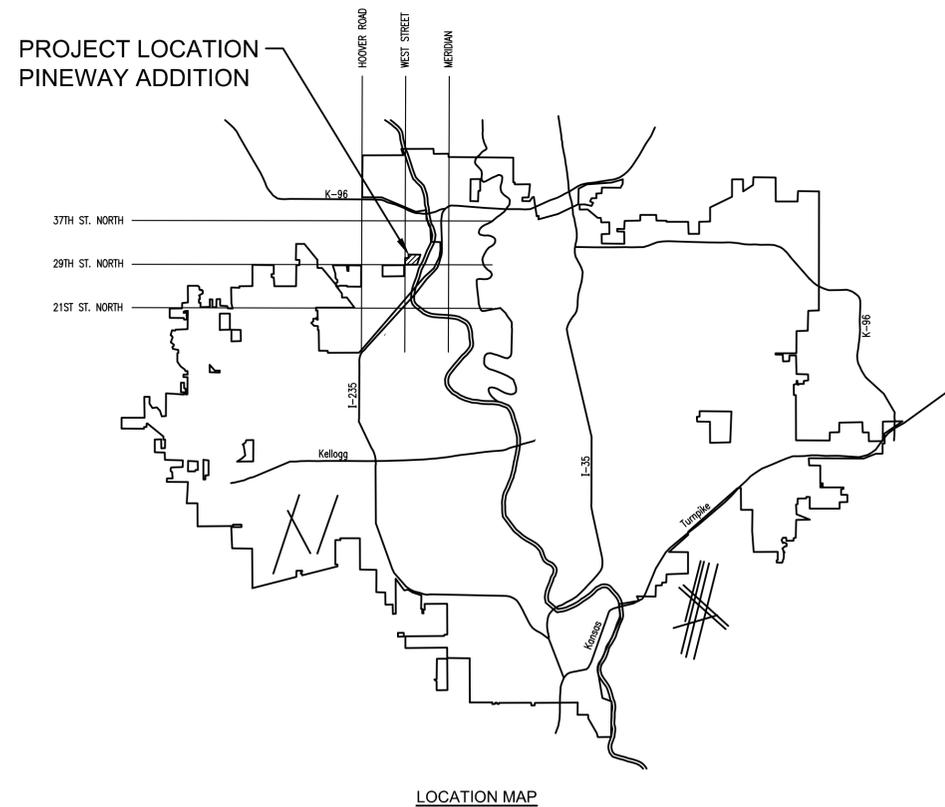


CONSTRUCTION PLANS FOR WEST STREET LANE TO SERVE PINEWAY ADDITION

CITY OF WICHITA ENGINEERING PROJECT NO. 472-2024-085960
ORG CODE: 47480825, MUNIS NO.: E5169

CITY OF WICHITA, KS
PAUL GUNZELMAN, P.E. - CITY ENGINEER



LOCATION MAP

DEVELOPER CONTACT
CRAIG SHARP
BLUE SKY VENTURES, LLC
4006 N. HOOVER COURT
WICHITA, KS 67205

SHEET INDEX

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- 2 KEY MAP & CONTROL POINTS
- 3 GENERAL NOTES
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- 8 PAVING DETAILS
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- 10 EROSION CONTROL PLANS
- 11-15 EROSION CONTROL DETAILS
- 16 MARKING & SIGNING PLANS
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- 18 CONSTRUCTION SEQUENCE PLAN
- 19-20 TRAFFIC DETAILS
- 21-22 CROSS SECTIONS

PAVING IMPROVEMENTS

**WEST STREET LANE TO SERVE
PINEWAY ADDITION**

PAUL GUNZELMAN CITY ENGINEER
CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:		

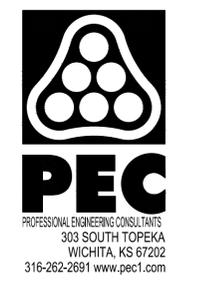
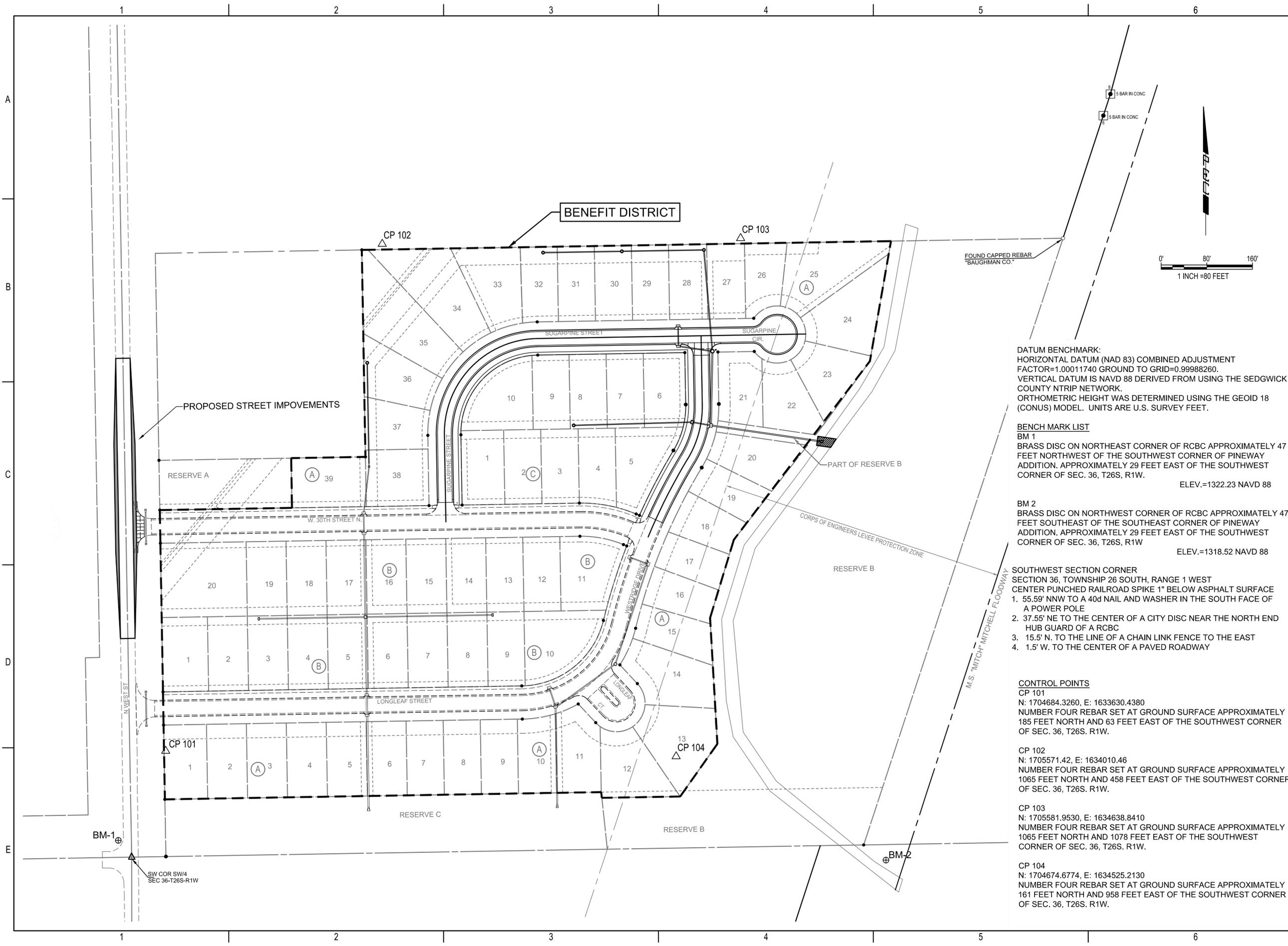
JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

TITLE SHEET

DECEMBER 2025

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 NOTES.DWG

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 VERTICAL CONTROLS & KEY MAP.DWG



DATUM BENCHMARK:
 HORIZONTAL DATUM (NAD 83) COMBINED ADJUSTMENT
 FACTOR=1.00011740 GROUND TO GRID=0.99988260.
 VERTICAL DATUM IS NAVD 88 DERIVED FROM USING THE SEDGWICK COUNTY NTRIP NETWORK.
 ORTHOMETRIC HEIGHT WAS DETERMINED USING THE GEOID 18 (CONUS) MODEL. UNITS ARE U.S. SURVEY FEET.

BENCH MARK LIST
 BM 1
 BRASS DISC ON NORTHEAST CORNER OF RCBC APPROXIMATELY 47 FEET NORTHWEST OF THE SOUTHWEST CORNER OF PINEWAY ADDITION. APPROXIMATELY 29 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W. ELEV.=1322.23 NAVD 88
 BM 2
 BRASS DISC ON NORTHWEST CORNER OF RCBC APPROXIMATELY 47 FEET SOUTHEAST OF THE SOUTHWEST CORNER OF PINEWAY ADDITION. APPROXIMATELY 29 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W ELEV.=1318.52 NAVD 88

SOUTHWEST SECTION CORNER
 SECTION 36, TOWNSHIP 26 SOUTH, RANGE 1 WEST
 CENTER PUNCHED RAILROAD SPIKE 1" BELOW ASPHALT SURFACE
 1. 55.59' NNW TO A 40d NAIL AND WASHER IN THE SOUTH FACE OF A POWER POLE
 2. 37.55' NE TO THE CENTER OF A CITY DISC NEAR THE NORTH END HUB GUARD OF A RCBC
 3. 15.5' N. TO THE LINE OF A CHAIN LINK FENCE TO THE EAST
 4. 1.5' W. TO THE CENTER OF A PAVED ROADWAY

CONTROL POINTS
 CP 101
 N: 1704684.3260, E: 1633630.4380
 NUMBER FOUR REBAR SET AT GROUND SURFACE APPROXIMATELY 185 FEET NORTH AND 63 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W.

CP 102
 N: 1705571.42, E: 1634010.46
 NUMBER FOUR REBAR SET AT GROUND SURFACE APPROXIMATELY 1065 FEET NORTH AND 458 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W.

CP 103
 N: 1705581.9530, E: 1634638.8410
 NUMBER FOUR REBAR SET AT GROUND SURFACE APPROXIMATELY 1065 FEET NORTH AND 1078 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W.

CP 104
 N: 1704674.6774, E: 1634525.2130
 NUMBER FOUR REBAR SET AT GROUND SURFACE APPROXIMATELY 161 FEET NORTH AND 958 FEET EAST OF THE SOUTHWEST CORNER OF SEC. 36, T26S, R1W.



PAVING IMPROVEMENTS

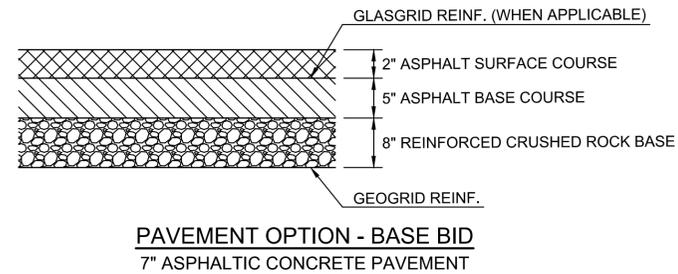
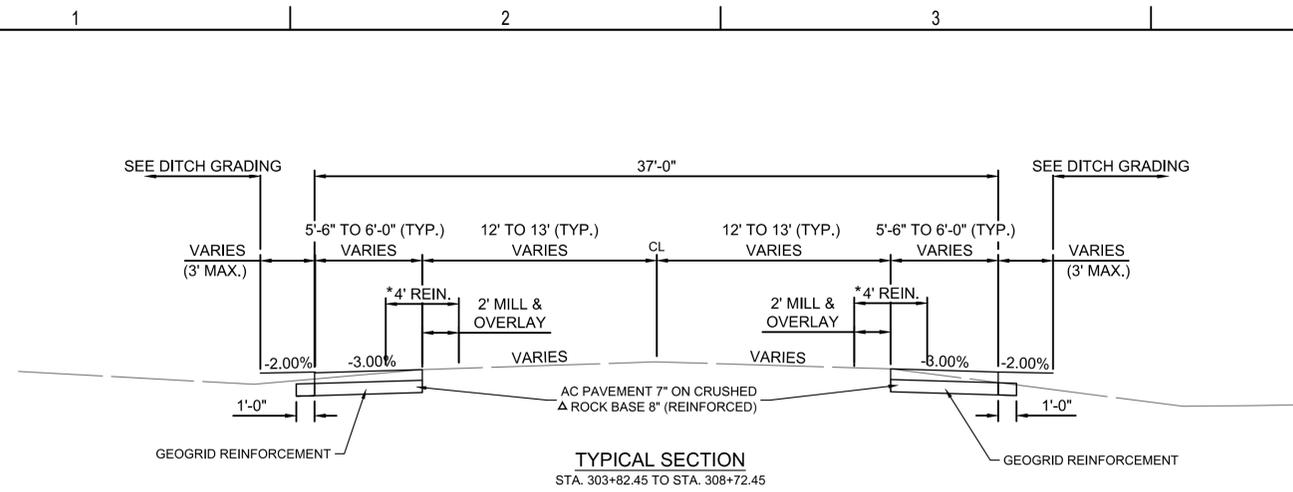
WEST STREET LANE TO SERVE
 PINEWAY ADDITION

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:			

JOB NO.	221170-015
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DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

HORIZONTAL & VERTICAL CONTROLS & KEY MAP



GENERAL NOTES:

GEOGRID REINFORCEMENT AND AGGREGATE BASE TO COMPLY WITH SEC. 404 OF THE CITY OF WICHITA STANDARD SPECIFICATIONS FOR THE CONSTRUCTION OF PUBLIC PROJECTS. GEOGRID REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CRUSHED ROCK SHALL BE UNIFORMLY GRADED FROM 1 - 1/2\"/>

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED TO AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFTS OF ASPHALTIC MATERIAL.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF OF PAVEMENT.

EARTHWORK SUMMARY (C.Y.)		
	Excavation	Compacted Fill (95%)
WEST STREET	763 CY	126 CY
GRAND TOTAL	763 CY	126 CY

* 4' GLASGRID REINFORCEMENT (GLASGRID 8502 OR APPROVED EQUIVALENT)

Δ ARTERIAL STREETS BM-2 PG 64-22 (BASE) PG 70-28 (SURFACE)

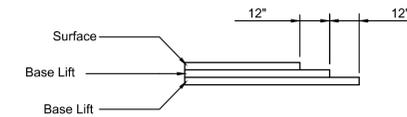
BASE COURSE THICKER THAN 4" SHALL BE INSTALLED IN TWO LIFTS

NO SHRINKAGE FACTOR HAS BEEN APPLIED.

CONTRACTOR IS RESPONSIBLE FOR HAULING OFF ANY EXCESS MATERIAL FROM THE SITE.

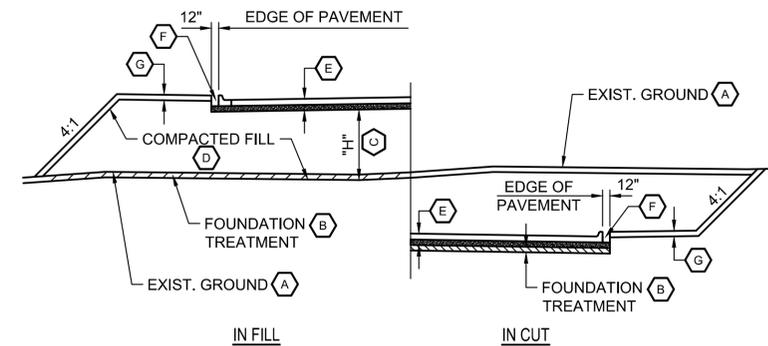
ANY DOUBLE-HANDLING OF EXCAVATED MATERIAL WHICH MAY BE REQUIRED WILL NOT BE MEASURED OR PAID FOR SEPARATELY, BUT SHALL BE SUBSIDIARY TO THE CONTRACT UNIT PRICE BID FOR EXCAVATION.

EARTHWORK QUANTITIES ARE BASED ON ASPHALT PAVEMENT SECTION.



TRANSVERSE CONSTRUCTION JOINTS

Transverse construction joints shall be constructed in flexible base pavement at locations where pavement joins existing flexible base pavement as shown by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards of pavement.



FOUNDATION TREATMENT & COMPACTION DIAGRAM

1" = 1'

- (A) STRIP TOPSOIL.
- (B) SCARIFY AND COMPACT (6 INCHES) TO 95% ASTM D698.
- (C) "H" EQUALS FULL EMBANKMENT HEIGHT.
- (D) COMPACT TO 95% ASTM D698.
- (E) PAVEMENT SECTION, SEE TYPICAL SECTIONS THIS SHEET.
- (F) BACKFILL WITH SATISFACTORY SOIL. COMPACT TO 95% ASTM D698.
- (G) PLACE TOPSOIL.

FOUNDATION TREATMENT & COMPACTION SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM "EXCAVATION".



PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

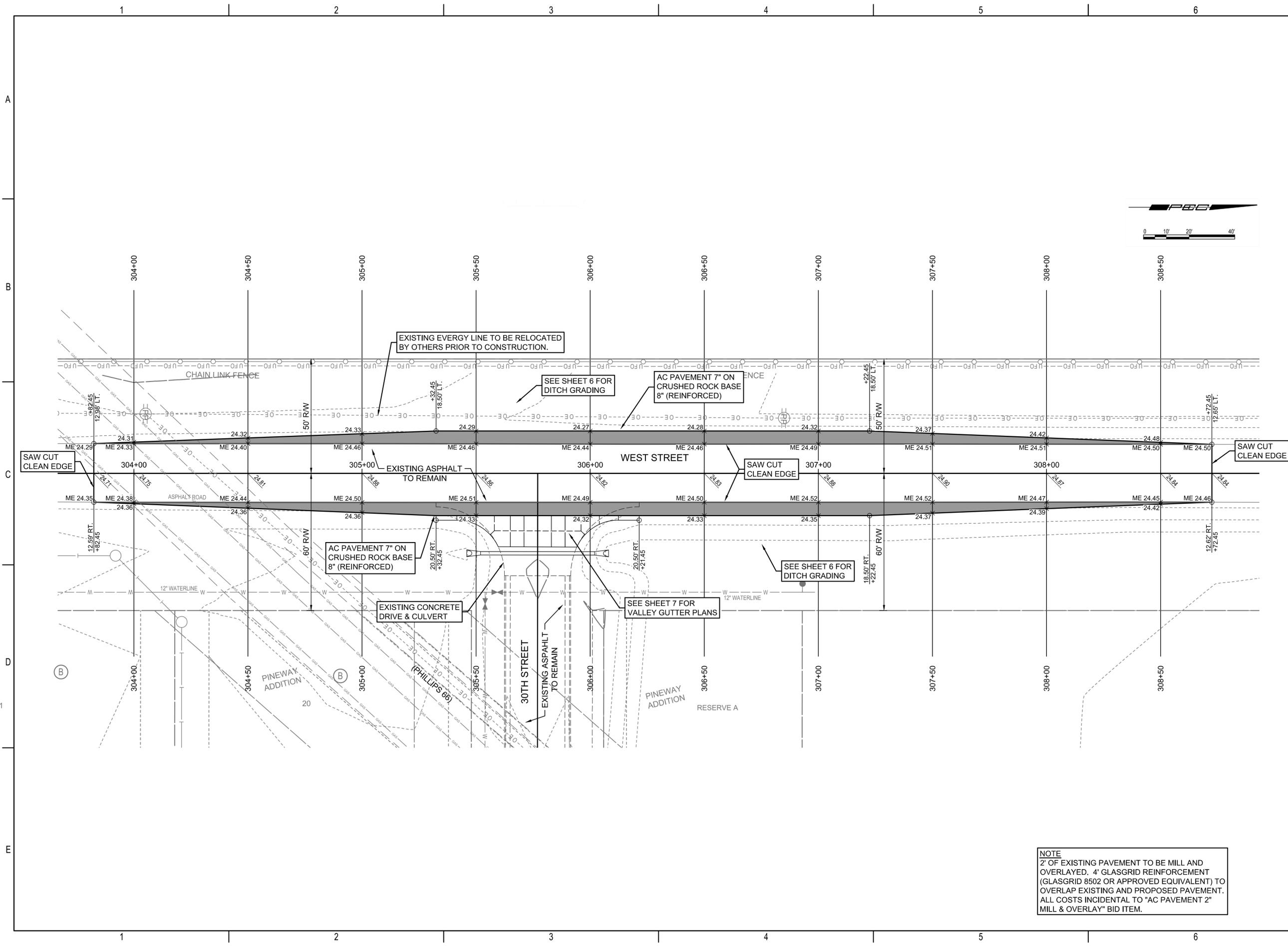
Issue:		

JOB NO.	221170-015
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PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

TYPICAL SECTIONS

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PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
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 PAUL GUNZELMAN CITY ENGINEER
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Issue:	

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CHECKED BY	KPG

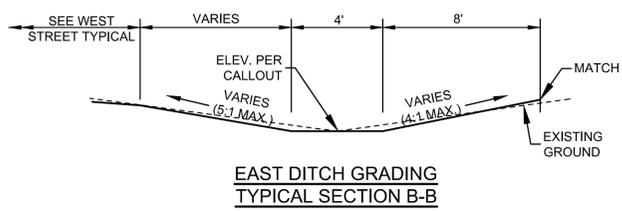
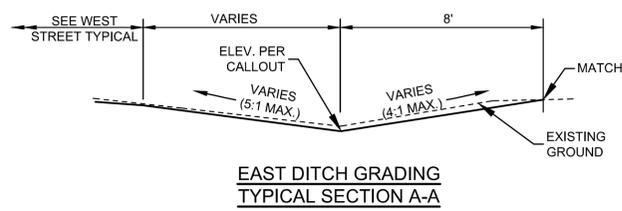
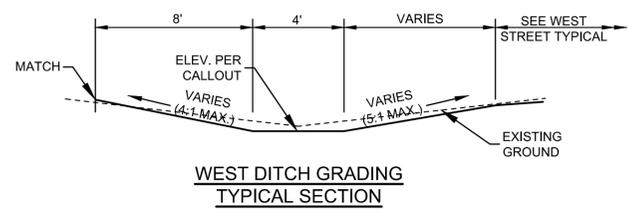
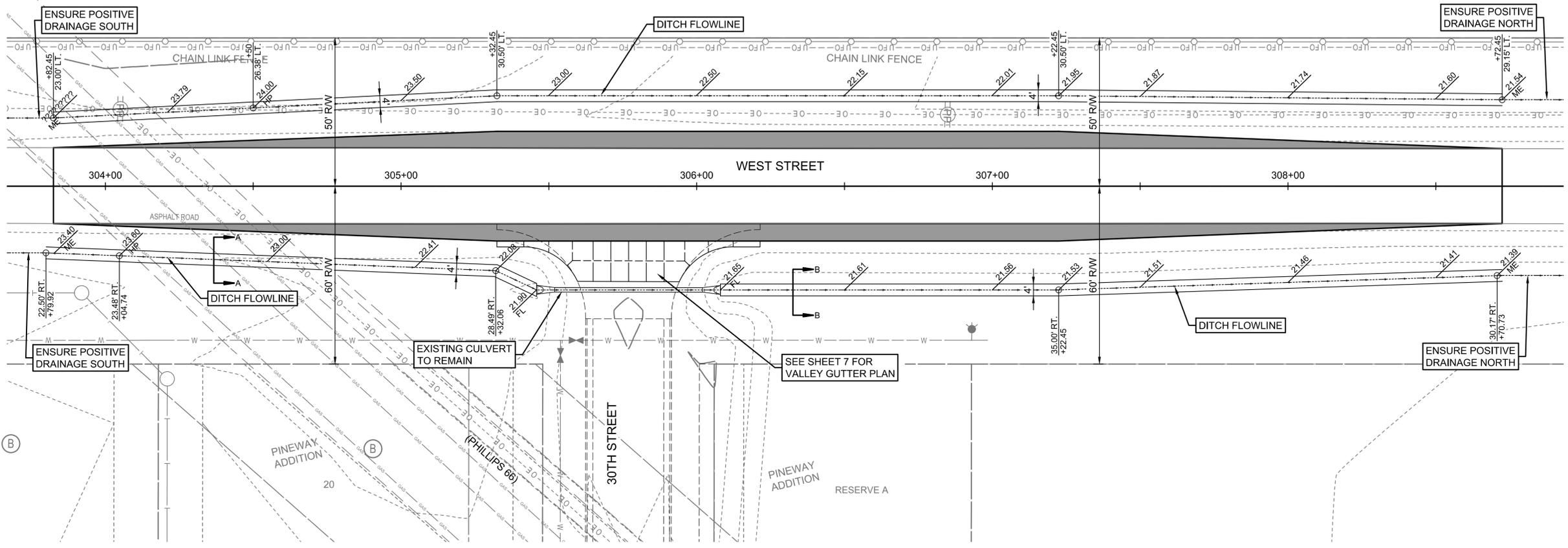
WEST STREET PLAN

NOTE
 2' OF EXISTING PAVEMENT TO BE MILL AND OVERLAYED. 4' GLASGRID REINFORCEMENT (GLASGRID 8502 OR APPROVED EQUIVALENT) TO OVERLAP EXISTING AND PROPOSED PAVEMENT. ALL COSTS INCIDENTAL TO "AC PAVEMENT 2" MILL & OVERLAY" BID ITEM.

1 2 3 4 5 6



NOTE
 PRIOR TO GRADING, CONTRACTOR TO VERIFY EXISTING UTILITY PIPE ELEVATIONS THROUGH WEST STREET RIGHT-OF-WAY. CONTRACTOR TO COORDINATE WITH RESPECTIVE UTILITY COMPANY AND REPORT FINDINGS TO THE ENGINEER. COST INCIDENTAL TO THE PROJECT.



PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:	

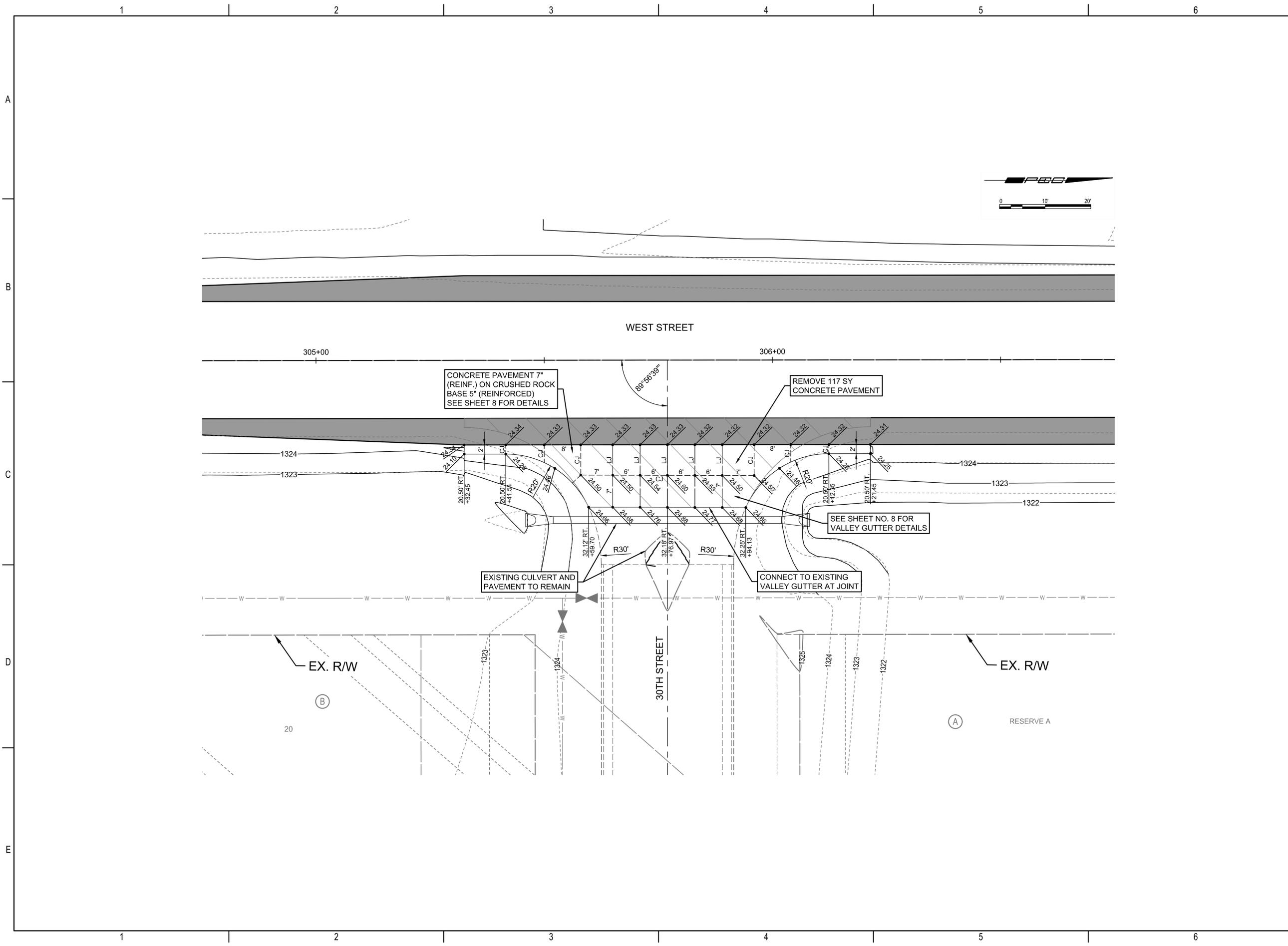
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DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

DITCH GRADING PLAN

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1 2 3 4 5 6

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PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
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 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

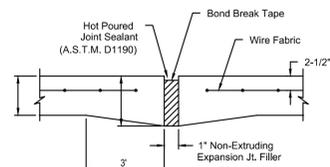
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JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

VALLEY GUTTER PLAN

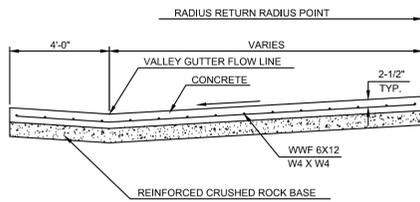
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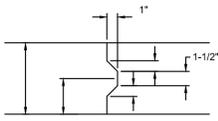


EXPANSION JOINT

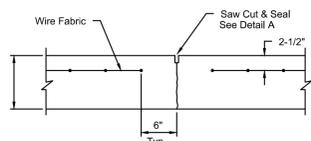
NOTE: Extra Thickness to be Subsidiary to Price of Square Yards Pavement



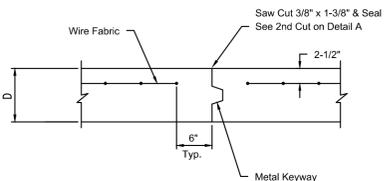
SECTION A-A



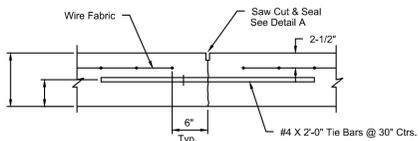
KEYWAY DETAIL



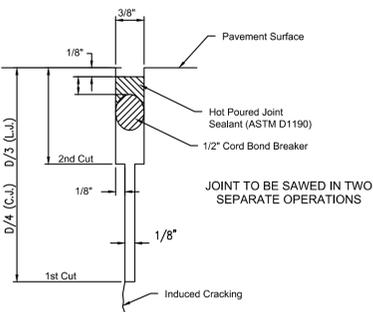
CONTRACTION JOINT DETAIL (C.J.)



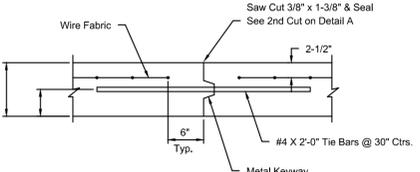
OPTIONAL CONTRACTION JOINT



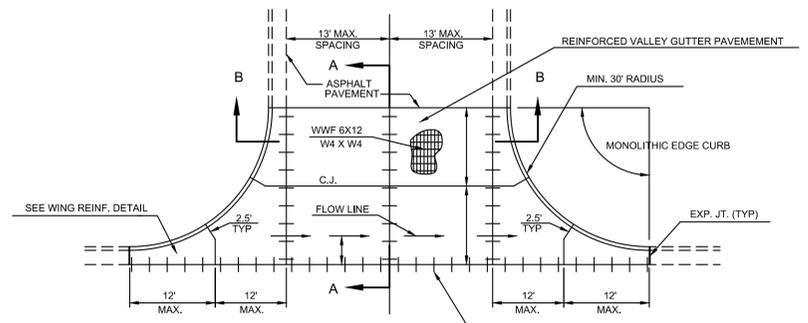
LONGITUDINAL JOINT DETAIL (L.J.)



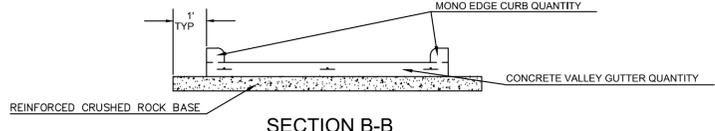
SAW JOINT DETAIL (DETAIL A)



OPTIONAL LONGITUDINAL JOINT DETAIL (L.J.)

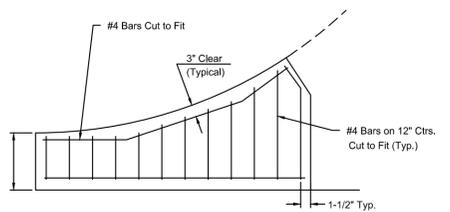


PLAN



SECTION B-B

REINFORCED VALLEY GUTTER DETAIL



WING REINFORCING DETAIL

REVISION MAY 2017 SECTION B-B, ROCK EXTENDED ONE FOOT BEYOND PAVEMENT	
CITY OF WICHITA	
VALLEY GUTTER DETAILS	
CITY ENGINEER GARY JANZEN, P.E.	
PROJECT NUMBER	OCA NUMBER
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501	
SHEET 12 of 40	

PV-109

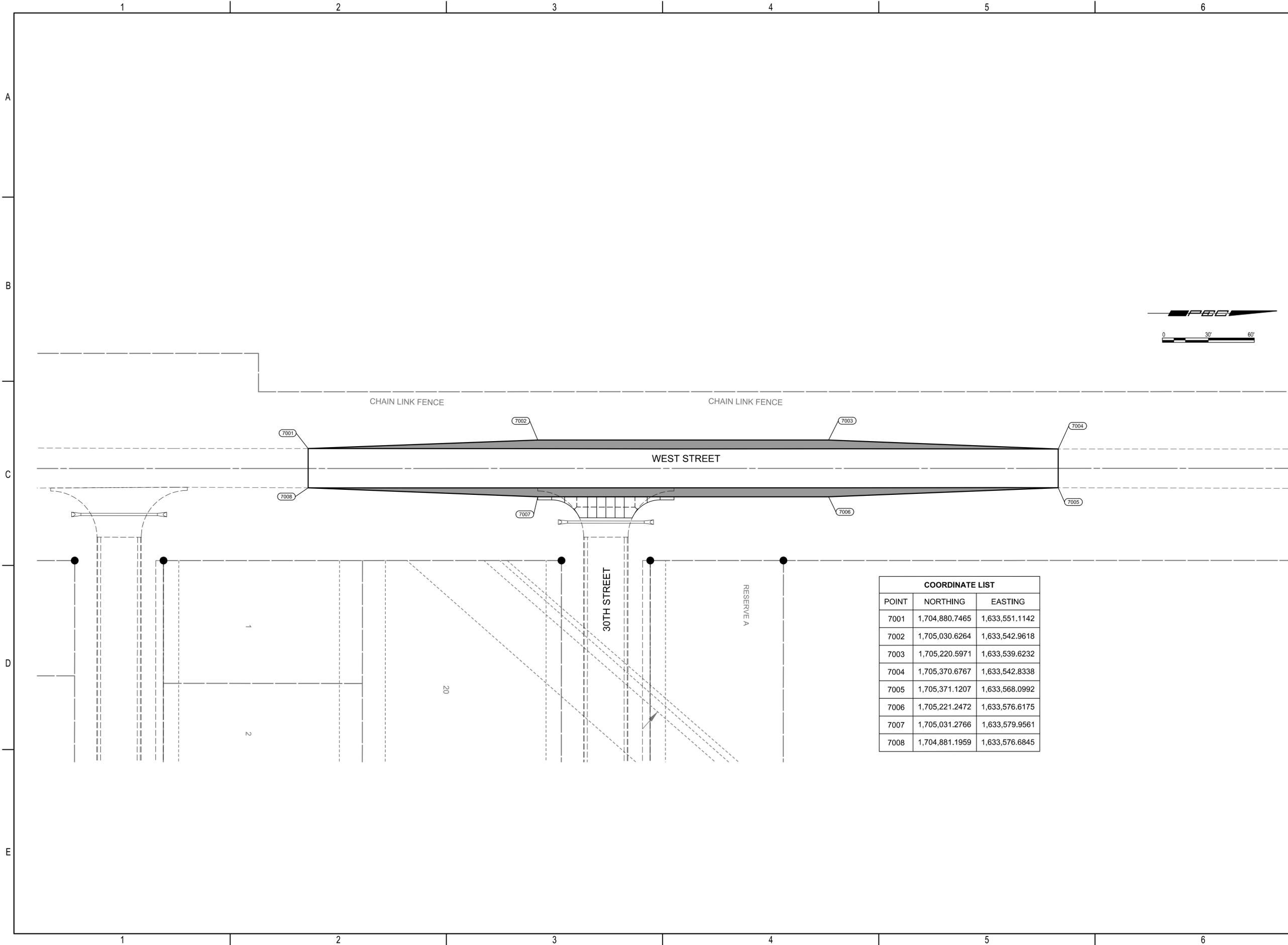


PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

ISSUE:	
JOB NO.	221170-015
DATE	DECEMBER 2025
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DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

VALLEY GUTTER DETAILS

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COORDINATE LIST		
POINT	NORTHING	EASTING
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7002	1,705,030.6264	1,633,542.9618
7003	1,705,220.5971	1,633,539.6232
7004	1,705,370.6767	1,633,542.8338
7005	1,705,371.1207	1,633,568.0992
7006	1,705,221.2472	1,633,576.6175
7007	1,705,031.2766	1,633,579.9561
7008	1,704,881.1959	1,633,576.6845



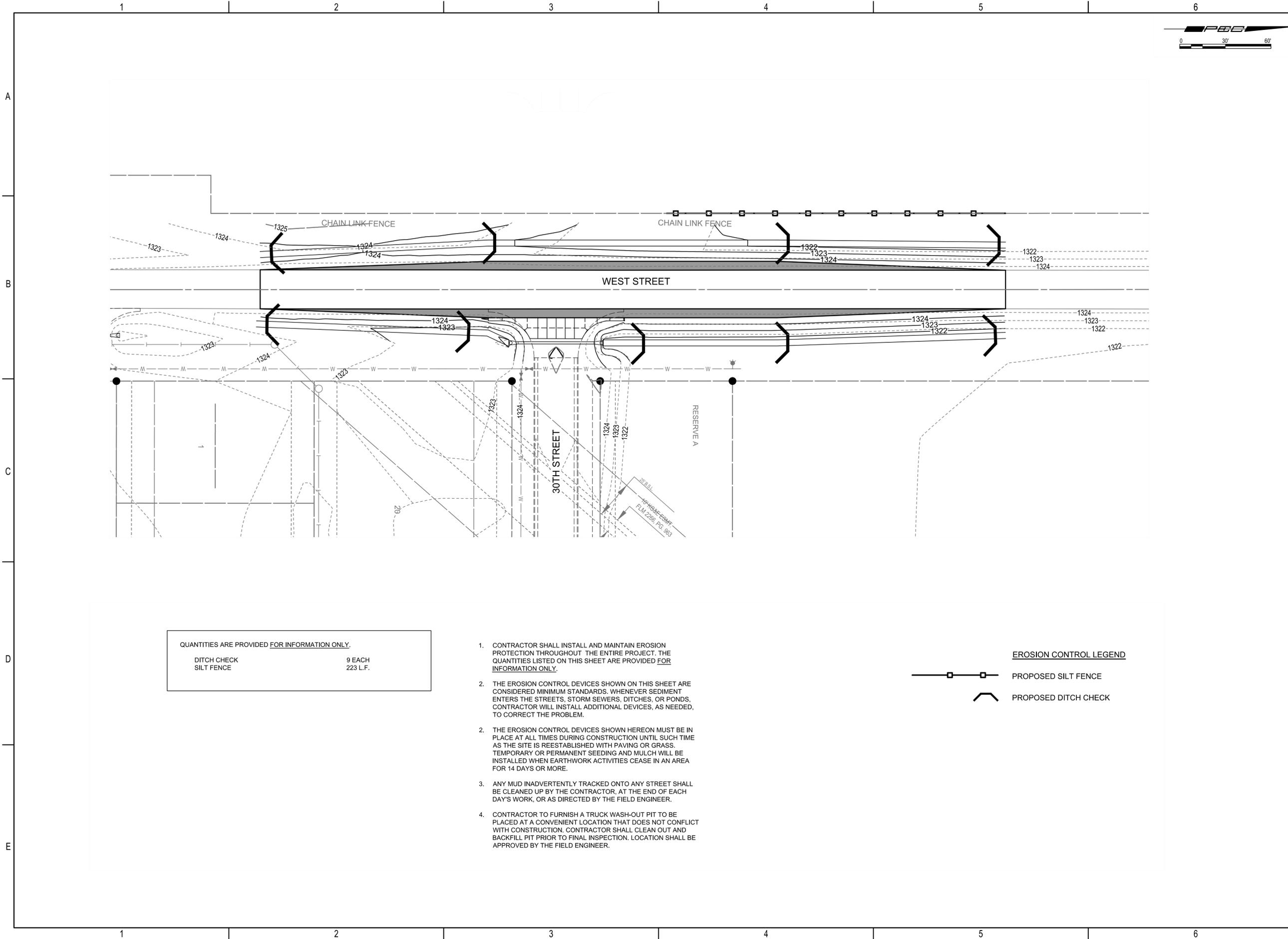
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COORDINATE GEOMETRY PLAN

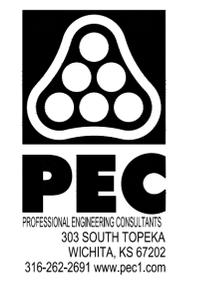
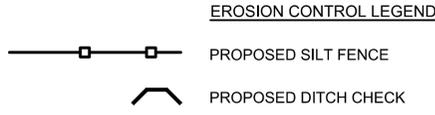
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 PLAN.DWG



QUANTITIES ARE PROVIDED FOR INFORMATION ONLY.

DITCH CHECK	9 EACH
SILT FENCE	223 L.F.

1. CONTRACTOR SHALL INSTALL AND MAINTAIN EROSION PROTECTION THROUGHOUT THE ENTIRE PROJECT. THE QUANTITIES LISTED ON THIS SHEET ARE PROVIDED FOR INFORMATION ONLY.
2. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED MINIMUM STANDARDS. WHENEVER SEDIMENT ENTERS THE STREETS, STORM SEWERS, DITCHES, OR PONDS, CONTRACTOR WILL INSTALL ADDITIONAL DEVICES, AS NEEDED, TO CORRECT THE PROBLEM.
2. THE EROSION CONTROL DEVICES SHOWN HEREON MUST BE IN PLACE AT ALL TIMES DURING CONSTRUCTION UNTIL SUCH TIME AS THE SITE IS REESTABLISHED WITH PAVING OR GRASS. TEMPORARY OR PERMANENT SEEDING AND MULCH WILL BE INSTALLED WHEN EARTHWORK ACTIVITIES CEASE IN AN AREA FOR 14 DAYS OR MORE.
3. ANY MUD INADVERTENTLY TRACKED ONTO ANY STREET SHALL BE CLEANED UP BY THE CONTRACTOR, AT THE END OF EACH DAY'S WORK, OR AS DIRECTED BY THE FIELD ENGINEER.
4. CONTRACTOR TO FURNISH A TRUCK WASH-OUT PIT TO BE PLACED AT A CONVENIENT LOCATION THAT DOES NOT CONFLICT WITH CONSTRUCTION. CONTRACTOR SHALL CLEAN OUT AND BACKFILL PIT PRIOR TO FINAL INSPECTION. LOCATION SHALL BE APPROVED BY THE FIELD ENGINEER.

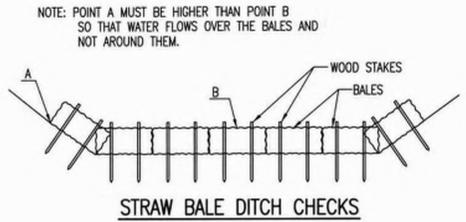


PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

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EROSION CONTROL PLAN



STRAW BALE DITCH CHECKS

MATERIAL SPECIFICATION:

BALE DITCH CHECKS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. OPTIONAL: THE DOWNSTREAM SCOUR APRON SHOULD BE CONSTRUCTED OF A DOUBLE-NETTED STRAW EROSION-CONTROL BLANKET AT LEAST 6' WIDE. OPTIONAL: THE METAL LANDSCAPE STAPLES USED TO ANCHOR THE EROSION-CONTROL BLANKET SHOULD BE AT LEAST 8" LONG.

PLACEMENT:

BALE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE DITCH CHECK SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. STRAW BALE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. BALES SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED. THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK SPACING	DITCH GRADE (%)	CHECK SPACING (FEET)
0.5	200	
1.0	200	
2.0	100	
3.0	65	
4.0	50	
5.0	40	
6.0	30	

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH-IT WILL BE USED LATER. OPTIONAL: ON THE DOWNSTREAM SIDE OF THE TRENCH, ROLL OUT A LENGTH OF EROSION-CONTROL BLANKET (SCOUR APRON) EQUAL TO THE LENGTH OF THE TRENCH. PLACE THE UPSTREAM EDGE OF THE EROSION-CONTROL BLANKET ALONG THE BOTTOM UPSTREAM EDGE OF THE TRENCH. THE EROSION CONTROL BLANKET SHOULD BE ANCHORED IN THE TRENCH WITH ONE ROW OF 8" LANDSCAPE STAPLES PLACED ON 18" CENTERS. THE REMAINDER OF THE EROSION-CONTROL BLANKET (THE PORTION THAT IS NOT LYING IN THE TRENCH) WILL SERVE AS THE DOWNSTREAM SCOUR APRON. THIS SECTION OF THE BLANKET SHOULD BE ANCHORED TO THE GROUND WITH 8" LANDSCAPE STAPLES PLACED AROUND THE PERIMETER OF THE BLANKET ON 18" CENTERS. THE REMAINDER OF THE BLANKET SHOULD BE ANCHORED USING TWO EVENLY SPACED ROWS OF 8" LANDSCAPE STAPLES ON 18" CENTERS PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSTREAM SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP AND EXTEND UPSTREAM NO MORE THAN 24".

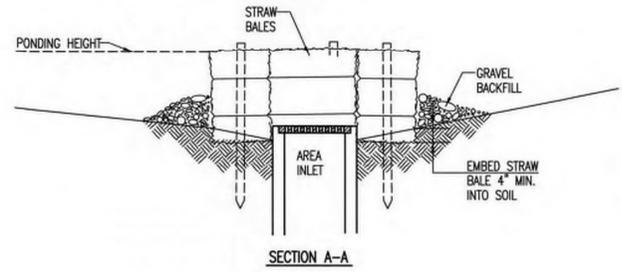
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

- DO NOT PLACE A BALE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW.
- DO NOT PLACE BALE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW.
- FOLLOW PRESCRIBED DITCH-CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS.
- DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE.
- DO NOT PLACE BALE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.
- BALE DITCH CHECKS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE CHECK.

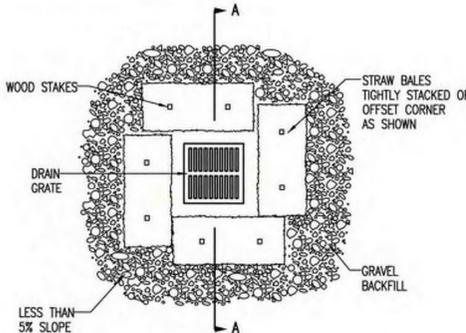
INSPECTION AND MAINTENANCE:

BALE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW AROUND THE DITCH CHECK?
 DOES WATER FLOW UNDER THE DITCH CHECK?
 DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
 ARE ANY BALES AND/OR SCOUR APRONS (OPTIONAL) DISLODGED?
 ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
 DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



SECTION A-A



STRAW BALE BARRIERS FOR AREA INLETS (INLET PROTECTION)

MATERIAL SPECIFICATION:

BALE AREA INLET BARRIERS SHOULD BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

BALE AREA INLET BARRIERS SHOULD BE PLACED DIRECTLY AROUND THE PERIMETER OF A DROP INLET. WHEN A BALE AREA INLET BARRIER IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 4" DEEP BY A BALE'S WIDTH WIDE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. SOME BALES MAY NEED TO BE SHORTENED TO FIT INTO THE TRENCH AROUND THE AREA INLET. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE RECEIVING SIDE OF THE BARRIER AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP. NOTE: WHEN A BALE AREA INLET BARRIER IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

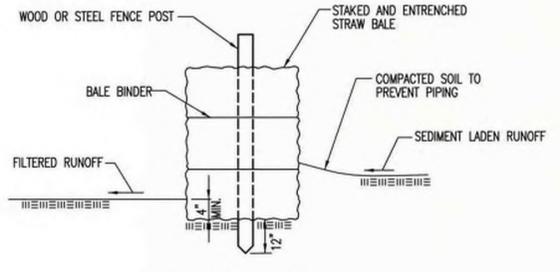
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

- BALES SHOULD BE PLACED DIRECTLY AGAINST THE PERIMETER OF THE AREA INLET. THIS AVOIDS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR.
- BALE AREA INLET BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE AREA INLET BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

DOES WATER FLOW UNDER THE AREA INLET BARRIER?
 DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
 ARE ANY BALES DISLODGED?
 ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
 DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



STRAW BALE BARRIERS

MATERIAL SPECIFICATION:

BALE SLOPE BARRIERS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, BALE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. BALE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSLOPE SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

- WHEN PRACTICAL, DO NOT PLACE BALE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. CONCENTRATED FLOW OVER A SLOPE BARRIER CREATES A SCOUR HOLE ON THE DOWNSLOPE SIDE OF THE BARRIER. THE SCOUR HOLE EVENTUALLY UNDERMINES THE BALES AND THE BARRIER FAILS.
- DO NOT PLACE BALE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.
- BALE SLOPE BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
 DOES WATER FLOW UNDER THE SLOPE BARRIER?
 DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
 ARE ANY BALES DISLODGED?
 ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
 DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2013



STRAW BALE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER: _____ OCA NUMBER: _____ DATE: _____

CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 455 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

SHEET _____

PAVING IMPROVEMENTS

WEST STREET LANE TO SERVE
 PINEWAY ADDITION

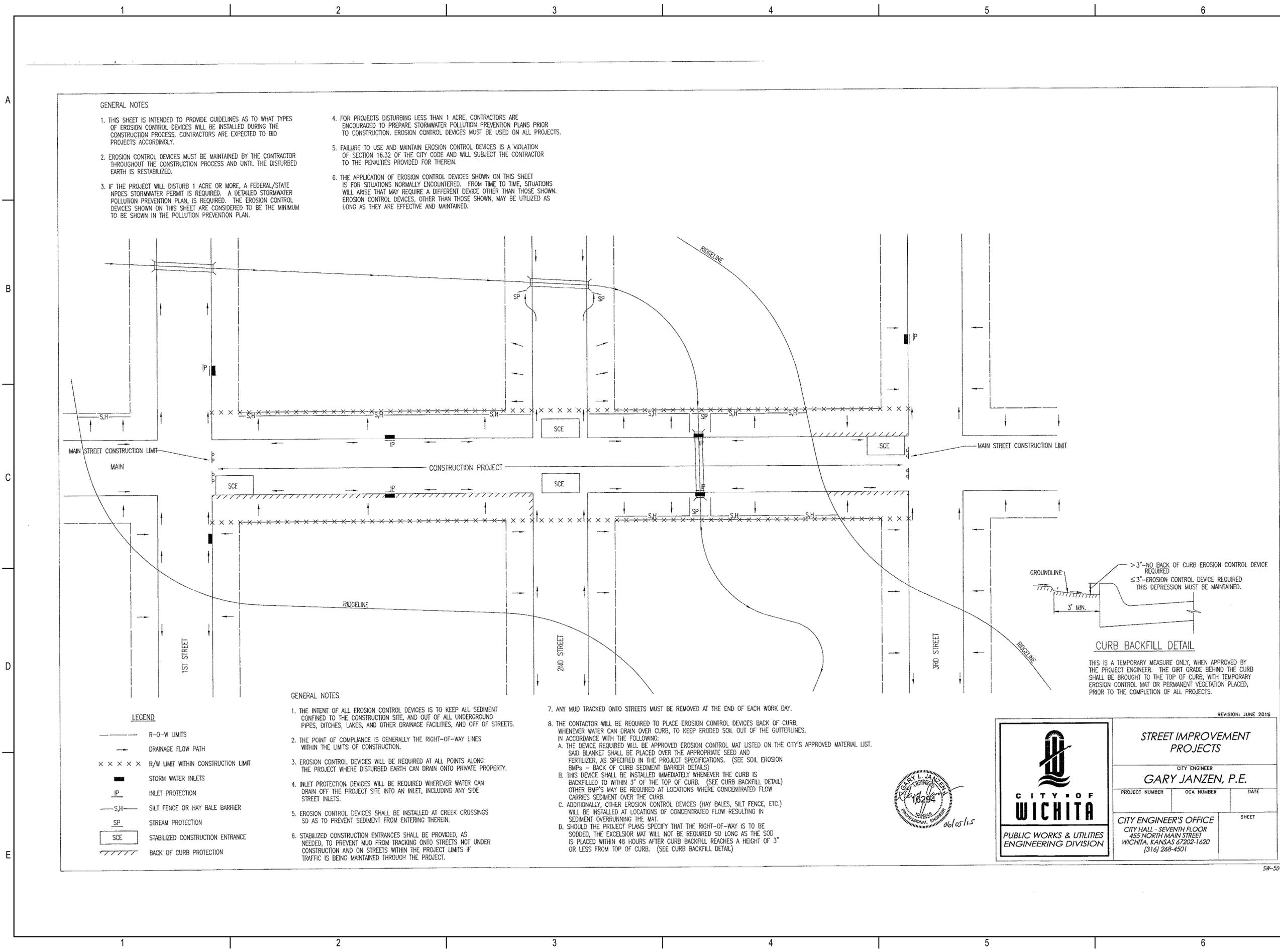
PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:	
JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

EROSION CONTROL DETAILS

SAVED 8/7/2025 9:51:46 AM BY BILL SEXSON
 PLOTTED 12/22/2025 9:17:14 AM BY HUNTER WILES
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 DETAILS.DWG

SAVED 8/7/2025 9:51:46 AM BY BILL SEXSON
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 U:\WICHITA-CIVIL\2022\22117010\12\22\PD4_PLANS\030\DRAWINGS\AUXILIARY_LANE\221170-012-EROSION CONTROL DETAILS.DWG



GENERAL NOTES

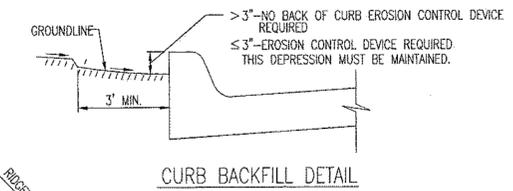
- THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPES OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
- EROSION CONTROL DEVICES MUST BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PROCESS AND UNTIL THE DISTURBED EARTH IS RESTABILIZED.
- IF THE PROJECT WILL DISTURB 1 ACRE OR MORE, A FEDERAL/STATE NPDES STORMWATER PERMIT IS REQUIRED. A DETAILED STORMWATER POLLUTION PREVENTION PLAN, IS REQUIRED. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED TO BE THE MINIMUM TO BE SHOWN IN THE POLLUTION PREVENTION PLAN.
- FOR PROJECTS DISTURBING LESS THAN 1 ACRE, CONTRACTORS ARE ENCOURAGED TO PREPARE STORMWATER POLLUTION PREVENTION PLANS PRIOR TO CONSTRUCTION. EROSION CONTROL DEVICES MUST BE USED ON ALL PROJECTS.
- FAILURE TO USE AND MAINTAIN EROSION CONTROL DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE CONTRACTOR TO THE PENALTIES PROVIDED FOR THEREIN.
- THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT DEVICE OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED AS LONG AS THEY ARE EFFECTIVE AND MAINTAINED.

GENERAL NOTES

- THE INTENT OF ALL EROSION CONTROL DEVICES IS TO KEEP ALL SEDIMENT CONFINED TO THE CONSTRUCTION SITE, AND OUT OF ALL UNDERGROUND PIPES, DITCHES, LAKES, AND OTHER DRAINAGE FACILITIES, AND OFF OF STREETS.
- THE POINT OF COMPLIANCE IS GENERALLY THE RIGHT-OF-WAY LINES WITHIN THE LIMITS OF CONSTRUCTION.
- EROSION CONTROL DEVICES WILL BE REQUIRED AT ALL POINTS ALONG THE PROJECT WHERE DISTURBED EARTH CAN DRAIN ONTO PRIVATE PROPERTY.
- INLET PROTECTION DEVICES WILL BE REQUIRED WHEREVER WATER CAN DRAIN OFF THE PROJECT SITE INTO AN INLET, INCLUDING ANY SIDE STREET INLETS.
- EROSION CONTROL DEVICES SHALL BE INSTALLED AT CREEK CROSSINGS SO AS TO PREVENT SEDIMENT FROM ENTERING THEREIN.
- STABILIZED CONSTRUCTION ENTRANCES SHALL BE PROVIDED, AS NEEDED, TO PREVENT MUD FROM TRACKING ONTO STREETS NOT UNDER CONSTRUCTION AND ON STREETS WITHIN THE PROJECT LIMITS IF TRAFFIC IS BEING MAINTAINED THROUGH THE PROJECT.
- ANY MUD TRACKED ONTO STREETS MUST BE REMOVED AT THE END OF EACH WORK DAY.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE EROSION CONTROL DEVICES BACK OF CURB, WHENEVER WATER CAN DRAIN OVER CURB, TO KEEP ERODED SOIL OUT OF THE GUTTERLINES, IN ACCORDANCE WITH THE FOLLOWING:
 - THE DEVICE REQUIRED WILL BE APPROVED EROSION CONTROL MAT LISTED ON THE CITY'S APPROVED MATERIAL LIST. SAID BLANKET SHALL BE PLACED OVER THE APPROPRIATE SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS. (SEE SOIL EROSION BMPs - BACK OF CURB SEDIMENT BARRIER DETAILS)
 - THIS DEVICE SHALL BE INSTALLED IMMEDIATELY WHENEVER THE CURB IS BACKFILLED TO WITHIN 3" OF THE TOP OF CURB. (SEE CURB BACKFILL DETAIL) OTHER BMP'S MAY BE REQUIRED AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB.
 - ADDITIONALLY, OTHER EROSION CONTROL DEVICES (HAY BALES, SILT FENCE, ETC.) WILL BE INSTALLED AT LOCATIONS OF CONCENTRATED FLOW RESULTING IN SEDIMENT OVERRUNNING THE MAT.
 - SHOULD THE PROJECT PLANS SPECIFY THAT THE RIGHT-OF-WAY IS TO BE SODDED, THE EXCELSIOR MAT WILL NOT BE REQUIRED SO LONG AS THE SOD IS PLACED WITHIN 48 HOURS AFTER CURB BACKFILL REACHES A HEIGHT OF 3" OR LESS FROM TOP OF CURB. (SEE CURB BACKFILL DETAIL)

LEGEND

- R-O-W LIMITS
- - - DRAINAGE FLOW PATH
- x x x x x R/W LIMIT WITHIN CONSTRUCTION LIMIT
- STORM WATER INLETS
- IP INLET PROTECTION
- S.H- SILT FENCE OR HAY BALE BARRIER
- SP STREAM PROTECTION
- SCE STABILIZED CONSTRUCTION ENTRANCE
- ////// BACK OF CURB PROTECTION



THIS IS A TEMPORARY MEASURE ONLY, WHEN APPROVED BY THE PROJECT ENGINEER. THE DIRT GRADE BEHIND THE CURB SHALL BE BROUGHT TO THE TOP OF CURB, WITH TEMPORARY EROSION CONTROL MAT OR PERMANENT VEGETATION PLACED, PRIOR TO THE COMPLETION OF ALL PROJECTS.



REVISION: JUNE 2015		
STREET IMPROVEMENT PROJECTS		
CITY ENGINEER		
GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE	SHEET	
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		

PAVING IMPROVEMENTS

**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**

PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

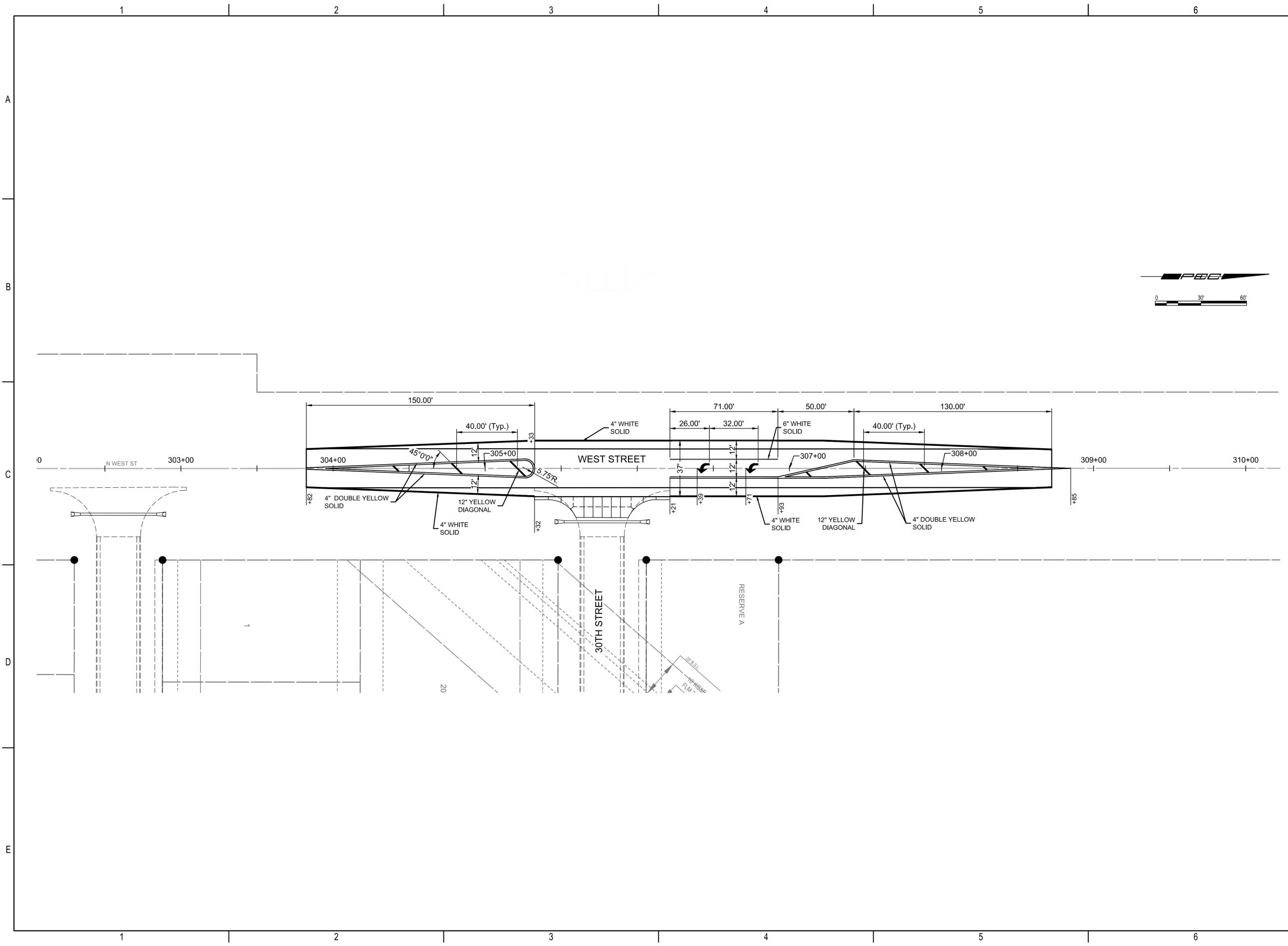
Issue:		

JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

EROSION CONTROL DETAILS



SAVED 11/14/2025 10:35:42 AM BY BRADLEY HAYNES
 PLOTTED 12/22/2025 9:17:25 AM BY HUNTER WILES
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 LANE\221170-012-PAV_MARK_SIGN_PLANS.DWG



PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:	

JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

PAVEMENT MARKING & SIGNING PLAN

TRAFFIC CONTROL NOTES

1. THE CONTRACTOR SHALL SUPPLY, INSTALL, AND MAINTAIN CONSTRUCTION TRAFFIC CONTROL AS NECESSARY TO PROTECT THE TRAVELING PUBLIC. TRAFFIC CONTROL FOR CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS (LATEST EDITION). ALL DEVICES SHALL BE REFLECTORIZED. ADDITIONAL SIGNS, BARRICADES, AND ANY OTHER TRAFFIC CONTROL DEVICES NOT SHOWN ON THE PLANS WHICH MAY BE DEEMED NECESSARY BY THE ENGINEER FOR TRAFFIC CONTROL DURING CONSTRUCTION WILL NOT BE MEASURED OR PAID FOR SEPARATELY, BUT SHALL BE SUBSIDIARY TO THE CONTRACT LUMP SUM PRICE BID FOR "TRAFFIC CONTROL". ALL WORK ASSOCIATED WITH TRAFFIC CONTROL AND PROPERTY ACCESS SHALL BE INCLUDED IN THE LUMP SUM BID ITEM "TRAFFIC CONTROL".
2. THE CONTRACTOR SHALL PROVIDE A DETAILED CONSTRUCTION SEQUENCING AND TRAFFIC CONTROL PLAN FOR ENGINEER REVIEW AND APPROVAL PRIOR TO START OF CONSTRUCTION.
3. THE CONTRACTOR SHALL PROVIDE DAILY VEHICULAR ACCESS TO PROPERTIES WITHIN THE PROJECT AREA THROUGHOUT CONSTRUCTION. CONTRACTOR SHALL ALSO NOTIFY PROPERTY OWNERS SEVENTY-TWO (72) HOURS IN ADVANCE BEFORE PAVING DRIVEWAYS/APPROACHES.
4. EMERGENCY TRAFFIC SHALL BE PERMITTED THROUGH AND ACROSS CONSTRUCTION AT ALL TIMES. THE CONTRACTOR SHALL PROTECT TRAFFIC BY USE OF PROPER AND NECESSARY FLAGS, LIGHTS, SIGNALS, BARRICADES OR OTHER WARNING DEVICES AS NEEDED, ALL IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).

MILESTONE	MAJOR CONSTRUCTION ITEMS	HANDLING OF TRAFFIC	REMARKS
1	CONSTRUCTION OF FULL IMPROVEMENTS OF WEST STREET.	MAINTAIN MINIMUM 1-WAY LOCAL ACCESS.	MAINTAIN ACCESS TO PROPERTIES AT ALL TIMES.
2	FINAL PAVEMENT MARKING, SIGNING, SEEDING, AND OTHER MISCELLANEOUS ITEMS.	LANES OPEN TO TRAFFIC AS WORK IS COMPLETED.	LANE CLOSURES WITH THE USE OF FLAGGERS WILL BE PERMITTED DURING DAYLIGHT HOURS WITH THE APPROVAL OF THE ENGINEER.



PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:		

JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

TRAFFIC CONTROL AND
 CONSTRUCTION
 SEQUENCING

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 SEQUENCING.DWG

SAVED 11/11/2025 4:07:01 PM BY HUNTER,WILES
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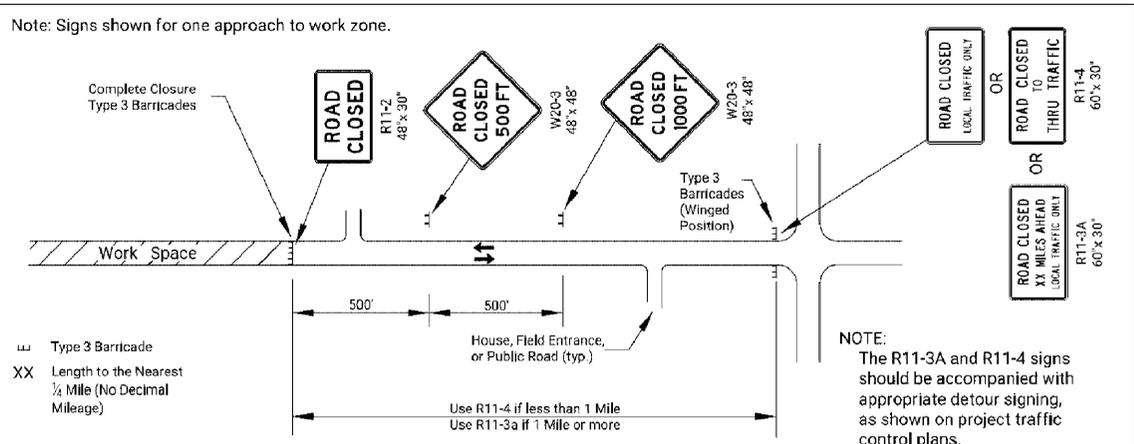


FIGURE 1: TYPICAL SIGNING FOR ROAD CLOSURE (MAINLINE OR SIDE ROAD)

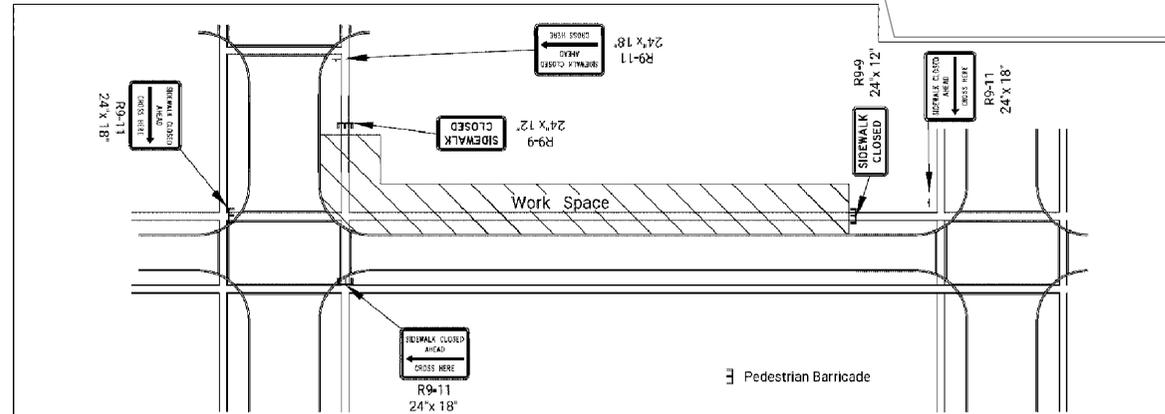


FIGURE 4: TYPICAL SIGNING FOR SIDEWALK CLOSED WITH OPPOSITE SIDEWALK AVAILABLE

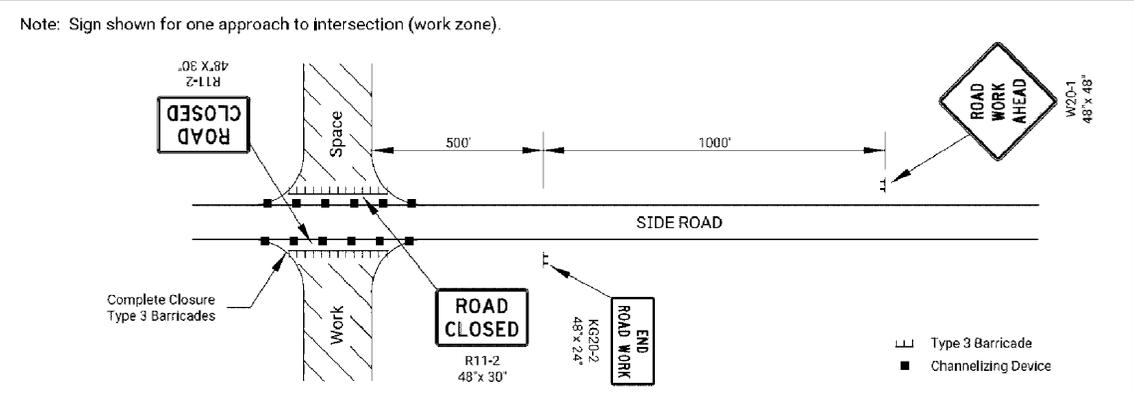


FIGURE 2: TYPICAL SIGNING FOR SIDE ROAD OPEN

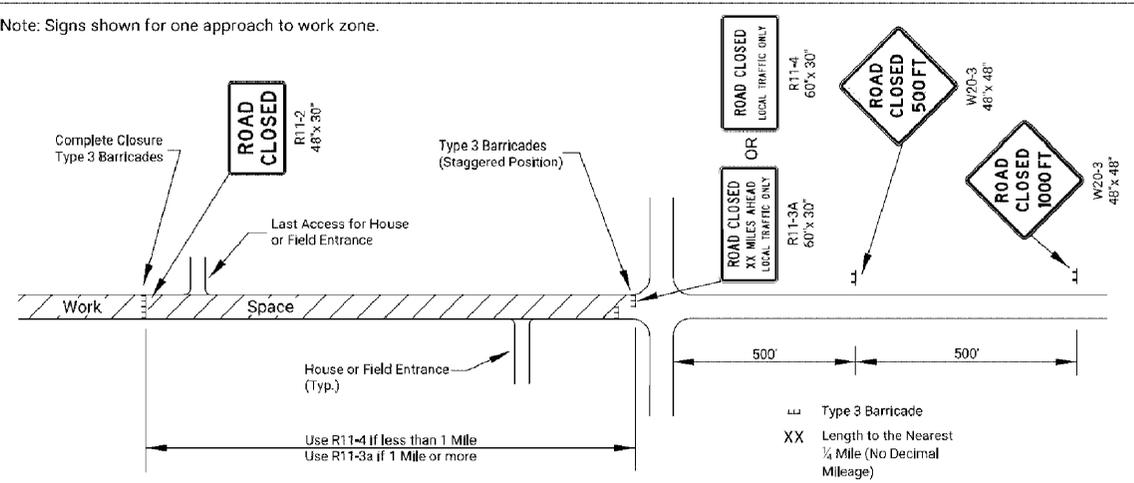
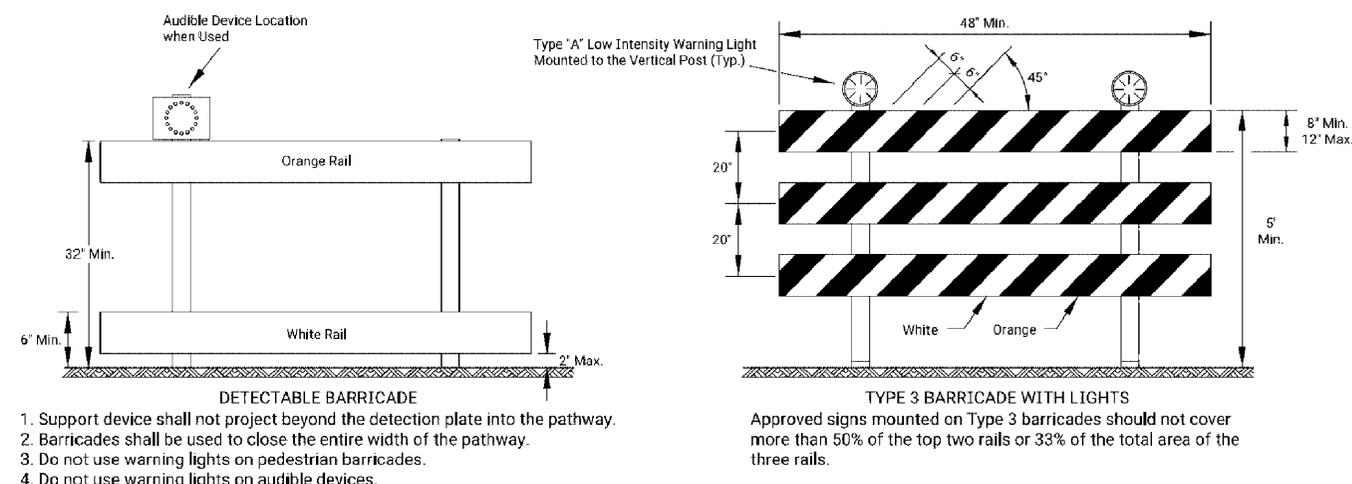


FIGURE 3: TYPICAL SIGNING FOR ROAD CLOSURE - LOCAL TRAFFIC ACCESS



ROAD CLOSED GENERAL NOTES

As shown in Figure 1, at the point where thru traffic must detour and local traffic can proceed to the location where the roadway is completely closed, the R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) or R11-4 (ROAD CLOSED LOCAL TRAFFIC ONLY or ROAD CLOSED TO THRU TRAFFIC) sign shall be used with Type 3 barricades (winged position), placed on the shoulders of roadway.

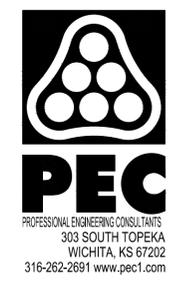
As shown in Figure 3, when local traffic must be allowed access into the work zone, Type 3 barricades shall be longitudinally staggered to maintain the appearance of a closed roadway. A second line of end-to-end Type 3 barricades shall be placed just beyond the last access point in the work zone, to completely close the roadway.

The R11-4 (ROAD CLOSED TO THRU TRAFFIC or ROAD CLOSED LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is less than 1 mile.

The R11-3a (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) sign shall be used when the distance to the point of complete closure of the roadway is 1 mile or greater.

The words "BRIDGE OUT" (or BRIDGE CLOSED) may be substituted for the words "ROAD CLOSED" on the R11-3a or R11-4 sign where applicable.

**ADOPTED FROM KDOT
DETAIL TE704**

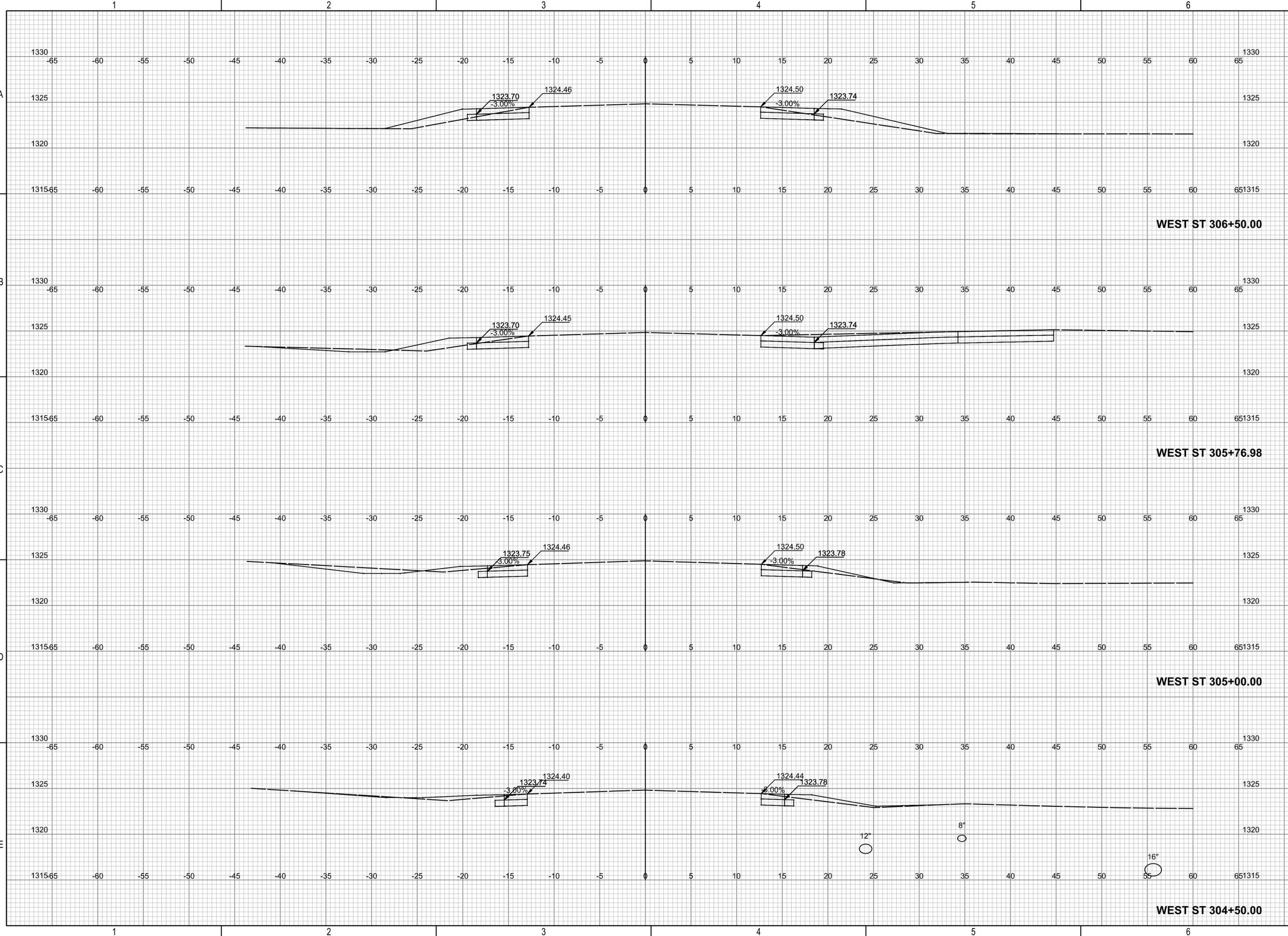


PAVING IMPROVEMENTS
WEST STREET LANE TO SERVE
PINEWAY ADDITION
 PAUL GUNZELMAN CITY ENGINEER
 CITY OF WICHITA PROJECT NO. 472-2024-085960

Issue:	
JOB NO.	221170-015
DATE	DECEMBER 2025
PM	KPG
DESIGNED BY	HJW
DRAWN BY	BJH
CHECKED BY	KPG

TRAFFIC DETAILS (2)

SAVED 12/22/2025 9:14:00 AM BY HUNTER.WILES
 PLOTTED 12/22/2025 9:17:55 AM BY HUNTER.WILES
 U:\WICHITA-CIVIL\2022\2211700\12\22\PD4_PLANS\030\DRAWINGS\AUXILIARY LANE\221170-012-CROSS-SECTIONS.DWG



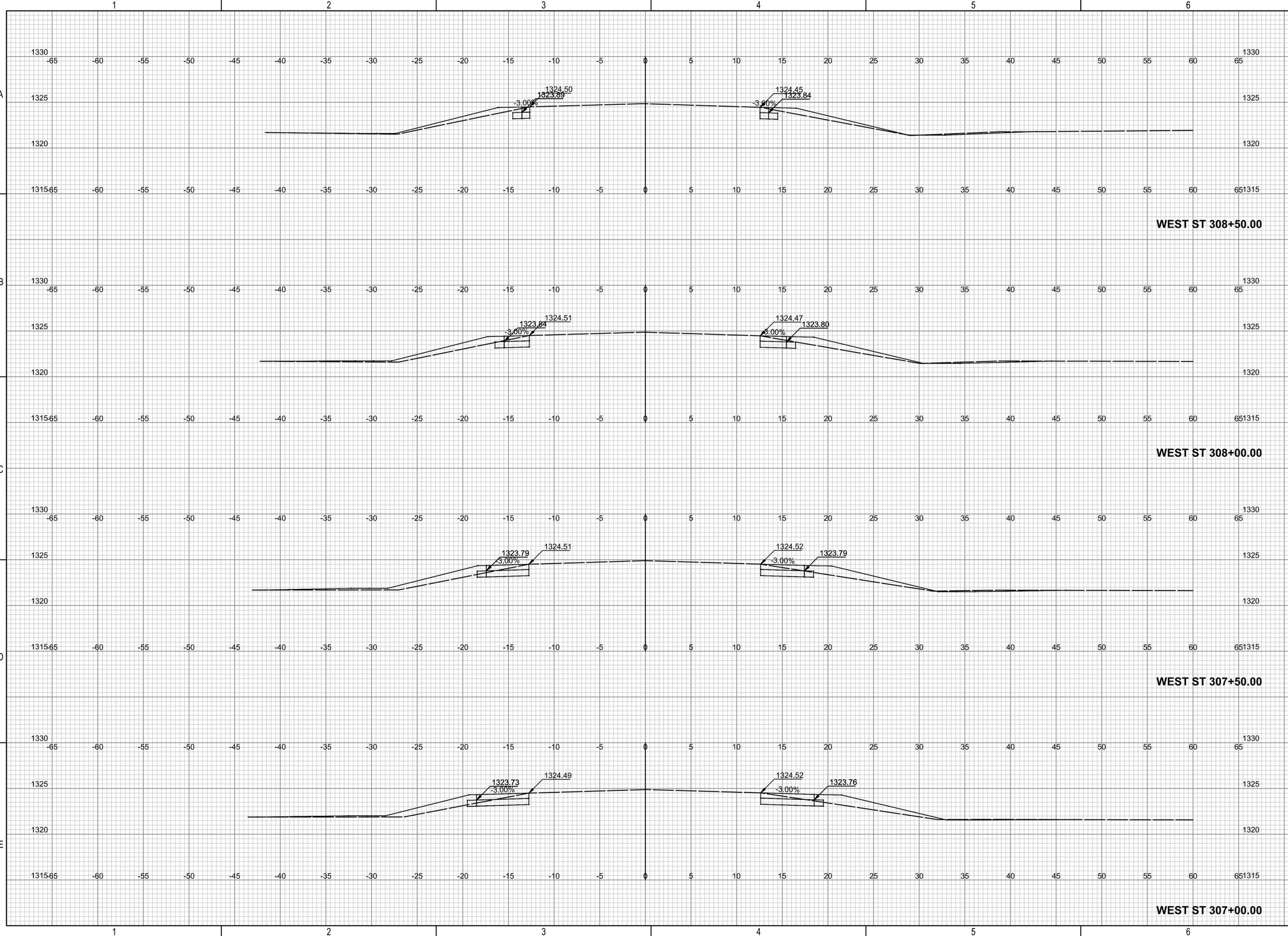
PAVING IMPROVEMENTS
**WEST STREET LANE TO SERVE
 PINEWAY ADDITION**
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WEST STREET CROSS SECTIONS

SAVED 12/22/2025 9:14:00 AM BY HUNTER.WILES
 PLOTTED 12/22/2025 9:17:56 AM BY HUNTER.WILES
 U:\WICHITA-CIVIL\2022\221170\12\2\PD4_PLANS\03\DRAWINGS\AUXILIARY LANE\221170-012-CROSS-SECTIONS.DWG



PAVING IMPROVEMENTS
WEST STREET LANE TO SERVE
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WEST STREET CROSS SECTIONS