

Street Paving for IKE STREET to serve ABILENE PLACE ADDITION - PHASE II

IKE ST: From the west line of the plat, east to the east line of Lot 24, Block B
 IKE CT (LOTS 1-3, G & LOTS 6-8, F): From the south line of Ike St, South to and Including the Hammerhead
 IKE CT (LOTS 1-5, F & LOTS 6-10 E): From the south line of Ike St, South to and Including the Hammerhead.

CITY OF WICHITA, KANSAS

Paul Gunzelman, P.E., City Engineer
 Project Number: 472-2021-085725
 Org Code: 47479725
 Munis Number: E5135

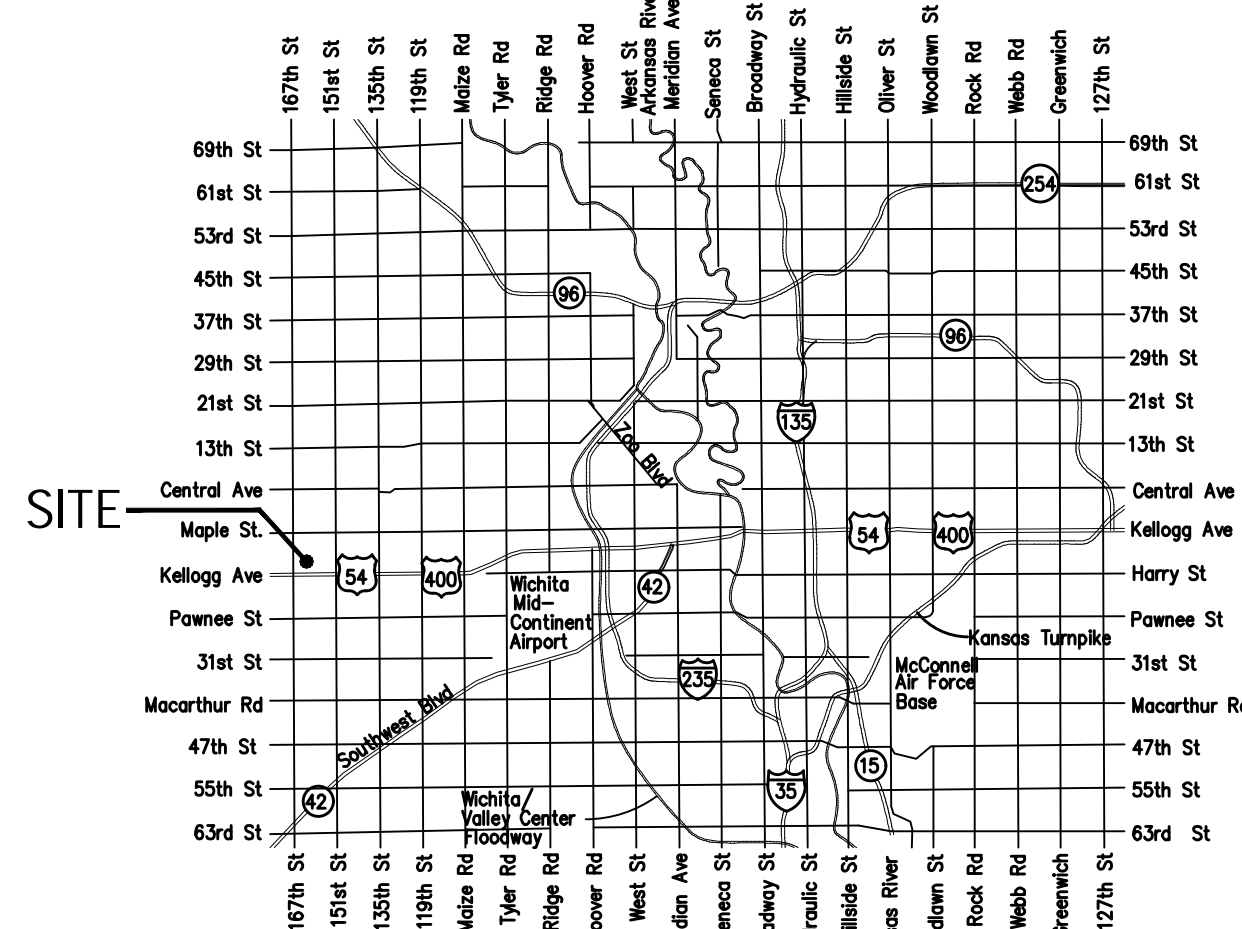
GENERAL NOTES:

- Contractor will be required to provide notice to utility companies a minimum of forty-eight (48) hours prior to any excavation, as follows:
 Kansas One-Call 687-2470
 The Contractor must notify the following in case of an emergency:
 Cox Communications 262-4270
 Kansas Gas Service 1-888-482-4950
 Westar Energy 383-8650
 Aquila Energy 1-800-303-0357
 ATT 268-2245
 City of Wichita Water Dept. 268-4563
 City of Wichita Sewer Maint. 268-4024
 City of Wichita Storm Sewer Maint. 268-4090
 City of Wichita Traffic Maint. 268-4034
 Conaco Phillips Pipeline Co. 1-877-267-2290
 Southern Star Pipeline Co. 529-6600
 Kinder-Morgan Pipeline Co. 1-888-844-5658
- Utility service lines, poles, valve boxes, meters, and etcetera are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the Contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The Contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction.
- Rubble from the removal of miscellaneous structures and excess excavation which is to be wasted shall be disposed of on sites to be provided by the Contractor. These sites shall be approved by the Engineer as to suitability, appearance and site location. Locations, in the opinion of the Engineer, that will leave an unsightly appearance will not be approved. All disposal sites must be approved by the Kansas Department of Health and Environment. Material either stockpiled or disposed of in a flood plain would require a Kansas State Board of Agriculture permit. Any material dumped in waters of the United States or wetlands is subject to U.S. Corps. of Engineers permitting regulations. Any material buried or stockpiled beyond approved construction limits would require additional archaeological investigations unless buried in a previously approved borrow location.
- Trees and shrubs in public right-of-way which are in direct conflict with proposed new construction shall be removed by the Contractor only as necessary for construction. Trees and shrubs which are not in direct conflict with proposed new construction shall be saved and protected from damage.
- The Contractor shall give all property owners and/or tenants of developed property abutting the construction of this project a minimum of ten (10) days notice prior to start of construction.
- The Contractor shall be responsible for preserving property irons. The Contractor will be required to re-establish any property irons which are damaged or destroyed by his construction operations. Such irons shall be re-established by a licensed Land Surveyor in accordance with state laws.
- All existing and proposed erosion control measures including silt fencing, erosion control mat, straw bales, inlet barriers, and const. entrance shall be maintained throughout construction by the Contractor and until project is accepted by the City of Wichita. The on-site Engineer shall complete weekly reports on the status of erosion control measures. The Contractor shall be required to comply with maintenance and/or replacement of erosion control measures as determined by the on-site Engineer until project is accepted by City of Wichita.
- A saw cut of at least one-half the depth of existing surface courses or one-fourth the depth of the existing total pavement thickness shall be provided at locations where proposed construction abuts the existing surface course or pavement for which partial removal of that surface or pavement is required. Sawed joint to facilitate removal within three (3) feet of existing joints will not be permitted and for such instances the limits of removal shall extend to the existing joint. Such saw cuts will not be paid for directly and this cost shall be considered as subsidiary to the removal of the surface or pavement.
- All areas disturbed during construction shall be seeded as follows: (Temporary Seeding)
 SEED -- Rye grass (PLS) -- 5 Lbs/1,000 Sq Ft.
- The Contractor shall adjust valve boxes as necessary to match proposed grade. Cost to be included in bid item, "Site Restoration."
- The Contractor shall drain any standing water in sump areas of the proposed pavement. Contractor shall overexcavate and recompact these areas as required to maintain compaction of 95% Standard Proctor Density. Cost to be incidental to pavement.
- Limits of earthwork shall match existing ground elevations at the right-of-way line unless otherwise noted on the plans with a new finished grade elevation. When a new finished grade elevation is shown, the earthwork shall extend one foot beyond the right-of-way line and then slope up or down using permissible slopes to match the existing ground surface.
- The Developer for this project is Abilene Place Properties, LLC, Jayson Russell, Member, (316) 644-8333.
- Any excess excavation shall be removed from the site. Cost of removal shall be included in bid item "Site Restoration."

BENCHMARKS

BENCHMARK #1:
 Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

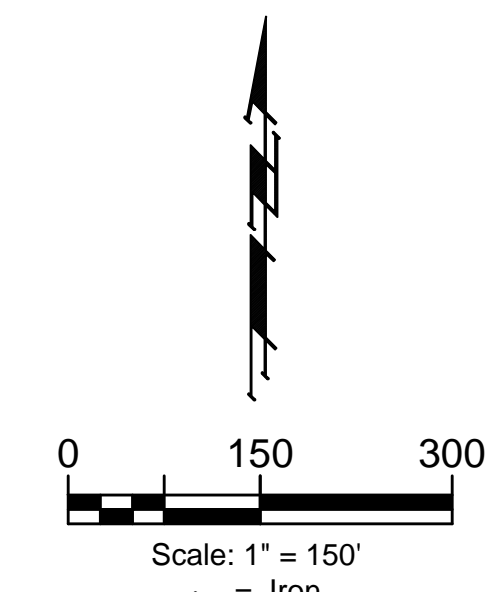
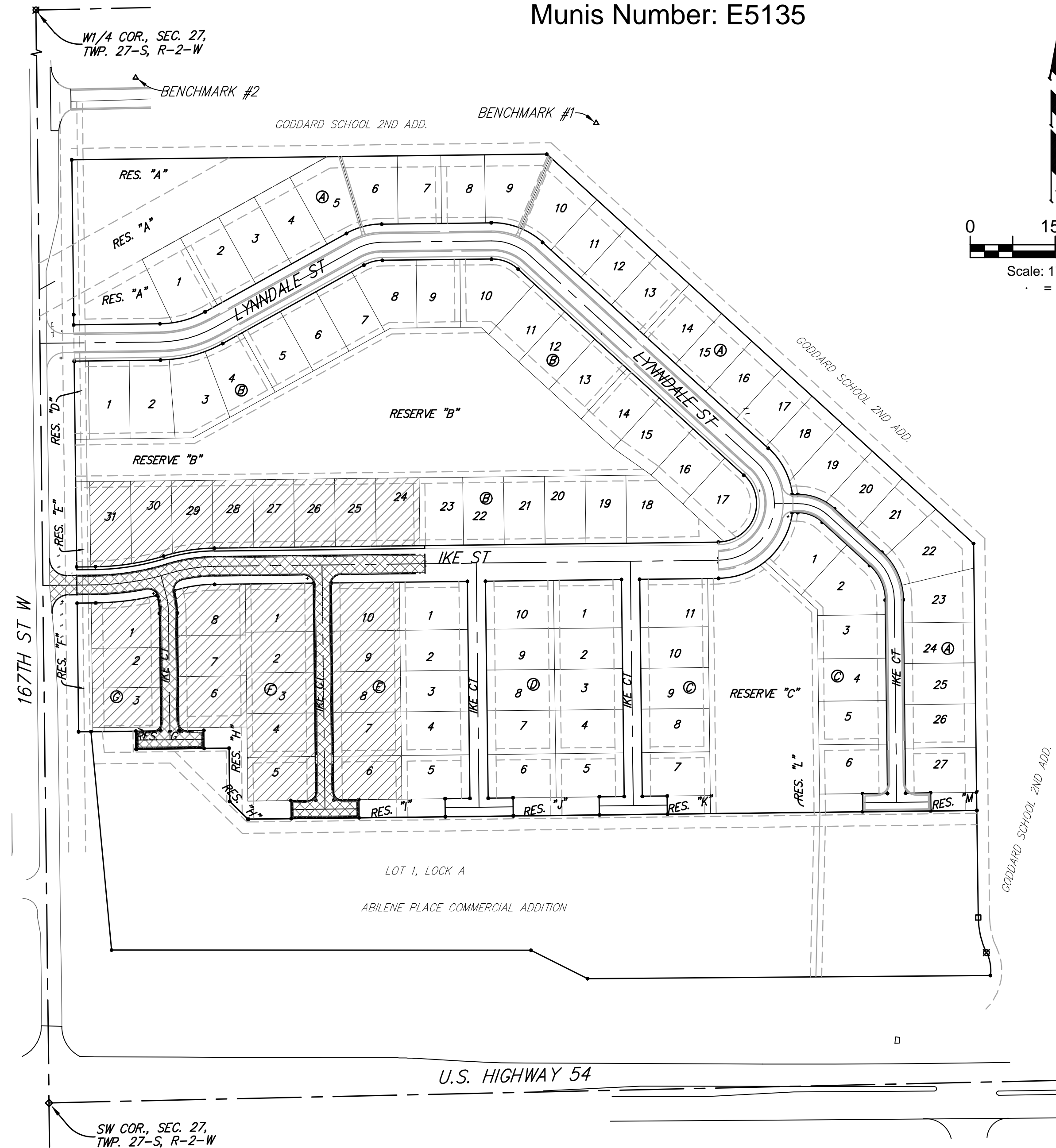
BENCHMARK #2:
 Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 14.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



VICINITY MAP

BENEFIT DISTRICT:
PROPOSED STREETS:

Project Earthwork Totals (CY unadjusted)	Excavation			Compacted Fill			Loose Fill		
	Street (R/W)	1,087	21	324	98	11	777	-	-
Total Project Length									
1,267 L.F. = 0.24 Miles									



SHEET INDEX

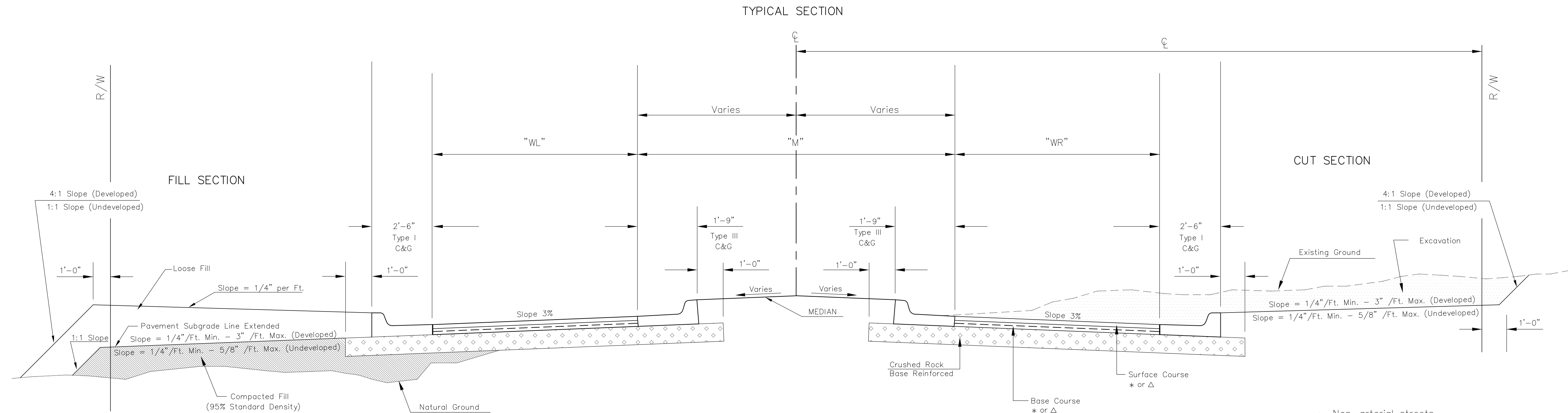
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Apr. 28, 2026

BAUGHMAN COMPANY
 315 Ellis St. Wichita, KS 67211 316-262-7271
 BaughmanCo.com

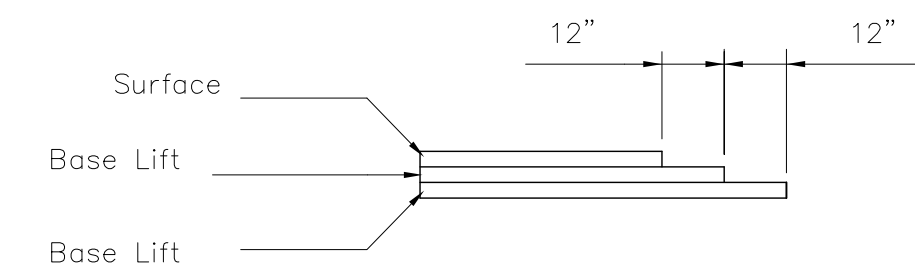
E:\Projects\Abilene Place Addition - 20-03-P540\Engineering\Phase 2\STR. PLANS.dwg



* Non-arterial streets
BC-1, SC-1 AND PG 64-22

△ Arterial streets
BM-2 PG 64-22(Base) PG 70-28(Surface)

Base Course thicker than 4" shall be installed in two lifts



TRANSVERSE CONSTRUCTION JOINTS

Transverse construction joints shall be constructed in flexible base pavement at locations where pavement joins existing flexible base pavement as show by the detail. All costs associated with the construction of the transverse joint shall be included in the bid price for Square Yards of pavement.

GENERAL NOTES

Fabric base reinforcement shall be an approved grid. Fabric base reinforcement shall be installed in accordance with manufacturer's recommendations. Crushed rock shall be uniformly graded from 1 - 1/2" maximum size to not more than 10% passing a No. 200 sieve. Rock quality shall be the same as specified for coarse aggregate for concrete mixes.

Rock base is to be compacted and smoothed with a steel faced roller prior to placement of asphalt. Tack coat will not be applied to rock base.

A tack coat of emulsified asphalt (SC-1H or CSS-1H) shall be applied to an approximate rate of 0.05 gallons per square yard between each lifts of asphaltic material.


Bituminous base and asphaltic concrete wearing surface shall be placed with a laydown machine having automatic controls for line and grade.

Construction joints in each lift shall be staggered a minimum distance of one (1) foot from joints in preceding lifts and placed so that a joint will be constructed on the centerline of the top lift.

The asphaltic concrete pavement between the combined curb and gutter shall be paid as square yards of of pavement.

STREET NAME	"WL"	"M"	"WR"	STATION	CENTER LINE	ROW DIMENSION	MEDIAN DESCRIPTION	SLOPE	ROCK THICKNESS	PAVEMENT THICKNESS	COMMENTS
Ike St	15'	-	15'	ALL	32'	32' L & R	-	3%	5"	5"	
Ike Ct (Lots 1-3, Blk G & 6-8, Blk F)	12'	-	12'	ALL	16'	16' L & R	-	3%	5"	5"	
Ike Ct (Lots 1-5, Blk F & 6-10, Blk E)	12'	-	12'	ALL	16'	16' L & R	-	3%	5"	5"	

REVISED: OCTOBER 2015



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

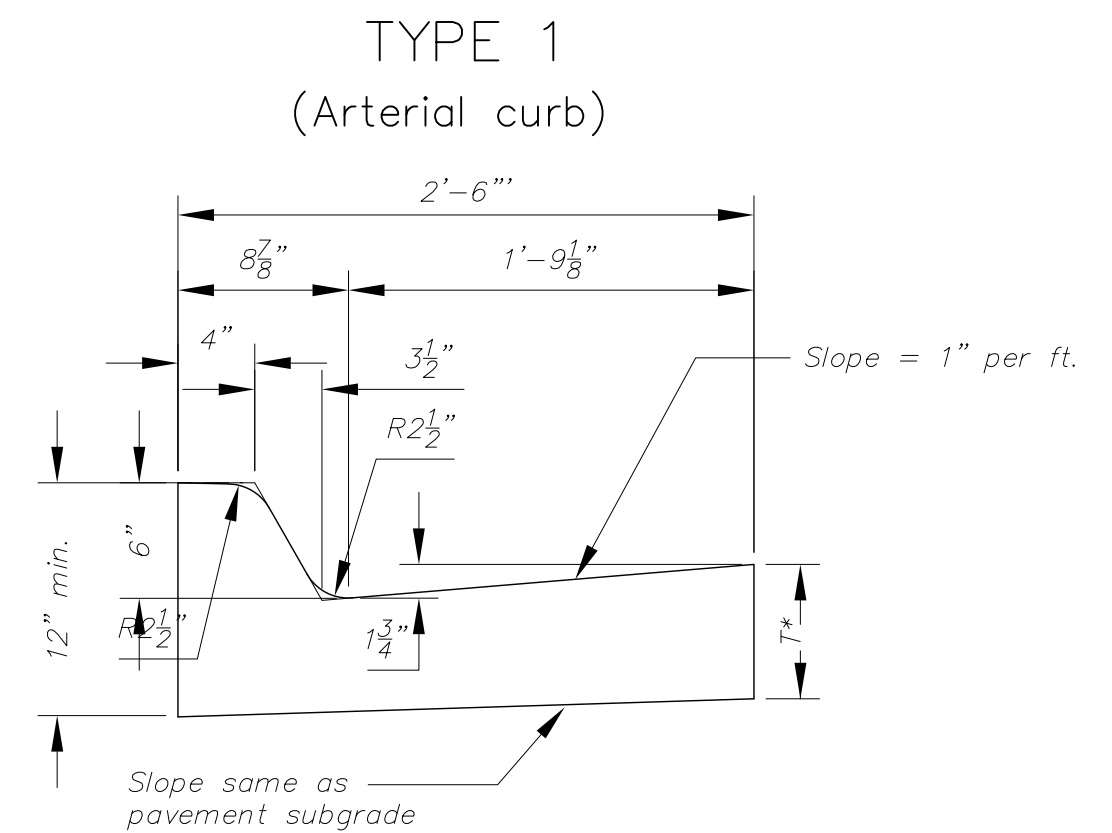
**ASPHALT PAVING
DETAIL**

CITY ENGINEER
GARY JANZEN, P.E.

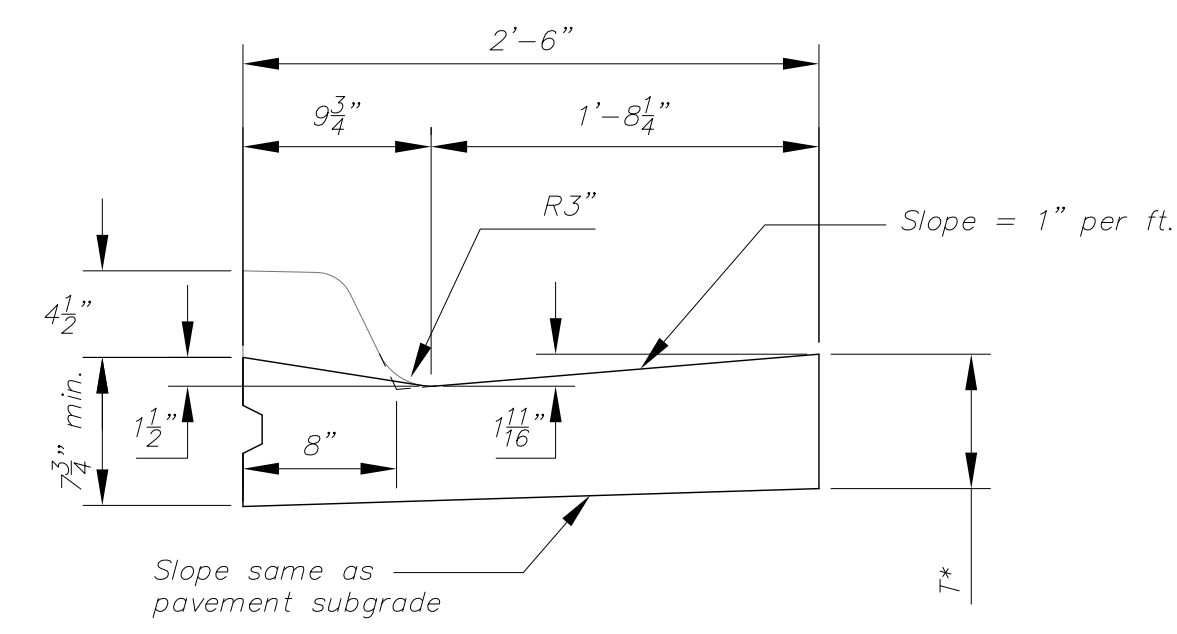
PROJECT NUMBER	OCA NUMBER	DATE

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

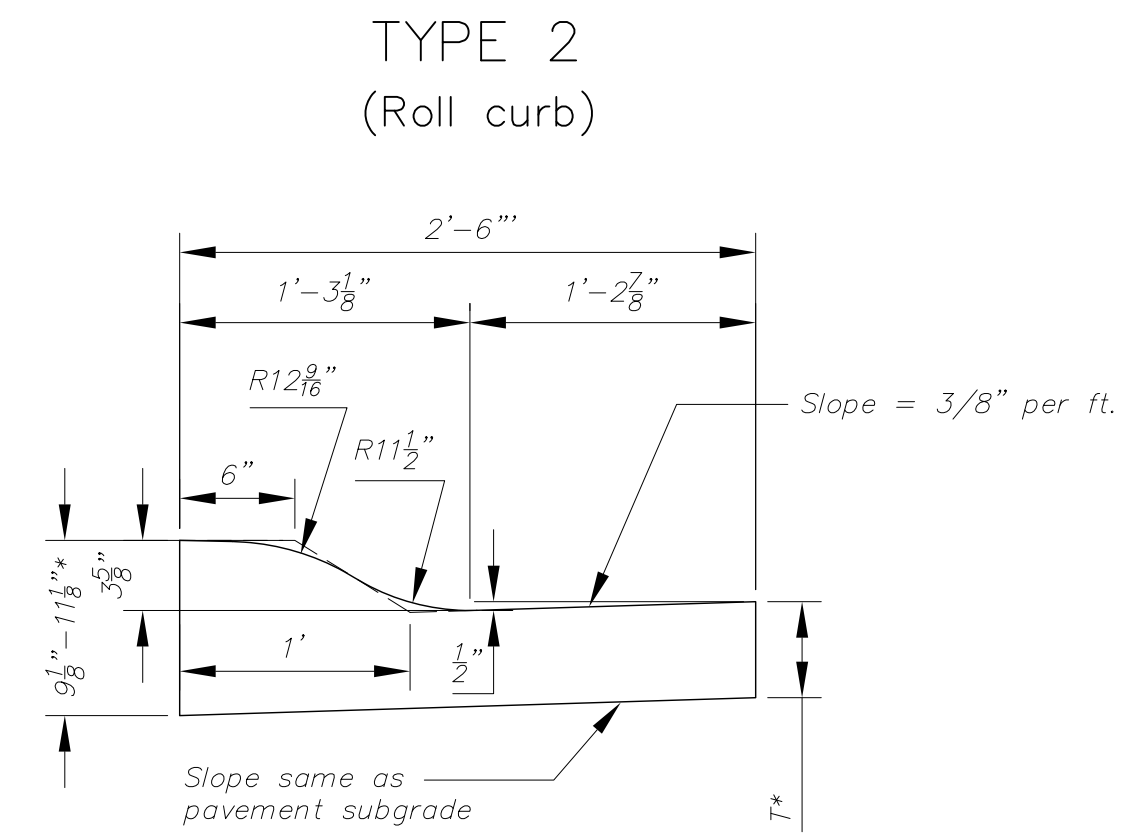
SHEET
2 of 20



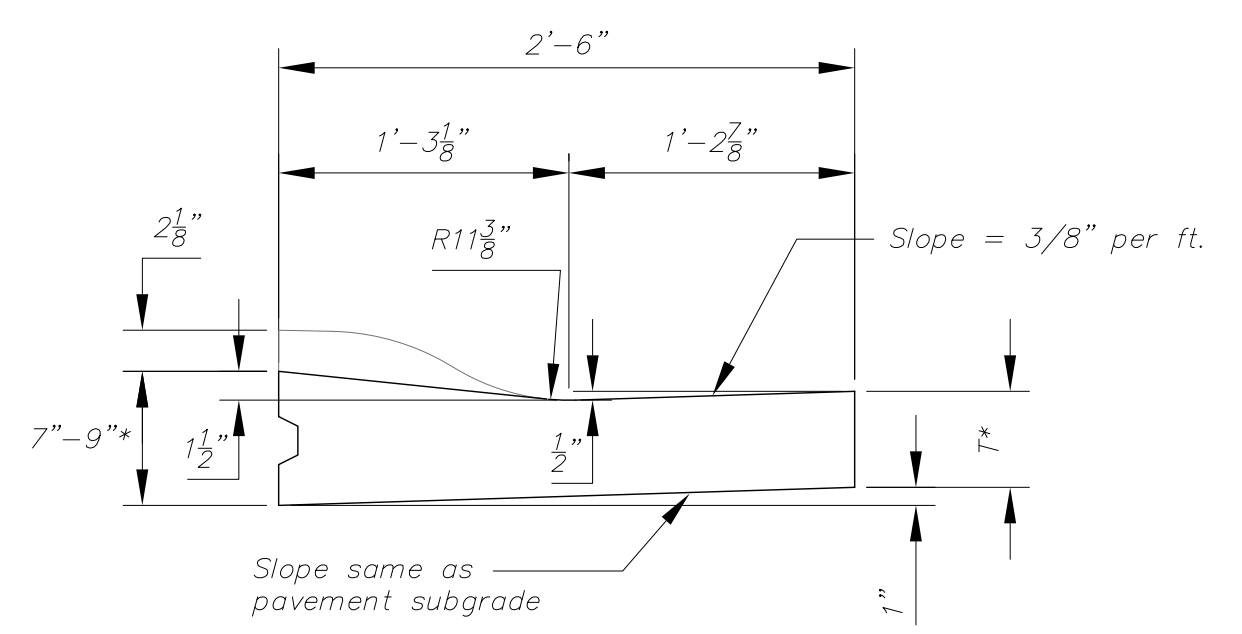
Combined Curb & Gutter (6")



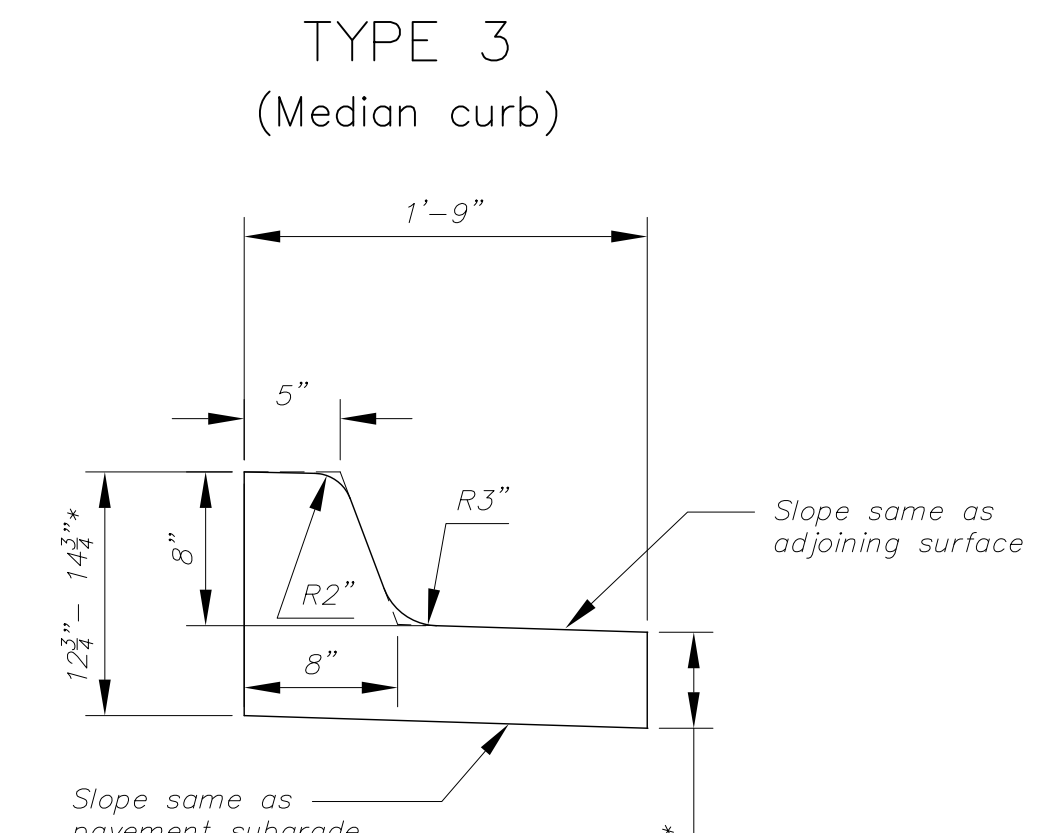
Combined Curb & Gutter (1 1/2")



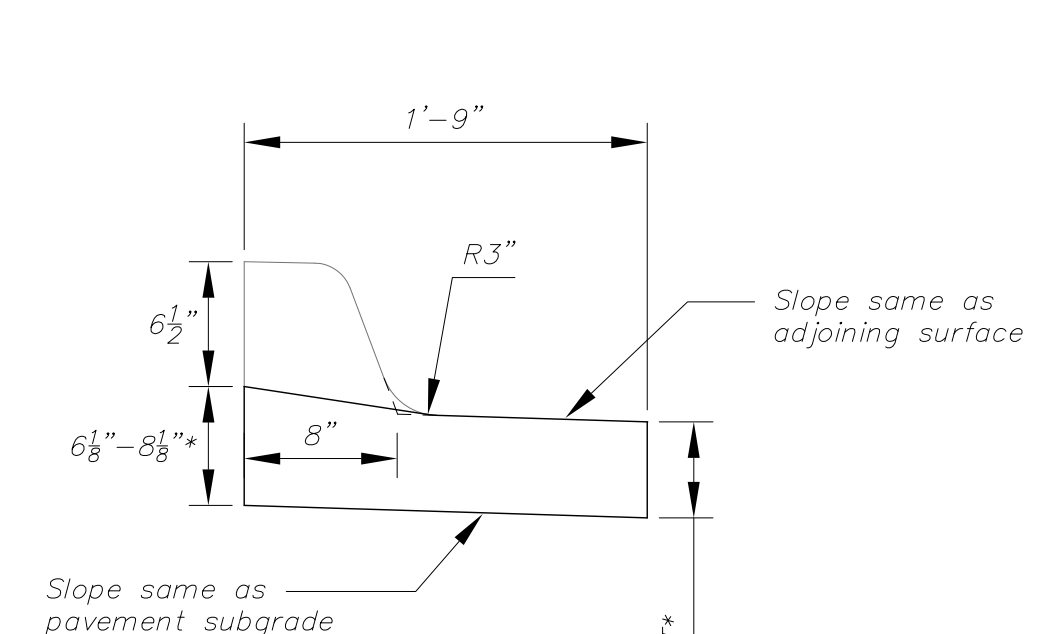
Combined Curb & Gutter (3 5/8")



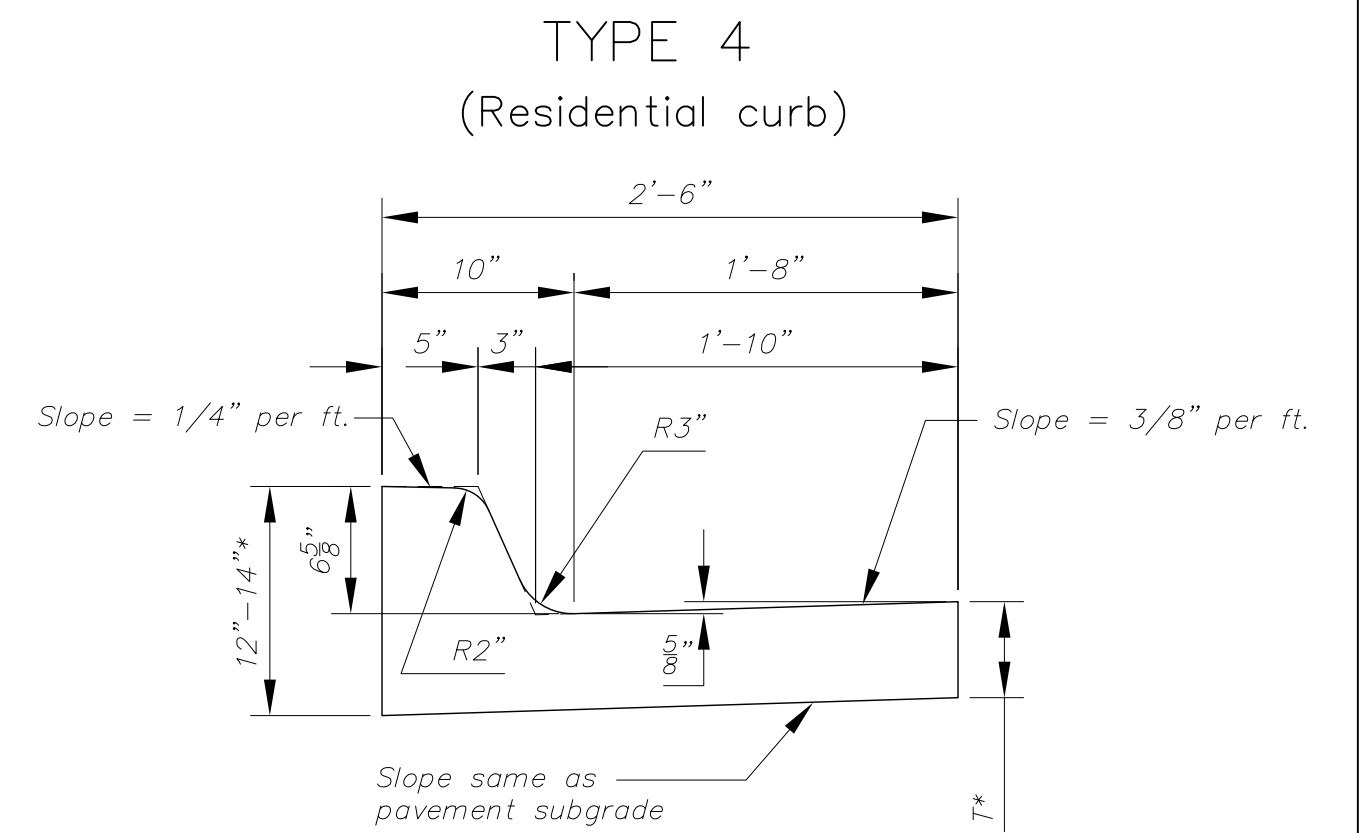
Combined Curb & Gutter (1 1/2")



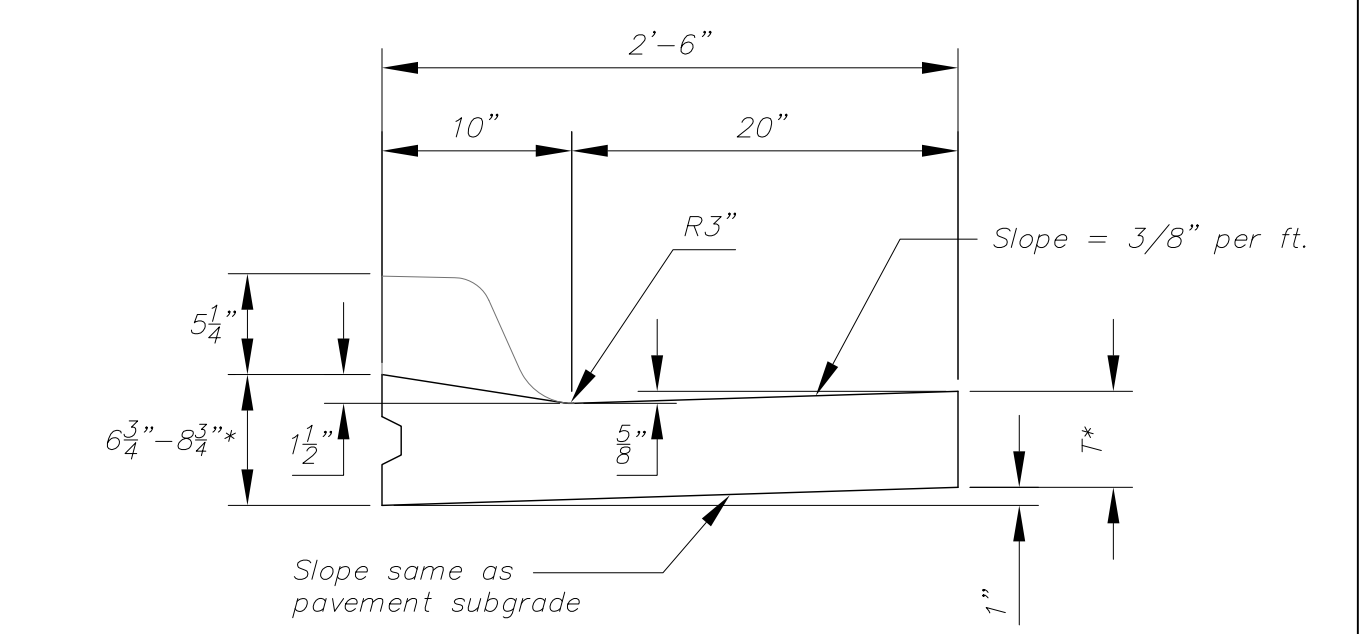
Combined Curb & Gutter (8")



Combined Curb & Gutter (1 1/2")



Combined Curb & Gutter (6 5/8")

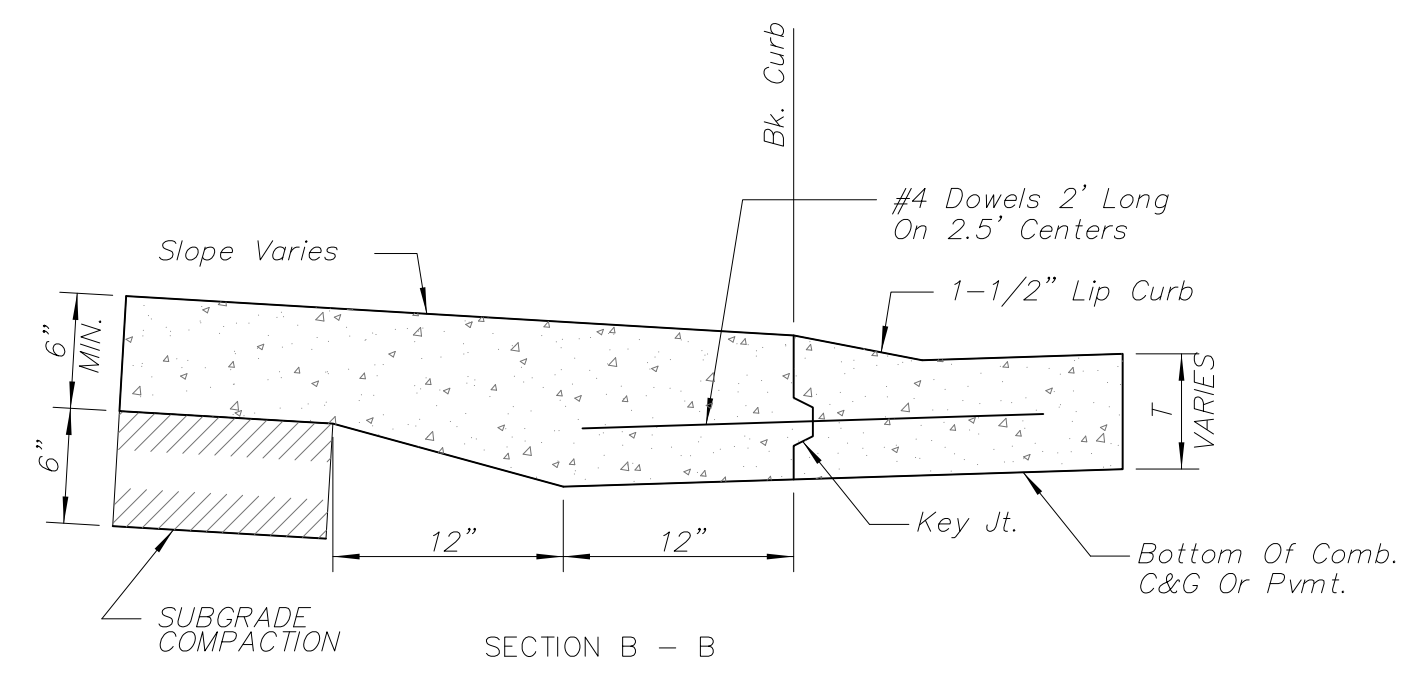


Combined Curb & Gutter (1 1/2")

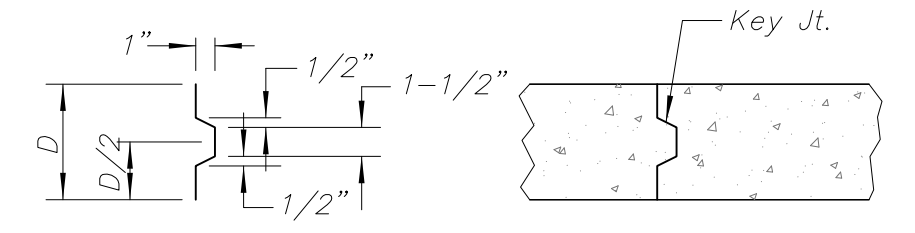
T* = Thickness of curb to adjust with pavement thickness

GENERAL NOTES

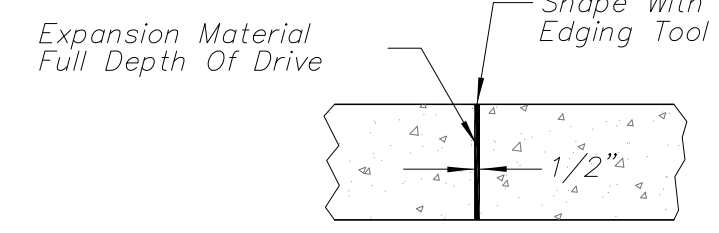
- Expansion (isolation) joints shall be constructed a maximum of 300' apart and at all Pls, PCs, cul-de-sac quadrants, and ends of returns.
- Contraction joints shall be constructed a minimum of 12' apart.
- Joint sealer shall be required at all joints on arterial and industrial streets and at intersections on residential streets.



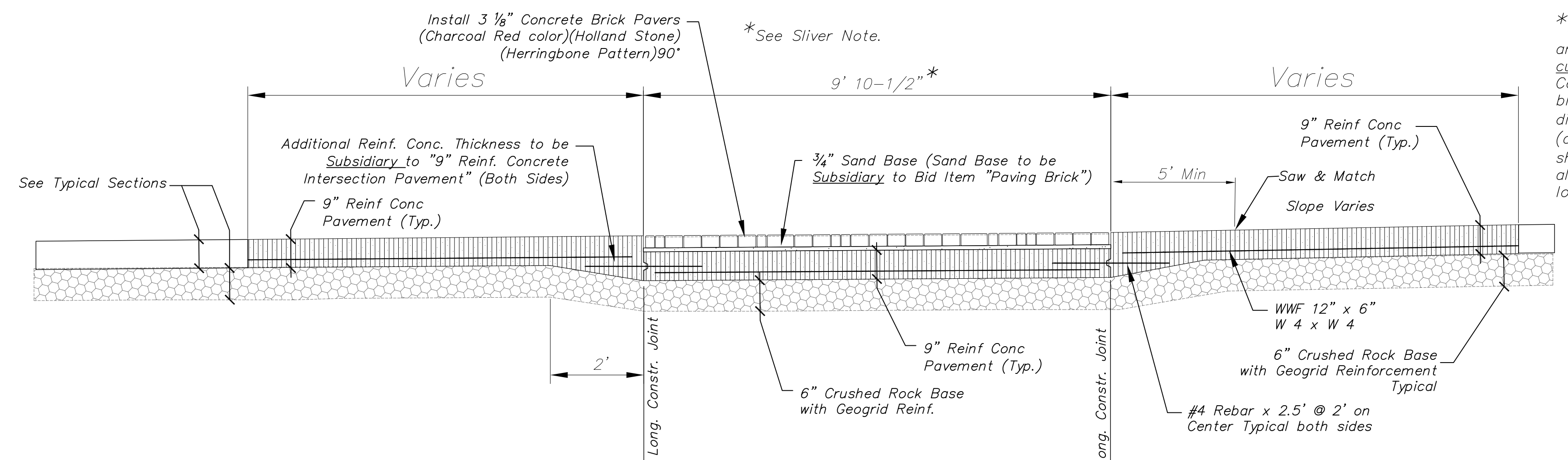
SECTION B - B
BACK OF CURB DETAIL



ALT. LONGITUDINAL CONSTRUCTION JOINT



EXPANSION JOINT (E.J.)

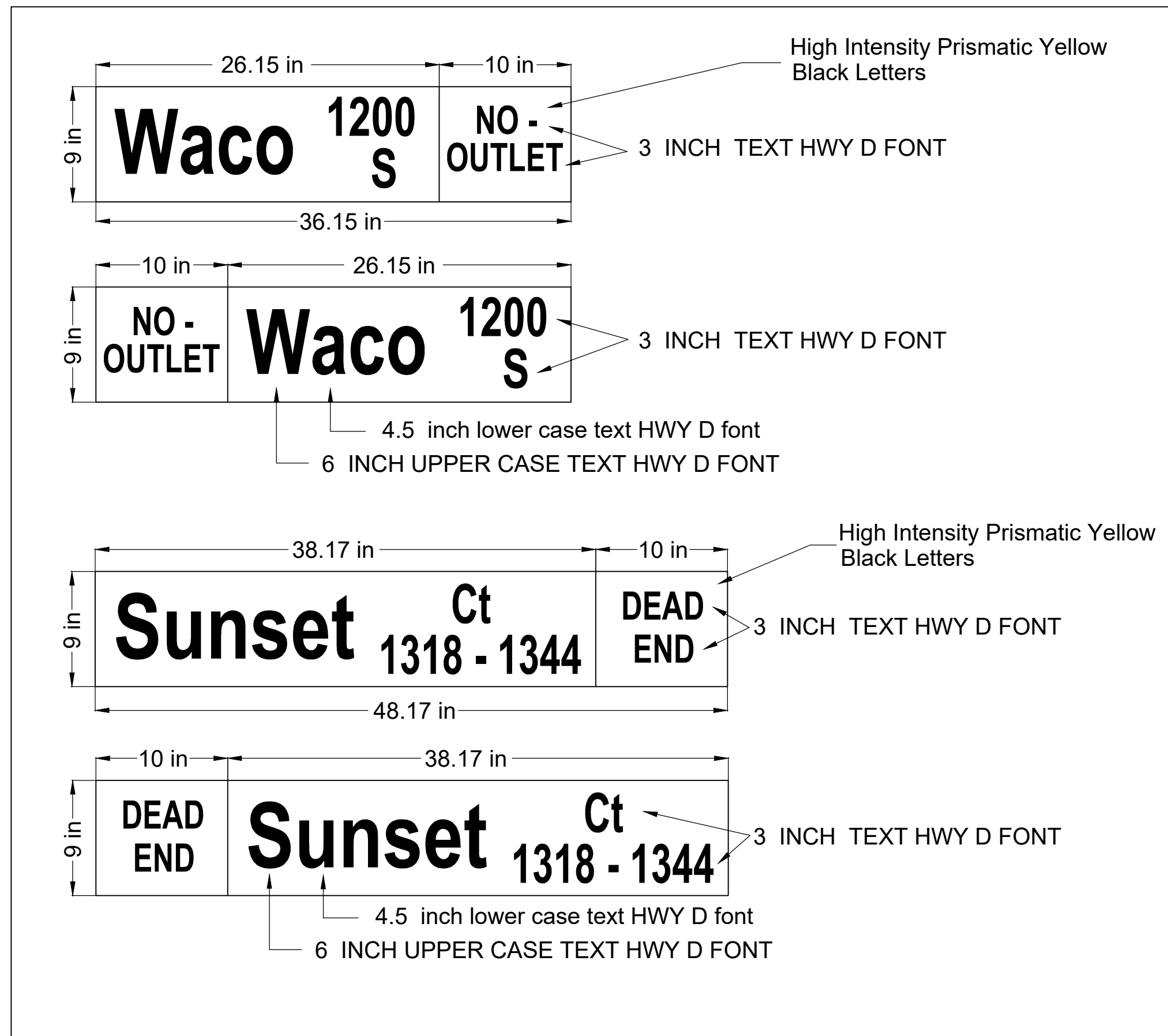


PAVING BRICK CROSSWALK DETAIL

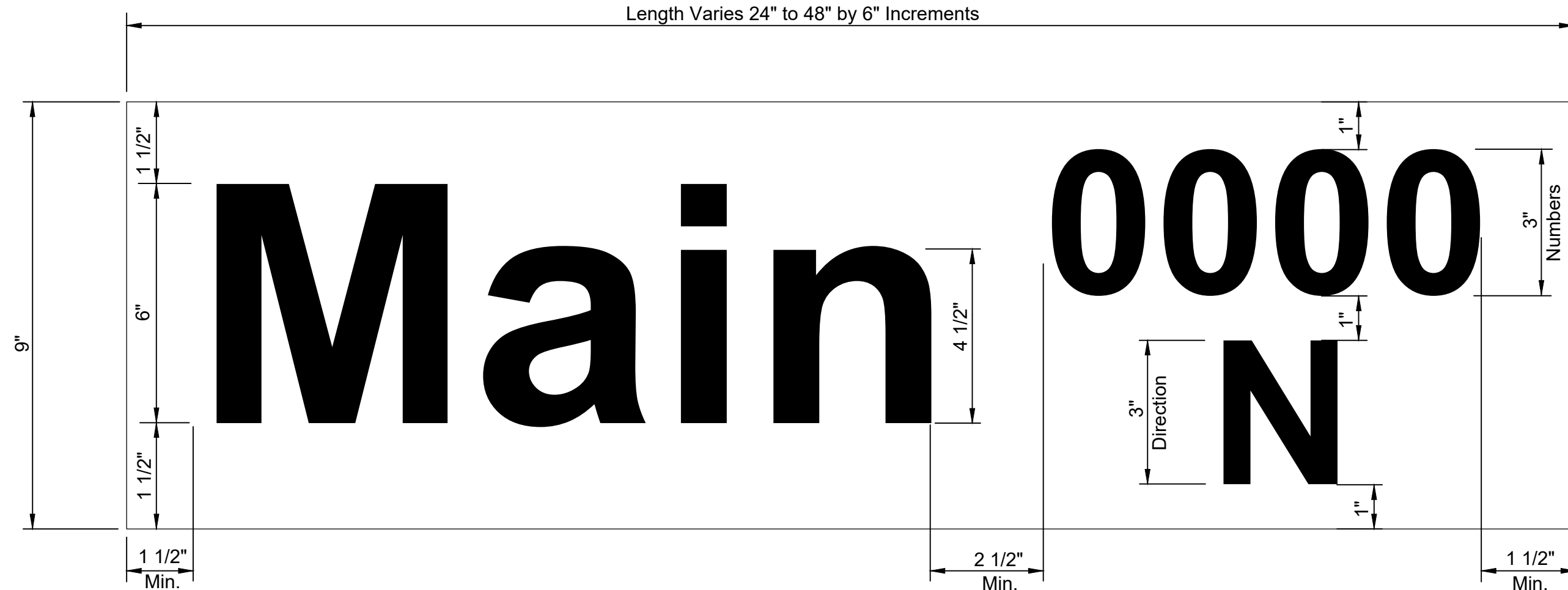
*Brick Pavers to be installed in crosswalk and/or median locations shall be custom cut to match final concrete dimensions. Contractor shall construct crosswalk blockouts to match nominal paver width dimensions. Fill in with "brick slivers" (defined as pieces smaller than 1/2 of the shortest paver dimension) shall not be allowed, except at the end of crosswalk locations.

CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

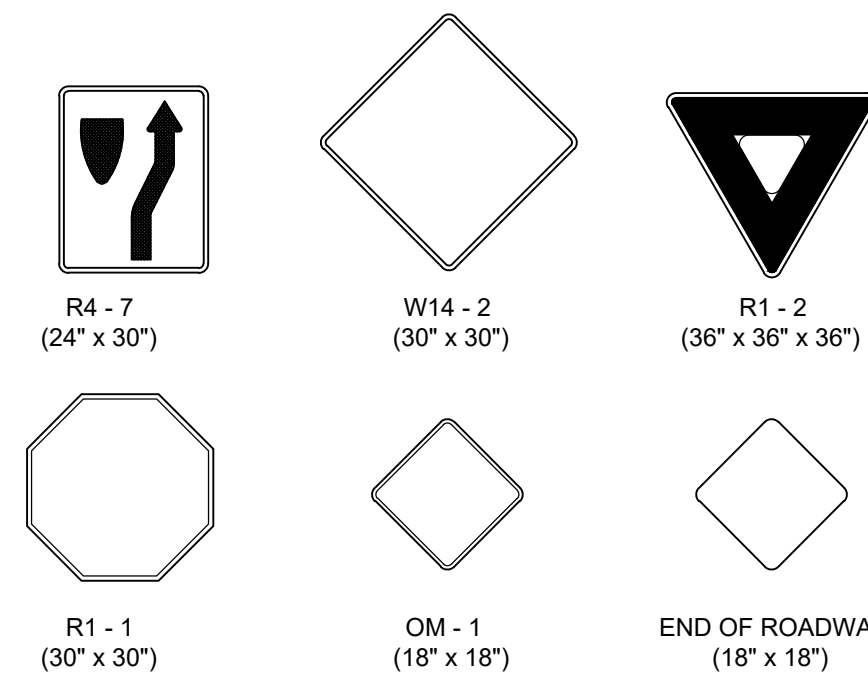
<p>REVISOR: OCTOBER 2015</p> <p>CURB & GUTTER & PAVING BRICK CROSSWALK DETAILS</p> <p>CITY ENGINEER GARY JANZEN, P.E.</p>		
PROJECT NUMBER	OCA NUMBER	DATE
<p>CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501</p>		<p>SHEET 3 of 20</p>



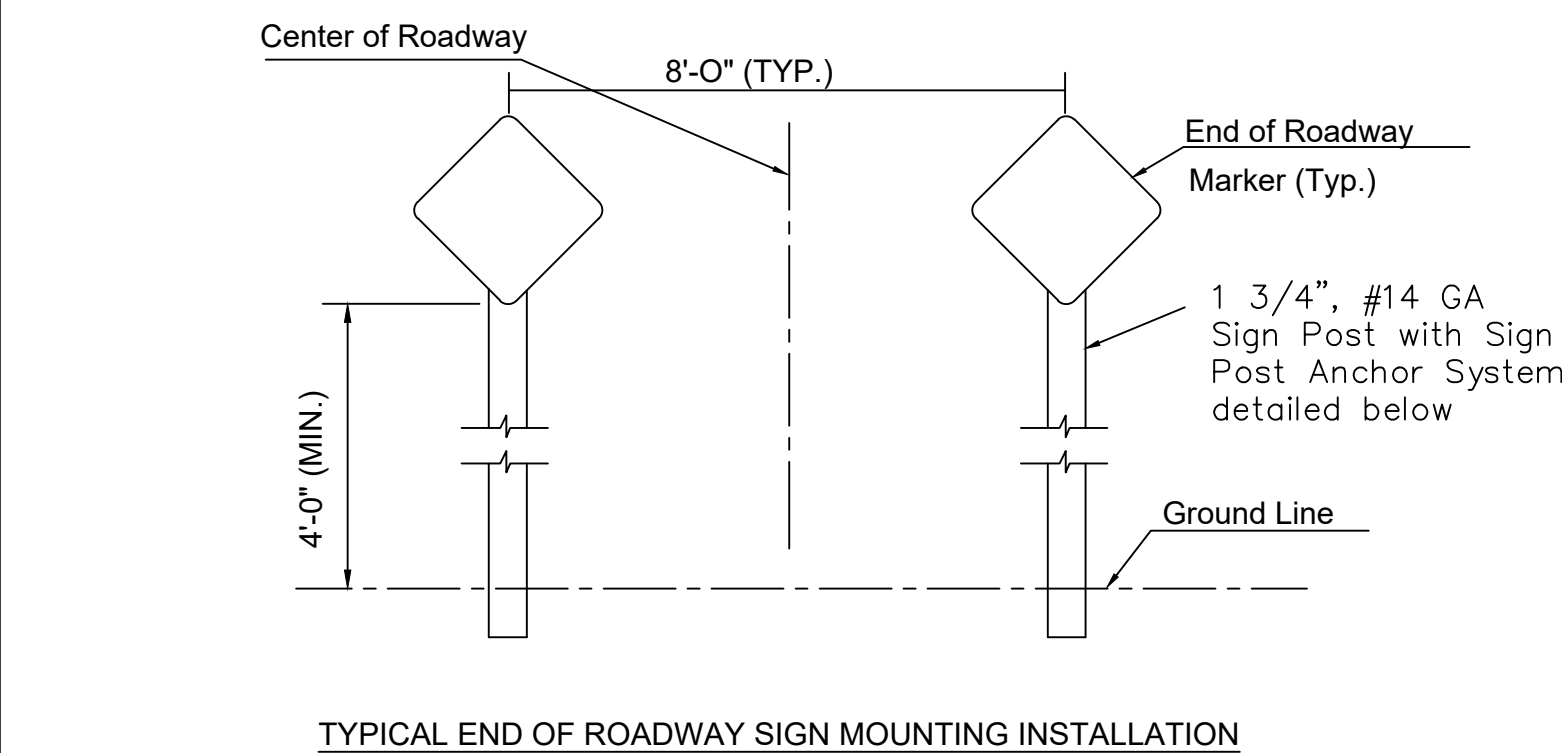
DETAIL A
9" STANDARD



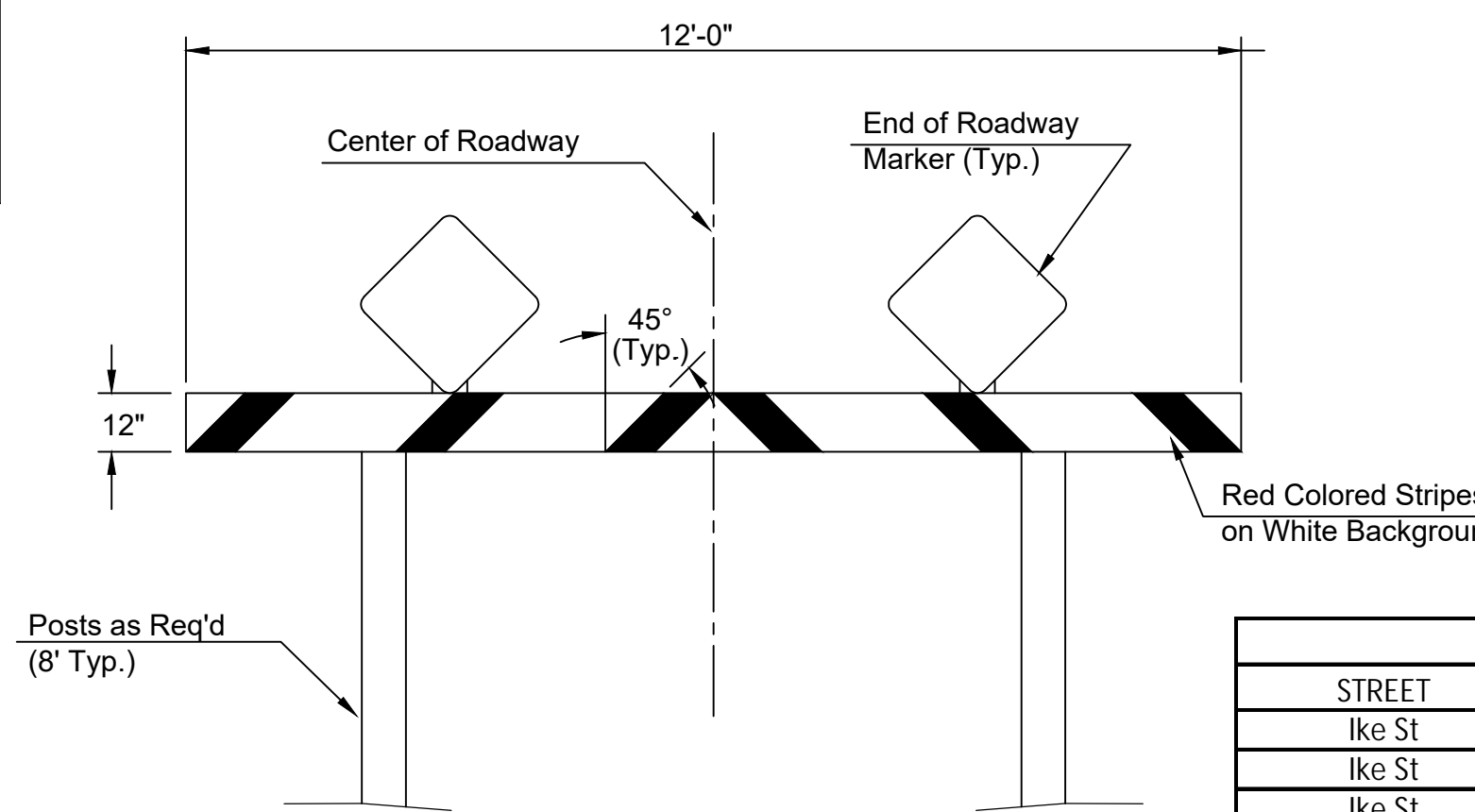
DETAIL B
9" METRO



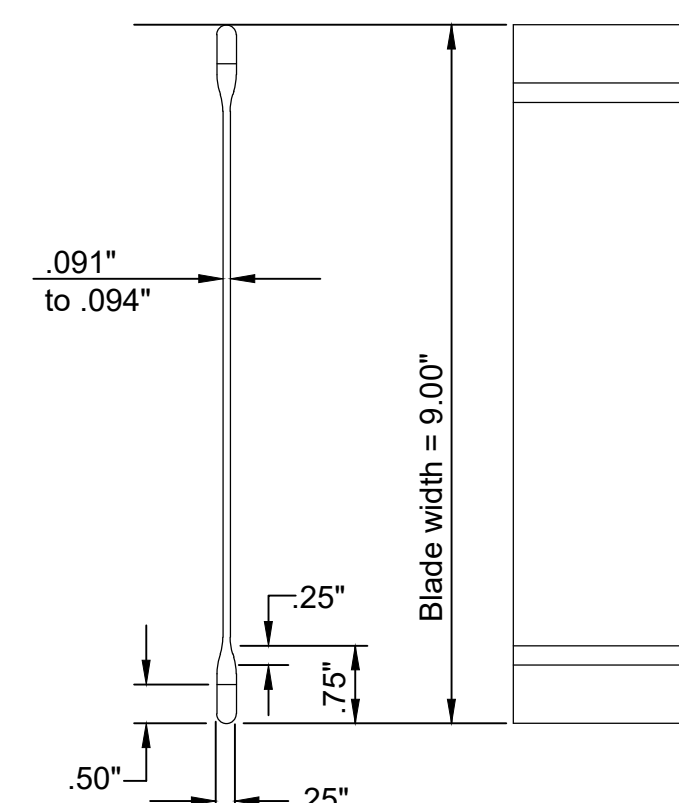
* IN NO CASE SHALL THE SPECIFICATIONS BE LESS THAN REQUIRED BY THE CURRENT MUTCD.



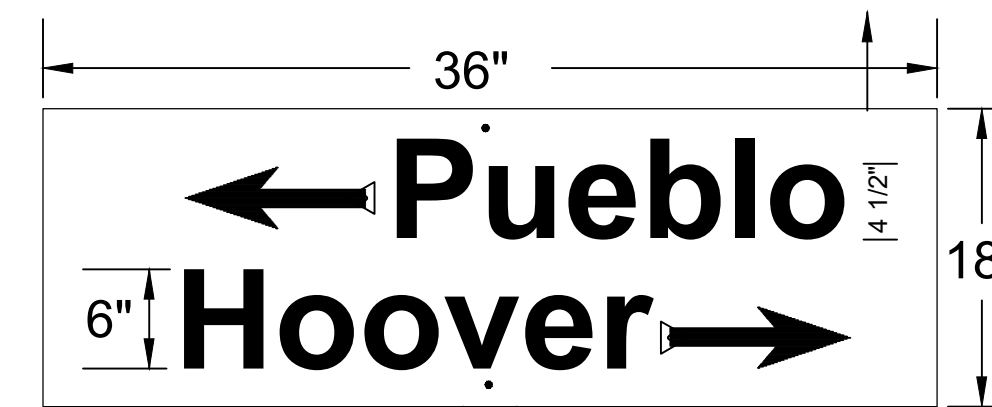
TYPICAL END OF ROADWAY SIGN MOUNTING INSTALLATION



TYPE I BARRICADE DETAIL W/ E.O.R. MARKERS

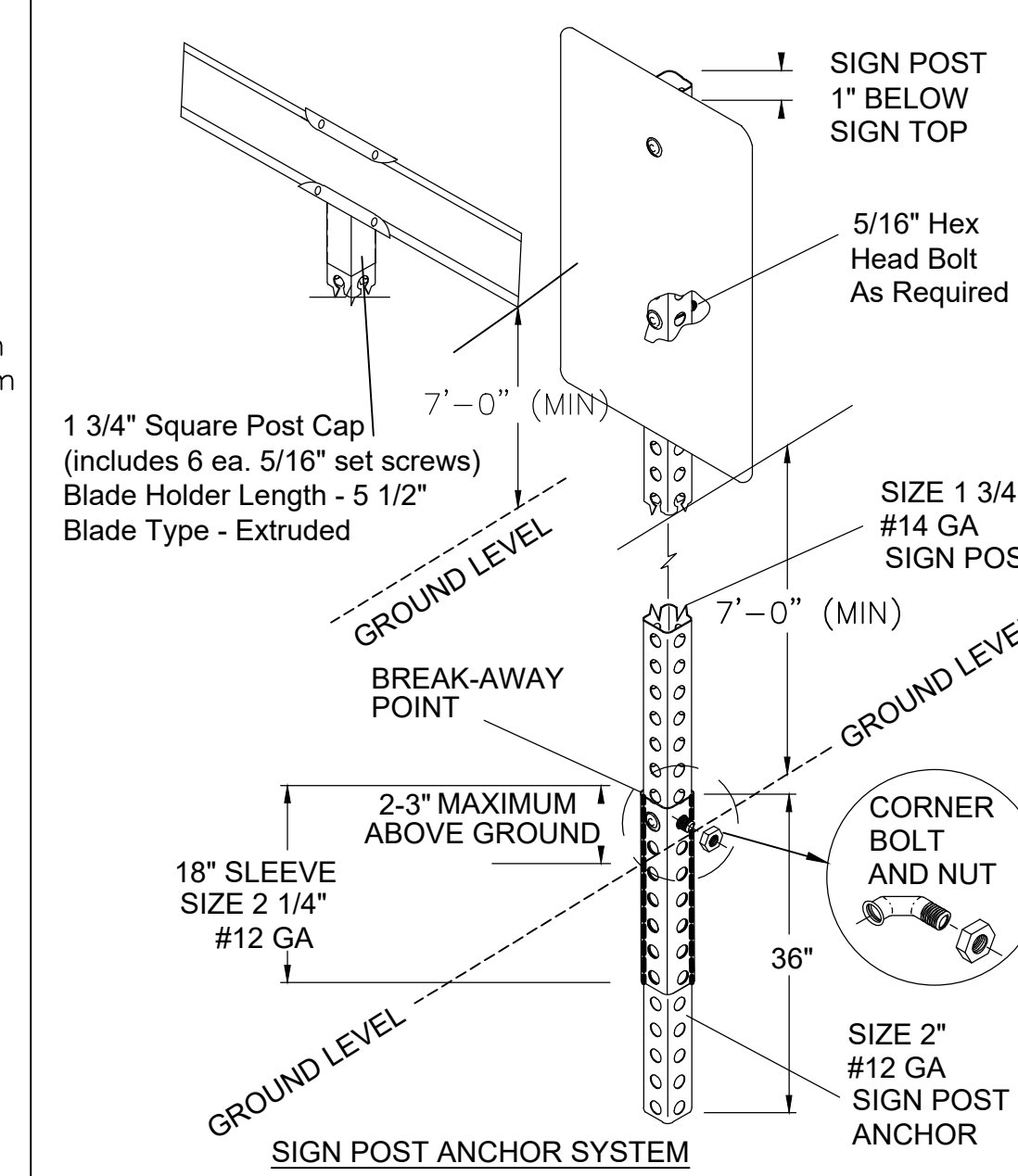


STREET NAME SIGN
BLADE DETAILS



FLAT PLATE STREET NAME SIGN

PERFORATED TUBE SIGN POST INSTALLATION



SIGN ASSEMBLY TABLE				
STREET	STATION	OFFSET	SIGN	QUANTITY
Ike St	0+34	28' Lt.	SNS	1
Ike St	0+51	21' Lt.	R1-1 (Stop)	1
Ike St	0+90	27' Rt.	SNS	1
Ike St	4+69	24' Rt.	SNS	1
Ike St	6+79.40	4' Lt & Rt	EOR	2
TOTAL				6

STREET NAME	NO. BLADES REQ'D	
	9" STD	9" METRO
167th St (S)	1	1
Ike (W)	1	1
Ike	2	1
Ike Ct (-)	1	1
Ike Ct (-)	2	2

NOTE: REFERENCES BELOW TO "STANDARD SPECIFICATIONS" DENOTE "STANDARD SPECIFICATION FOR STATE ROAD AND BRIDGE CONSTRUCTION EDITION 2015" BY THE KANSAS DEPARTMENT OF TRANSPORTATION.

- FABRICATION AND INSTALLATION OF ALL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD.
- POST ANCHORS: POSTS SHALL BE ANCHORED WITH A YIELDING BASE POST SUPPORT AS DETAILED.
- POSTS FOR TRAFFIC CONTROL SIGNS: POSTS SHALL BE GALVANIZED AND CONFORM TO THE REQUIREMENTS OF SUBSECTION 1620 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT ALL POSTS SHALL WEIGH 3 LBS./FT. MINIMUM.
- POSTS FOR END OF ROADWAY SIGN TO BE 8' LONG AND INSTALLED A MINIMUM OF 4' FROM ROADWAY TO BOTTOM OF SIGN.
- SIGN BLANKS FOR TRAFFIC CONTROL SIGNS: SIGN BLANKS SHALL BE FABRICATED FROM 0.080" ALUMINUM ALLOY 6063-T6 CONFORMING TO THE REQUIREMENTS OF SUBSECTION 1627 OF THE STANDARD SPECIFICATIONS.
- SIGN BLADES FOR STREET NAME SIGNS: EXTRUDED ALUMINUM BLADES SHALL BE ALUMINUM ALLOY CONFORMING TO 6063-T6 OR 5052-H38 (ASTM SPECIFICATION B221, LATEST ISSUE). BLADES SHALL HAVE AN ALODINE OR PHOSPHATE ETCHED FINISH. BLADES SHALL HAVE SQUARE CORNERS AND NO HOLES. MINIMUM BLADE LENGTH SHALL BE 24". MAXIMUM BLADE LENGTH SHALL BE 48". LENGTH VARIES BY INCREMENTS OF 6". BLADES BEARING THE STREET NAMES SHALL BE FIRMLY ATTACHED TO THE MOUNTING BRACKETS USING ALLEN-TYPE CONICAL SET SCREWS. THE BLADES SHALL BE ORIENTED PARALLEL TO THE STREET.
- MOUNTING BRACKETS FOR SIGNS: DIE-CAST ALUMINUM BRACKETS SHALL BE ALUMINUM ALLOY 360 HAVING A TENSILE STRENGTH OF 44,000 PSI. THE BRACKETS SHALL BE SMOOTHLY FINISHED FREE OF PITS, BURRS, AND FLAWS. EACH BRACKET SHALL BE TAPPED AND DRILLED FOR 5/16" ZINC-PLATED ALLEN-TYPE SET SCREWS HAVING SELF-LOCKING SAW-TOOTH ENDS.
- FASTENERS: ALL STEEL FASTENERS FOR TRAFFIC CONTROL SIGNS SHALL BE GALVANIZED AND SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 1614 OF THE STANDARD SPECIFICATIONS.
- REFLECTIVE SHEETING: REFLECTIVE SHEETING SHALL BE A MINIMUM OF HIGH INTENSITY PRISMATIC.
- PROCESS INK: ALL PROCESS INK SHALL CONFORM TO THE REQUIREMENTS OF SUBSECTION 2202 OF THE STANDARD SPECIFICATIONS.
- DETAILS - SNS: THE REFLECTIVE SHEETING FOR THE 9" STANDARD SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVER-WHITE #2 COPY WITH 6" UPPER CASE AND 4 1/2" LOWER CASE PRIMARY COPY AND SUFFIX COPY. BOTH SERIES "C". FACES TO TRIM TO A 8 1/2". (SEE DETAIL A.) THE REFLECTIVE SHEETING FOR THE 9" METRO SIZE SNS IS TO BE THE HIGHWAY GREEN BACKGROUND WITH SILVERWHITE #2 COPY WITH 6" UPPER CASE AND 4 1/2" LOWER CASE PRIMARY COPY AND SUFFIX COPY. BOTH SERIES "C". THE CARDINAL DIRECTION CENTERED DIRECTLY BELOW THE BLOCK NUMBER SHALL BE AN UPPER CASE, 3" SERIES "C" LETTER. FACES TO TRIM TO A 8 1/2" WIDTH. (SEE DETAIL B.) FOR CUL-DE-SAC STREETS, A 9" METRO SIZE BLADE SHALL BE USED WITH THE BLOCK NUMBERS DISPLAYED BENEATH THE STREET NAME. IF BLOCK NUMBERS ARE NOT SHOWN ON THE PLANS THE CONTRACTOR SHALL CONTACT THE TRAFFIC ENGINEER AT 268-4501 PRIOR TO MANUFACTURING THE SIGN. SHOP DRAWINGS OF LAYOUT FOR SNS SHALL BE SUBMITTED TO THE TRAFFIC ENGINEERING DIVISION OF THE CITY OF WICHITA FOR APPROVAL PRIOR TO FABRICATION. THE FINISHED SIGNS AS SUPPLIED SHALL BE OF GOOD APPEARANCE, FREE FROM RAGGED EDGES, CRACKS SCALES OR BLISTERS AND SHALL BE CLEAN-CUT. SIGNS SHALL BE PACKED IN SUCH MANNER AS TO PREVENT DAMAGE OR DEFAACEMENT DURING SHIPMENT OR STORAGE.
- PERMANENT TRAFFIC CONTROL AND SNS: PERMANENT TRAFFIC CONTROL AND SNS SHALL BE MEASURED AND PAID FOR AT THE LUMP SUM PRICE FOR SIGNING. THE PAYMENT AS SET FORTH ABOVE SHALL BE CONSIDERED FULL COMPENSATION FOR ALL EXCAVATION, BACKFILLING, POSTS, ANCHORS, FASTENERS, MATERIALS, LABOR, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK.

REVISED: December 2018 | TM | Updated Edition year and Subsection Number in Notes

SIGN DETAILS

TRAFFIC ENGINEER
Mike Armour, P.E.

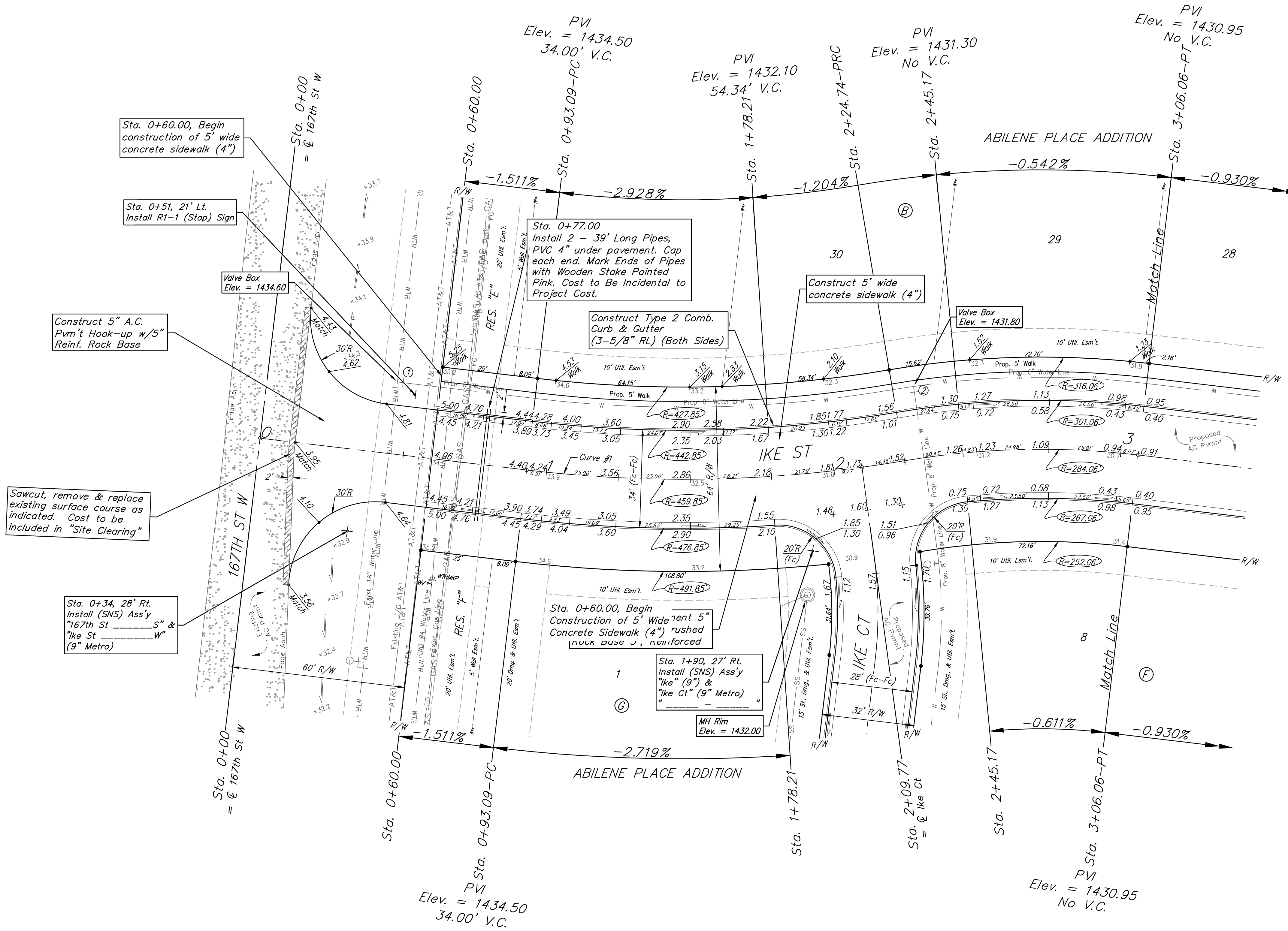
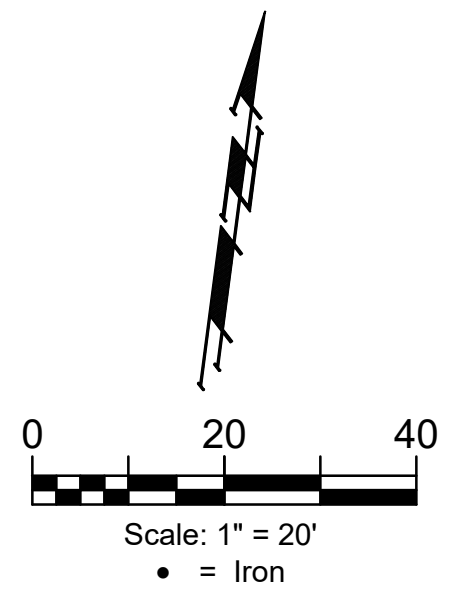
PROJECT NUMBER	OCA NUMBER	DATE
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CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET
4 of 20

BENCHMARK #1:
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Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



Construct 5" A.C. Pvm't Hook-up w/5" Reinf. Rock Base

Sawcut, remove & replace existing surface course as indicated. Cost to be included in "Site Clearing"

Sta. 0+34, 28' Rt. Install (SNS) Ass'y "Ike St" (9" W" & "Ike Ct" (9" Metro)

Sta. 0+60.00, Begin construction of 5' wide concrete sidewalk (4")

Sta. 0+51, 21' Lt. Install R1-1 (Stop) Sign

Valve Box Elev. = 1434.60

Sta. 0+77.00 Install 2 - 39" Long Pipes, PVC 4" under pavement. Cap each end. Mark Ends of Pipes with Wooden Stake Painted Pink. Cost to be Incidental to Project Cost.

Construct Type 2 Comb. Curb & Gutter (3-5/8" RL) (Both Sides)

Construct 5' wide concrete sidewalk (4")

Valve Box Elev. = 1431.80

Sta. 0+60.00, Begin Construction of 5' Wide rashed Concrete Sidewalk (4") rashed rock base, reinforced

Sta. 1+90, 27' Rt. Install (SNS) Ass'y "Ike" (9") & "Ike Ct" (9" Metro)

MH Rim Elev. = 1432.00

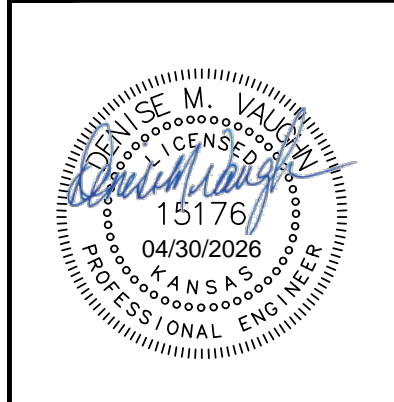
Curve #1
Curve Data Based on Centerline
Rad. = 459.85' Delta = 162°41'0" Tangent = 66.28'
Arc = 131.65' L.C. = 131.20' Def./Ft. = 3.73781 Min.

Station	Face Chord Lengths		Defl.	Total Defl.
	8' Left	8' Right		
0+93.09	-	-	0'00"00"	0'00"00"
1+00.00	6.91'	6.53'	7.29'	0'25"50"
1+25.00	25.00'	23.64'	26.36'	1'33"26"
1+50.00	25.00'	23.64'	26.36'	1'33"27"
1+75.00	25.00'	23.64'	26.36'	1'33"27"
1+78.21	3.21'	3.04'	3.38'	0'12"00"
2+00.00	21.79'	20.60'	22.97'	1'21"27"
2+24.74	24.74'	23.39'	26.08'	1'32"28"

Curve #2
Curve Data Based on Centerline
Rad. = 284.06' Delta = 162°41'0" Tangent = 40.94'
Arc = 81.32' L.C. = 81.04' Def./Ft. = 6.05120 Min.

Station	Face Chord Lengths		Defl.	Total Defl.
	8' Left	8' Right		
2+24.74	-	-	0'00"00"	0'00"00"
2+25.00	0.26'	0.28'	0.24'	0'01"34"
2+45.17	20.17'	21.94'	18.39'	2'02"04"
2+50.00	4.83'	5.26'	4.40'	0'29"13"
2+75.00	25.00'	27.19'	22.79'	2'31"17"
3+00.00	25.00'	27.19'	22.79'	2'31"17"
3+06.06	6.06'	6.59'	5.53'	0'36"40"

Roll type curb & gutter to be constructed on the pavement on this sheet.
Top of curb elevation are given for full height curb.



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ABILENE PLACE ADDITION
PHASE II

IKE STREET

Sta. 0+00 to Sta. 3+06.06
Street Paving Improvements

PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

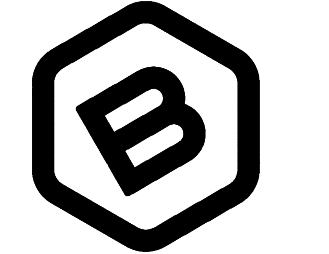
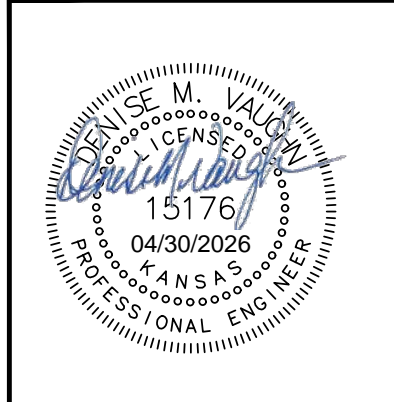
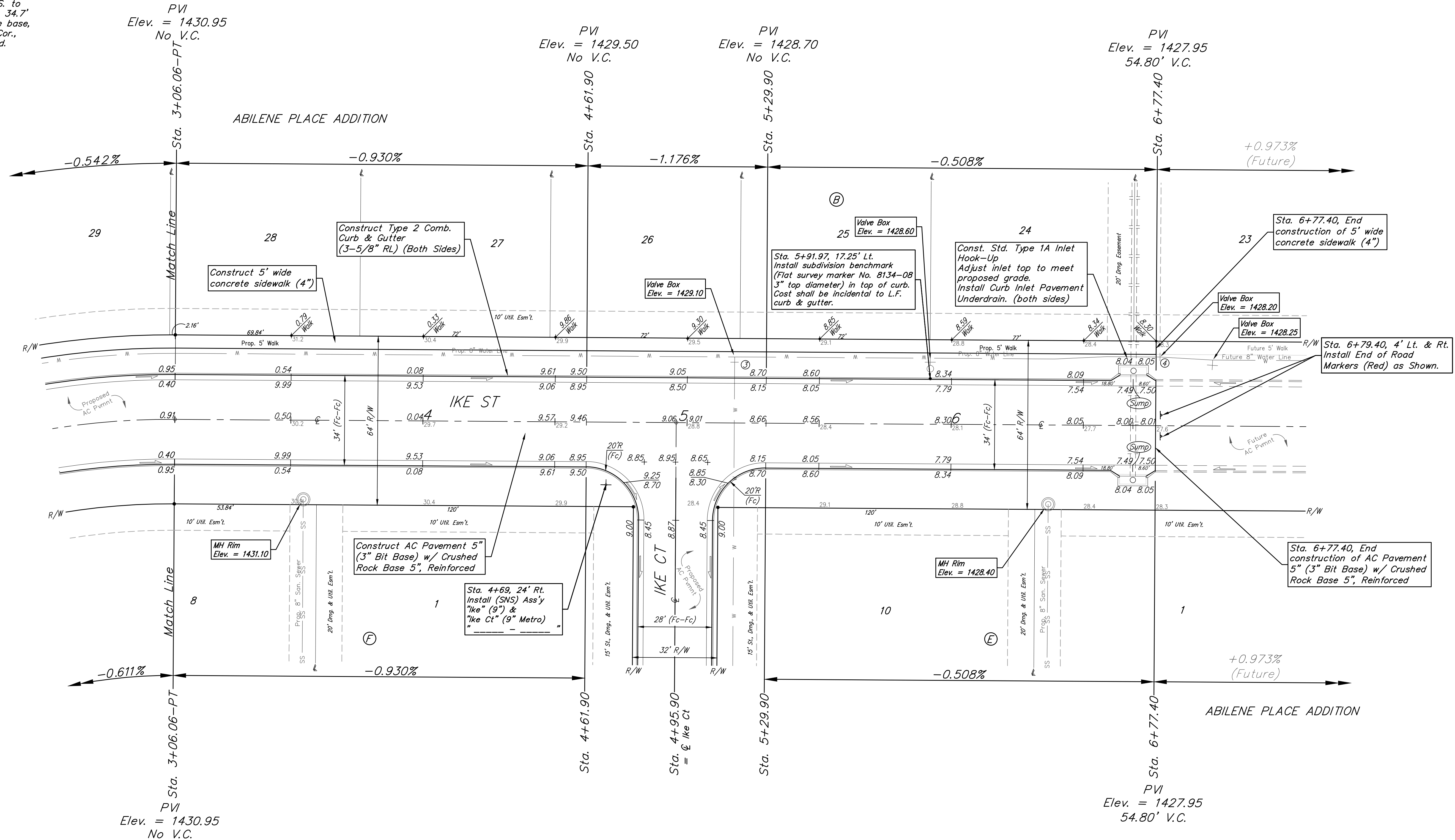
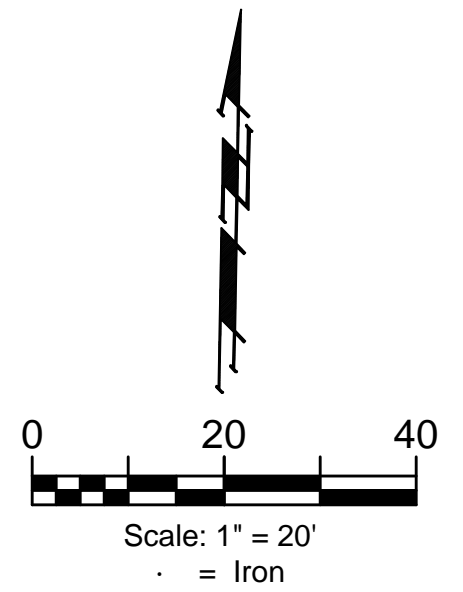
DATE: Apr. 28, 2026

SHEET **5** OF **20**

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

BENCHMARK #1:
Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

BENCHMARK #2:
Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



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ABILENE PLACE ADDITION
PHASE II

IKE STREET

Sta. 3+06.06 to Sta. 6+77.40
Street Paving Improvements

PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

DATE: Apr. 28, 2026

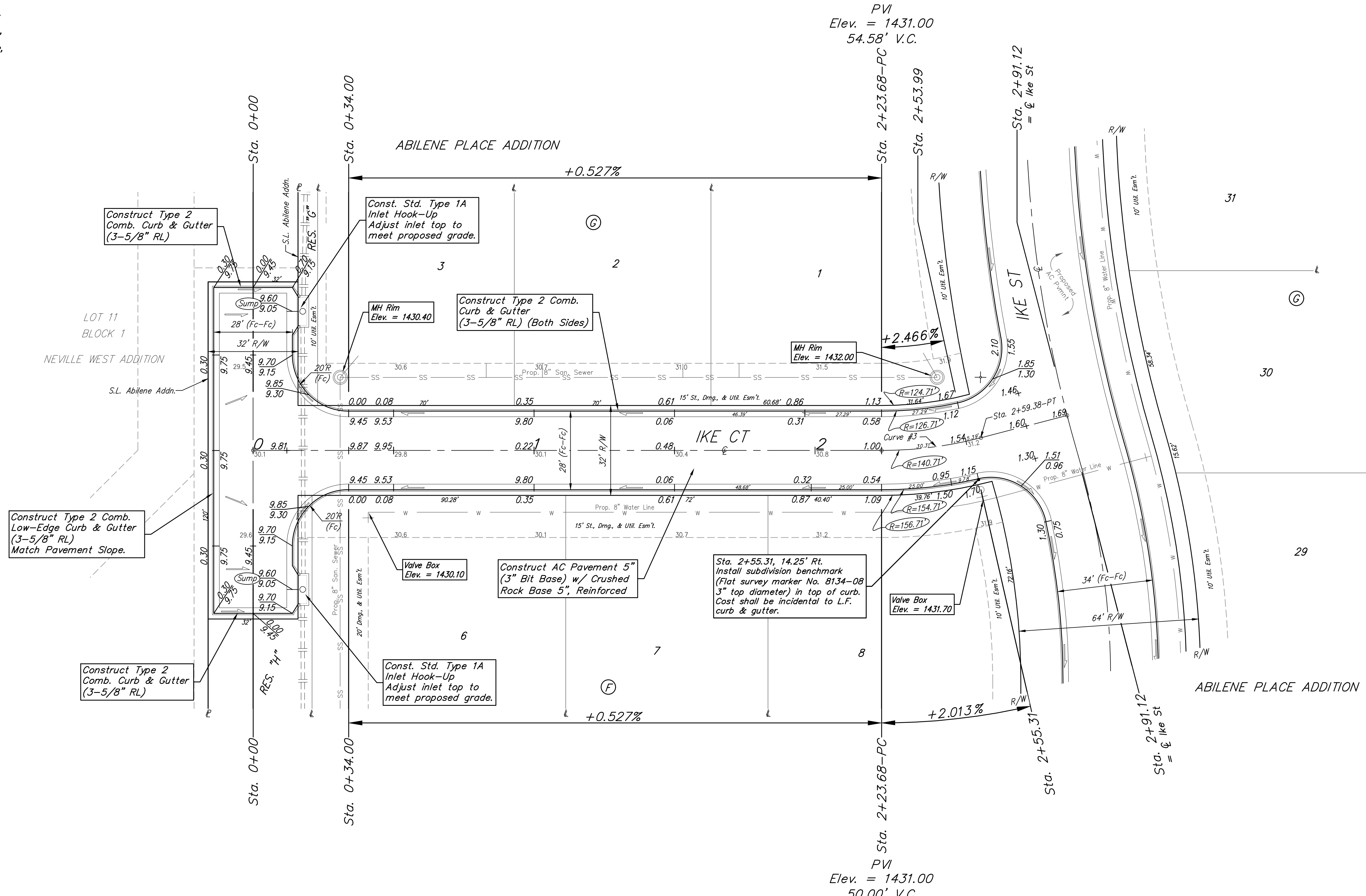
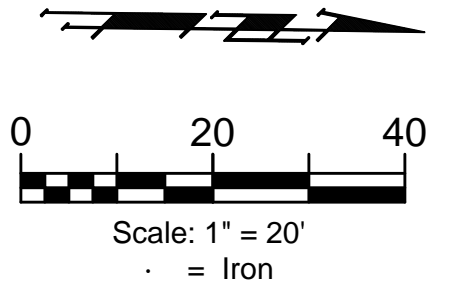
SHEET 6 OF 20

Roll type curb & gutter to be constructed on the pavement on this sheet.
Top of curb elevation are given for full height curb.

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

BENCHMARK #1:
Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

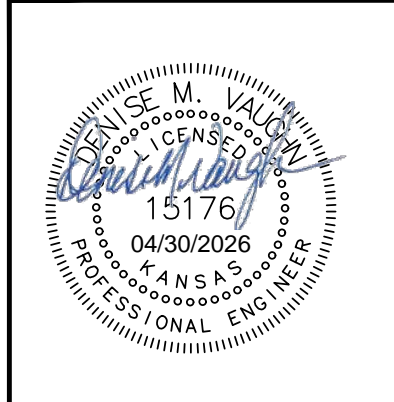
BENCHMARK #2:
Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



Curve #3
Curve Data Based on Centerline
Rad. = 140.71' Delta = 14°32'19" Tangent = 17.95'
Arc = 35.70' L.C. = 35.61' Def./Ft. = 12.21732 Min.

Station	Arc	Face Chord Lengths		Defl.	Total Defl.
		8' Left	8' Right		
2+23.68	-	-	-	0'00"00"	0'00"00"
2+25.00	1.32'	1.11'	1.53'	0'16"08"	0'16"08"
2+50.00	25.00'	21.07'	28.87'	5'05"26"	5'21"34"
2+53.99	3.99'	3.37'	4.61'	0'48"44"	6'10"18"
2+55.31	1.32'	1.11'	1.53'	0'16"08"	6'26"26"
2+59.38	4.07'	3.43'	4.71'	0'49"43"	7'16"09"

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ABILENE PLACE ADDITION
PHASE II

IKE CT

Sta. 0+00 to Sta. 2+55.31
Street Paving Improvements

PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

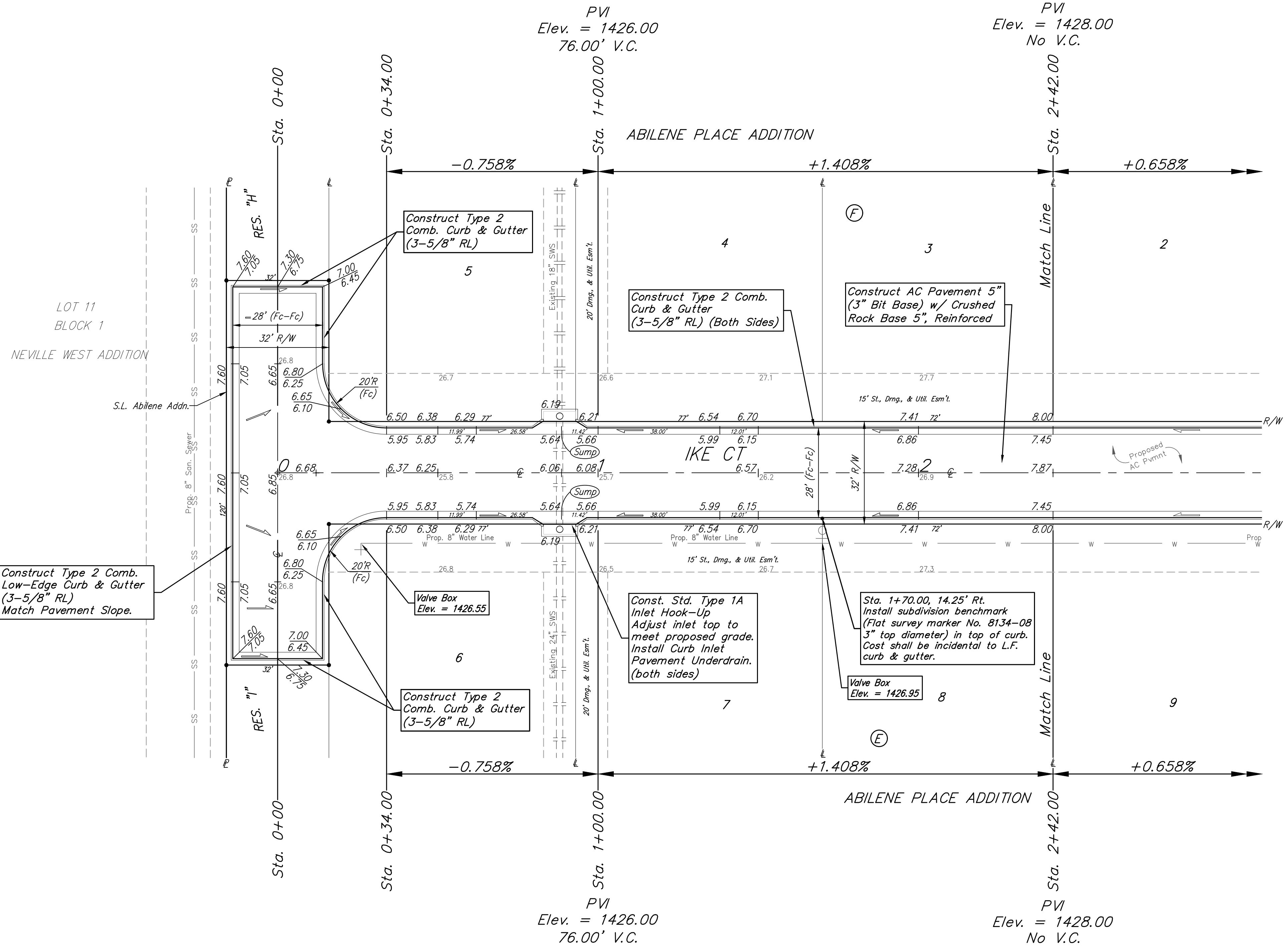
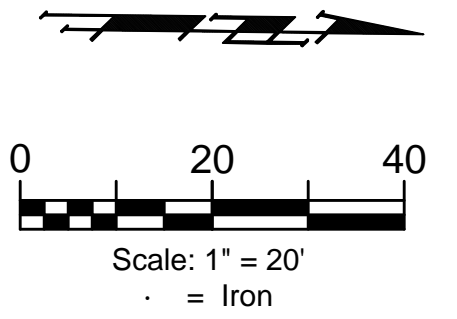
DATE: Apr. 28, 2026

SHEET 7 OF 20

File: E:\Projects\Abilene Place Addition\20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

BENCHMARK #1:
 Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

BENCHMARK #2:
 Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



LOT 11
 BLOCK 1
 NEVILLE WEST ADDITION

Construct Type 2 Comb. Low-Edge Curb & Gutter (3-5/8" RL) Match Pavement Slope.

Construct Type 2 Comb. Curb & Gutter (3-5/8" RL)

Construct Type 2 Comb. Curb & Gutter (3-5/8" RL) (Both Sides)

Construct AC Pavement 5" (3" Bit Base) w/ Crushed Rock Base 5", Reinforced

Const. Std. Type 1A Inlet Hook-Up Adjust inlet top to meet proposed grade. Install Curb Inlet Pavement Underdrain. (both sides)

Sta. 1+70.00, 14.25' Rt. Install subdivision benchmark (Flat survey marker No. 8134-08 3" top diameter) in top of curb. Cost shall be incidental to L.F. curb & gutter.

Construct Type 2 Comb. Curb & Gutter (3-5/8" RL)

Roll type curb & gutter to be constructed on the pavement on this sheet.
 Top of curb elevation are given for full height curb.

Professional Engineer Seal for Denise M. Valdez, License No. 15176, dated 04/30/2026, State of Kansas.

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ABILENE PLACE ADDITION
 PHASE II

IKE CT

Sta. 0+00 to Sta. 2+42.00
 Street Paving Improvements

PROJECT NUMBER:

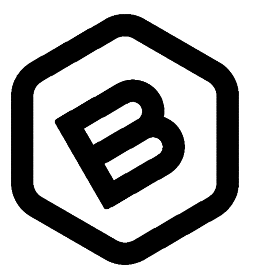
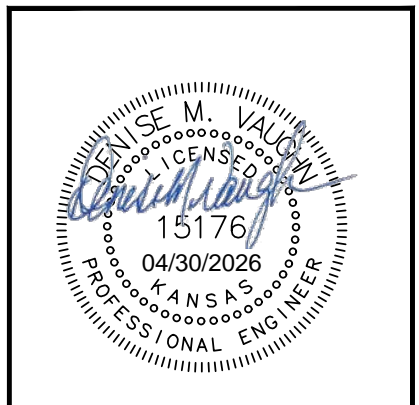
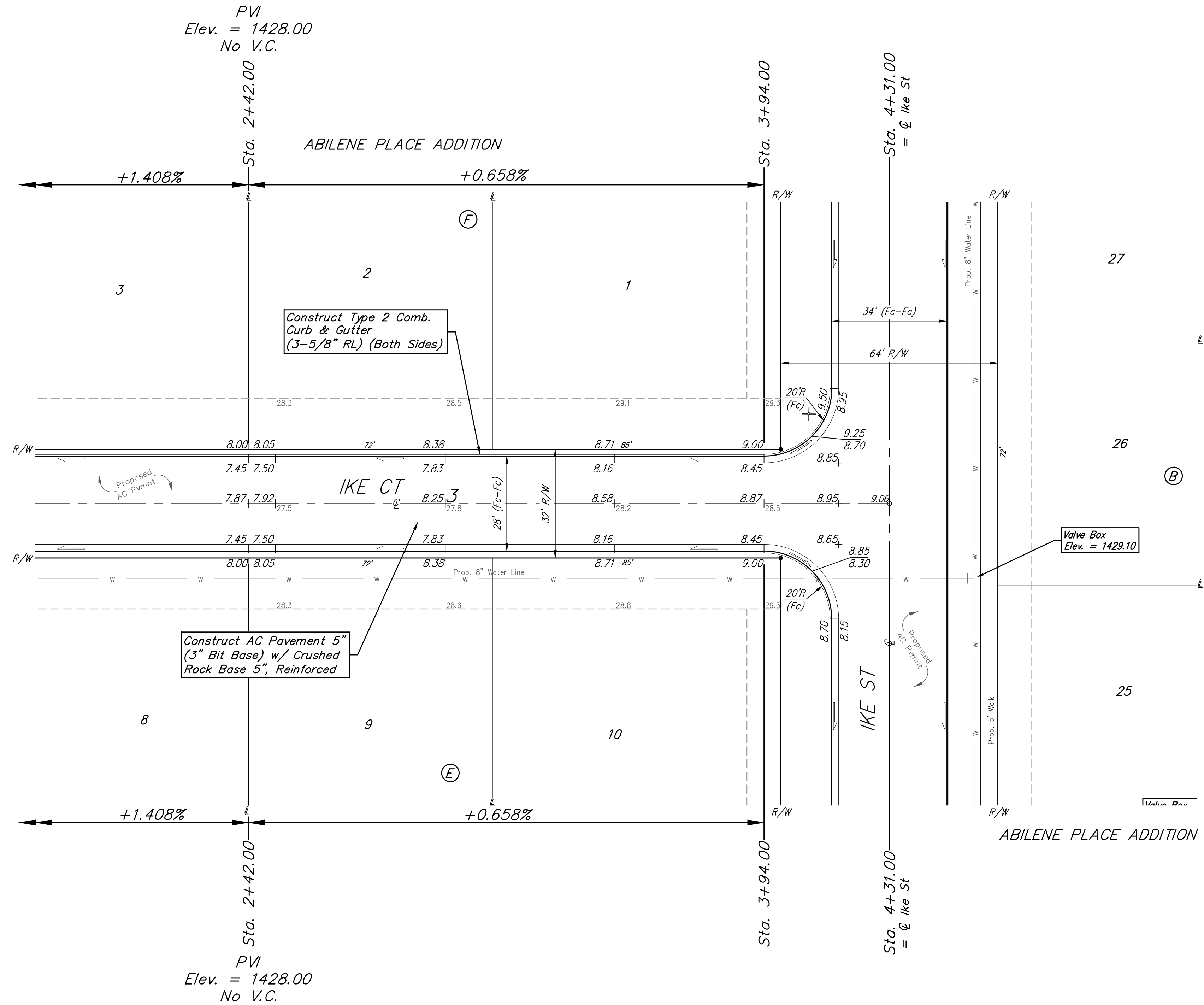
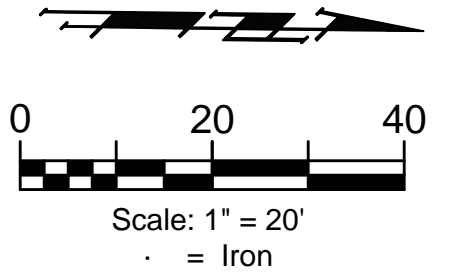
DESIGN: DMV DRAWN: JAK
 DATE: Apr. 28, 2026

SHEET 8 OF 20

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

BENCHMARK #1:
 Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

BENCHMARK #2:
 Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



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ABILENE PLACE ADDITION
 PHASE II

IKE CT

Sta. 2+42.00 to Sta. 3+94.00
 Street Paving Improvements

PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

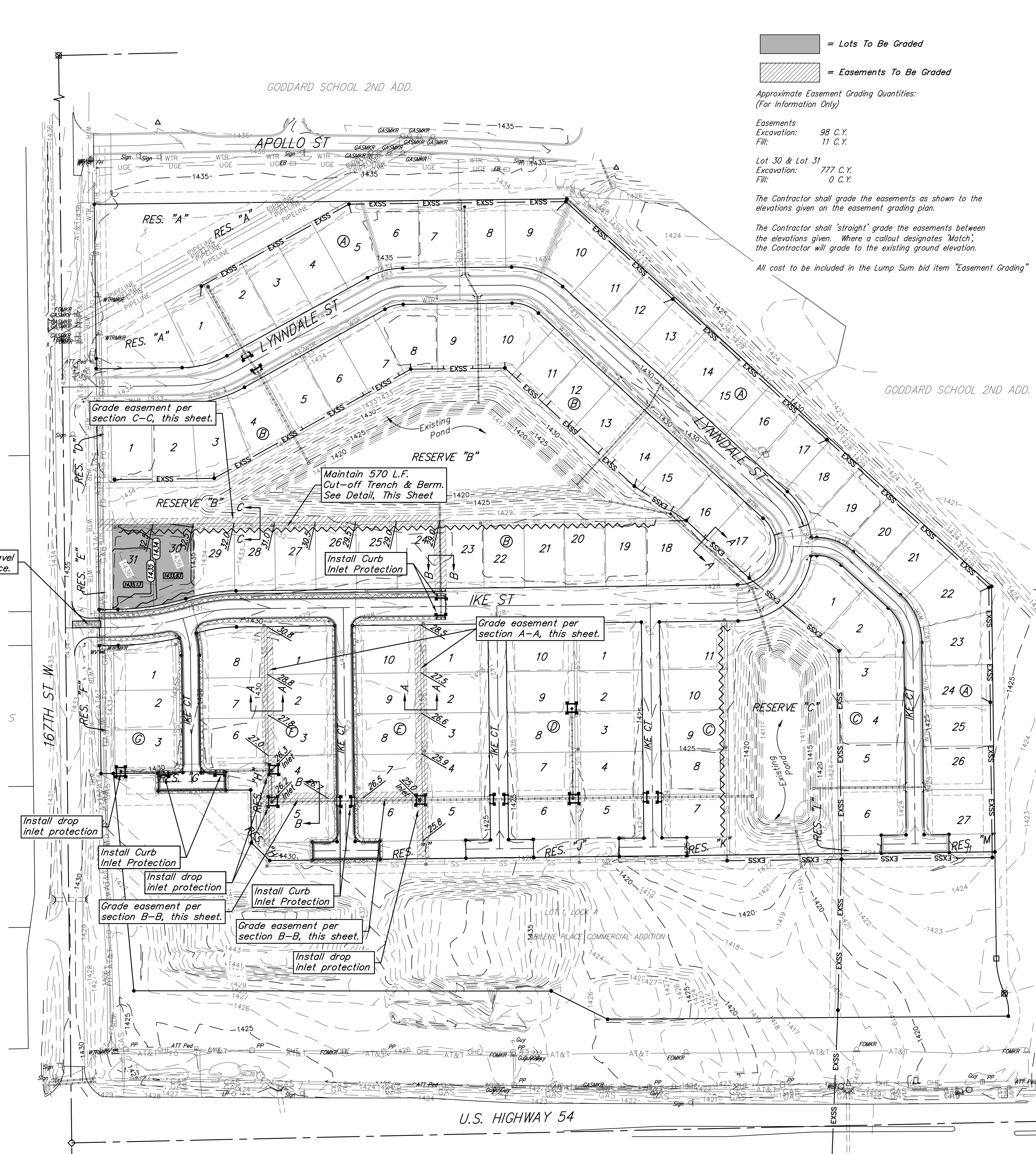
DATE: Apr. 28, 2026

SHEET 9 OF 20

Roll type curb & gutter to be constructed on the pavement on this sheet.
 Top of curb elevation are given for full height curb.

BENCHMARK #1:
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= Lots To Be Graded
 = Easements To Be Graded

Approximate Easement Grading Quantities:
(For Information Only)

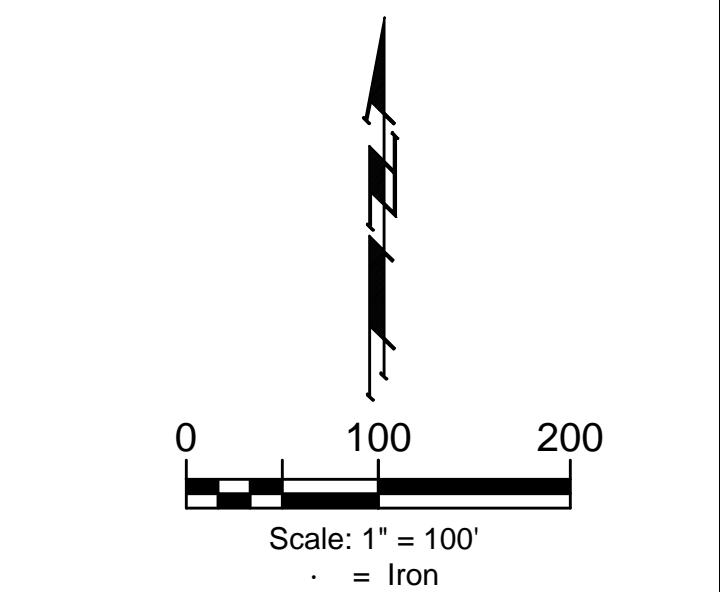
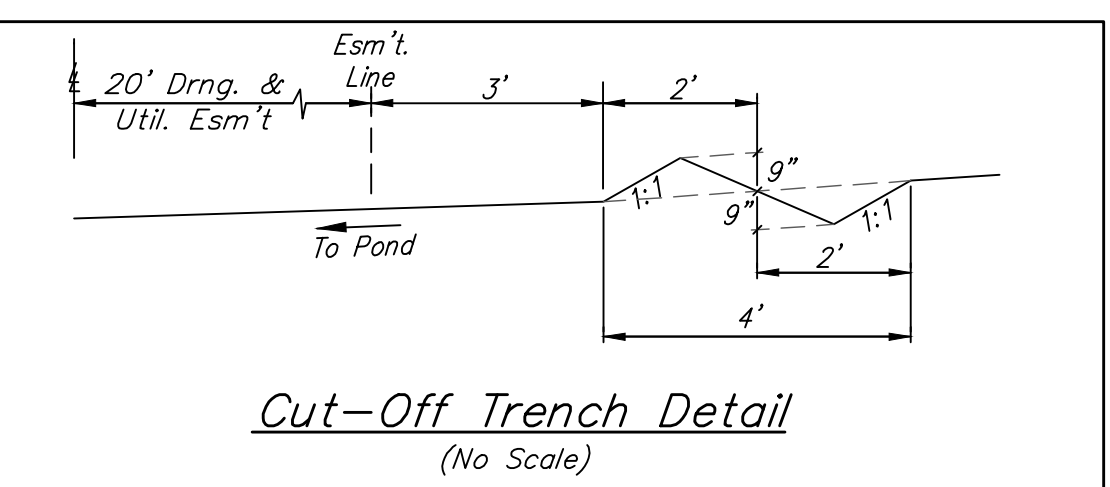
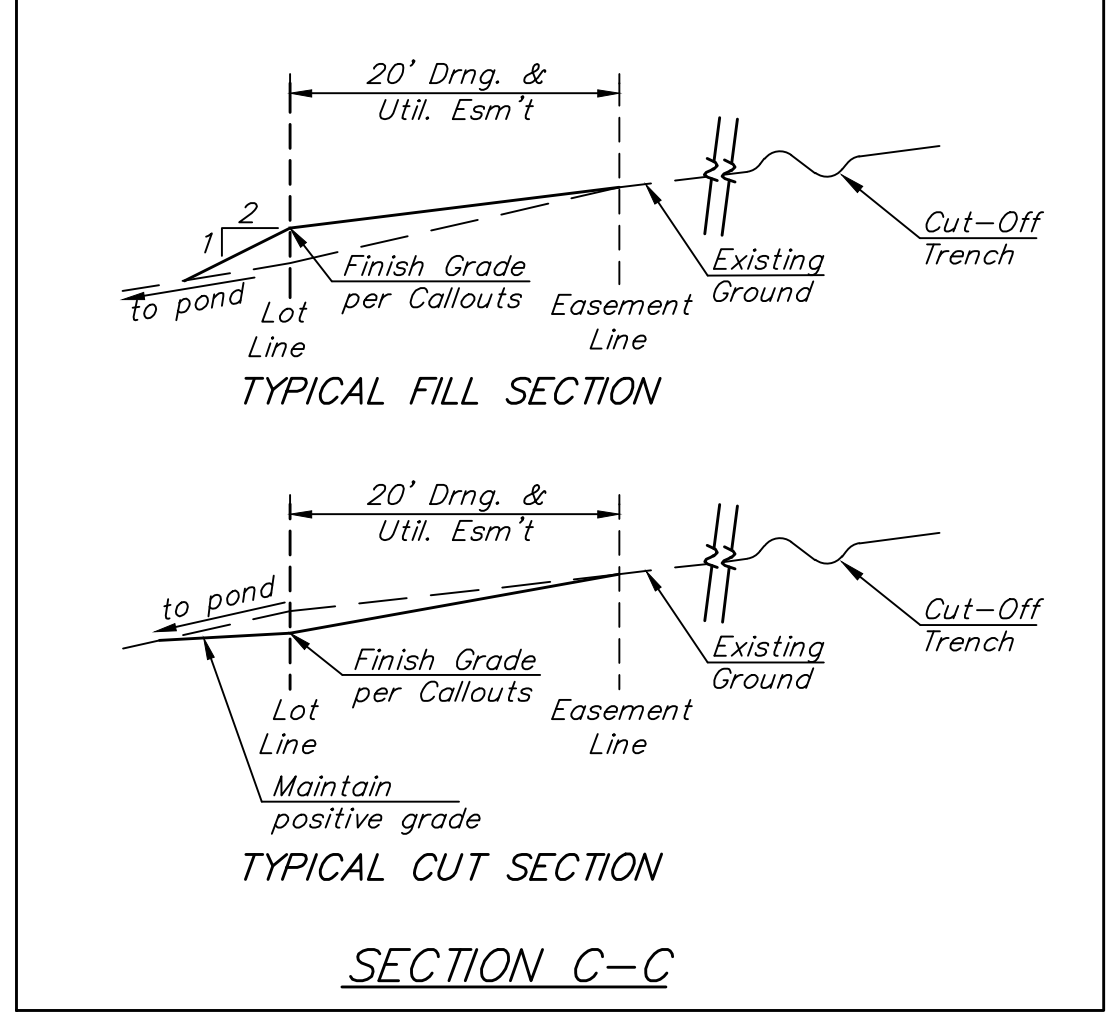
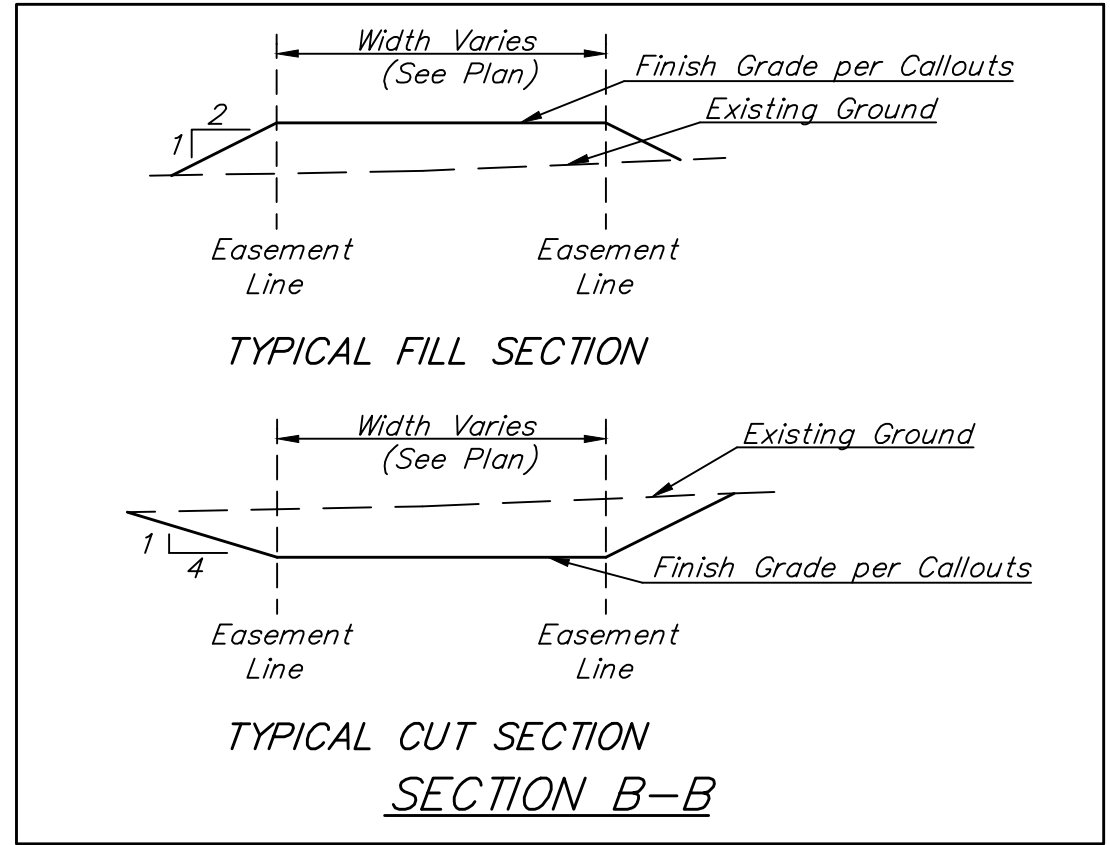
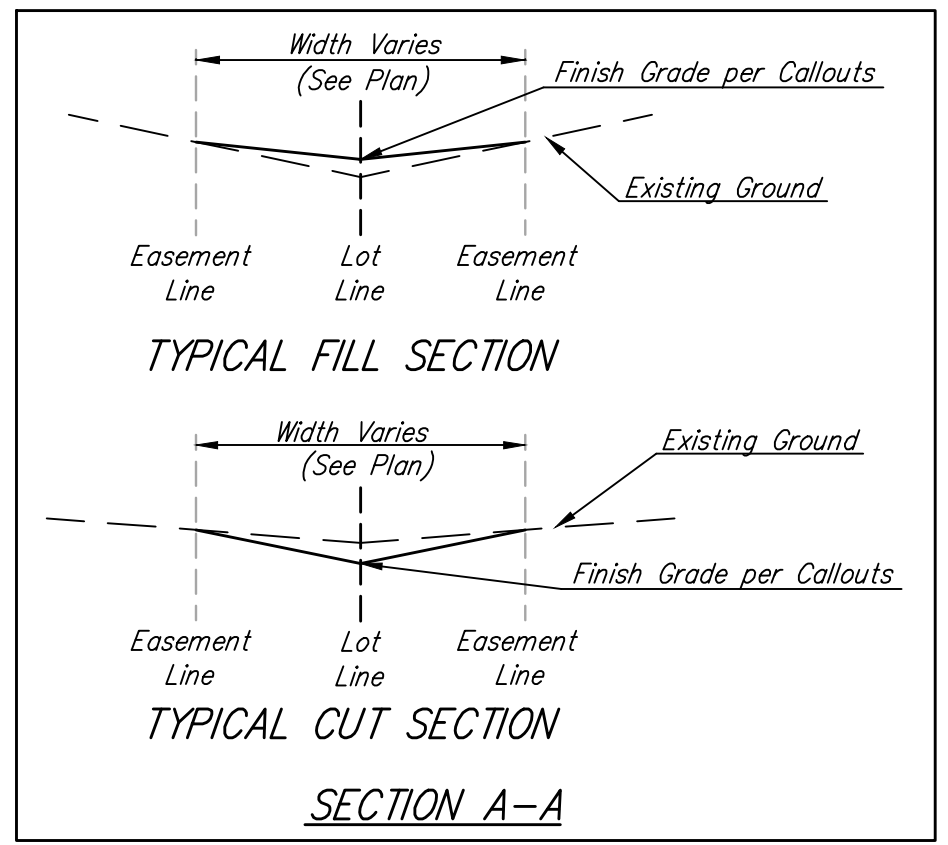
Easements
Excavation: 98 C.Y.
Fill: 11 C.Y.

Lot 30 & Lot 31
Excavation: 777 C.Y.
Fill: 0 C.Y.

The Contractor shall grade the easements as shown to the elevations given on the easement grading plan.

The Contractor shall 'straight' grade the easements between the elevations given. Where a callout designates 'Match', the Contractor will grade to the existing ground elevation.

All cost to be included in the Lump Sum bid item "Easement Grading"



EROSION CONTROL LEGEND

- DROP INLET PROTECTION
- CURB INLET PROTECTION
- CUT-OFF TRENCH
- BACK OF CURB PROTECTION
- SILT FENCING

- NOTES:**
- Contractor shall make sure all erosion control is in place before project is accepted. This plan represents the minimum standard. Any additional erosion control measures shall be installed by the Contractor as needed.
 - Removal of Gravel Construction Entrance for pavement construction shall be included in bid item "Site Restoration"
 - All areas disturbed during construction shall be seeded, mulched, and fertilized as per Cover Sheet General Notes.

EROSION CONTROL MEASURE	INSTALL	MAINTAIN
BACK OF CURB PROTECTION (LF)	2,952	0
CONSTRUCTION ENTRANCE (EA)	0	1
CURB INLET BARRIER (EA)	6	0
DROP INLET PROTECTION (EA)	4	0
MAINTAIN EROSION CONTROL BMP'S (LS)	0	1

* ALL EXISTING BMP'S INCLUDING CONSTRUCTION ENTRANCE, SEDIMENT BARRIERS, SILT FENCE, CUT-OFF TRENCH, AND EROSION CONTROL MAT SHALL BE MAINTAINED AND REPAIRED IF NECESSARY.



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ABILENE PLACE ADDITION
PHASE II

EROSION CONTROL

Sta. 0+59.06 to Sta. 3+34.42
Street Paving Improvements

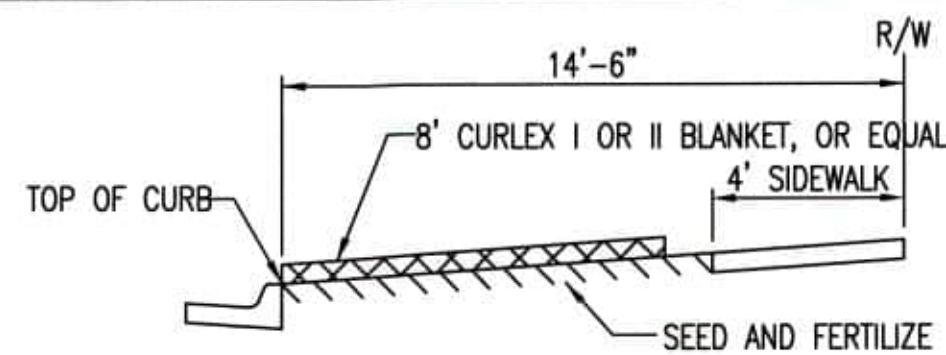
PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

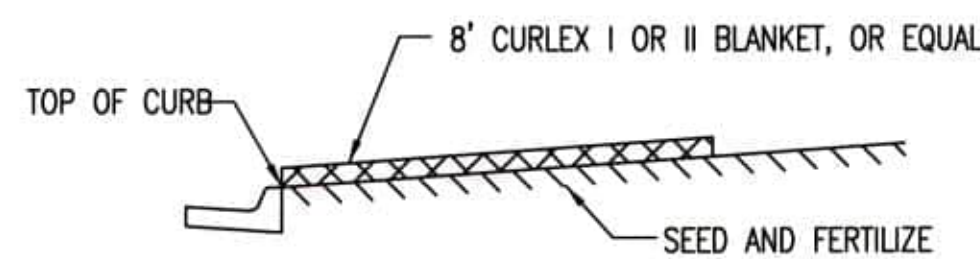
DATE: Apr. 28, 2026

SHEET OF
10 20

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

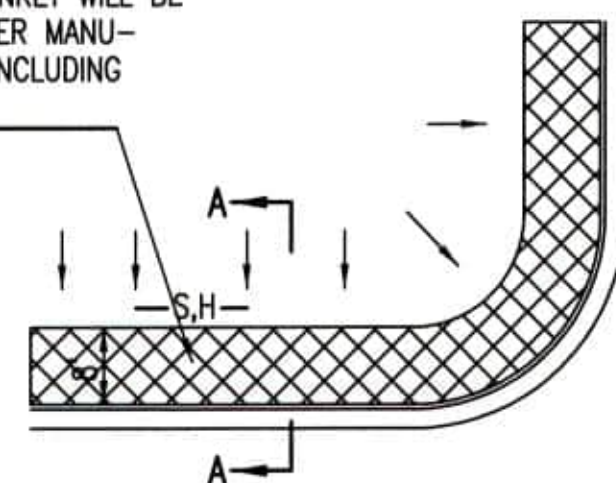


SECTION B-B

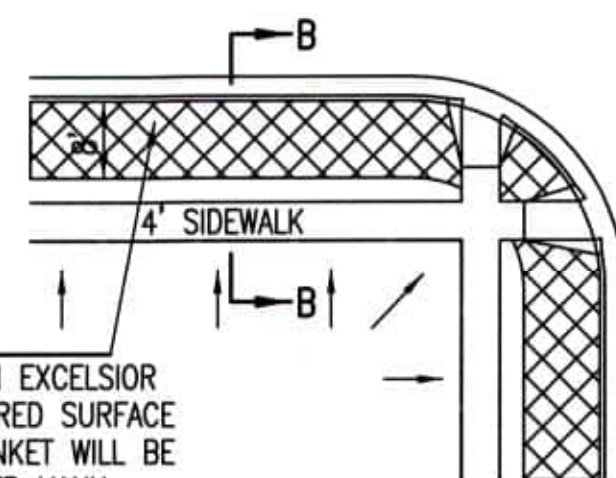


SECTION A-A

INSTALL 8' WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURERS RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)



SOUTH STREET

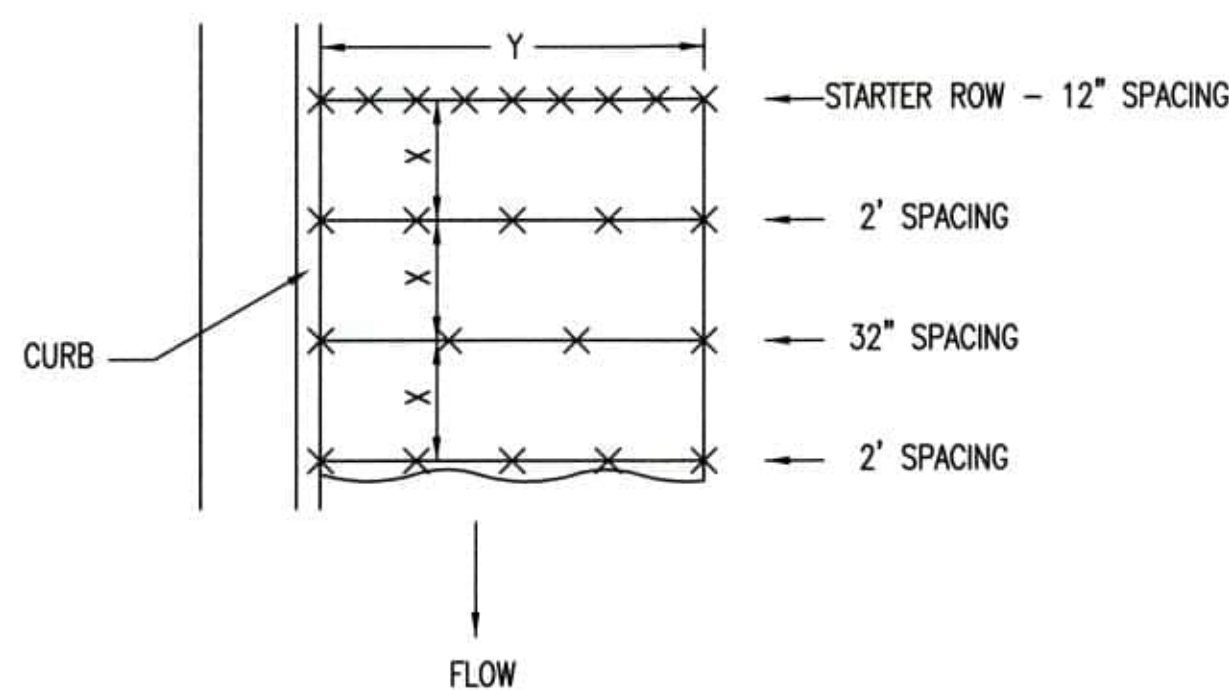


INSTALL 8' WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURERS RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)

GENERAL NOTES

- EXCELSIOR MAT TO BE INSTALLED WHEN SOD IS NOT SPECIFIED ON PROJECT.
- EXCELSIOR BLANKET TO BE INSTALLED OVER SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS.
- AFTER INSTALLATION OF EXCELSIOR BLANKET, AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB AND INTO THE GUTTER, SUPPLEMENTAL EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS NEEDED, TO FIX THE PROBLEM.

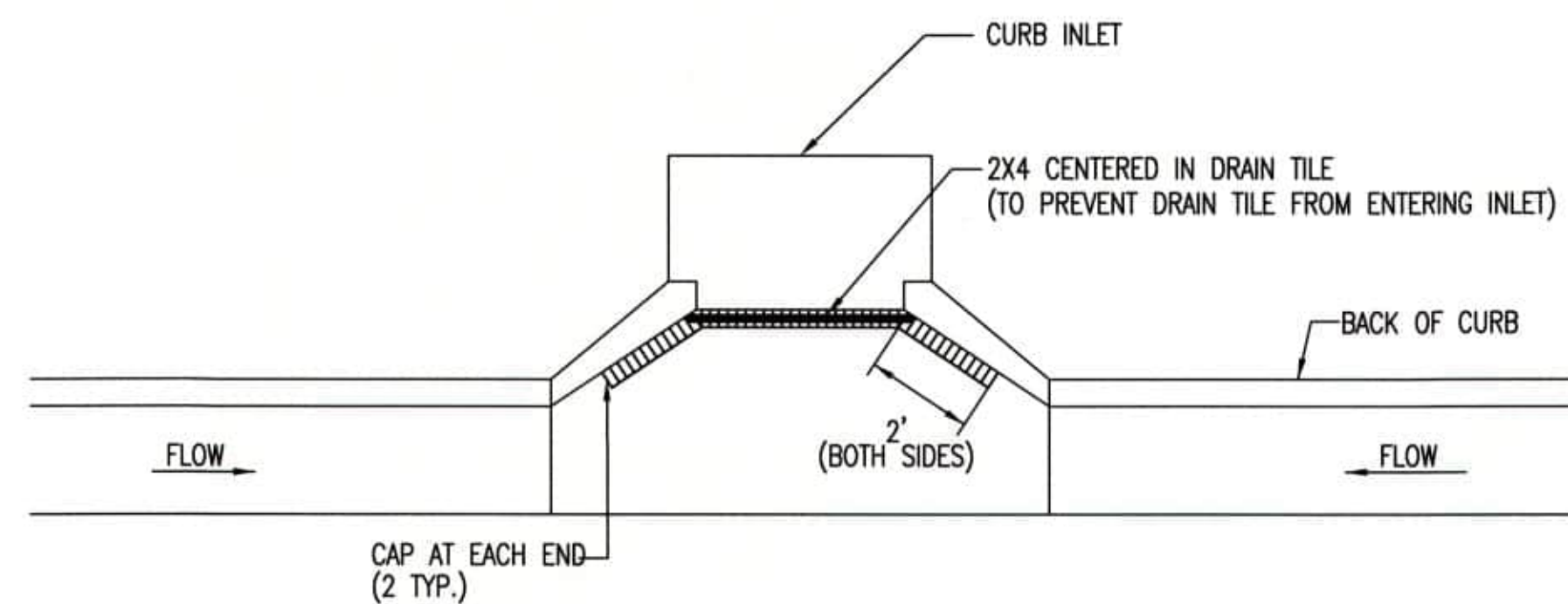
BACK OF CURB PROTECTION DETAIL



STAPLE PATTERN

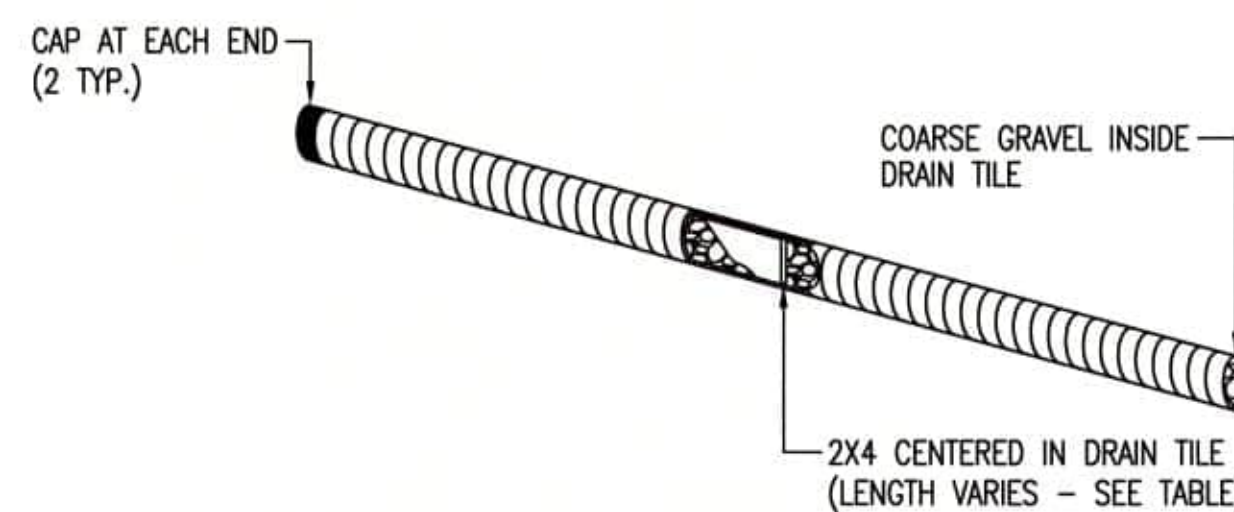
NOTES: USE 6" SEAM OVERLAP
(X & Y = RECOMMENDED BY MANUFACTURE)

DETAILS FOR APPROVED EROSION CONTROL MAT

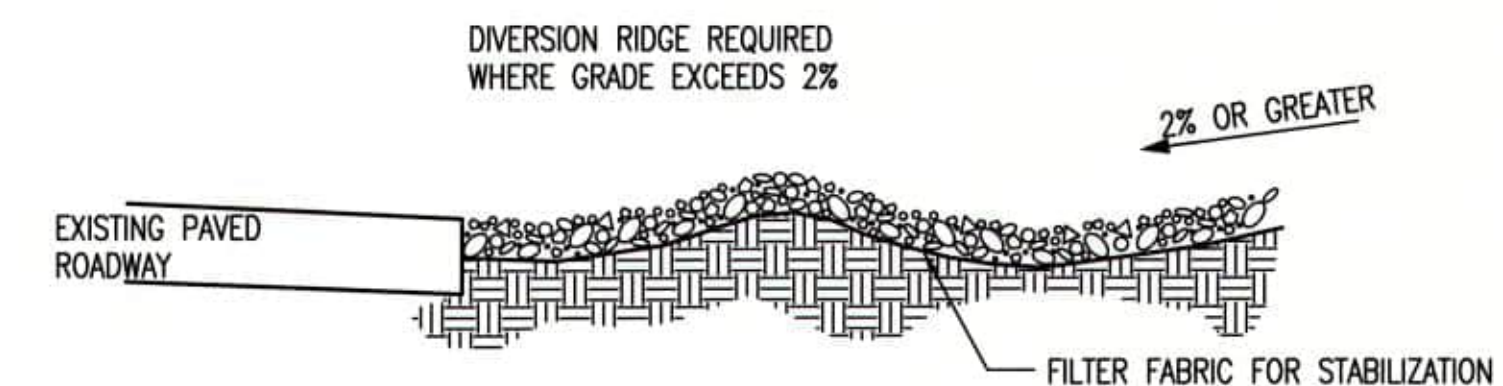


NOTE: PLACE 4" PERFORATED PVC PIPE, FILLED WITH 1/2"-1" DIA. GRAVEL, IN FRONT OF CURB INLET AS SHOWN.

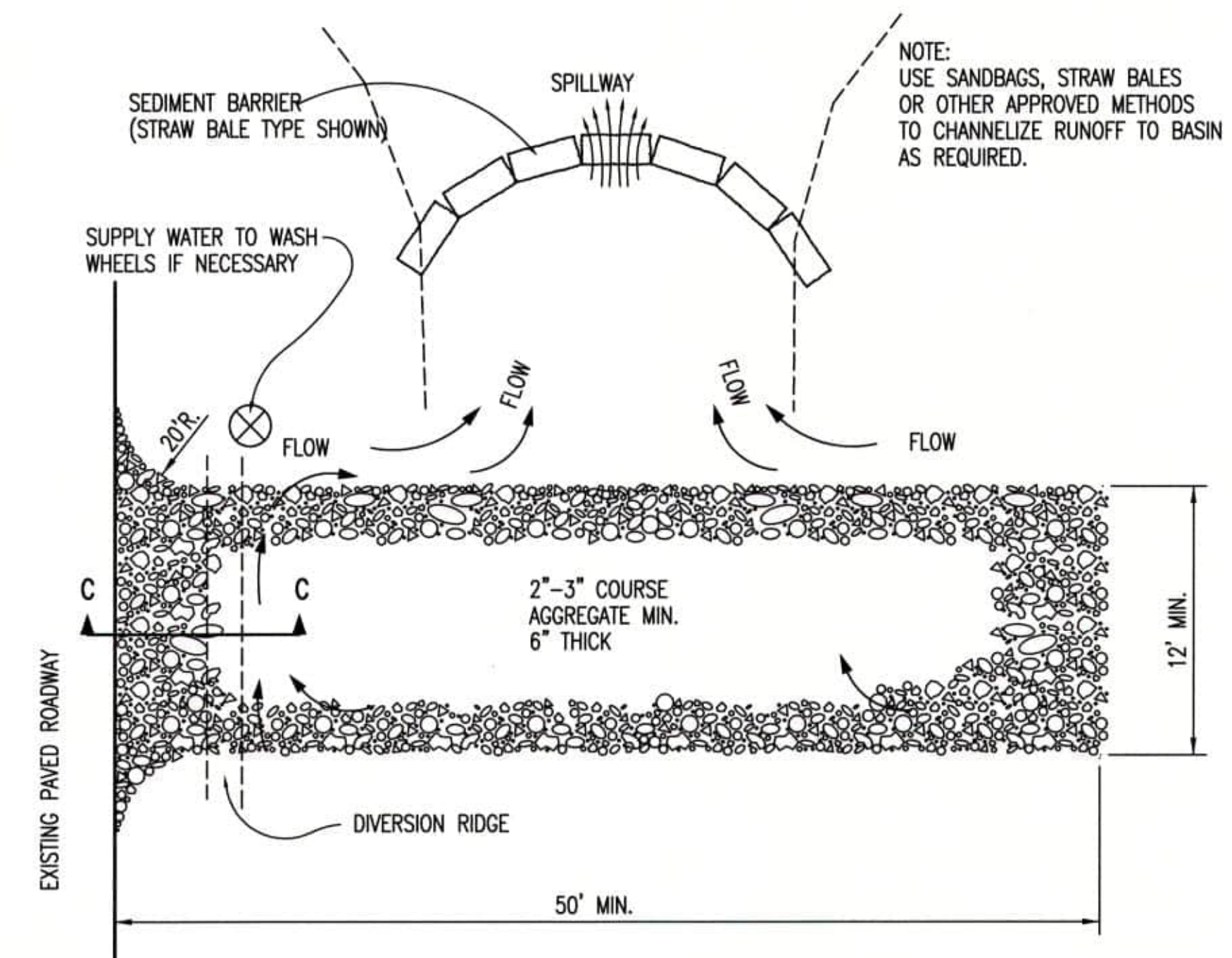
2X4 LENGTH	INLET TYPE	INLET OPENING
5'-6"	1-A	5'-0"
10'-6"	1-A	10'-0"
15'-6"	1-A	15'-0"



CURB INLET PROTECTION
4" PERFORATED PIPE W/ GRAVEL



SECTION C-C



STABILIZED CONSTRUCTION ENTRANCE

GENERAL NOTES

- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
- WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN, AS SHOWN ABOVE.
- DRIVE ENTRANCES ONTO RESIDENTIAL LOTS WILL NOT BE REQUIRED TO HAVE THE SEDIMENT BARRIER SHOWN, BUT WHEEL WASHING MAY BE REQUIRED IF STABILIZED ENTRANCE IS NOT SUFFICIENT TO KEEP MUD FROM BEING TRACKED ONTO ADJACENT STREET. ENTRANCE SHALL EXTEND FROM BACK OF CURB TO DWELLING.

REVISION DATE: MAY 2013



05/30/13

CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

BACK OF CURB PROTECTION,
CURB INLET PROTECTION AND
CONSTRUCTION ENTRANCE

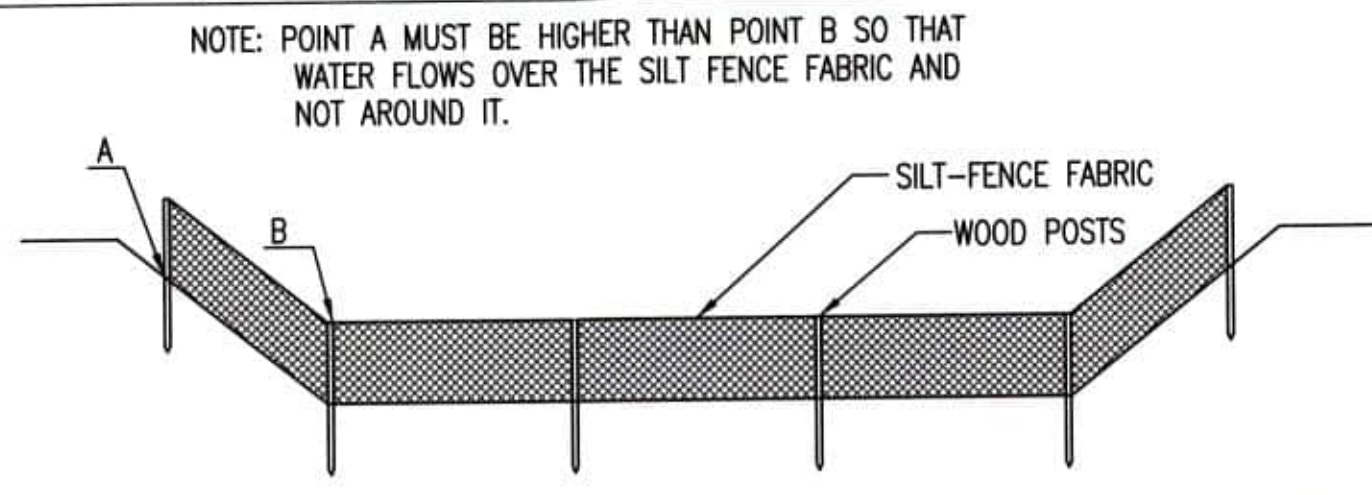
CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	OCA NUMBER	DATE
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CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET

11 of 20



ELEVATION
SILT FENCE DITCH CHECKS
(STREAM PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK, NOT OVER IT. SILT FENCE DITCH CHECKS OFTEN FAIL WHEN OVERTOPPED. SILT FENCE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE SILT FENCE SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE TOP OF THE LOW POINT OF THE FENCE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. SILT FENCE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. SILT FENCE SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.

THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK DITCH GRADE (%)	SPACING CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS AT LEAST 12" DEEP BY 6" WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE OF THE TRENCH. LINE TWO SIDES OF THE TRENCH WITH THE FABRIC AS SHOWN ON DETAIL. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE ON THE UPSLOPE SIDE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 24". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

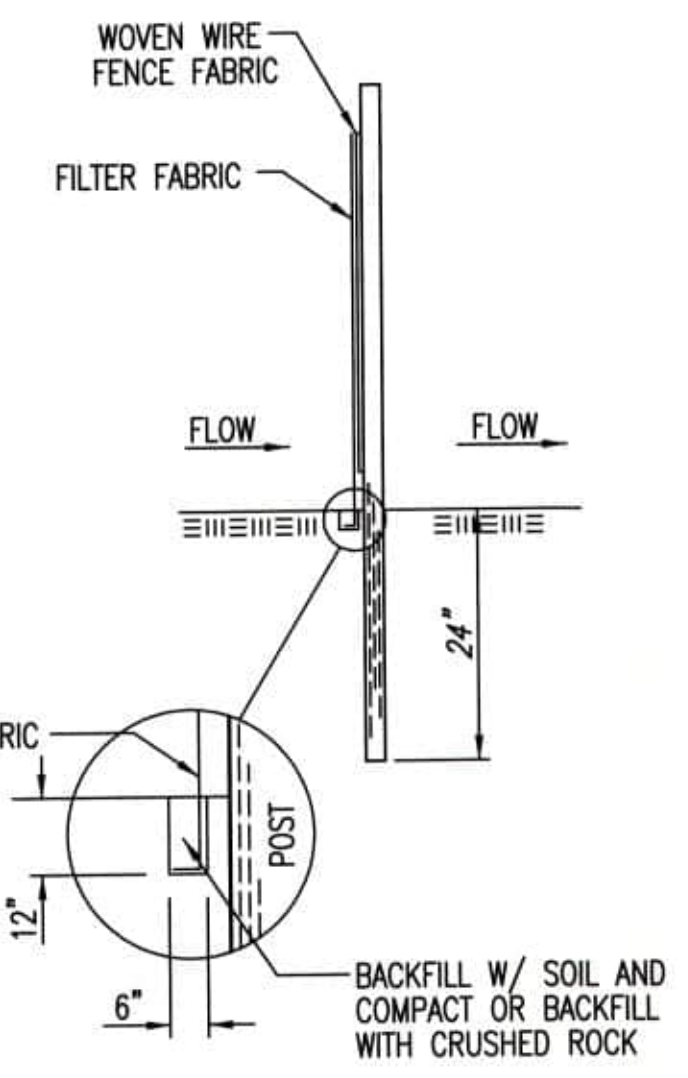
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK—NOT OVER IT. PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. SILT FENCE INSTALLATIONS QUICKLY DETERIORATE WHEN WATER OVERTOPS THEM. DO NOT PLACE SILT FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE A SILT FENCE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE SILT FENCE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE LOW POINT ON THE TOP OF THE FENCE. DO NOT PLACE SILT FENCE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

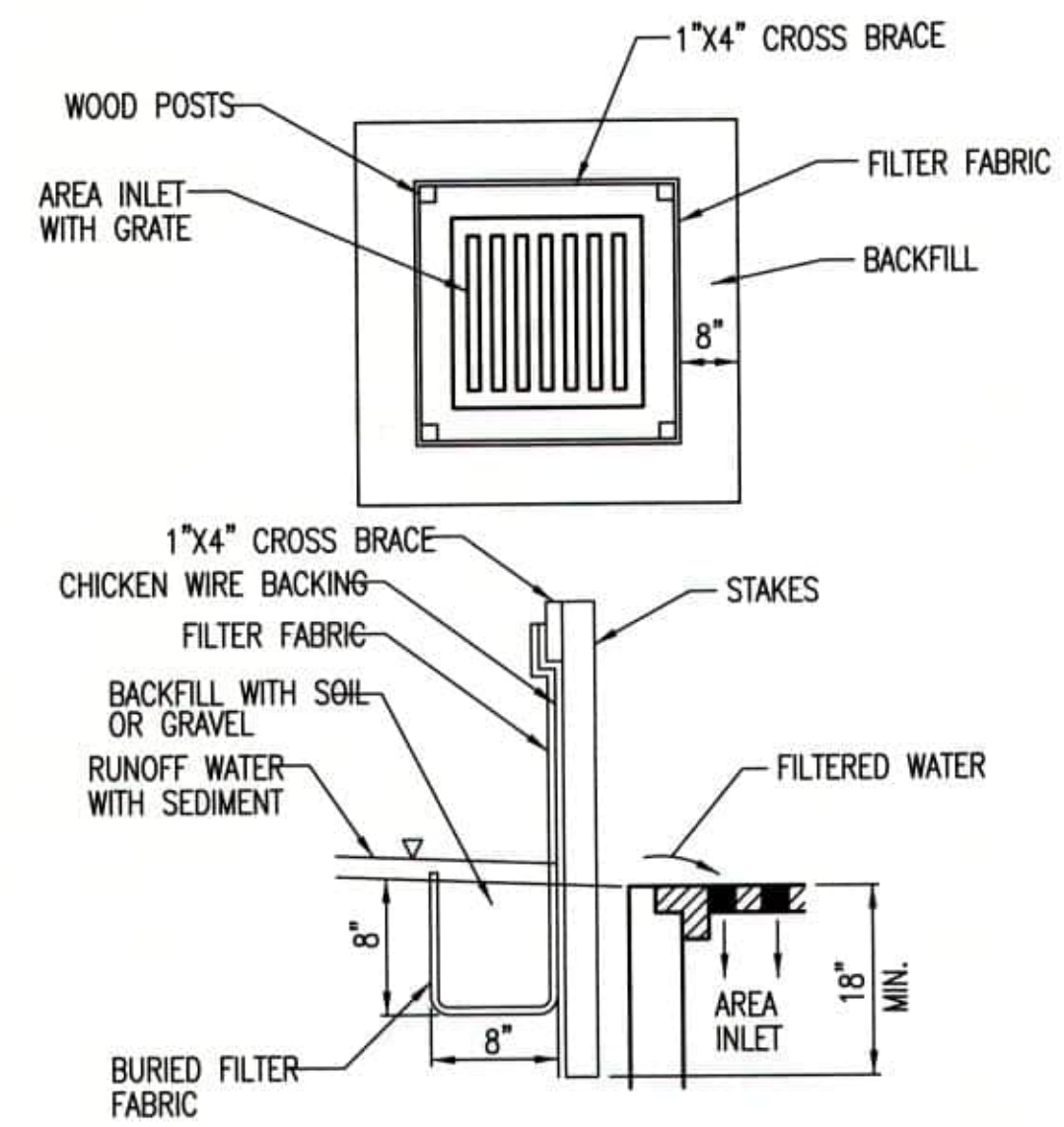
INSPECTION AND MAINTENANCE:

SILT FENCE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



ANCHOR TRENCH DETAIL



SILT FENCE BARRIERS FOR AREA INLETS
(INLET PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE WIRE OR POLYMERIC MESH BACKING USED TO HELP SUPPORT THE SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. THE MATERIAL USED TO FRAME THE TOPS OF THE POSTS SHOULD BE 1" BY 4" BOARDS. SILT FENCE FABRIC AND SUPPORT BACKING SHOULD BE ATTACHED TO THE WOODEN POSTS AND FRAME WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE A SILT FENCE DROP INLET BARRIER IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. WATER SHOULD FLOW THROUGH SILT FENCE, NOT OVER IT. SILT FENCE BARRIERS FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. WHEN USED AS A BARRIER FOR AREA INLETS, SILT FENCE FABRIC AND POSTS MUST BE SUPPORTED AT THE TOP BY A WOODEN FRAME. WHEN A SILT FENCE BARRIER FOR AREA INLETS IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 8" DEEP BY 8" WIDE. DRIVE POSTS TO A DEPTH OF AT LEAST 18" AROUND THE PERIMETER OF THE AREA INLET. THE DISTANCE BETWEEN POSTS SHOULD BE 4' OR LESS. IF THE DISTANCE BETWEEN TWO ADJACENT CORNER POSTS IS MORE THAN 4', ADD ANOTHER POST(S) BETWEEN THEM. CONNECT THE TOPS OF ALL THE POSTS WITH A WOODEN FRAME MADE OF 1" BY 4" BOARDS. USE NAILS OR SCREWS FOR FASTENING. ATTACH THE WIRE OR POLYMERIC-MESH BACKING TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC LONG ENOUGH TO WRAP AROUND THE PERIMETER OF THE AREA INLET. ADD MORE LENGTH FOR OVERLAPPING THE FABRIC JOINT. PLACE THE EDGE OF THE FABRIC IN THE TRENCH, STARTING AT THE OUTSIDE EDGE OF THE TRENCH. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. ATTACH THE SILT FENCE TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. THE JOINT SHOULD BE OVERLAPPED TO THE NEXT POST.

NOTE: WHEN A SILT FENCE BARRIER FOR AREA INLET IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

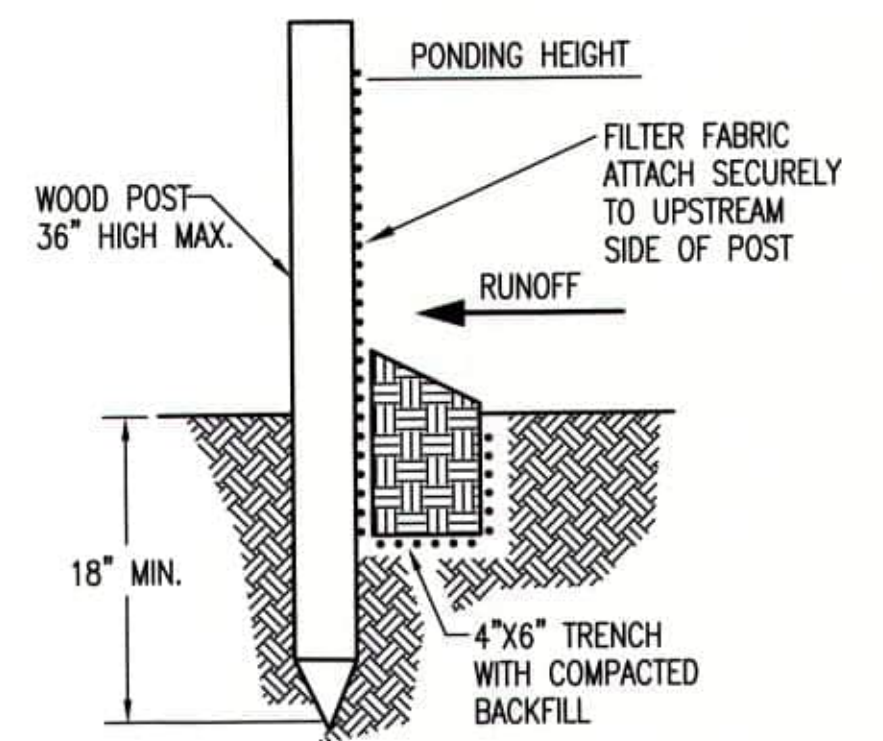
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE BARRIER FOR AREA INLET—NOT OVER IT. PLACE A SILT FENCE BARRIER FOR AREA INLET IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. SILT FENCE BARRIER FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. DO NOT PLACE POSTS ON THE OUTSIDE OF THE SILT FENCE BARRIER FOR AREA INLET. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT INSTALL SILT FENCE BARRIER FOR AREA INLETS WITHOUT FRAMING THE TOP OF THE POSTS. THE CORNER POSTS AROUND AREA INLETS ARE STRESSED IN TWO DIRECTIONS WHEREAS A NORMAL SILT FENCE IS ONLY STRESSED IN ONE DIRECTION. THIS ADDED STRESS REQUIRES MORE SUPPORT.

INSPECTION AND MAINTENANCE:

SILT FENCE BARRIER FOR AREA INLETS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE SILT FENCE?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



SILT FENCE BARRIERS

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, SILT FENCE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. SILT FENCE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 6" DEEP BY 4" WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT-FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE UPSLOPE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 18". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICABLE, DO NOT PLACE SILT FENCE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. WHEN THE FLOW CONCENTRATES, IT OVERTOPS THE BARRIER AND THE SILT FENCE SLOPE BARRIER QUICKLY DETERIORATES. DO NOT PLACE SILT-FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE SILT FENCE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT SUFFICIENTLY ANCHORED, IT WILL WASH OUT. SILT FENCE SLOPE BARRIERS MUST BE DUG INTO THE GROUND—SILT FENCE AT GROUND LEVEL DOES NOT WORK BECAUSE WATER WILL FLOW UNDERNEATH.

INSPECTION AND MAINTENANCE:

SILT FENCE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DOES THE SILT FENCES SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2013

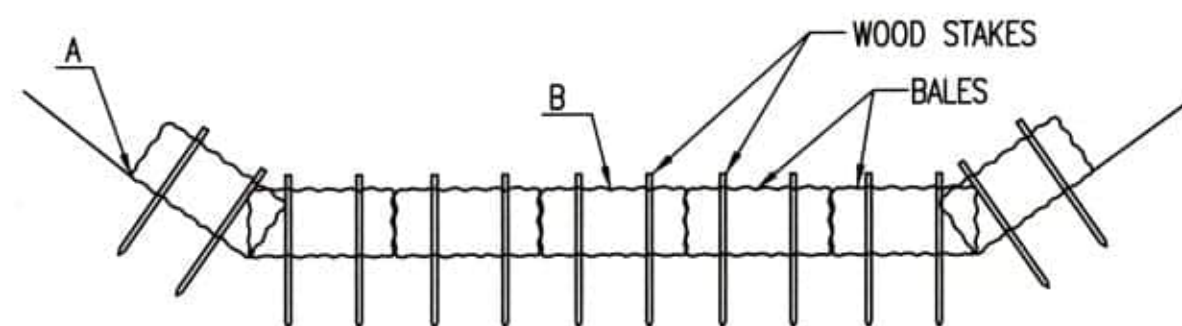


CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

SILT FENCE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 12 of 20

NOTE: POINT A MUST BE HIGHER THAN POINT B SO THAT WATER FLOWS OVER THE BALES AND NOT AROUND THEM.



STRAW BALE DITCH CHECKS

MATERIAL SPECIFICATION:

BALE DITCH CHECKS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. OPTIONAL: THE DOWNSTREAM SCOUR APRON SHOULD BE CONSTRUCTED OF A DOUBLE-NETTED STRAW EROSION-CONTROL BLANKET AT LEAST 6' WIDE. OPTIONAL: THE METAL LANDSCAPE STAPLES USED TO ANCHOR THE EROSION-CONTROL BLANKET SHOULD BE AT LEAST 8" LONG.

PLACEMENT:

BALE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE DITCH CHECK SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK.

STRAW BALE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD.

BALES SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.

THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH GRADE (%)	DITCH CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH-IT WILL BE USED LATER.

OPTIONAL: ON THE DOWNSTREAM SIDE OF THE TRENCH, ROLL OUT A LENGTH OF EROSION-CONTROL BLANKET (SCOUR APRON) EQUAL TO THE LENGTH OF THE TRENCH. PLACE THE UPSTREAM EDGE OF THE EROSION-CONTROL BLANKET ALONG THE BOTTOM UPSTREAM EDGE OF THE TRENCH. THE EROSION CONTROL BLANKET SHOULD BE ANCHORED IN THE TRENCH WITH ONE ROW OF 8" LANDSCAPE STAPLES PLACED ON 18" CENTERS. THE REMAINDER OF THE EROSION-CONTROL BLANKET (THE PORTION THAT IS NOT LYING IN THE TRENCH) WILL SERVE AS THE DOWNSTREAM SCOUR APRON. THIS SECTION OF THE BLANKET SHOULD BE ANCHORED TO THE GROUND WITH 8" LANDSCAPE STAPLES PLACED AROUND THE PERIMETER OF THE BLANKET ON 18" CENTERS. THE REMAINDER OF THE BLANKET SHOULD BE ANCHORED USING TWO EVENLY SPACED ROWS OF 8" LANDSCAPE STAPLES ON 18" CENTERS PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH.

PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.

ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSTREAM SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP AND EXTEND UPSTREAM NO MORE THAN 24".

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

DO NOT PLACE A BALE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW.

DO NOT PLACE BALE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW.

FOLLOW PRESCRIBED DITCH-CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS.

DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE.

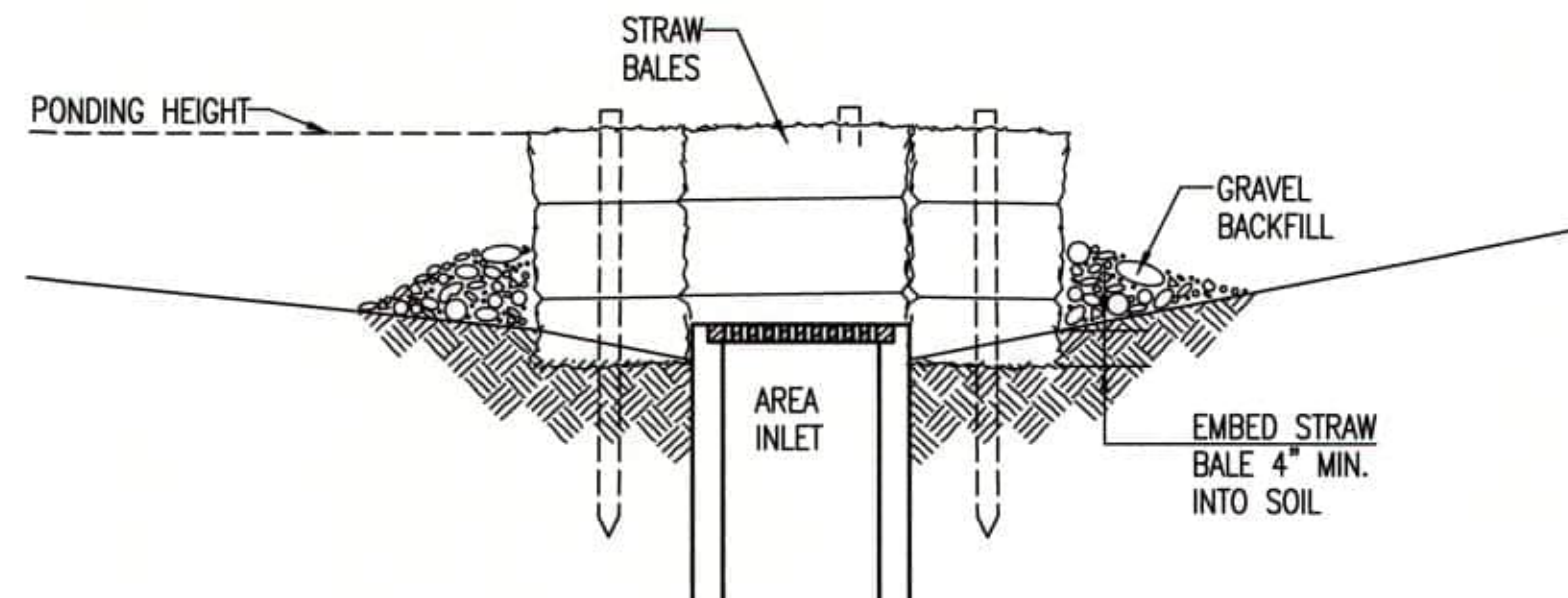
DO NOT PLACE BALE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

BALE DITCH CHECKS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE CHECK.

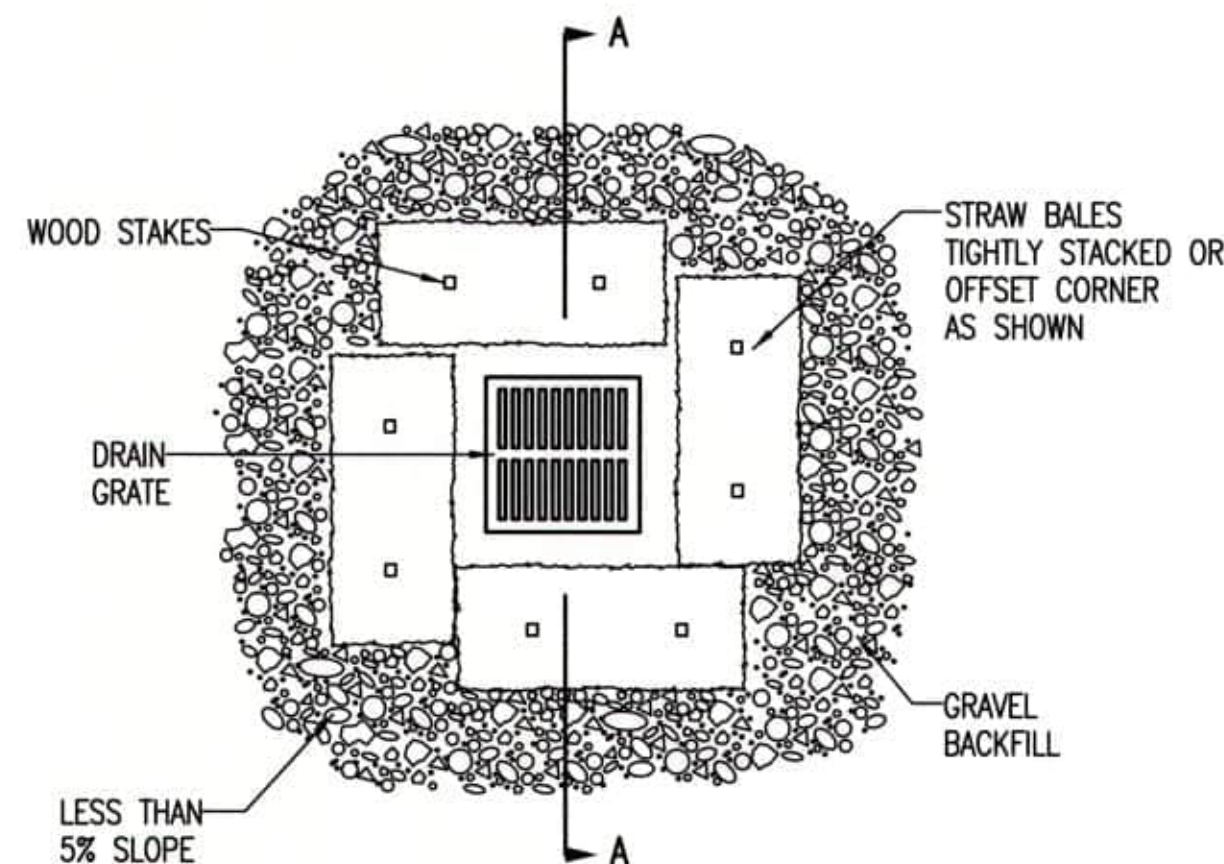
INSPECTION AND MAINTENANCE:

BALE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES AND/OR SCOUR APRONS (OPTIONAL) DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



SECTION A-A



STRAW BALE BARRIERS FOR AREA INLETS (INLET PROTECTION)

MATERIAL SPECIFICATION:

BALE AREA INLET BARRIERS SHOULD BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

BALE AREA INLET BARRIERS SHOULD BE PLACED DIRECTLY AROUND THE PERIMETER OF A DROP INLET. WHEN A BALE AREA INLET BARRIER IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 4" DEEP BY A BALE'S WIDTH WIDE.

PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. SOME BALES MAY NEED TO BE SHORTENED TO FIT INTO THE TRENCH AROUND THE AREA INLET. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.

ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE RECEIVING SIDE OF THE BARRIER AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

NOTE: WHEN A BALE AREA INLET BARRIER IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

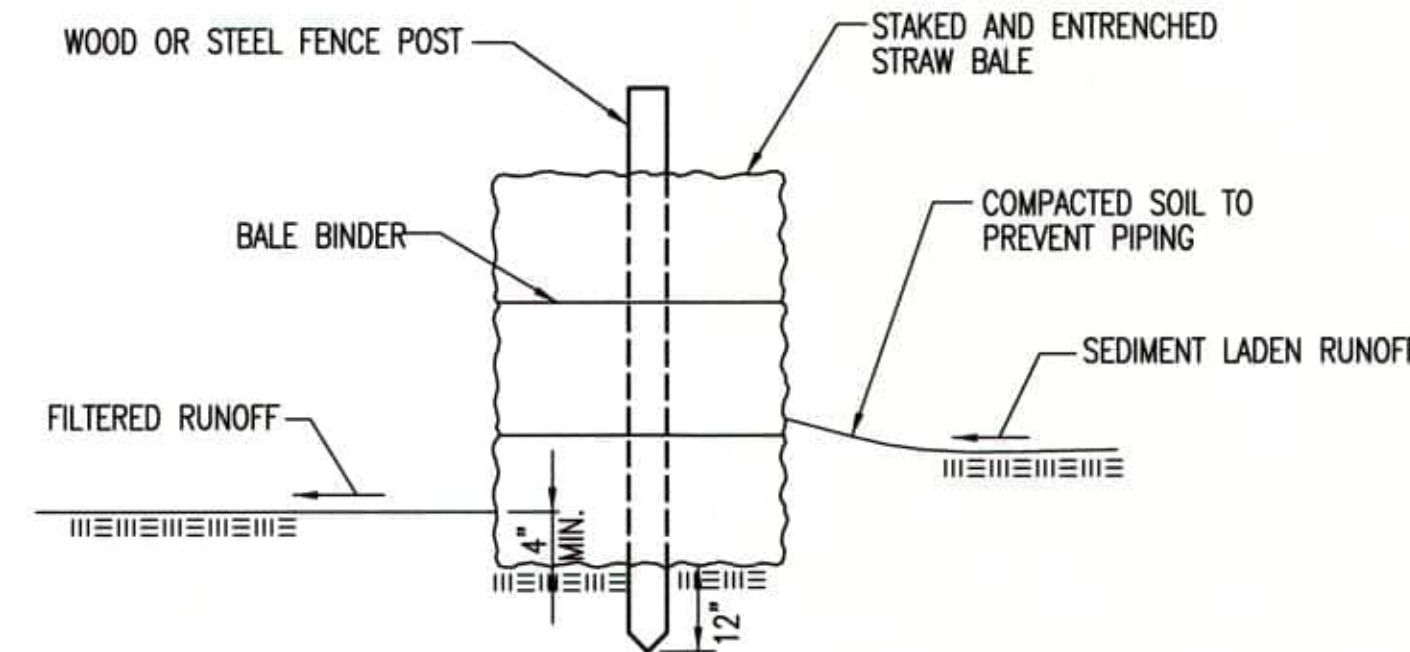
LIST OF COMMON PLACEMENT INSTALLATION MISTAKES TO AVOID:

BALES SHOULD BE PLACED DIRECTLY AGAINST THE PERIMETER OF THE AREA INLET. THIS ALLOWS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR. BALE AREA INLET BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE AREA INLET BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE AREA INLET BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



STRAW BALE BARRIERS

MATERIAL SPECIFICATION:

BALE SLOPE BARRIERS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT.

WHEN PRACTICABLE, BALE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW.

BALE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE.

PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND.

ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSLOPE SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICAL, DO NOT PLACE BALE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. CONCENTRATED FLOW OVER A SLOPE BARRIER CREATES A SCOUR HOLE ON THE DOWNSLOPE SIDE OF THE BARRIER. THE SCOUR HOLE EVENTUALLY UNDERMINES THE BALES AND THE BARRIER FAILS.

DO NOT PLACE BALE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

BALE SLOPE BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2013



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

STRAW BALE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER
GARY JANZEN, P.E.

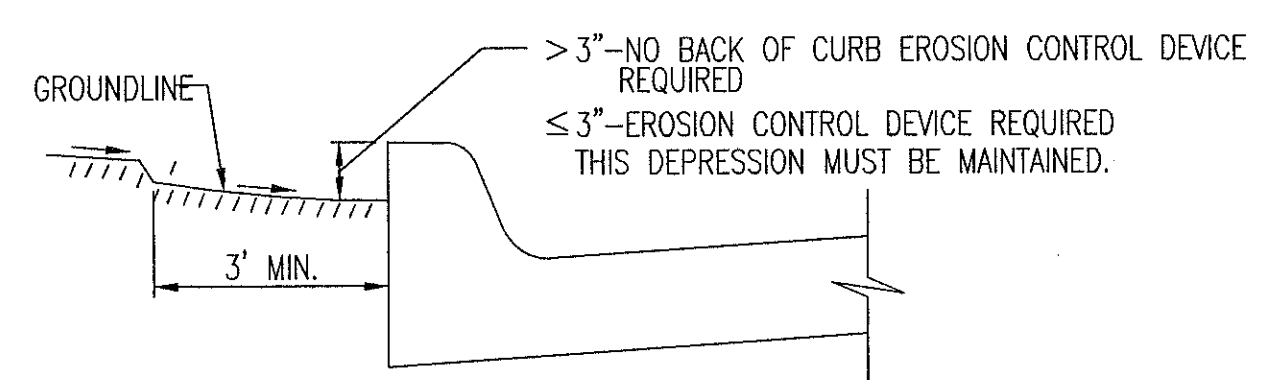
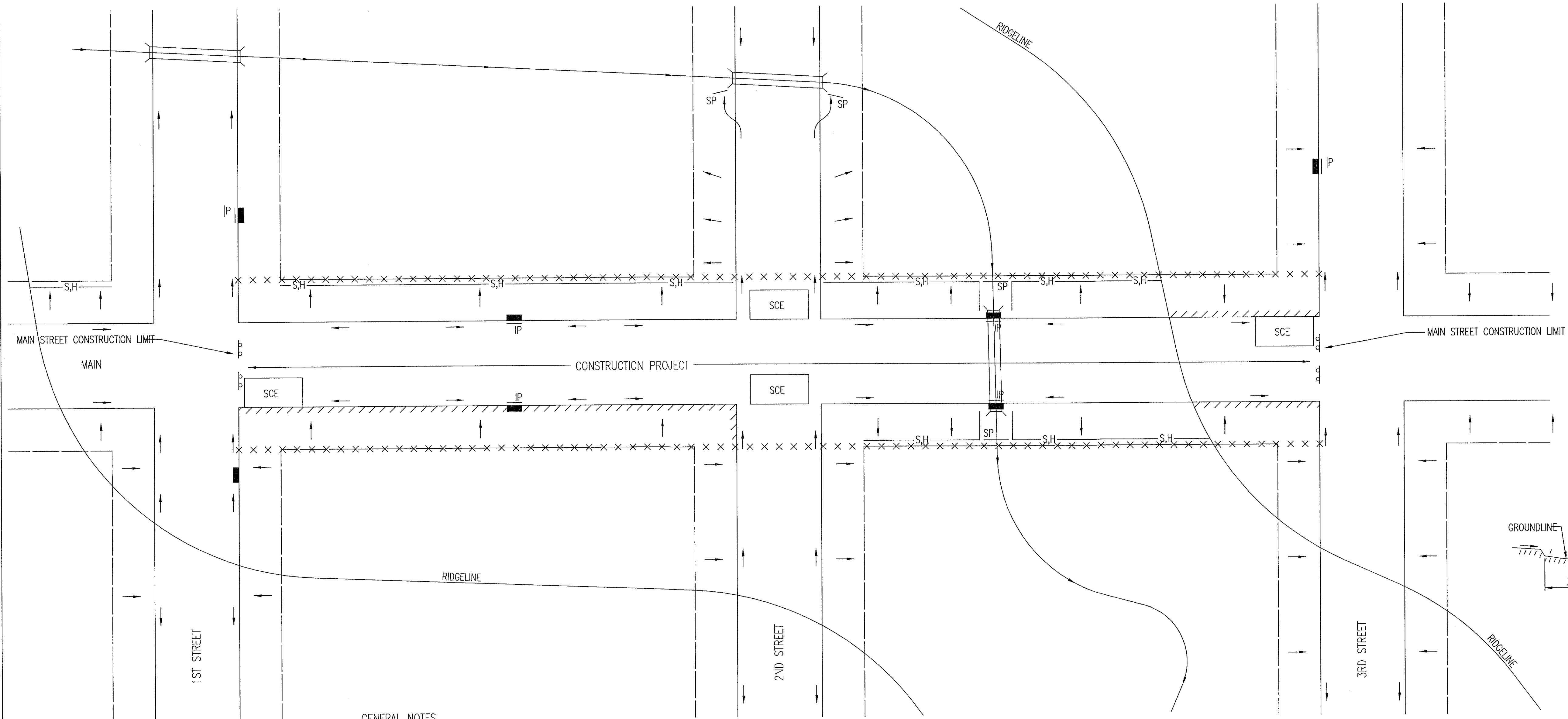
PROJECT NUMBER OCA NUMBER DATE

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET
13 of 20

GENERAL NOTES

1. THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPES OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
2. EROSION CONTROL DEVICES MUST BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PROCESS AND UNTIL THE DISTURBED EARTH IS RESTABILIZED.
3. IF THE PROJECT WILL DISTURB 1 ACRE OR MORE, A FEDERAL/STATE NPDES STORMWATER PERMIT IS REQUIRED. A DETAILED STORMWATER POLLUTION PREVENTION PLAN, IS REQUIRED. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED TO BE THE MINIMUM TO BE SHOWN IN THE POLLUTION PREVENTION PLAN.
4. FOR PROJECTS DISTURBING LESS THAN 1 ACRE, CONTRACTORS ARE ENCOURAGED TO PREPARE STORMWATER POLLUTION PREVENTION PLANS PRIOR TO CONSTRUCTION. EROSION CONTROL DEVICES MUST BE USED ON ALL PROJECTS.
5. FAILURE TO USE AND MAINTAIN EROSION CONTROL DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE CONTRACTOR TO THE PENALTIES PROVIDED FOR THEREIN.
6. THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT DEVICE OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED AS LONG AS THEY ARE EFFECTIVE AND MAINTAINED.



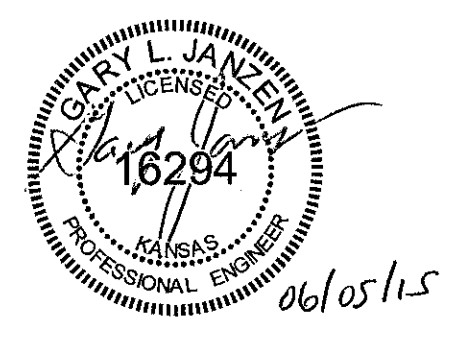
THIS IS A TEMPORARY MEASURE ONLY, WHEN APPROVED BY THE PROJECT ENGINEER. THE DIRT GRADE BEHIND THE CURB SHALL BE BROUGHT TO THE TOP OF CURB, WITH TEMPORARY EROSION CONTROL MAT OR PERMANENT VEGETATION PLACED, PRIOR TO THE COMPLETION OF ALL PROJECTS.

GENERAL NOTES

1. THE INTENT OF ALL EROSION CONTROL DEVICES IS TO KEEP ALL SEDIMENT CONFINED TO THE CONSTRUCTION SITE, AND OUT OF ALL UNDERGROUND PIPES, DITCHES, LAKES, AND OTHER DRAINAGE FACILITIES, AND OFF OF STREETS.
2. THE POINT OF COMPLIANCE IS GENERALLY THE RIGHT-OF-WAY LINES WITHIN THE LIMITS OF CONSTRUCTION.
3. EROSION CONTROL DEVICES WILL BE REQUIRED AT ALL POINTS ALONG THE PROJECT WHERE DISTURBED EARTH CAN DRAIN ONTO PRIVATE PROPERTY.
4. INLET PROTECTION DEVICES WILL BE REQUIRED WHEREVER WATER CAN DRAIN OFF THE PROJECT SITE INTO AN INLET, INCLUDING ANY SIDE STREET INLETS.
5. EROSION CONTROL DEVICES SHALL BE INSTALLED AT CREEK CROSSINGS SO AS TO PREVENT SEDIMENT FROM ENTERING THEREIN.
6. STABILIZED CONSTRUCTION ENTRANCES SHALL BE PROVIDED, AS NEEDED, TO PREVENT MUD FROM TRACKING ONTO STREETS NOT UNDER CONSTRUCTION AND ON STREETS WITHIN THE PROJECT LIMITS IF TRAFFIC IS BEING MAINTAINED THROUGH THE PROJECT.
7. ANY MUD TRACKED ONTO STREETS MUST BE REMOVED AT THE END OF EACH WORK DAY.
8. THE CONTRACTOR WILL BE REQUIRED TO PLACE EROSION CONTROL DEVICES BACK OF CURB, WHENEVER WATER CAN DRAIN OVER CURB, TO KEEP ERODED SOIL OUT OF THE GUTTERLINES, IN ACCORDANCE WITH THE FOLLOWING:
 - A. THE DEVICE REQUIRED WILL BE APPROVED EROSION CONTROL MAT LISTED ON THE CITY'S APPROVED MATERIAL LIST. SAID BLANKET SHALL BE PLACED OVER THE APPROPRIATE SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS. (SEE SOIL EROSION BMPs - BACK OF CURB SEDIMENT BARRIER DETAILS)
 - B. THIS DEVICE SHALL BE INSTALLED IMMEDIATELY WHENEVER THE CURB IS BACKFILLED TO WITHIN 3" OF THE TOP OF CURB. (SEE CURB BACKFILL DETAIL) OTHER BMP'S MAY BE REQUIRED AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB.
 - C. ADDITIONALLY, OTHER EROSION CONTROL DEVICES (HAY BALES, SILT FENCE, ETC.) WILL BE INSTALLED AT LOCATIONS OF CONCENTRATED FLOW RESULTING IN SEDIMENT OVERRUNNING THE MAT.
 - D. SHOULD THE PROJECT PLANS SPECIFY THAT THE RIGHT-OF-WAY IS TO BE SODDED, THE EXCELSIOR MAT WILL NOT BE REQUIRED SO LONG AS THE SOD IS PLACED WITHIN 48 HOURS AFTER CURB BACKFILL REACHES A HEIGHT OF 3" OR LESS FROM TOP OF CURB. (SEE CURB BACKFILL DETAIL)

LEGEND

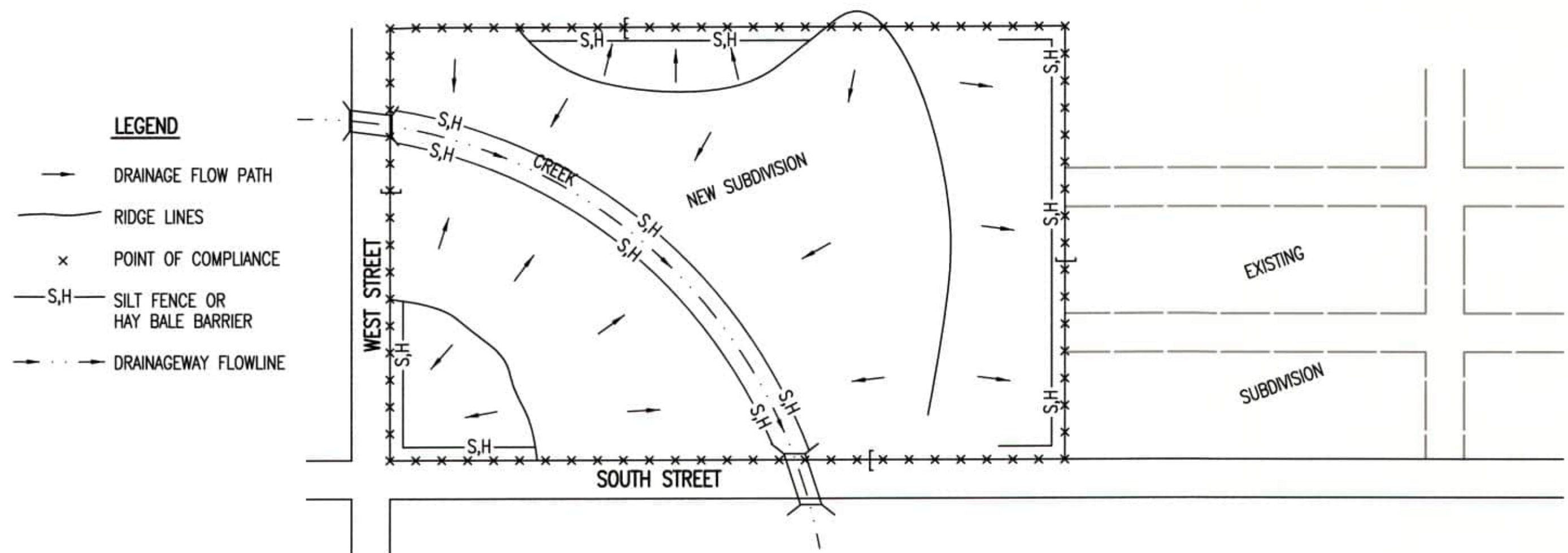
- R-O-W LIMITS
- DRAINAGE FLOW PATH
- × × × × R/W LIMIT WITHIN CONSTRUCTION LIMIT
- STORM WATER INLETS
- IP INLET PROTECTION
- S,H SILT FENCE OR HAY BALE BARRIER
- SP STREAM PROTECTION
- SCE STABILIZED CONSTRUCTION ENTRANCE
- //// BACK OF CURB PROTECTION



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

REVISION: JUNE 2015		
STREET IMPROVEMENT PROJECTS		
CITY ENGINEER GARY JANZEN, P.E.		
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 14 of 20

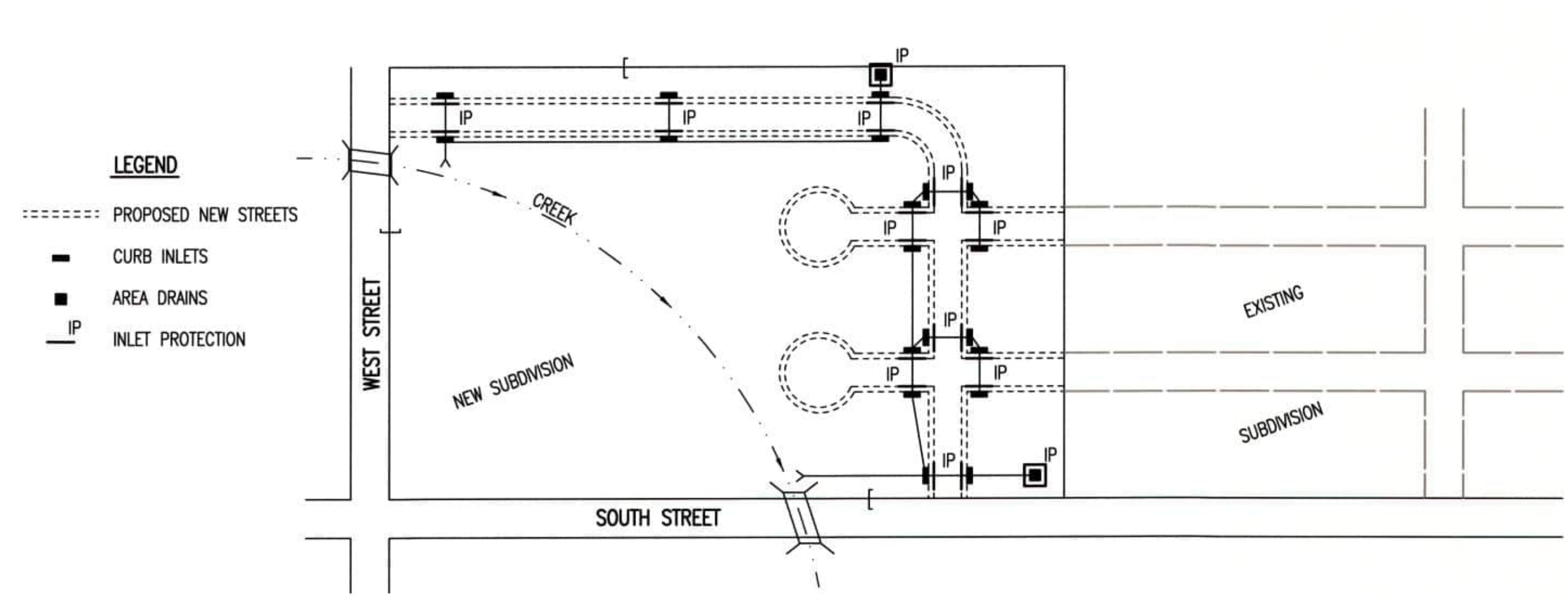
PHASE 1 – INITIAL EARTHWORK AND UTILITIES (EXCEPT STORM SEWER)



- LEGEND**
- DRAINAGE FLOW PATH
 - RIDGE LINES
 - x POINT OF COMPLIANCE
 - S.H. SILT FENCE OR HAY BALE BARRIER
 - DRAINAGEWAY FLOWLINE

1. DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, THE POINTS OF COMPLIANCE ARE THE PERIMETER BOUNDARIES AND ANY DRAINAGE WAYS OR STORM SEWERS DRAINING THROUGH OR FROM THE SITE. SHOULD LAKES BE CONSTRUCTED WITHIN THE SUBDIVISION THAT WILL DISCHARGE DURING STORMS, THEY ARE ALSO A POINT OF COMPLIANCE.
2. HAY BALES OR SILT FENCE MUST BE CONSTRUCTED ALONG THE PROPERTY LINE WHERE ON SITE WATER CAN DRAIN OFF THE PROPERTY. THESE EROSION CONTROL DEVICES WILL ALSO BE INSTALLED ALONG ANY DRAINAGE DITCH OR LAKE THAT CAN DISCHARGE.
3. SHOULD SILT OR SEDIMENT ENTER THE DITCHES OR STREETS ON THE ADJACENT BOUNDARY STREETS, APPROPRIATE EROSION CONTROL DEVICES WILL BE PLACED WITHIN THE SUBDIVISION TO PREVENT THIS.
4. ANY MUD TRACKED ONTO ADJACENT STREETS WILL BE REMOVED WITHIN 48 HOURS OR BY FRIDAY AT 6:00 PM, WHICHEVER IS EARLIER.
5. CONTRACTORS WORKING WITHIN THE SITE WILL NOT BE REQUIRED TO USE INDIVIDUAL EROSION CONTROL DEVICES AS LONG AS THOSE SPECIFIED ABOVE ARE IN PLACE AND EFFECTIVE. CONTRACTORS WORKING ON THE BOUNDARY LINE STREETS OR ON ADJACENT PROPERTIES TO EXTEND UTILITIES ARE EXPECTED TO USE EROSION CONTROL DEVICES AT THEIR WORK LOCATIONS, AS NEEDED.
6. UTILIZE STABILIZED CONSTRUCTION ENTRANCE AT ENTRANCE AND EXIT ONTO ANY EXISTING PUBLIC STREETS.
7. IF THE INITIAL EARTH WORK AND UTILITIES ARE DONE AS PART OF A PUBLIC IMPROVEMENT PROJECT, THESE EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS SPECIFIED IN THE INDIVIDUAL PROJECT CONTRACTS. THE CONTRACTOR WILL MAINTAIN THE DEVICES UNTIL COMPLETION OF THE CONTRACT, AT WHICH TIME THE DEVELOPER WILL ASSUME MAINTENANCE RESPONSIBILITIES. IF THESE CONTRACTS ARE NOT PUBLIC IMPROVEMENT PROJECTS, THE DEVELOPER WILL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THESE DEVICES.
8. WITHIN 14 DAYS OF COMPLETION OF EARTHWORK ACTIVITIES IN ANY GIVEN AREA, THAT AREA SHALL BE TEMPORARILY OR PERMANENTLY SEEDED AND MULCHED.

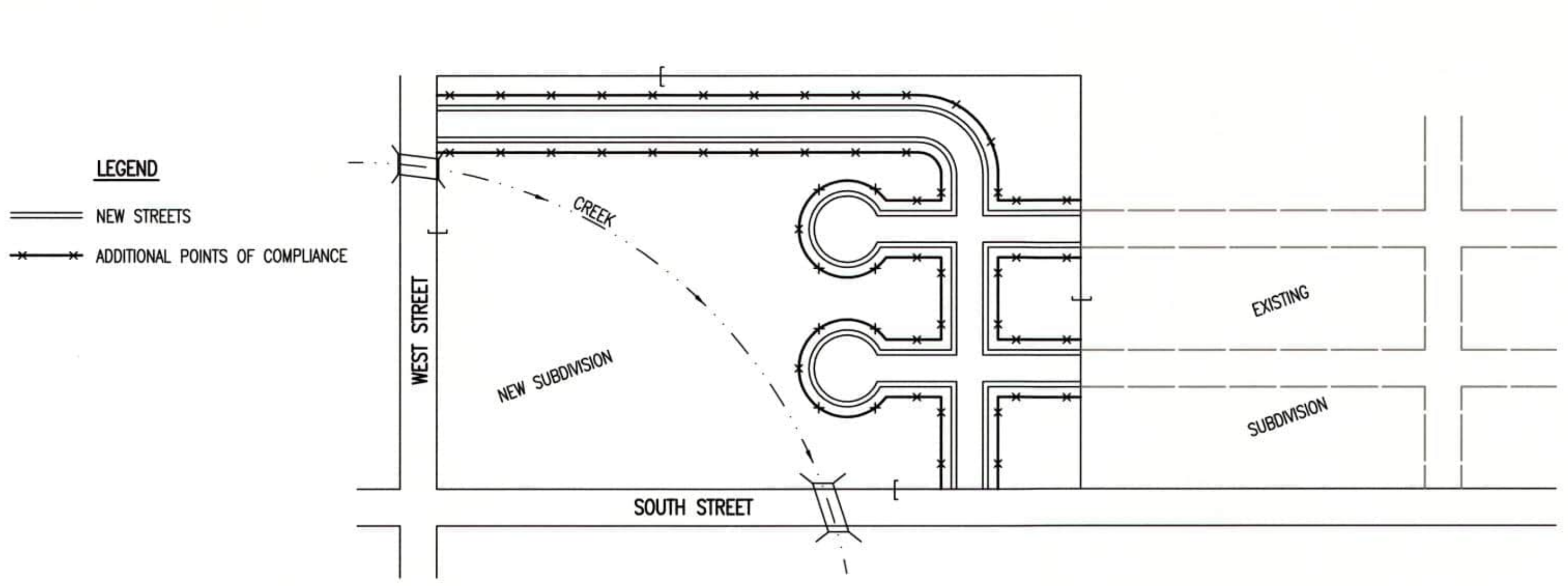
PHASE 2 – INSTALLATION OF STORM SEWER



- LEGEND**
- PROPOSED NEW STREETS
 - CURB INLETS
 - AREA DRAINS
 - IP INLET PROTECTION

1. DURING THIS PHASE OF SUBDIVISION DEVELOPMENT, ALL EROSION CONTROL DEVICES REQUIRED IN PHASE 1 SHALL REMAIN IN PLACE AND BE MAINTAINED.
2. AS NEW STORM SEWERS, WITH INLETS, ARE INSTALLED, THE STORM SEWERS MUST NOW BE PROTECTED SO ALL NEW INLETS BECOME POINTS OF COMPLIANCE.
3. AREA DRAINS – AS SOON AS WATER CAN FLOW INTO THESE DRAINS, HAY BALE OR SILT FENCE PROTECTION WILL BE INSTALLED AROUND THEM.
4. CURB OPENING INLETS – AS SOON AS WATER CAN FLOW INTO THESE DRAINS, INLET PROTECTION DEVICES MUST BE INSTALLED. IF WATER CANNOT FLOW INTO CURB INLETS UNTIL STREET CONSTRUCTION IS COMPLETE, THEN STREET CONTRACTOR WILL INSTALL INLET PROTECTION. SEE PHASE 3 – STREET CONSTRUCTION.
5. THE STORM SEWER CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING THESE DEVICES.
6. THE SUBDIVISION DEVELOPER WILL MAINTAIN THESE EROSION CONTROL DEVICES ONCE INSTALLED.
7. ALL DISTURBED GROUND WILL BE FINAL GRADED AND TEMPORARILY OR PERMANENTLY SEEDED WITHIN 14 DAYS IF COMPLETION OF WORK IN ANY GIVEN PART OF THE SUBDIVISION.
8. ONCE ALL DISTURBED GROUND DRAINING TO AN INLET HAS BEEN RESTABILIZED WITH GRASS OR SOD, THE SUBDIVISION DEVELOPER WILL BE RESPONSIBLE FOR PERMANENTLY REMOVING THE INLET PROTECTION.

PHASE 3 – STREET CONSTRUCTION

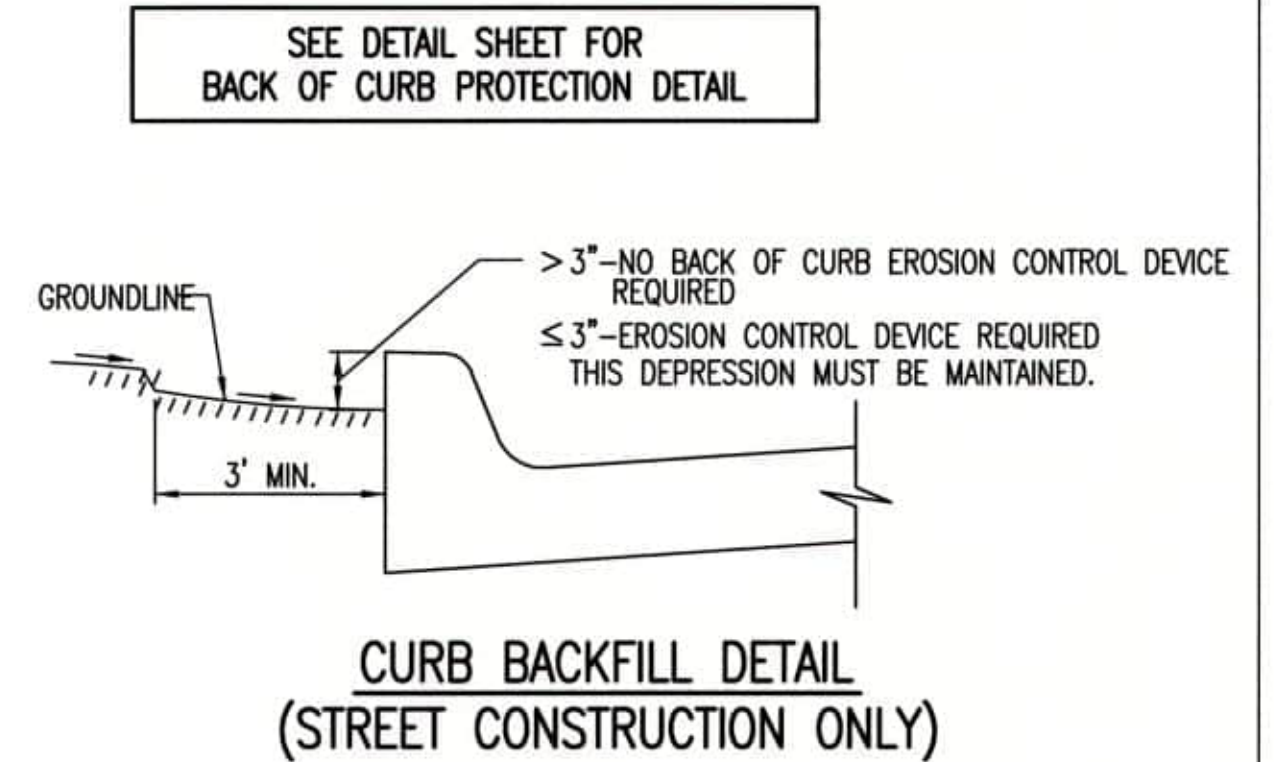


- LEGEND**
- == NEW STREETS
 - x-x-x-x ADDITIONAL POINTS OF COMPLIANCE

1. DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, NEW STREETS ARE INSTALLED. ALL EROSION CONTROL DEVICES INSTALLED DURING PHASE 1 AND 2 MUST STILL BE MAINTAINED. THE POINT OF COMPLIANCE NOW SHIFTS TO THE BACK OF CURB ALONG EACH STREET.
2. CURB OPENING INLET PROTECTION:
 - A. SUMP AREAS – INLET PROTECTION SHALL BE PROVIDED WHEN STREET SUBGRADE WORK IS COMPLETED.
 - B. NON-SUMP LOCATIONS – PROVIDE INLET PROTECTION AS SOON AS BASE COURSE ASPHALT IS INSTALLED, BEFORE THE SURFACE COURSE LIFT.
3. EROSION CONTROL DEVICES WILL BE REQUIRED BACK OF CURB WHEREVER WATER CAN FLOW OVER THE CURB AND THE CURB HAS BEEN BACKFILLED TO WITHIN 3" OR LESS OF THE TOP OF CURB (SEE CURB BACKFILL DETAIL). FOR CURBS NOT YET ENTIRELY BACKFILLED (3" OR MORE BELOW TOP OF CURB), ADDITIONAL DEVICES WILL BE REQUIRED AT POINTS WHERE WATER BREAKS OVER CURB WHICH COULD RESULT IN THE PLACEMENT OF SEDIMENT IN THE GUTTER.
4. SEE DETAIL SHEET FOR BACK OF CURB PROTECTION.
5. THE BACK OF CURB PROTECTION SPECIFIED ON THIS PLAN MAY HAVE TO BE SUPPLEMENTED WITH HAY BALE OR SILT FENCE EROSION CONTROL DEVICES AT LOCATIONS WHERE CONCENTRATED FLOW RESULTS IN SEDIMENT BEING CARRIED OVER THE EXCELSIOR MATS.
6. THE STREET CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING BACK OF CURB EROSION CONTROL DEVICES.
7. THE INDIVIDUAL LOT OWNERS WILL BE RESPONSIBLE FOR MAINTAINING THE BACK OF CURB EROSION CONTROL DEVICES IN FRONT OF THEIR LOTS UNTIL SUCH TIME AS ADJACENT DISTURBED EARTH IS STABILIZED WITH GRASS OR SOD.

GENERAL NOTES

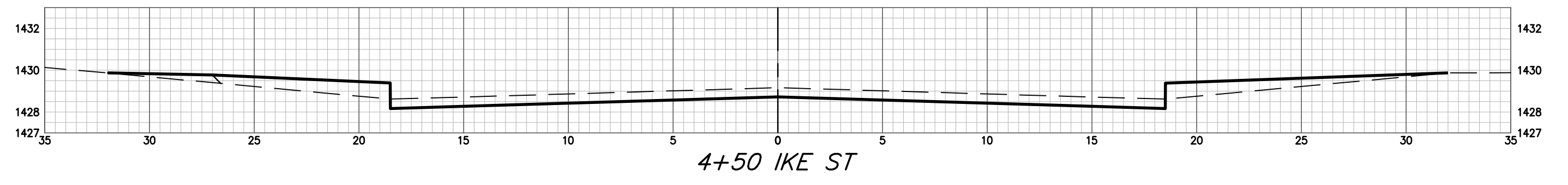
1. THE INTENT OF ALL EROSION CONTROL DEVICES IS TO PREVENT ERODED SOIL FROM ENTERING DITCHES, STORM SEWERS, LAKES, STREETS OR ANY OTHER OTHER DRAINAGE FEATURE.
2. THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPE OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
3. EROSION CONTROL DEVICES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS TO REMAIN EFFECTIVE. MAINTENANCE SHALL BE AS INDICATED ON SOIL EROSION BMP'S DETAIL SHEETS.
4. PERSONS DESTROYING EROSION CONTROL DEVICES SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING THEM OR INSTALLING SUITABLE REPLACEMENT DEVICES.
5. THE DEVELOPMENT OF ANY SUBDIVISION THAT DISTURBS 1 ACRE OR MORE WILL REQUIRE A FEDERAL/STATE NPDES STORMWATER PERMIT. THE PREPARATION OF A STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. EROSION CONTROL DEVICES ARE REQUIRED. THE DETAILS SHOWN ON THIS SHEET ARE THE MINIMUM STANDARDS TO BE SHOWN ON POLLUTION PREVENTION PLANS.
6. FOR SUBDIVISIONS SMALLER THAN 1 ACRE, SOIL EROSION DEVICES ARE REQUIRED. ALSO, DEVELOPERS AND CONTRACTORS ARE ENCOURAGED TO DEVELOP POLLUTION PREVENTION PLANS FOR EACH PROJECT PRIOR TO CONSTRUCTION.
7. FAILURE TO USE AND MAINTAIN SOIL EROSION DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE SUBDIVISION DEVELOPER AND CONTRACTORS TO THE PENALTIES PROVIDED THEREIN.
8. THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE DEVICES OTHER THAN THAT SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED SO LONG AS THEY ARE EFFECTIVE AND MAINTAINED.
9. A STABILIZED EARTH SURFACE IS DEFINED AS ONE THAT IS HARD SURFACED WITH CONCRETE, ASPHALT, OR THE LIKE, OR ONE ON WHICH 70% OF THE GRASS HAS GERMINATED ON THE ENTIRE SURFACE.



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

SUBDIVISION DEVELOPMENT PROCESS
CITY ENGINEER
GARY JANZEN, P.E.

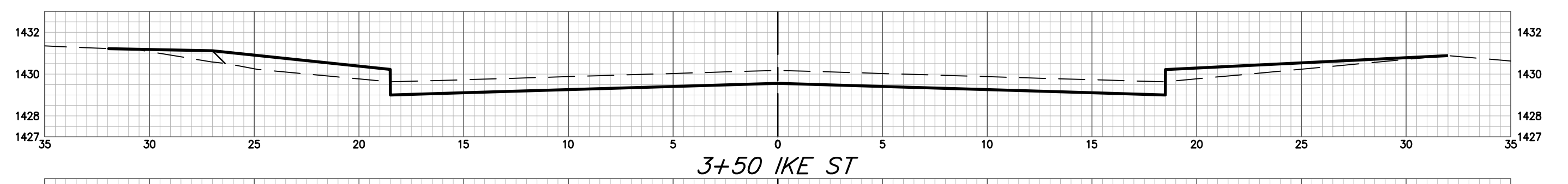
PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		SHEET 15 of 20



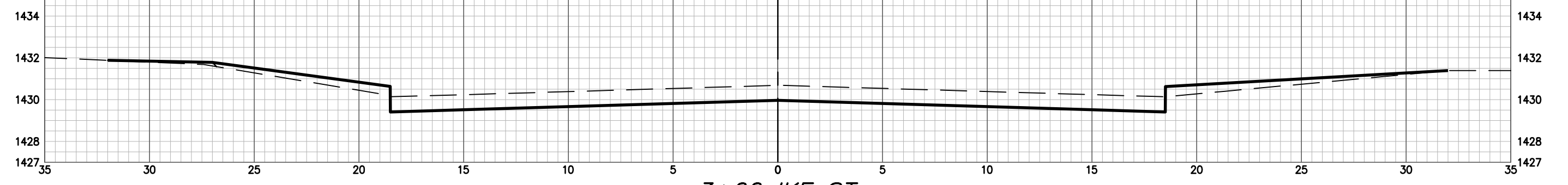
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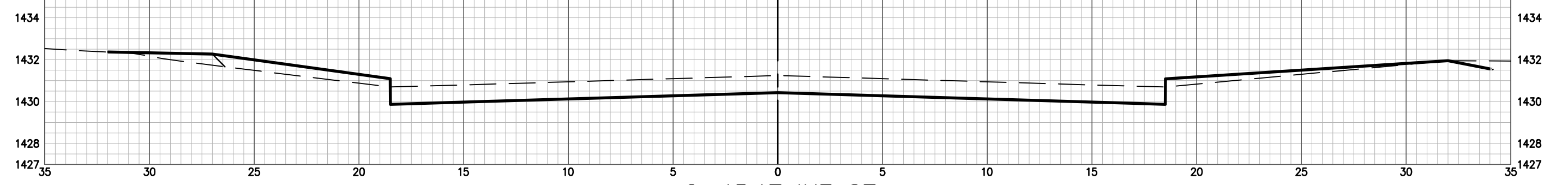
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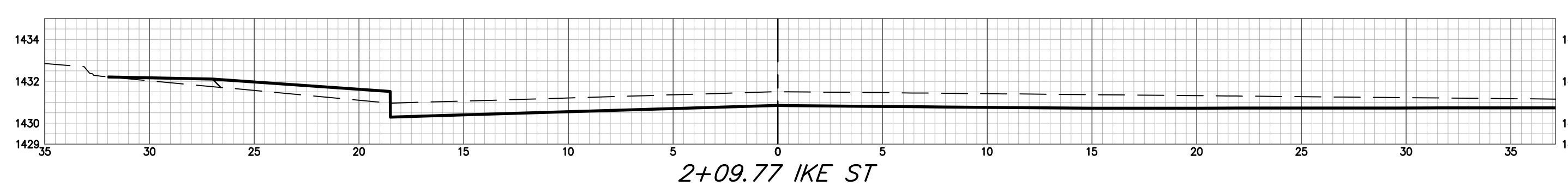
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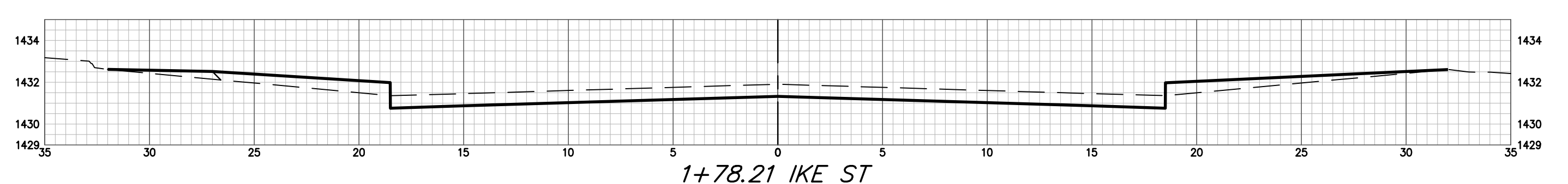
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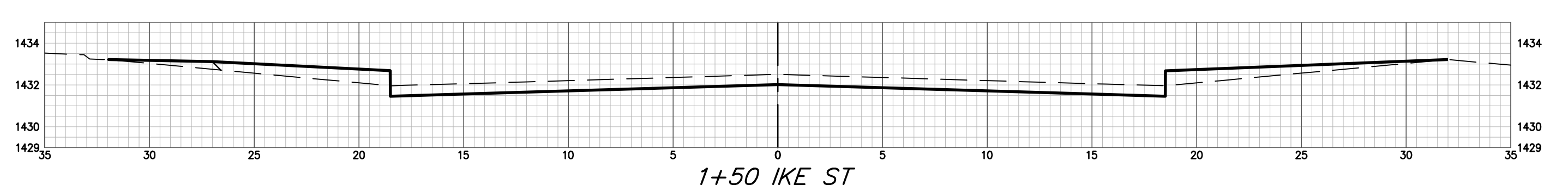
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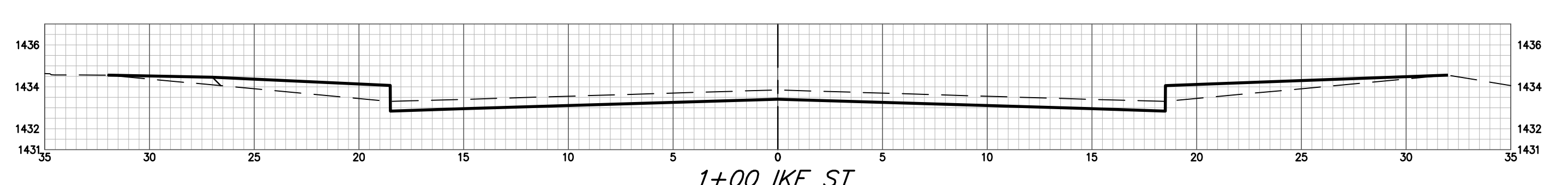
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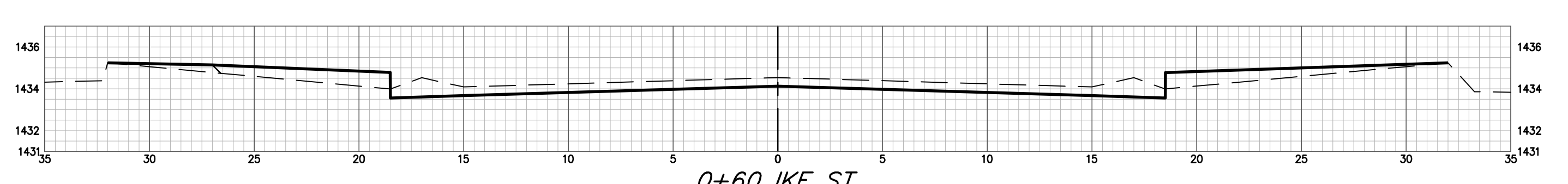
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1+50 IKE ST



1+00 IKE ST

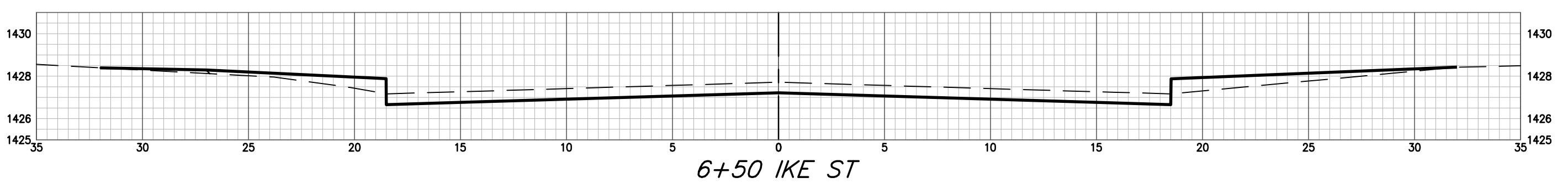


0+60 IKE ST

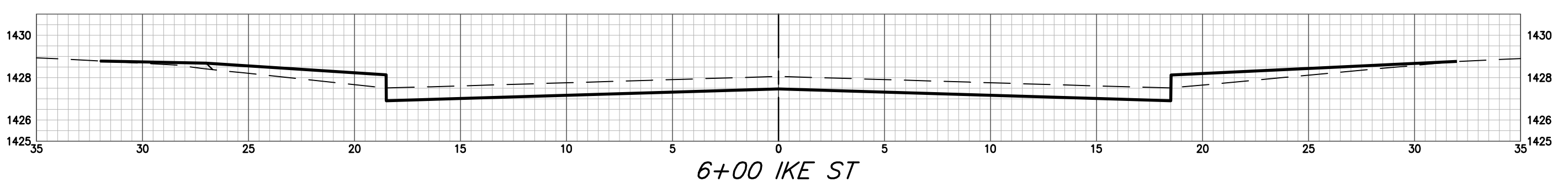
IKE ST CY Excavation (unadjusted)				IKE ST CY Compacted Fill (unadjusted)				IKE ST CY Loose Fill (unadjusted)			
Station	Area	Volume	Total	Station	Area	Volume	Total	Station	Area	Volume	Total
0+60	17.20	0.00	0.00	0+60	0.98	0.00	0.00	0+60	10.16	0.00	0.00
1+00	16.56	25.01	25.01	1+00	0.98	1.46	1.46	1+00	9.79	14.78	14.78
1+50	18.14	32.14	57.15	1+50	0.99	1.71	3.17	1+50	9.35	17.76	32.54
1+78.21	21.71	20.82	77.97	1+78.21	0.99	0.97	4.13	1+78.21	8.30	9.23	41.77
2+09.77	33.83	32.79	110.76	2+09.77	0.98	1.08	5.21	2+09.77	3.82	6.98	48.75
2+45.17	30.67	42.08	152.84	2+45.17	1.32	1.56	6.77	2+45.17	6.38	6.77	55.52
3+00	26.52	58.03	210.87	3+00	0.32	1.83	8.60	3+00	5.88	12.47	67.99
3+50	23.11	45.95	256.81	3+50	1.19	1.42	10.02	3+50	8.99	13.77	81.76
4+00	19.69	39.63	296.44	4+00	0.98	2.02	12.04	4+00	8.90	16.57	98.33
4+50	16.28	33.30	329.74	4+50	0.98	1.82	13.86	4+50	9.91	17.42	115.75
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6+00	21.88	43.61	462.99	6+00	0.53	1.43	18.95	6+00	7.89	14.09	155.59
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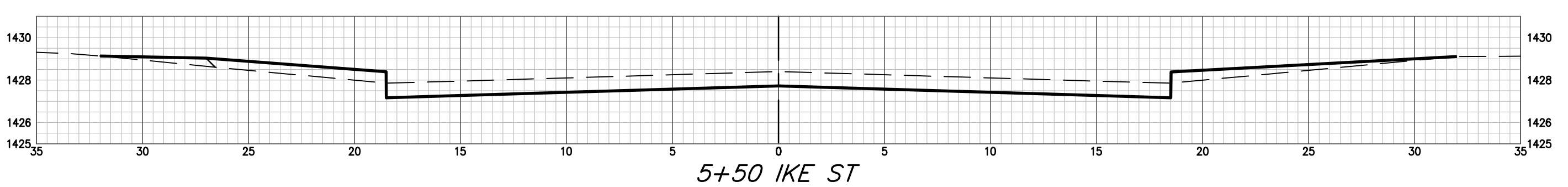
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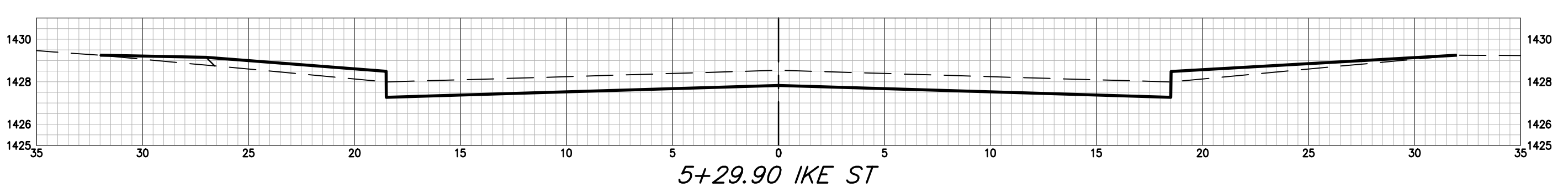
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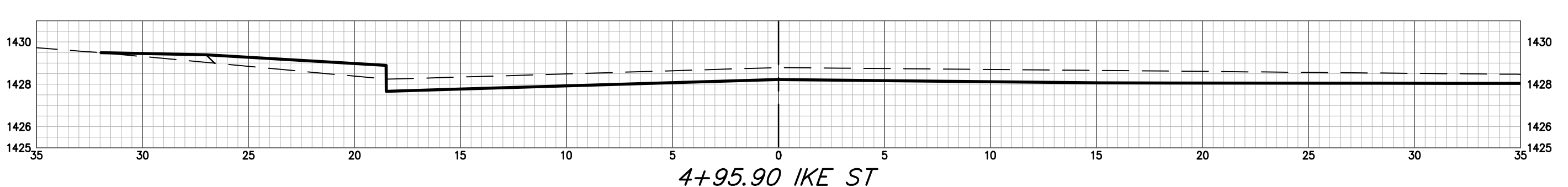
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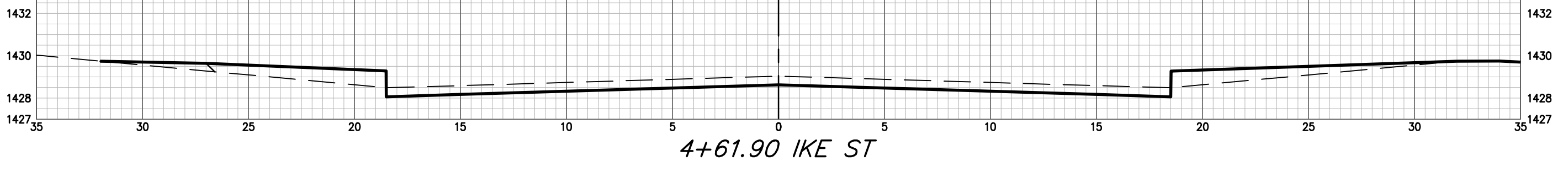
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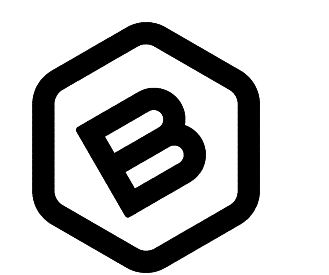
5+29.90 IKE ST



4+95.90 IKE ST



4+61.90 IKE ST



**BAUGHMAN
COMPANY**
315 Ellis St.
Wichita, KS 67211
316-262-7271
BaughmanCo.com

ABILENE PLACE ADDITION
PHASE II

**EARTHWORK
CROSS
SECTIONS**

Street Paving Improvements

PROJECT NUMBER:

DESIGN: DMV DRAWN: JAK

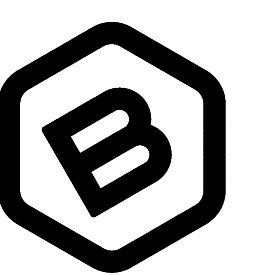
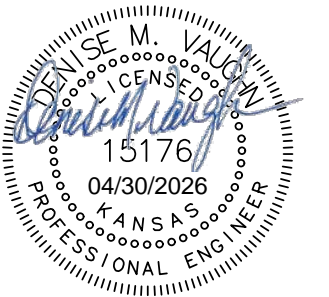
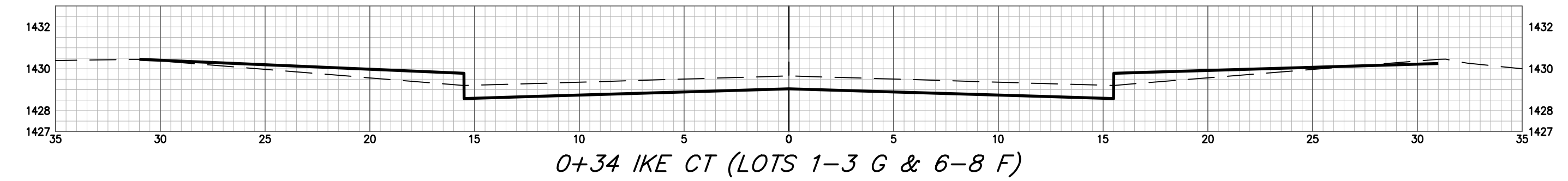
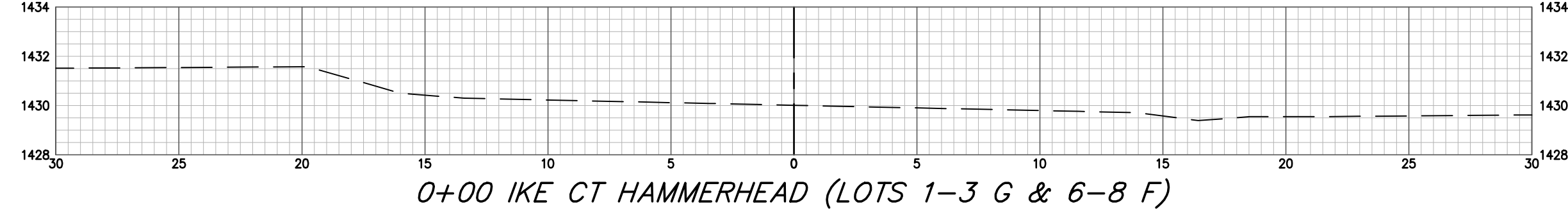
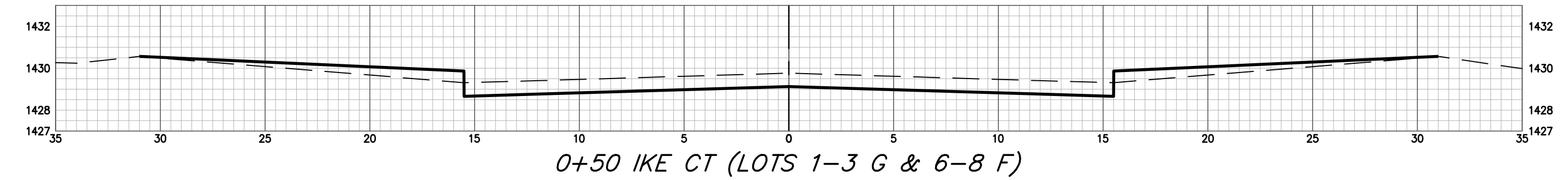
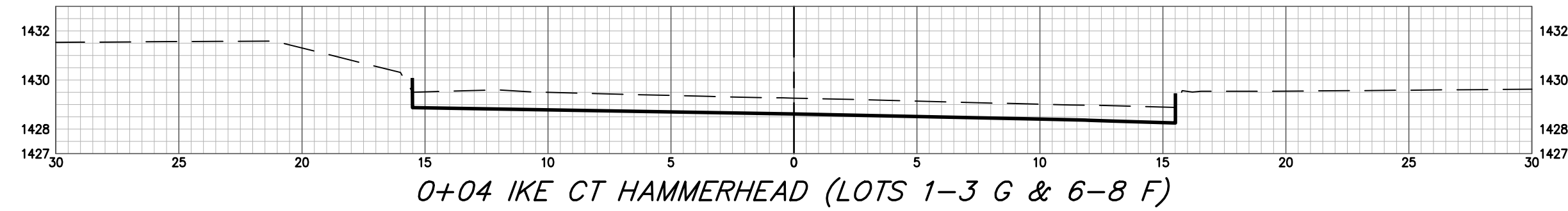
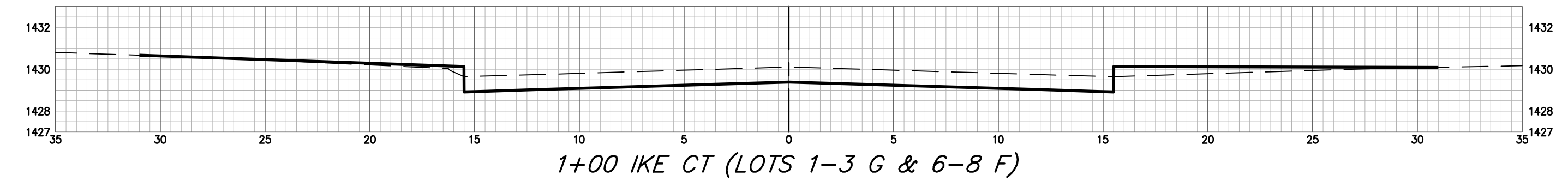
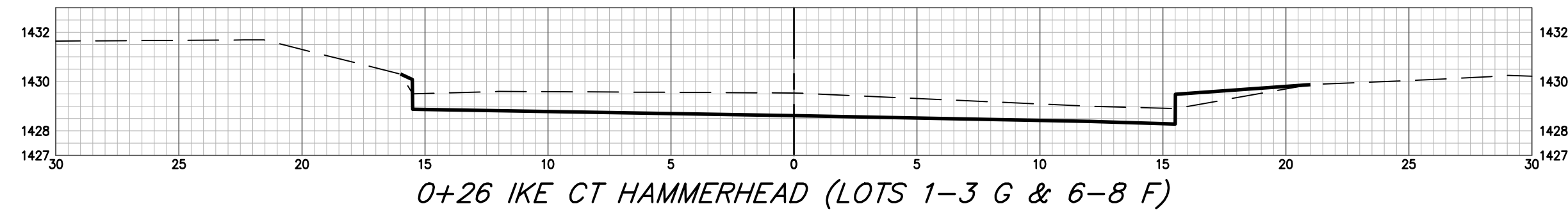
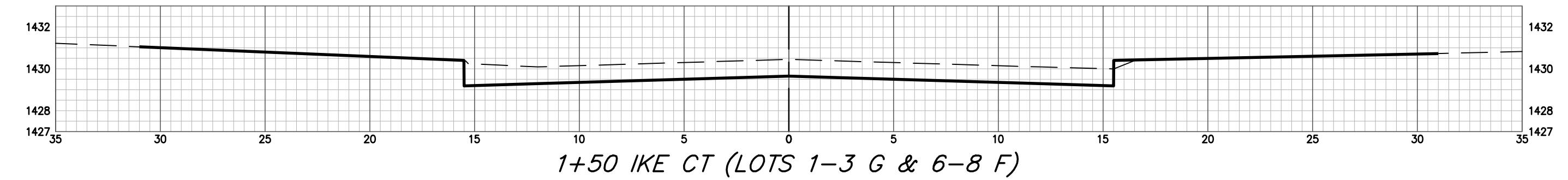
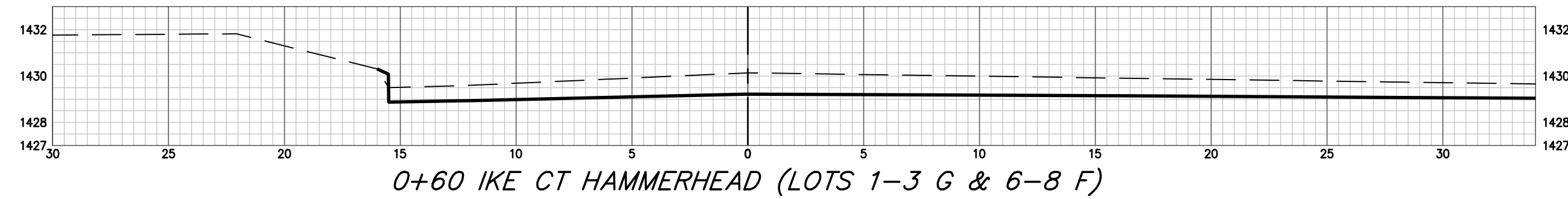
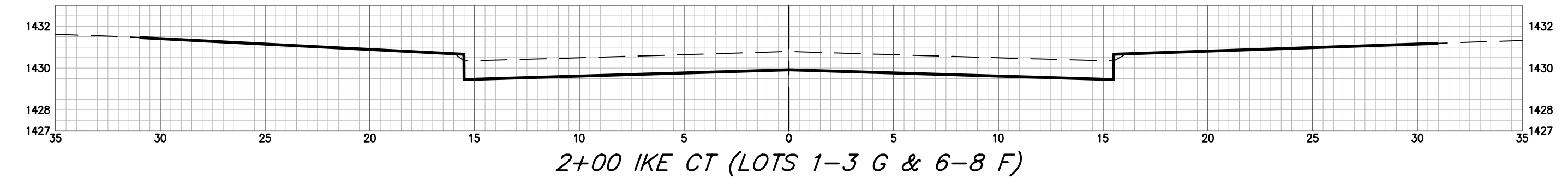
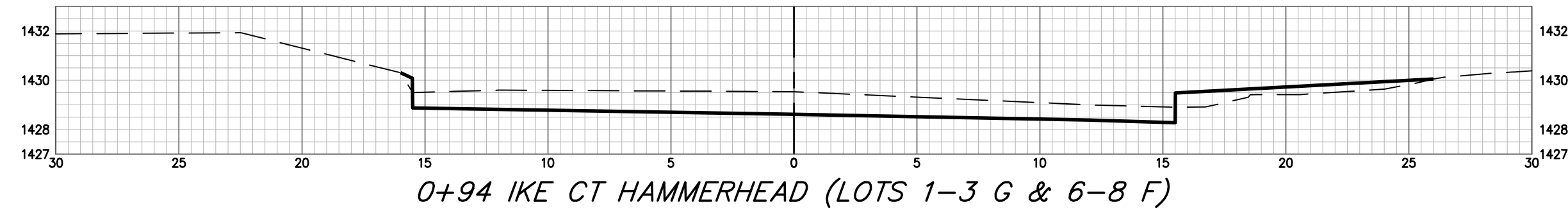
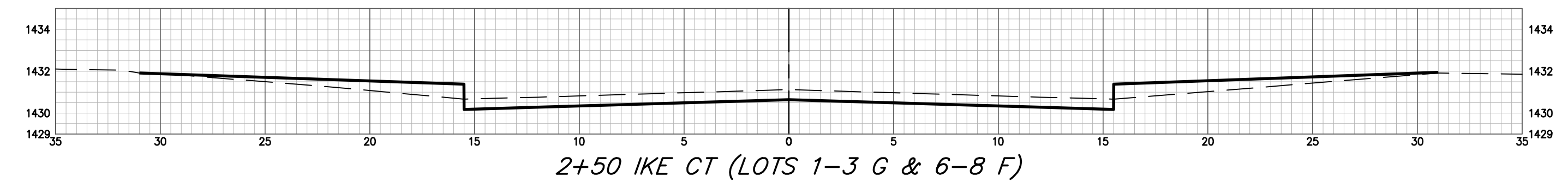
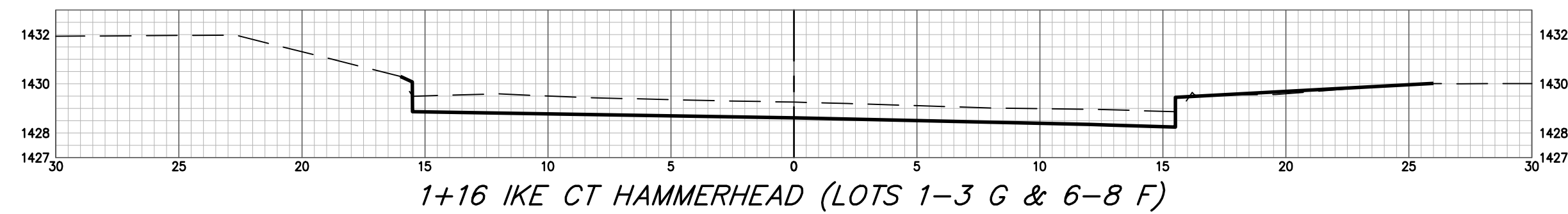
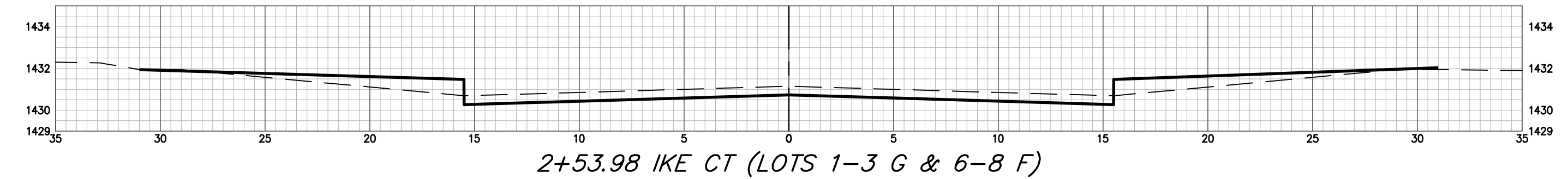
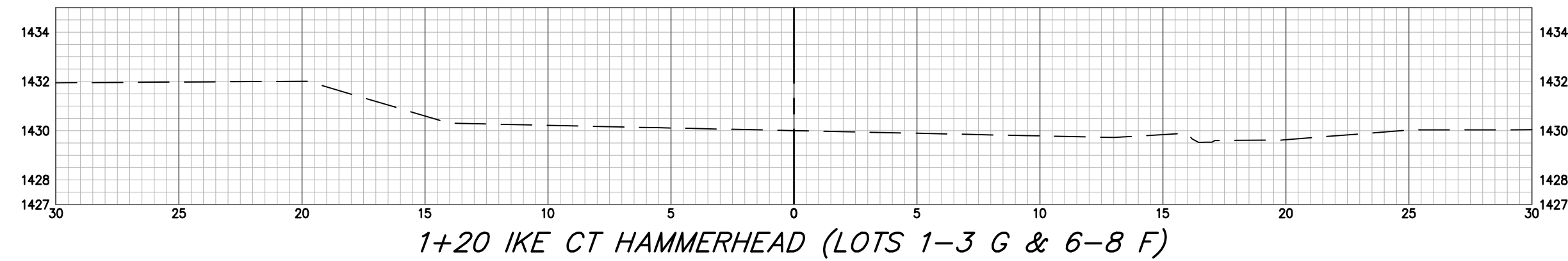
DATE: Apr. 28, 2026

SHEET **16** OF **20**

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

IKE CT HAMMERHEAD (LOTS 1-3 G & 6-8 F) CY Excavation (unadjusted)				IKE CT HAMMERHEAD (LOTS 1-3 G & 6-8 F) CY Loose Fill (unadjusted)			
Station	Area	Volume	Total	Station	Area	Volume	Total
0+00	0.00	0.00	0.00	0+00	0.00	0.00	0.00
0+04	20.30	1.50	1.50	0+04	0.00	0.00	0.00
0+26	24.05	18.07	19.58	0+26	1.74	0.71	0.71
0+60	37.57	38.80	58.37	0+60	0.14	1.18	1.89
0+94	24.05	38.80	97.17	0+94	3.77	2.46	4.35
1+16	20.24	18.05	115.21	1+16	0.70	1.82	6.17
1+20	0.00	1.50	116.71	1+20	0.00	0.05	6.23

IKE CT (LOTS 1-3 G & 6-8 F) CY Excavation (unadjusted)				IKE CT (LOTS 1-3 G & 6-8 F) CY Loose Fill (unadjusted)			
Station	Area	Volume	Total	Station	Area	Volume	Total
0+34	19.56	0.00	0.00	0+34	7.87	0.00	0.00
0+50	19.93	11.70	11.70	0+50	8.64	4.89	4.89
1+00	22.35	39.15	50.85	1+00	4.37	12.05	16.94
1+50	25.31	44.13	94.98	1+50	0.38	4.40	21.34
2+00	27.21	48.63	143.61	2+00	0.17	0.50	21.84
2+50	14.99	39.08	182.69	2+50	10.60	10.06	31.91
2+53.98	13.05	2.07	184.76	2+53.98	10.37	1.57	33.48



**BAUGHMAN
COMPANY**
315 Ellis St.
Wichita, KS 67211
316-262-7271
BaughmanCo.com

ABILENE PLACE ADDITION
PHASE II

**EARTHWORK
CROSS
SECTIONS**

Street Paving Improvements
PROJECT NUMBER:

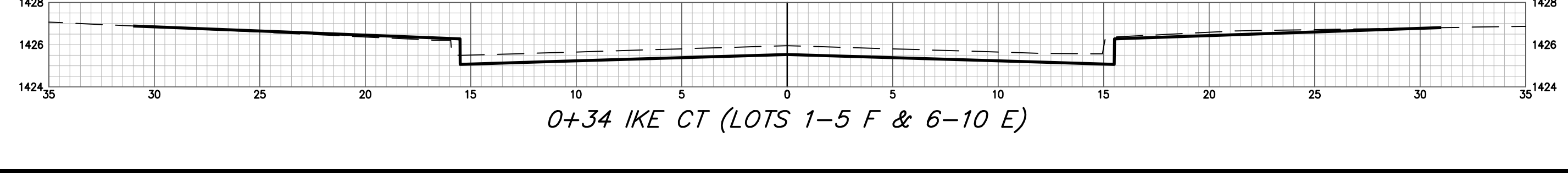
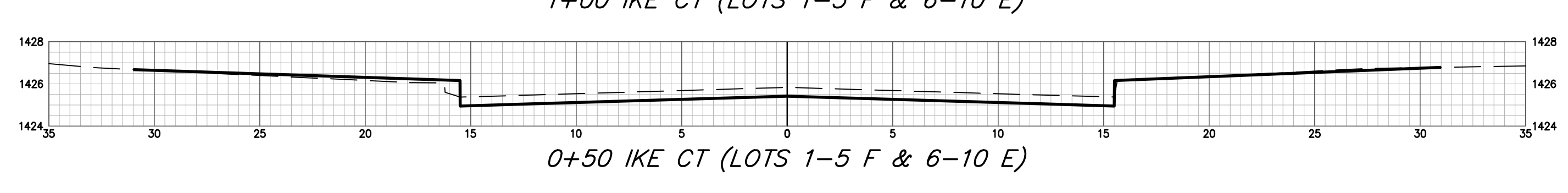
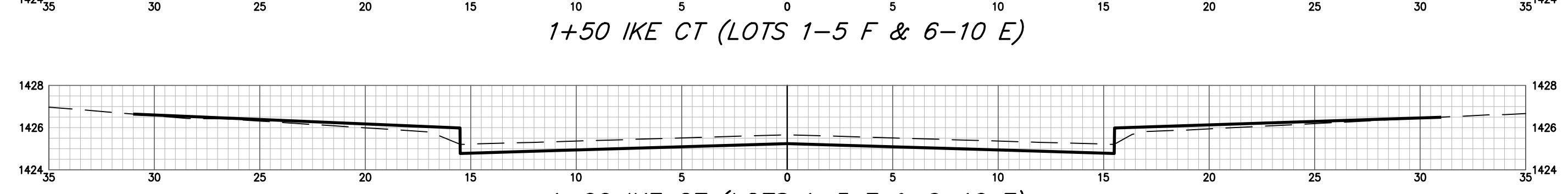
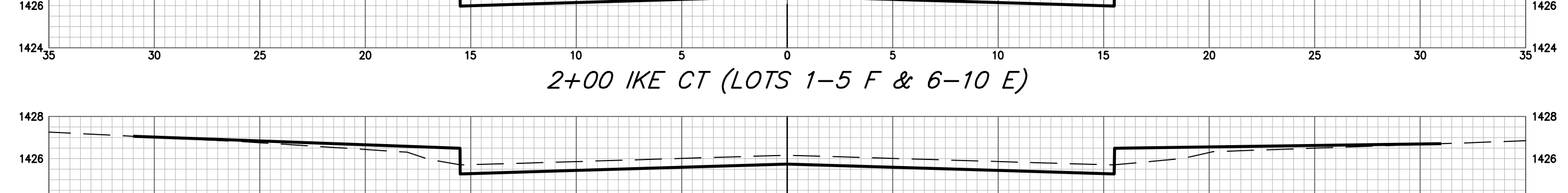
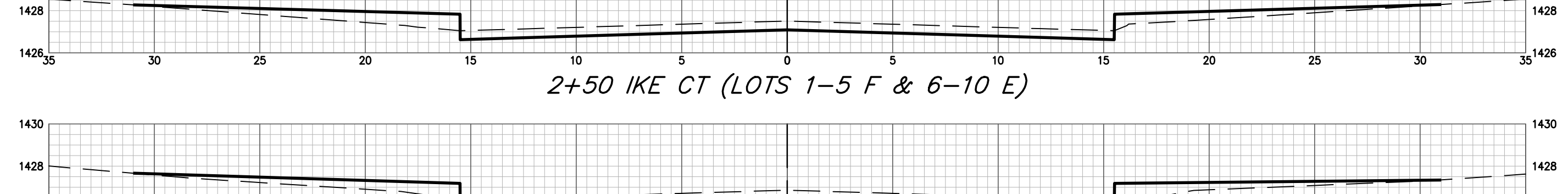
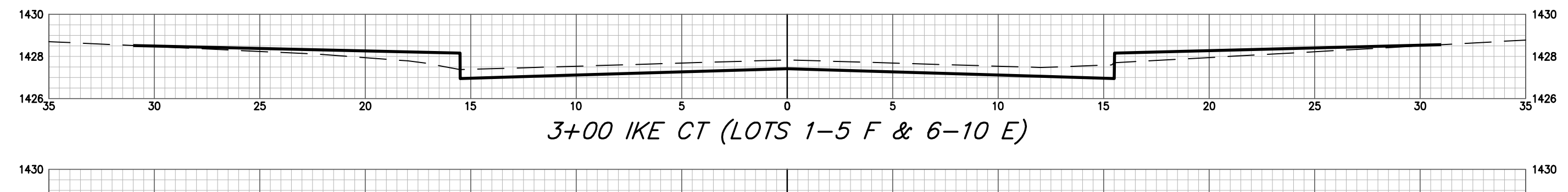
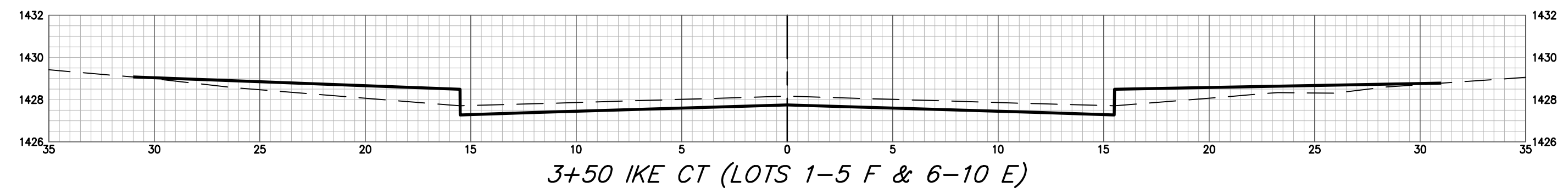
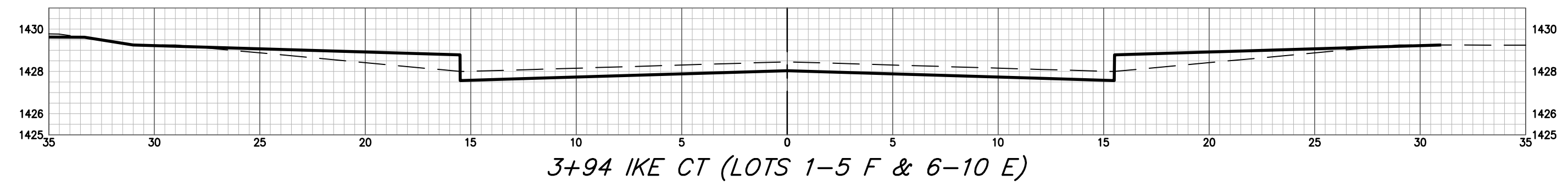
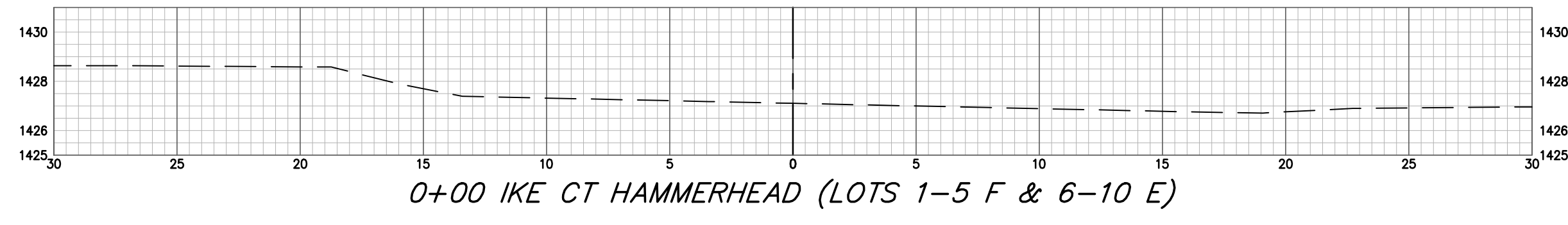
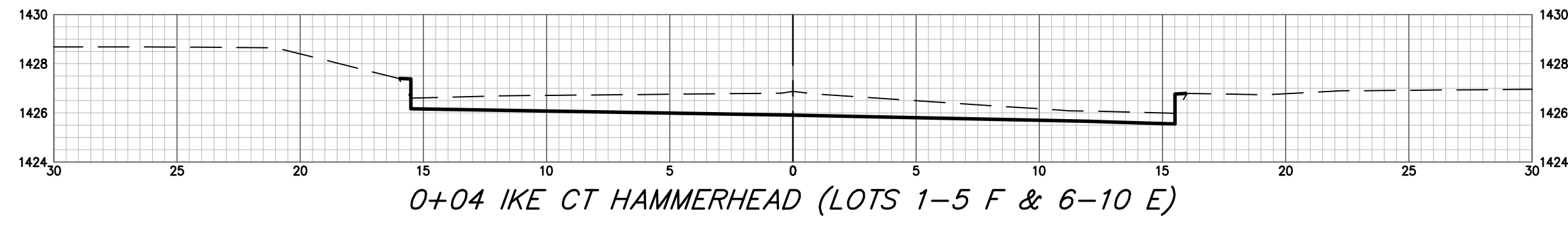
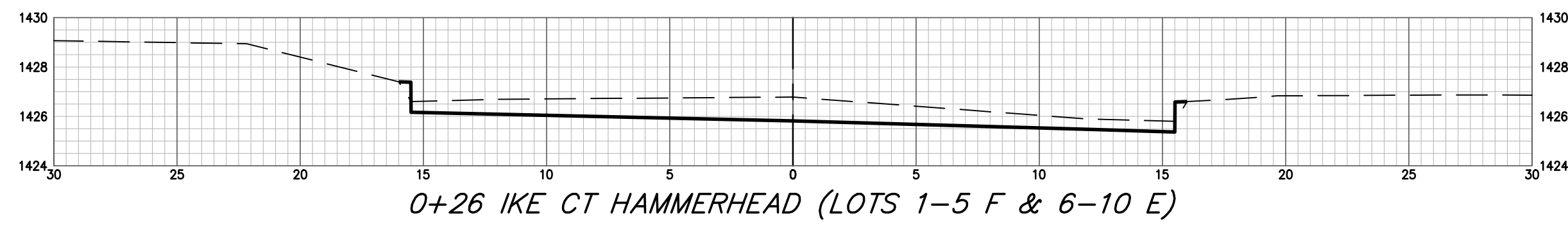
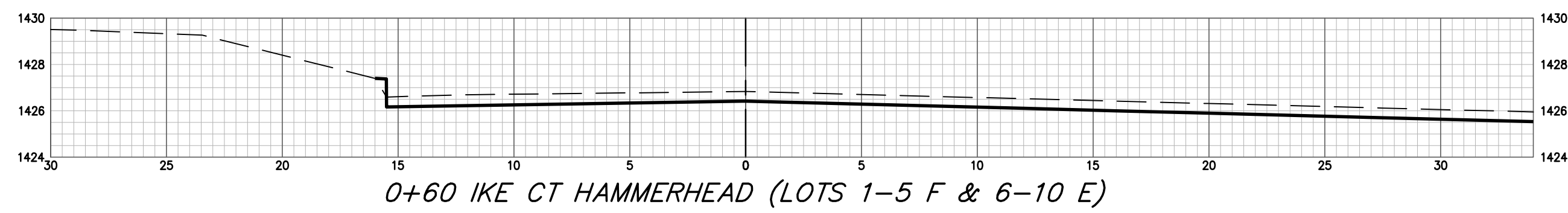
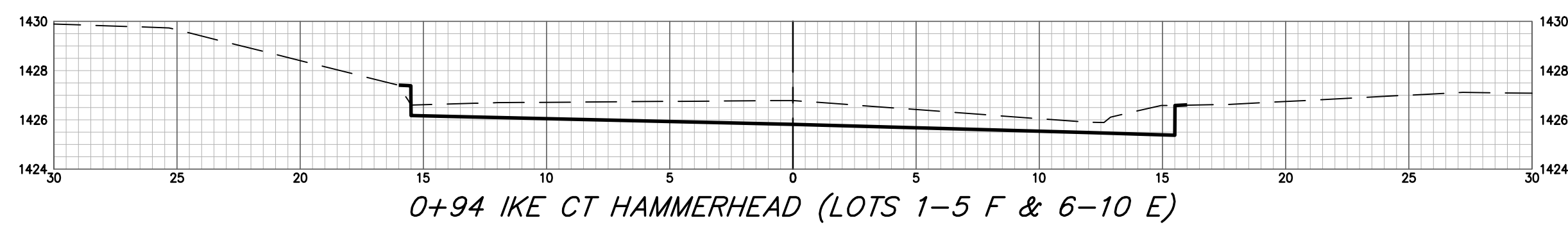
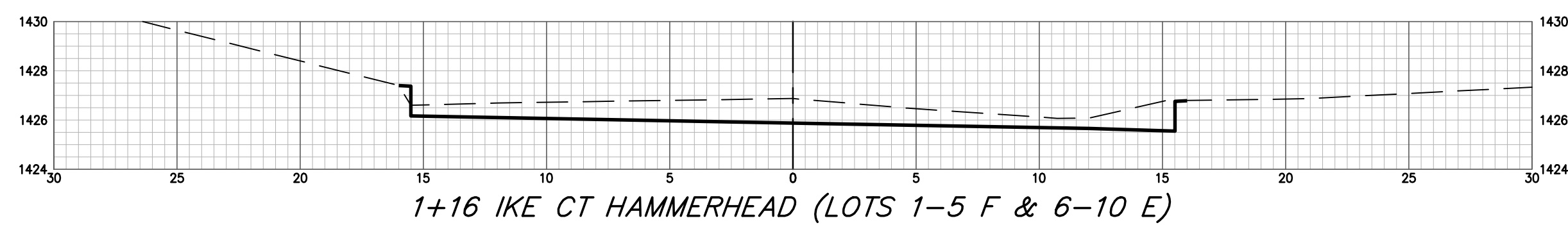
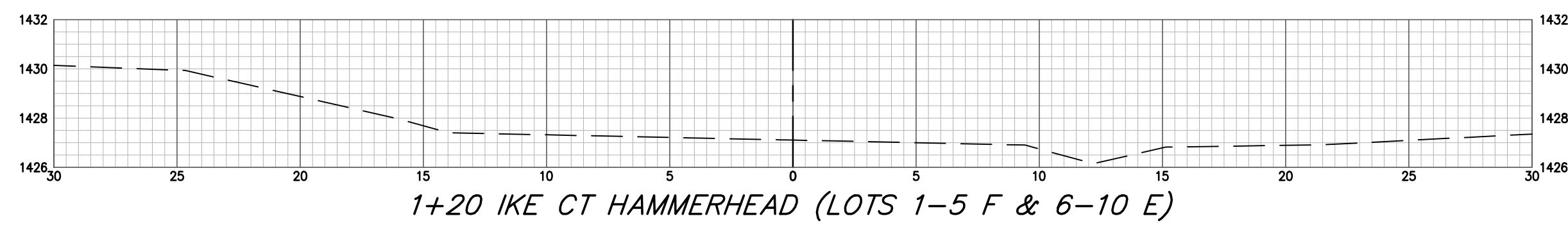
DESIGN: DMV DRAWN: JAK
DATE: Apr. 28, 2026

SHEET **17** OF **20**

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

IKE CT HAMMERHEAD (LOTS 1-5 F & 6-10 E) CY Excavation (unadjusted)				IKE CT HAMMERHEAD (LOTS 1-5 F & 6-10 E) CY Loose Fill (unadjusted)			
Station	Area	Volume	Total	Station	Area	Volume	Total
0+00	0.00	0.00	0.00	0+00	0.00	0.00	0.00
0+04	20.00	1.48	1.48	0+04	0.38	0.03	0.03
0+26	21.02	16.71	18.19	0+26	0.38	0.31	0.34
0+60	21.06	26.50	44.69	0+60	0.19	0.36	0.70
0+94	22.54	27.45	72.14	0+94	0.19	0.24	0.94
1+16	22.04	18.16	90.30	1+16	0.19	0.16	1.09
1+20	0.00	1.63	91.93	1+20	0.00	0.01	1.11

IKE CT (LOTS 1-5 F & 6-10 E) CY Excavation (unadjusted)				IKE CT (LOTS 1-5 F & 6-10 E) CY Loose Fill (unadjusted)			
Station	Area	Volume	Total	Station	Area	Volume	Total
0+34	15.10	0.00	0.00	0+34	1.23	0.00	0.00
0+50	13.49	8.47	8.47	0+50	2.04	0.97	0.97
1+00	13.05	24.58	33.05	1+00	4.78	6.31	7.28
1+50	12.97	24.10	57.15	1+50	6.79	10.72	17.99
2+00	12.97	24.02	81.17	2+00	9.65	15.22	33.21
2+50	12.97	24.02	105.19	2+50	10.03	18.22	51.43
3+00	13.37	24.39	129.58	3+00	7.29	16.04	67.47
3+50	12.97	24.39	153.97	3+50	12.56	18.38	85.85
3+94	13.30	21.40	175.37	3+94	9.83	18.24	104.10



BAUGHMAN COMPANY
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ABILENE PLACE ADDITION
 PHASE II

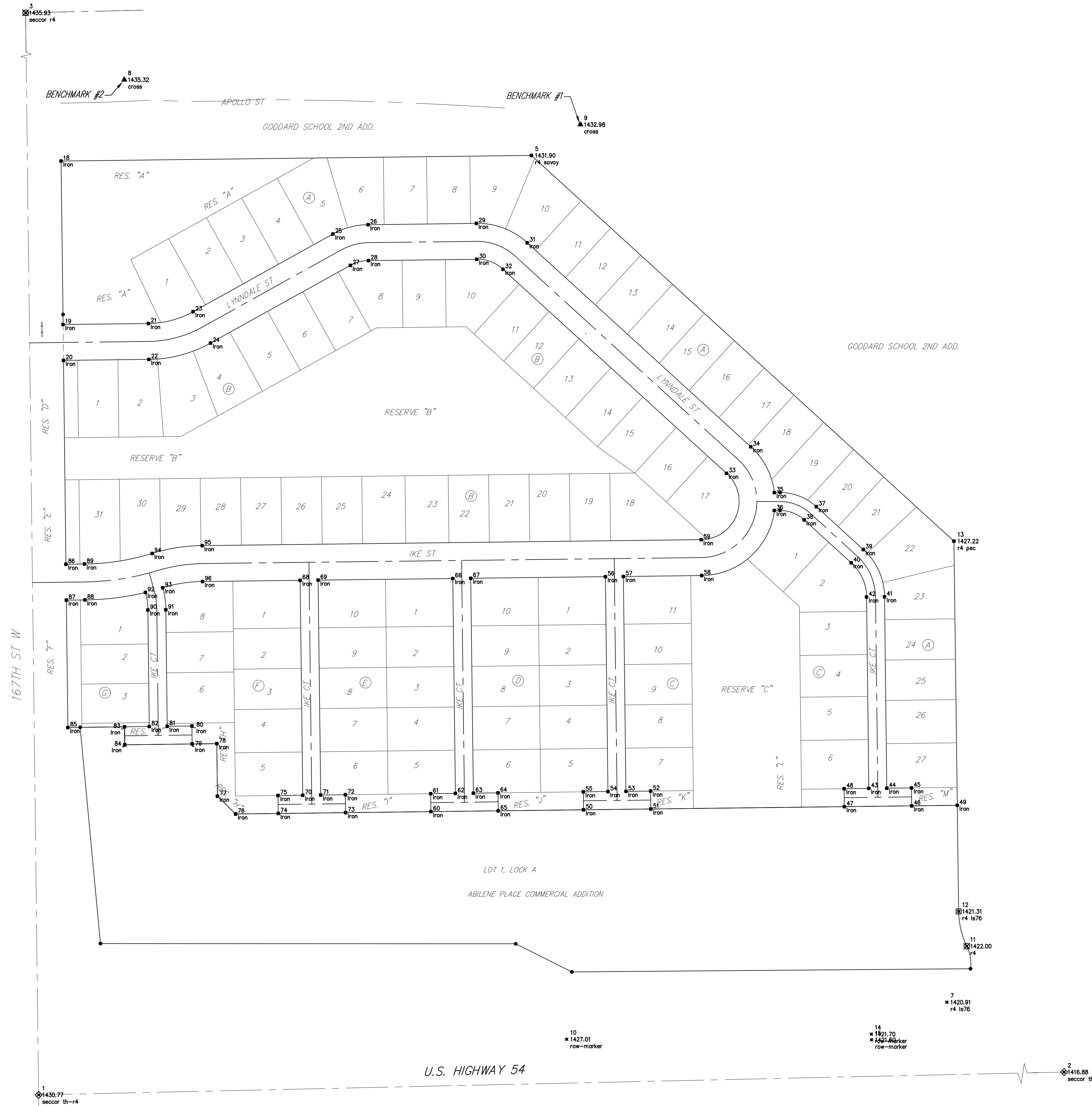
**EARTHWORK
 CROSS
 SECTIONS**

Street Paving Improvements
 PROJECT NUMBER:
 DESIGN: DMV DRAWN: JAK
 DATE: Apr. 28, 2026
 SHEET **18** OF **20**

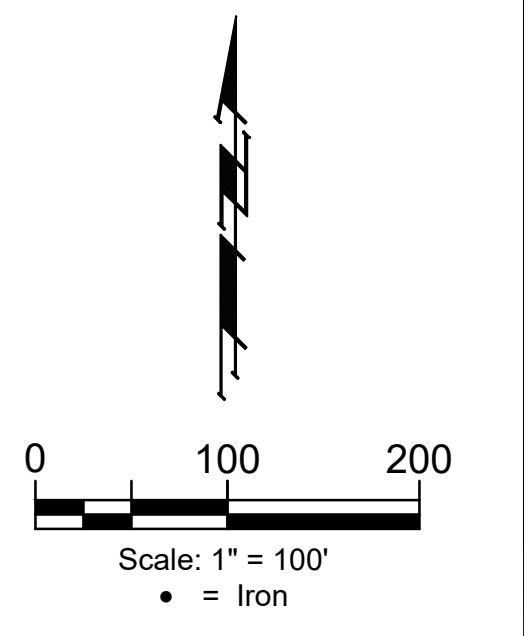
File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

BENCHMARK #1:
Chiseled plus cut inside chiseled square, SE Cor. of catch basin on S. side of Apollo St., 32.5' W. to line of back of east curb of drive north, 11' E. to line of end of curb north, 103.2' ENE. of deflection cor., N. Line. Lot 9, Block A, Abilene Place Add. Elev. = 1432.98 NAVD88

BENCHMARK #2:
Chiseled plus cut inside chiseled square, top of curb, most southerly SW Cor. of parking lot, 17.6' S. to back of N. curb of Apollo St., 34.7' WSW to centerline of light pole base, 114.3' E. & 143.5' N. of NW Cor., Reserve "A", Abilene Place Add. Elev. = 1435.32 NAVD88



Point #	Northing	Easting	Description
1	1,676,957.80	1,591,694.91	seccor th-r4
2	1,677,016.42	1,594,327.73	seccor th-r4
3	1,679,598.84	1,591,663.25	seccor r4
9	1,678,687.37	1,592,660.29	cross
10	1,677,056.97	1,592,635.81	row-marker
20	1,678,266.95	1,591,739.22	Iron
21	1,678,332.77	1,591,890.64	Iron
22	1,678,268.77	1,591,891.41	Iron
23	1,678,353.89	1,591,970.20	Iron
24	1,678,297.94	1,592,001.27	Iron
30	1,678,447.19	1,592,475.72	Iron
56	1,677,881.49	1,592,704.91	Iron
57	1,677,881.87	1,592,736.91	Iron
58	1,677,883.55	1,592,876.42	Iron
59	1,677,947.54	1,592,875.65	Iron
60	1,677,462.73	1,592,393.90	Iron
61	1,677,494.73	1,592,393.52	Iron
62	1,677,495.26	1,592,437.52	Iron
63	1,677,495.64	1,592,469.52	Iron
64	1,677,496.17	1,592,513.51	Iron
66	1,677,878.23	1,592,432.93	Iron
67	1,677,878.62	1,592,464.93	Iron
68	1,677,874.97	1,592,160.95	Iron
69	1,677,875.36	1,592,192.95	Iron
70	1,677,492.00	1,592,165.54	Iron
71	1,677,492.38	1,592,197.54	Iron
72	1,677,492.91	1,592,241.53	Iron
73	1,677,460.91	1,592,241.92	Iron
74	1,677,459.47	1,592,121.92	Iron
75	1,677,491.47	1,592,121.54	Iron
76	1,677,458.56	1,592,045.93	Iron
77	1,677,490.18	1,592,013.55	Iron
78	1,677,583.89	1,592,012.43	Iron
79	1,677,583.37	1,591,968.43	Iron
80	1,677,615.36	1,591,968.05	Iron
81	1,677,614.84	1,591,924.05	Iron
82	1,677,614.45	1,591,892.05	Iron
83	1,677,613.93	1,591,848.05	Iron
84	1,677,581.93	1,591,848.44	Iron
85	1,677,612.72	1,591,747.06	Iron
86	1,677,903.70	1,591,743.57	Iron
87	1,677,839.70	1,591,744.34	Iron
88	1,677,840.10	1,591,777.43	Iron
89	1,677,904.09	1,591,776.66	Iron
90	1,677,822.12	1,591,889.56	Iron
91	1,677,822.50	1,591,921.56	Iron
92	1,677,853.37	1,591,885.19	Iron
93	1,677,861.78	1,591,916.07	Iron
94	1,677,922.95	1,591,897.26	Iron
95	1,677,936.88	1,591,986.35	Iron
96	1,677,872.89	1,591,987.12	Iron



BAUGHMAN COMPANY
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Wichita, KS 67211
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ABILENE PLACE ADDITION
PHASE II

COORDINATES

Street Paving Improvements
PROJECT NUMBER:
DESIGN: DMV DRAWN: JAK
DATE: Apr. 28, 2026
SHEET 19 OF 20

File: E:\Projects\Abilene Place Addition_20-03-P540\Engineering\Phase 2\STR_PLANS.dwg

ABILENE PLACE ADDITION WICHITA, SEDGWICK COUNTY, KANSAS

NOTE:
 ALL LOTS WITHIN ABILENE PLACE
 ADDITION SHALL HAVE A 5 FOOT
 MINIMUM INTERIOR SIDEYARD SETBACK.

- = #4 REBAR W/ "BAUGHMAN" CAP (SET)
- = #4 REBAR W/ "SAVOY" CAP (FOUND)
- = #4 REBAR W/ "PEC" CAP (FOUND)
- = #5 REBAR W/ "LS-76" CAP (FOUND)
- ◇ = #4 REBAR (FOUND) (ORIGIN UNKNOWN)
- ◇ = #4 REBAR IN THIMBLE (FOUND)

- (M) = MEASURED
- (D) = DESCRIBED
- (R) = RECORD MEASUREMENT
- (CM) = CALCULATED FROM MEASURED INFO.
- (CD) = CALCULATED FROM DESCRIBED INFO.
- (CS) = FLATTED INFO. FROM GODDARD SCHOOL 2ND ADDITION
- (CDS) = CALCULATED INFO. FROM GODDARD SCHOOL 2ND ADDITION
- (CWN) = CALCULATED INFO. FROM NEVILLE WEST ADDITION

NOTE:
 ADDITIONAL BUILDING SETBACK REQUIREMENTS
 AND RESTRICTIONS PER NEVILLE PROPERTY
 COMMUNITY UNIT PLAN DP-241.

LOT	BLOCK	ELEVATION NAVD88
1-16, 18-31	B	1430.1
1-11	C	1426.0

BENCHMARK #1:
 CHISELED PLUS CUT INSIDE CHISELED SQUARE, SE. COR. OF
 CATCH BASIN ON S. SIDE OF APOLLO ST., 32.5' N. TO LINE
 OF BACK OF EAST CURB OF DRIVE NORTH, 11' E. TO LINE
 OF END OF CURB NORTH, 103.2' ENE. OF DEFLECTION COR.
 N. LINE, LOT 8, BLOCK 4, ABILENE PLACE ADD.
 ELEV. = 1432.98 NAVD88

BENCHMARK #2:
 CHISELED PLUS CUT INSIDE CHISELED SQUARE, TOP OF
 CURB, MOST SOUTHERLY SW. COR. OF PARKING LOT, 17.6' S.
 TO BACK OF N. CURB OF APOLLO ST., 34.7' NSW TO
 CENTERLINE OF LIGHT POLE BASE, 114.3' E. & 143.5' N. OF
 HW. COR., RESERVE "A", ABILENE PLACE ADD.
 ELEV. = 1435.32 NAVD88

of June 9th day
 of June 2021 at 11:13 o'clock A.M. and is duly recorded.

This is to certify that this plat has been
 filed for record in the office of the Register of Deeds, this 11th day
 of June, 2021 at 11:13 o'clock A.M. and is duly recorded.

Tonya Buckingham, Register of Deeds

Nancy Zehring, Deputy

State of Kansas) SS We, Baughman Company, P.A., Surveyors in
 Sedgwick County) aforesaid county and state do hereby certify that we have surveyed and
 platted "ABILENE PLACE ADDITION", Wichita, Sedgwick County, Kansas and
 that the accompanying plat is a true and correct exhibit of the property
 surveyed, described as Reserves A and C and Lots 1, 2, 3, 4, and 5, Block
 1 and Lots 1, 2, 3, and 4, Block 2, Neville West Addition, Wichita, Sedgwick
 County, Kansas, EXCEPT that part of Lots 3 and 4 in said Block 2, lying
 East of a line described as beginning at a point on the South line of Lot 4,
 in said Block 2, S89°22'10"E, 11.55 feet from the Southwest corner thereof
 (said point being on the West line of Lot 1, Block 1, Goddard School 2nd
 Addition, an Addition to Wichita, Sedgwick County, Kansas); thence
 N00°29'16"E, along the West line of said Goddard School 2nd Addition,
 357.73 feet to a point on the Northeastly line of Lot 3, in said Block 2
 for a point of termination, TOGETHER with that part of the Southwest
 Quarter of Section 27, Township 27 South, Range 2 West of the 6th P.M.,
 Sedgwick County, Kansas, described as beginning at a point on the West
 line of said Southwest Quarter, said point being N00°32'21"E, 1390.55 feet
 of the Southwest Corner of said Southwest Quarter (said point being the
 Northwest Corner of Neville West Addition, Wichita, Sedgwick County,
 Kansas); thence N00°32'21"E, along the West line of said Southwest Quarter,
 273.39 feet; thence S89°27'39"E, 898.12 feet; thence S46°22'09"E, 439.02
 feet to the Northerly most corner of Lot 1, Block 2, in said Neville West
 Addition; thence S43°37'51"W, along the Northwesterly line of Lot 1, in said
 Block 2, 250 feet to the Westerly most corner of Lot 1, in said Block 2;
 thence Northwesterly and Westerly, along the Northerly line of Woodbine
 Circle as platted in said Neville West Addition with the following chords and
 bearings: N46°22'09"W, 297.73 feet to the P.C. of a curve to the left,
 having a radius of 182 feet; thence Northwesterly, along said curve and
 through a central angle of 43°05'30", 136.88 feet to the P.T. of said curve;
 thence N89°27'39"W, 201.17 feet to the P.C. of a curve to the left, having
 a radius of 182 feet; thence Southwesterly, along said curve and through a
 central angle of 29°37'15", 94.09 feet to the P.R.C. of a curve to the right,
 having a radius of 150 feet; thence Westerly, along said curve and through
 a central angle of 29°37'15", 77.55 feet to the P.T. of said curve; thence
 N89°27'39"W, 340.91 feet to the place of beginning, subject to
 right-of-way for 167th Street West, TOGETHER with that part of the
 Southwest Quarter of Section 27, Township 27 South, Range 2 West of the
 6th P.M., Sedgwick County, Kansas, described as beginning at the
 Southwest Corner of Lot 4, Block 2, Neville West Addition, Wichita, Sedgwick
 County, Kansas; thence S89°22'10"E, along the South line of Lot 4, in said
 Block 2, 11.55 feet; thence S00°29'16"W, 63.77 feet to a point on the East
 line of Woodbine Circle as platted in said Neville West Addition, said point
 being the P.C. of a curve, having a radius of 182 feet; thence Northerly,
 along the East line of said Woodbine Circle and said curve to the left,
 through a central angle of 20°31'09", 65.18 feet to the place of beginning,
 and TOGETHER with that part of Lots 6, 7, and 11 in said Block 1, that
 part of Reserve "B", and that part of Woodbine Circle, all as platted and
 dedicated in said Neville West Addition, lying generally north of and abutting
 the following described line: Commencing at the southwest corner of said
 Southwest Quarter; thence N00°41'12"W coincident with the west line of
 said Southwest Quarter, 654.24 feet; thence N89°18'48"E, 60.00 feet to the
 intersection with the east right-of-way line of 167th Street West as
 dedicated in said Neville West Addition, (said intersection also being a point
 on the west line of said Lot 1), and for a point of beginning; thence
 continuing N89°18'48"E into said Lot 11, 101.00 feet; thence S00°41'12"E,
 32.00 feet; thence N89°18'48"E, 164.00 feet; thence S00°41'12"E, 93.72
 feet; thence S45°41'12"E, 45.25 feet; thence N89°18'48"E, 580.82 feet to
 the intersection with a lot line common to said Lot 11 and said Reserve
 "B"; thence continuing N89°18'48"E into said Reserve "B", 402.63 feet to
 the intersection with the lot line common to said Reserve "B" and said Lot
 6; thence continuing N89°18'48"E into said Lot 6, 56.56 feet to the
 intersection with the lot line common to said Lots 6 and 7; thence
 continuing N89°18'48"E into said Lot 7, 202.03 feet to the intersection the
 east line of said Lot 7, said intersection being 120.15 feet north of the
 southeast corner of said Lot 7, said intersection also being a point on the
 west right-of-way line of said Woodbine Circle; thence continuing
 N89°18'48"E into said Woodbine Circle, 64.00 feet to the intersection with
 the east right-of-way line of said Woodbine Circle, and for a point of
 termination.

Existing public easements, building setback lines,
 dedications, and access controls, if any, being
 vacated by virtue of K.S.A. 12-512b, as amended.
 All being situated in the Southwest Quarter of
 Section 27, Township 27 South, Range 2 West of the
 Sixth Principal Meridian, Sedgwick County, Kansas.
 Baughman Company, P.A.

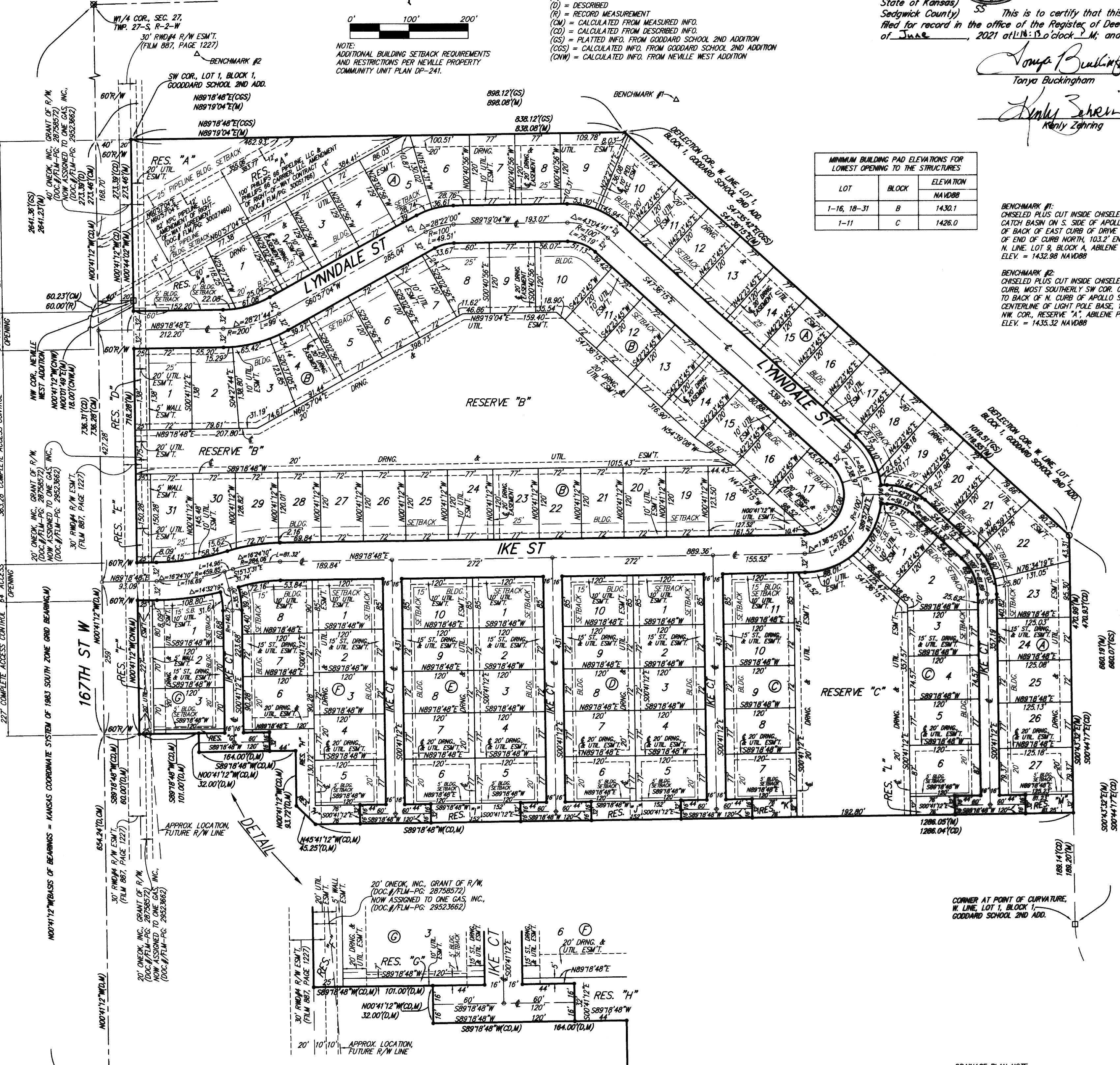
This plot approved and all dedications
 shown hereon accepted by the City Council of the City of Wichita,
 Kansas, this 1st day of June, 2021.

Reviewed in accordance with K.S.A. 58-2005
 on this 4th day of June, 2021.

We, the undersigned holders of a mortgage on the
 above described property, do hereby consent to this plat of "ABILENE
 PLACE ADDITION", Wichita, Sedgwick County, Kansas.
 Legacy Bank

State of Kansas) SS The foregoing instrument acknowledged before
 Sedgwick County) me, this 26th day of April, 2021, by Roy Reynolds,
 SVP of Legacy Bank, on behalf of the bank.

My App't. Exp. 9-26-2021



U.S. HIGHWAY 54
 CONDEMNATION CASE NO. A-38302

Drainage Plan Note:
 A master drainage plan has been developed for this plat. All
 drainage easements, rights-of-way, and reserves shall remain at
 established grades (unless modified with the approval of the City
 Engineer) and shall be unobstructed to allow for the conveyance of
 stormwater in accordance with the Stormwater Manual. The
 maintenance of all drainages and drainage facilities in backyard
 drainage easements and reserves shall be the responsibility of the
 property owner, and shall be enforced by the Homeowners' Association
 and be provided for in the Homeowners' Association covenants.