

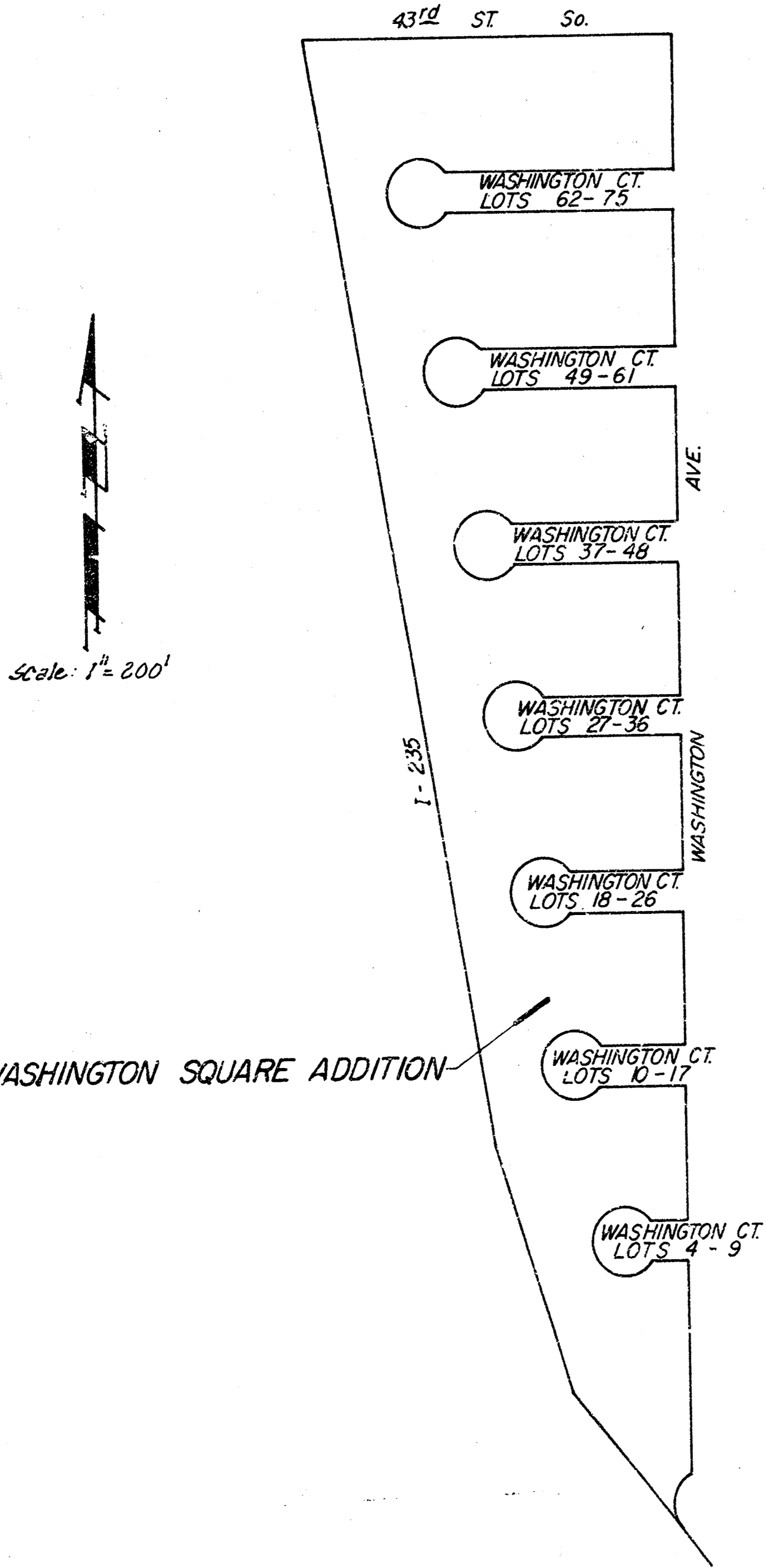
# STREETS IN WASHINGTON SQUARE ADDITION

- WASHINGTON COURT, LOTS 4-9, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 10-17, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 18-26, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 27-36, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 37-48, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 49-61, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC
- WASHINGTON COURT, LOTS 62-75, W. L. WASHINGTON TO & INCLD. CUL-DE-SAC

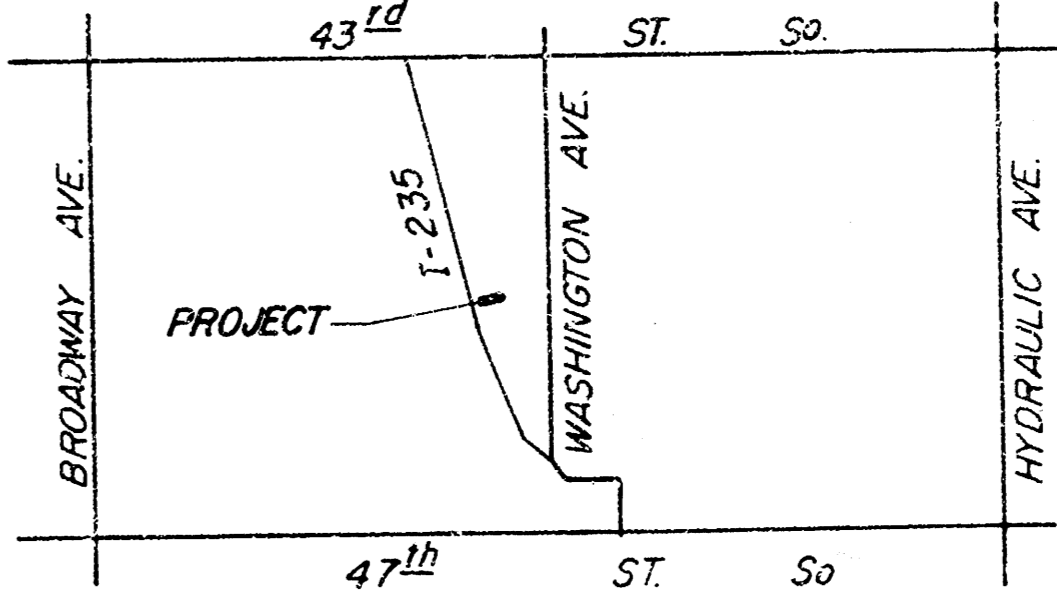
CITY OF WICHITA, KANSAS  
 R. W. LINN — CITY ENGINEER  
 PROJ. No. 472 76 245 80687 000 000 001  
 DATE:

EARTHWORK

EXCAVATION	=	4,498.5 C.Y.
10 %	=	449.9 C.Y.
<b>TOTAL</b>	=	<b>4,948.4 C.Y.</b>



SITE PLAN

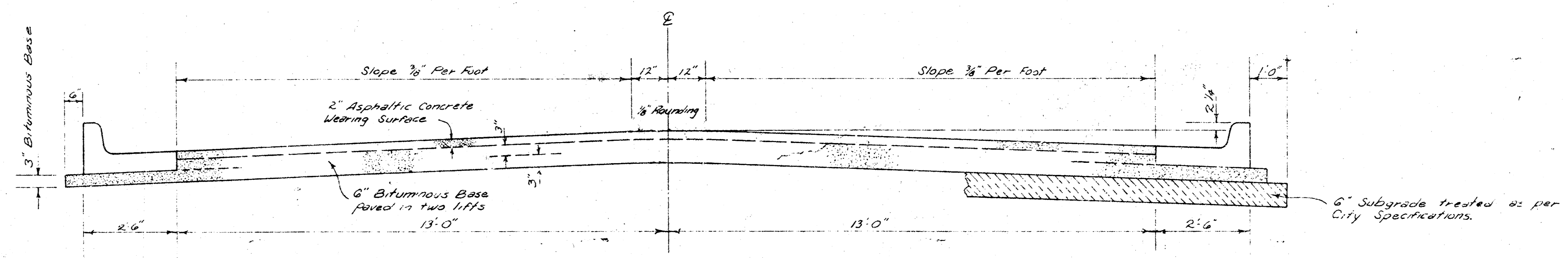


LOCATIC'N MAP

Note: Trees to be removed are marked thus, except that any tree marked for removal which in the opinion of the Engineer can be saved, shall be spared.



# COURTS IN WASHINGTON SQUARE ADDITION



## TYPICAL SECTION

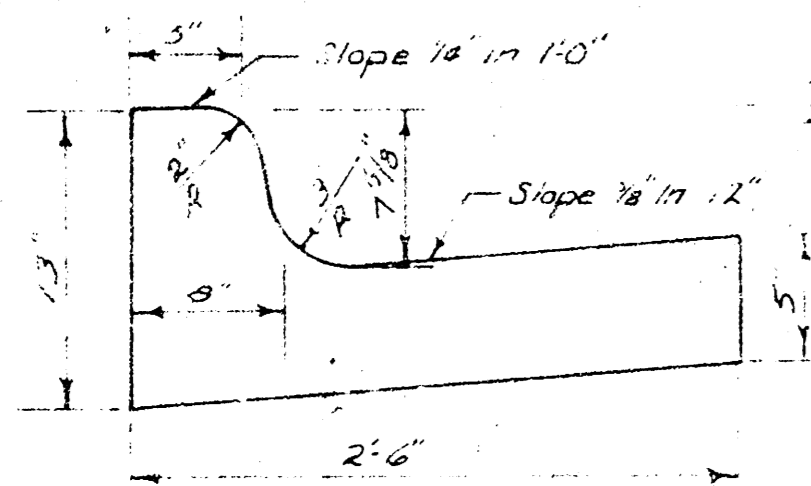
### 31' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

A TACK COAT OF EMULSIFIED ASPHALT (SS-IH) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQ. YD. BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.

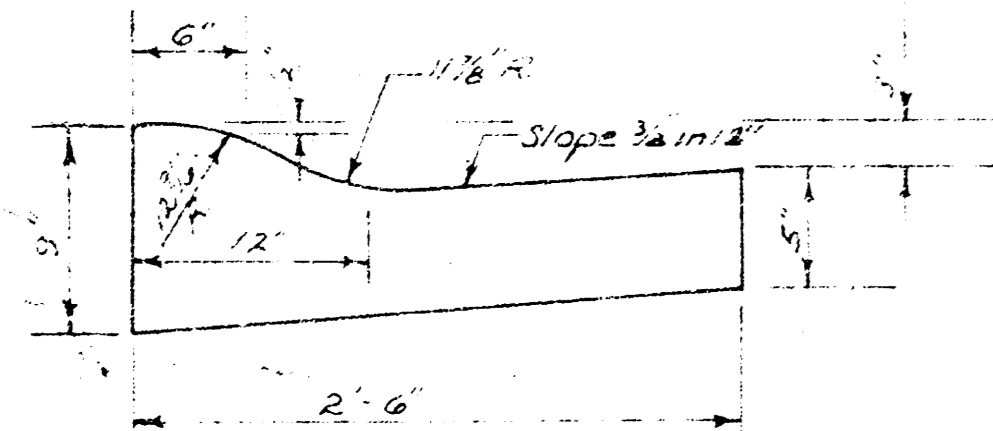
BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.

*The A.C. pavement between the Comb. Curb & Gutter shall be paid as 3 1/2% 3" A.C. Pavement (6" Bituminous Base). The Bituminous Base under the Comb. Curb & Gutter shall be paid as 3 1/2% 3" Bituminous Base.*

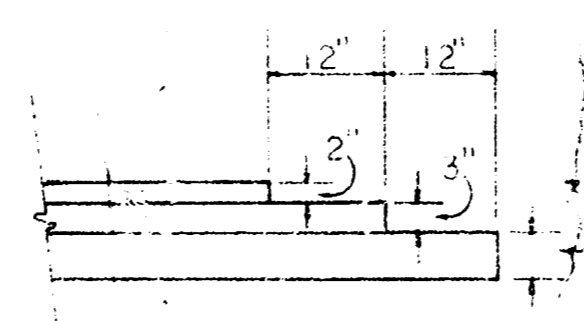
COMBINED CURB & GUTTER



ROLL TYPE CURB & GUTTER



### DETAIL OF TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS ASPHALTIC CONCRETE PAVEMENT.

CITY OF WICHITA, KANSAS

DEPARTMENT OF PUBLIC WORKS — ENGINEERING

R. W. LINN CITY ENGINEER

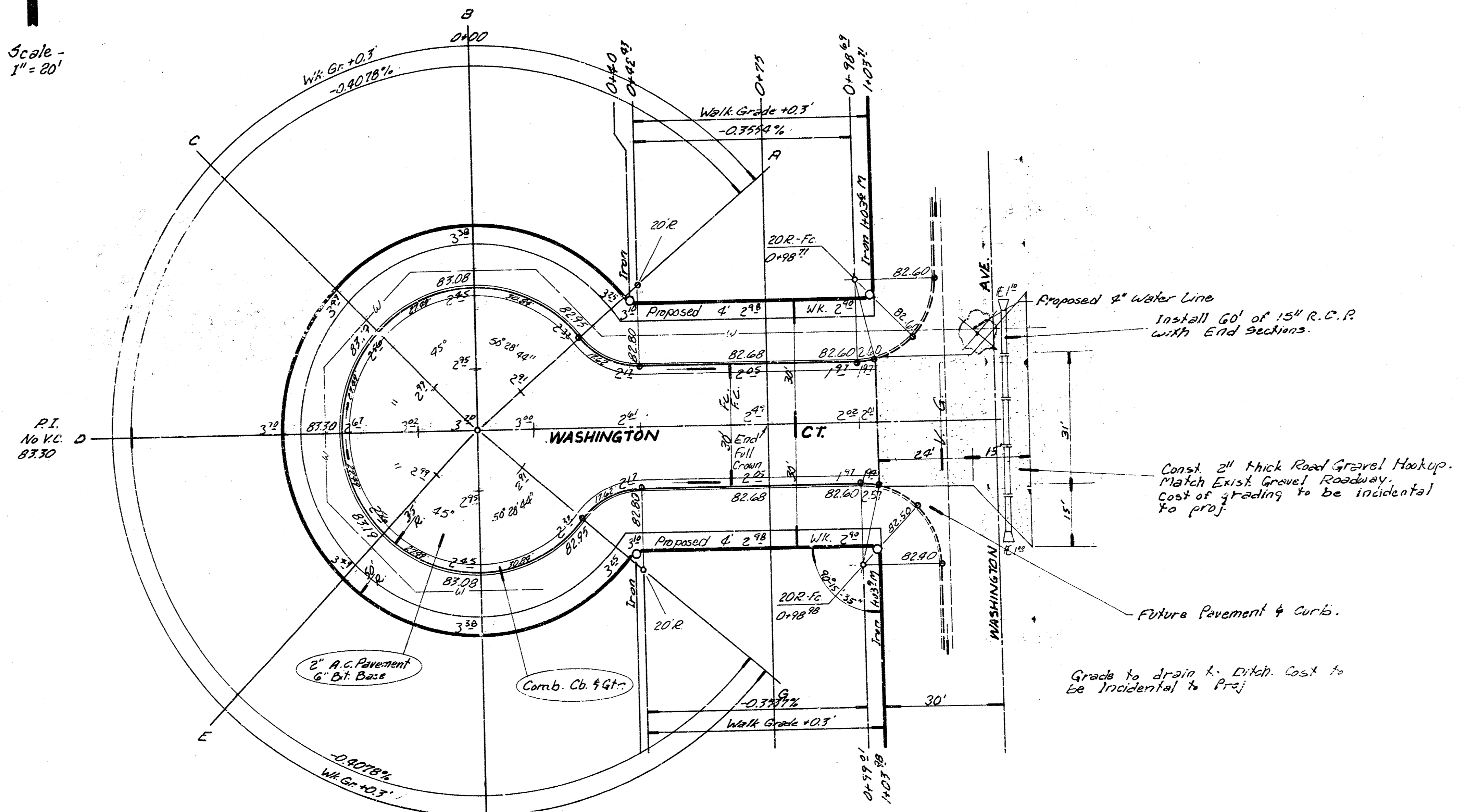
DATE \_\_\_\_\_ PROJ. NO. 472 75 245 80687000 000 001

2/12

BM 82<sup>33</sup> City Std. 2' S & 6' E of NE Cor. Ida & 47<sup>th</sup> St. So. Baseline is @ Washington Ct.  
 BM 84<sup>25</sup> NW Cor. Conc. Porch Ho. # 4654 So. Washington  
 BM 84<sup>35</sup> SW Cor. Conc. Porch Ho. # 4630 So. Washington



Scale - 1" = 20'



**EARTHWORK**

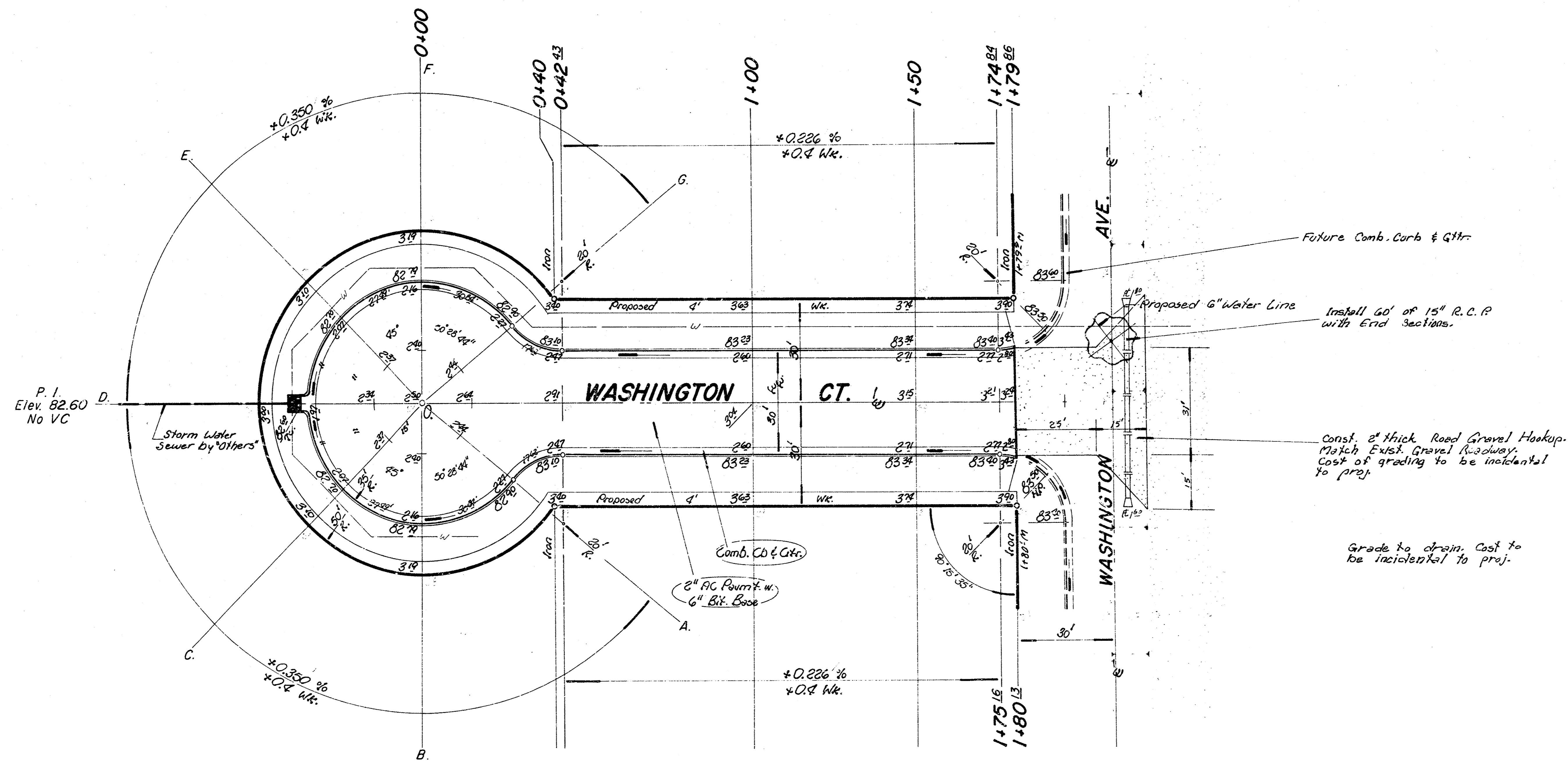
EXCAVATION	=	114.3 C.Y.
10 %	=	11.4 C.Y.
<b>TOTAL</b>	=	<b>125.7 C.Y.</b>

WASHINGTON COURT  
 (Lots 4-9)  
 W.L. Washington Co & Inc. Civil & Sec

JOHN KRUG  
 P.E.  
 1143  
 S.W.  
 1143  
 S.W.  
 C.S.B.

B.M. 84<sup>35</sup> SW Cor. Conc. Porch Ho. # 4630 So. Washington  
 B.M. 84<sup>35</sup> NW Cor. Conc. Porch Ho. # 4634 So. Washington  
 B.M. 83<sup>74</sup> R.R. Spike W. Face Street Light Pole NE Cor.  
 45<sup>th</sup> St So. # Washington

Baseline is  $\frac{1}{2}$  Washington Ct.



**EARTHWORK**

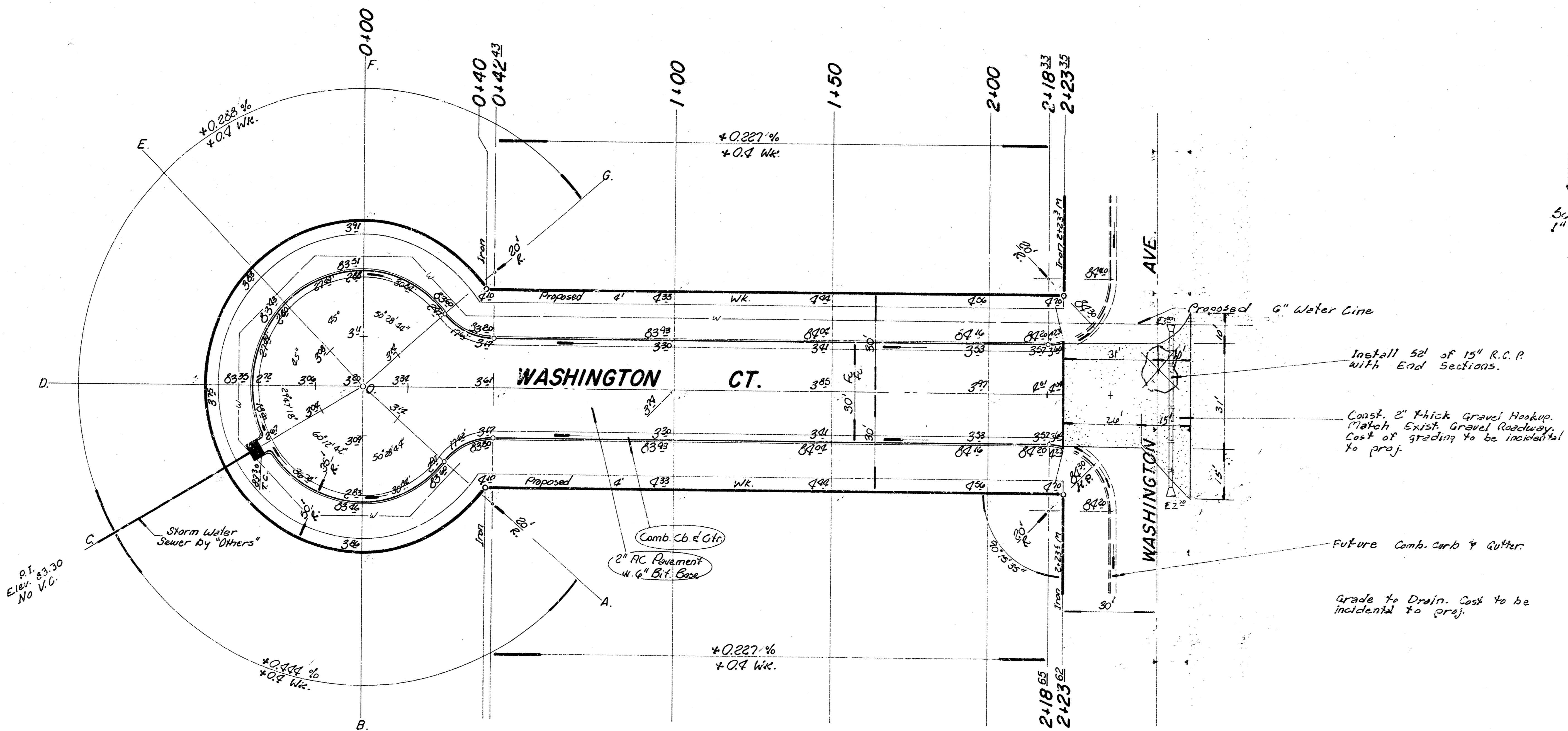
EXCAVATION	=	373.0 C.Y.
10%	=	37.3 C.Y.
<b>TOTAL</b>	=	<b>410.3 C.Y.</b>

TYPE OF MATERIAL TO BE USED SHALL BE DETERMINED BY THE ENGINEER. THE GRADE INDICATED IS THE PROPOSED GRADE. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR ANY MODIFICATION OR CORRECTION OF THESE.

**WASHINGTON CT.**  
 Lots 10-17

4/17

BM 83<sup>74</sup> R.R. Spike W-face Street Light Pole NE Cor  
 45th So. & Washington  
 BM 86<sup>28</sup> R.R. Spike W-face Street Light Pole E-Side Washington St.  
 3 Ft. N of E Washington Ct. Lots 27 thru 36  
 Baseline is E Washington Ct.



**EARTHWORK**

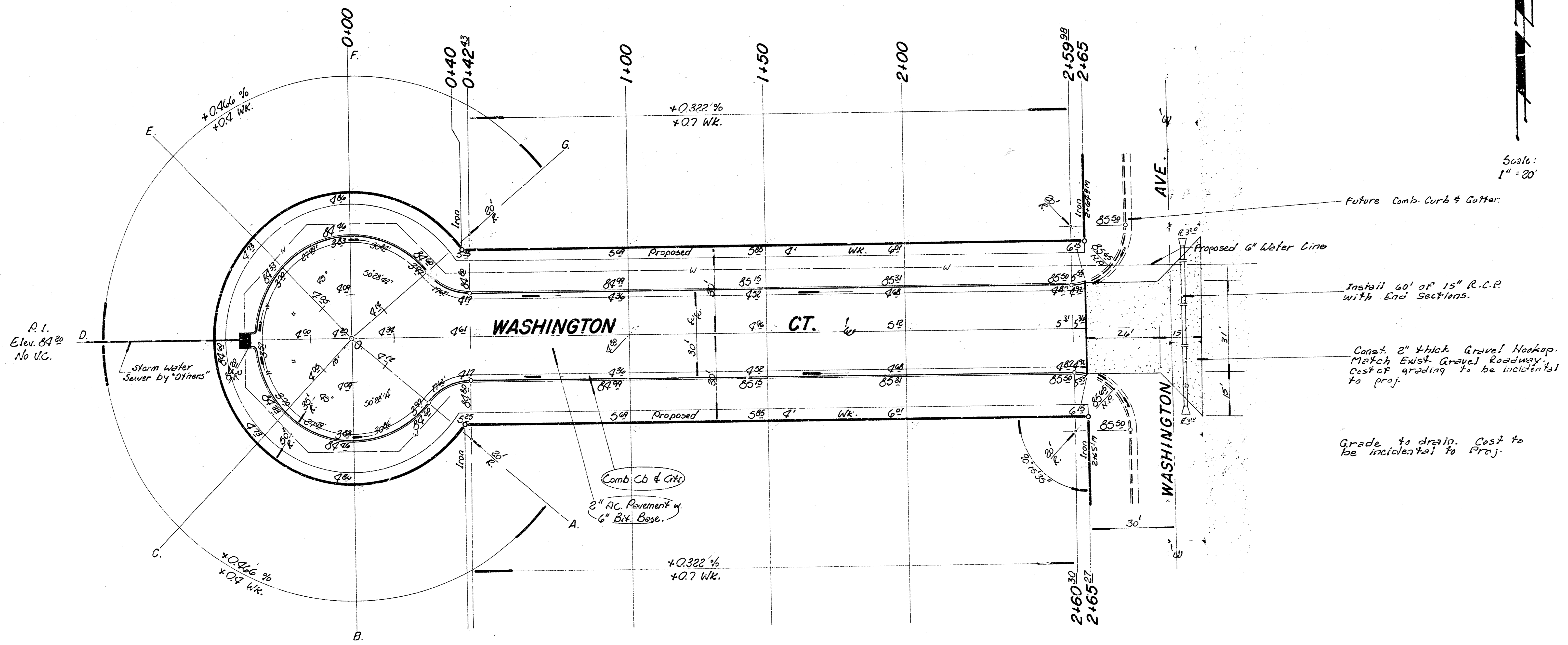
EXCAVATION	= 765.4 C.Y.
10%	= 76.5 C.Y.
<b>TOTAL</b>	<b>= 841.9 C.Y.</b>

**WASHINGTON CT.**  
 Lot 18-26

5/19

BM 83<sup>25</sup> R.R. Spike W. Face Street Light Pole NE Cor  
 45<sup>th</sup> St. So. & Washington  
 BM 86<sup>08</sup> R.R. Spike W. Face Street Light Pole E. Side Washington St.  
 3<sup>rd</sup> St. N. of & Washington Ct. Lots 27 thru 36

Baseline is & Washington Ct.



Scale: 1" = 20'

**EARTHWORK**

EXCAVATION	=	867.3 C.Y.
10%	=	86.7 C.Y.
<b>TOTAL</b>	=	<b>954.0 C.Y.</b>

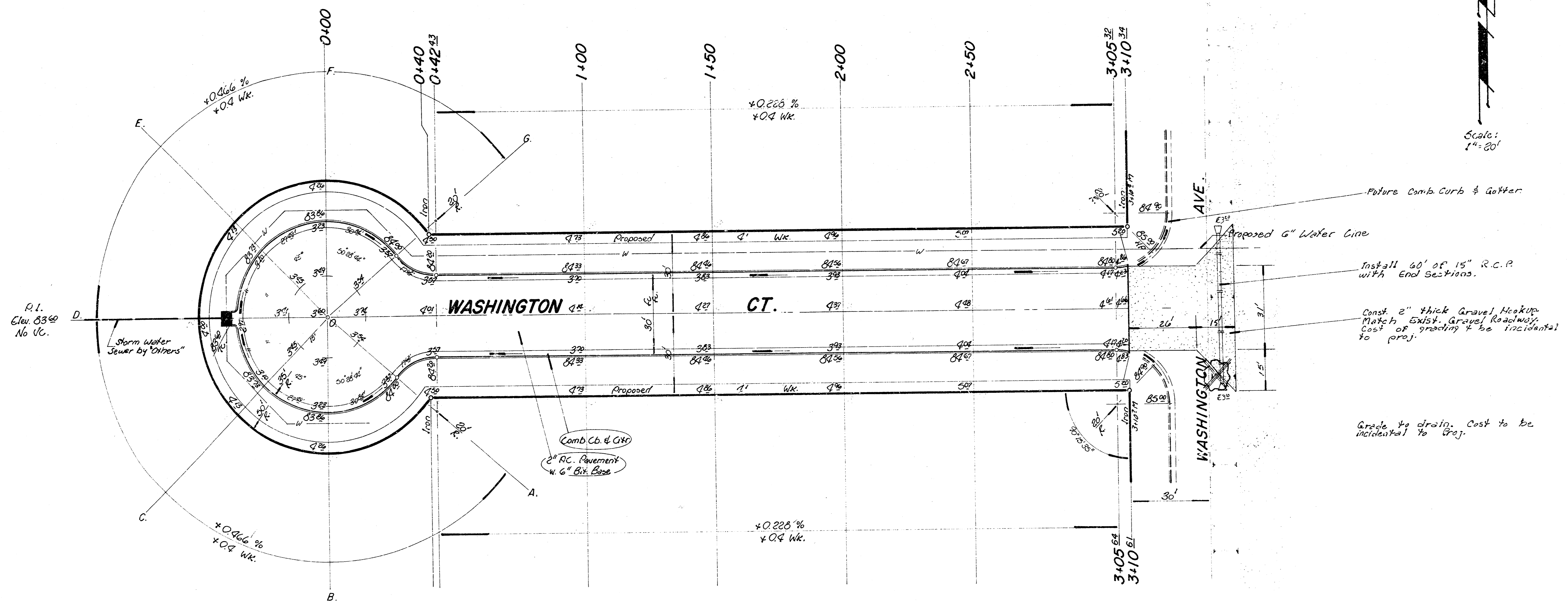
**WASHINGTON CT.**

Lots 27-36

6/9

BM 85<sup>00</sup> R.R. Spike S. Face Street Light Pole NE Cor.  
 44<sup>th</sup> St. S. & Washington  
 BM 86<sup>00</sup> R.R. Spike W. Face Street Light Pole E. Side Washington St.  
 3<sup>rd</sup> N. of E Washington Ct. Lots 27 thru 36

Baseline is E Washington Ct.



Scale: 1" = 20'

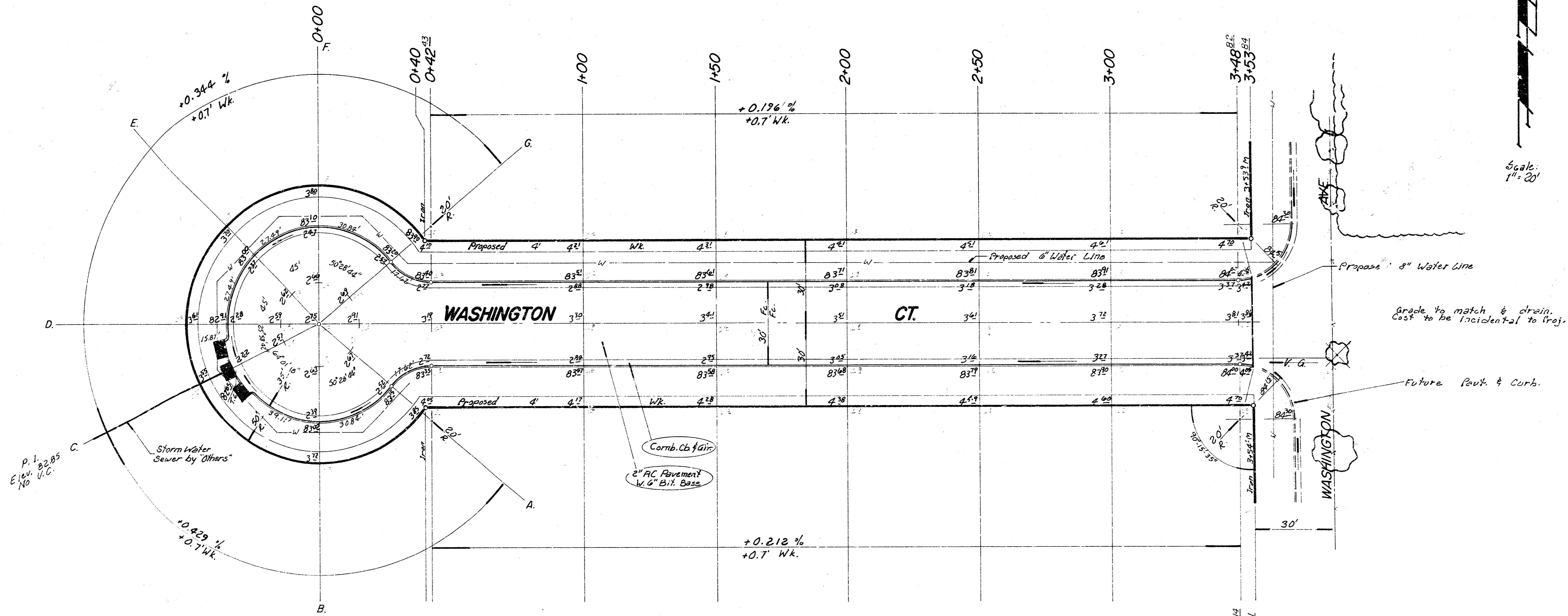
**EARTHWORK**

EXCAVATION	=	663.9 C.Y.
10 %	=	66.4 C.Y.
<b>TOTAL</b>	=	<b>730.3 C.Y.</b>

**WASHINGTON CT.**  
 Lots 37-48

2/19

B.M. 85<sup>5/8</sup> R.R. Spike S-Face Street Light Pole NE Cor. 44<sup>th</sup> St. So. & Washington  
 Baseline is  $\frac{1}{2}$  Washington Ct



P.I. 82.85  
 Elev. 42.00

Storm Water  
 Sewer by Others

Comb. Cb. & dir.  
 2" AC Pavement  
 w/ 6" Bit. Base

Grade to match & drain.  
 Cost to be incidental to Proj.

Future Pavt. & Corb.

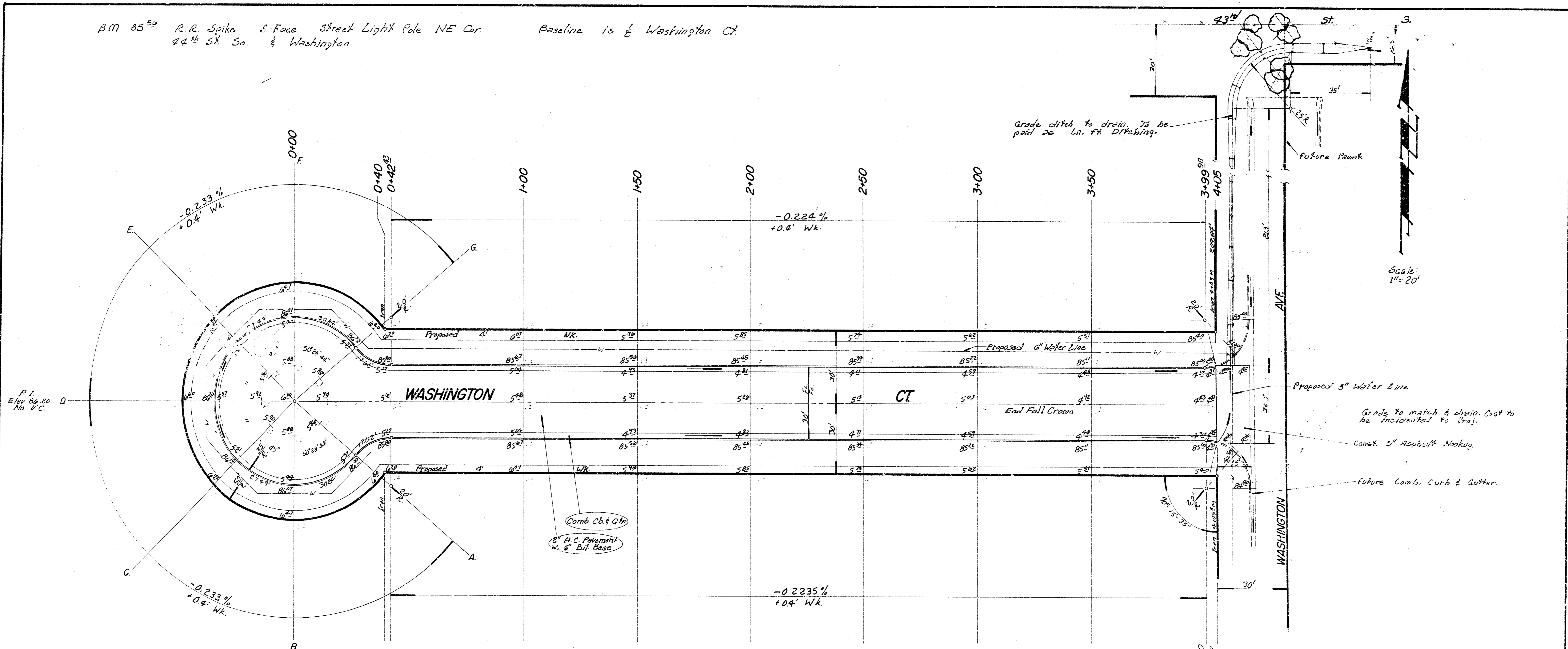
**EARTHWORK**

EXCAVATION	=	1,605.0 C.Y.
10%	=	160.5 C.Y.
<b>TOTAL</b>	=	<b>1,765.5 C.Y.</b>

NOTE: ALL DIMENSIONS AND ELEVATIONS SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL FIELD DATA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL FIELD DATA.

**WASHINGTON CT.**  
 LOTS 49-61

BM 85<sup>50</sup> R.R. Spike S-Face Street Light Pole NE Cor  
 42<sup>nd</sup> St So. & Washington  
 Baseline is E Washington Ct



**EARTHWORK**

EXCAVATION	=	1096 C.Y.
10% =		11.0 C.Y.
<b>TOTAL</b>	=	<b>120.6 C.Y.</b>

**FINISH GRADE**  
 FINISH GRADE SHALL BE DETERMINED BY THE ENGINEER AND SHALL BE BASED ON THE FOLLOWING:  
 1. FIELD SURVEY DATA.  
 2. EXISTING UTILITIES.  
 3. PROPOSED UTILITIES.  
 4. PROPOSED PAVEMENT THICKNESS.

WASHINGTON CT.  
 LOTS 62-75