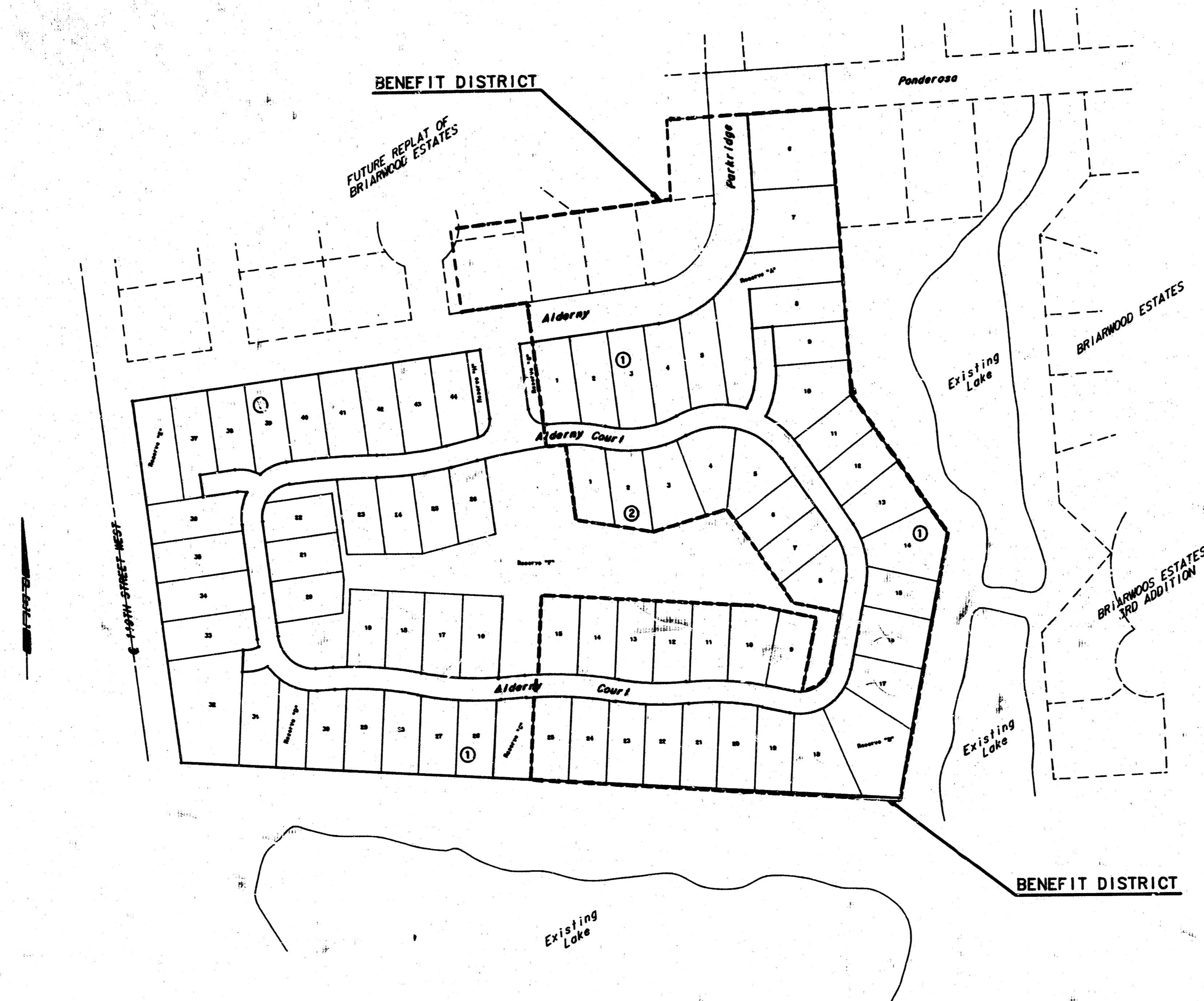


CITY OF WICHITA, KANSAS  
**ALDERNY COURT - E. L. LOT 26, BLK. 1 TO S. L. ALDERNY**  
**ALDERNY / PARKRIDGE - E. L. LOT 44, BLK. 1 TO S. L. PONDEROSA**  
**PONDEROSA - W. L. PARKRIDGE TO E. L. BLK. 1**  
**(BRIARWOOD ESTATES 4TH ADDITION)**  
 CITY OF WICHITA PROJECT NO. 472-76-245-81375-000-000-001

INDEX OF SHEETS

SHEET NO. 1	TITLE SHEET
SHEET NO. 2	PLAT (SURVEY CONTROL)
SHEET NO. 3	TYPICAL SECTION
SHEET NO. 4-7	PLAN SHEETS
SHEET NO. 8	CURVE TABLES
SHEET NO. 9	PLAN-PROFILE STORM SEWER
SHEET NO. 10	STANDARD TYPE 1A CURB INLET
SHEET NO. 11	FRAME AND COVER DETAIL



GENERAL NOTES

1. EARTHWORK FOR THIS PROJECT SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE BID FOR THE ITEM "EARTHWORK". THIS WORK SHALL INCLUDE ALL EXCAVATION, COMPACTED FILL, MANIPULATED FILL, BORROW MATERIAL, LOOSE FILL, WASTE, WATER, AND INCIDENTALS NECESSARY TO BUILD THE SUBGRADE TO THE LINES AND GRADES SHOWN ON THE PLANS.  
 THE CONTRACTOR SHALL VIEW THE SITE PRIOR TO SUBMITTING HIS BID TO ASCERTAIN THE SCOPE OF EARTHWORK INVOLVED BASED UPON EXISTING GRADE STAKES. ADDITIONAL MATERIAL FOR BORROW, IF REQUIRED IS AVAILABLE ON SITE.
2. ONLY ONE TYPE OF SUBGRADE STABILIZATION MATERIAL, PEBBLE LIME, SHALL BE ALLOWED. THE RATE OF APPLICATION SHALL BE DETERMINED BY CITY OF WICHITA PERSONNEL. THE QUANTITY OF PEBBLE LIME INCLUDED IN THE BID DOCUMENTS IS FOR THE SOLE PURPOSE OF SOLICITING COMPETITIVE BIDS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR SHALL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAW.
4. THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
5. THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2-1/2" BITUMINOUS BASE.
6. A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
7. BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
8. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.



**JUNE, 1984**  
 PLANS PREPARED BY  
**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
**ENGINEERS**  
**WICHITA, KANSAS**

*Note: Expansion Joints Shall Be Constructed At All Radius Points On This Project. A Radius Point Shall Be Defined To Include All P.C.'s, P.T.'s, P.R.C.'s & P.C.C.'s.*

# Briarwood Estates 4th Addition

To Wichita, Sedgwick County, Kansas

We, the Mellon Bank, N.A., Pittsburg, Pa., holder of a mortgage on the above described property, do hereby consent to the plotting of Briarwood Estates 4th Addition to Wichita, Sedgwick County, Kansas.

State of Kansas  
County of Sedgwick

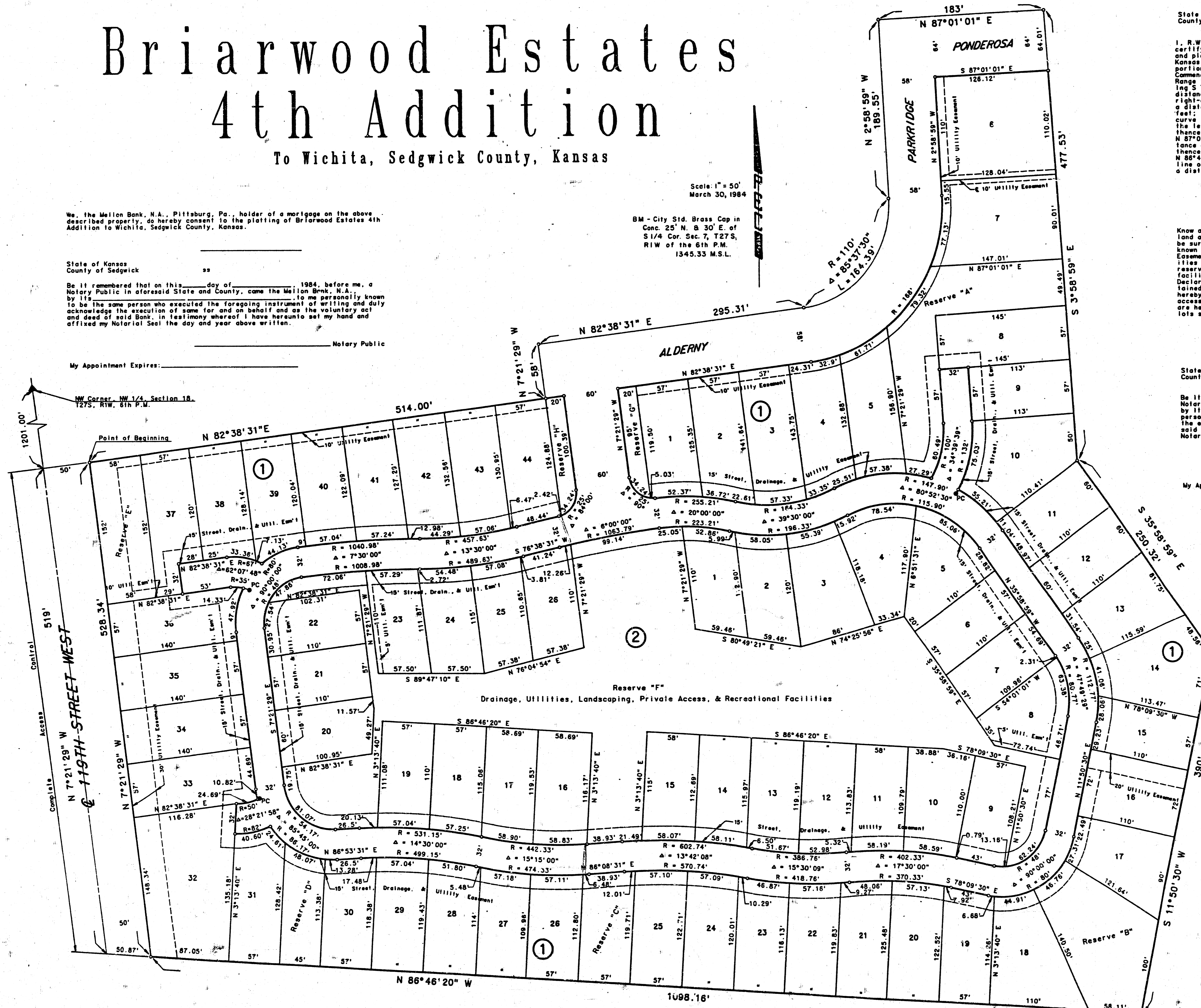
Be it remembered that on this \_\_\_\_\_ day of \_\_\_\_\_, 1984, before me, a Notary Public in aforesaid State and County, came the Mellon Bank, N.A., to me personally known to be the same person who executed the foregoing instrument of writing and duly acknowledge the execution of same for and on behalf and as the voluntary act and deed of said Bank, in testimony whereof I have hereunto set my hand and affixed my Notarial Seal the day and year above written.

Notary Public

My Appointment Expires: \_\_\_\_\_

Scale: 1" = 50'  
March 30, 1984

BM - City Std. Brass Cap in  
Cone 25' N. & 30' E. of  
S 1/4 Cor. Sec. 7, T27S,  
R1W of the 6th P.M.  
1345.33 M.S.L.



State of Kansas  
County of Sedgwick

I, R.W. Linn, a Professional Engineer in aforesaid State and County, do hereby certify that on this \_\_\_\_\_ day of \_\_\_\_\_, 1984, I have caused to be surveyed and plotted "BRIARWOOD ESTATES 4TH ADDITION," to Wichita, Sedgwick County, Kansas, into lots, blocks, streets and reserves, the same being a replat of a portion of Briarwood Estates, Wichita, Sedgwick County, Kansas, described as: Commencing at the NW Corner of the NW 1/4 of Section 18, Township 27 South, Range 1 West of the 6th P.M.; thence along the west line of said NW 1/4 bearing S 72°21'29" E a distance of 1201.00 feet; thence bearing N 82°38'31" E a distance of 50.00 feet to the point of beginning, said point being on the east right-of-way line of 119th Street West; thence continuing bearing N 82°38'31" E a distance of 514.00 feet; thence bearing N 7°21'29" W a distance of 58.00 feet; thence bearing N 82°38'31" E a distance of 295.31 feet to the P.C. of a curve to the left having a radius of 102.00 feet; thence along said curve to the left through a central angle of 82°37'30" an arc distance of 162.39 feet; thence bearing N 2°58'59" W a distance of 189.55 feet; thence bearing N 87°01'01" E a distance of 126.12 feet; thence bearing S 3°58'59" E a distance of 477.53 feet; thence bearing S 35°59'59" E a distance of 250.32 feet; thence bearing S 11°50'30" W a distance of 390.00 feet; thence bearing N 86°46'20" W a distance of 1098.16 feet to a point on the east right-of-way line of 119th Street West; thence along said right-of-way bearing N 72°21'29" W a distance of 528.34 feet to the point of beginning.

R.W. Linn, P.E. No. 3684

Know all men by these presents that we, the undersigned property owners of the land as above set forth in the Engineer's Certificate have caused the land to be surveyed and plotted into lots, blocks, streets and reserves, the same to be known as "BRIARWOOD ESTATES 4TH ADDITION," to Wichita, Sedgwick County, Kansas. Easements, as indicated, for the construction and maintenance of public utilities and drainage, are hereby granted. The reserves, as indicated, shall be reserved for drainage, utilities, landscaping, private access, and recreational facilities in accordance with the restrictive covenants established by the Declaration of Landowners Association Agreement; and shall be owned and maintained by the Briarwood Estates Homeowners' Association. The streets are hereby dedicated to and for the use of the public. All abutters' rights of access to and from 119th Street West over and across the east line of Block 1 are hereby granted to the City of Wichita. Minimum Pad Elevation for all lots shall be 1335.00 M.S.L.

Inland Investment Co., Inc.

Larry A. Chambers President

State of Kansas  
County of Sedgwick

Be it remembered that on this \_\_\_\_\_ day of \_\_\_\_\_, 1984, before me, a Notary Public in aforesaid State and County, came Inland Investment Co., Inc., by its President, Larry A. Chambers, to me personally known to be the same person who executed the foregoing instrument of writing and duly acknowledge the execution of same for and on behalf and as the voluntary act and deed of said company, in testimony whereof I have hereunto set my hand and affixed my Notarial Seal the day and year above written.

Notary Public

My Appointment Expires: \_\_\_\_\_

This plat has been submitted to and approved by the Wichita-Sedgwick County Metropolitan Area Planning Commission, Wichita, Kansas.

Dated this \_\_\_\_\_ day of \_\_\_\_\_, 1984.

Robert K. Chisholm Chairman  
Robert A. Lakin Secretary

This plat approved and all dedications shown hereon are accepted by the City Commission of the City of Wichita, Kansas.

Dated this \_\_\_\_\_ day of \_\_\_\_\_, 1984.

Margalee Wright Mayor  
Donald C. Giesick City Clerk

This is to certify that this instrument was filed for record in the Register of Deeds Office at \_\_\_\_\_ m. on this \_\_\_\_\_ day of \_\_\_\_\_, 1984.

Bette F. McCarf Register of Deeds  
Pat Kettler Deputy

This plat approved and all dedications shown hereon are accepted by the Board of County Commissioners of Sedgwick County, Kansas.

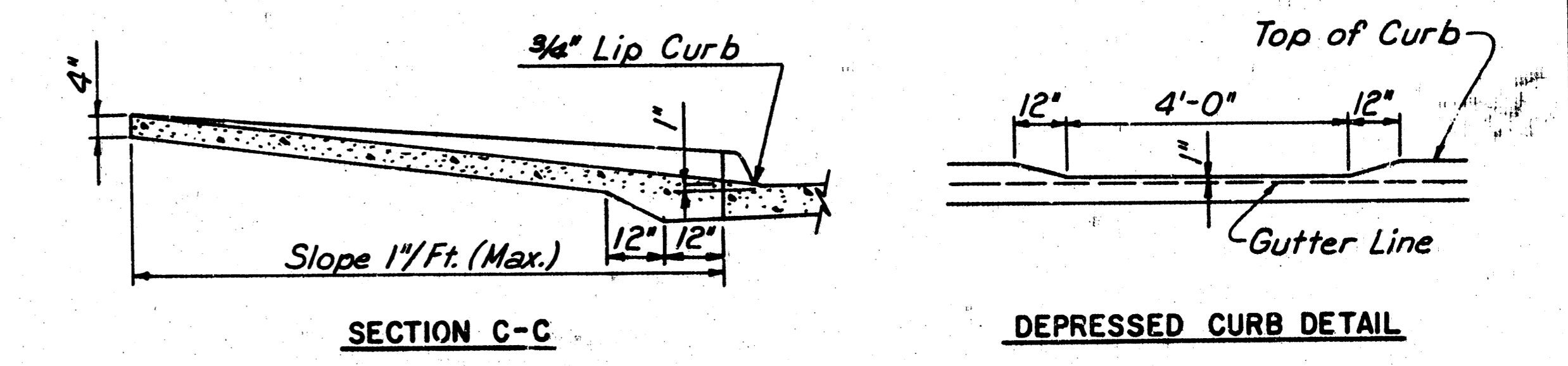
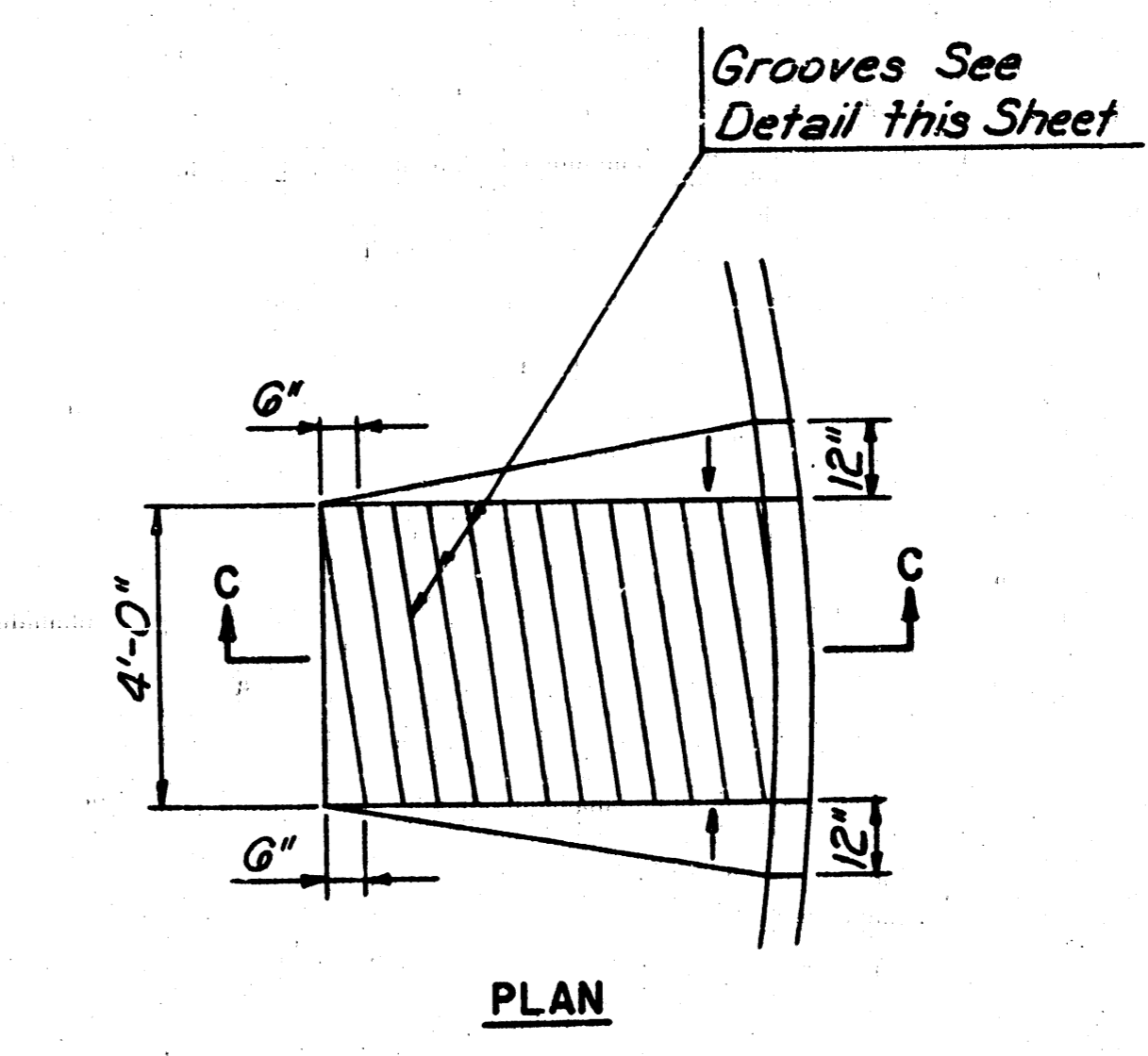
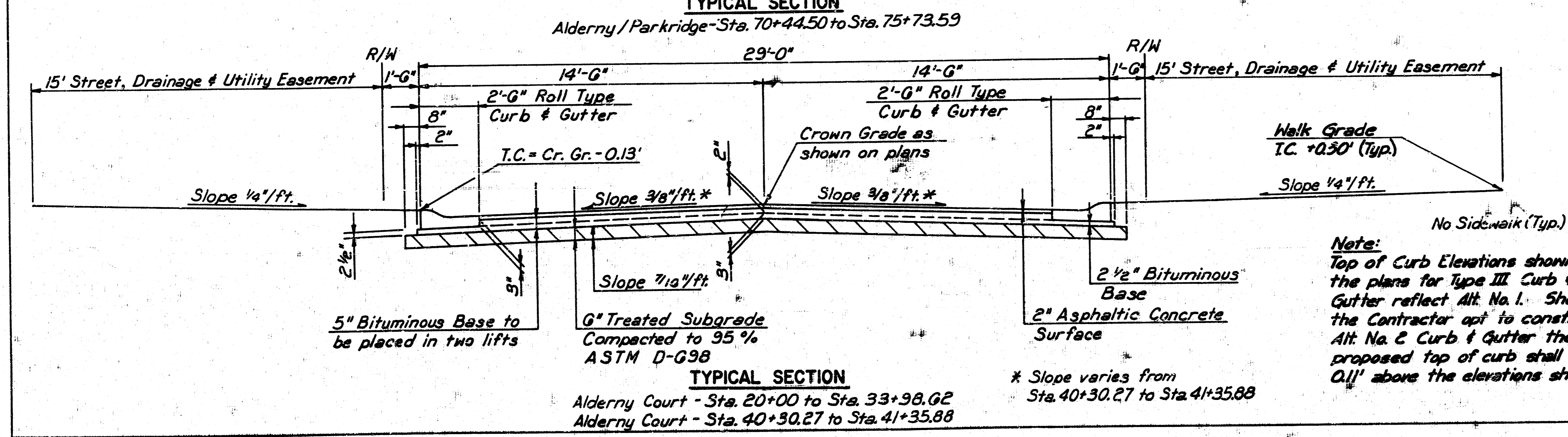
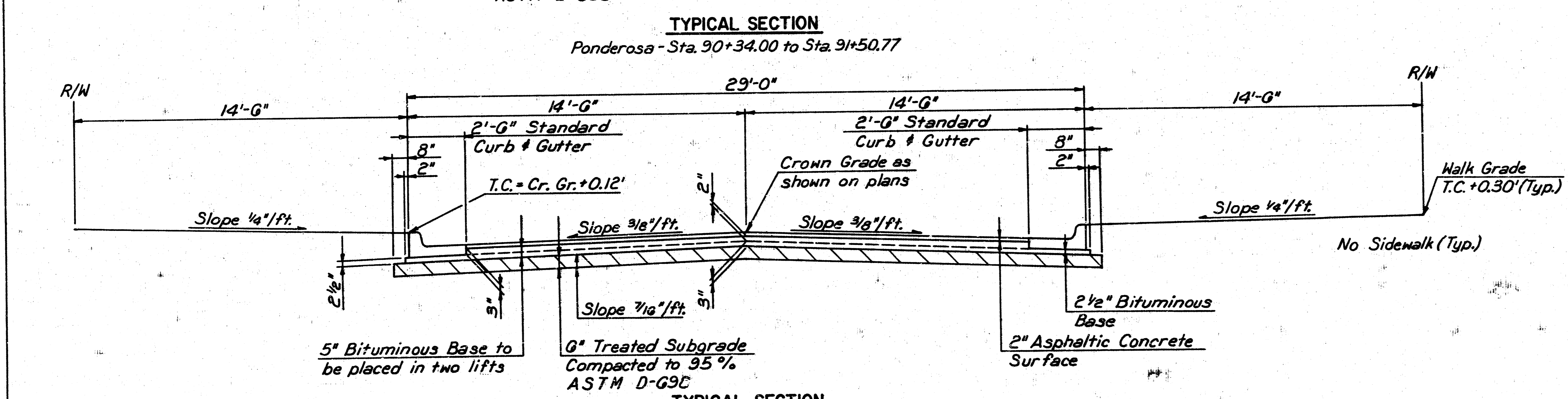
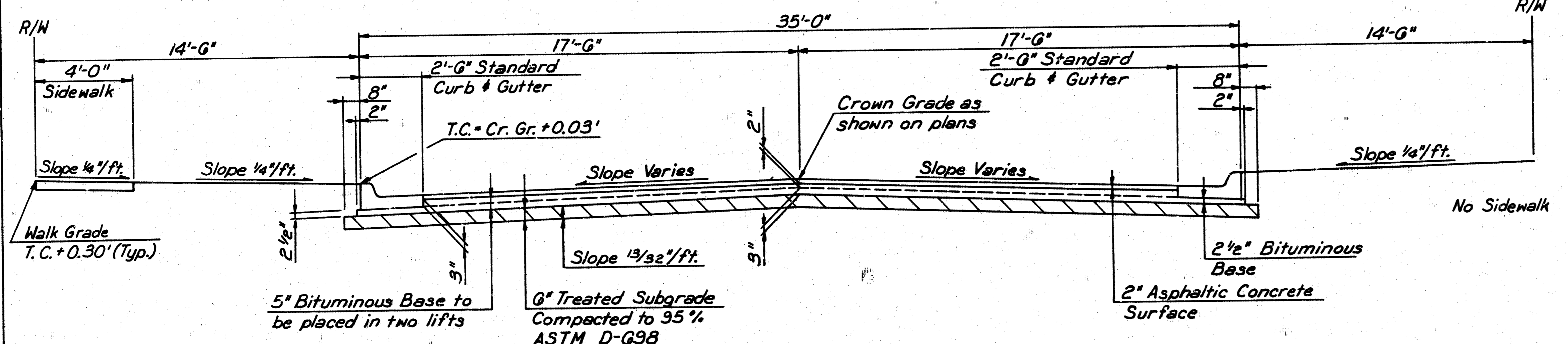
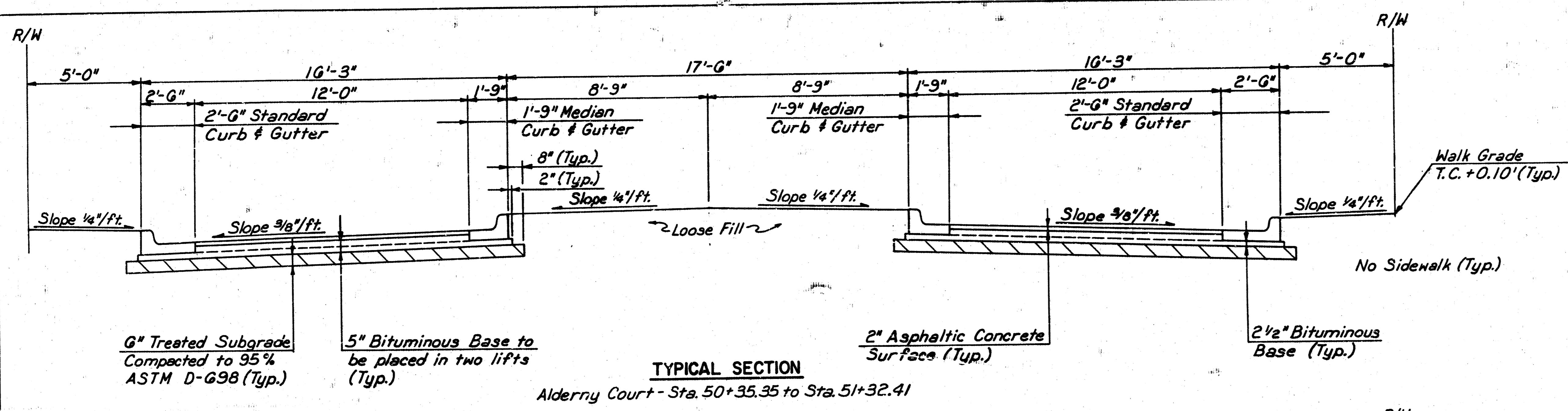
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Jack Spratt Chairman  
Donald E. Gragg Commissioner  
Tom Scott Commissioner  
Dorothy K. White County Clerk

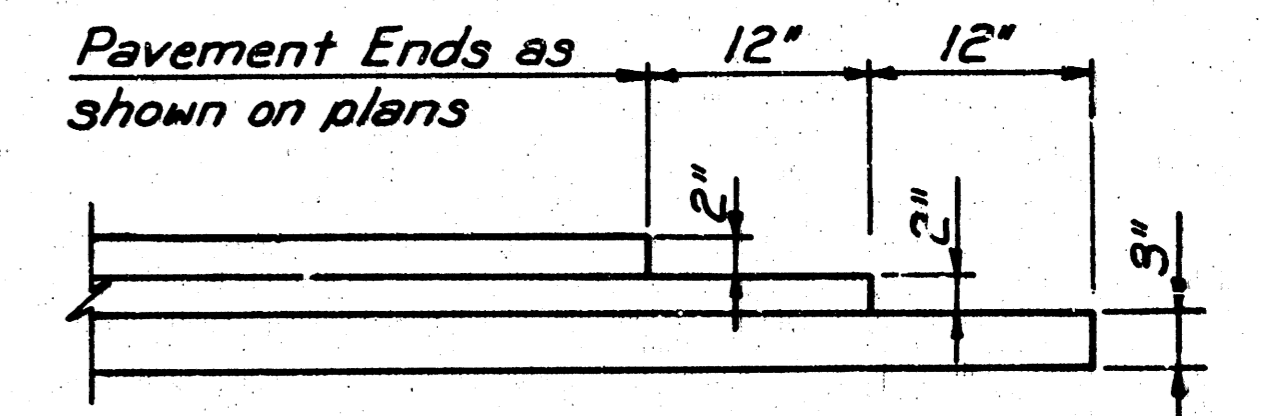
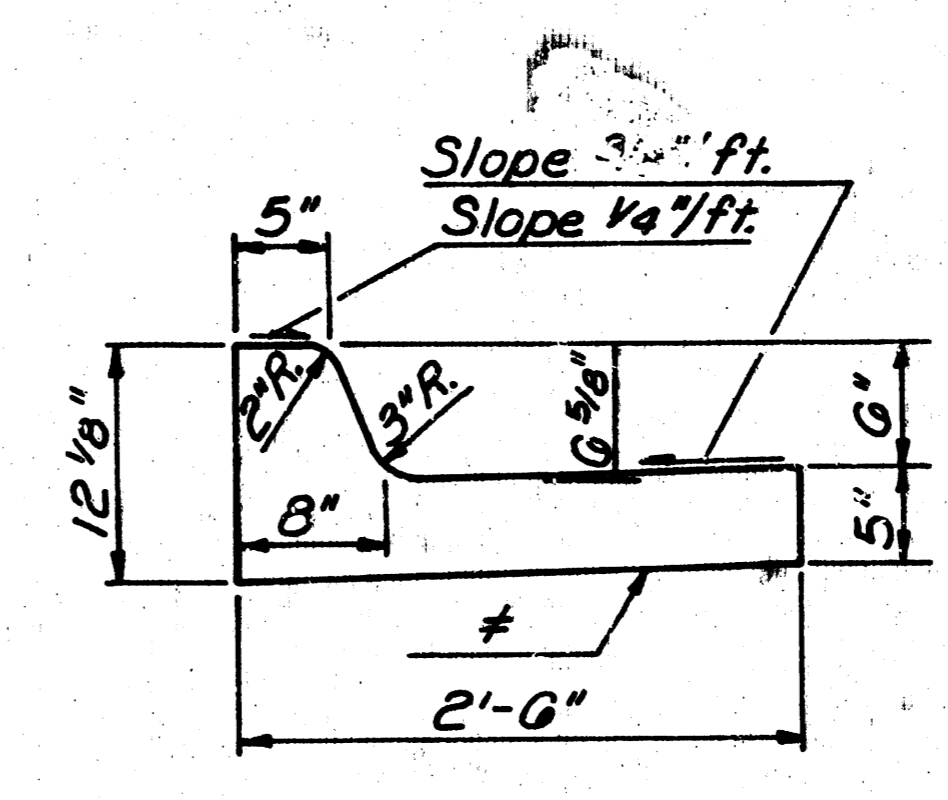
Entered on Transfer Record this \_\_\_\_\_ day of \_\_\_\_\_, 1984.

Dorothy K. White County Clerk

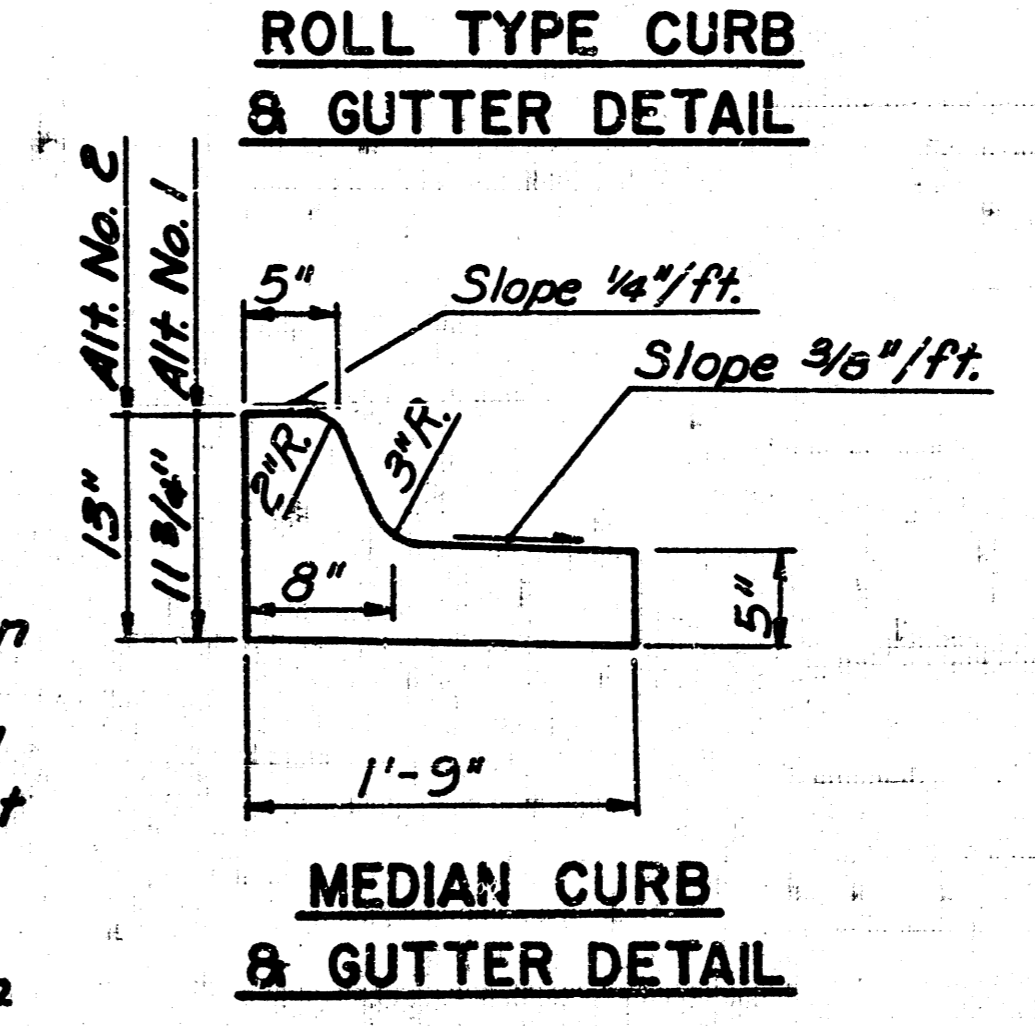
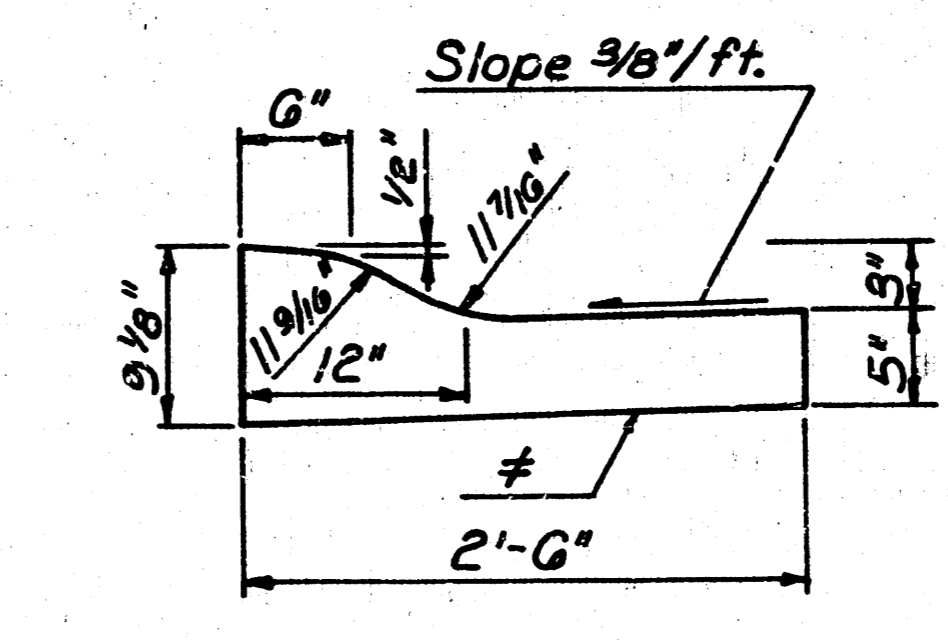
PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81375-000-001	9	11



**WHEELCHAIR RAMP DETAILS**



Transverse Construction Joints shall be constructed in flexible base pavements at locations where pavement temporarily ends to facilitate future pavement construction as shown by detail. The cost of constructing the Transverse Construction Joint shall not be measured or paid for directly, but shall be included in the price bid for square yard of asphaltic concrete pavement.



**Note:**  
Top of Curb Elevations shown on the plans for Type III Curb & Gutter reflect Alt. No. 1. Should the Contractor opt to construct Alt. No. 2 Curb & Gutter the proposed top of curb shall be 0.11' above the elevations shown.

**TYPICAL SECTIONS**

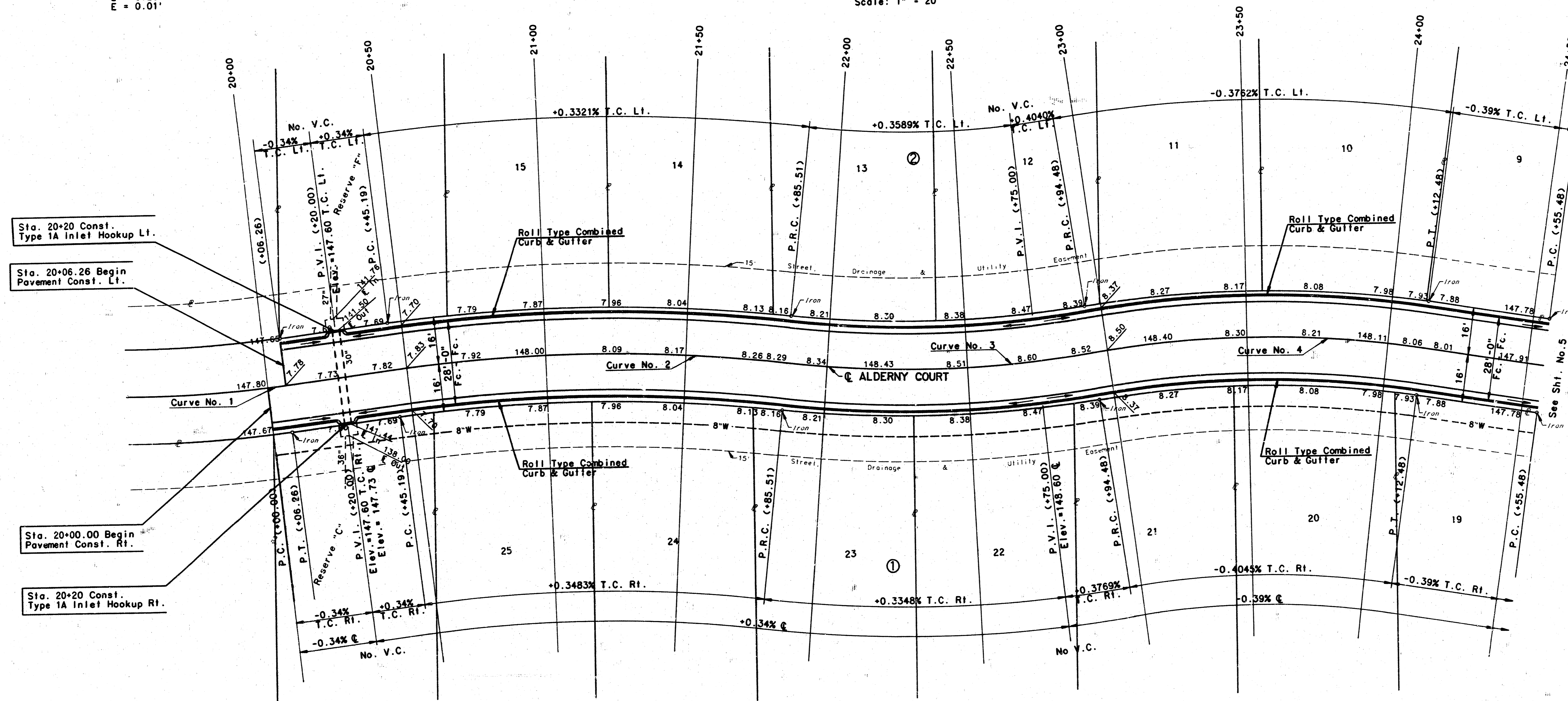
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
ENGINEERS  
WICHITA, KANSAS

Designed by BER  
Checked by  
Drawn by JGP  
Date June, 1984  
Job No. 8373-1

Curve No. 1  
 P.I. Sta. 20+03.13 (Bk.) =  
 P.I. Sta. 20+03.13 (Ahd.) =  
 @ Curve Data  
 $\Delta = 0^\circ 46' 57''$   
 $D = 12^\circ 30' 04''$   
 $R = 458.33'$   
 $T = 3.13'$   
 $L = 6.26'$   
 $E = 0.01'$

Curve No. 3  
 P.I. Sta. 22+40.33 (Bk.) =  
 P.I. Sta. 22+39.66 (Ahd.) =  
 @ Curve Data  
 $\Delta = 15^\circ 30' 09''$   
 $D = 14^\circ 13' 33''$   
 $R = 402.75'$   
 $T = 54.82'$   
 $L = 108.97'$   
 $E = 3.71'$

Scale: 1" = 20'



Sta. 20+20 Const. Type 1A Inlet Hookup Lt.

Sta. 20+06.26 Begin Pavement Const. Lt.

Sta. 20+00.00 Begin Pavement Const. Rt.

Sta. 20+20 Const. Type 1A Inlet Hookup Rt.

Curve No. 2  
 P.I. Sta. 21+15.69 (Bk.) =  
 P.I. Sta. 21+15.01 (Ahd.) =  
 @ Curve Data  
 $\Delta = 13^\circ 42' 08''$   
 $D = 9^\circ 45' 54''$   
 $R = 586.74'$   
 $T = 70.50'$   
 $L = 140.32'$   
 $E = 4.22'$

See Sh1. No. 8 for Curve Tables

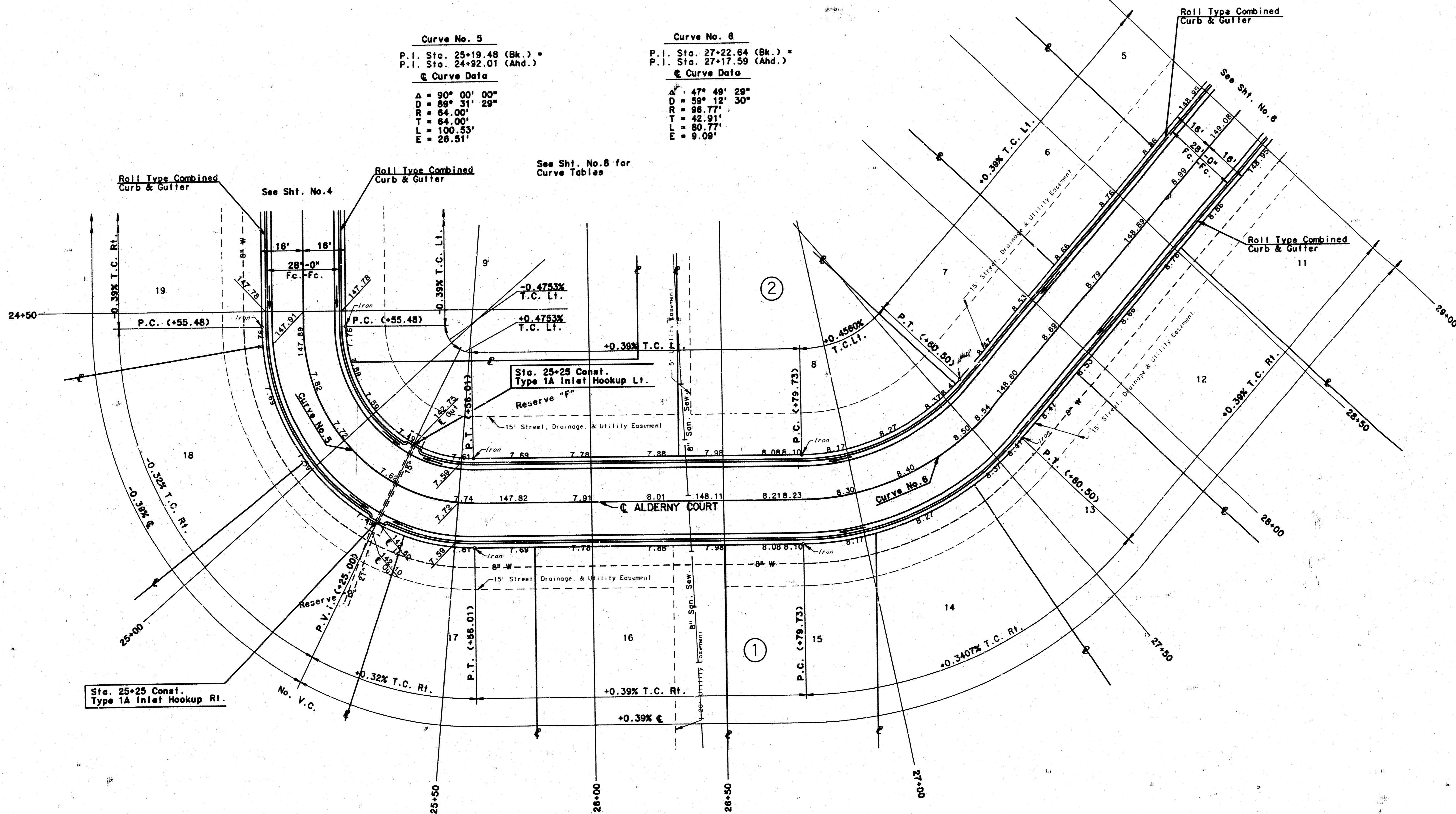
Curve No. 4  
 P.I. Sta. 23+53.94 (Bk.) =  
 P.I. Sta. 23+53.02 (Ahd.) =  
 @ Curve Data  
 $\Delta = 17^\circ 30' 00''$   
 $D = 14^\circ 49' 51''$   
 $R = 388.33'$   
 $T = 59.46'$   
 $L = 118.00'$   
 $E = 4.55'$

No.	Revision	By	Date
<b>ALDERNY COURT</b> STA. 20+00 TO STA. 24+50 Proj. No. 472-76-245-81375-000-000-001 <b>PROFESSIONAL ENGINEERING CONSULTANTS, P.A.</b> ENGINEERS WICHITA, KANSAS			
Designed by BER		Job No. 83713-1	
Drawn by YODA, JGP		Date June, 1984	
			4/10 Sh1. 4 of 11

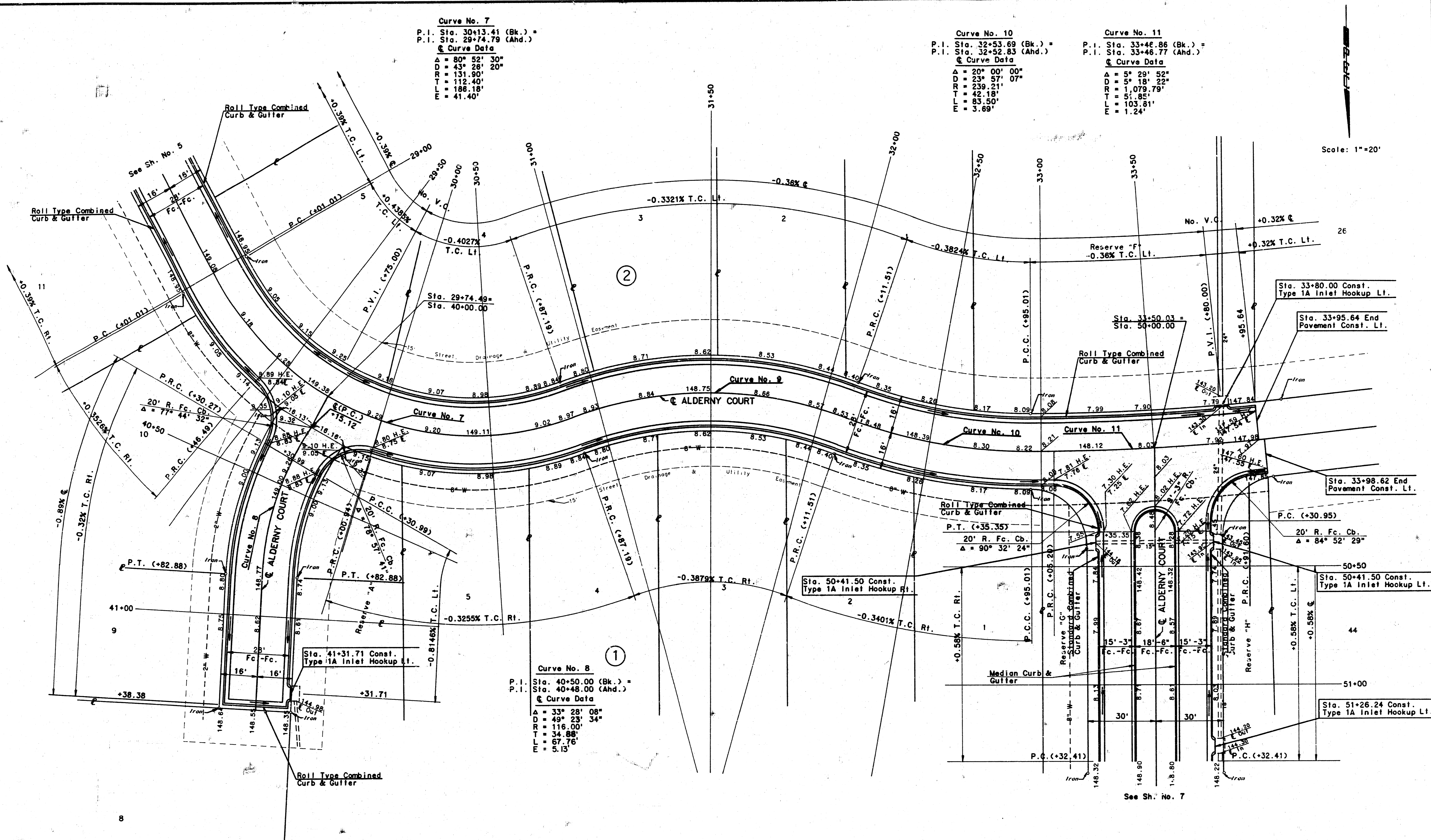
Scale: 1" = 20'

**Curve No. 5**  
 P.I. Sta. 25+19.48 (Bk.) =  
 P.I. Sta. 24+32.01 (Ahd.) =  
 @ Curve Data  
 A = 90° 00' 00"  
 D = 89° 31' 29"  
 R = 84.00'  
 T = 64.00'  
 L = 100.53'  
 E = 26.51'

**Curve No. 6**  
 P.I. Sta. 27+22.64 (Bk.) =  
 P.I. Sta. 27+17.59 (Ahd.) =  
 @ Curve Data  
 A = 47° 49' 29"  
 D = 59° 12' 30"  
 R = 98.77'  
 T = 42.91'  
 L = 80.77'  
 E = 9.09'



No.	Revision	By	Date
<b>ALDERNY COURT</b> <b>STA. 24+50 TO STA. 29+00</b> Proj. No. 472-76-245-81375-000-000-001 <b>PROFESSIONAL ENGINEERING CONSULTANTS, P.A.</b> ENGINEERS WICHITA, KANSAS			
Designed by	BER	Job No.	83713-1
Drawn by	Yoda, JGP	Date	July, 1984
		Sheet	5 of 11



**Curve No. 7**  
 P.I. Sta. 30+13.41 (Bk.) =  
 P.I. Sta. 29+74.79 (Ahd.)  
 @ Curve Data  
 Δ = 80° 52' 30"  
 D = 43' 28' 20"  
 R = 131.90'  
 T = 112.40'  
 L = 186.18'  
 E = 41.40'

**Curve No. 10**  
 P.I. Sta. 32+53.69 (Bk.) =  
 P.I. Sta. 32+52.83 (Ahd.)  
 @ Curve Data  
 Δ = 29° 00' 00"  
 D = 23' 57' 07"  
 R = 239.21'  
 T = 42.18'  
 L = 83.50'  
 E = 3.69'

**Curve No. 11**  
 P.I. Sta. 33+46.86 (Bk.) =  
 P.I. Sta. 33+46.77 (Ahd.)  
 @ Curve Data  
 Δ = 5° 29' 52"  
 D = 5' 18' 22"  
 R = 1,079.79'  
 T = 51.86'  
 L = 103.81'  
 E = 1.24'

**Curve No. 8**  
 P.I. Sta. 40+50.00 (Bk.) =  
 P.I. Sta. 40+48.00 (Ahd.)  
 @ Curve Data  
 Δ = 33° 28' 08"  
 D = 49° 23' 34"  
 R = 116.00'  
 T = 34.88'  
 L = 67.76'  
 E = 5.13'

**Curve No. 9**  
 P.I. Sta. 31+52.25 (Bk.) =  
 P.I. Sta. 31+47.07 (Ahd.)  
 @ Curve Data  
 Δ = 39° 30' 00"  
 D = 31' 48' 22"  
 R = 180.33'  
 T = 84.75'  
 L = 124.32'  
 E = 11.27'

**INTERSECTION QUANTITIES**  
 239.00 S.Y. 7" Asphaltic Concrete Pavement (5" Bit. Base)  
 28.57 S.Y. 2 1/2" Bituminous Base  
 274.25 S.Y. Manipulation  
 3.43 Tons Pebble Lime  
 72.65 L.F. Combined Curb & Gutter (Roll Type)  
 29.08 L.F. 1'-9" Median Curb & Gutter

**INTERSECTION QUANTITIES**  
 141.32 S.Y. 7" Asphaltic Concrete Pavement (5" Bit. Base)  
 18.77 S.Y. 2 1/2" Bituminous Base  
 161.00 S.Y. Manipulation  
 2.01 Tons Pebble Lime  
 54.70 L.F. Combined Curb & Gutter (Roll Type)

Scale: 1"=20'

No.	Revision	By	Date

**ALDERNY COURT**  
 STA. 29+00 TO STA. 33+98.62  
 Proj. No. 472-76-245-81375-000-000-001

**PROFESSIONAL ENGINEERING CONSULTANTS, P.A.**  
 ENGINEERS  
 WICHITA, KANSAS

Designed by BER      Job No. 83713-1  
 Drawn by Yoda, JGP      Date July, 1984      Sht. 6 of 11

**INTERSECTION QUANTITIES**

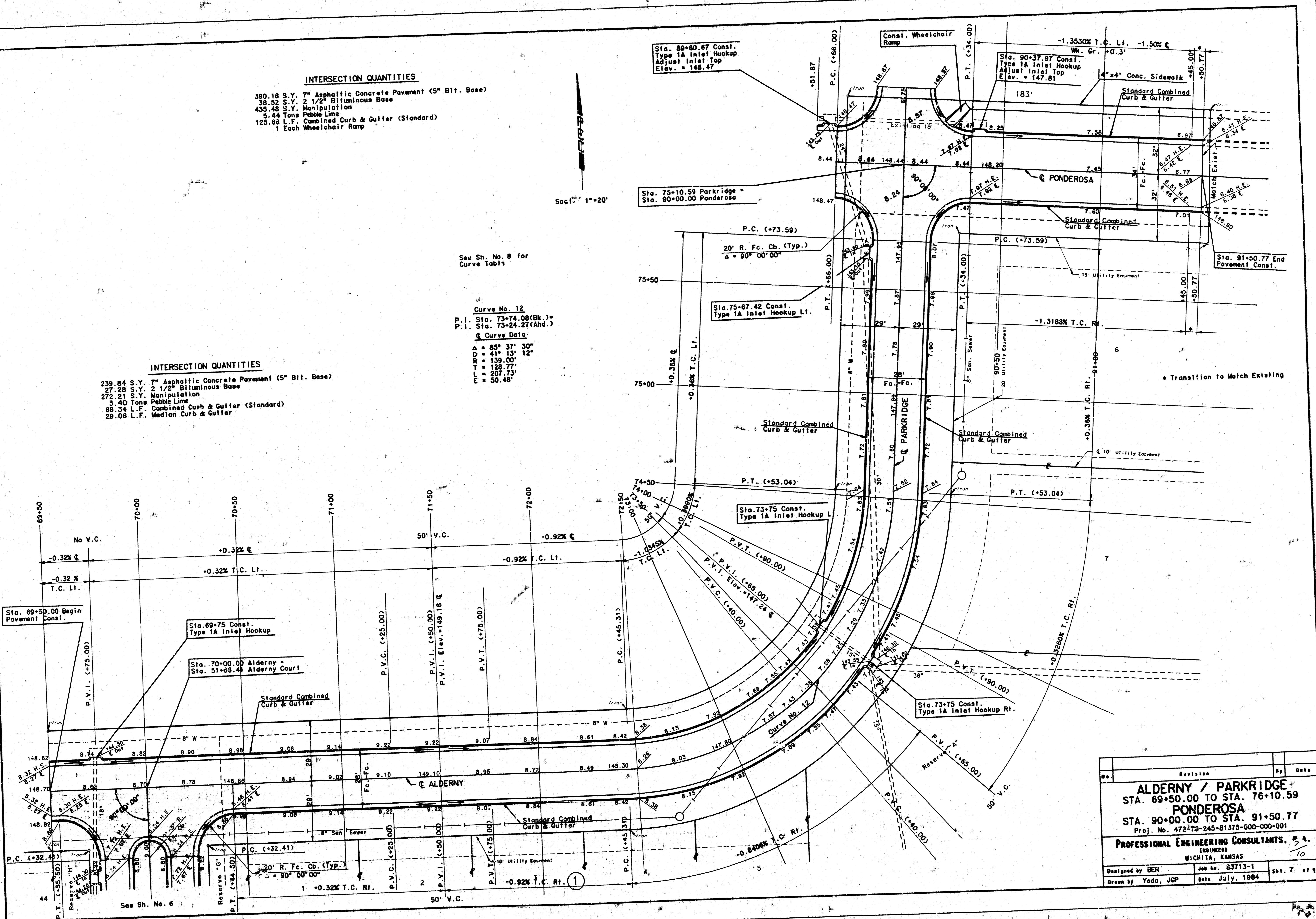
390.18 S.Y. 7" Asphaltic Concrete Pavement (5" Bit. Base)  
 38.52 S.Y. 2 1/2" Bituminous Base  
 435.48 S.Y. Manipulation  
 5.44 Tons Pebble Lime  
 125.66 L.F. Combined Curb & Gutter (Standard)  
 1 Each Wheelchair Ramp

**INTERSECTION QUANTITIES**

239.84 S.Y. 7" Asphaltic Concrete Pavement (5" Bit. Base)  
 27.29 S.Y. 2 1/2" Bituminous Base  
 272.21 S.Y. Manipulation  
 3.40 Tons Pebble Lime  
 68.34 L.F. Combined Curb & Gutter (Standard)  
 29.06 L.F. Median Curb & Gutter

See Sh. No. 8 for Curve Table

Curve No. 12  
 P.I. Sta. 73+74.08(Bk.)=  
 P.I. Sta. 73+24.27(Ahd.)  
 & Curve Data  
 A = 85° 37' 30"  
 D = 41° 13' 12"  
 R = 139.00'  
 T = 128.77'  
 L = 207.73'  
 E = 50.48'



No.	Revision	By	Date
<b>ALDERNY / PARKRIDGE</b> <b>STA. 69+50.00 TO STA. 76+10.59</b> <b>PONDEROSA</b> <b>STA. 90+00.00 TO STA. 91+50.77</b> Proj. No. 472-75-245-81375-000-000-001			
<b>PROFESSIONAL ENGINEERING CONSULTANTS, P.A.</b> ENGINEERS WICHITA, KANSAS			
Designed by	BER	Job No.	83713-1
Drawn by	Yoda, JGP	Date	July, 1984
			Sheet 7 of 11

**CURVE NO. 1**  
**Q CURVE DATA**  
 $\Delta=0^{\circ}46'57''$   $D=12^{\circ}30'04''$   $R=458.33'$   $L=6.26'$   $T=3.13'$   $E=0.01'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=0^{\circ}23'28.5''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
20+00.00	6.26'		6.56'	0°23'28.5"	0°23'28.5"
20+06.26					

**CURVE NO. 2**  
**Q CURVE DATA**  
 $\Delta=13^{\circ}42'08''$   $D=9^{\circ}45'54''$   $R=586.74'$   $L=140.32'$   $T=70.50'$   $E=4.22'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=6^{\circ}51'04''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
20+45.19			4.83'	0°14'06"	0°14'06"
20+50.00	4.81'	4.99'	4.83'	1°13'14"	1°27'20"
20+75.00	25.00'	25.93'	24.06'	1°13'14"	2°40'34"
21+00.00	25.00'	25.93'	24.06'	1°13'14"	3°53'48"
21+25.00	25.00'	25.93'	24.06'	1°13'14"	5°07'02"
21+50.00	25.00'	25.93'	24.06'	1°13'14"	6°20'16"
21+75.00	25.00'	25.93'	24.06'	1°13'14"	7°33'30"
21+85.51	10.51'	10.91'	10.12'	0°30'48"	8°04'18"

**CURVE NO. 3**  
**Q CURVE DATA**  
 $\Delta=15^{\circ}30'09''$   $D=14^{\circ}13'33''$   $R=402.76'$   $L=108.97'$   $T=54.82'$   $E=3.71'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=7^{\circ}45'04.5''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
21+85.51			15.28'	1°01'50.4"	1°01'50.4"
22+00.00	14.49'	13.70'	15.28'	1°48'42"	2°48'32.4"
22+25.00	25.00'	23.63'	26.38'	1°48'42"	4°35'14.4"
22+50.00	25.00'	23.63'	26.38'	1°48'42"	6°21'56.4"
22+75.00	25.00'	23.63'	26.38'	1°48'42"	8°08'38.4"
22+94.48	19.48'	18.41'	20.54'	1°23'08.1"	9°31'46.5"

**CURVE NO. 4**  
**Q CURVE DATA**  
 $\Delta=17^{\circ}30'00''$   $D=14^{\circ}49'51''$   $R=386.33'$   $L=118.00'$   $T=59.46'$   $E=4.55'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=8^{\circ}45'00''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
22+94.48			5.20'	0°24'33"	0°24'33"
23+00.00	5.52'	5.83'	5.20'	1°51'14"	2°15'47"
23+25.00	25.00'	26.42'	23.57'	1°51'14"	4°07'01"
23+50.00	25.00'	26.42'	23.57'	1°51'14"	5°58'15"
23+75.00	25.00'	26.42'	23.57'	1°51'14"	7°49'29"
24+00.00	25.00'	26.42'	23.57'	1°51'14"	9°40'43"
24+12.48	12.48'	13.19'	11.77'	0°55'31"	10°36'14"

**CURVE NO. 5**  
**Q CURVE DATA**  
 $\Delta=90^{\circ}00'00''$   $D=89^{\circ}31'29''$   $R=64.00'$   $L=100.53'$   $T=64.00'$   $E=26.51'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=45^{\circ}00'00''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
24+55.48			28.13'	8°44'18"	8°44'18"
24+75.00	19.52'	12.28'	16.30'	11°11'28"	19°55'42"
25+00.00	25.00'	16.30'	33.38'	11°11'28"	31°07'08"
25+25.00	25.00'	16.30'	33.38'	11°11'28"	42°18'34"
25+50.00	25.00'	16.30'	33.38'	11°11'28"	53°29'58"
25+58.01	8.01'	3.94'	8.07'	2°41'28"	56°11'26"

**CURVE NO. 6**  
**Q CURVE DATA**  
 $\Delta=47^{\circ}49'29''$   $D=59^{\circ}12'30''$   $R=96.77'$   $L=80.77'$   $T=42.91'$   $E=9.09'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=23^{\circ}54'44.5''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
26+79.73			24.83'	6°00'04"	6°00'04"
27+00.00	20.27'	15.83'	24.83'	7°24'05"	13°24'09"
27+25.00	25.00'	19.26'	30.60'	7°24'05"	20°48'14"
27+50.00	25.00'	19.26'	30.60'	7°24'05"	28°12'19"
27+60.50	10.50'	8.11'	12.88'	3°08'30.5"	31°20'49.5"

**CURVE NO. 7**  
**Q CURVE DATA**  
 $\Delta=80^{\circ}52'30''$   $D=43^{\circ}26'20''$   $R=131.90'$   $L=186.18'$   $T=112.40'$   $E=41.40'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=40^{\circ}26'15''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
29+01.01			27.95'	5°12'38"	5°12'38"
29+25.00	23.99'	19.96'	25.05'	4°40'03"	9°52'41"
29+48.49	21.49'	17.89'	25.05'	4°40'03"	14°32'44"
29+50.00	3.51'	2.92'	4.09'	0°45'44"	15°18'28"
29+75.00	25.00'	20.80'	29.13'	5°25'48"	16°04'13"
30+00.00	25.00'	20.80'	29.13'	5°25'48"	21°29'58"
30+00.94	0.94'	0.78'	1.10'	0°12'15"	21°42'13"
30+25.00	24.06'	20.02'	28.03'	5°13'32"	26°55'48"
30+50.00	25.00'	20.80'	29.13'	5°25'48"	32°21'36"
30+75.00	25.00'	20.80'	29.13'	5°25'48"	37°47'24"
30+87.19	12.19'	10.15'	14.22'	2°38'51"	40°26'15"

**CURVE NO. 8**  
**Q CURVE DATA**  
 $\Delta=33^{\circ}28'08''$   $D=49^{\circ}23'34''$   $R=116.00'$   $L=57.76'$   $T=34.88'$   $E=5.13'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=16^{\circ}44'04''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
40+15.12			18.01'	3°44'30"	3°44'30"
40+30.27	15.15'	12.22'	18.01'	0°10'40"	3°55'10"
40+30.99	0.72'	0.58'	0.86'	4°41'41"	8°36'51"
40+50.00	19.01'	15.39'	22.59'	6°10'27"	14°47'18"
40+75.00	25.00'	20.22'	29.68'	1°58'46"	16°44'04"
40+82.88	7.88'	6.38'	9.37'		

**CURVE NO. 9**  
**Q CURVE DATA**  
 $\Delta=39^{\circ}30'00''$   $D=31^{\circ}46'22''$   $R=180.33'$   $L=124.32'$   $T=64.75'$   $E=11.27'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=19^{\circ}45'00''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
30+87.19			11.24'	2°02'08"	2°02'08"
31+00.00	12.81'	14.37'	11.24'	3°58'18"	6°00'24"
31+25.00	25.00'	28.03'	21.93'	3°58'18"	9°58'42"
31+50.00	25.00'	28.03'	21.93'	3°58'18"	13°57'00"
31+75.00	25.00'	28.03'	21.93'	3°58'18"	17°55'18"
32+00.00	25.00'	28.03'	21.93'	3°58'18"	19°45'00"
32+11.51	11.51'	12.91'	10.10'	1°49'42"	

**CURVE NO. 10**  
**Q CURVE DATA**  
 $\Delta=20^{\circ}00'00''$   $D=23^{\circ}57'07''$   $R=239.21'$   $L=83.50'$   $T=42.18'$   $E=3.69'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=10^{\circ}00'00''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
32+11.51			14.73'	1°36'58"	1°36'58"
32+25.00	13.49'	12.25'	14.73'	2°59'38.5"	4°36'34.5"
32+50.00	25.00'	22.69'	27.29'	2°59'38.5"	7°36'13"
32+75.00	25.00'	22.69'	27.29'	2°59'38.5"	10°35'51.5"
32+95.01	20.01'	18.16'	21.84'	2°23'47"	10°00'00"

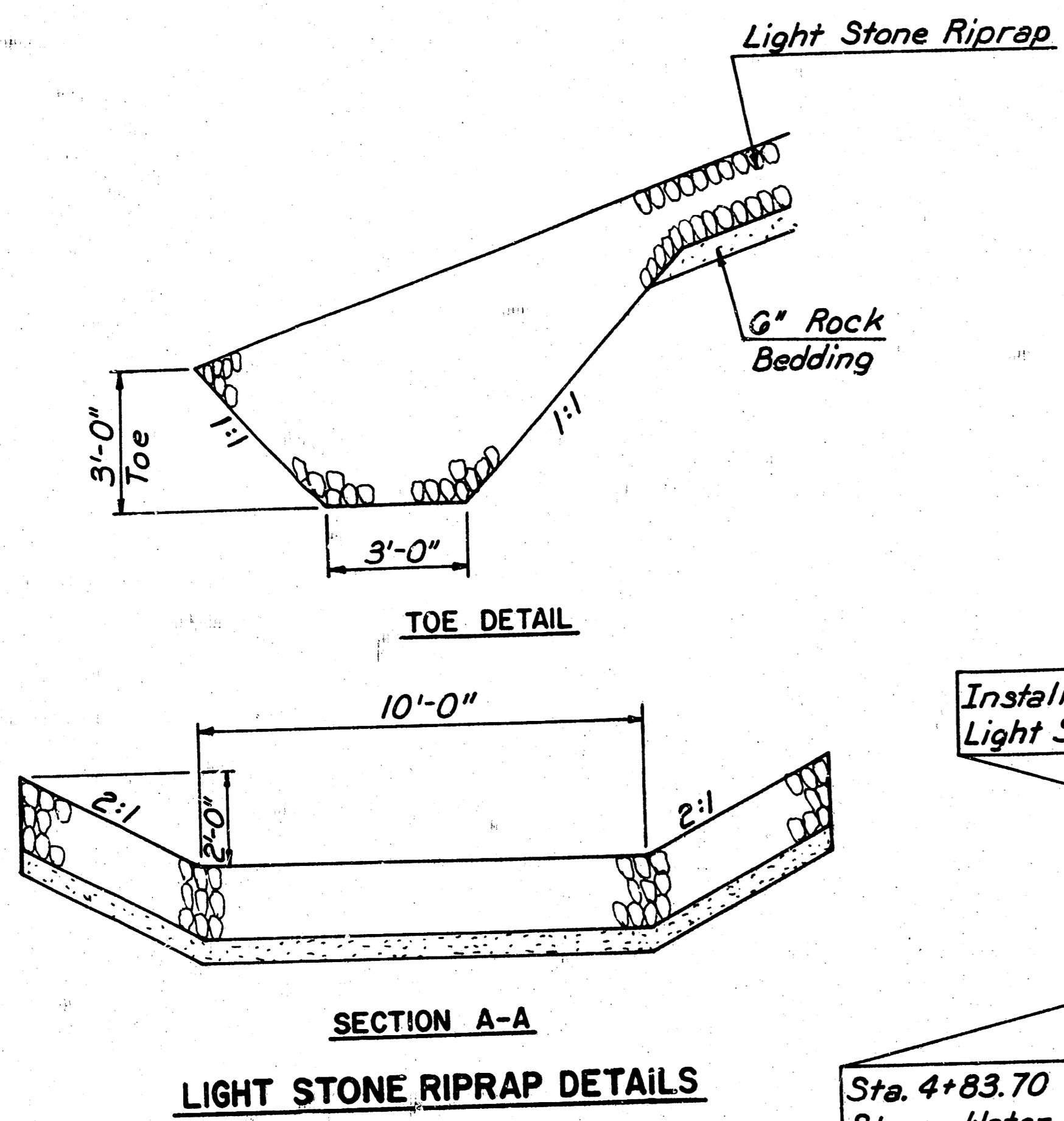
**CURVE NO. 11**  
**Q CURVE DATA**  
 $\Delta=5^{\circ}29'52''$   $D=5^{\circ}18'22''$   $R=1,079.79'$   $L=103.61'$   $T=51.85'$   $E=1.24'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=2^{\circ}44'56''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
32+95.01			5.10'	0°07'57"	0°07'57"
33+00.00	4.99'	4.89'	5.10'	0°08'17"	0°16'14"
33+05.20	5.20'	5.10'	5.31'	0°31'31"	0°47'45"
33+25.00	19.80'	19.40'	20.20'	0°39'48"	1°27'33"
33+50.00	25.00'	24.50'	25.51'	0°39'48"	2°07'21"
33+75.00	25.00'	24.50'	25.51'	0°26'25"	2°33'46"
33+91.60	16.60'	16.26'	16.93'	0°06'26"	2°40'12"
33+95.64	4.04'	3.96'	4.12'	0°04'44"	2°44'56"
33+98.62	2.98'	2.91'	3.03'		

**CURVE NO. 12**  
**Q CURVE DATA**  
 $\Delta=85^{\circ}37'30''$   $D=41^{\circ}13'12''$   $R=139.00'$   $L=207.73'$   $T=128.77'$   $E=50.48'$   
**CURVE DATA BASED ON Q RADIUS  $\Delta/2=42^{\circ}48'45''$**

Q STATION	Q ARC LENGTH	CHORD LENGTH		Q DEFLECTION ANGLE	Q TOTAL DEFLECTION
		B' OFF LEFT FACE CURB	B' OFF RIGHT FACE CURB		
72+45.31			5.43'	0°57'59"	0°57'59"
72+50.00	4.69'	3.95'	5.43'	5°09'09"	6°07'08"
72+75.00	25.00'	21.01'	28.92'	5°09'09"	11°16'17"
73+00.00	25.00'	21.01'	28.92'	5°09'09"	16°25'26"
73+25.00	25.00'	21.01'	28.92'	5°09'09"	21°34'35"
73+50.00	25.00'	21.01'	28.92'	5°09'09"	26°43'44"
73+75.00	25.00'	21.01'	28.92'	5°09'09"	31°52'53"
74+00.00	25.00'	21.01'	28.92'	5°09'09"	37°02'02"
74+25.00	25.00'	21.01'	28.92'	5°09'09"	42°11'11"
74+50.00	25.00'	21.01'	28.92'	5°09'09"	47°20'20"
74+53.04	3.04'	2.58'	3.52'	0°37'34"	42°48'45"

**PLAN** DRAWN BY: JGP  
 CHECKED BY: JGP  
 DATE: 6/1/84  
 PROJECT: STORM WATER SEWER  
 SHEET NO. 9 OF 11

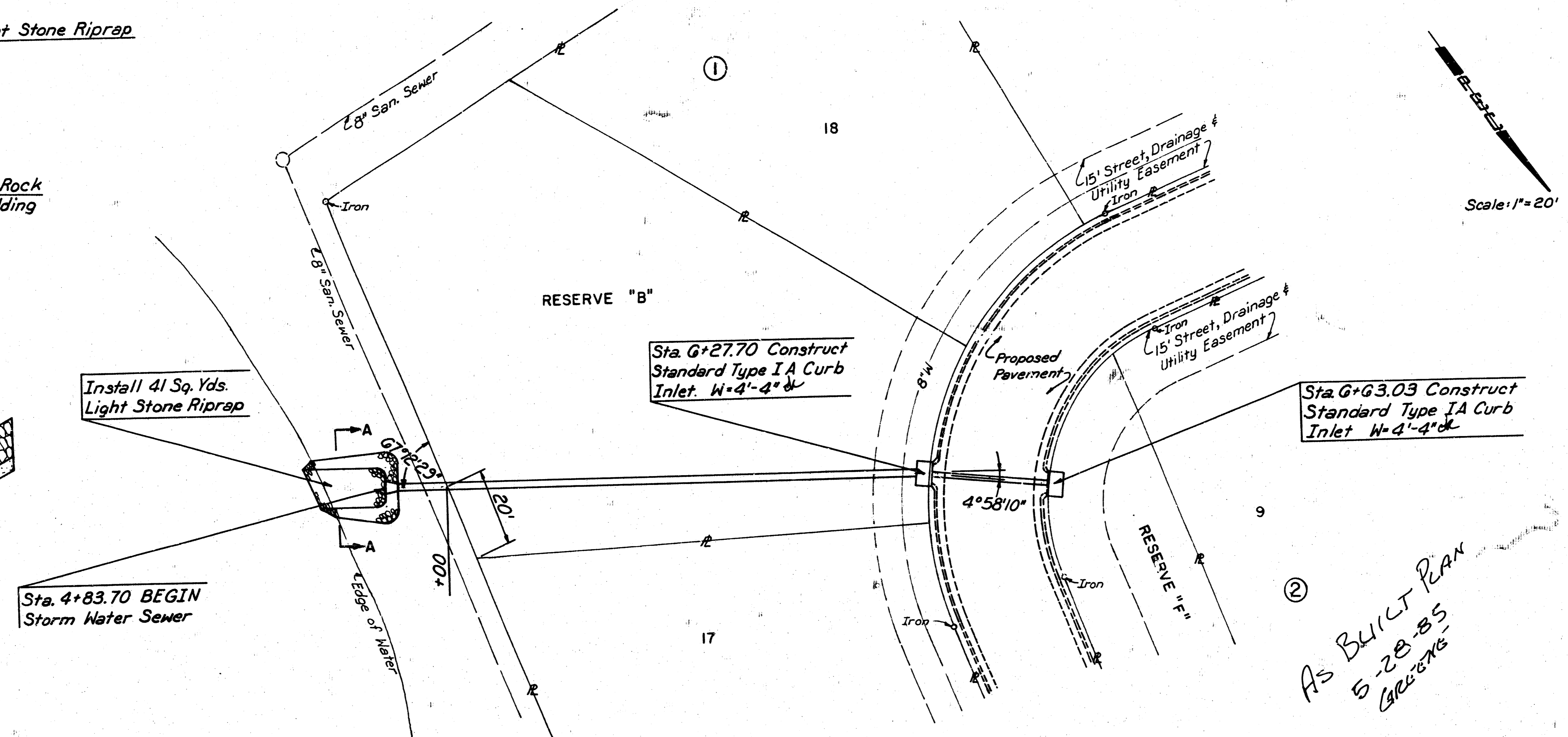


Sta. 4+83.70 BEGIN Storm Water Sewer

Install 41 Sq. Yds. Light Stone Riprap

Sta. 6+27.70 Construct Standard Type IA Curb Inlet W=4'-4" dk

Sta. 6+63.03 Construct Standard Type IA Curb Inlet W=4'-4" dk



AS BUILT PLAN  
 5-28-85  
 GREGG

**PROFILE** DRAWN BY: JGP  
 CHECKED BY: JGP  
 DATE: 6/1/84  
 PROJECT: STORM WATER SEWER  
 SHEET NO. 9 OF 11

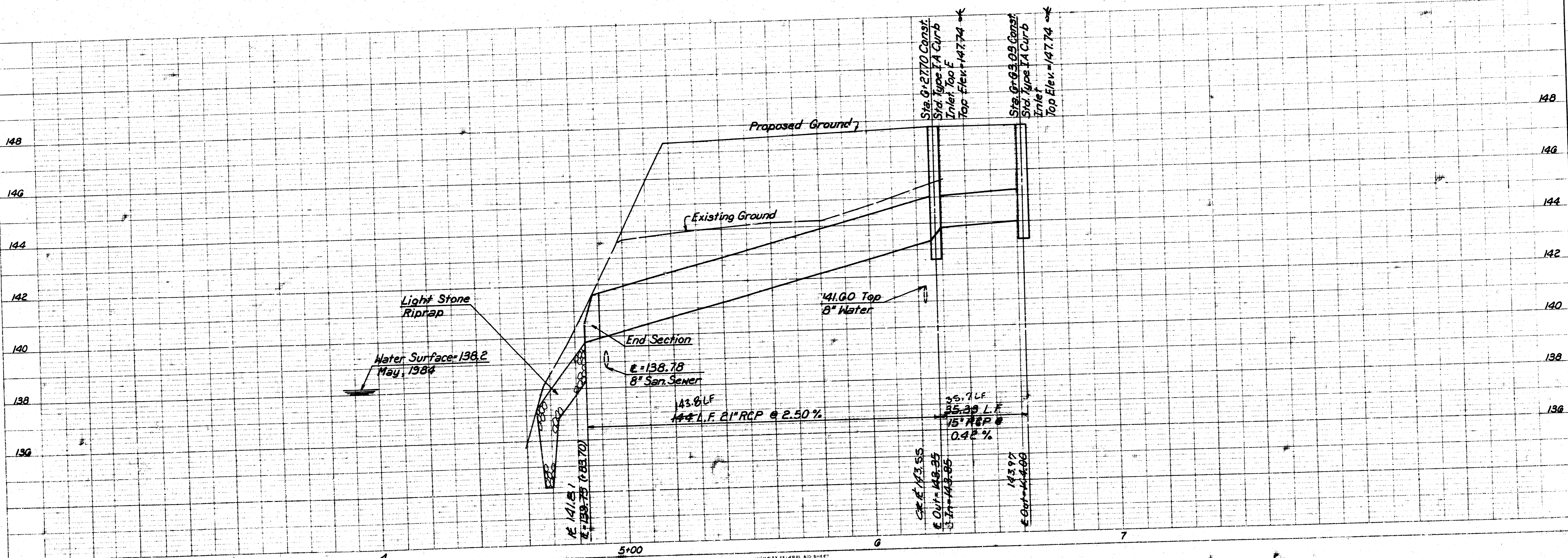
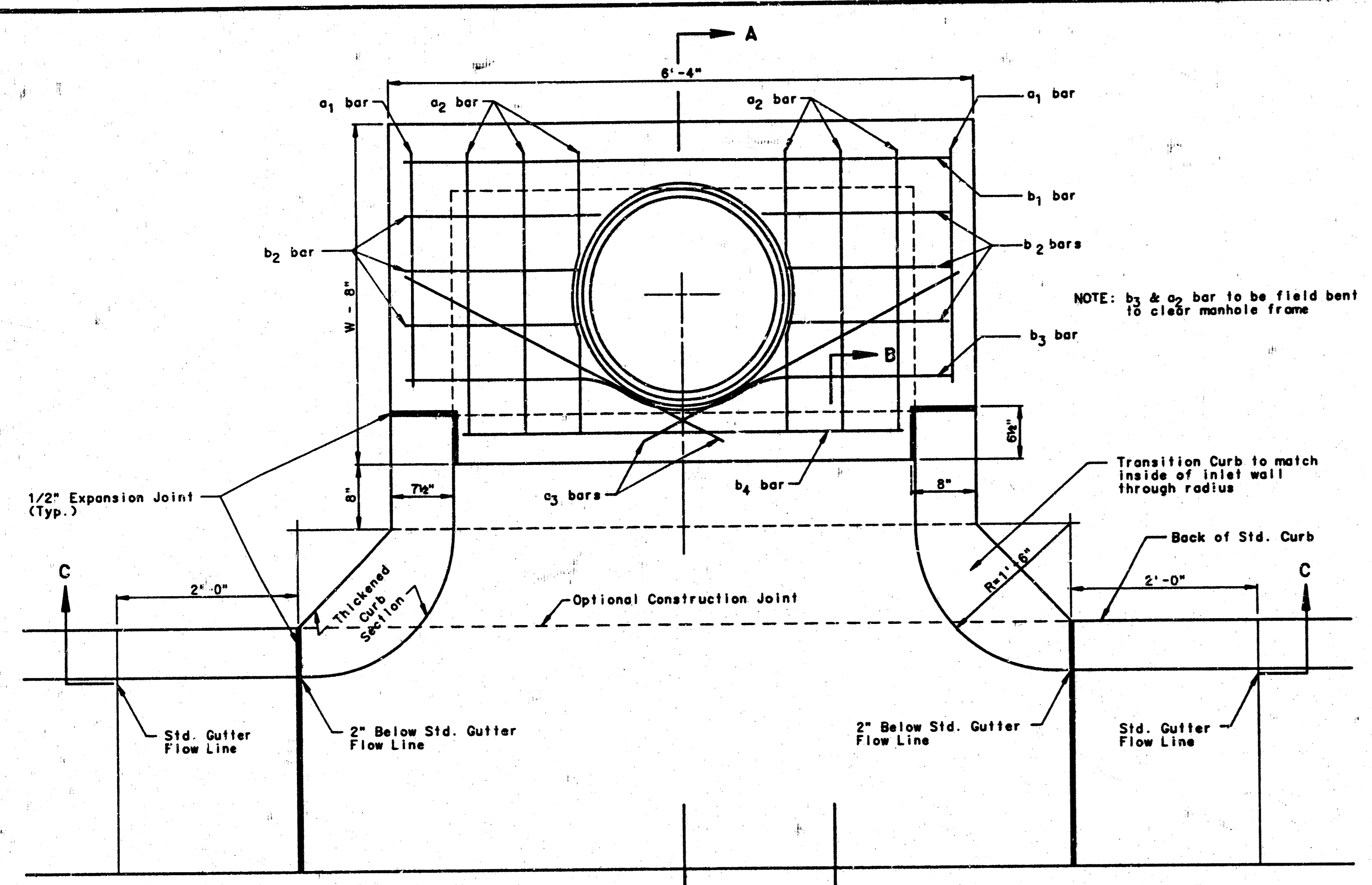


PLATE 1-SINGLE PLAN AND PROFILE FULL LINE  
 DRAWN BY: JGP  
 CHECKED BY: JGP  
 DATE: 6/1/84

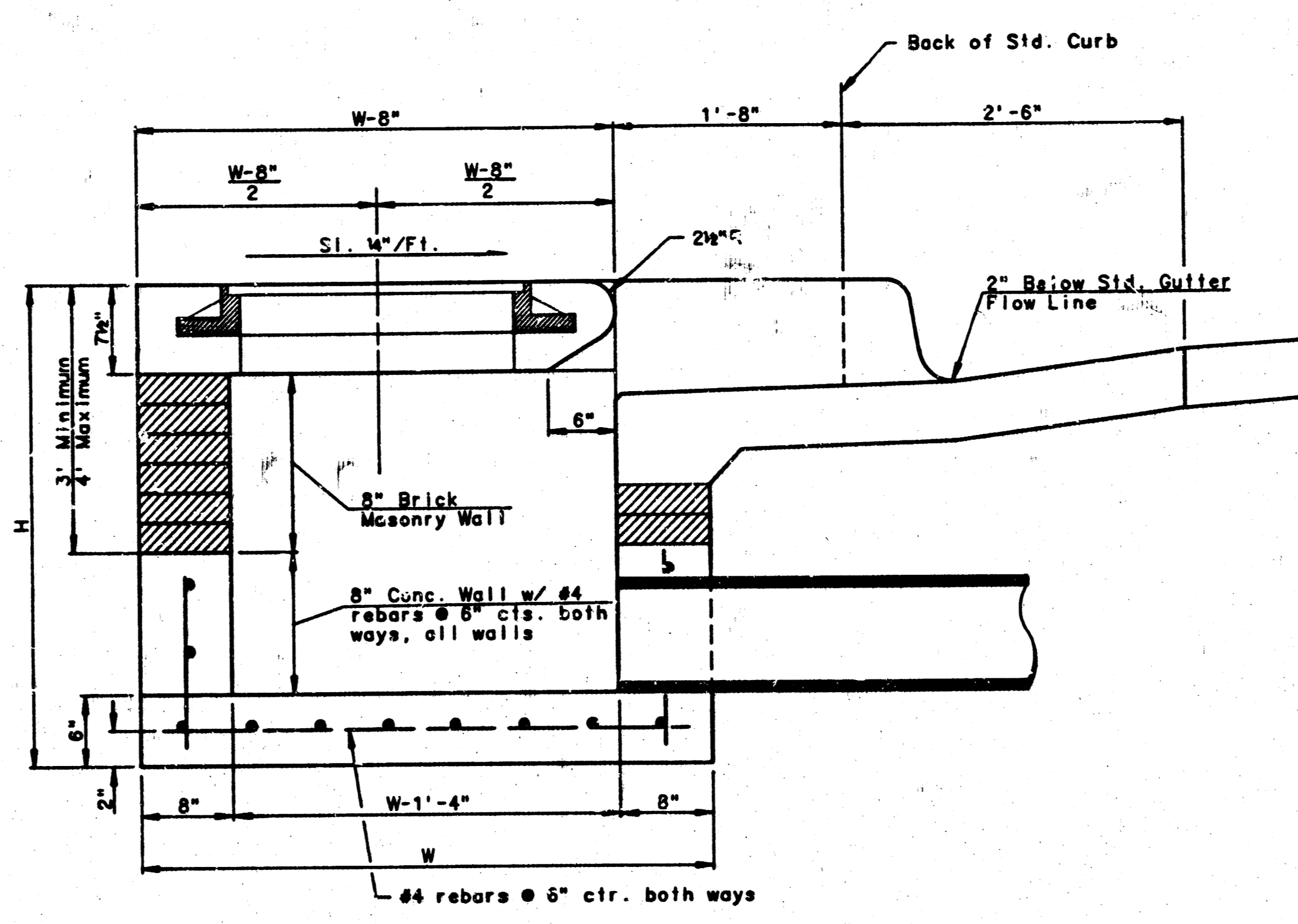
**STORM WATER SEWER**  
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
 ENGINEERS  
 WICHITA, KANSAS  
 Job No. 8373-1  
 Date: June, 1984  
 Sheet 9 of 11  
 Drawn by JGP

PROJ. NO. 472-76-245-8375-000-000-001

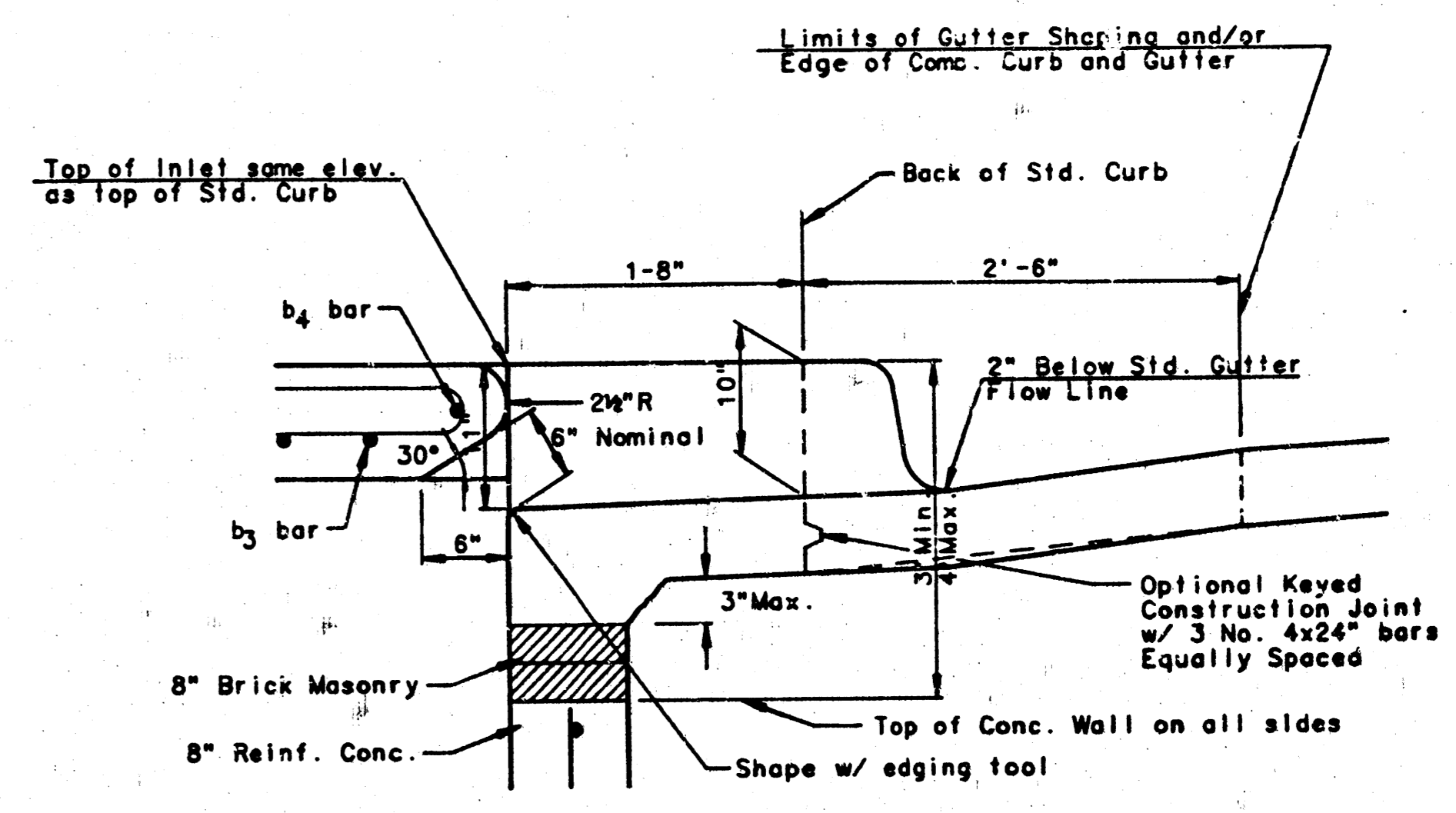
PROJECT NO.	SHEET NO.	TOTAL SHEETS
472-76-245-81375-000-000-001	10	11



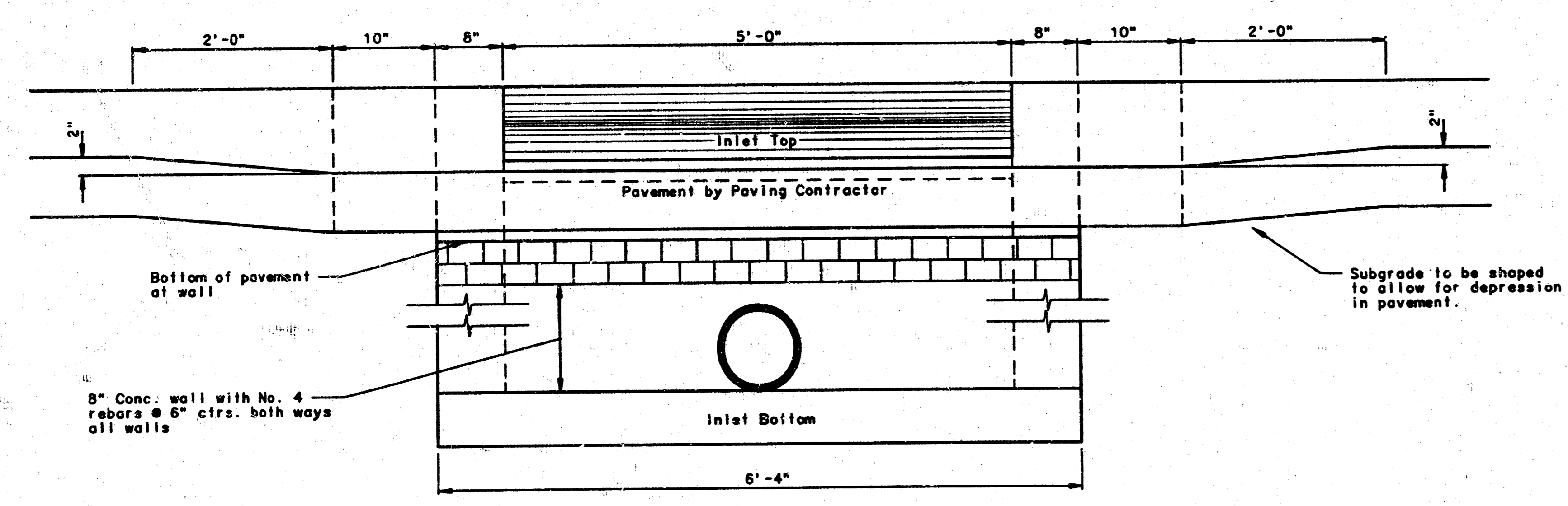
PLAN



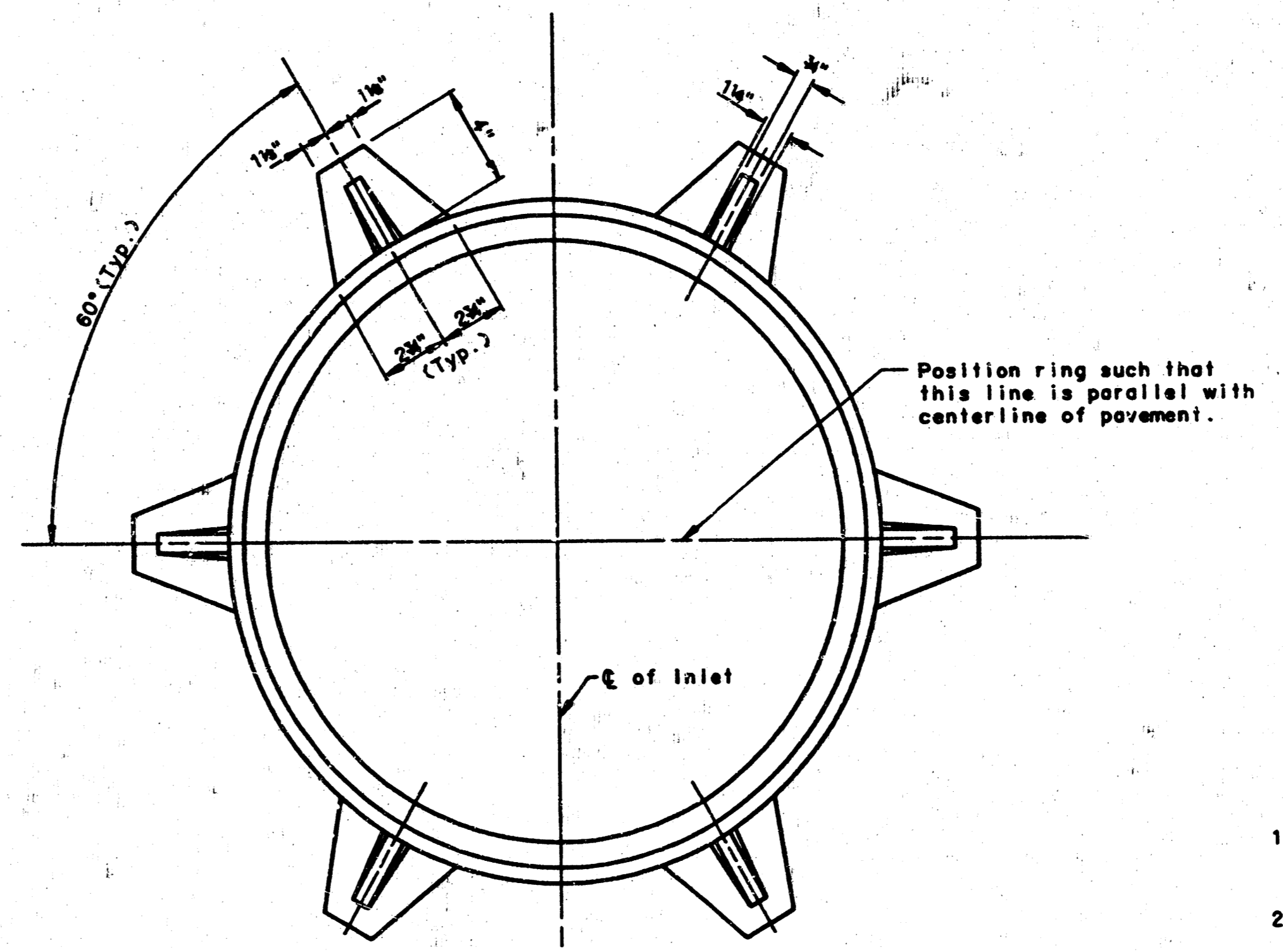
SECTION A-A



SECTION B-B



SECTION C-C



MANHOLE FRAME  
(WEIGHT = 180 LBS.)

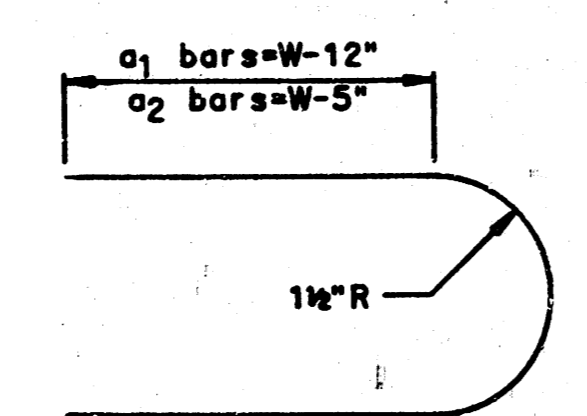
GENERAL NOTES

- The contractor shall have the option of constructing 8" brick masonry walls between the concrete inlet base and top on this inlet when W = 4'-4" and H = 6'-0" or less.
- Additional curb and gutter construction necessary to connect setback inlet to pavement will be paid for at the unit price bid for each inlet hookup.
- Inlet invert shall be shaped with 8 sack sand mix concrete to create flow channels and to increase hydraulic efficiency such that the inlet will be self cleaning between all inlet and/or outlet pipes.
- Concrete tops to be installed on thin mortar cushion to insure full support along brick walls. Concrete tops may be cast in place or precast. Concrete used for inlet construction shall be concrete pavement mix.
- Inlet top reinforcing shall be spaced on 6" max. centers. Inlet lids shall be notched out as indicated to facilitate construction of curb.

**STEEL SCHEDULE**

BAR NUMBER	a <sub>1</sub>	a <sub>2</sub>	a <sub>3</sub>	b <sub>1</sub>	b <sub>2</sub>	b <sub>3</sub>	b <sub>4</sub>	WT. LBS.
4	4	2	1	3	5	7	9	6
SIZE	#4	#4	#4	#4	#4	#4	#4	#8
LENGTH	W-4'-4"	5'-9"	6'-11"	4'-0"	6'-1"	--	--	1'-9"
	W-5'-4"	7'-9"	8'-11"	5'-0"	--	8'-1"	--	1'-9"
	W-6'-4"	9'-9"	0'-11"	6'-0"	--	8'-1"	--	1'-9"
	W-7'-4"	11'-9"	2'-11"	7'-0"	--	8'-1"	--	1'-9"
	W-8'-4"	13'-9"	4'-11"	8'-0"	--	8'-1"	1'-9"	6'-2"
								4'-8"
								83#
								110#
								130#
								150#

\*NOTE: a<sub>3</sub> Bars to be placed approx. 2" below top of inlet cover.



BENDING DIAGM

**STANDARD CURB INLET PRECAST TOPS**

W	PRECAST TOP SIZE	PIPE SIZE	CU. YD. CONC.
4'-4"	3'-8" x 6'-4" x 7 1/2"	24" & Smaller	0.38#
5'-4"	4'-8" x 6'-4" x 7 1/2"	24" & 30"	0.51#
6'-4"	5'-8" x 6'-4" x 7 1/2"	36" & 42"	0.64#
7'-4"	6'-8" x 6'-4" x 7 1/2"	48" & 54"	0.77#
8'-4"	7'-8" x 6'-4" x 7 1/2"	60" & 66"	0.90#

CITY OF WICHITA, KANSAS  
**STD TYPE 1A CURB INLET**  
 INLET OPENING = 6" x 5'-0"

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.  
 ENGINEERS  
 WICHITA, KANSAS

Designed by: City of Wichita  
 Drawn by: Yoda  
 Checked by: [Signature]  
 Date: June, 1984