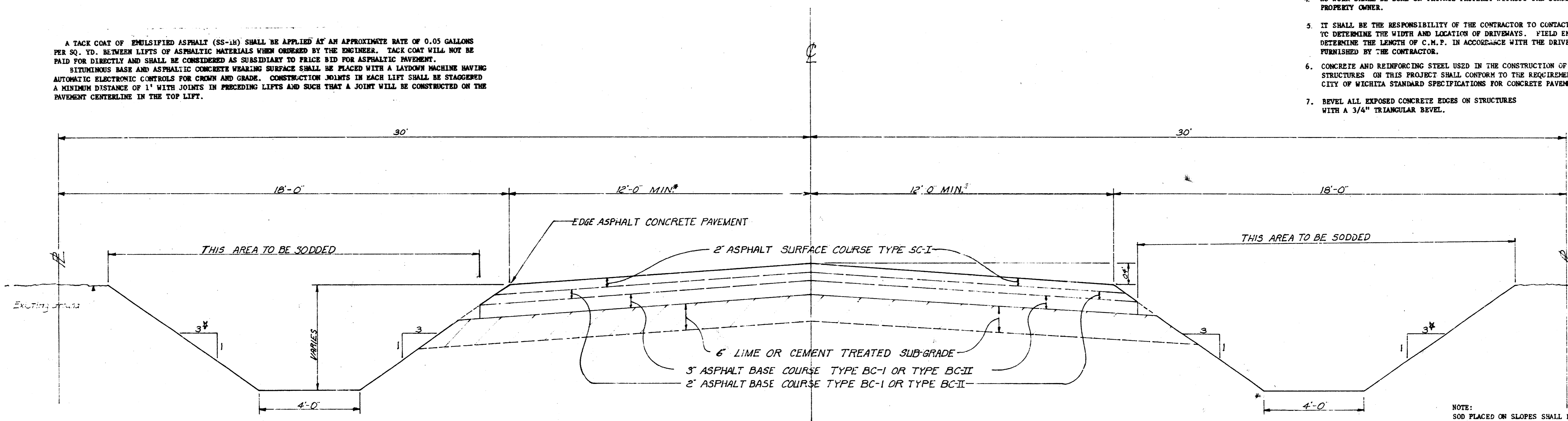


**WILLOW LANE
N. E. 13th TO AND INCLUDING CUL-DE-SAC**

A TACK COAT OF EMULSIFIED ASPHALT (SS-1M) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQ. YD. BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.
BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CURVE AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.

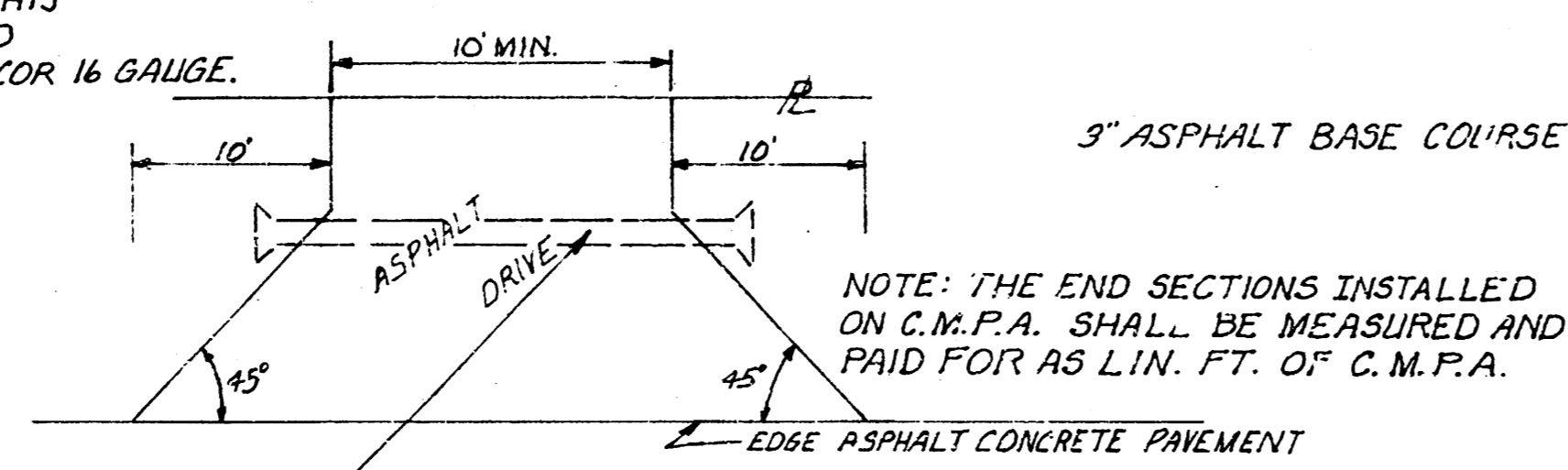
GENERAL NOTES

1. TWENTY-FOUR FOOT SHALL BE THE MAXIMUM PAY WIDTH FOR ASPHALT CONCRETE PAVEMENT EXCEPT FROM STA. 0+40 TO 0+11.18 EXCEPT IN INTERSECTIONS AND EXCEPT AT THE HAMMER HEAD AT THE NORTH END OF THE PROJECT. AT THESE LOCATIONS 1/2 PAY WIDTH SHALL BE MEASURED FROM ONE EDGE OF THE ASPHALT CONCRETE PAVEMENT TO THE OTHER EDGE OF THE ASPHALT CONCRETE PAVEMENT AND SHALL NOT EXCEED THE DIMENSIONS AS SHOWN ON THE PLANS.
2. THE PAY WIDTH FOR MANIPULATION SHALL EXTEND 2'-6" BEYOND THE MEASURED WIDTH OF THE ASPHALT CONCRETE PAVEMENT IN BOTH DIRECTIONS.
3. THREE TO ONE BACKSLOPES SHALL BE USED WHERE THE RIGHT-OF-WAY PERMITS. BACKSLOPES SHALL NOT EXTEND BEYOND FENCES.
4. NO WORK SHALL BE DONE ON PRIVATE PROPERTY WITHOUT THE CONSENT OF THE PROPERTY OWNER.
5. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT PROPERTY OWNERS TO DETERMINE THE WIDTH AND LOCATION OF DRIVEWAYS. FIELD ENGINEER SHALL DETERMINE THE LENGTH OF C.M.P. IN ACCORDANCE WITH THE DRIVEWAY WIDTHS FURNISHED BY THE CONTRACTOR.
6. CONCRETE AND REINFORCING STEEL USED IN THE CONSTRUCTION OF ALL CONCRETE STRUCTURES ON THIS PROJECT SHALL CONFORM TO THE REQUIREMENTS OF THE CITY OF WICHITA STANDARD SPECIFICATIONS FOR CONCRETE PAVEMENT.
7. REVEL ALL EXPOSED CONCRETE EDGES ON STRUCTURES WITH A 3/4" TRIANGULAR BEVEL.



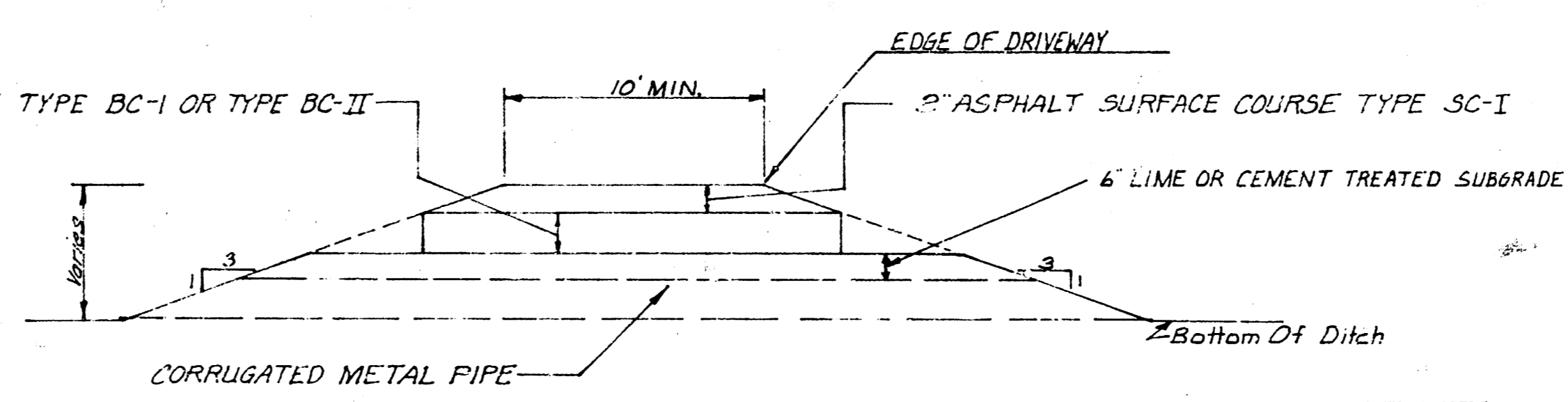
**TYPICAL SECTION ASPHALT CONCRETE PAVEMENT
BITUMINOUS BASE**

NOTE: ALL PIPE INSTALLED ON THIS PROJECT SHALL BE CORRUGATED METAL PIPE ARCH (C.M.P.A.) HEL-COR 16 GAUGE.



PLAN VIEW TYPICAL DRIVEWAY

NOTE: THE END SECTIONS INSTALLED ON C.M.P.A. SHALL BE MEASURED AND PAID FOR AS L.I.N. FT. OF C.M.P.A.

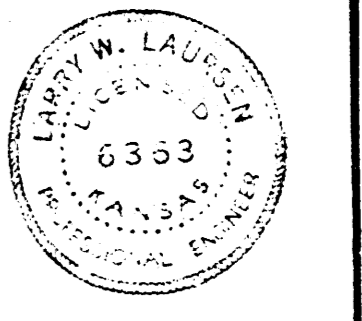
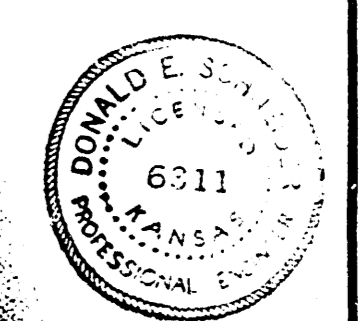


TYPICAL SECTION DRIVEWAY

DRIVEWAY NOTES

1. THE COMPLETION OF EARTHWORK AT DRIVEWAY LOCATIONS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED AS A SUBSIDIARY COST TO THE OTHER BID ITEMS ON THIS PROJECT.
2. THE PAY WIDTH FOR ASPHALT CONCRETE PAVEMENT USED AT DRIVEWAY LOCATIONS SHALL BE MEASURED FROM ONE EDGE OF THE DRIVEWAY TO THE OTHER EDGE OF THE DRIVEWAY AS NOTED ON DRIVEWAY TYPICAL SECTION.

NOTE: SOD PLACED ON SLOPES SHALL BE STAKED WITH FROM TWO TO FOUR STAKES PER SQUARE YARD OR ROLL, AS DETERMINED BY THE ENGINEER. STAKES SHALL BE OF LATH OR SIMILAR MATERIAL AND SHALL BE DRIVEN SIX INCHES INTO THE GROUND, LEAVING APPROXIMATELY ONE-HALF INCH OF THE TOP ABOVE THE SOD L'VE.
NOTE: THE REMOVAL OF CONCRETE HEADMILLS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID PER L.I.N. FOOT OF PIPE REMOVED.
NOTE: ALL ASPHALT MAT REMOVED ON THIS PROJECT SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE VOLUME OF EXCAVATION AND SHALL BE PAID FOR AS CU. YARDS OF EXCAVATION.



CITY OF WICHITA, KANSAS

DEPARTMENT OF PUBLIC WORKS - ENGINEERING

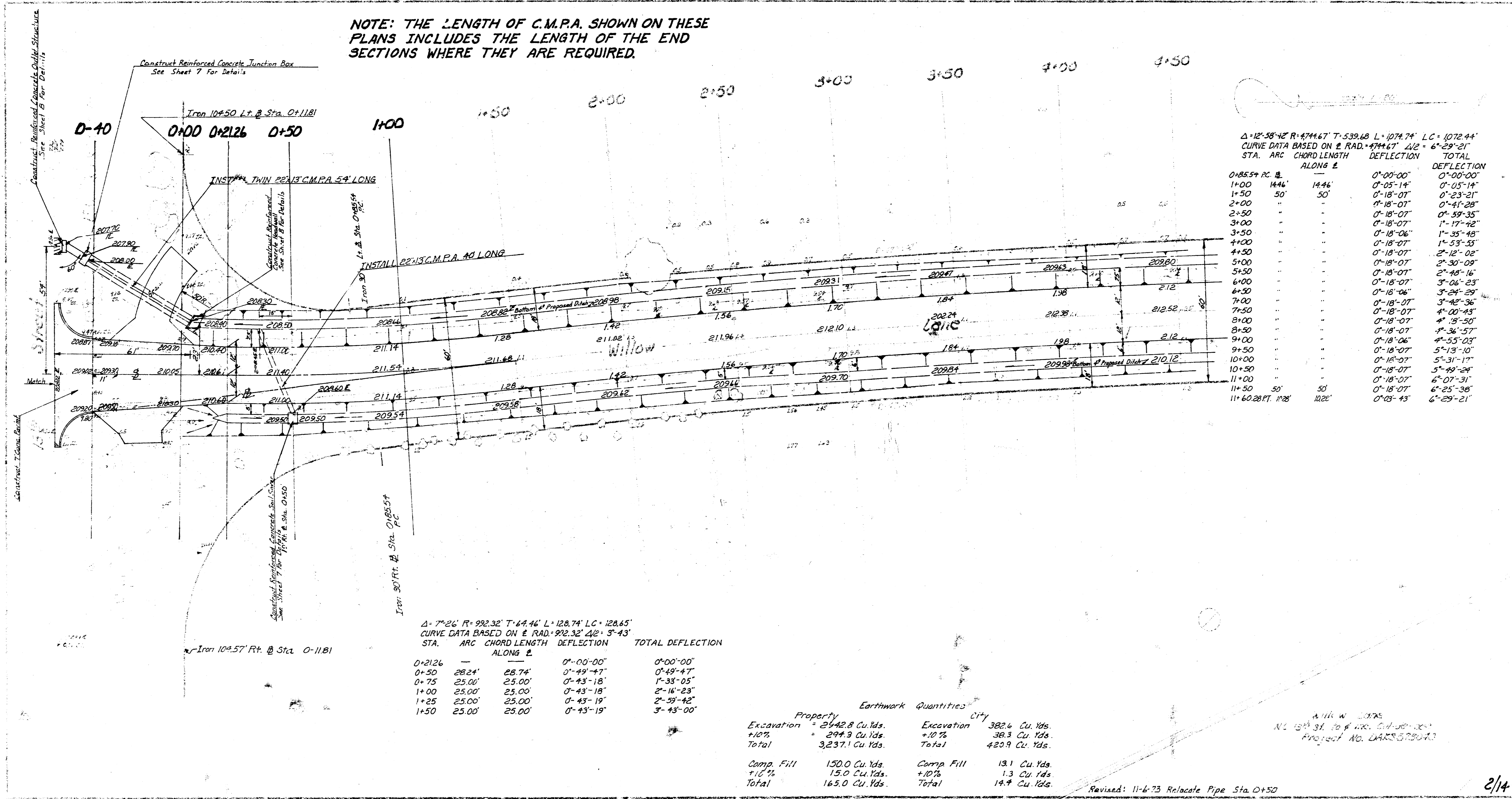
R. W. LINN CITY ENGINEER

DATE: 7-73

PROJECT NO. DAKS 573040
Revised: 11-6-73

1/4

NOTE: THE LENGTH OF C.M.P.A. SHOWN ON THESE PLANS INCLUDES THE LENGTH OF THE END SECTIONS WHERE THEY ARE REQUIRED.



$\Delta = 12^\circ 58' 42''$ R = 474.67' T = 539.18 L = 1074.74' LC = 1072.44'
 CURVE DATA BASED ON E. RAD. 474.67' $\Delta/2 = 6^\circ 29' 21''$
 STA. ARC CHORD LENGTH DEFLECTION TOTAL DEFLECTION

ALONG E	ALONG E	DEFLECTION	TOTAL
0+00	144'	0'-00'-00"	0'-00'-00"
1+00	144'	0'-05'-14"	0'-05'-14"
2+00	50'	0'-18'-07"	0'-23'-21"
3+00	"	0'-18'-07"	0'-41'-28"
4+00	"	0'-18'-07"	0'-59'-35"
5+00	"	0'-18'-07"	1'-17'-42"
6+00	"	0'-18'-06"	1'-35'-48"
7+00	"	0'-18'-07"	1'-53'-55"
8+00	"	0'-18'-07"	2'-12'-02"
9+00	"	0'-18'-07"	2'-30'-09"
10+00	"	0'-18'-07"	2'-48'-16"
11+00	"	0'-18'-06"	3'-06'-23"
12+00	"	0'-18'-07"	3'-24'-29"
13+00	"	0'-18'-07"	3'-42'-36"
14+00	"	0'-18'-07"	4'-00'-43"
15+00	"	0'-18'-07"	4'-18'-50"
16+00	"	0'-18'-07"	4'-36'-57"
17+00	"	0'-18'-06"	4'-55'-03"
18+00	"	0'-18'-07"	5'-13'-10"
19+00	"	0'-18'-07"	5'-31'-17"
20+00	"	0'-18'-07"	5'-49'-24"
21+00	"	0'-18'-07"	6'-07'-31"
22+00	50'	0'-18'-07"	6'-25'-38"
23+00	132'	0'-03'-43"	6'-29'-21"

$\Delta = 7^\circ 26'$ R = 992.32' T = 44.46' L = 128.74' LC = 128.65'
 CURVE DATA BASED ON E. RAD. 992.32' $\Delta/2 = 3^\circ 43'$
 STA. ARC CHORD LENGTH DEFLECTION TOTAL DEFLECTION

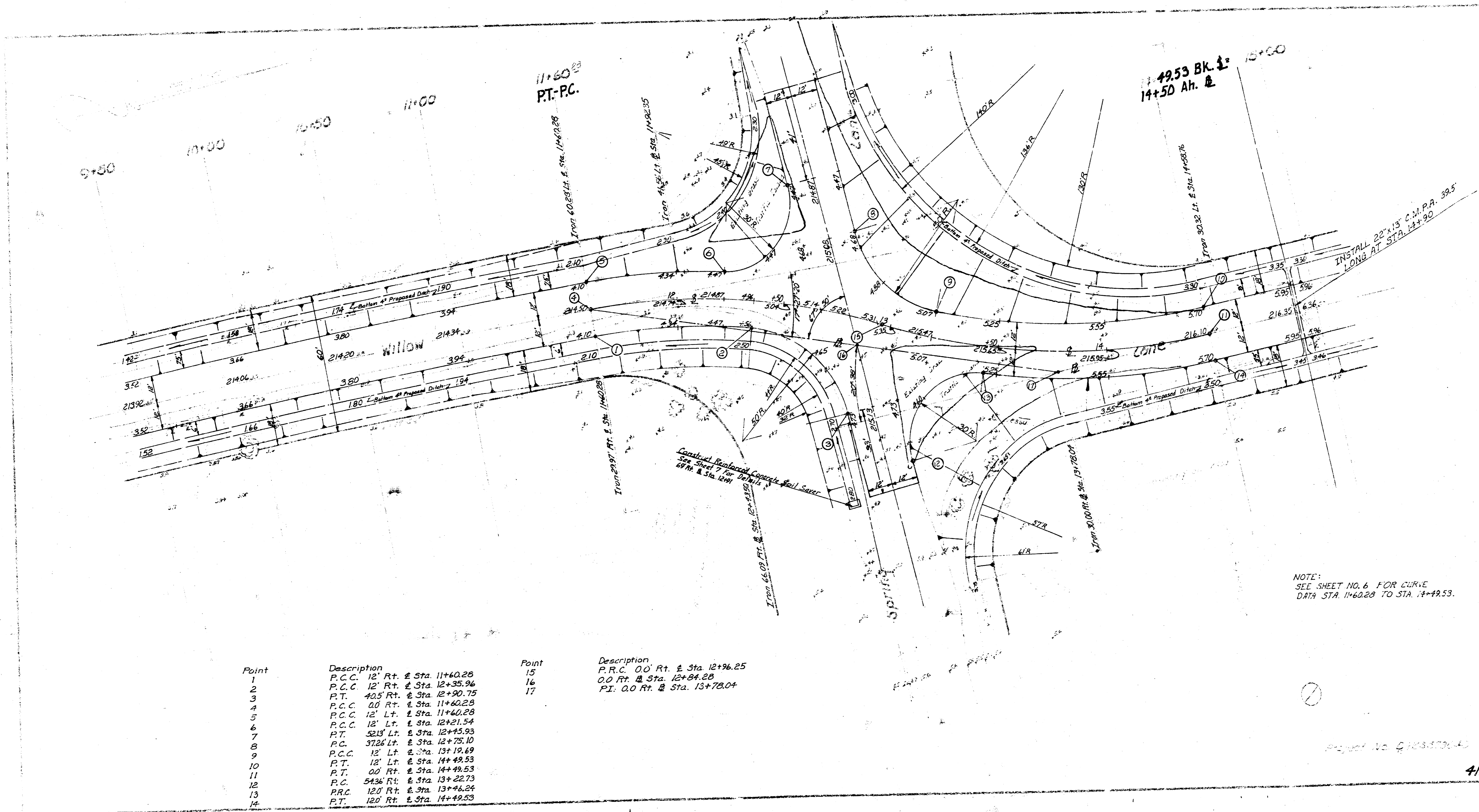
ALONG E	ALONG E	DEFLECTION	TOTAL
0+21.26	-	0'-00'-00"	0'-00'-00"
0+50	28.74'	0'-49'-47"	0'-49'-47"
0+75	25.00'	0'-43'-18"	1'-33'-05"
1+00	25.00'	0'-43'-18"	2'-16'-23"
1+25	25.00'	0'-43'-19"	2'-59'-42"
1+50	25.00'	0'-43'-19"	3'-43'-00"

Earthwork Quantities

Property	Excavation	Comp. Fill	City	Excavation	Comp. Fill
	2642.8 Cu. Yds.	150.0 Cu. Yds.		382.6 Cu. Yds.	13.1 Cu. Yds.
+10%	294.3 Cu. Yds.	15.0 Cu. Yds.		38.3 Cu. Yds.	1.3 Cu. Yds.
Total	3237.1 Cu. Yds.	165.0 Cu. Yds.		420.9 Cu. Yds.	14.4 Cu. Yds.

A. W. W. 2075
 N.E. 15th St. To 8th Ave. Cul-de-sac
 Project No. 24K5323073

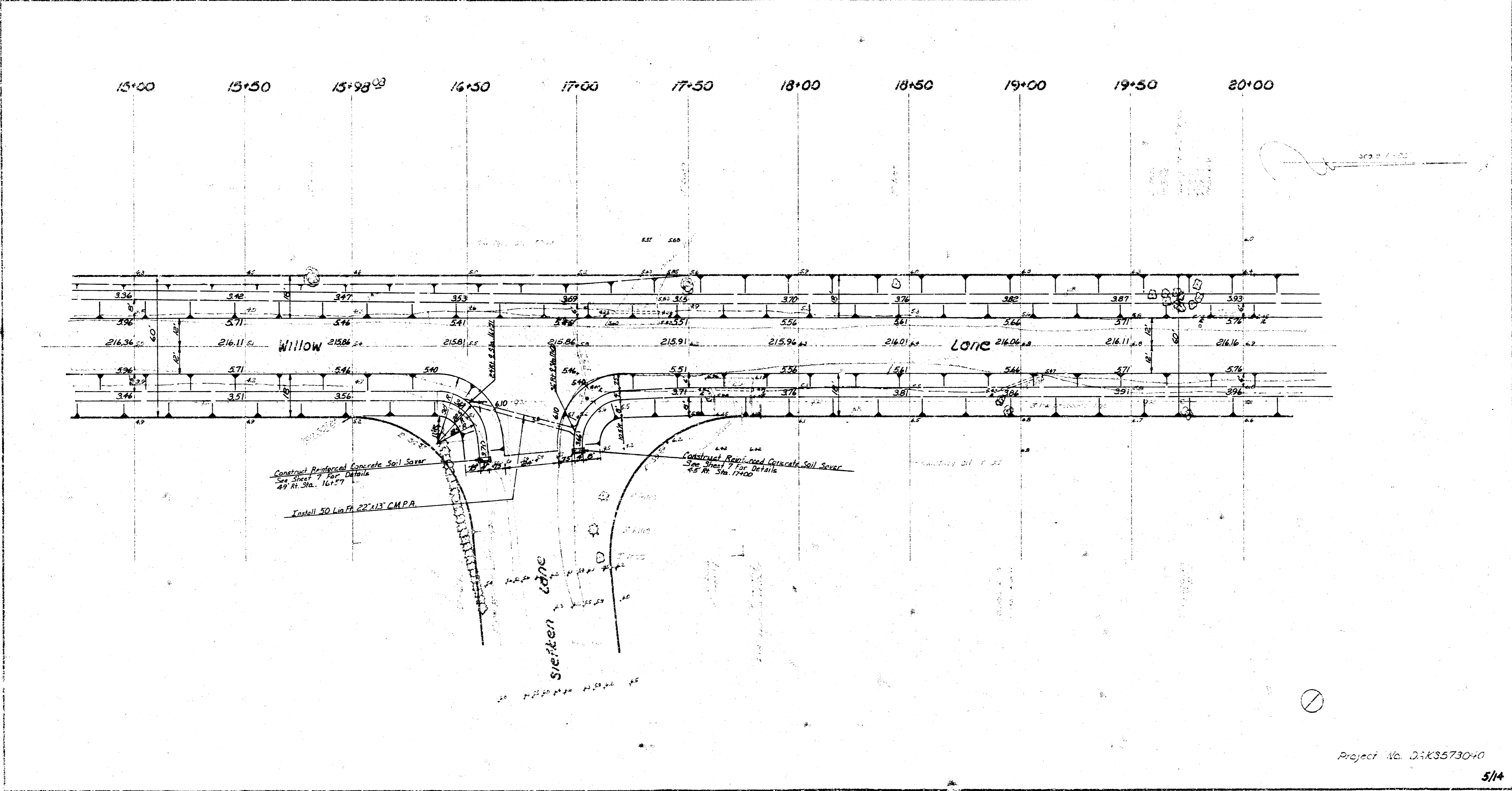
Revised: 11-6-73 Relocate Pipe Sta. 0+50

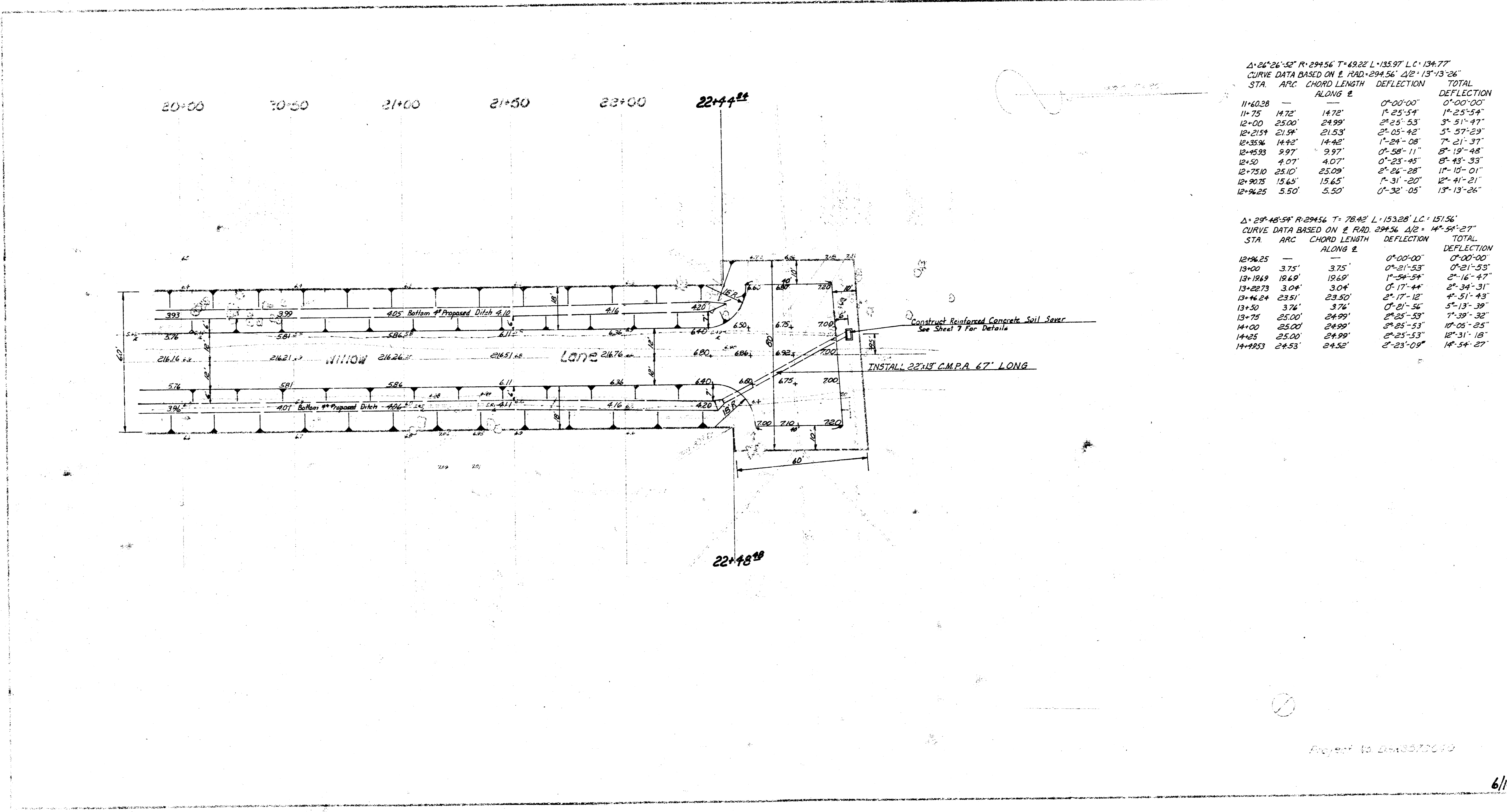


NOTE:
SEE SHEET NO. 6 FOR CURVE
DATA STA. 11+60.28 TO STA. 14+49.53.

Point	Description	Point	Description
1	P.C.C. 12' Rt. @ Sta. 11+40.28	15	P.R.C. 0.0' Rt. @ Sta. 12+96.25
2	P.C.C. 12' Rt. @ Sta. 12+35.94	16	0.0' Rt. @ Sta. 12+84.28
3	P.T. 40.5' Rt. @ Sta. 12+90.75	17	P.I. 0.0' Rt. @ Sta. 13+78.04
4	P.C.C. 0.0' Rt. @ Sta. 11+60.28		
5	P.C.C. 12' Lt. @ Sta. 11+40.28		
6	P.C.C. 12' Lt. @ Sta. 12+21.54		
7	P.T. 52.5' Lt. @ Sta. 12+45.93		
8	P.C. 37.26' Lt. @ Sta. 12+72.10		
9	P.C.C. 12' Lt. @ Sta. 13+19.69		
10	P.T. 12' Lt. @ Sta. 14+49.53		
11	P.T. 0.0' Rt. @ Sta. 14+49.53		
12	P.C. 54.36' Rt. @ Sta. 13+22.73		
13	P.R.C. 12.0' Rt. @ Sta. 13+94.24		
14	P.T. 12.0' Rt. @ Sta. 14+49.53		

Project No. 2703-570-40





$\Delta = 24^{\circ}26'32''$ R = 29456' T = 4922' L = 13597' LC = 13477'
 CURVE DATA BASED ON E. RAD. 29456' $\Delta/2 = 12^{\circ}13'13.26''$
 STA. ARC CHORD LENGTH DEFLECTION TOTAL DEFLECTION

ALONG E	ALONG E	DEFLECTION	TOTAL DEFLECTION
11+40.28	—	0°-00'-00"	0°-00'-00"
11+75	14.72'	1°-25'-54"	1°-25'-54"
12+00	25.00'	2°-25'-53"	3°-51'-47"
12+21.54	21.54'	2°-05'-42"	5°-57'-29"
12+35.96	14.42'	1°-24'-08"	7°-21'-37"
12+45.93	9.97'	0°-58'-11"	8°-19'-48"
12+50	4.07'	0°-23'-45"	8°-43'-33"
12+75.10	25.10'	2°-24'-28"	11°-10'-01"
12+90.75	15.65'	1°-31'-20"	12°-41'-21"
12+94.25	5.50'	0°-32'-05"	13°-13'-26"

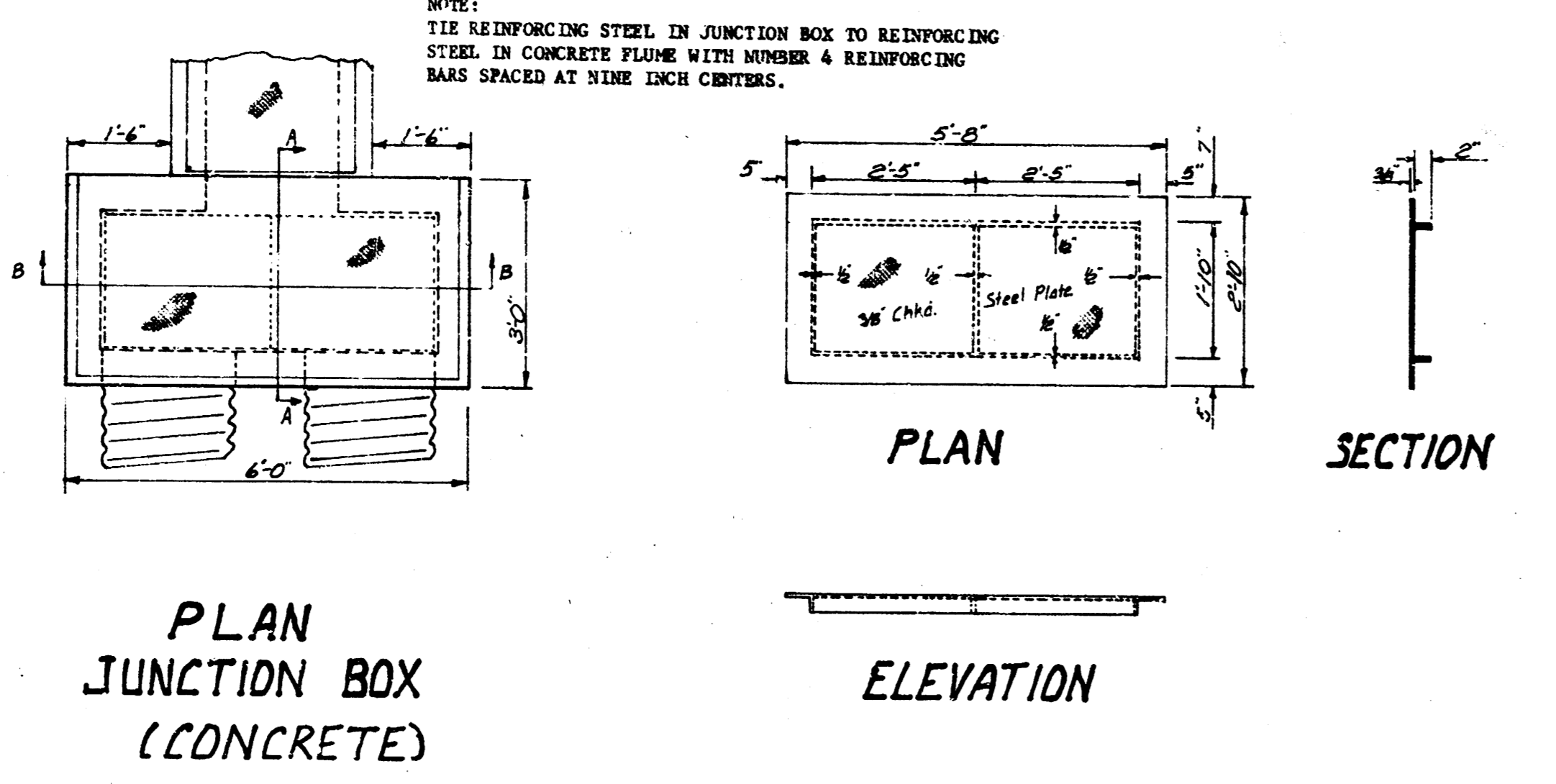
$\Delta = 29^{\circ}48'54''$ R = 29456' T = 7842' L = 15328' LC = 15156'
 CURVE DATA BASED ON E. RAD. 29456' $\Delta/2 = 14^{\circ}54'27''$
 STA. ARC CHORD LENGTH DEFLECTION TOTAL DEFLECTION

ALONG E	ALONG E	DEFLECTION	TOTAL DEFLECTION
12+96.25	—	0°-00'-00"	0°-00'-00"
13+00	3.75'	0°-21'-53"	0°-21'-53"
13+19.49	19.49'	1°-54'-54"	2°-16'-47"
13+22.73	3.04'	0°-17'-44"	2°-34'-31"
13+46.24	23.51'	2°-17'-12"	4°-51'-43"
13+50	3.76'	0°-21'-56"	5°-13'-39"
13+75	25.00'	2°-25'-53"	7°-39'-32"
14+00	25.00'	2°-25'-53"	10°-05'-25"
14+25	25.00'	2°-25'-53"	12°-31'-18"
14+49.53	24.53'	2°-23'-09"	14°-54'-27"

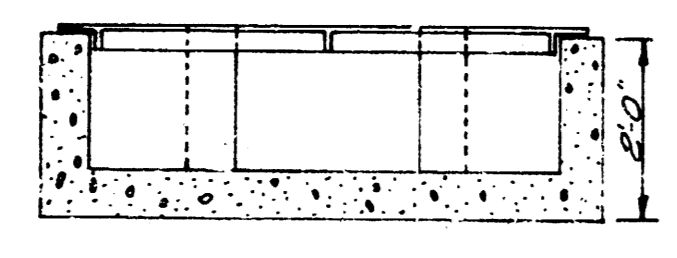
Construct Reinforced Concrete Soil Saver
See Sheet 7 For Details

INSTALL 22#4 C.M.P.A. 67' LONG

Project to 2010/07/09

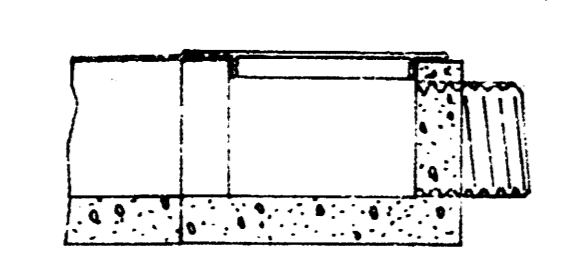


STRUCTURAL STEEL JUNCTION BOX COVER



SECTION BB

NOTE: REINFORCING STEEL SHALL BE PLACED IN STRUCTURES SO THAT THERE WILL BE 2" CLEARANCE FROM THE BOTTOM OF THE STRUCTURE AND 2" CLEARANCE FROM THE OUTSIDE OF THE WALLS OF THE STRUCTURE.



SECTION AA

NOTE: SHAPE FLOOR TO DRAIN WITH EIGHT SACK CONCRETE SAND MIX AS PER CITY OF WICHITA ASPHALT CONCRETE PAVEMENT SPECIFICATIONS.

NOTE: THE FLOOR AND THE WALLS SHALL BE SIX INCHES IN THICKNESS IN THIS JUNCTION BOX.

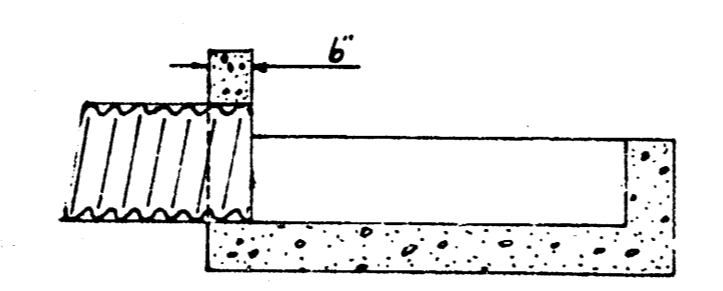
NOTE: ALL COSTS OF CONCRETE, REINFORCING STEEL, STRUCTURAL STEEL, EXCAVATION AND ANY OTHER INCIDENTAL COSTS INCURRED IN CONSTRUCTION OF THIS CONCRETE JUNCTION BOX SHALL BE INCLUDED IN THE BID PRICE OF REINFORCED CONCRETE JUNCTION BOX.

NOTE: NUMBER 4 REINFORCING BARS SHALL BE PLACED ON NINE INCH CENTERS IN THE FLOOR OF THIS STRUCTURE WITH STEEL RUNNING IN BOTH DIRECTIONS. THE REINFORCING STEEL TO THE FLOOR SHALL BE BENT UP AND EXTENDED INTO THE WALLS A DISTANCE OF ONE FOOT.

NOTE: ALL COSTS OF CONCRETE, REINFORCING STEEL, EXCAVATION AND ANY OTHER INCIDENTAL COSTS INCURRED IN CONSTRUCTION OF THIS CONCRETE SOIL SAVER SHALL BE INCLUDED IN THE BID PRICE OF REINFORCED CONCRETE SOIL SAVER.

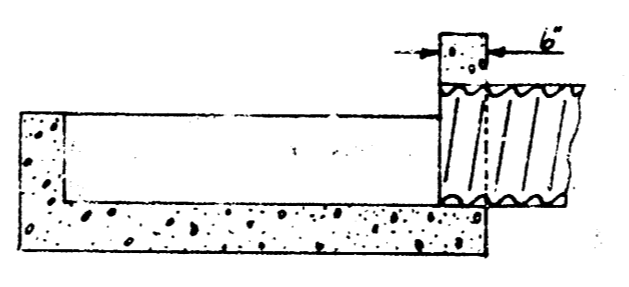
NOTE: SHAPE FLOOR TO DRAIN WITH EIGHT SACK CONCRETE SAND MIX AS PER CITY OF WICHITA ASPHALT CONCRETE PAVEMENT SPECIFICATIONS.

NOTE: REINFORCING STEEL SHALL BE PLACED IN STRUCTURES SO THAT THERE WILL BE 2" CLEARANCE FROM THE BOTTOM OF THE STRUCTURE AND 2" CLEARANCE FROM THE OUTSIDE OF THE WALLS OF THE STRUCTURE.



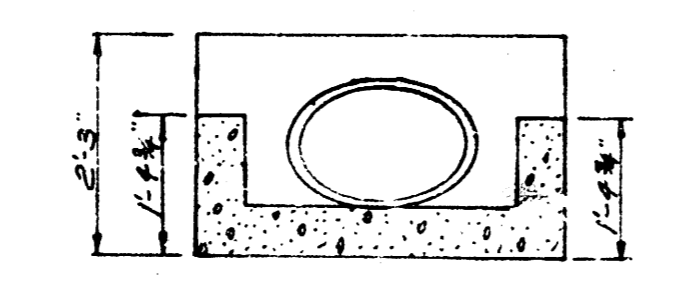
SECTION BB

NOTE: NUMBER 4 REINFORCING BARS SHALL BE PLACED ON ONE FOOT CENTERS IN THE FLOOR OF THIS STRUCTURE WITH STEEL RUNNING IN BOTH DIRECTIONS. THE REINFORCING STEEL IN THE FLOOR SHALL BE BENT UP AND EXTENDED INTO THE WALLS A DISTANCE OF ONE FOOT WHERE THIS IS POSSIBLE.

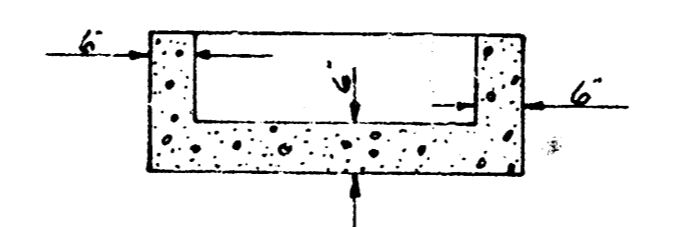


SECTION AA

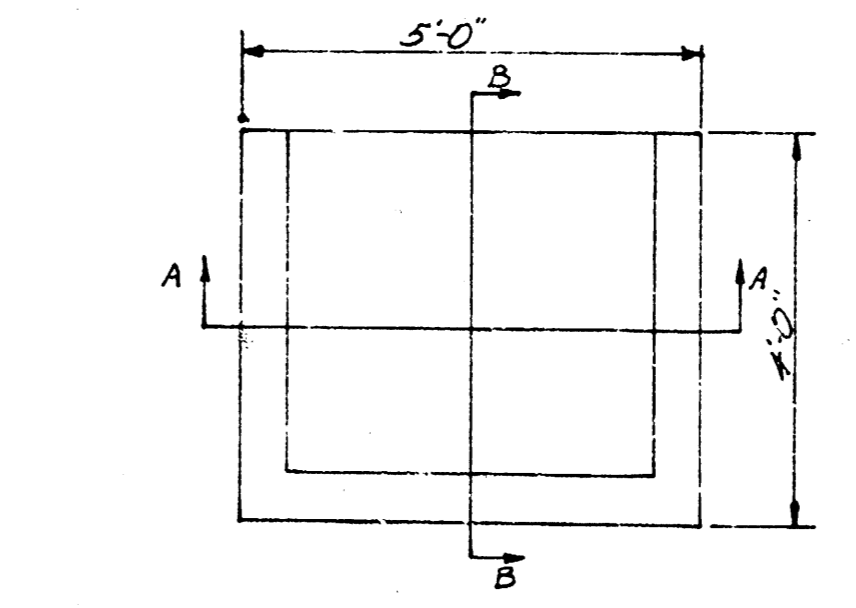
PLAN SOIL SAVER (CONCRETE)
19' RT. & STA. 0+50



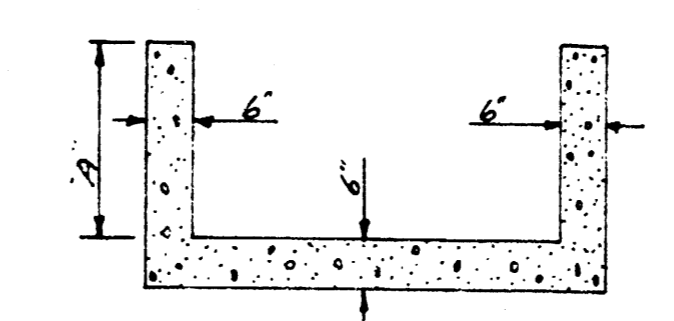
SECTION CC



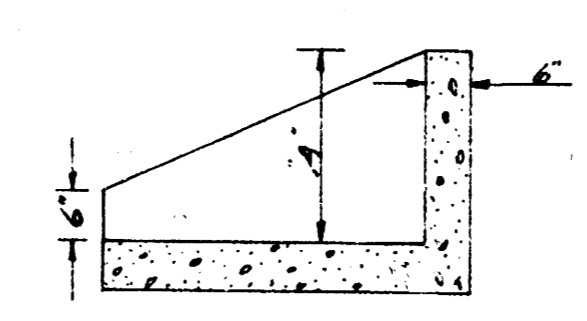
SECTION DD



PLAN SOIL SAVER (CONCRETE)
49' RT. & STA. 16+57
45' RT. & STA. 17+00



SECTION AA



SECTION BB

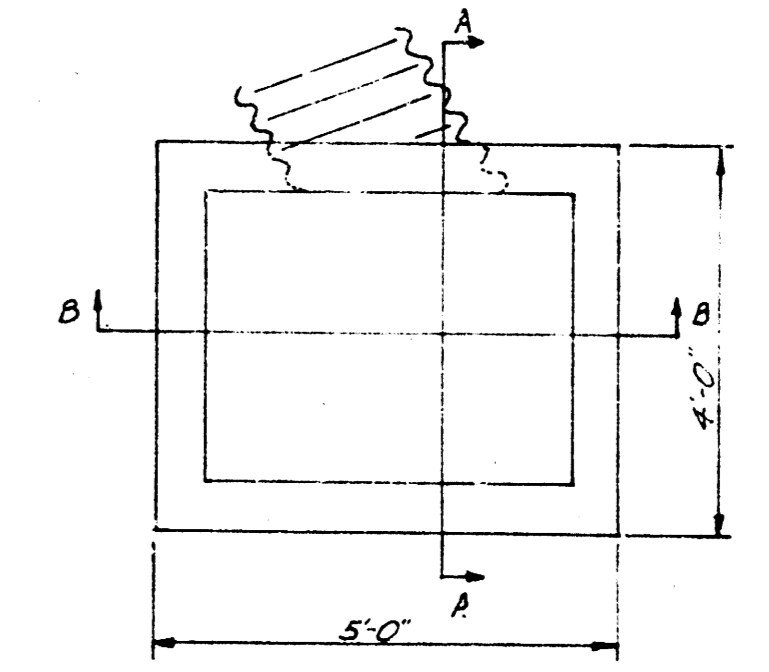
STATION	A'
49' RT. & STA. 12+91	1'-6"
49' RT. & STA. 16+57	1'-6"
45' RT. & STA. 17+00	1'-6"

NOTE: ALL COSTS OF CONCRETE, REINFORCING STEEL, EXCAVATION AND ANY OTHER INCIDENTAL COSTS INCURRED IN CONSTRUCTION OF THIS CONCRETE SOIL SAVER SHALL BE INCLUDED IN THE BID PRICE OF REINFORCED CONCRETE SOIL SAVER.

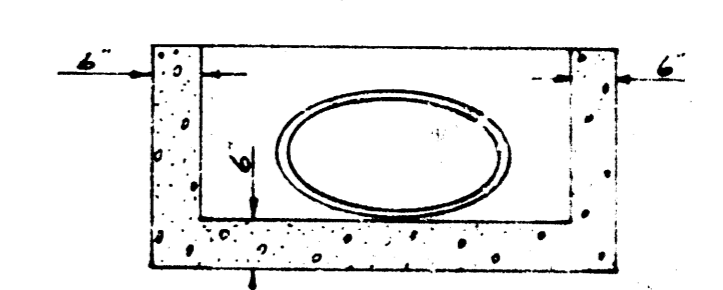
NOTE: SHAPE FLOOR TO DRAIN WITH EIGHT SACK CONCRETE SAND MIX AS PER CITY OF WICHITA ASPHALT CONCRETE PAVEMENT SPECIFICATIONS.

NOTE: REINFORCING STEEL SHALL BE PLACED IN STRUCTURES SO THAT THERE WILL BE 2" CLEARANCE FROM THE BOTTOM OF THE STRUCTURE AND 2" CLEARANCE FROM THE OUTSIDE OF THE WALLS OF THE STRUCTURE.

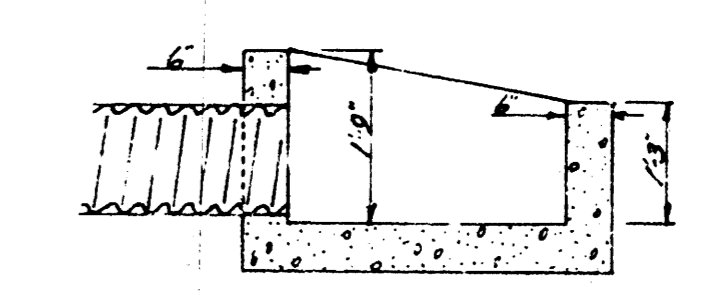
NOTE: NUMBER 4 REINFORCING BARS SHALL BE PLACED ON ONE FOOT CENTERS IN THE FLOOR OF THIS STRUCTURE WITH STEEL RUNNING IN BOTH DIRECTIONS. THE REINFORCING STEEL IN THE FLOOR SHALL BE BENT UP AND EXTENDED INTO THE WALLS A DISTANCE OF ONE FOOT WHERE THIS IS POSSIBLE.



PLAN SOIL SAVER (CONCRETE)
8.5' LT. & STA. 23+02



SECTION BB



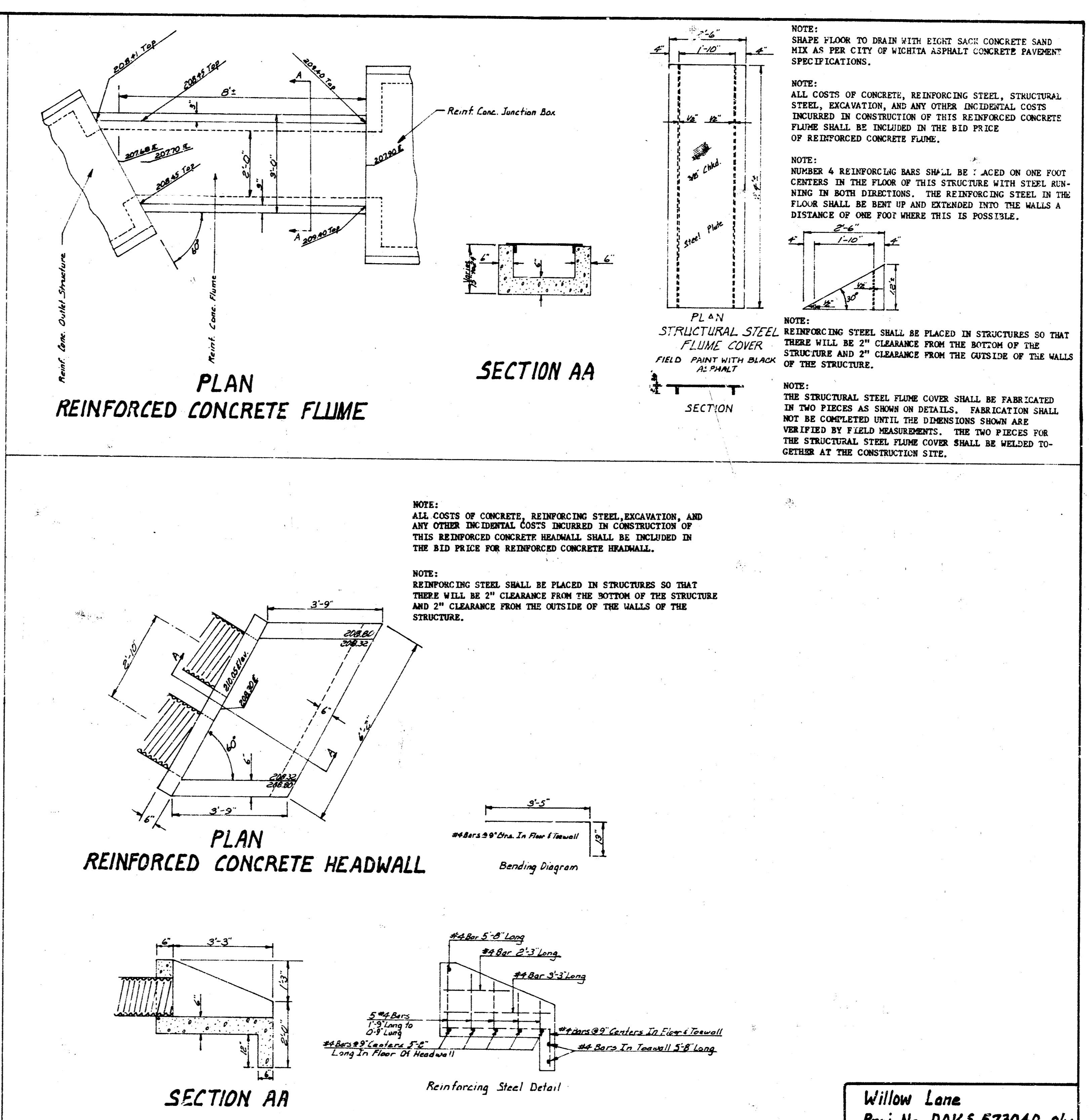
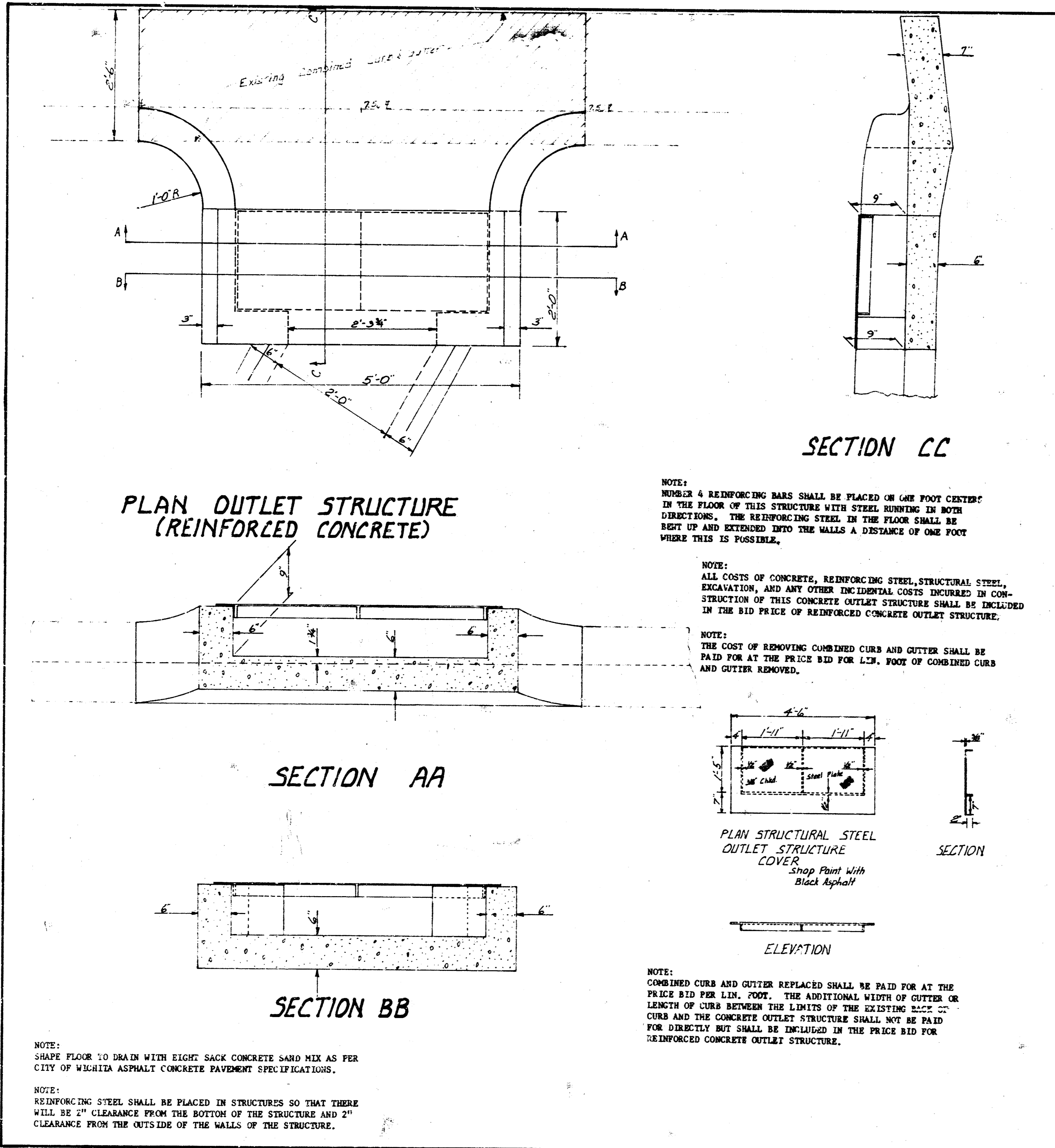
SECTION AA

NOTE: ALL COSTS OF CONCRETE, REINFORCING STEEL, EXCAVATION AND ANY OTHER INCIDENTAL COSTS INCURRED IN CONSTRUCTION OF THIS CONCRETE SOIL SAVER SHALL BE INCLUDED IN THE BID PRICE OF REINFORCED CONCRETE SOIL SAVER.

NOTE: SHAPE FLOOR TO DRAIN WITH EIGHT SACK CONCRETE SAND MIX AS PER CITY OF WICHITA ASPHALT CONCRETE PAVEMENT SPECIFICATIONS.

NOTE: REINFORCING STEEL SHALL BE PLACED IN STRUCTURES SO THAT THERE WILL BE 2" CLEARANCE FROM THE BOTTOM OF THE STRUCTURE AND 2" CLEARANCE FROM THE OUTSIDE OF THE WALLS OF THE STRUCTURE.

NOTE: NUMBER 4 REINFORCING BARS SHALL BE PLACED ON ONE FOOT CENTERS IN THE FLOOR OF THIS STRUCTURE WITH STEEL RUNNING IN BOTH DIRECTIONS. THE REINFORCING STEEL IN THE FLOOR SHALL BE BENT UP AND EXTENDED INTO THE WALLS A DISTANCE OF ONE FOOT WHERE THIS IS POSSIBLE.



Willow Lane
 Proj. No. DAKS 573040 8/4