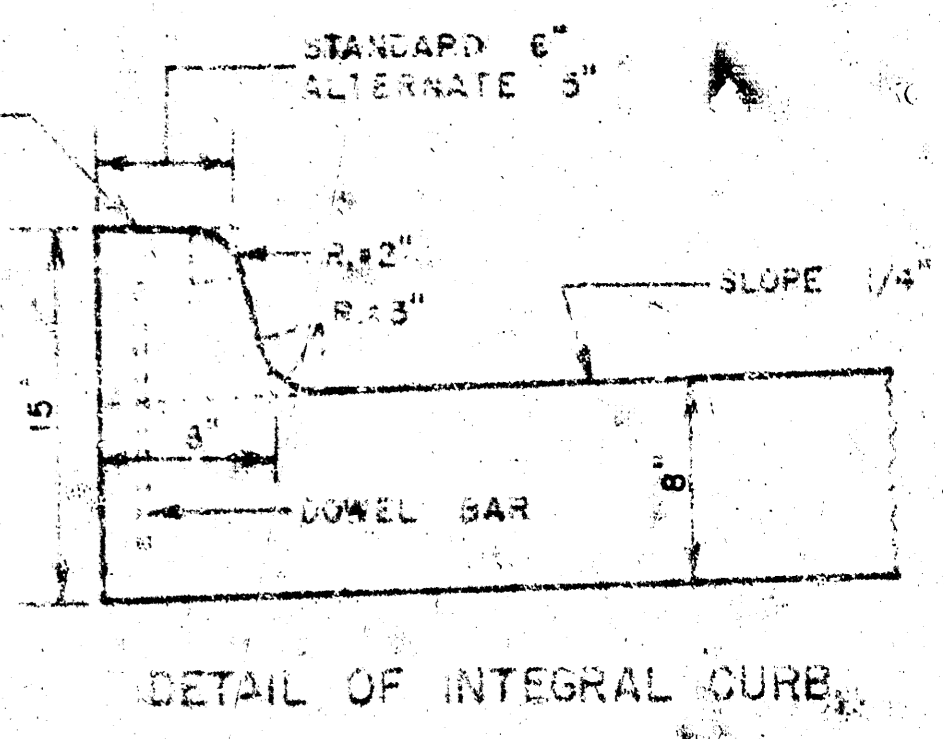
SAWED CONTRACTION JOINTS ARE TO BE CONSTRUCTED AT 30' 0" INTERVALS EXCEPT WHERE AN EXPANSION JOINT IS USED.



EXPANSION JOINTS ARE TO BE CONSTRUCTED AT MAXIMUM INTERVALS OF 120' 0".

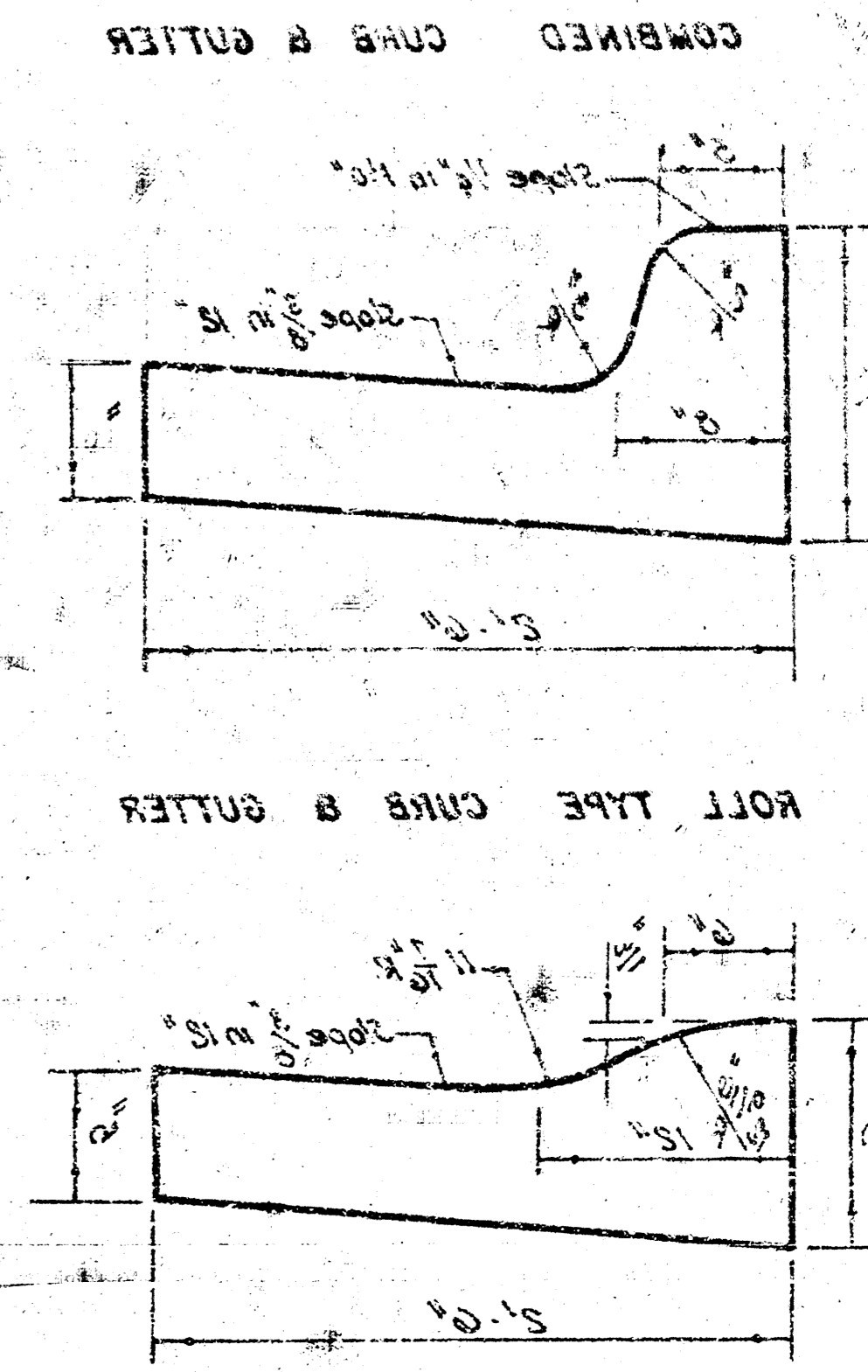


APPROVED AS NOTED
BY CITY ENGINEER
Date: 11-13-75
Engineering Div: best



INTEGRAL CURB SHALL BE CUT THROUGH TO PAVEMENT IN UNIFORM LENGTHS OF NOT MORE THAN TEN FEET INTERVALS BETWEEN EXPANSION JOINTS. EXPANSION JOINTS HAVING THE SAME THICKNESS AS THE EXPANSION JOINTS IN THE PAVEMENT SHALL BE CONSTRUCTED IN THE INTEGRAL CURB AT THE SPECIFIED LOCATIONS. NUMBER 4 OR NUMBER 6 CURB SHALL BE INSTALLED IN THE INTEGRAL CURB AS SHOWN ON APPROXIMATELY 2' 6" CENTERS. PAVEMENT GRADES SHOWN ON PLANS ARE FOR STANDARD CROWN.

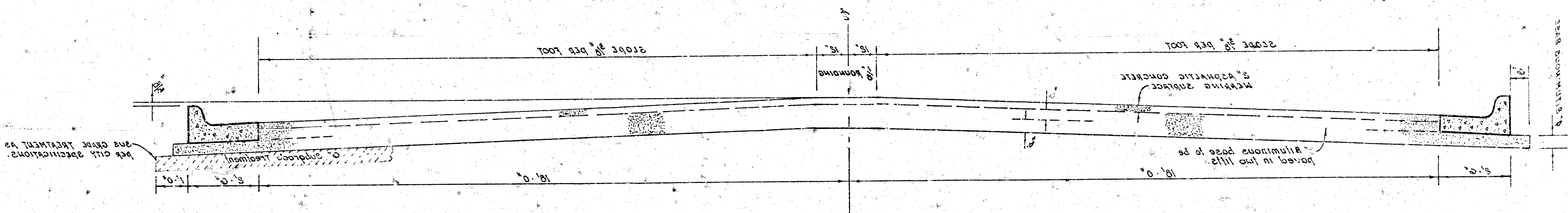




CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.
 BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE
 HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL
 BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE
 PER SQ YD BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT
 A TACK COAT OF EMULSIFIED ASPHALT (SS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE .025 GALLONS

4' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

TYPICAL SECTION



The A.C. pavement between the comp curb & gutter shall
 be paid as 4' A.C. pavement (2" bituminous base).
 The bituminous base under the comp curb & gutter shall
 be paid as 2' A.C. pavement (2" bituminous base).
 subgrade treatment as
 per city specifications.

CITY OF WICHITA, KANSAS
 DEPARTMENT of PUBLIC WORKS - Engineering
 Division
 R. W. LIND
 CITY ENGINEER
 DATE _____
 Proj. No. _____

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WICHITA, KANSAS

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11-13-75 *2021*

