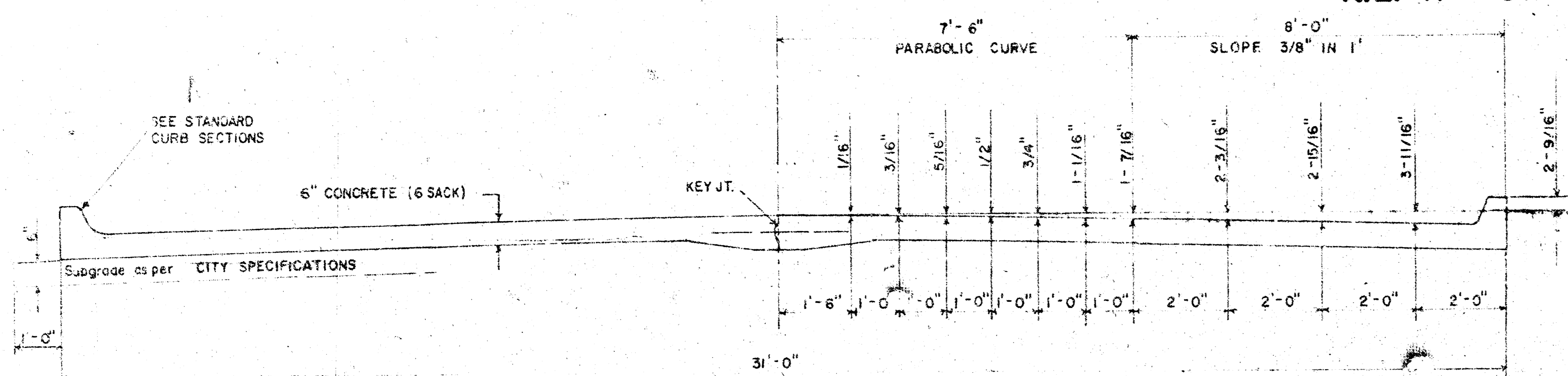
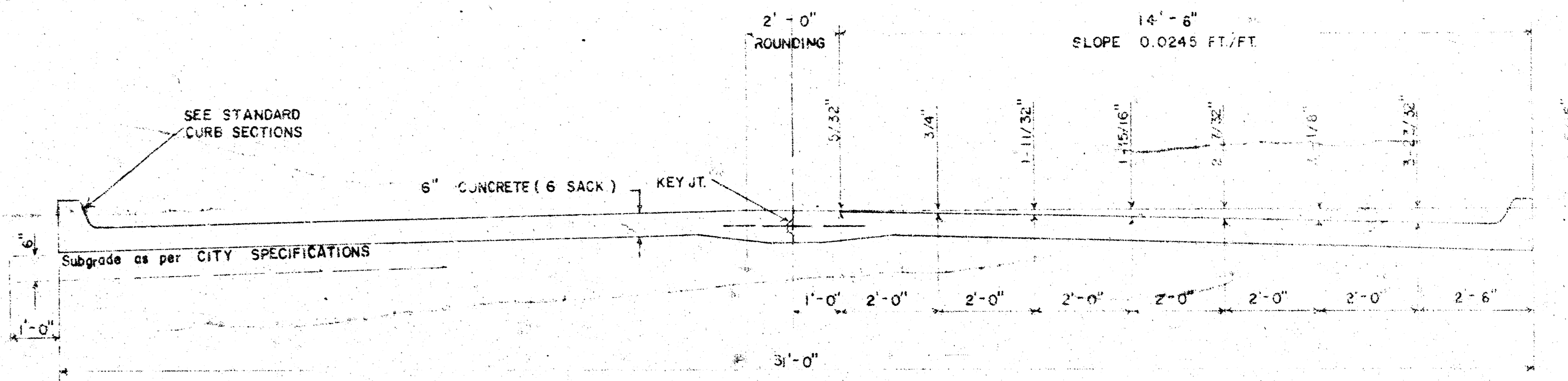


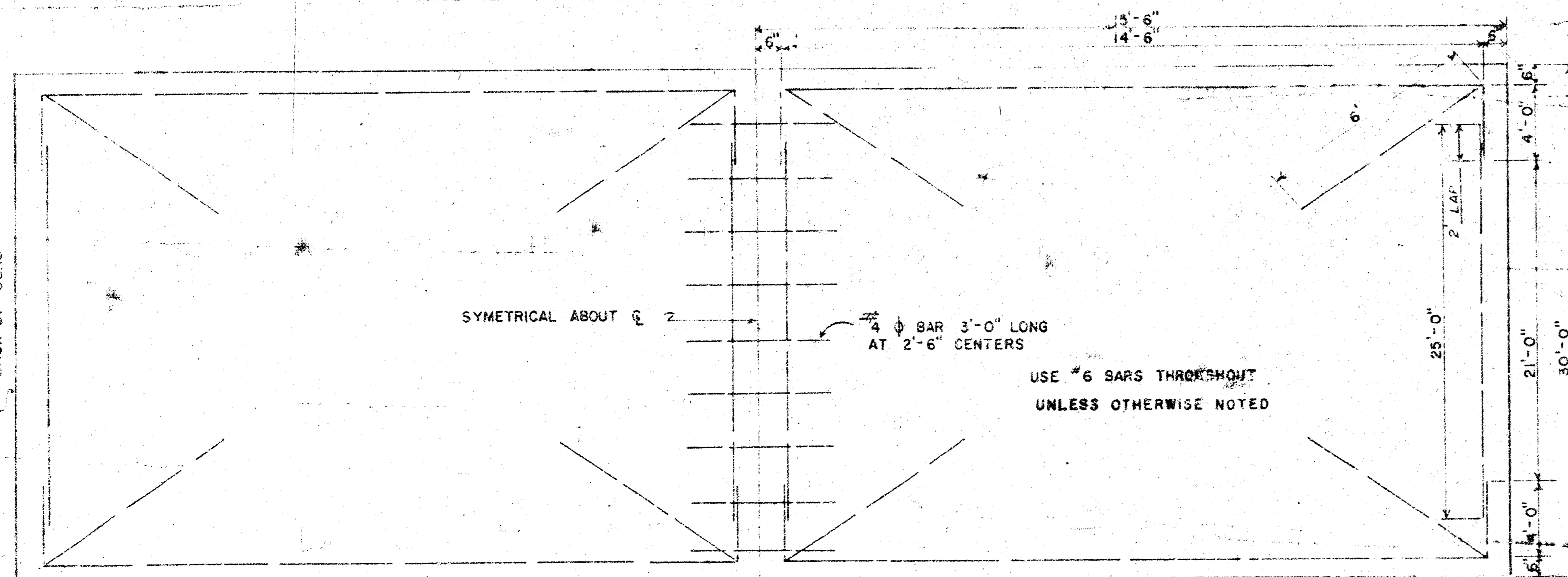
TYPICAL SECTIONS OF 30' CONCRETE PAVEMENT
 CLAYTON STREET
 N.L. 17th ST. to the S.L. 18th ST.



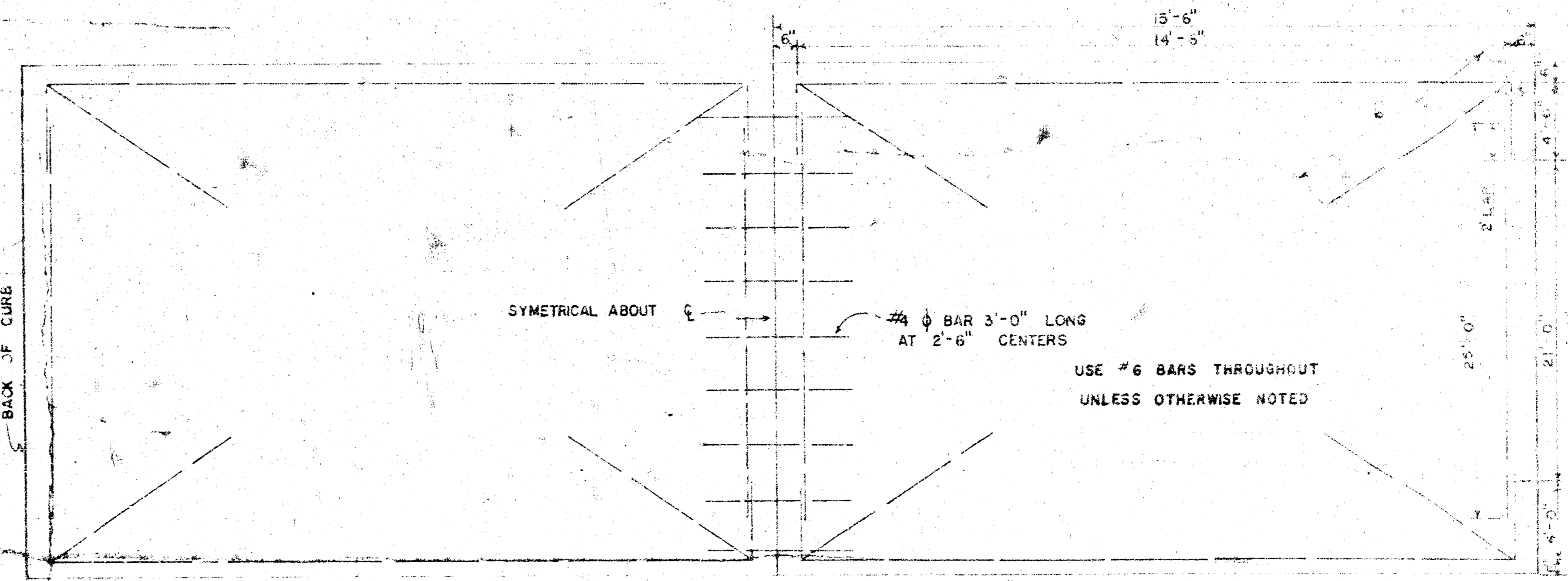
TYPICAL SECTION WITH STANDARD CROWN



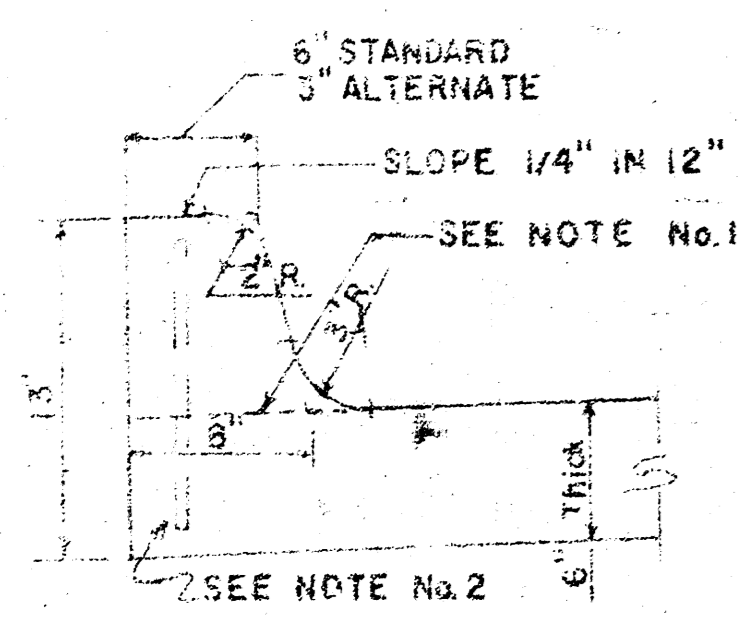
TYPICAL SECTION WITH ALTERNATE CROWN



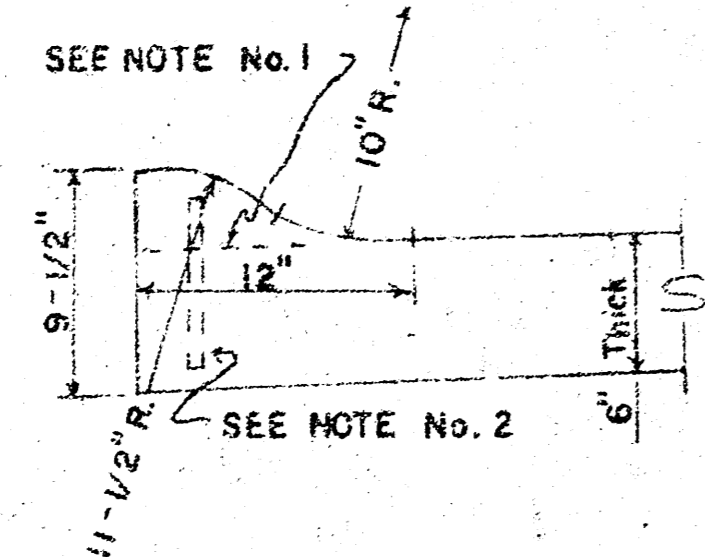
STEEL PATTERN



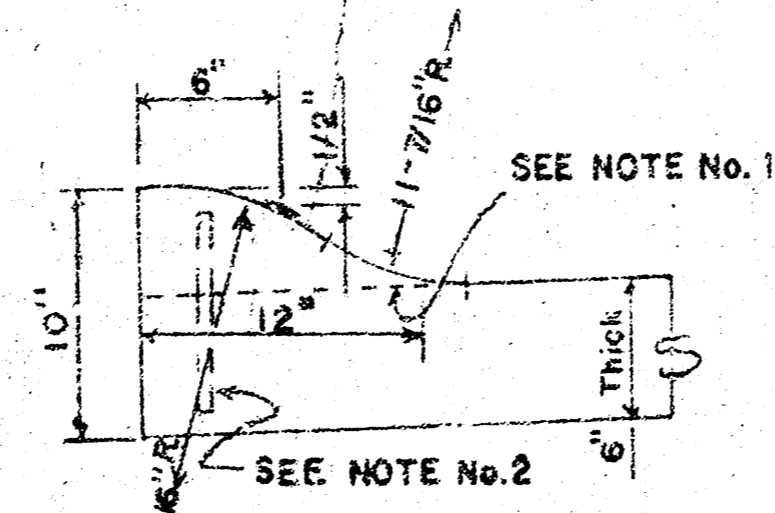
STEEL PATTERN



INTEGRAL CURB

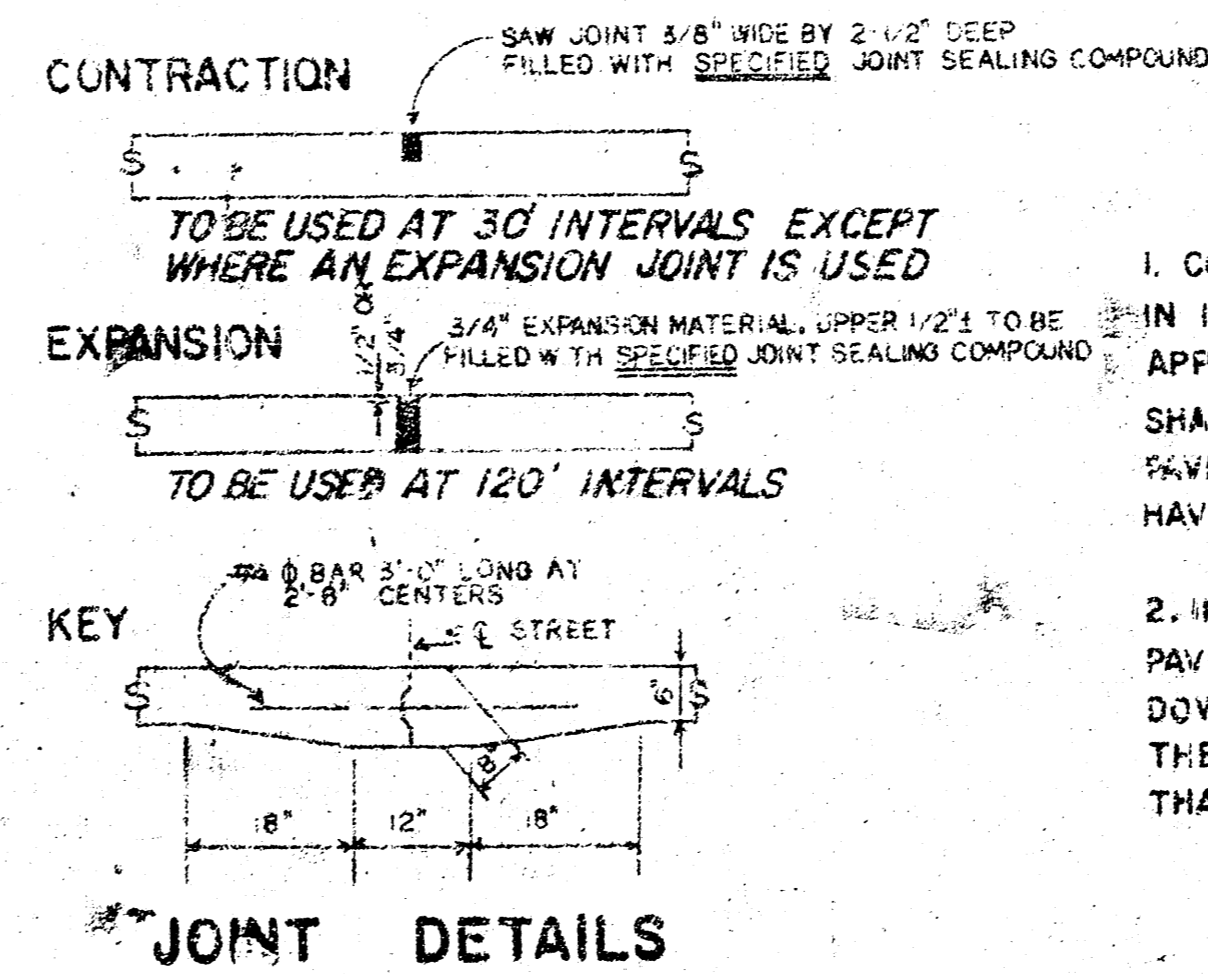


ROLL-TYPE



ALTERNATE ROLL-TYPE

STANDARD SECTIONS OF



GENERAL NOTES

- CONTRACTION JOINTS MAY BE CONSTRUCTED IN INTEGRAL CURB BY SAWING WITH AN APPROVED CONCRETE SAW. THE SAW CUT SHALL EXTEND THROUGH THE CURB TO THE PAVEMENT. SAWED CONTRACTION JOINTS SHALL HAVE A MAXIMUM SPACING OF 10'.
- INTEGRAL CURB SHALL BE TIED TO THE PAVEMENT BASE WITH SHORT DEFORMED DOVEL BARS SPACED AT 2'-6" INTERVALS. THESE DOVEL BARS SHALL NOT BE LESS THAN 1/2" OR MORE THAN 3/4" IN DIAMETER.



CITY OF WICHITA, KANSAS

Department of Public Works Engineering Division

R.W. Linn

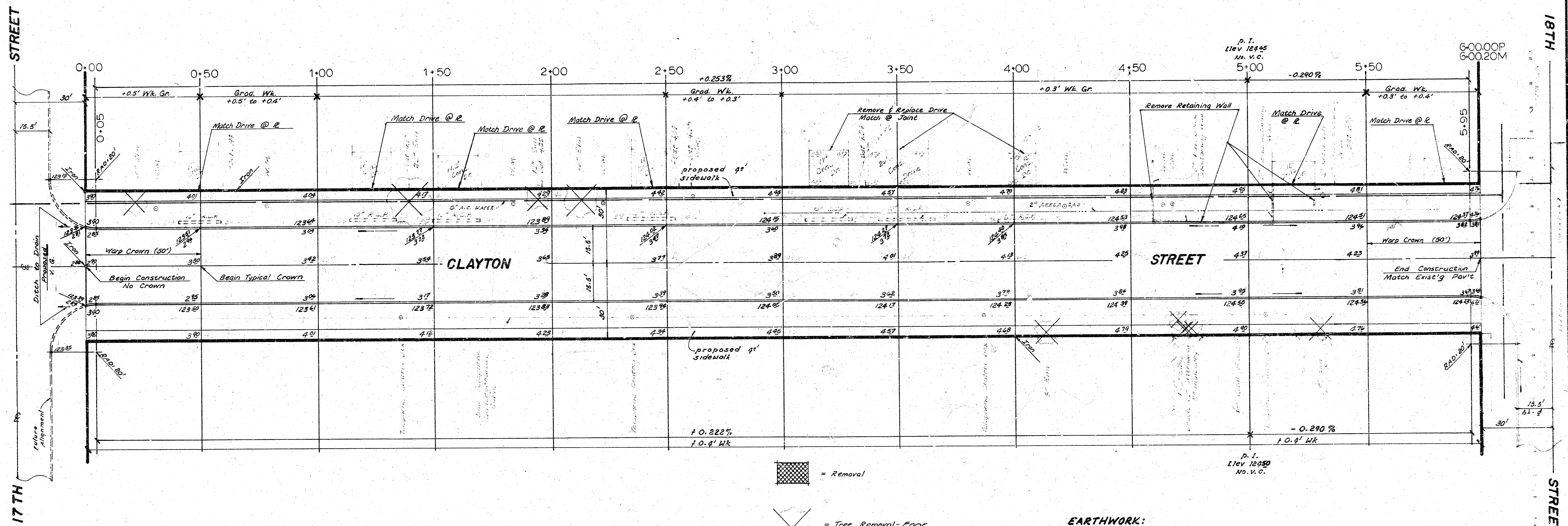
City Engineer

Date:

Project No. DAK-559019

B.M. 125.15 CITY bench mark disc, S.E. COR SHERIDAN and 17TH STREET
32' E. - 23.5' So. of both.

SCALE 1" = 20'



Survey: 10-10-50
Plan: 10-10-50
Exc: 10-10-50
Checked: 10-10-50

= Removal

= Tree Removal - Engr. to use discretion in removal of trees.

SUB-GRADE
TYPE OF SUB-GRADE TREATMENT SHALL BE DETERMINED BY THE FIELD ENGINEER. SUB-GRADE TREATMENT MAY CONSIST OF LIME TREATMENT, CEMENT TREATMENT, SUB-GRADE MODIFICATION, OR ANY COMBINATION OF THESE.

NOTE TO FIELD ENGINEER & CONTRACTOR
Gravel parking and clear right-of-way for proposed sidewalk. Compact fill in sidewalk area. TO BE CONSTRUCTED BY "OTHERS".

EARTHWORK:

Excavation	766.9 C.Y.
+10%	76.7 C.Y.
Total	843.6 C.Y.
Comp. Fill	24.2 C.Y.
+10%	2.6 C.Y.
Total	26.8 C.Y.
Manipulation = 2200 S.Y.	

CLAYTON STREET
N.L. 17TH ST. to the S.L. 18TH ST.
CITY of WICHITA, KANSAS
(30'-C" Concrete & Curb)
R.W. LINN CITY ENGINEER
DATE: _____ Proj No. DAKS55901 2/2

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