

TYPICAL SECTION

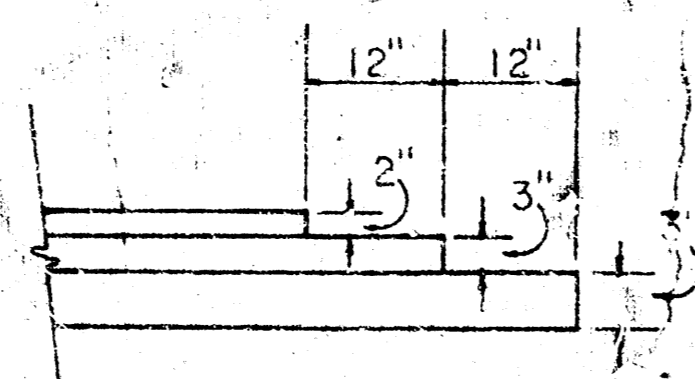
3' ASPHALTIC CONCRETE PAVEMENT WITH BITUMINOUS BASE

A TACK COAT OF EMULSIFIED ASPHALT (SS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQ. YD. BETWEEN LIFTS OF ASPHALTIC MATERIALS WHEN ORDERED BY THE ENGINEER. TACK COAT WILL NOT BE PAID FOR DIRECTLY AND SHALL BE CONSIDERED AS SUBSIDIARY TO PRICE BID FOR ASPHALTIC PAVEMENT.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC ELECTRONIC CONTROLS FOR CROWN AND GRADE. CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF 1' WITH JOINTS IN PRECEDING LIFTS AND SUCH THAT A JOINT WILL BE CONSTRUCTED ON THE PAVEMENT CENTERLINE IN THE TOP LIFT.

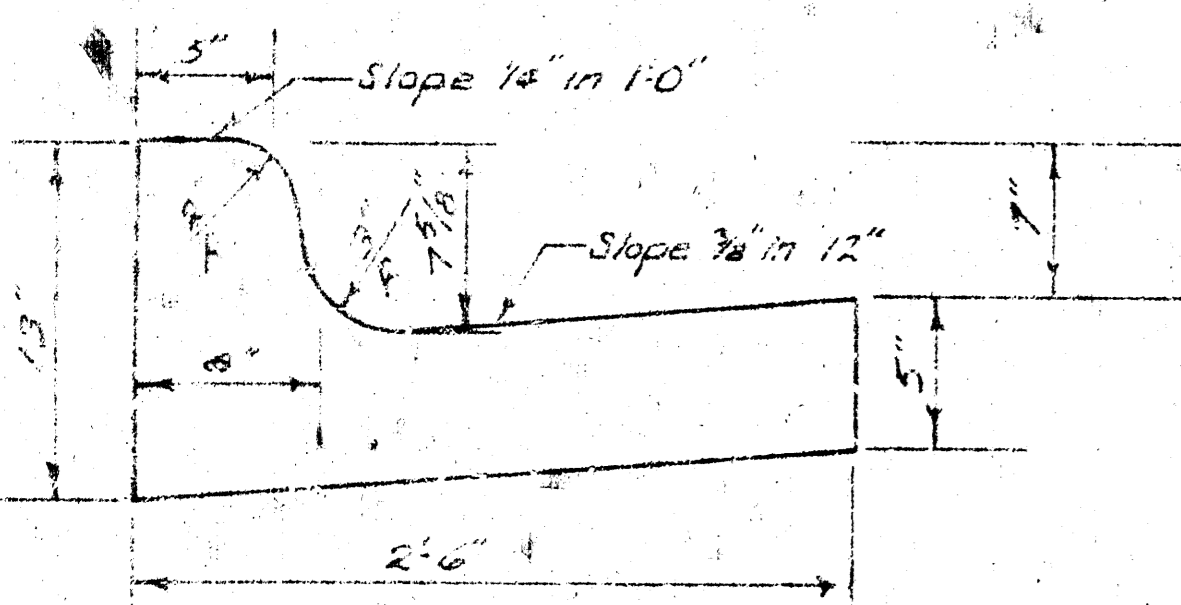
The A.C. pavement between the Comb. Curb & gutter shall be paid as Sq. Yds. 3" A.C. Pavement (6" Bituminous Base). The Bituminous Base under the Comb. Curb & gutter shall be paid as Sq. Yds. 3" Bituminous Base.

DETAIL OF TRANSVERSE CONSTRUCTION JOINTS

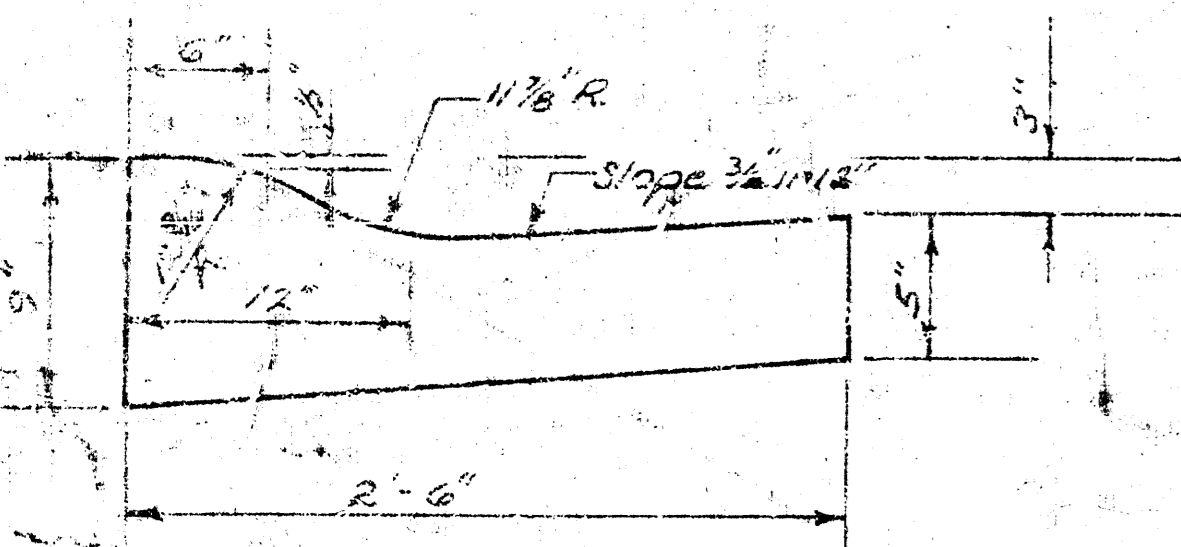


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT TEMPORARILY ENDS TO FACILITATE FUTURE PAVEMENT CONSTRUCTION AS SHOWN BY DETAIL. THE COST OF CONSTRUCTING THE TRANSVERSE CONSTRUCTION JOINTS SHALL NOT BE MEASURED OR PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS OF ASPHALTIC CONCRETE PAVEMENT.

COMBINED CURB & GUTTER



ROLL TYPE CURB & GUTTER



CITY OF WICHITA, KANSAS

DEPARTMENT OF PUBLIC WORKS — ENGINEERING

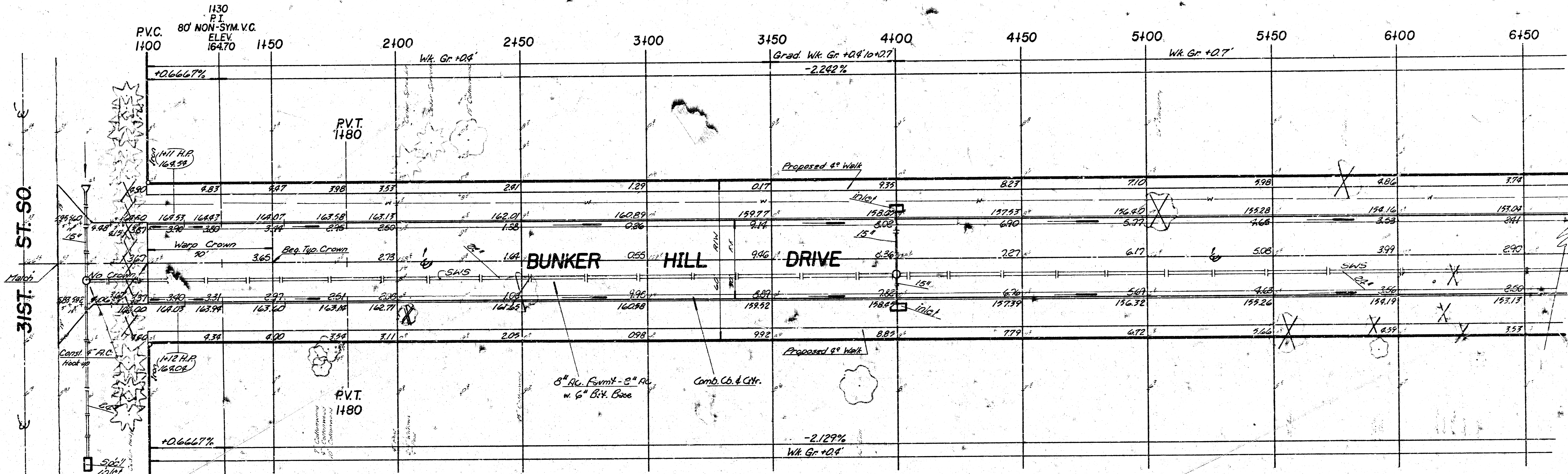
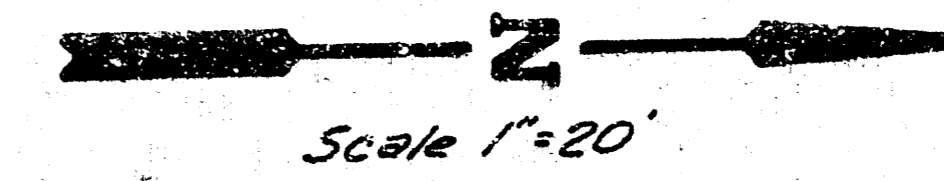
R. W. LINN

CITY ENGINEER

DATE: _____

PROJ. NO. _____

B.M. 166.50 R.R. Sp. Sa Face RP 70' West of W.L.
 Bunker Hill Dr & 3rd No. of E. 31st St. Sa
 @ is @ Bunker Hill Dr



P.V.C. P.I. 80' NON-SYM. V.C. ELEV. 164.20

- GENERAL NOTES** EQUIVALENT
- No More Than 12'-20' Drives or the Equivalent May Be Constructed on this Project.
 - Ditching to be Incidental to Project.
 - Paving Contractor to Coordinate Construction with Drainage Contractor.

Trees to be removed are marked with ✂, except that any tree which in the opinion of the Engineer can be saved shall be spared. Contractor to take ties and shall replace any 'trees' either moved or destroyed during construction.

EARTHWORK

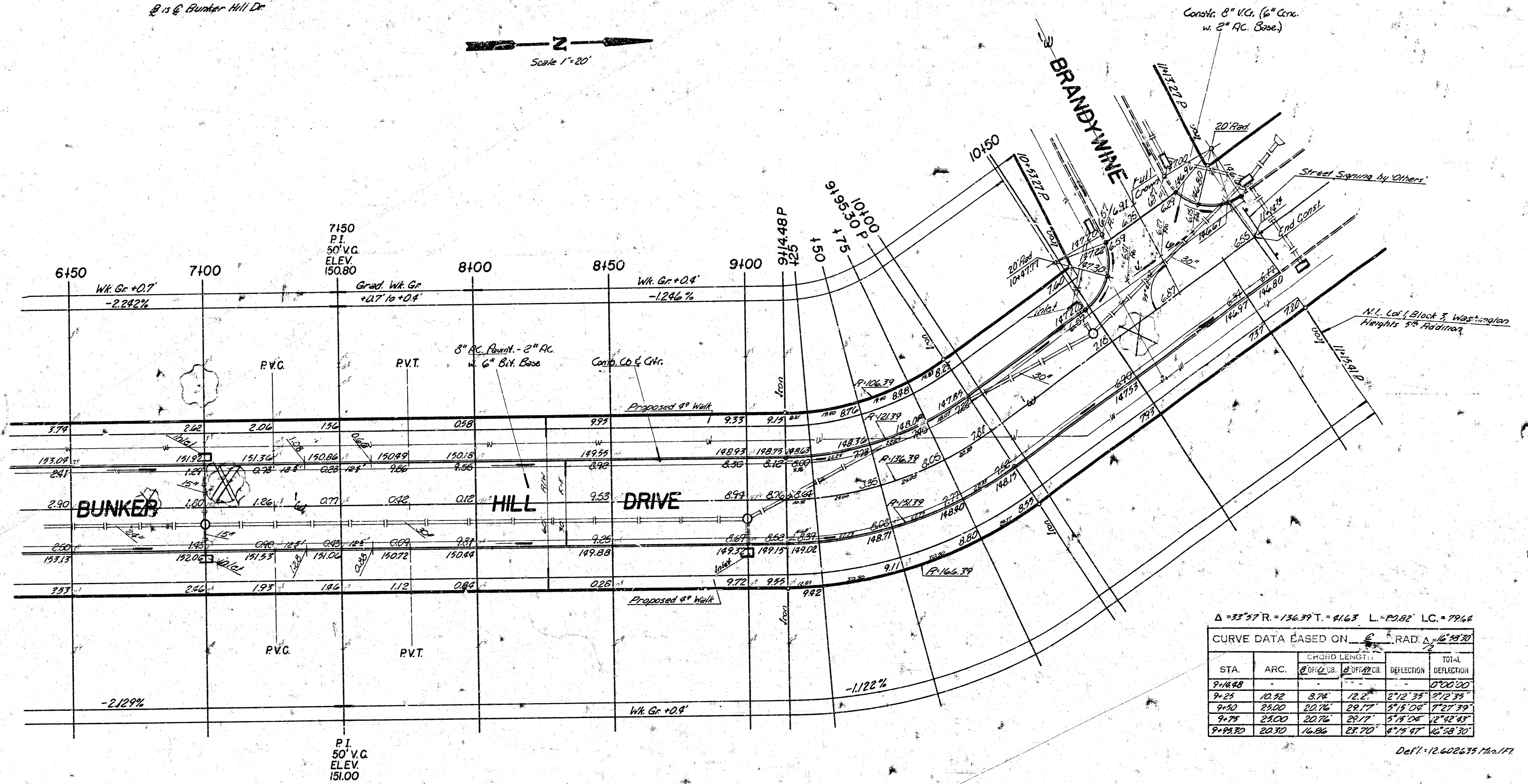
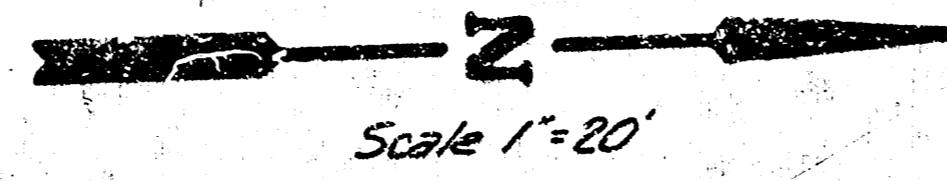
| | Property | City |
|----------------|---------------|---------------|
| Excavation | = 1855 C.Y. | = 1193 C.Y. |
| +10% | = 185.5 | = 119.3 |
| Total | = 2040.5 C.Y. | = 1312.3 C.Y. |
| Compacted Fill | = 365.2 C.Y. | = 74 C.Y. |
| +10% | = 36.5 | = 7.4 |
| Total | = 401.7 C.Y. | = 81.4 C.Y. |
| Manipulation | = 3607.8 C.Y. | = 173.8 C.Y. |

NOTE TO FIELD ENGINEER & CONTRACTOR
 Grace parking and clear right-of-way for proposed sidewalk. Compact fill in sidewalk area TO BE CONSTRUCTED BY "OTHERS".

SUB-GRADE
 TYPE OF SUB-GRADE TREATMENT SHALL BE DETERMINED BY THE FIELD ENGINEER. SUB-GRADE TREATMENT MAY CONSIST OF LIME TREATMENT, CEMENT TREATMENT, SUB-GRADE MODIFICATION, OR ANY COMBINATION OF THESE.

BUNKER HILL DRIVE
 N. L. 31st. St. So. - N. L. Lot 1, Blk. 3, Washington Heights 5th Addition
 DAKS575026

B.M. 166.90 R.R. Spk. Sa Face P.P. 70' West of
 W.L. Bunker Hill Dr. & 34' N. of & 31' S. of
 @ 15 @ Bunker Hill Dr.



$\Delta = 33^{\circ}57' R = 136.39' T = 91.63' L = 102.82' LC = 79.64'$

CURVE DATA BASED ON $\Delta = 33^{\circ}57'$ RAD $\Delta = 16^{\circ}19'30''$

| STA. | ARC. | CHORD LENGTH | | DEFLECTION | TOTAL DEFLECTION |
|----------|-------|--------------|--------|------------|------------------|
| | | OFF CB. | ON CB. | | |
| 9+14.48 | - | - | - | - | 0°00'00" |
| 9+25 | 10.92 | 3.74 | 12.2 | 2°12'35" | 2°12'35" |
| 9+30 | 23.00 | 20.76 | 29.17 | 5°15'04" | 7°27'39" |
| 9+75 | 23.00 | 20.76 | 29.17 | 5°15'04" | 12°42'43" |
| 9+114.30 | 20.30 | 16.86 | 23.70 | 4°15'47" | 16°58'30" |

Def'l = 12.602639 Min. 1/71

BUNKER HILL DRIVE
 DAKS575026