

CITY OF WICHITA, KANSAS
SEDGWICK COUNTY, KANSAS

STREET IMPROVEMENTS
PHASE V-A

SOUTH SIDE SHERIAC - PRESCOTT (WEST END OF EXISTING PAVEMENT) TO PRESCOTT
PRESCOTT - SHERIAC TO N.L. LOT 7, BLOCK 1 & N.L. LOT 1, BLOCK 4
DOUGLAS - E.L. OF PRESCOTT TO E.L. LOT 12, BLOCK 5
PRESCOTT COURT - W.L. OF PRESCOTT TO A INCLUDING CUL-DE-SAC
SERVING LOTS 8 THROUGH 15 INCLUSIVE, BLOCK 1
PRESCOTT COURT - SE.L. PRESCOTT TO A INCLUDING CUL-DE-SAC
SERVING LOTS 3 THROUGH 7 INCLUSIVE, BLOCK 5

(OAK CLIFF ESTATES 3RD)

CITY OF WICHITA PROJECT NO. 472-76-245-81436-000-000-001

INDEX OF SHEETS

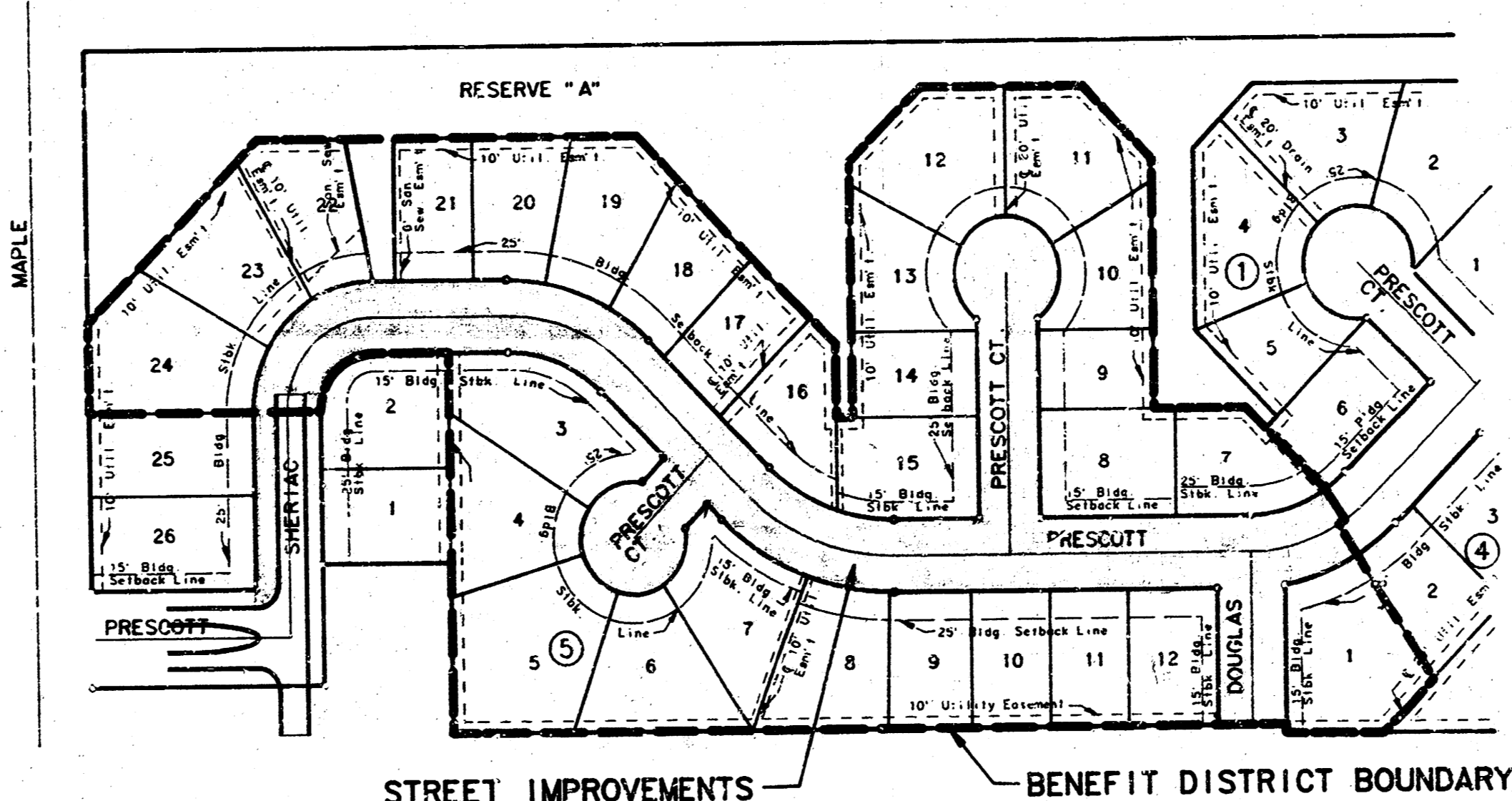
- 1 TITLE SHEET
- 2-3 TYPICAL SECTIONS
- 4 PLAT & SHERIAC TYPICAL SECTION
- 5-9 PLAN SHEETS
- 10-18 CROSS SECTIONS

NOTE: THE MAJORITY OF THIS PROJECT SHALL BE CONSTRUCTED WITH ROLL CURB. WHERE THE PLANS SHOW USE OF ROLL CURB, DEDUCT 0.25 FEET FROM THE TOP OF CURB ELEVATION SHOWN.

EARTHWORK

EXCAVATION	
X-SECTIONS	3,763.0 CU. YDS.
10X	376.3 CU. YDS.
TOTAL	4,139.3 CU. YDS.
MANIPULATED FILL	
X-SECTIONS	14.0 CU. YDS.
10X	1.4 CU. YDS.
TOTAL	15.4 CU. YDS.
6,960 SQ. YDS. MANIPULATION	

SCALE: 1" = 150'



GENERAL NOTES

LOCAL ACCESS TO LOTS 1 AND 2, BLOCK 5 SHALL BE MAINTAINED AT ALL TIMES.

UTILITY SERVICE LINES, POLES, VALVE BOXES, METERS, AND ETCETERA ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO OR DURING CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION AS SHOWN ON THE PLANS REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.

A SAW CUT OF AT LEAST ONE-HALF THE DEPTH OF EXISTING SURFACE COURSES OR ONE-FOURTH THE DEPTH OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ADJUTS AN EXISTING SURFACE COURSE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THAT SURFACE OR PAVEMENT IS REQUIRED. SAWED JOINTS TO FACILITATE REMOVAL WITHIN THREE (3) FEET OF EXISTING JOINTS WILL NOT BE PERMITTED AND FOR SUCH INSTANCES THE LIMITS OF REMOVAL SHALL EXTEND TO THE EXISTING JOINT. SUCH SAW CUTS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE REMOVAL OF THE SURFACE OR PAVEMENT.

RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND PAVEMENT REMOVAL WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE, AND SITE LOCATION. LOCATIONS THAT, IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED.

CITY OF WICHITA FORCES WILL REMOVE AND REPLACE EXISTING STREET SIGNING AS CONSTRUCTION IS STARTED AND COMPLETED. THE CONTRACTOR WILL COORDINATE HIS ACTIVITIES WITH THE DEPARTMENT OF OPERATIONS AND MAINTENANCE RELATIVE TO TEMPORARY TRAFFIC SIGNING AND REMOVAL OR INSTALLATION OF STREET SIGNING.

WIDENED CUTTER SECTION OF COMBINED CURB AND GUTTER WILL NOT BE PAID FOR DIRECTLY, AND THIS COST SHALL BE CONSIDERED AS SUBSIDIARY TO THE OTHER CONTRACT PAY ITEMS OF WORK.

TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR WITH THE ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.

LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH THE EXISTING GROUND SURFACE.

THE CONTRACTOR WILL BE PERMITTED TO BID ONLY ONE OF THE ALTERNATE TYPES OF SUBGRADE TREATMENT. THE TYPE BID BY THE SUCCESSFUL BIDDER WILL BE THE TYPE OF SUBGRADE TREATMENT USED TO CONSTRUCT THE PROJECT.

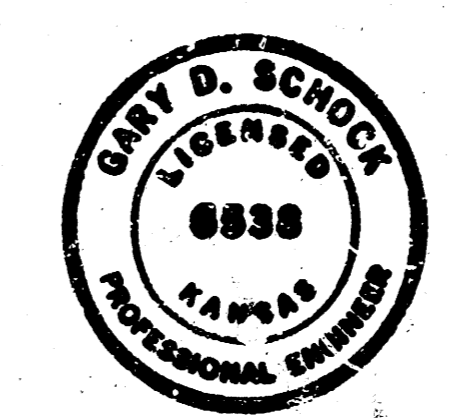
THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.

ADEQUATE TRAFFIC CONTROL SHALL BE PROVIDED ON EXISTING SHERIAC AND PRESCOTT IN THE VICINITY OF NEW CONSTRUCTION DURING CONSTRUCTION.

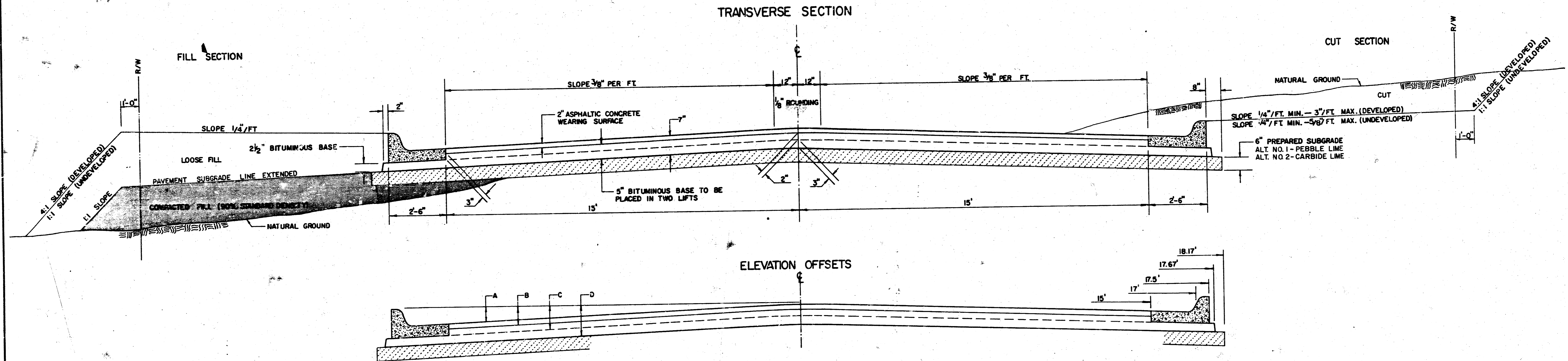
ALL EXCESS EXCAVATED MATERIAL FROM THIS PROJECT SHALL BE WASTED ON SITE. NO EXCESS MATERIAL SHALL BE PLACED WITHIN STREET RIGHTS-OF-WAY. THE CONTRACTOR SHALL CONTACT THE OWNER'S ENGINEER AT 262-2691 FOR INFORMATION PERTAINING TO ACCEPTABLE LOCATIONS FOR THE DISPOSITION OF WASTE MATERIAL. WASTE MATERIAL SHALL BE BLADED SMOOTH AND SLOPED TO DRAIN. THIS WORK SHALL BE SUBSIDIARY TO OTHER BID ITEMS.

MAY, 1985

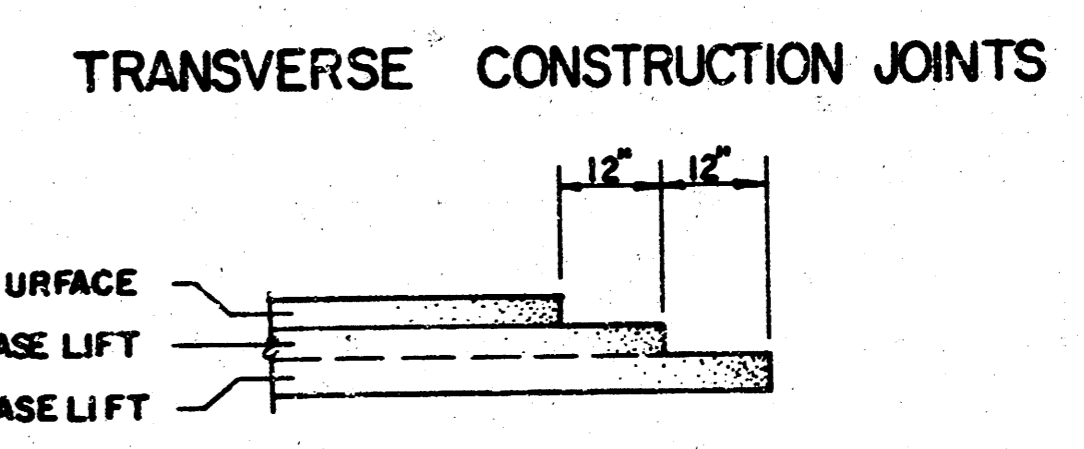
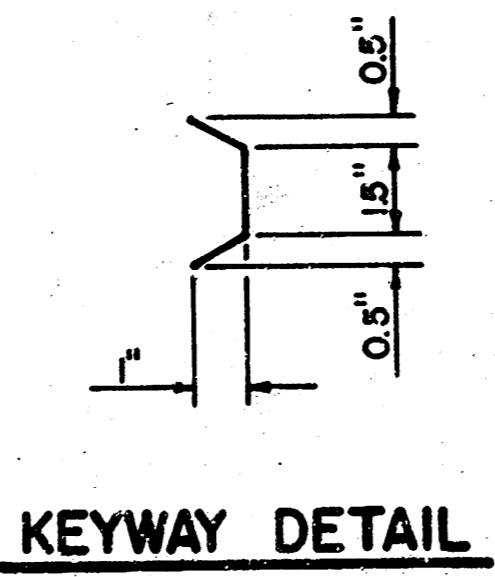
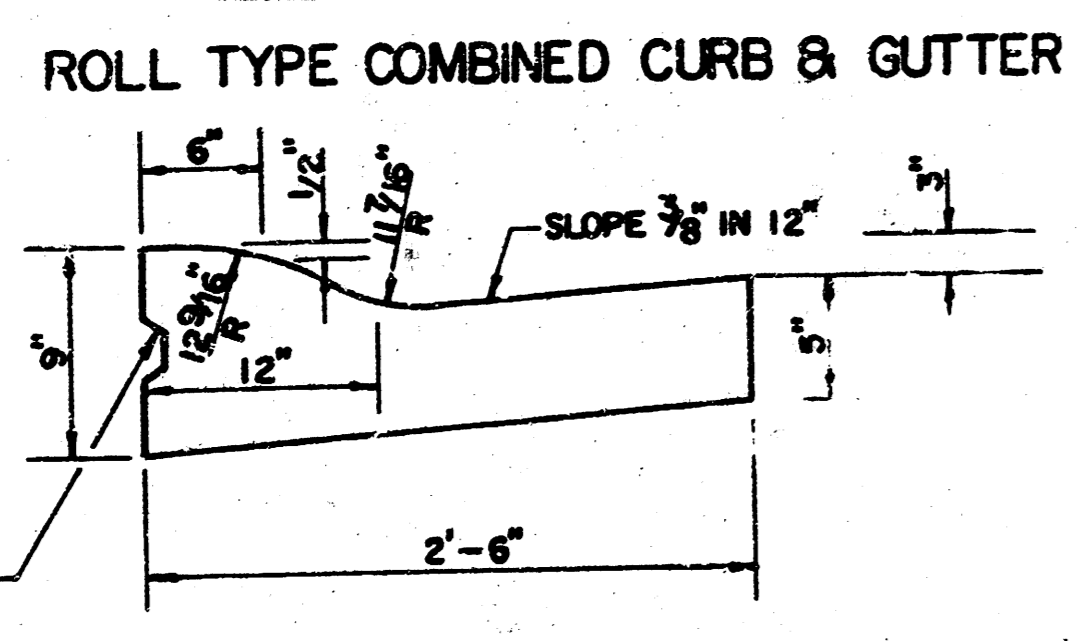
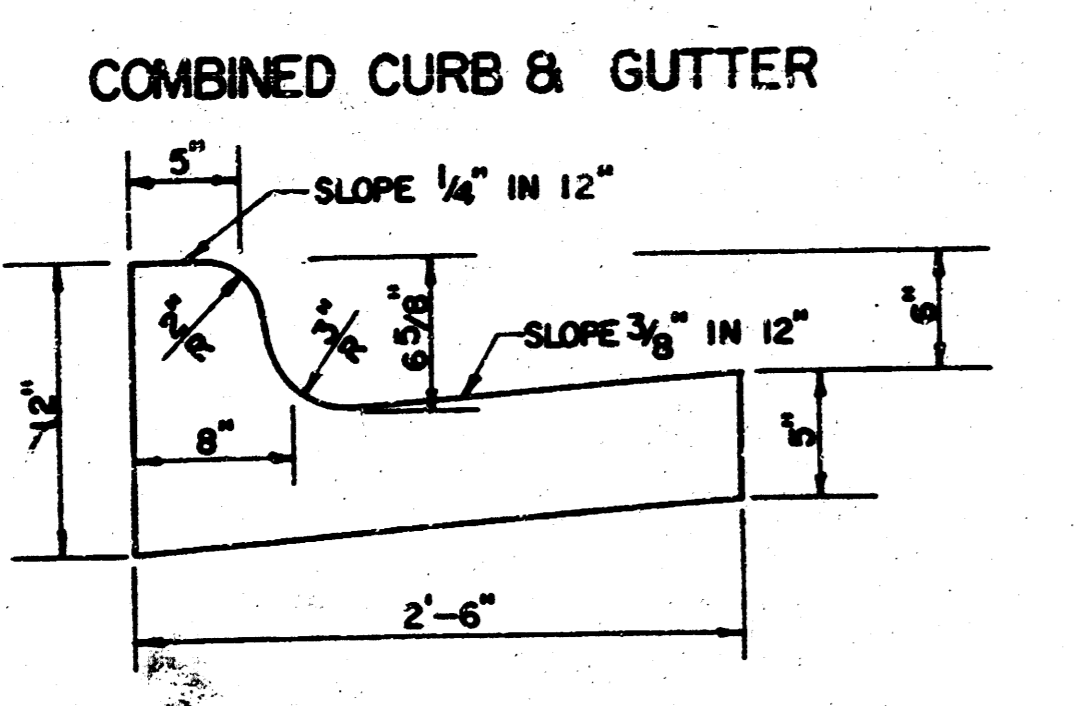
PLANS PREPARED BY
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS



TYPICAL 35' PAVEMENT DETAILS



	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0'	2'	4'	6'	8.5'	10'	12'	14'	15'	17'	17.5'	17.67'	18.17'
A. TOP OF CURBS TO TOP OF SURFACE LIFT	0.04	0.08	0.14	0.21	0.29	0.33	0.39	0.46	0.49	—	—	—	—
B. TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.21	0.25	0.31	0.37	0.45	0.50	0.56	0.62	0.65	—	—	—	—
C. TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.37	0.43	0.50	0.57	0.67	0.72	0.79	0.87	0.90	0.98	1.00	1.00	—
D. TOP OF CURBS TO TOP OF SUBGRADE	0.62	0.67	0.74	0.81	0.90	0.95	1.02	1.08	1.12	1.19	1.21	1.21	1.23



GENERAL NOTES

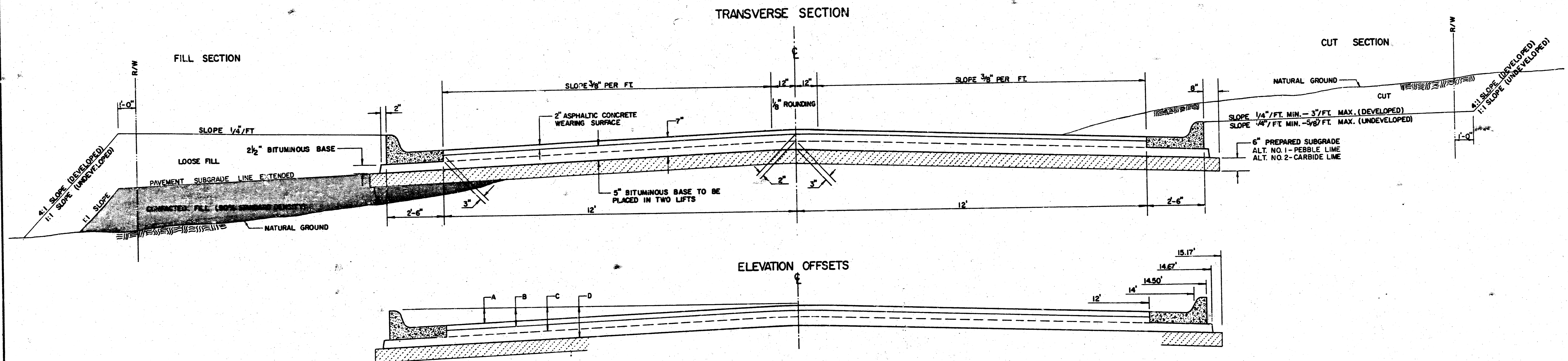
- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LANDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

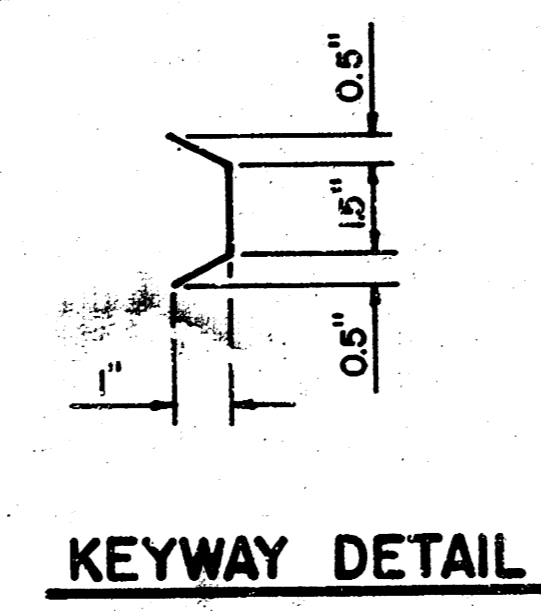
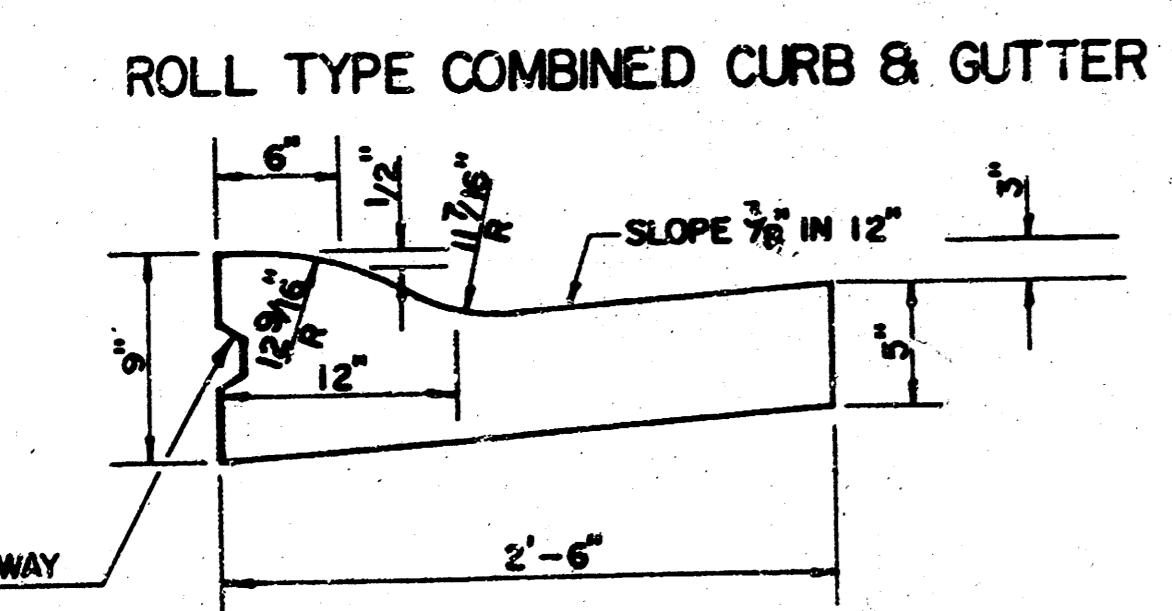
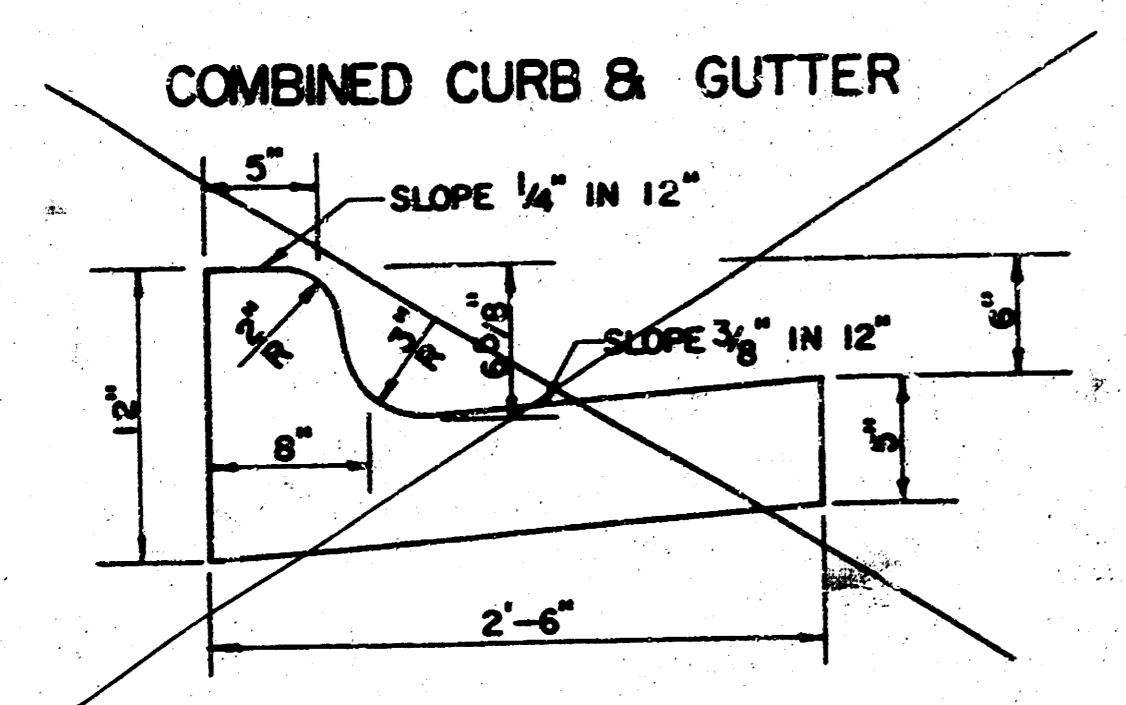
7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
 CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 412 16 245 81436 000 000 001

Oak Cliff

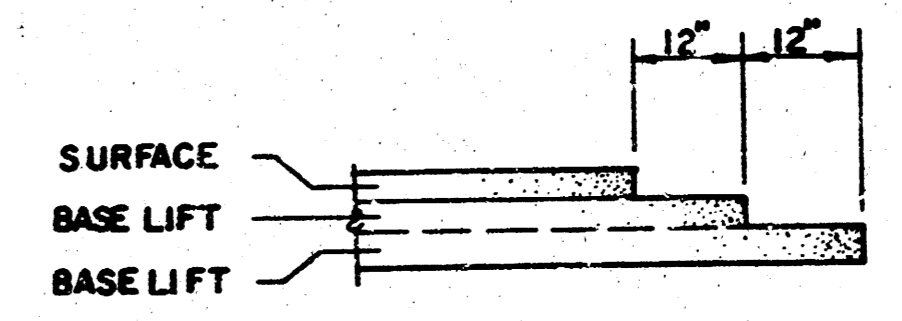
TYPICAL 29' PAVEMENT DETAILS



	DISTANCE FROM CENTERLINE (LT. & RT)											
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	14.67'	15.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.47	0.52	0.60	0.68	0.71	0.75	0.83	0.90	0.98	1.00	1.01	—
D: TOP OF CURBS TO TOP OF SUBGRADE	0.72	0.77	0.84	0.91	0.94	0.98	1.05	1.12	1.19	1.21	1.21	1.23



TRANSVERSE CONSTRUCTION JOINTS



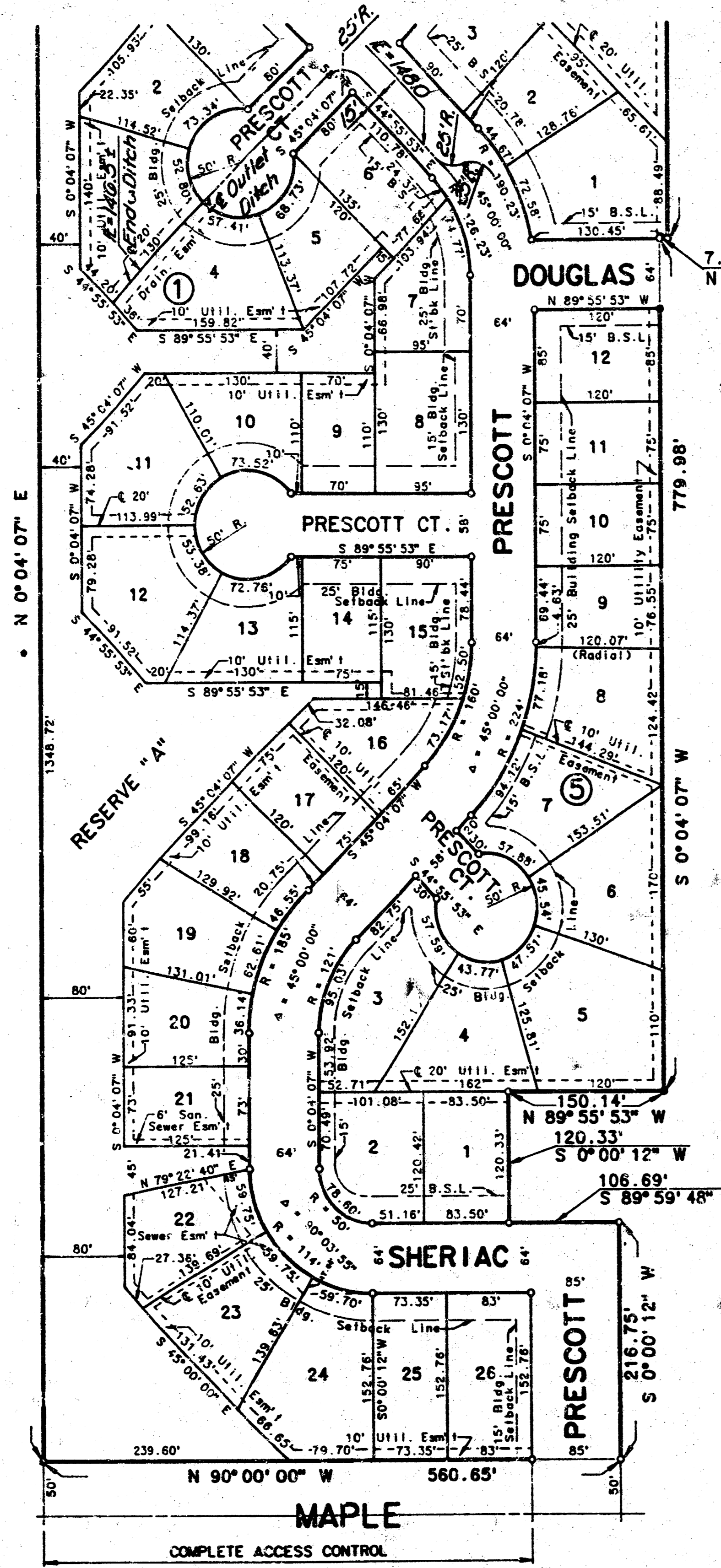
TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

GENERAL NOTES

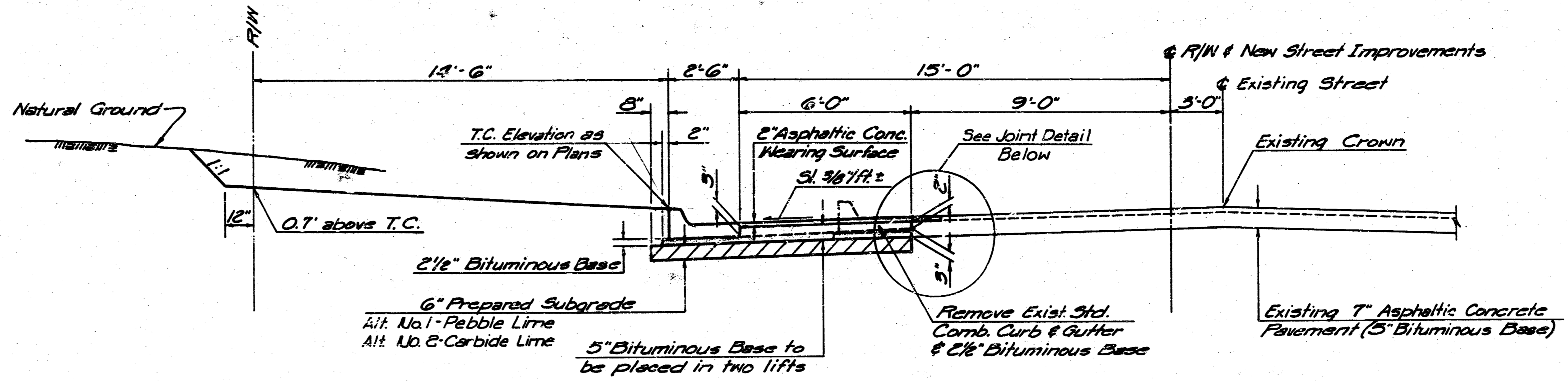
- 1) ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 072 76 265 5/1436 000 000 001

out cliff

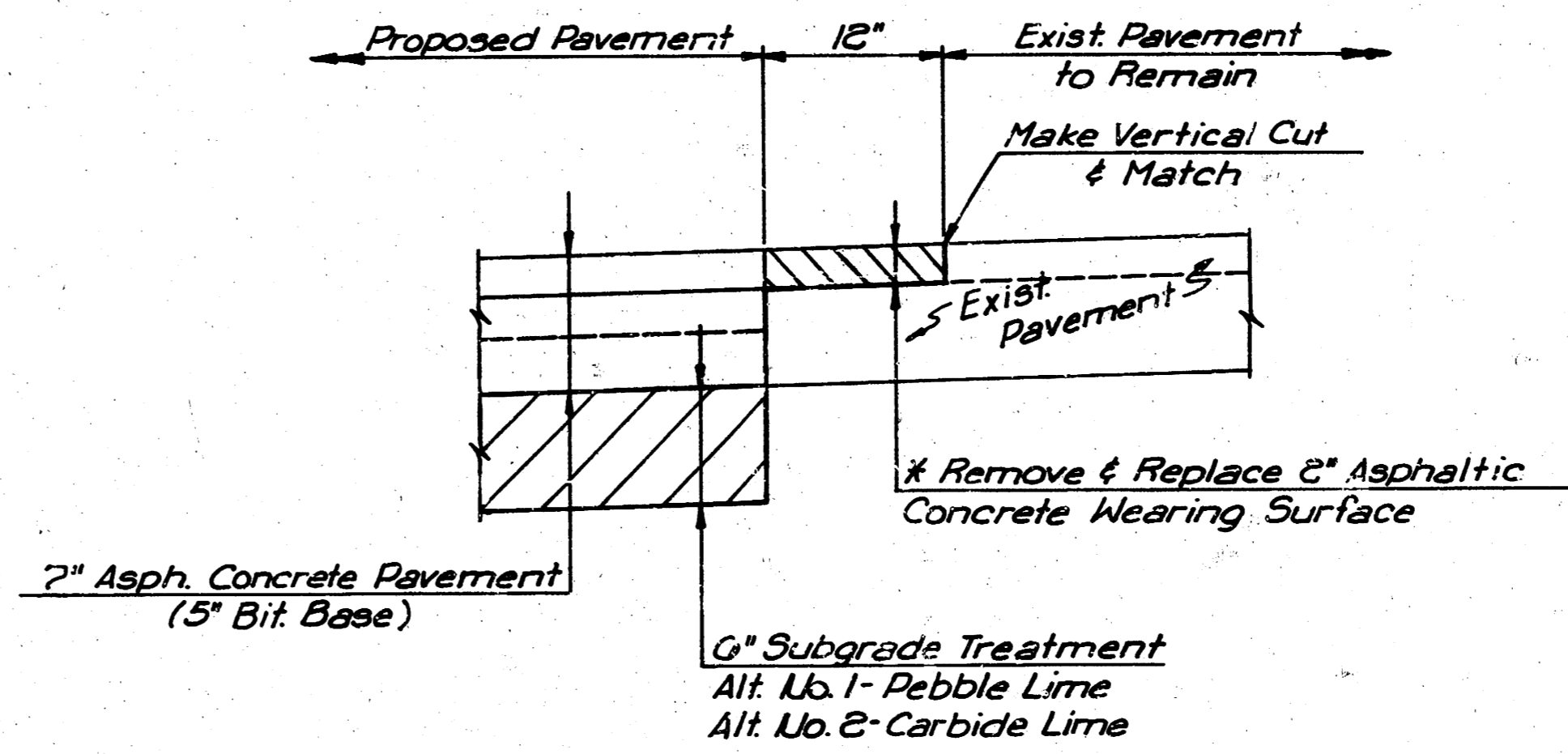


TYPICAL SECTION
OUTLET DITCH



TYPICAL SECTION
@ Sheriac Sta. 8+48.65 to Sta. 10+00

Note: See Sh. No. 2 for
Combined Curb & Gutter
Detail.



JOINT DETAIL

*Immediately prior to placing the 2" Surface Course on the widening, the Contractor shall remove the existing 2" Surface Course along the outside 1'-0" of the existing pavement as shown. Replacement of the surface course removed shall be accomplished in conjunction with the placement of the surface course on the widening. The removal and replacement of the 2" Surface Course shall not be paid for directly, but shall be considered incidental to other paving items.

- Iron Found
- Iron to be Set

Bench Mark - Maple Road and Maple, City of Wichita bench mark disc. 43" high and 52.7" east of E. 5th St. City Datum 1329.60 M.S.L.

B.M.#2 - Chis.'d on N. Curb Sheriac @ E. Return to Drive @ 1100G Sheriac Elev. 145.02
B.M.#5 - Chis.'d on W. Curb Parkdale St @ S. Return Douglas Avenue Elev. 147.74

MINIMUM PAD ELEVATIONS:
Minimum Pad Elevation for Lots 1 through 11, Block 9, shall be 1324.0 M.S.L. and for Lots 1 through 26, Block 1, shall be 1333.0 M.S.L.

Scale: 1" = 100'
October 18, 1984

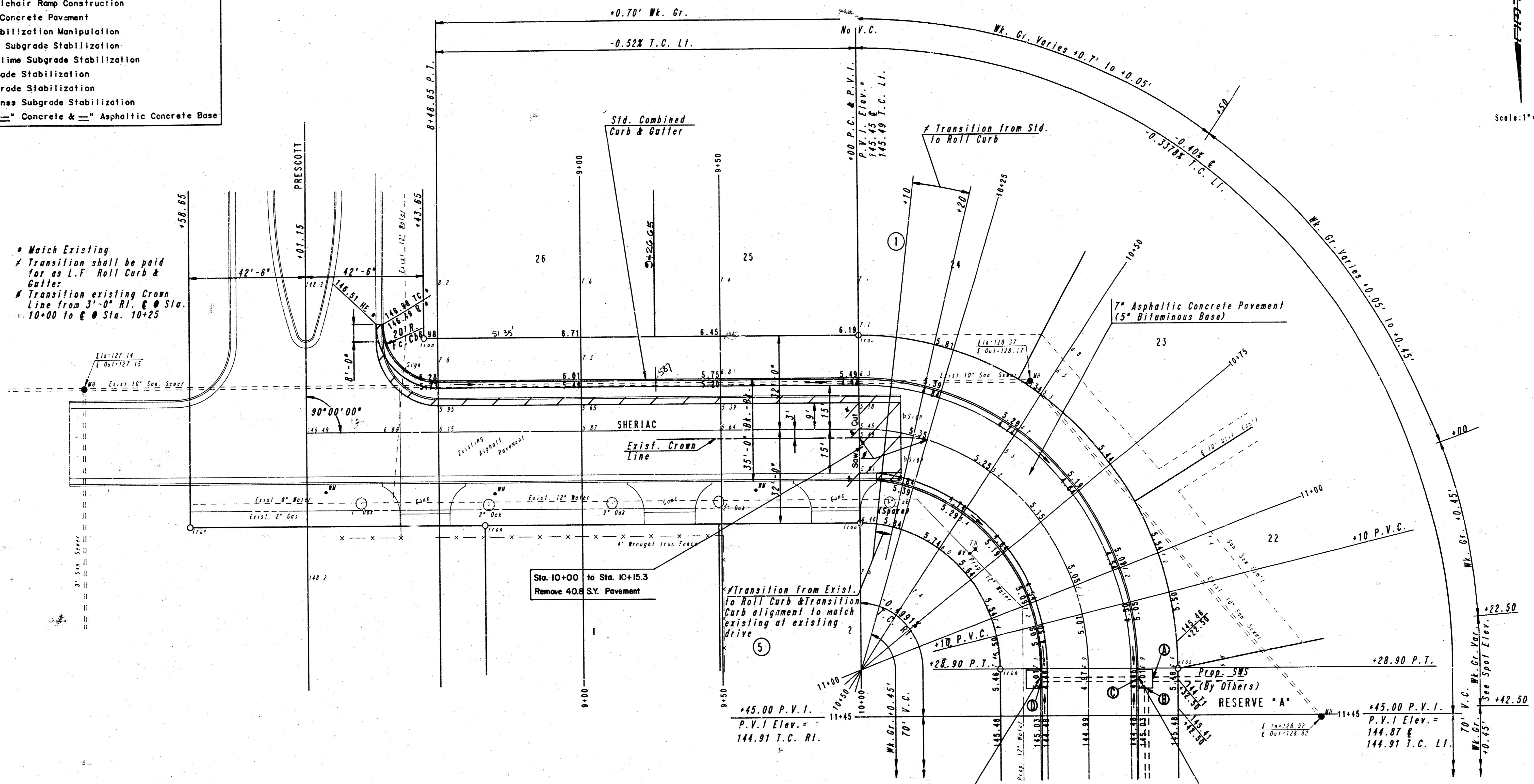
No.	Revision	By	Date
PLAT & SHERIAC TYPICAL SECTION Proj. No. 81276 245 81436 000 000 001 PROFESSIONAL ENGINEERING CONSULTANTS, P.A. ENGINEERS WICHITA, KANSAS			
Designed by	GDD	Job No.	85059
Drawn by	GDD	Date	May, 1985
			Sh. 4 of 13

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INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 14.7 S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)
- 9.6 S.Y. 2 1/2" Bituminous Base
- 31.4 L.F. Combined Curb & Gutter (6 1/2" & 1 1/2")
- L.F. Monolithic Edge Curb (2")
- Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 26.0 S.Y. Subgrade Stabilization Manipulation
- Tons Carbide Lime Subgrade Stabilization
- Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

**BEGIN ROLL CURB
AT STA. 10+20**



* Match Existing
* Transition shall be paid for as L.F. Roll Curb & Gutter
* Transition existing Crown Line from 3'-0" R.I. @ Sta. 10+00 to @ Sta. 10+25

Sta. 10+00 to Sta. 10+15.3
Remove 40E S.Y. Pavement

* Transition from Exist. to Roll Curb & Transition Curb alignment to match existing at existing drive

Sta. 11+32.50 Const. Type IA Inlet Hookup

Sta. 11+32.50 Const. Type IA Inlet Hookup

Q CURVE DATA

Δ=90° 03' 55" D=69° 52' 23" R=82.00' L=128.90' T=82.09' E=34.03'

CURVE DATA BASED ON Q RADIUS Δ/2=45° 01' 57.5"

Q STATION	Q ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	Q TOTAL DEFLECTION
		8' OFF LEFT FACE CURB	8' OFF LEFT FACE CURB		
10+00				0° 00' 00"	0° 00' 00"
+25	25.00'	32.50'	17.31'	8° 44' 02.5"	8° 44' 02.5"
+50	25.00'	32.50'	17.31'	8° 44' 02.5"	17° 28' 05"
+75	25.00'	32.50'	17.31'	8° 44' 02.5"	26° 12' 07.5"
11+00	25.00'	32.50'	17.31'	8° 44' 02.5"	34° 56' 10"
+10	10.00'	13.04'	6.95'	3° 29' 37"	38° 25' 47"
+28.90	18.90'	24.61'	13.11'	6° 38' 10.5"	45° 01' 57.5"

- Ⓐ E Out=139.55 (24" RCP) or E Out=139.30 (30" CMP)
- Ⓑ E In=140.05 (18" RCP) or E In=139.40 (21" CMP)
- Ⓒ E In=140.11 (18" RCP) or E In=140.11 (18" CMP)
- Ⓓ E Out=140.50 (15" RCP) or E Out=140.50 (18" CMP)

**SHERIAC/PRESCOTT
PRESCOTT TO STA. 11+45.00**

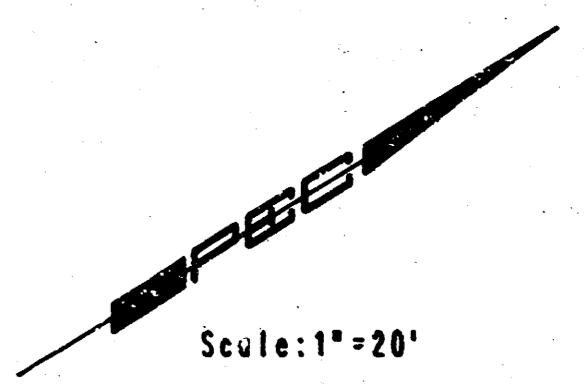
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by	GDS	Checked by	
Drawn by	JGP	Date	May, 1985
		Job No.	85039

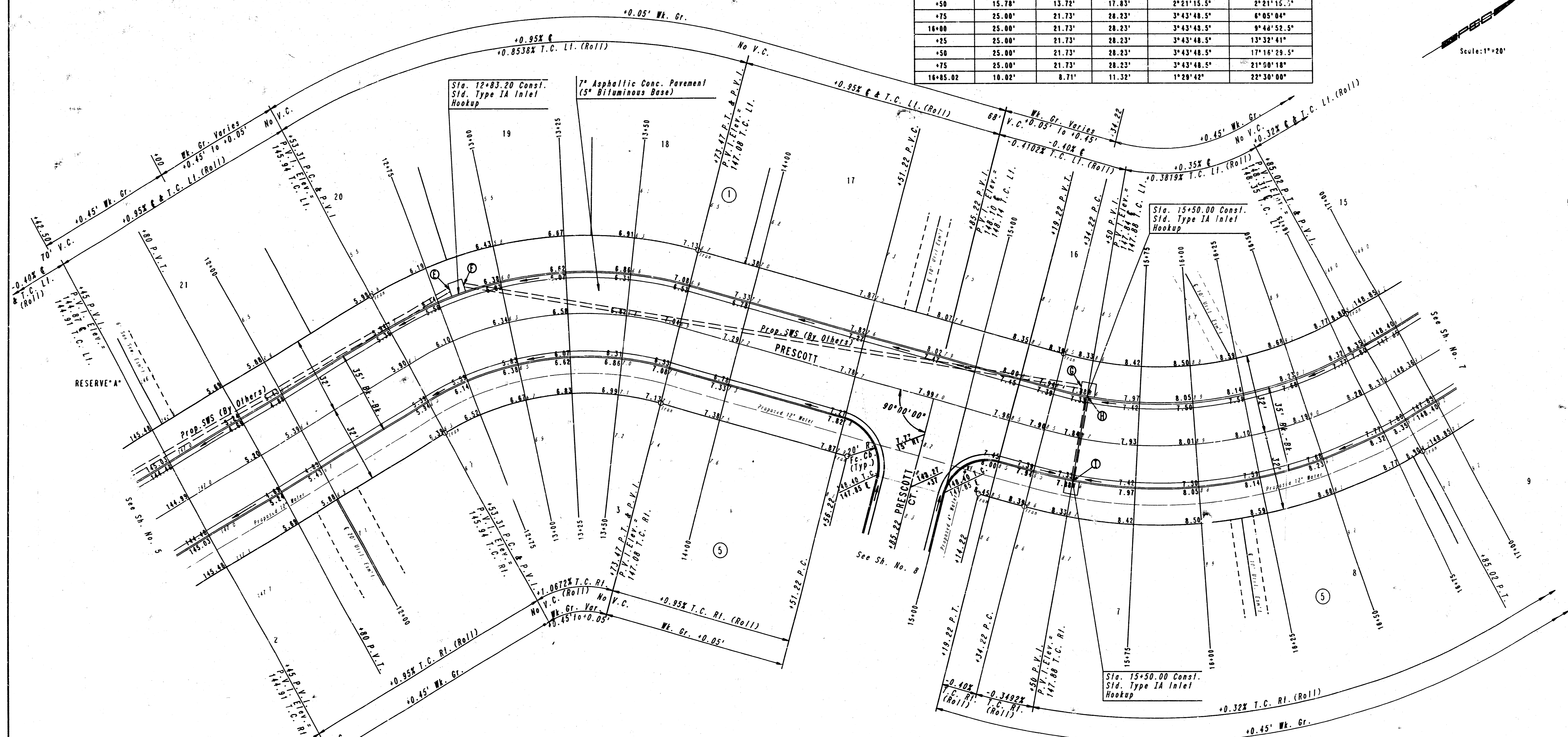
CONSTRUCT ROLL CURB

C CURVE DATA					
Δ=45° 00' 00"		D=29° 50' 30"		R=192.00'	L=150.80'
				T=79.53'	E=15.82'
CURVE DATA BASED ON C RADIUS Δ/2=22° 30' 00"					
C STATION	C ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	C TOTAL DEFLECTION
		8' OFF LEFT FACE CURB	8' OFF LEFT FACE CURB		
15+34.22				0° 00' 00"	0° 00' 00"
+50	15.78'	13.72'	17.83'	2° 21' 15.5"	2° 21' 15.5"
+75	25.00'	21.73'	28.23'	3° 43' 48.5"	6° 05' 04"
+100	25.00'	21.73'	28.23'	3° 43' 48.5"	9° 48' 52.5"
+125	25.00'	21.73'	28.23'	3° 43' 48.5"	13° 32' 41"
+150	25.00'	21.73'	28.23'	3° 43' 48.5"	17° 16' 29.5"
+175	25.00'	21.73'	28.23'	3° 43' 48.5"	21° 00' 18"
16+85.02	10.02'	8.71'	11.32'	1° 29' 42"	22° 30' 00"

PROJECT NO. 472-76-245-81436-000-C-0-001
SHEET NO. 6
TOTAL SHEETS 18



Scale: 1"=20'



C CURVE DATA					
Δ=45° 00' 00"		D=37° 28' 54"		R=153.00'	L=120.17'
				T=63.37'	E=12.60'
CURVE DATA BASED ON C RADIUS Δ/2=22° 30' 00"					
C STATION	C ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	C TOTAL DEFLECTION
		8' OFF LEFT FACE CURB	8' OFF LEFT FACE CURB		
12+53.31				0° 00' 00"	0° 00' 00"
+75	21.69'	25.21'	18.13'	4° 03' 41.5"	4° 03' 41.5"
+100	25.00'	29.05'	20.89'	4° 40' 52.5"	8° 44' 34"
+125	25.00'	29.05'	20.89'	4° 40' 52.5"	13° 25' 26.5"
+150	25.00'	29.05'	20.89'	4° 40' 52.5"	18° 06' 16"
13+73.47	23.47'	27.28'	19.62'	4° 23' 41"	22° 30' 00"

- INTERSECTION QUANTITIES**
- S.Y. — Unreinforced Concrete Pavement
 - 135.1 S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)
 - 19.3 S.Y. 2 1/2" Bituminous Base
 - 82.8 L.F. Combined Curb & Gutter (35" & 1/2")
 - L.F. Monolithic Edge Curb (—)
 - Ea. Standard Wheelchair Ramp Construction
 - S.F. 4" Sidewalk Concrete Pavement
 - 217.7 S.Y. Subgrade Stabilization Manipulation
 - Tons Carbide Lime Subgrade Stabilization
 - Tons Pebble Quicklime Subgrade Stabilization
 - Tons Cement Subgrade Stabilization
 - Tons Fly Ash Subgrade Stabilization
 - Tons Preheater Fines Subgrade Stabilization
 - S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base

- Ⓐ E Out=140.51 (18" RCP) or E Out=140.68 (21" CMP)
- Ⓑ E In=140.61 (18" RCP) or E In=140.78 (21" CMP)
- Ⓒ E Out=143.07 (18" RCP) or E Out=143.04 (21" CMP)
- Ⓓ E In=143.32 (15" RCP) or E In=143.14 (18" RCP)
- Ⓔ E Out=143.47 (15" RCP) or E Out=143.52 (18" CMP)

PRESCOTT
STA. 11+45 TO STA. 17+00

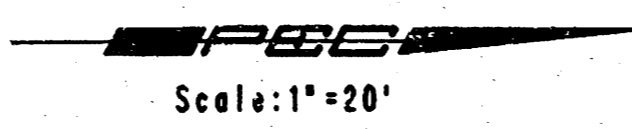
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by GDS
Checked by J6P
Date May, 1985
Job No. 85035

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INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 195.1 S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)
- 19.3 S.Y. 2 1/2" Bituminous Base
- 62.8 L.F. Combined Curb & Gutter (3 5/8" & 1 1/2")
- L.F. Monolithic Edge Curb (—)
- Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 217.7 S.Y. Subgrade Stabilization Manipulation
- Tons Carbide Lime Subgrade Stabilization
- Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base



Q CURVE DATA

$\Delta=33^{\circ}56'24''$ $D=36^{\circ}12'38''$ $R=158.23'$ $L=93.73'$ $T=48.29'$ $E=7.20'$

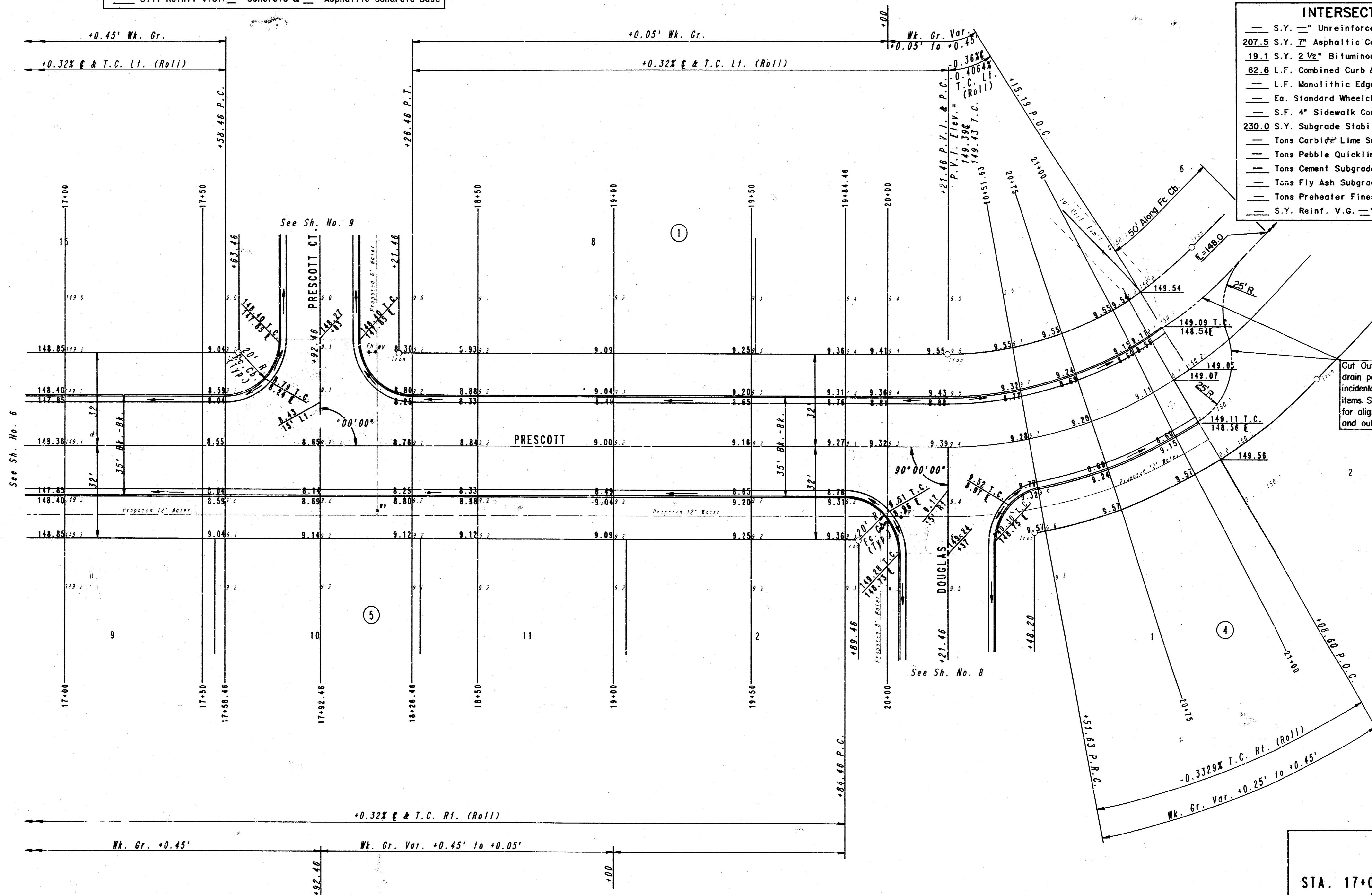
CURVE DATA BASED ON Q RADIUS $\Delta/2=16^{\circ}58'12''$

Q STATION	Q ARC LENGTH	CHORD LENGTH		DEFLECTION ANGLE	Q TOTAL DEFLECTION
		8' OFF LEFT FACE CURB	8' OFF LEFT FACE CURB		
20+21.46	—	—	—	0°00'00"	0°00'00"
+51.63	30.17'	25.36'	34.88'	5°27'44.5"	5°27'44.5"
+75	23.37'	19.66'	27.04'	4°13'52"	9°41'36.5"
21+00	25.00'	21.03'	28.92'	4°31'35"	14°13'11"
+08.60	8.60'	7.24'	9.96'	1°33'25.5"	15°46'37"
+15.19	6.59'	5.55'	—	1°11'35"	16°58'12"

PROJECT NO. 472-76-245-81436-000-000-001	SHEET NO. 7	TOTAL SHEETS 18
---------------------------------------------	----------------	--------------------

INTERSECTION QUANTITIES

- S.Y. — Unreinforced Concrete Pavement
- 207.5 S.Y. 7" Asphaltic Conc. Pavement (5" Bituminous Base)
- 19.1 S.Y. 2 1/2" Bituminous Base
- 62.6 L.F. Combined Curb & Gutter (3 5/8" & 1 1/2")
- L.F. Monolithic Edge Curb (—)
- Ea. Standard Wheelchair Ramp Construction
- S.F. 4" Sidewalk Concrete Pavement
- 230.0 S.Y. Subgrade Stabilization Manipulation
- Tons Carbide Lime Subgrade Stabilization
- Tons Pebble Quicklime Subgrade Stabilization
- Tons Cement Subgrade Stabilization
- Tons Fly Ash Subgrade Stabilization
- Tons Preheater Fines Subgrade Stabilization
- S.Y. Reinf. V.G. — Concrete & — Asphaltic Concrete Base



CONSTRUCT ROLL CURB

PRESCOTT
STA. 17+00 TO N.L. LOT 7, BLOCK 1 & N.L. LOT 1, BLOCK 4

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

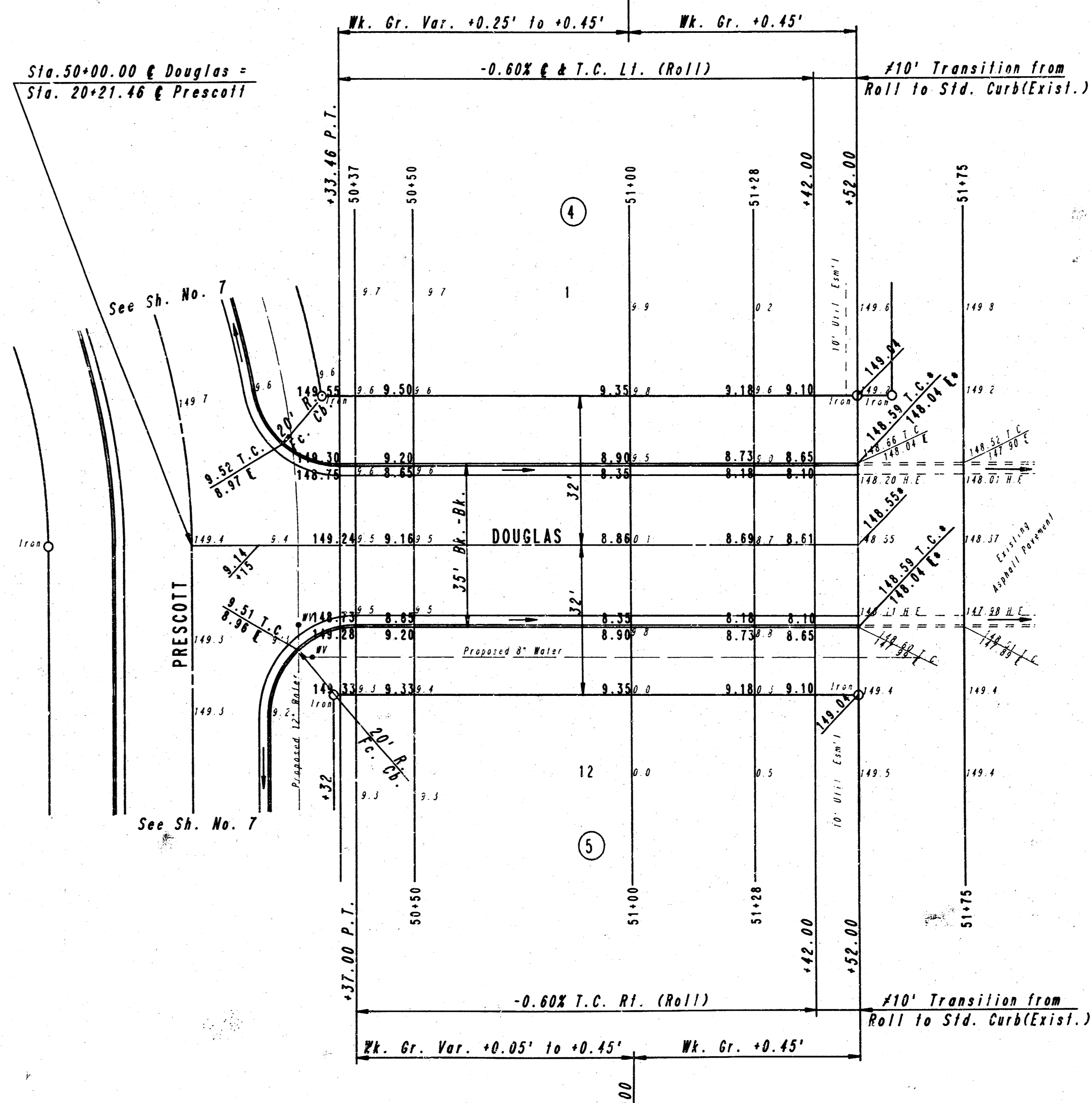
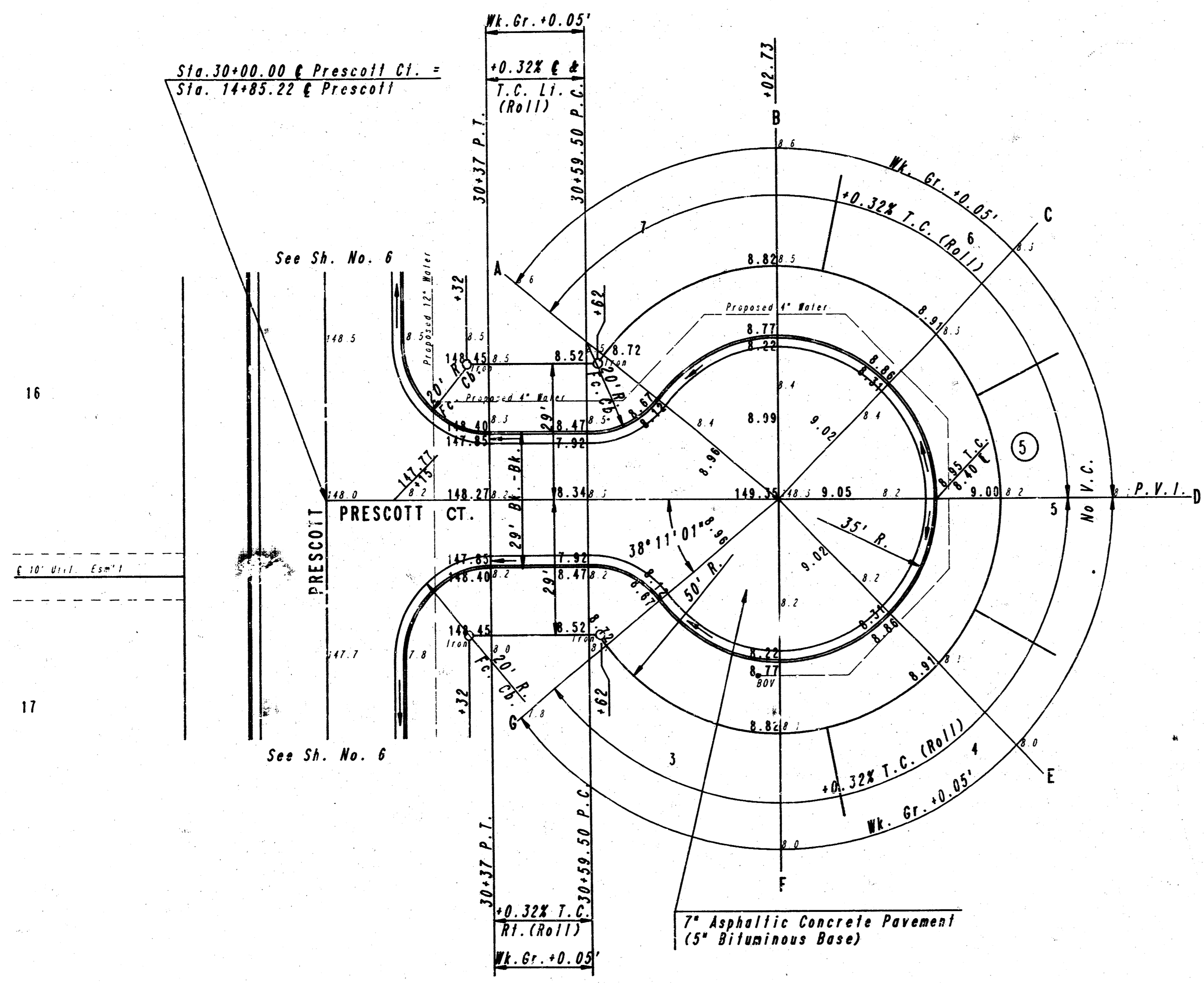
Designed by	GDS	Checked by	
Drawn by	JGP	Date	May, 1965
		Job No.	85039

FILMED FROM THE BEST AVAILABLE COPY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81436-000-001	8	18

Scale: 1"=20'

Scale: 1"=20'



CONSTRUCT ROLL CURB

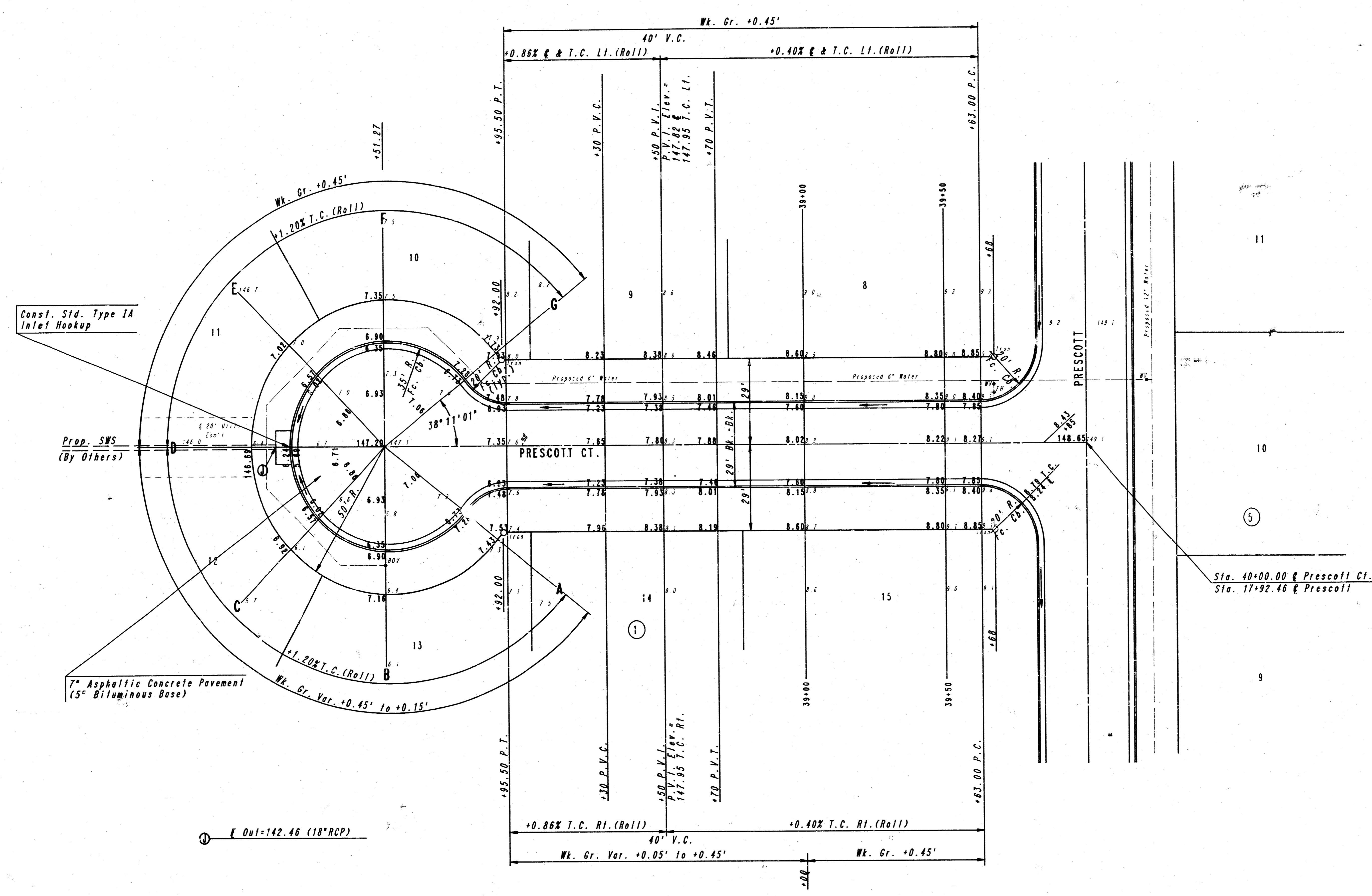
* Match Existing
 / Transition shall be paid for as L.F. Roll Curb & Gutter

PRESCOTT CT.
 E.L. PRESCOTT TO AND INCLUDING CUL-DE-SAC
DOUGLAS
 E.L. PRESCOTT TO E.L. LOT 12, BLOCK 5
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS

Designed by GDS	Checked by
Drawn by JGP	Date May, 1985 Job No. 85033

FILMED FROM THE BEST AVAILABLE COPY

PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81436-000-000-001	9	18



Scale: 1" = 20'

CONSTRUCT ROLL CURB

PRESCOTT CT.
CUL-DE-SAC TO W.L. PRESCOTT

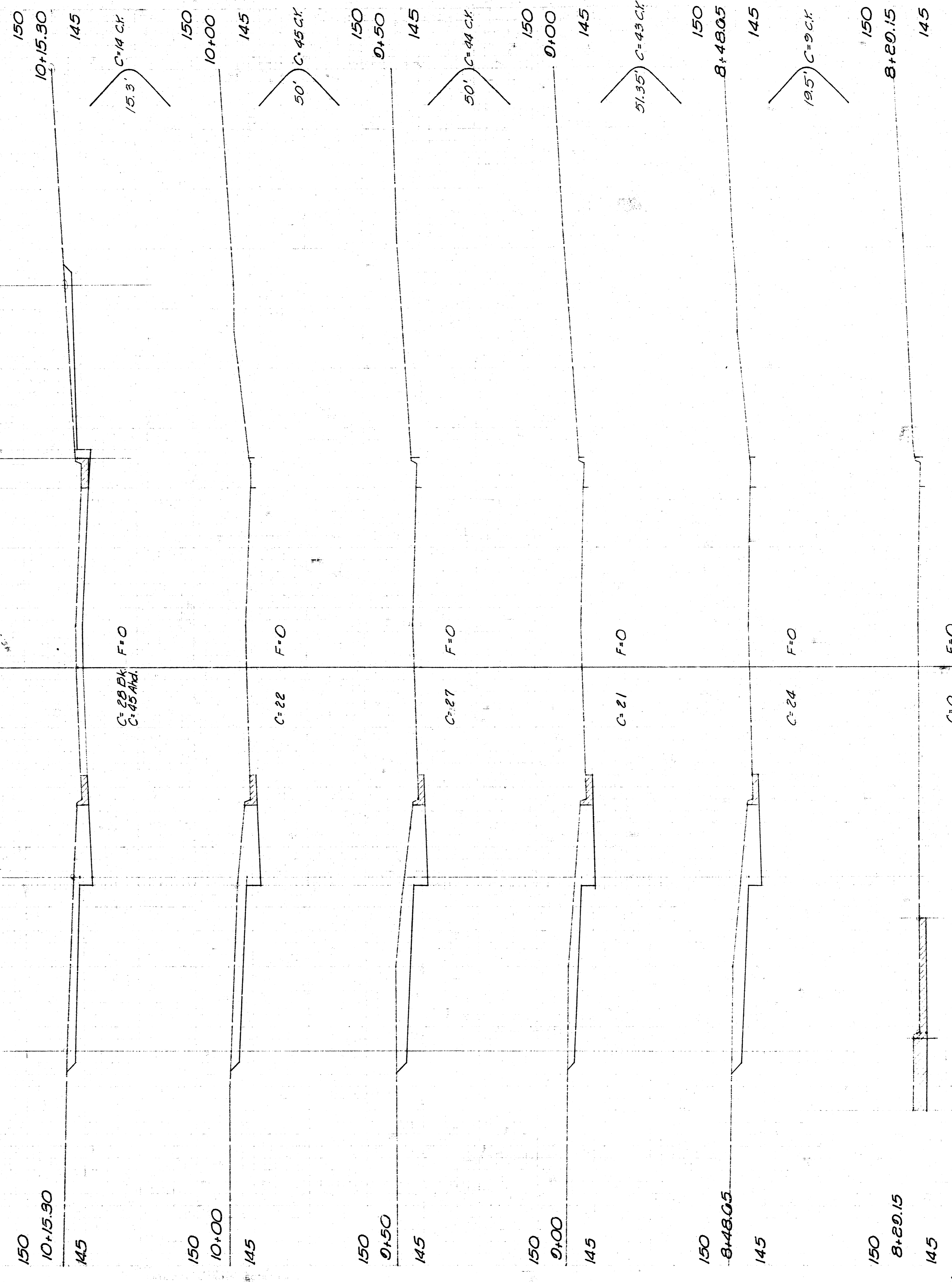
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by	GDS	Checked by	
Drawn by	JGP	Date	May, 1985
		Job No.	85039

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J. Mass
D. Pausanep
5/5/85

50 40 30 20 10 0 10 20 30 40 50



Oak Cliff 3rd
Proj. No. 472.76 245 8436 000000 001

150 8+01.15
145

Loose Fill
X-Sections
117.0 Cu Yds.
10%
11.7 Cu Yds.
128.7 Cu Yds.

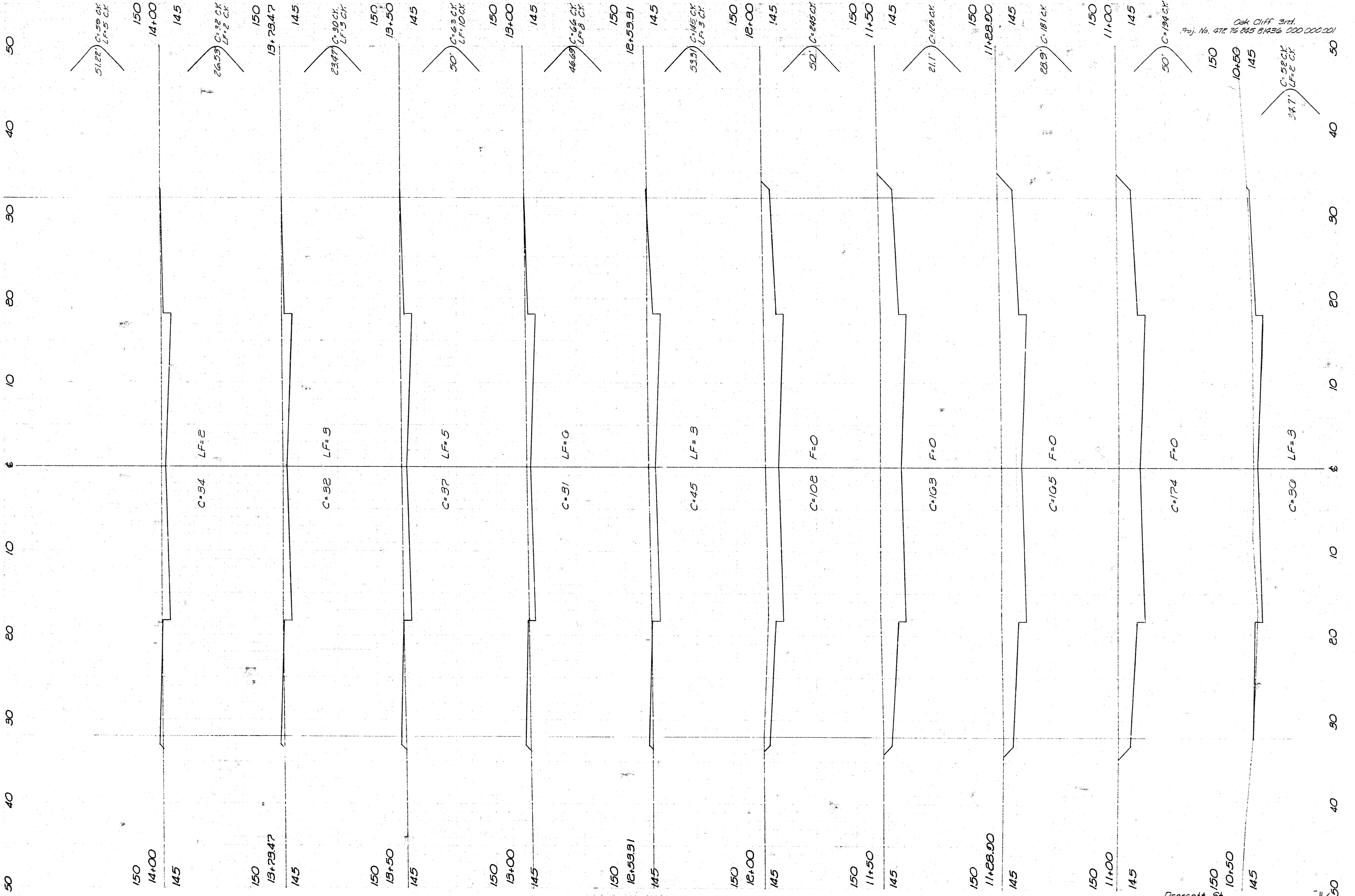
Sheriac
Sta. 8+01.15 to Sta. 10+15.30

50 40 30 20 10 0 10 20 30 40 50

EXAMINED FROM THE BEST

U. Moore
D. Raubenberg

5/15/53
5/15/53



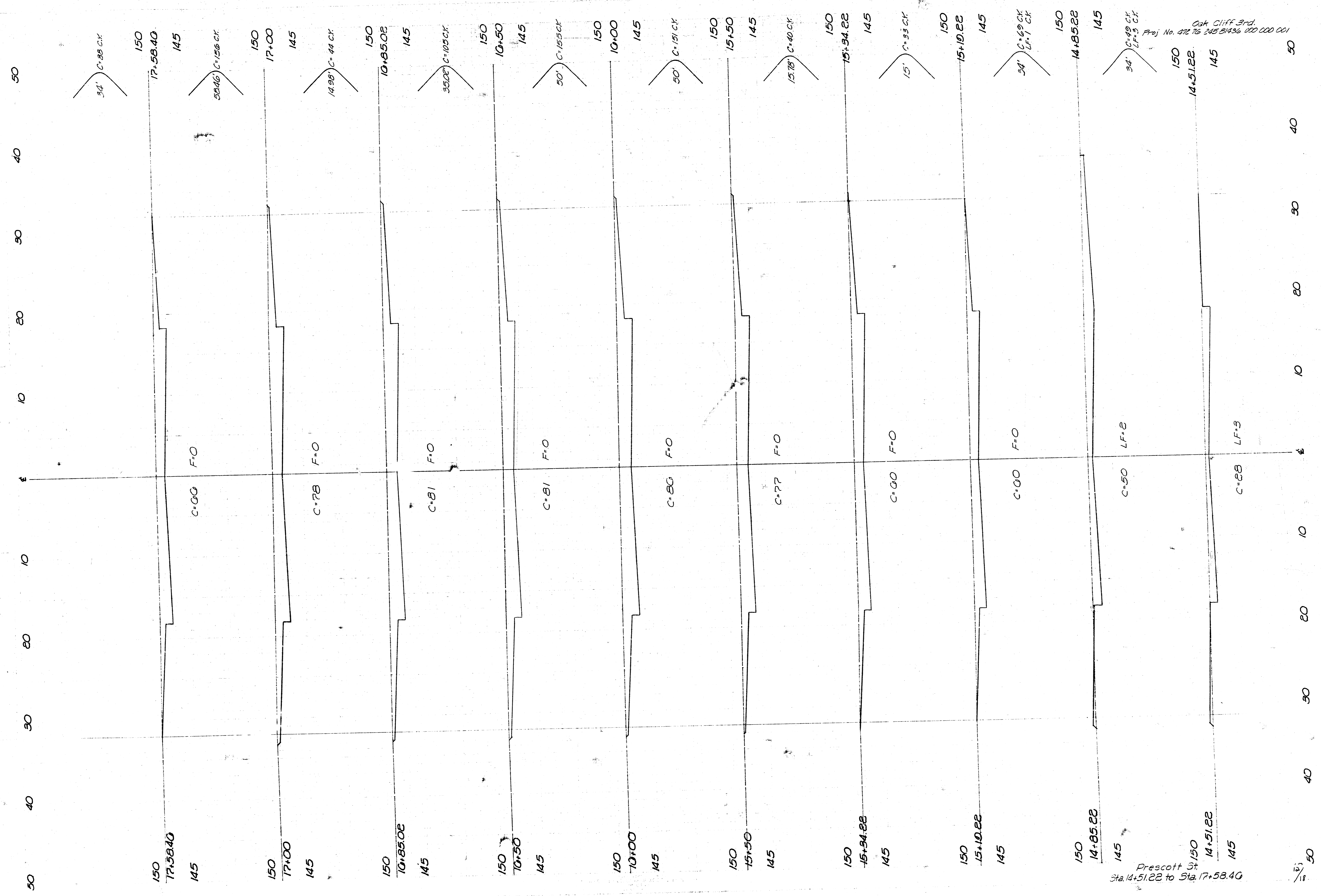
Oak Cliff 3rd.
Proj. No. 472 76 245 2/436 000 000 001

Prescott St.
Sta. 10+50 to Sta. 14+00

FILMED FROM THE BEST

1/18

J. Moore
D. Fuchsberger
5/15/85



Oak Cliff 3rd.
Proj. No. 472 76 245 81436 000 000 001

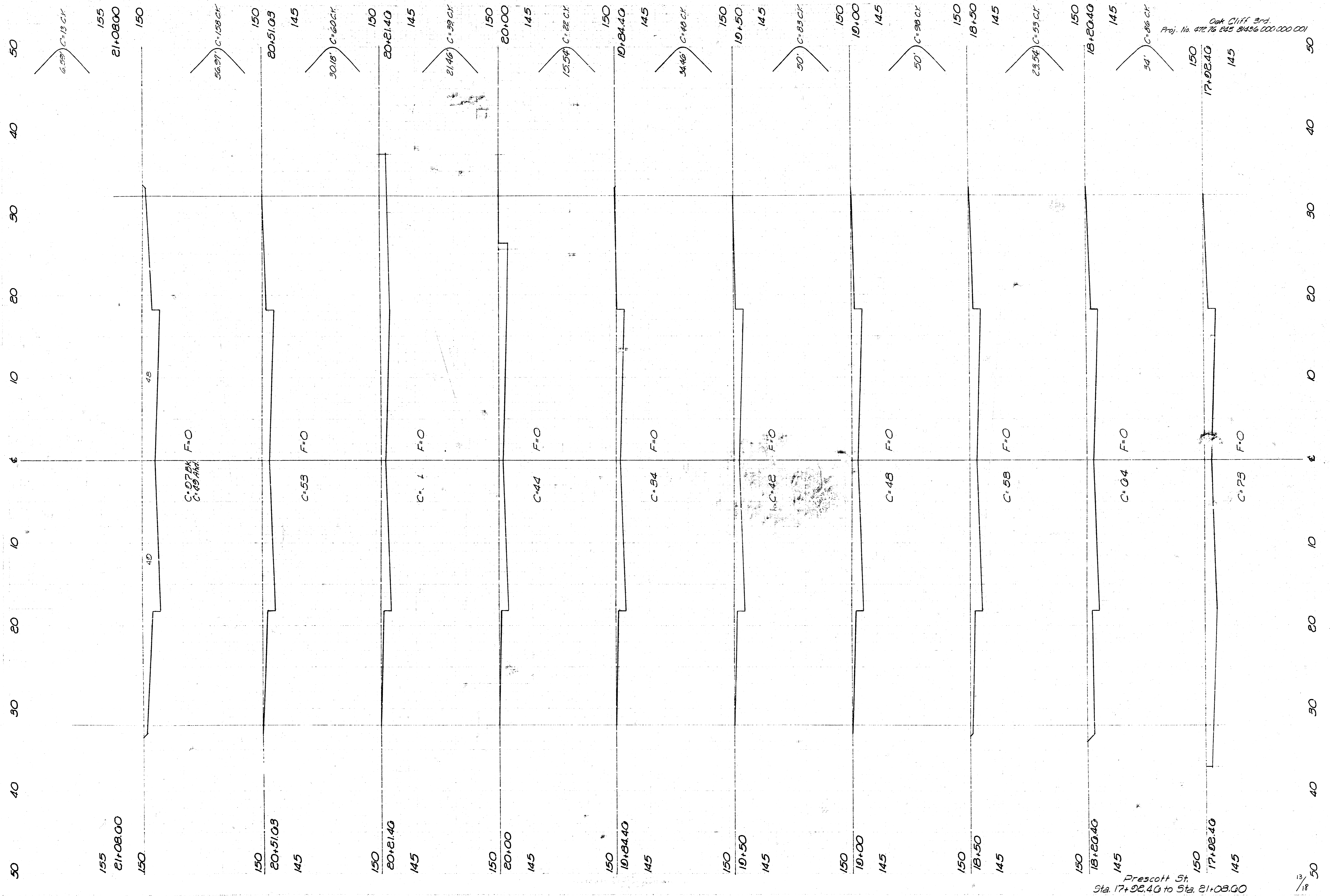
Prescott St.
Sta. 14+51.22 to Sta. 17+58.40

FILMED FROM THE BEST

1. Name
D. Reschman

5-15-55

5-15-55



Oak Cliff 3rd
Proj. No. 47276 245 2456 000 000 001

Prescott St
Sta. 17+02.40 to Sta. 21+08.00

13/18

FILMED FROM THE BEST

DATE
BY

1/20/20
5/15/20

155
21+45.73
150

Oak Cliff 3rd.
Proj. No. 472 76 245 21436 000 000 001

155
21+15.10
150

50

40

30

20

10

C+O F+O

C+54 F+O

10

20

30

40

155
21+45.73
150

155
21+15.10
150

50

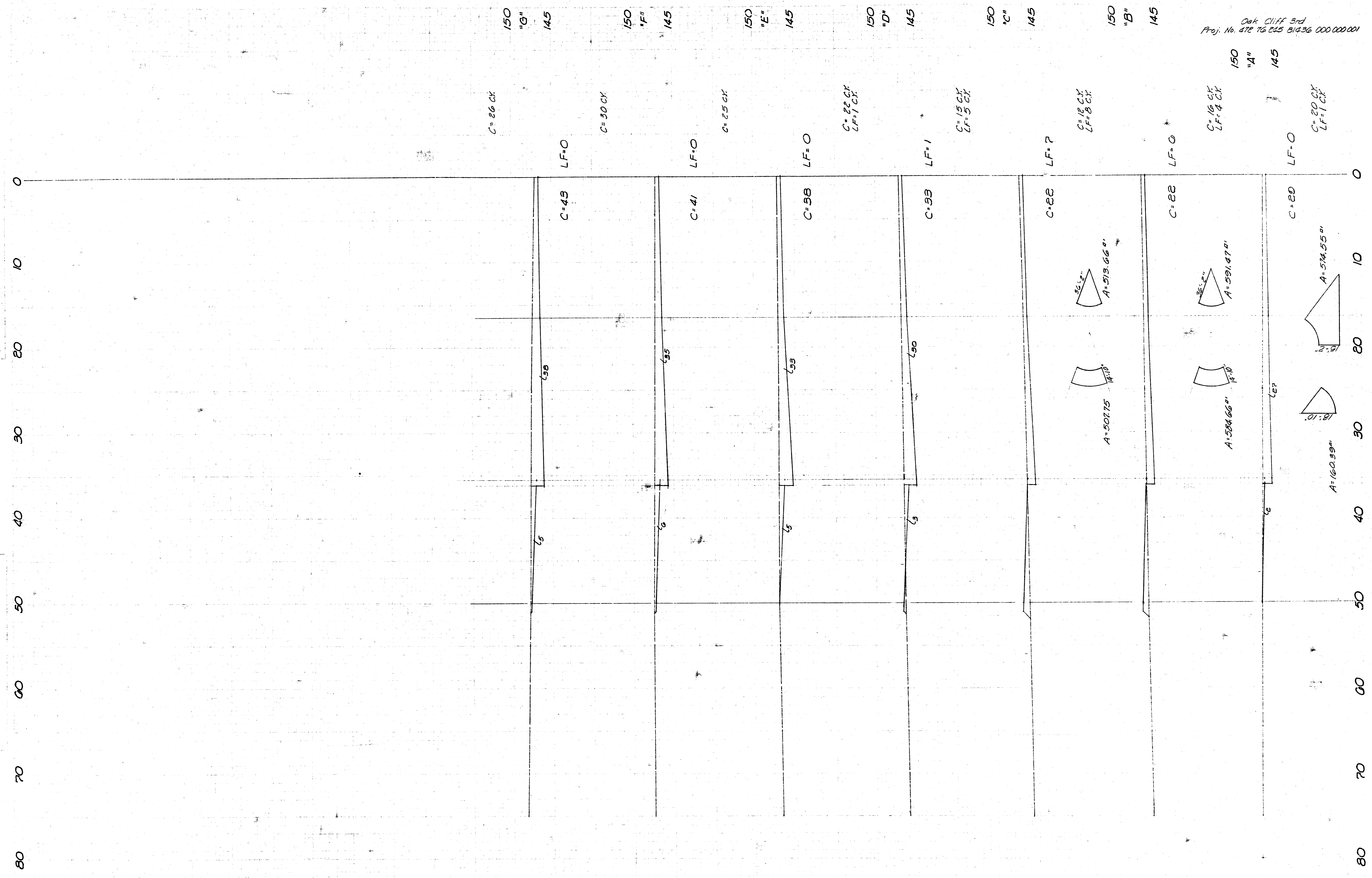
Prescott St.
Sta. 21+15.10 to Sta. 21+45.73

11/18

FILMED FROM THE BEST

Prescott
Cul-De-Sac
5/1/35

Prescott
Cul-De-Sac
5/1/35



150
"G"
145

150
"F"
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150
"E"
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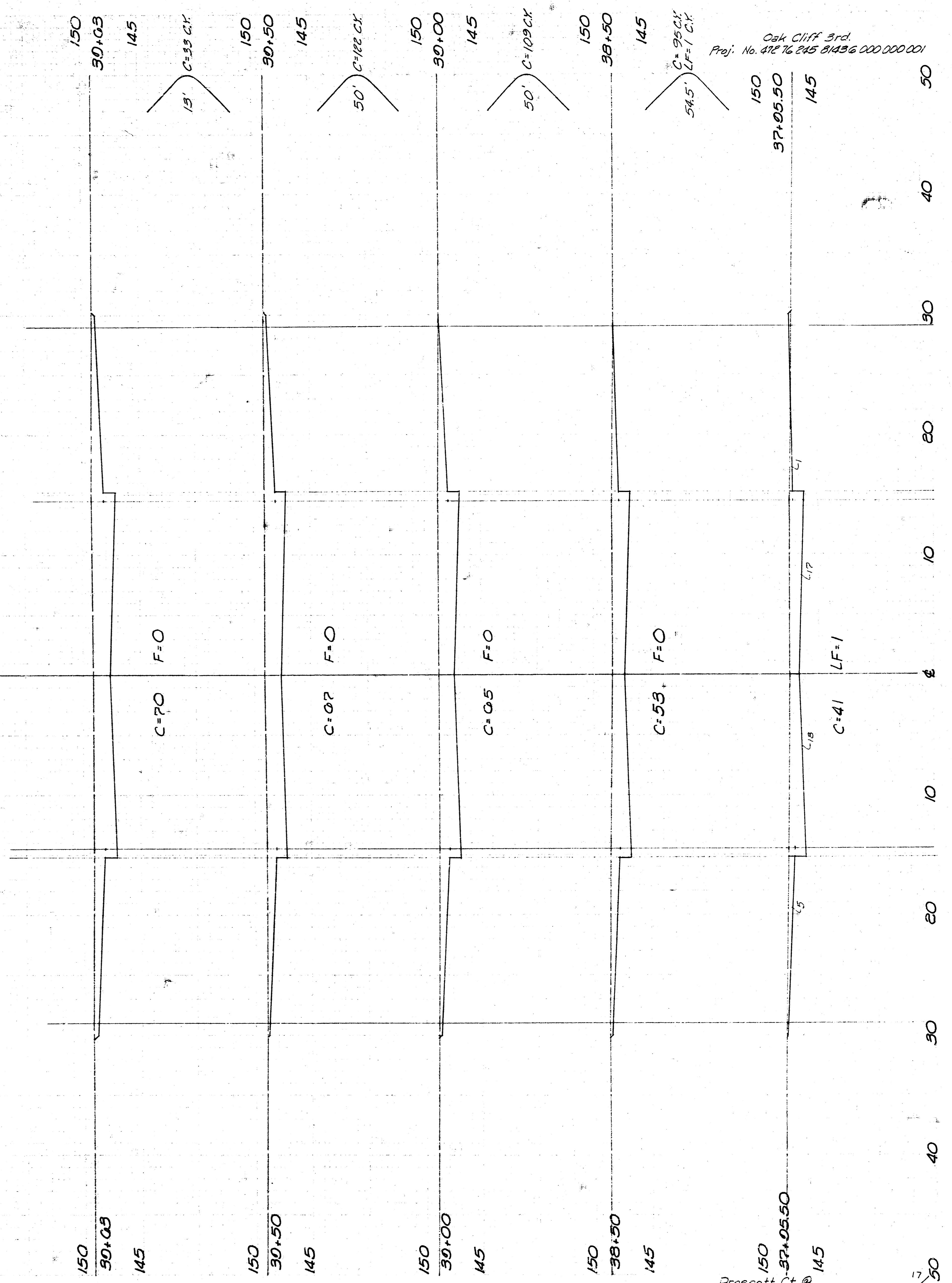
Prescott Ct. Cul-De-Sac @
Prescott St. Sta. 17+92.46

Oak Cliff 3rd
Proj. No. 472 76 245 51436 000 000 001

J. Moore
D. A. Rauschberg

1/18/53
1/18/53
1/18/53

50 40 30 20 10 0 10 20 30 40 50



Oak Cliff 3rd
Proj. No. 42 76 245 81436 000 000 001

Prescott Ct @
Prescott St Sta 17+92.46

FILMED FROM THE BEST

