

CITY OF WICHITA, KANSAS
 MICHAEL E. LINDEBAK, P.E., CITY ENGINEER

STREET IMPROVEMENTS

BROADMOOR/FOSTER - FROM N.L. OF ONEIDA TO THE N.L. OF DOREEN

IN

FAIRFIELD ESTATES 2ND ADDITION

CITY OF WICHITA PROJECT NO. 472-76-245-82000-000-000-001

INDEX NO. 761007

INDEX OF SHEETS

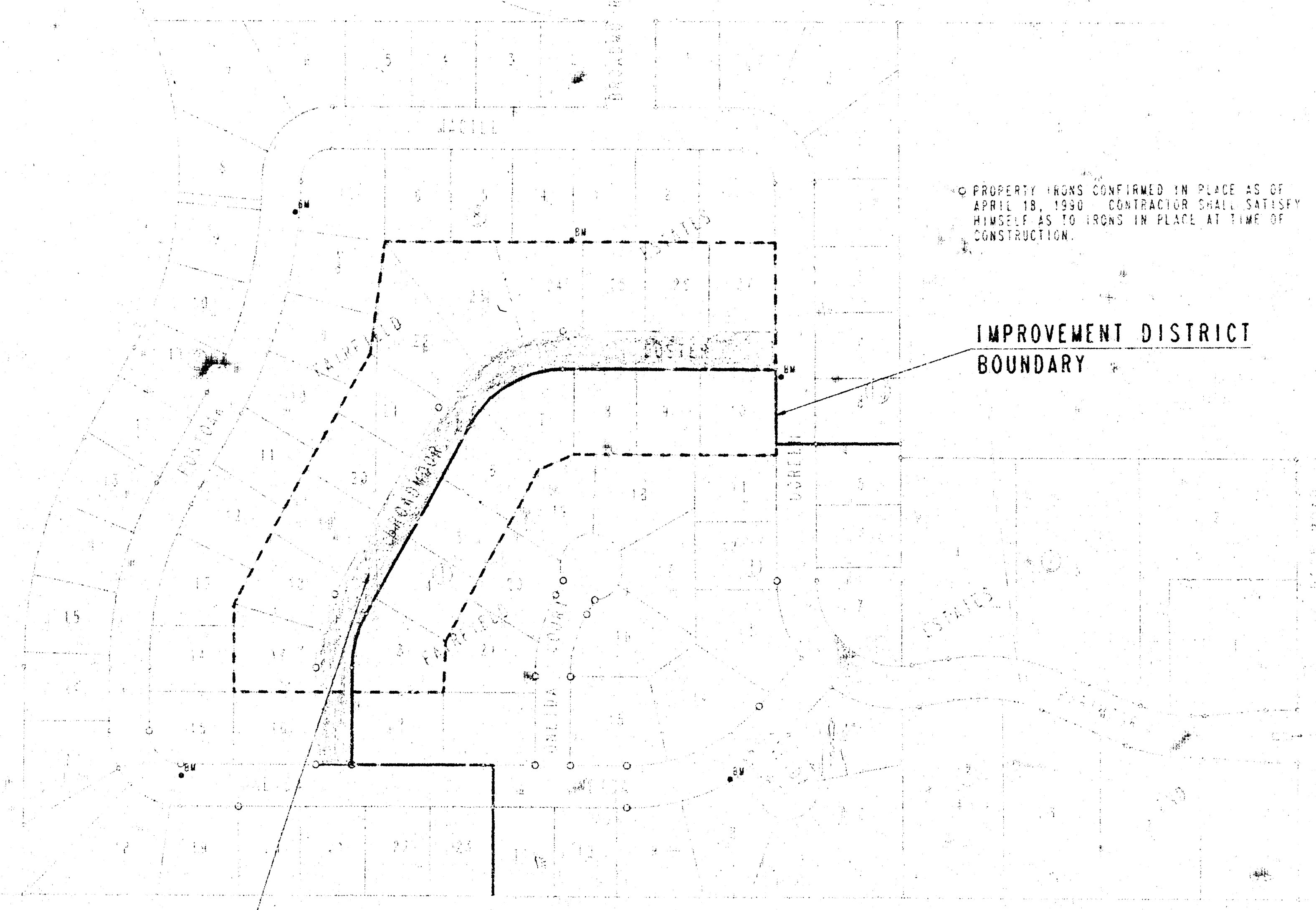
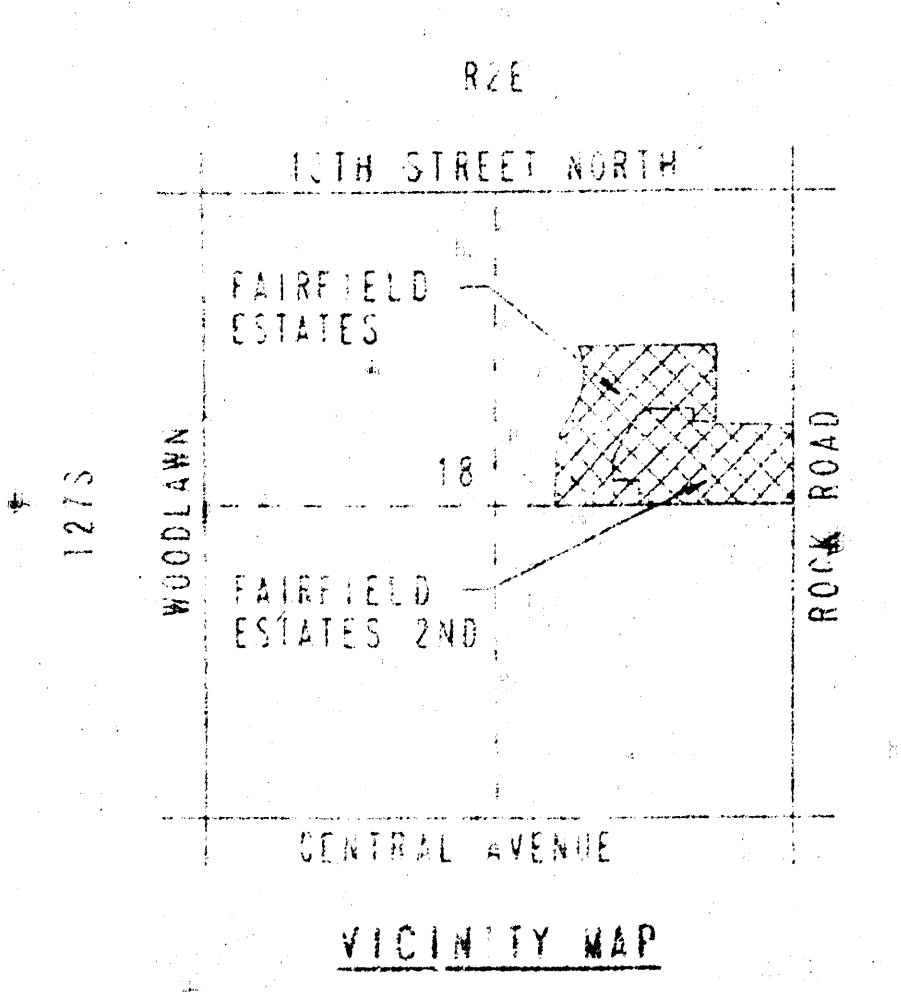
- 1. THE SHEET
- 2. TYPICAL SECTION 17" A.C. PAVEMENT
- 3. TYPICAL SECTION 15" A.C. PAVEMENT W/ IMPROVED ROCK BASE
- 4. DRAINAGE PLANS
- 5. CROSS SECTIONS

PROJECT SURVEY CONTROL

- BENCH MARK: 401 SPIKE SET IN SOUTHWEST CORNER OF WOOD FENCE AT THE SOUTHWEST CORNER OF LOT 3, BLOCK 8, FAIRFIELD ESTATES. ELEV: 1186.59
- BENCH MARK: CHISELED "0" ON TOP OF CURB ON WEST SIDE DOREEN STREET ON SOUTH END CURB RETURN AT FOSTER STREET NEAR THE NORTHEAST CORNER OF LOT 10, BLOCK 1, FAIRFIELD ESTATES 2ND ADDITION. ELEV: 1192.88
- BENCH MARK: CHISELED "0" ON TOP OF CURB SOUTH SIDE ONEIDA ON SOUTHWEST END CURB RETURN AT ONEIDA CURB NEAR NORTH P.I. LOT 9, BLOCK 8, FAIRFIELD ESTATES 2ND ADDITION. ELEV: 1178.69
- BENCH MARK: CHISELED "0" ON TOP OF CURB AT P.T. ON NORTH SIDE ONEIDA, SOUTH OF THE SOUTHWEST CORNER OF LOT 15, BLOCK 8, FAIRFIELD ESTATES. ELEV: 1177.43
- BENCH MARK: CHISELED "0" ON TOP OF CURB ON EAST SIDE OF POSTOAK STREET ON NORTH RETURN OF SIDEWALK CURB INLET, 30' NORTH OF THE SOUTHWEST CORNER OF LOT 7, BLOCK 8, FAIRFIELD ESTATE. ELEV: 1183.53

EARTHWORK

EXCAVATION: 1,710 CU. YDS.
 COMPACTED FILL: 97 CU. YDS.
 SUBJECT MANIPULATION: 3,775 CU. YDS.
 PROJECT LENGTH: 1,118.9 L.F.



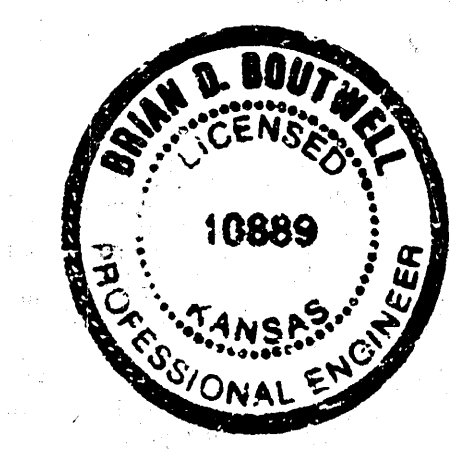
PROPERTY IRONS CONFIRMED IN PLACE AS OF APRIL 18, 1990. CONTRACTOR SHALL SATISFY HIMSELF AS TO IRONS IN PLACE AT TIME OF CONSTRUCTION.

IMPROVEMENT DISTRICT BOUNDARY

STREET IMPROVEMENTS

AUGUST, 1991

PLANS PREPARED BY
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS
 WICHITA, KANSAS



GENERAL NOTES

- UNDERGROUND UTILITIES (ELECTRIC, GAS, WATER, SEWER, TELEPHONE, CABLEVISION, ETC.) SHALL BE FIELD LOCATED AND MARKED PRIOR TO CONSTRUCTION UNLESS OTHERWISE SPECIFICALLY NOTED OTHERWISE BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION OBTAINABLE FOR THE LOCATION INFORMATION HAS BEEN OBTAINED FROM THE AIRPHOTO SURVEY SUPPLIED AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE CONTRACTOR WILL BE RESPONSIBLE TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT CONFLICT WITH PROPOSED CONSTRUCTION.
- EXCESS EXCAVATION WHICH IS TO BE FASHD SHALL BE DISPOSED OF AS DIRECTED BY THE OWNER. PUBLIC WORKS DEPARTMENT SPECIAL PERMITS SHALL BE OBTAINED OF ON SITES PROVIDED BY THE CONTRACTOR AND SUPERVISED BY THE ENGINEER.
- CONTRACTOR SHALL SATISFY HIMSELF OF SUBSURFACE CONDITIONS PRIOR TO CONSTRUCTION.
- TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN THE RIGHT-OF-WAY SHALL BE REMOVED BY THE CONTRACTOR WITH THE ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONTACT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.
- TEMPORARY SURFACING MATERIAL (GRAVEL, ASPHALT, ETC.) MAY HAVE BEEN PLACED WITHIN STREET RIGHTS-OF-WAY FOR SIDE ROADS AND TEMPORARY ACCESS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS TO DETERMINE EXTENT OF SUCH SURFACING. CONTRACTOR SHALL REMOVE SAID TEMPORARY SURFACING IN THE SAME MANNER AS NOTED ABOVE FOR PUBLIC. THIS REMOVAL SHALL BE SUBORDINATE TO OTHER BID ITEMS.
- CONTRACTOR SHALL PROVIDE A MINIMUM FORTY-EIGHT (48) HOUR ADVANCE NOTICE (EXCLUDING WEEKENDS AND HOLIDAYS) PRIOR TO BEGINNING ANY EXCAVATION, TO KANSAS ONE CALL SYSTEM, A UTILITY LOCATION SERVICE, AT 1-800-842-4343 TO REQUEST THE FOLLOWING UTILITIES: POWER, GAS, TELEPHONE, CABLEVISION, TELEPHONE, CABLEVISION, CITY OF WICHITA SEWER MAINTENANCE AND CITY OF WICHITA WATER DEPARTMENT.
- LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND ONE FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPE UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH THE EXISTING GROUND SURFACE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAW.
- THE WATER DEPARTMENT SHALL FIELD LOCATE WATER VALVES ONE TIME DURING CONSTRUCTION WHEN REQUESTED BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PRESERVE SUCH FIELD LOCATIONS DURING THE CONSTRUCTION PROJECT. WATER VALVES, WATER VALVE BOXES OR FIRE HYDRANTS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL ADJUST WATER VALVE BOXES AS DIRECTED BY THE ENGINEER. THIS WORK TO BE SUBORDINATE TO OTHER BID ITEMS.
- PROPERTIES WITHIN THE PROJECT LIMITS MAY HAVE UNDERGROUND SPRINKLER SYSTEMS IN PUBLIC RIGHTS-OF-WAY WHICH CONFLICT WITH NEW CONSTRUCTION. CONTRACTOR SHALL BE REQUIRED TO REMOVE SUCH IMPROVEMENTS SHOULD THEY NOT BE REMOVED BY THEIR OWNER AT THE TIME OF CONSTRUCTION OF THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SAVE ALL SPRINKLER HEADS AND/OR VALVES AND GIVE SUCH MATERIAL TO THEIR OWNER. PORTIONS OF UNDERGROUND SPRINKLER SYSTEMS NOT IN CONTACT WITH NEW CONSTRUCTION SHALL BE PROTECTED FROM DAMAGE AND SHALL REMAIN IN PLACE. ALL WORK IN CONTACT WITH UNDERGROUND SPRINKLER SYSTEMS SHALL BE CONSIDERED AS SUBORDINATE TO THE CONTRACT PAY ITEMS OF WORK.
- THIS PROJECT DOES NOT INCLUDE ANY PROVISIONS FOR CONSTRUCTION OF SIDEWALKS.
- THIS PROJECT INCLUDES AN ALTERNATE TYPICAL SECTION CONSISTING OF AN 18" DRAINAGE PAVEMENT OVER A FABRIC-REINFORCED PUSHED-ROCK BASE. THE ENGINEER WILL REQUIRE USE OF THE ALTERNATE CONSTRUCTION TYPE IN LIEU OF ASPHALT PAVEMENT OVER A TREATED SUBGRADE. THE CONTRACTOR SHALL BID BASE ON THIS AND ALTERNATE ITEMS, AND THE FINAL SELECTION OF CONSTRUCTION TYPE SHALL BE MADE BY THE ENGINEER AT THE TIME OF CONSTRUCTION. THE USE OF EITHER TYPE OF ENGINEER SHALL BE USED FOR THE ENTIRE PROJECT. EARTHWORK QUANTITIES SHOWN ON THE PLANS WERE CALCULATED BASED ON USE OF A TREATED SUBGRADE.

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FAIRFIELD ESTATES

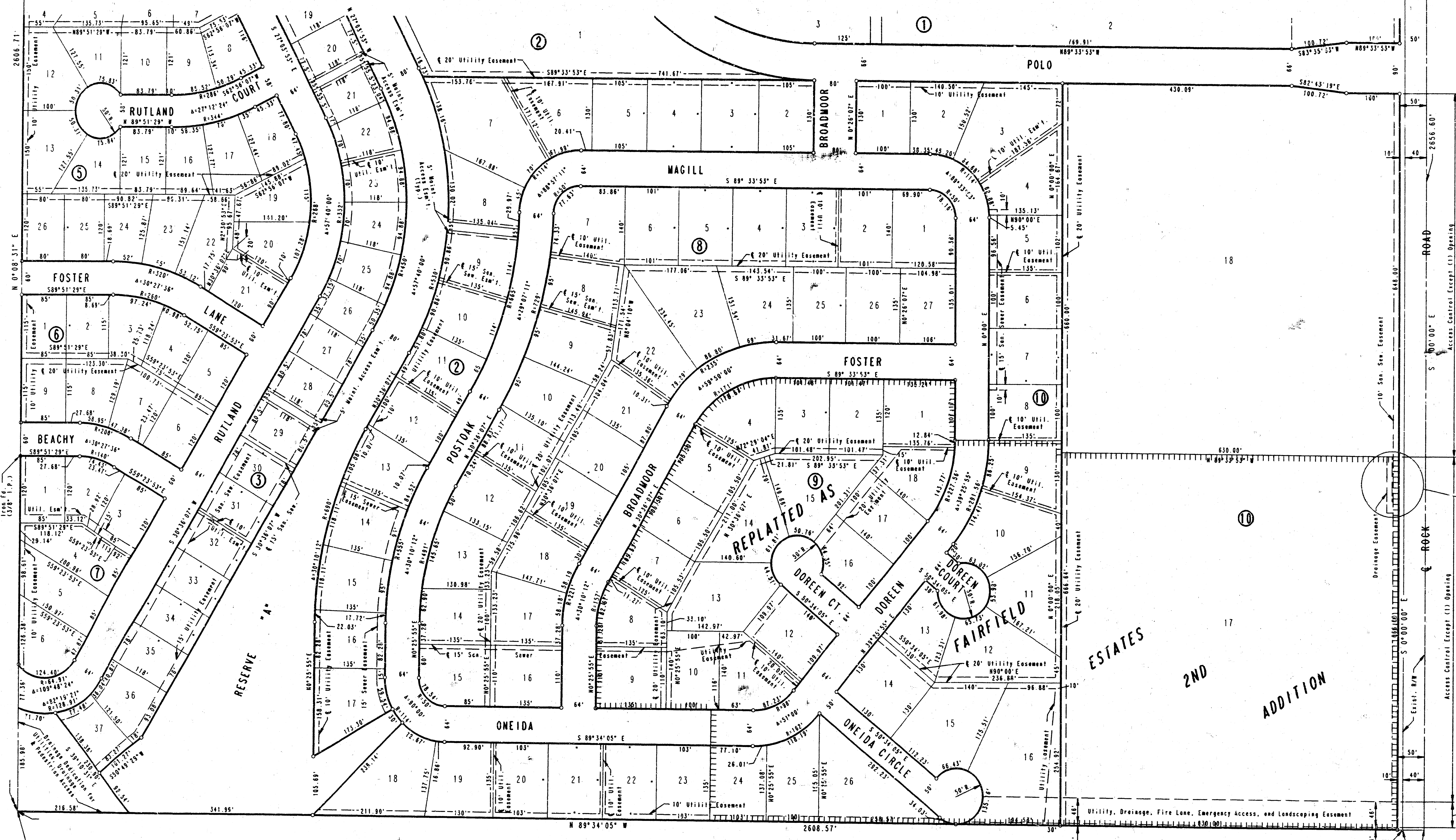
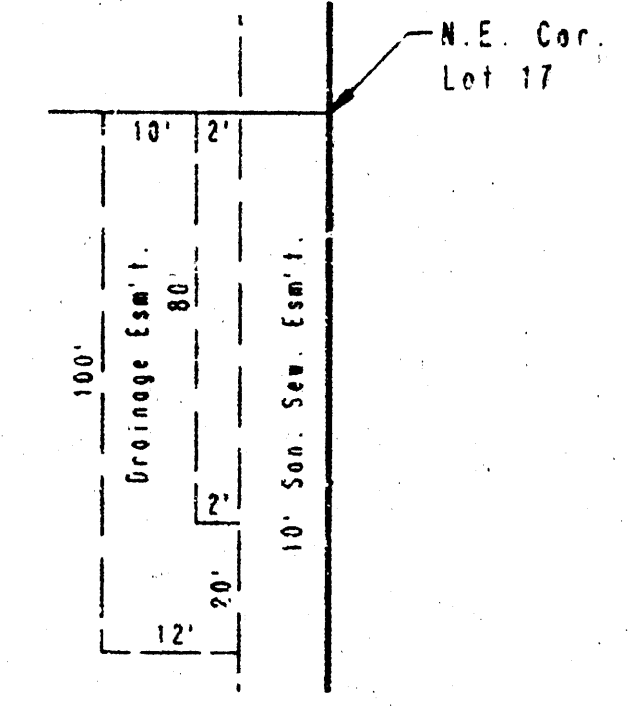
WICHITA, SEDGWICK COUNTY, KANSAS

PROJECT NO. 472-76-245-82000-000-001	SHEET NO. 2	TOTAL SHEETS 11
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SCALE: 1" = 100'
C.A.C.-COMPLETE ACCESS CONTROL
SEE SHEET NO. 1 FOR BENCH MARK

BUILDING SETBACKS AS PER THE REQUIREMENTS OF THE FAIRFIELD COMMERCIAL C.U.P. (DP-144), FAIRFIELD RESIDENTIAL C.U.P. (DP-146), AND FAIRFIELD ESTATES C.U.P. (DP-149) ON FILE WITH THE WICHITA SEDGWICK COUNTY METROPOLITAN AREA PLANNING DEPARTMENT



SW Cor. NE 1/4
Sec. 18, T27S, R2E
of the 6th P.M.

Point of Beginning
SE Cor. NE 1/4
Sec. 18, T27S, R2E
of the 6th P.M.

FAIRFIELD ESTATES/FAIRFIELD ESTATES 2ND ADDITION

PLAT

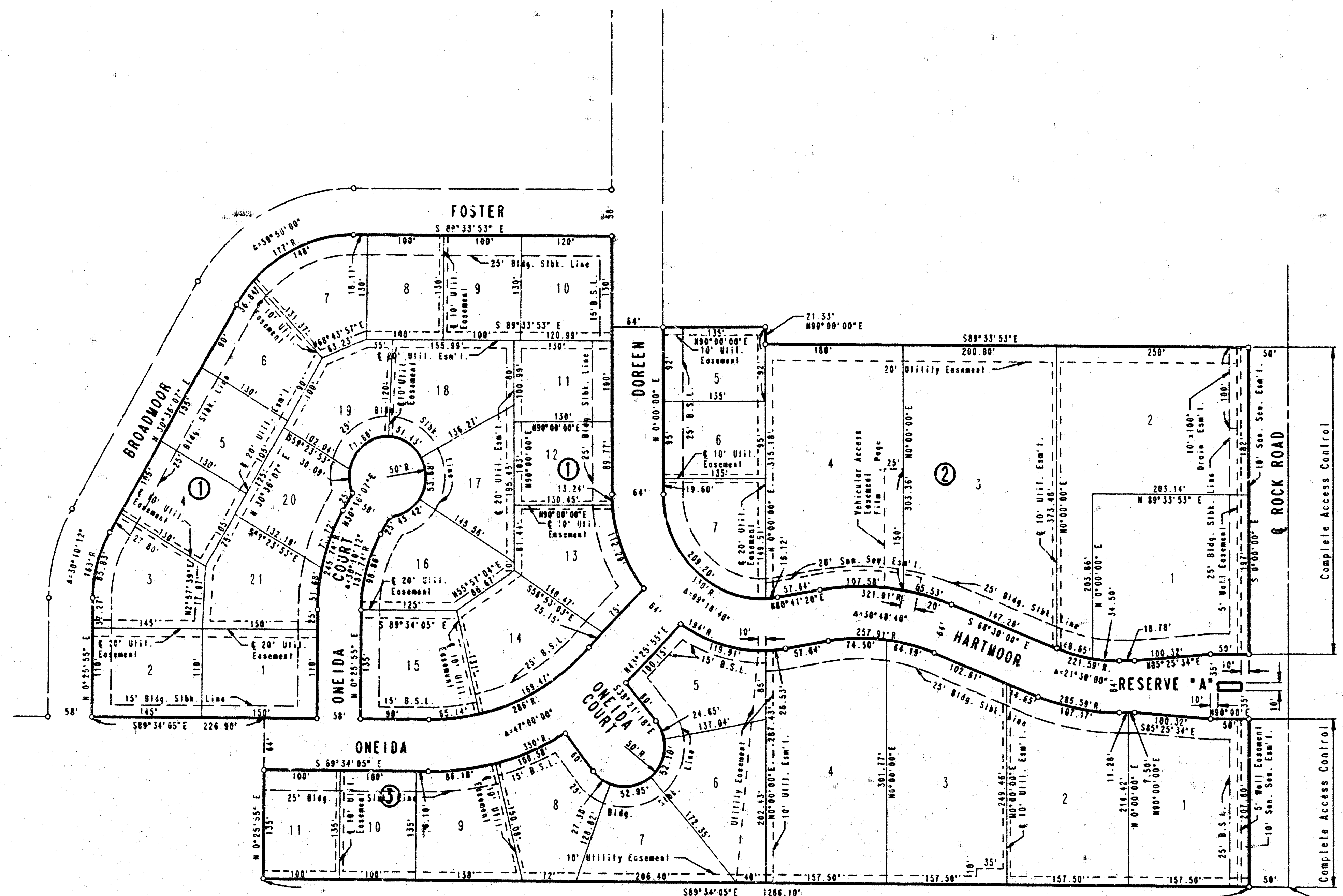
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
ENGINEERS
WICHITA, KANSAS

Designed by	Checked by
Drawn by	Date
DEF	AUG 1981 Job No. 91364

FAIRFIELD ESTATES 2ND ADDITION

AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

PROJECT NO. 472-16-245-82000-000-001	SHEET NO. 3	TOTAL SHEETS 11
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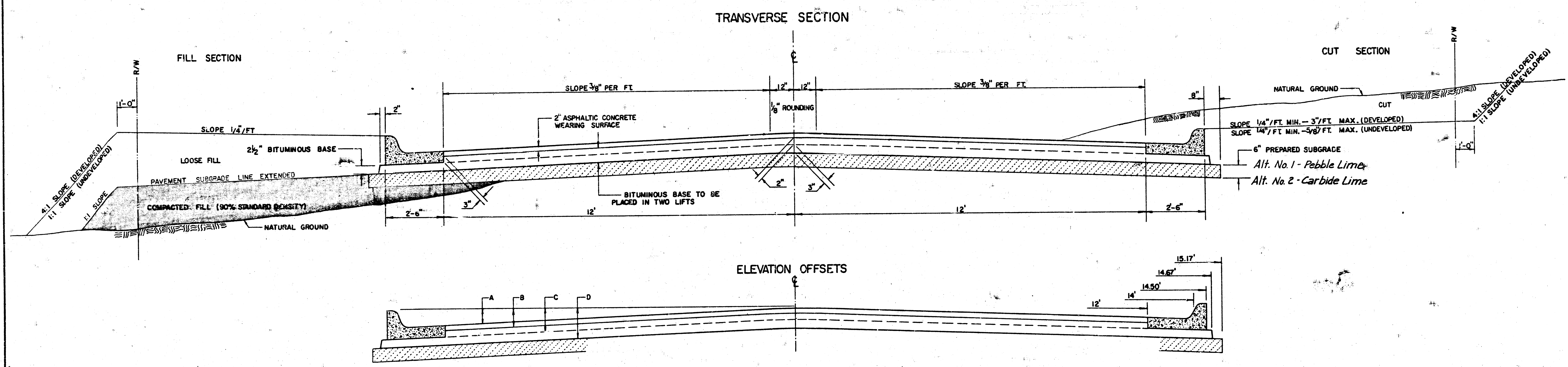


SCALE: 1" = 100'
 ○ = IRON SET
 B.S.L. - BUILDING SETBACK LINE
 B.M. - CHISELED 4" S. END CURB RETURN
 WEST SIDE ROCK RD. AT E. 1/4 COR.
 SEC. 18, T27S, R2E.
 ELEV. = 179.18 CITY DATUM

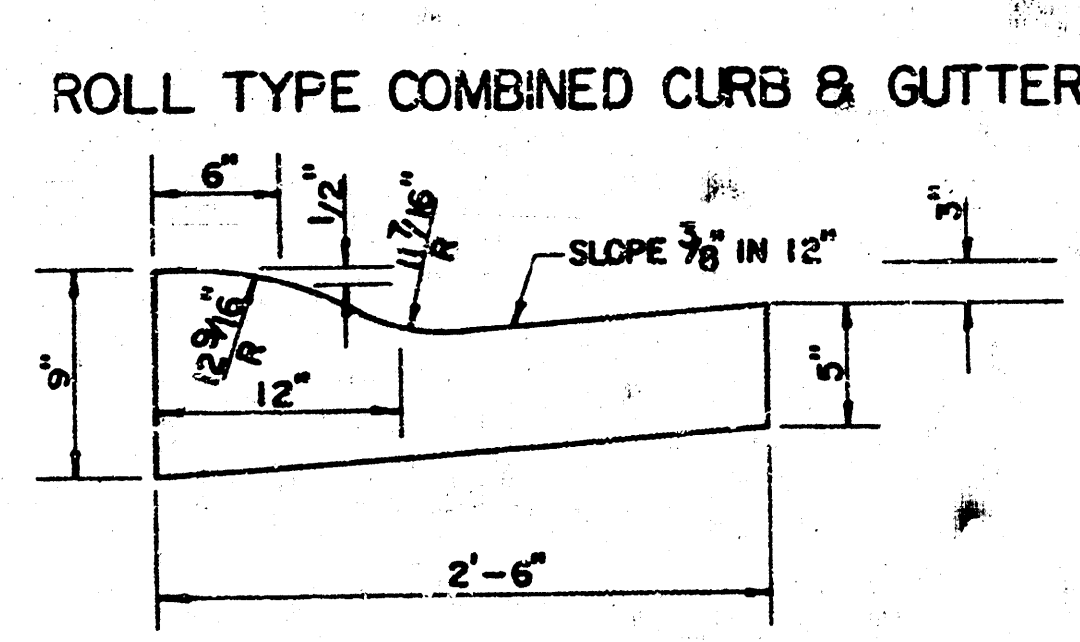
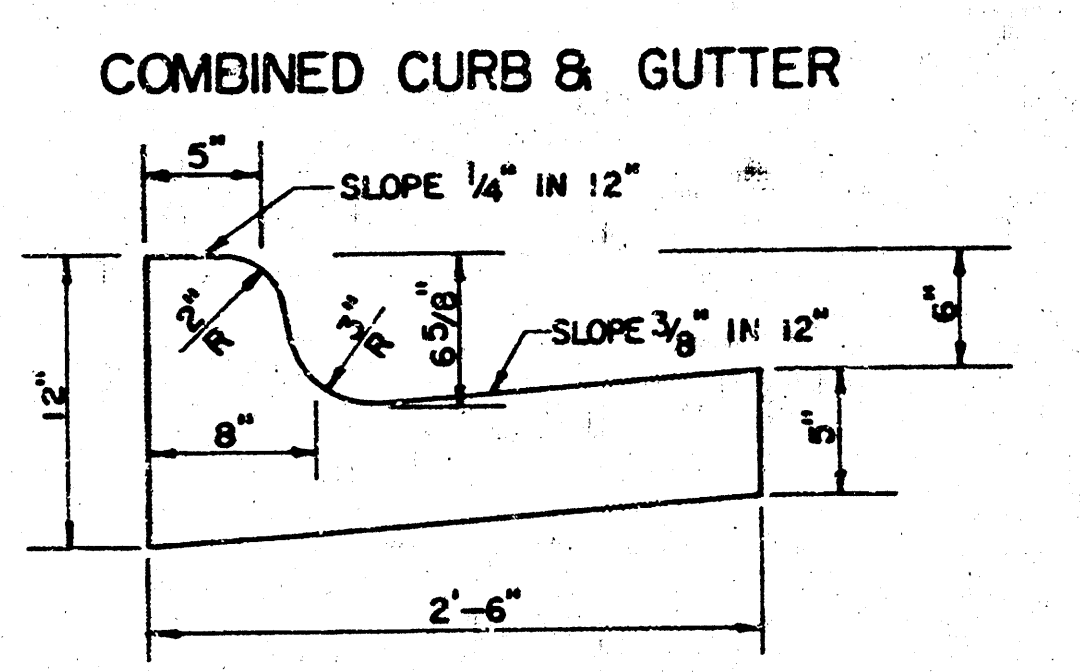
S.E. Cor. N.E. 1/4
 Sec. 18, T27S, R2E
 of the 6th P.M.

FAIRFIELD ESTATES ADDITION/FAIRFIELD ESTATES 2ND ADDITION			
PLAT			
PROFESSIONAL ENGINEERING CONSULTANTS, P.A.			
ENGINEERS WICHITA, KANSAS			
Designed by		Checked by	
Drawn by	DEP	Date	AUG. 1991 Job No. 91364

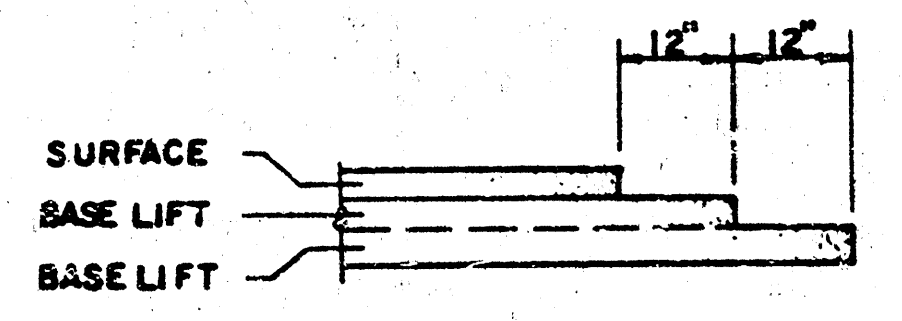
TYPICAL 29' PAVEMENT DETAILS



	DISTANCE FROM CENTERLINE (LT. & RT.)												
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	14.67'	15.17'	
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	—	—	—	—	
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	—	—	—	—	
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.47	0.52	0.60	0.68	0.71	0.75	0.83	0.90	0.98	1.00	1.01	—	
D: TOP OF CURBS TO TOP OF SUBGRADE	0.72	0.77	0.84	0.91	0.94	0.98	1.05	1.12	1.19	1.21	1.21	1.23	



TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

GENERAL NOTES

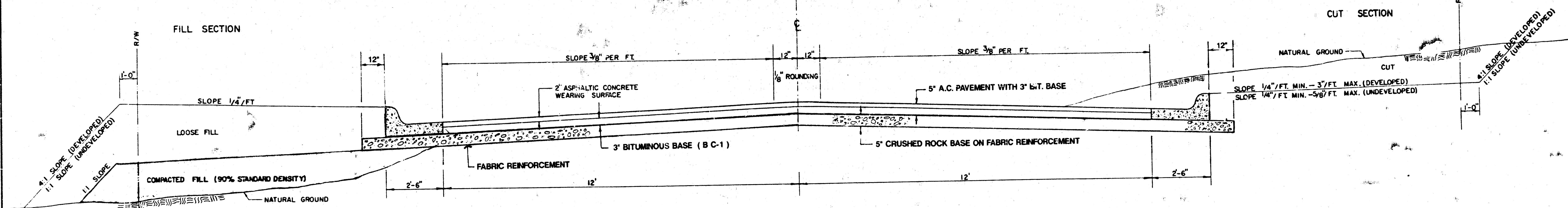
- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE
CITY OF WICHITA, KANSAS
 PROJECT NUMBER
 472-82000

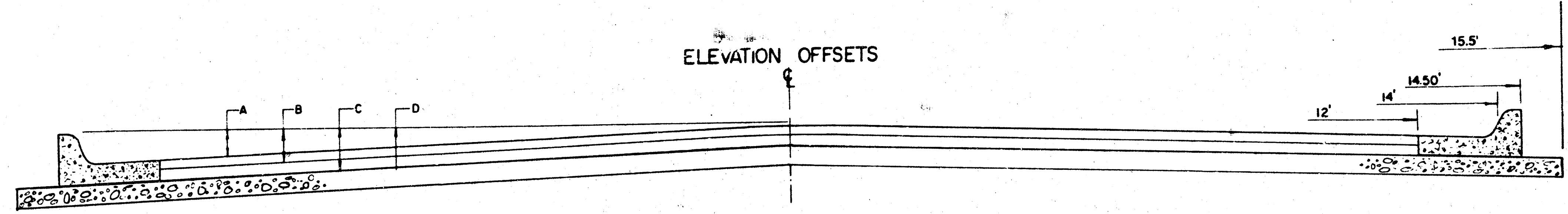
TYPICAL 29' PAVEMENT DETAILS

5 11

TRANSVERSE SECTION



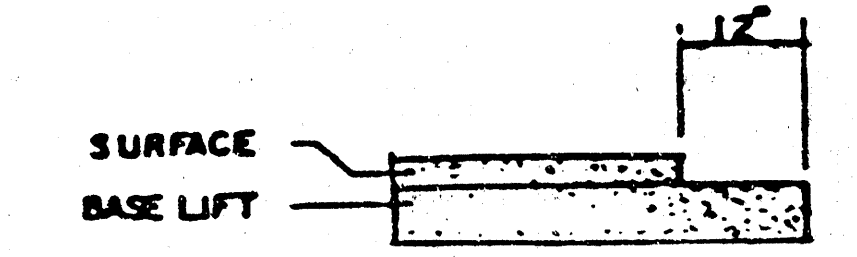
ELEVATION OFFSETS



	DISTANCE FROM CENTERLINE (LT. & RT.)										
	0'	2'	4'	6'	7'	8'	10'	12'	14'	14.5'	
A. TOP OF CURBS TO TOP OF SURFACE LIFT	0.13	0.18	0.24	0.30	0.33	0.36	0.43	0.49	—	—	—
B. TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.30	0.35	0.41	0.47	0.50	0.53	0.60	0.66	—	—	—
C. TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.47	0.52	0.60	0.68	0.71	0.75	0.83	0.90	0.98	1.00	—
D. TOP OF CURBS TO TOP OF SUBGRADE											

FABRIC BASE REINFORCEMENT SHALL BE 8 X 1100 GEOGRID AS MANUFACTURED BY TENSAR CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS. CRUSHED ROCK SHALL BE UNIFORMLY GRADED FROM 1-1/2" MAXIMUM SIZE TO NOT MORE THAN 10% PASSING A No. 200 SIEVE. ROCK QUALITY SHALL BE THE SAME AS SPECIFIED FOR COARSE AGGREGATE FOR CONCRETE MIXES.

TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 5' ASPHALTIC CONCRETE PAVEMENT (3' BITUMINOUS BASE)

GENERAL NOTES

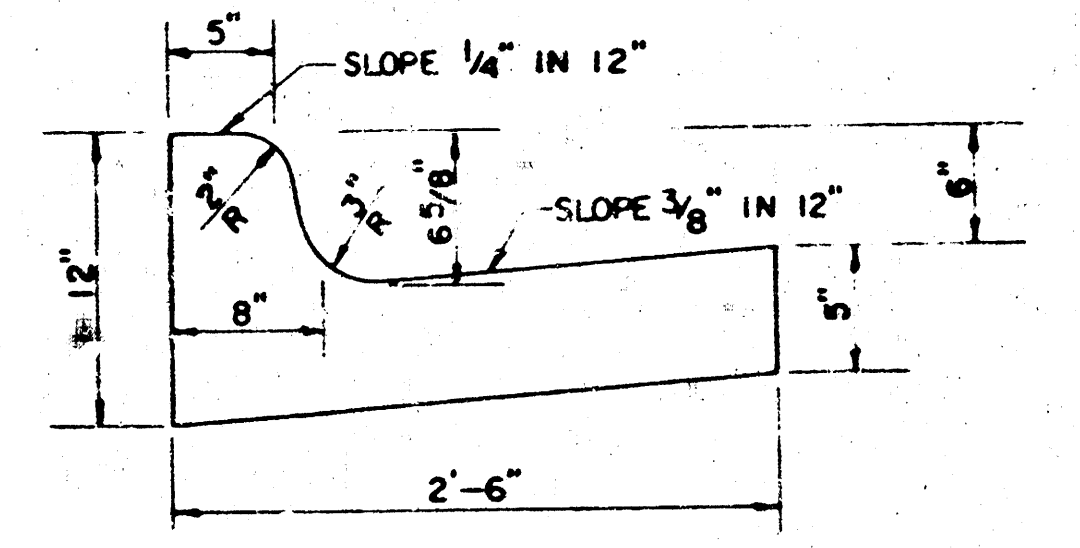
- ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.
- A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

Project No 472-82000

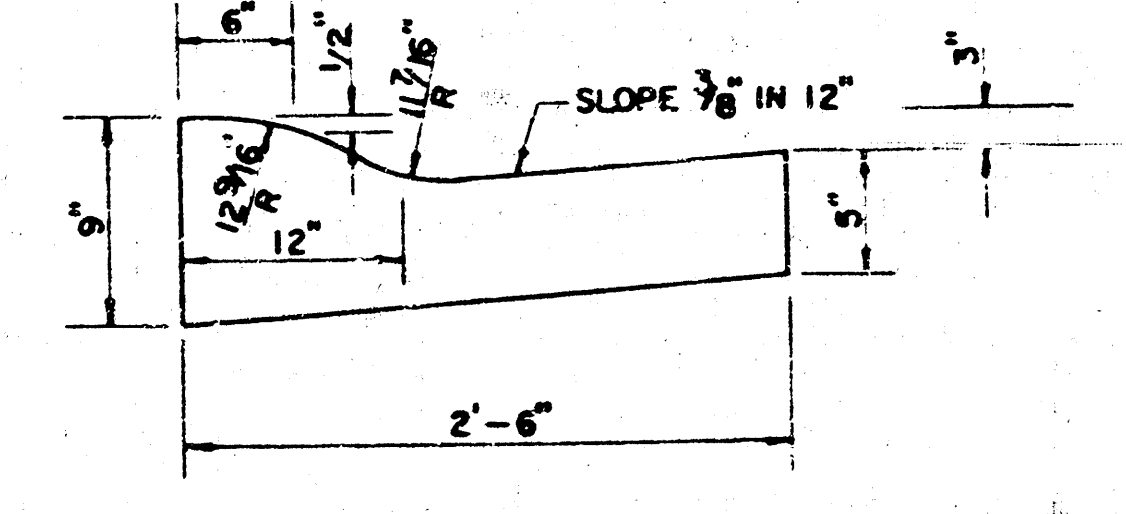
5' RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH CRUSHED ROCK BASE ON FABRIC REINFORCEMENT

10 1 5 31

COMBINED CURB & GUTTER



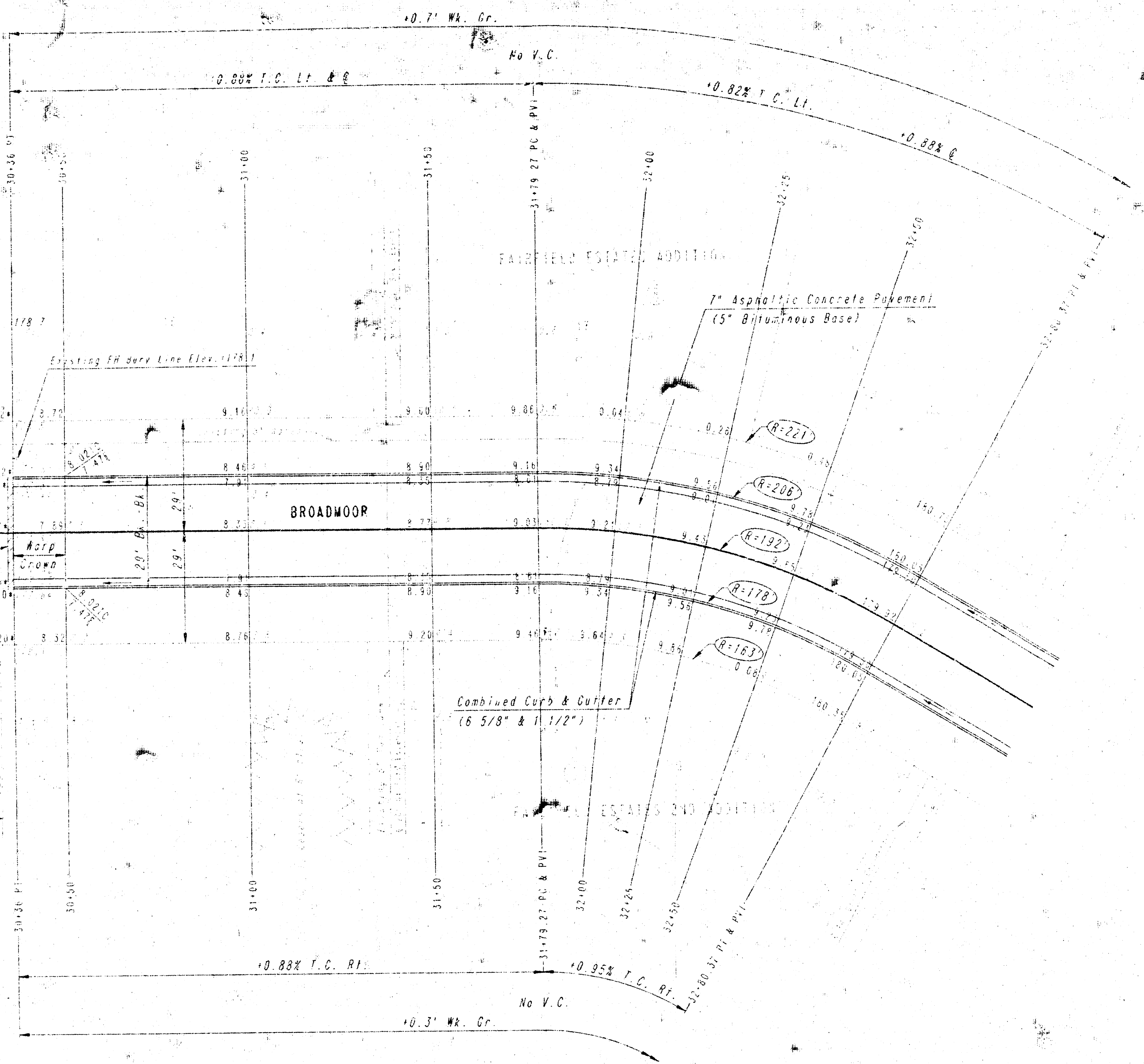
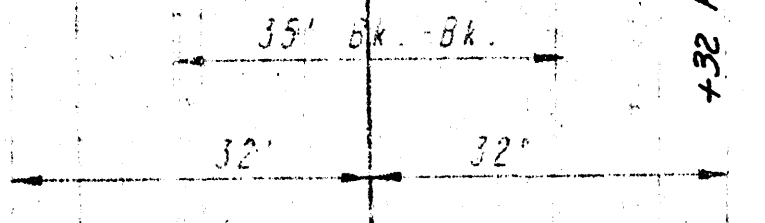
ROLL TYPE COMBINED CURB & GUTTER



100 E. 1"=100'

Sta. 30+00.00 & Broadmoor & Oneida

Work on existing pavement to provide road line. Remove and replace 1" surface course as necessary. This work to be substituted to other bid items.



PLAN SURVEIL
 NOT BROUGHT
 NO. 101 003 4

CONTRACT NO. 101 003 4
 BROADMOOR 1
 DESIGNER: J. W. BROWN
 CHECKED: J. W. BROWN
 DATE: 10/1/55
 SPECIAL INSTRUCTIONS:

Q CURVE DATA
 A=10°10'12" C=29°40'10" R=192.00' E=101.10' F=51.75' E=6.85'

CURVE DATA BASED ON Q RADIUS Δ/2=15°05'06"

Q STATION	Q ARC LENGTH	FACE CURB LENGTH	CHORD LENGTH	DEFLECTION ANGLE	Q TOTAL DEFLECTION
		LEFT CURB	RIGHT CURB	OF CURB	
31+74.27	0.00	0.00	0.00	0°00'00"	0°00'00"
31+75	2.75	22.24	19.22	23.09'	18.35'
31+76	5.50	24.80	21.80	27.85'	22.12'
31+77	8.25	26.82	24.18	27.85'	22.12'
31+78	11.00	28.22	26.18	27.85'	22.12'
31+79	13.75	29.00	27.85'	22.12'	19.43'49"
31+80	16.50	29.17	28.85'	22.12'	19.43'49"
31+80.37	5.37	5.37	4.97	5.98'	4.76'
TOTAL	101.10'	108.42'	93.73'		Defl. 111' 8.952522' n/a

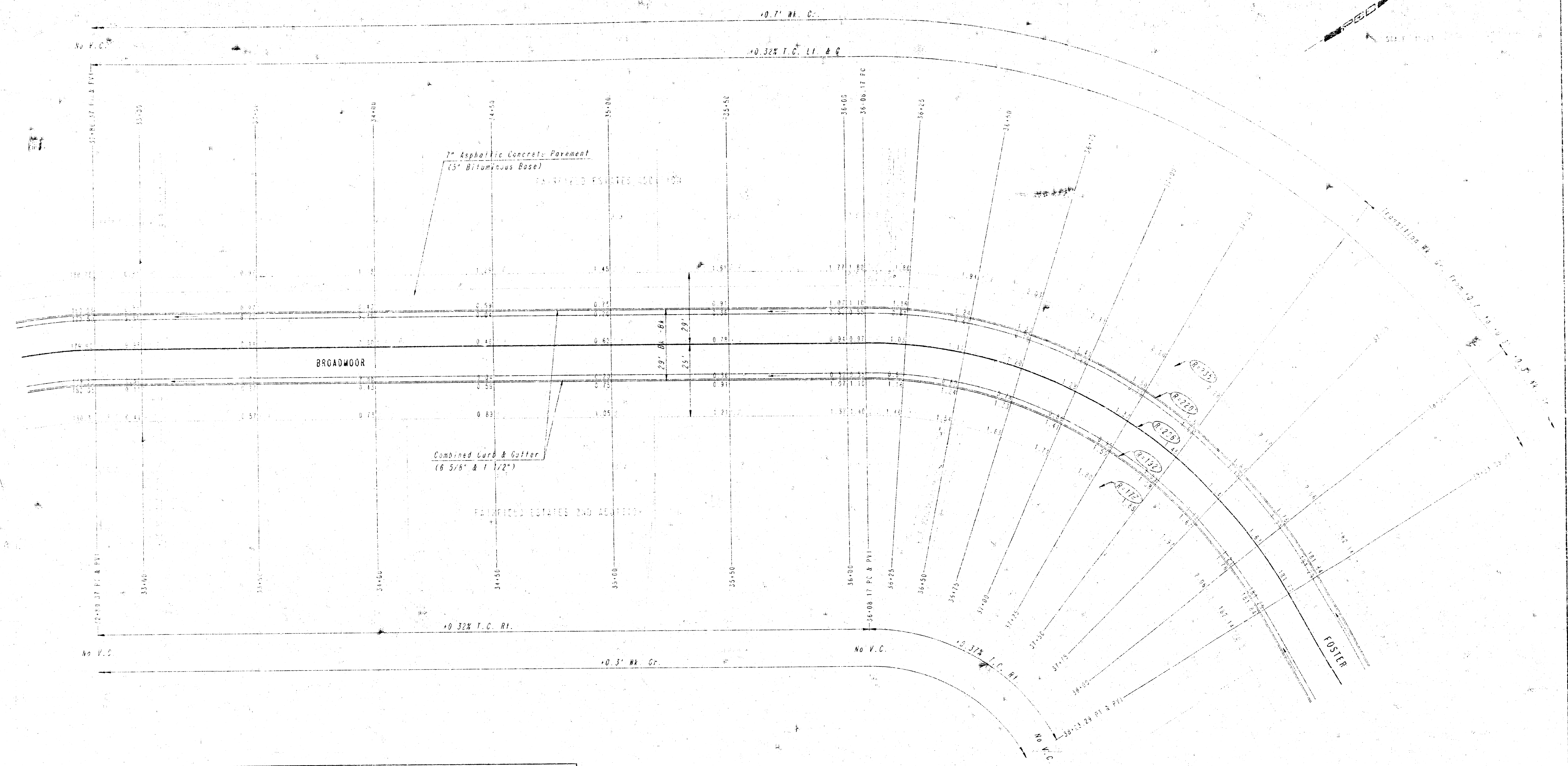
FAIRFIELD ESTATES ADDITION

BROADMOOR
 STA. 30+00.00 TO STA. 32+80.37

PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 ENGINEERS

WICHITA, KANSAS

Designed by: JWB, EDD Checked by: JWB
 Drawn by: DEP Date: 10/1/55 101 003 4



PLAN
 BROADMOOR
 32+80.37 TO 38+23.29
 1/2" = 10'-0"

Q CURVE DATA
 Δ=50°50'02" R=205.00' L=715.12' T=118.54' E=31.67'
 CURVE DATA BASED ON Q RADIUS Δ/2=25°55'00"

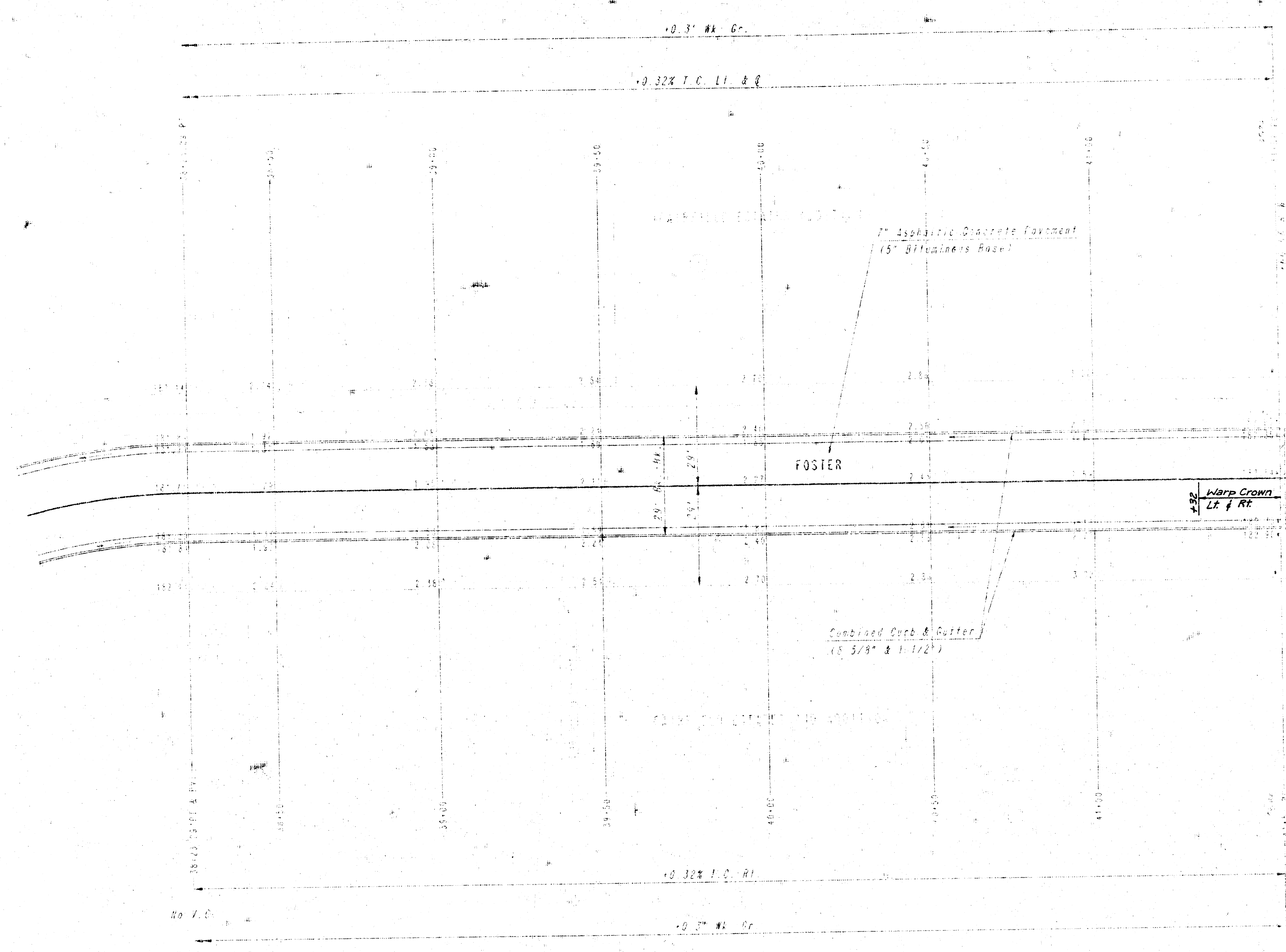
Q STATION	Q APP. LENGTH	FACE CURB LENGTH		CHORD LENGTH		DEFLECTION ANGLE	Q TOTAL DEFLECTION
		LEFT CURB	RIGHT CURB	FACE CURB	OFF RIGH. FACE CURB		
36+68.17						0°00'00"	0°00'00"
36+75	16.83	17.97	15.89	18.62	15.65	3°20'26"	3°20'26"
36+80	25.00	26.70	23.30	27.65	22.32	3°28'36"	6°48'52"
36+85	25.00	26.70	23.30	27.65	22.32	3°28'36"	9°17'33"
36+90	25.00	26.70	23.30	27.65	22.32	3°28'36"	12°46'15"
36+95	25.00	26.70	23.30	27.65	22.32	3°28'36"	16°14'51"
37+00	25.00	26.70	23.30	27.65	22.32	3°28'36"	19°43'27"
37+05	25.00	26.70	23.30	27.65	22.32	3°28'36"	23°12'04"
37+10	25.00	26.70	23.30	27.65	22.32	3°28'36"	26°40'40"
37+15	25.00	26.70	23.30	27.65	22.32	3°28'36"	30°09'16"
37+20	25.00	26.70	23.30	27.65	22.32	3°28'36"	33°37'52"
37+25	25.00	26.70	23.30	27.65	22.32	3°28'36"	37°06'28"
37+30	25.00	26.70	23.30	27.65	22.32	3°28'36"	40°35'04"
37+35	25.00	26.70	23.30	27.65	22.32	3°28'36"	44°03'40"
37+40	25.00	26.70	23.30	27.65	22.32	3°28'36"	47°32'16"
37+45	25.00	26.70	23.30	27.65	22.32	3°28'36"	51°00'52"
37+50	25.00	26.70	23.30	27.65	22.32	3°28'36"	54°29'28"
37+55	25.00	26.70	23.30	27.65	22.32	3°28'36"	58°08'04"
37+60	25.00	26.70	23.30	27.65	22.32	3°28'36"	61°36'40"
37+65	25.00	26.70	23.30	27.65	22.32	3°28'36"	65°05'16"
37+70	25.00	26.70	23.30	27.65	22.32	3°28'36"	68°33'52"
37+75	25.00	26.70	23.30	27.65	22.32	3°28'36"	72°02'28"
37+80	25.00	26.70	23.30	27.65	22.32	3°28'36"	75°31'04"
37+85	25.00	26.70	23.30	27.65	22.32	3°28'36"	79°09'40"
37+90	25.00	26.70	23.30	27.65	22.32	3°28'36"	82°48'16"
37+95	25.00	26.70	23.30	27.65	22.32	3°28'36"	86°26'52"
38+00	25.00	26.70	23.30	27.65	22.32	3°28'36"	90°05'28"
38+05	25.00	26.70	23.30	27.65	22.32	3°28'36"	93°44'04"
38+10	25.00	26.70	23.30	27.65	22.32	3°28'36"	97°22'40"
38+15	25.00	26.70	23.30	27.65	22.32	3°28'36"	101°01'16"
38+20	25.00	26.70	23.30	27.65	22.32	3°28'36"	104°19'52"
38+23.29	23.29	24.87	21.74	25.76	20.79	3°11'20"	107°38'28"
TOTAL	215.72	223.74	200.30				

Def't 1/1" = 8.34180 min.

FAIRFIELD ESTATE AND ADDITION
BROADMOOR
 STA. 32+80.37 TO STA. 38+23.29
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 WICHITA, KANSAS
 Designed by: [Signature] Drawn by: [Signature]

FILMED FROM THE BEST AVAILABLE COPY

PLAN
 PROJECT
 DATE
 DRAWN BY
 CHECKED BY
 SCALE



STATE OF PENNSYLVANIA
 COMMONWEALTH
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
 HARRISBURG, PA.

FOSTER
 STA. 48+25.29 TO STA. 41+33.13
 PROFESSIONAL ENGINEERING CONSULTANTS, P.A.
 HARRISBURG, PA.

FILMED FROM THE BEST AVAILABLE COPY

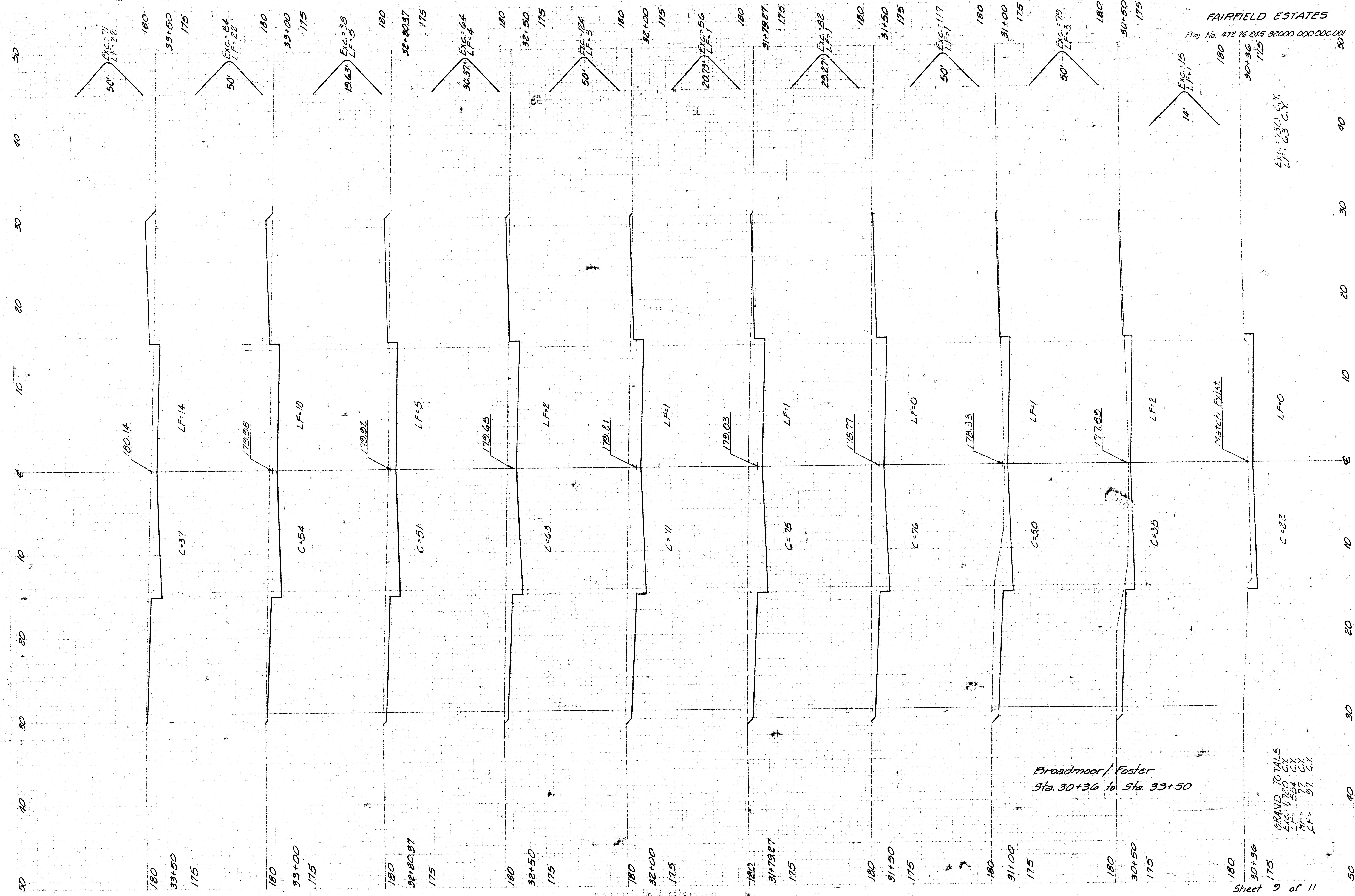
FAIRFIELD ESTATES

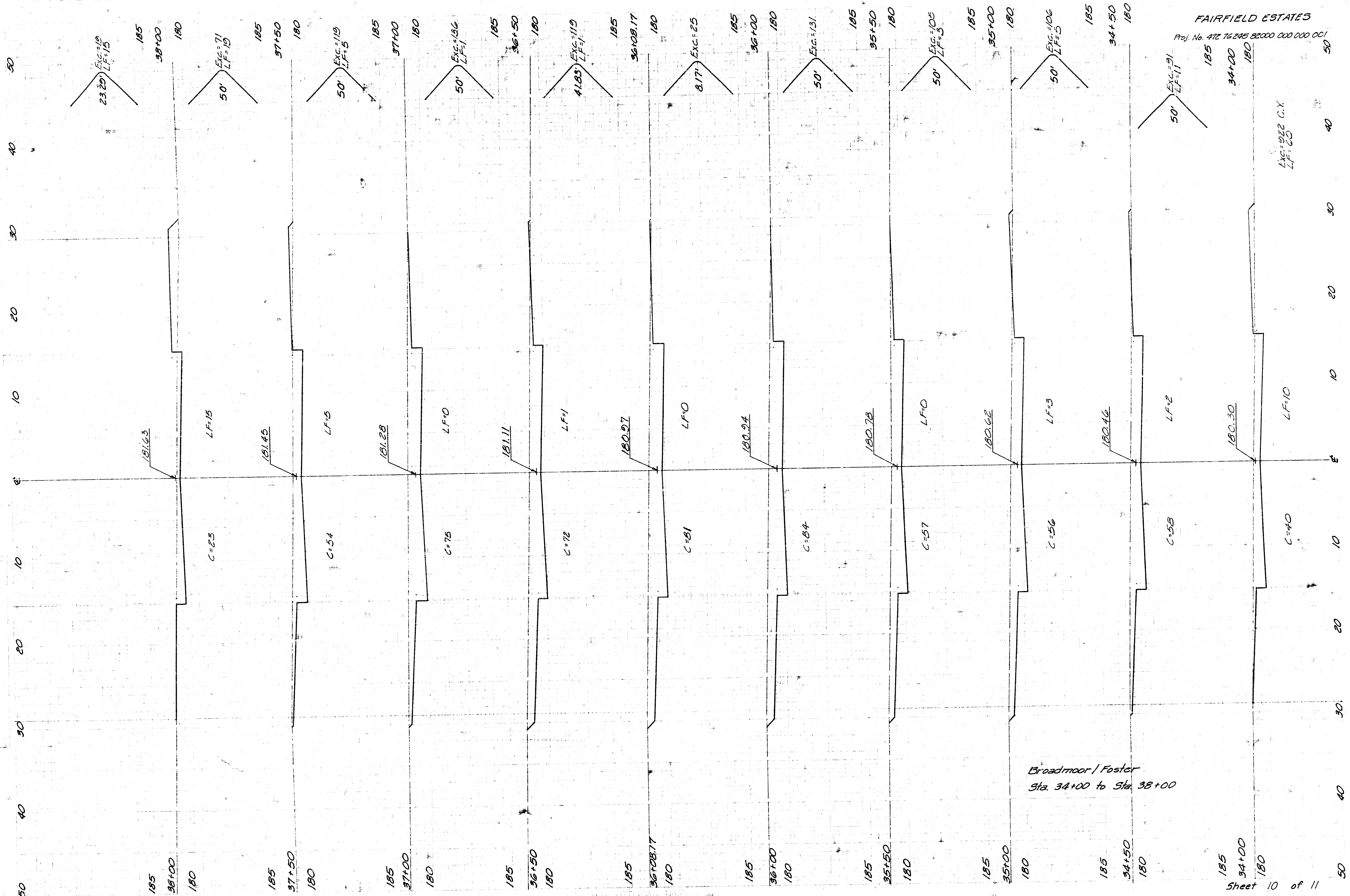
Proj. No. 472 76 245 82000 000 000 001

Exc. = 230 C.Y.
LF = 63 C.Y.

GRAND TOTALS
Exc. = 120 C.Y.
LF = 234 C.Y.
MF = 77 C.Y.
LF = 51 C.Y.

Broadmoor/Foster
Sta. 30+36 to Sta. 33+50





FAIRFIELD ESTATES
 Proj. No. 472 76245 82000 000 000 001

Broadmoor / Foster
 Sta. 34+00 to Sta. 38+00

50 40 30 20 10 0 10 20 30 40 50

FAIRFIELD ESTATES

Proj. No. 472 76 245 82000 000 000 001

38+23.29
185
180

Exc: 68
LF: 43
MF: 31
CF: 37 C.Y.

38+23.29
185
180

Sheet 11 of 11

Broadmoor/Foster
Sta 38+23.29 to Sta 41+57

