

SAYLER ADDITION STREET IMPROVEMENTS EVERGREEN LANE

PROJECT NO. 472-81831-001

INDEX CODE 761106

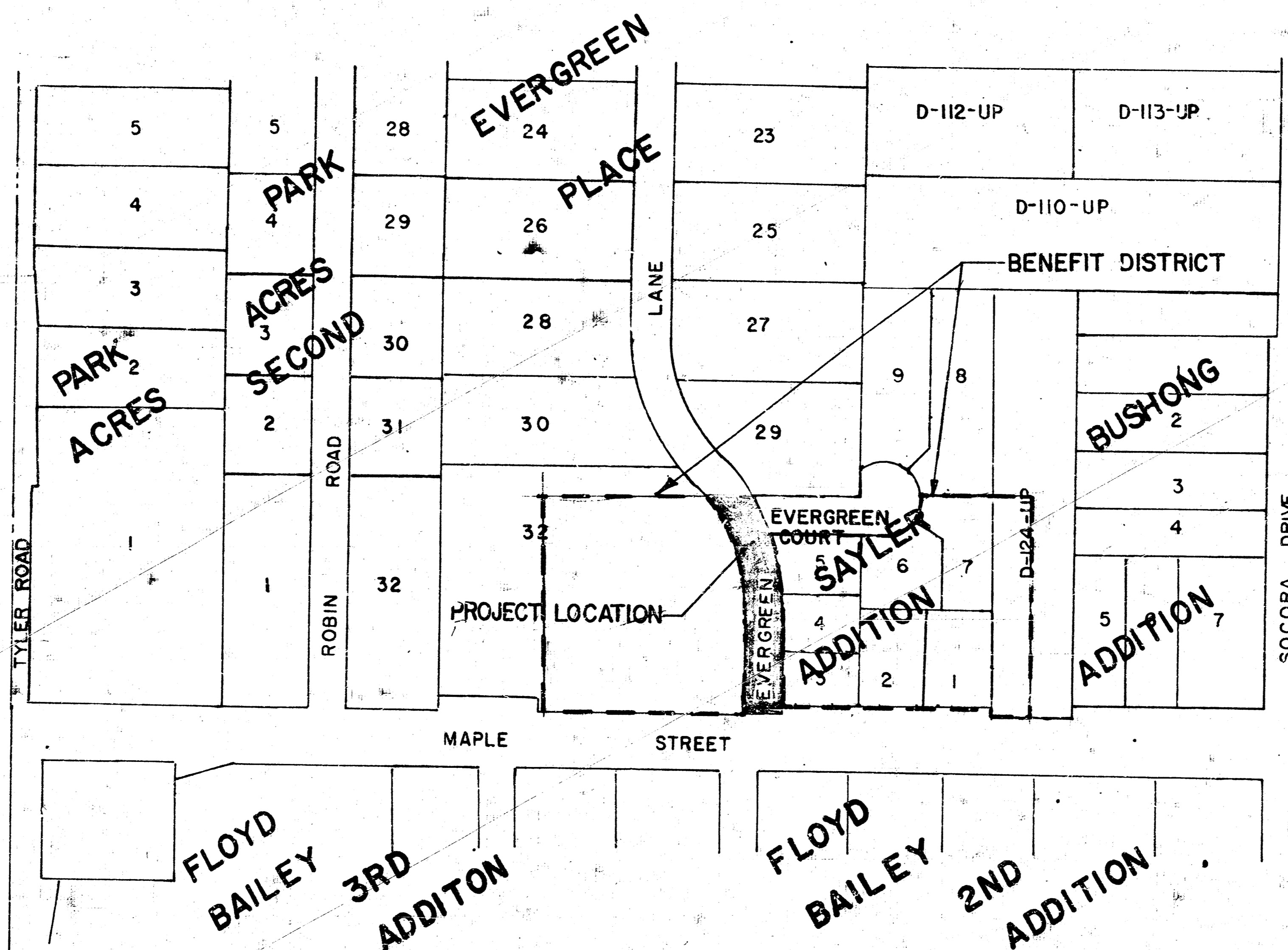
CITY OF WICHITA, KANSAS
MICHAEL E. LINDEBAK, CITY ENGINEER

APRIL, 1988

DRAWING INDEX

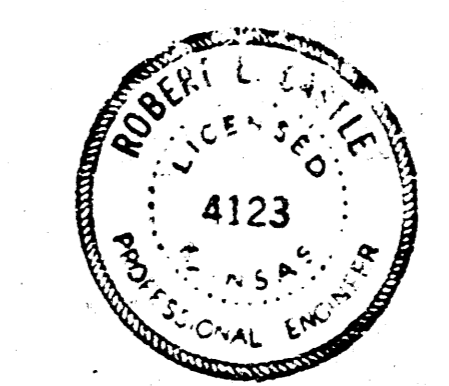
SHEET	TITLE
1	MAP AND BENEFIT DISTRICT
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4	PLAN EVERGREEN LANE
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- GENERAL NOTES
- CONTRACTOR WILL BE REQUIRED TO PROVIDE A MINIMUM ADVANCE NOTICE OF TWENTY-FOUR (24) HOURS TO UTILITY COMPANIES PRIOR TO STARTING ANY EXCAVATION AS FOLLOWS:
KANSAS ONE-CALL 687-2470
THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF AN EMERGENCY:
CABLEVISION 262-4270 OR 263-2061
NPL GAS SERVICE COMPANY 263-7511
KANSAS GAS & ELECTRIC 264-1141
NEPLA GAS COMPANY 942-8350 OR 263-8161
KANSAS BELL TELEPHONE CO. 1-571-2611
CITY OF WICHITA WATER DEPARTMENT 268-4908
CITY OF WICHITA SEWER MAINTENANCE 268-4071
 - UTILITY SERVICE LINES, T-PIES, VALVE BOXES, METERS, AND MISC. ARE TO BE ADJUSTED AS NECESSARY BY OTHERS PRIOR TO OR DURING CONSTRUCTION UNLESS THE PLANS SPECIFICALLY CALL FOR THEIR ADJUSTMENT BY THE CONTRACTOR. EXISTING UTILITIES AND THEIR LOCATION AS SHOWN ON THE PLANS, REPRESENT THE BEST INFORMATION AVAILABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORDS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED. THE CONTRACTOR WILL BE REQUIRED TO WORK AROUND EXISTING UTILITIES WITHIN THE RIGHT-OF-WAY WHICH DO NOT COMPLECT WITH PROPOSED CONSTRUCTION.
 - THE CONTRACTOR WILL BE PERMITTED TO BID ONLY ONE OF THE ALTERNATE TYPES OF SUBGRADE TREATMENT. THE TYPE BID BY THE SUCCESSFUL BIDDER WILL BE THE TYPE OF SUBGRADE TREATMENT USED TO CONSTRUCT THE PROJECT.
 - NOT MORE THAN ONE 20' DRIVE ENTRANCE SHALL BE CONSTRUCTED ON THIS PROJECT.
 - LIMITS OF EARTHWORK SHALL MATCH EXISTING GROUND ELEVATIONS AT THE RIGHT-OF-WAY LINE UNLESS OTHERWISE NOTED ON THE PLANS WITH A NEW FINISHED GRADE ELEVATION. WHEN A NEW FINISHED GRADE ELEVATION IS SHOWN, THE EARTHWORK SHALL EXTEND 0.5' FOOT BEYOND THE RIGHT-OF-WAY LINE AND THEN SLOPED UP OR DOWN USING PERMISSIBLE SLOPES TO MATCH THE EXISTING GROUND SURFACE.
 - THE CITY OF WICHITA'S SEWER MAINTENANCE DIVISION OF THE DEPARTMENT OF WATER AND WATER POLLUTION CONTROL HAS TELEVISED SEWERS WITHIN THE LIMITS OF THE PROJECT AND HAVE FOUND NO DEFECTS REQUIRING REPAIR. THE DIVISION SHALL BE NOTIFIED AND AFFORDED THE OPPORTUNITY TO RE-TELEVISION SEWER LINES AFTER SUBGRADE WORK HAS BEEN COMPLETED AND PRIOR TO PAVEMENT CONSTRUCTION TO DETERMINE IF SUCH SEWER LINES HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATIONS. DAMAGED SEWER LINES WILL BE REPAIRED BY THE CONTRACTOR, AS DIRECTED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE. TELEVISION LOGS ARE AVAILABLE FOR INSPECTION BY THE CONTRACTOR DURING NORMAL OFFICE HOURS AT THE SEWER MAINTENANCE DIVISION'S OFFICE AT CITY HALL.
 - THE CONTRACTOR SHALL NOTIFY PIPELINE COMPANIES AT LEAST 24 HOURS IN ADVANCE OF ANY WORK BEING PERFORMED ACROSS AND/OR ADJACENT TO PIPELINES.
 - TREES AND SHRUBS IN PUBLIC RIGHT-OF-WAY WHICH ARE IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE REMOVED BY THE CONTRACTOR WITH THE ENGINEER'S APPROVAL. TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH PROPOSED NEW CONSTRUCTION SHALL BE SAVED AND PROTECTED FROM DAMAGE.
 - RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES AND EXCESS EXCAVATION WHICH IS TO BE WASTED SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE AND SITE LOCATION. LOGS THAT, IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR OR A LICENSED PROFESSIONAL ENGINEER IN ACCORDANCE WITH STATE LAWS.
 - CONTRACTOR SHALL GIVE PROPERTY OWNERS ADJUTING THIS PROJECT, WHOSE YARDS WILL BE LOWER THAN THE NEW FINISHED GRADE ELEVATIONS AT THE RIGHT-OF-WAY LINE, AN OPPORTUNITY TO UTILIZE EXCESS EXCAVATED MATERIAL FROM THE PROJECT TO REGRADE THEIR YARDS TO DRAIN TO THE NEW PAVEMENT. CONTRACTOR WILL BE REQUIRED TO DUMP AND SPREAD THE EXCESS MATERIAL AS REQUESTED BY THE SPECIFYING AGENY REQUESTED BY THE PROPERTY OWNER. THE CONTRACTOR SHALL ASCERTAIN THAT A DIRT ORDER FORM HAS BEEN PROPERLY EXCUTED BY THE PROPERTY OWNER BEFORE ANY SUCH EXCESS MATERIAL IS DELIVERED TO SUCH PROPERTIES.
 - ALL ELEVATIONS SHOWN ARE CITY OF WICHITA DATUM (USGS-1187.4 - CITY DATUM).



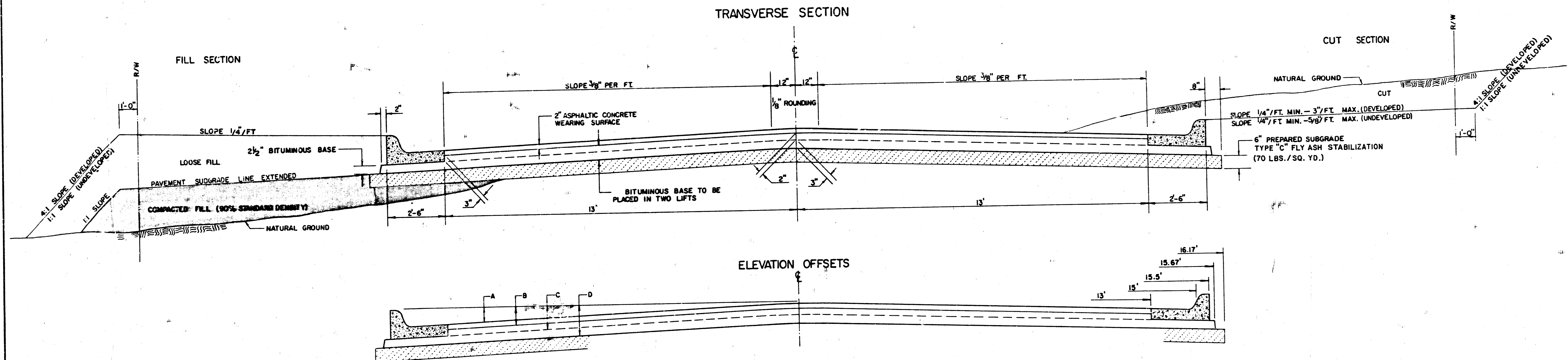
SCALE 1" = 150'

*Booked Per Plans
2/18/93, LKP
D-200*



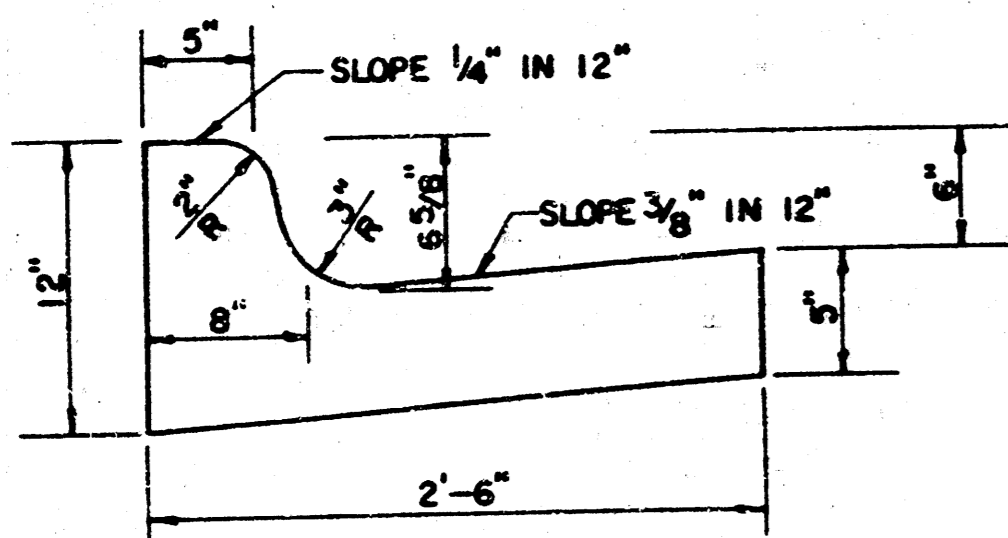
EARL M. SAYLER STREET IMPROVEMENTS	
MAP AND BENEFIT DISTRICT SAYLER ADDITION	
PROJ. NO. 472-81831-001	
CASTLE & ASSOCIATES ENGINEERS WICHITA, KANSAS	DSR LJK DR. LJK CH. RMB
DATE APRIL 1988	DWG FILE NO. 8740-D-80,101
APP. PLC	SHEET NO 1 OF 9

TYPICAL 3' PAVEMENT DETAILS

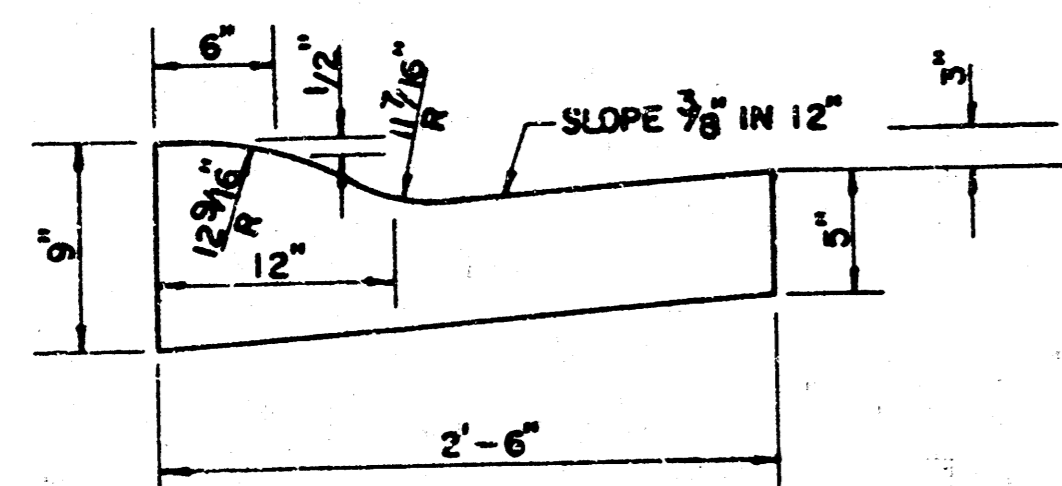


	DISTANCE FROM CENTERLINE (LT. & RT.)											
	0'	2'	4'	6'	7.5'	10'	12'	13'	15'	15.5'	15.67'	16.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	0.10	0.14	0.21	0.27	0.32	0.39	0.46	0.49	—	—	—	—
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	0.27	0.31	0.38	0.44	0.49	0.56	0.63	0.66	—	—	—	—
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	0.44	0.49	0.57	0.64	0.70	0.79	0.87	0.90	0.98	1.00	1.00	—
D: TOP OF CURBS TO TOP OF SUBGRADE	0.69	0.73	0.80	0.87	0.93	1.01	1.08	1.12	1.19	1.21	1.21	1.23

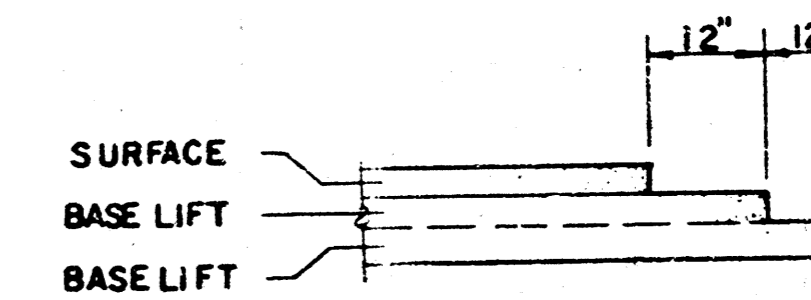
COMBINED CURB & GUTTER



ROLL TYPE COMBINED CURB & GUTTER



TRANSVERSE CONSTRUCTION JOINTS



TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).

GENERAL NOTES

- 1) THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 7" ASPHALTIC CONCRETE (5" BITUMINOUS BASE).
- 2) THE BITUMINOUS BASE UNDER AND BEHIND THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 2 1/2" BITUMINOUS BASE.
- 3) A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.
- 4) BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
- 5) CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.
- 6) CONTRACTOR TO BID ONLY ONE SUBGRADE TREATMENT ALTERNATE WHEN ALTERNATES ARE PROVIDED IN THE PROPOSAL AND CONTRACT. THE ALTERNATE CHOSEN BY THE SUCCESSFUL BIDDER SHALL BE USED IN CONSTRUCTING THIS PROJECT.

8740-D-80,102

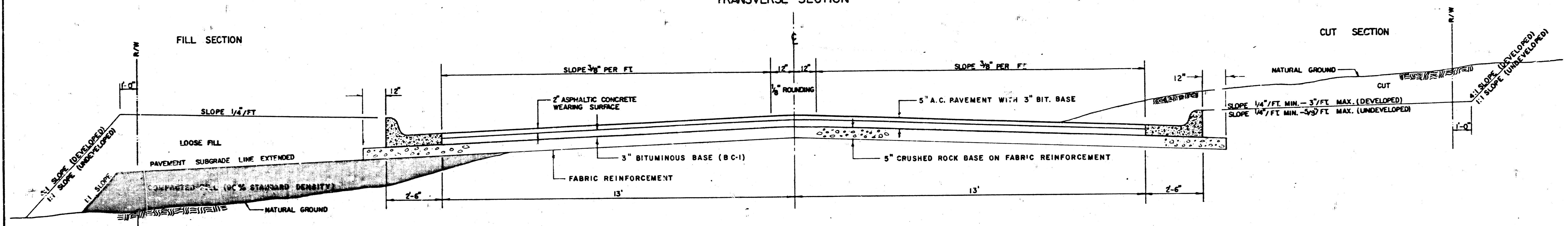
SHEET 2 OF 9

7 INCH RESIDENTIAL ASPHALTIC CONCRETE PAVEMENT WITH 5 INCH BITUMINOUS BASE CITY OF WICHITA, KANSAS

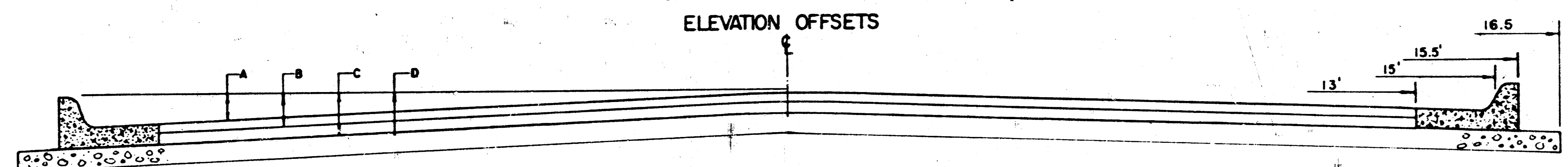
PROJECT NUMBER 472-81831-001

TYPICAL 31' PAVEMENT DETAILS

TRANSVERSE SECTION

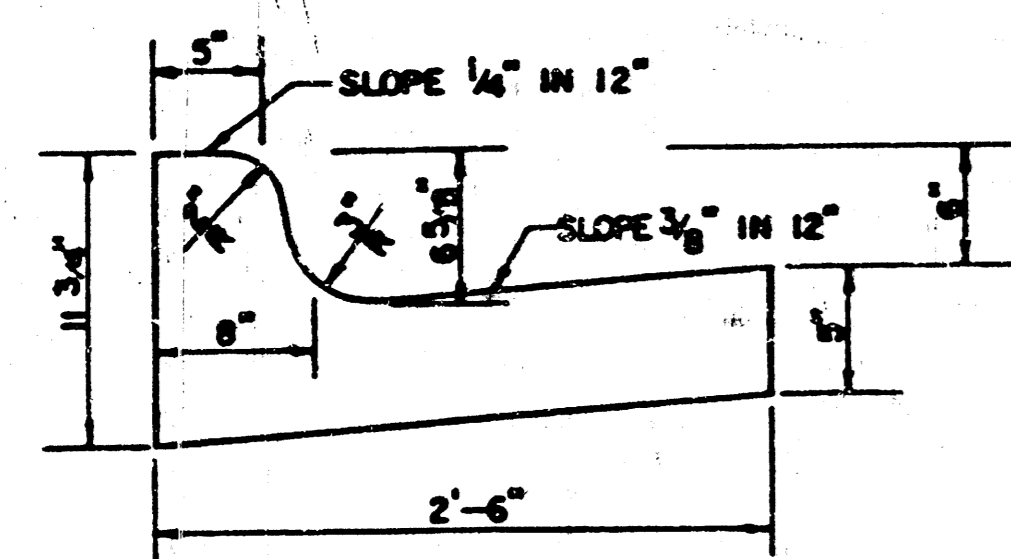


ELEVATION OFFSETS

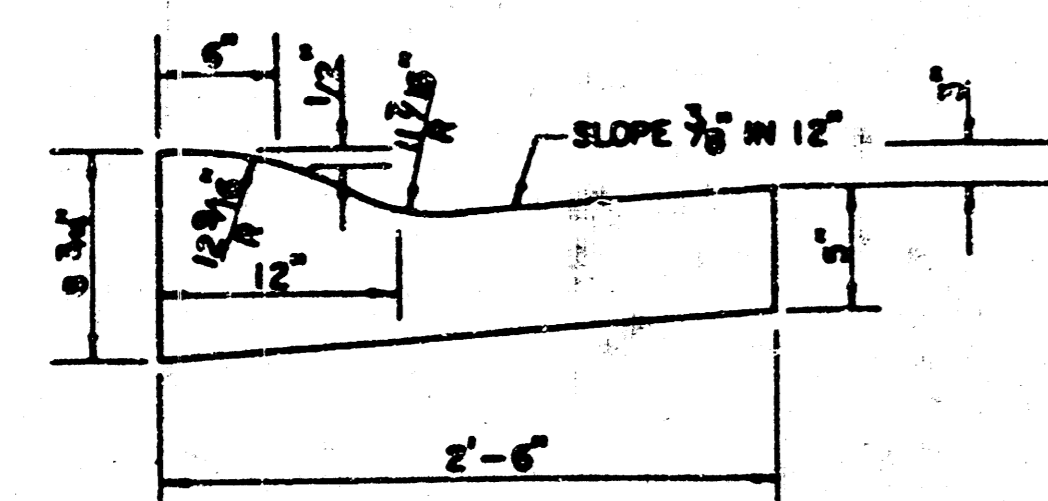


	DISTANCE FROM CENTERLINE (LT. & RT.)											
	0'	2'	4'	6'	7.5'	10'	12'	13'	15'	15.5'	15.67'	16.17'
A: TOP OF CURBS TO TOP OF SURFACE LIFT	.10	.14	.21	.27	.32	.39	.46	.49	---	---	---	---
B: TOP OF CURBS TO TOP OF UPPER BASE LIFT	.27	.31	.37	.44	.48	.56	.62	.65	---	---	---	---
C: TOP OF CURBS TO TOP OF LOWER BASE LIFT	.52	.56	.62	.69	.73	.81	.87	.90	.97	.98	.99	---
D: TOP OF CURBS TO TOP OF SUBGRADE												

COMBINED CURB & GUTTER



ROLL TYPE COMBINED CURB & GUTTER



GENERAL NOTES

FABRIC BASE REINFORCEMENT SHALL BE B x 1100 GEOGRID AS MANUFACTURED BY TENSAR CORPORATION OR APPROVED EQUAL. FABRIC BASE REINFORCEMENT SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. CRUSHED ROCK SHALL BE UNIFORMLY GRADED FROM 1-1/2" MAXIMUM SIZE TO NOT MORE THAN 10% PASSING A NO. 200 SIEVE. ROCK QUALITY SHALL BE THE SAME AS SPECIFIED FOR COARSE AGGREGATE FOR CONCRETE MIXES.

ROCK BASE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF ASPHALT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.

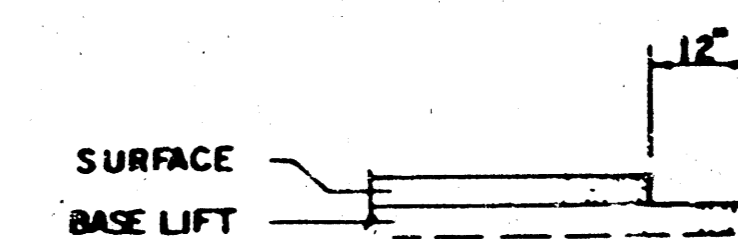
A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.05 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM DISTANCE OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFTS AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.

THE ASPHALTIC CONCRETE PAVEMENT BETWEEN THE COMBINED CURB AND GUTTER SHALL BE PAID AS SQUARE YARDS OF 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

TRANSVERSE CONSTRUCTION JOINTS



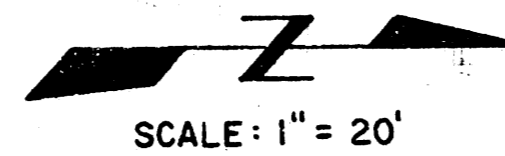
TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE BASE PAVEMENTS AT LOCATIONS WHERE PAVEMENT JOINTS EXISTING FLEXIBLE BASE PAVEMENT AS SHOWN BY THE DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR SQUARE YARDS 5" ASPHALTIC CONCRETE (3" BITUMINOUS BASE).

3740-D-80,110 PROJECT NUMBER SHEET NO. 3 OF 9
472-81831-001

**5" RESIDENTIAL ASPHALTIC CONCRETE
PAVEMENT WITH CRUSHED ROCK BASE
ON FABRIC REINFORCEMENT**

BENCHMARK NO. 1 - RAILROAD SPIKE IN UTILITY POLE ON THE NORTH SIDE OF MAPLE AT SOCCORA. ELEV. - 142.99 CITY DATUM

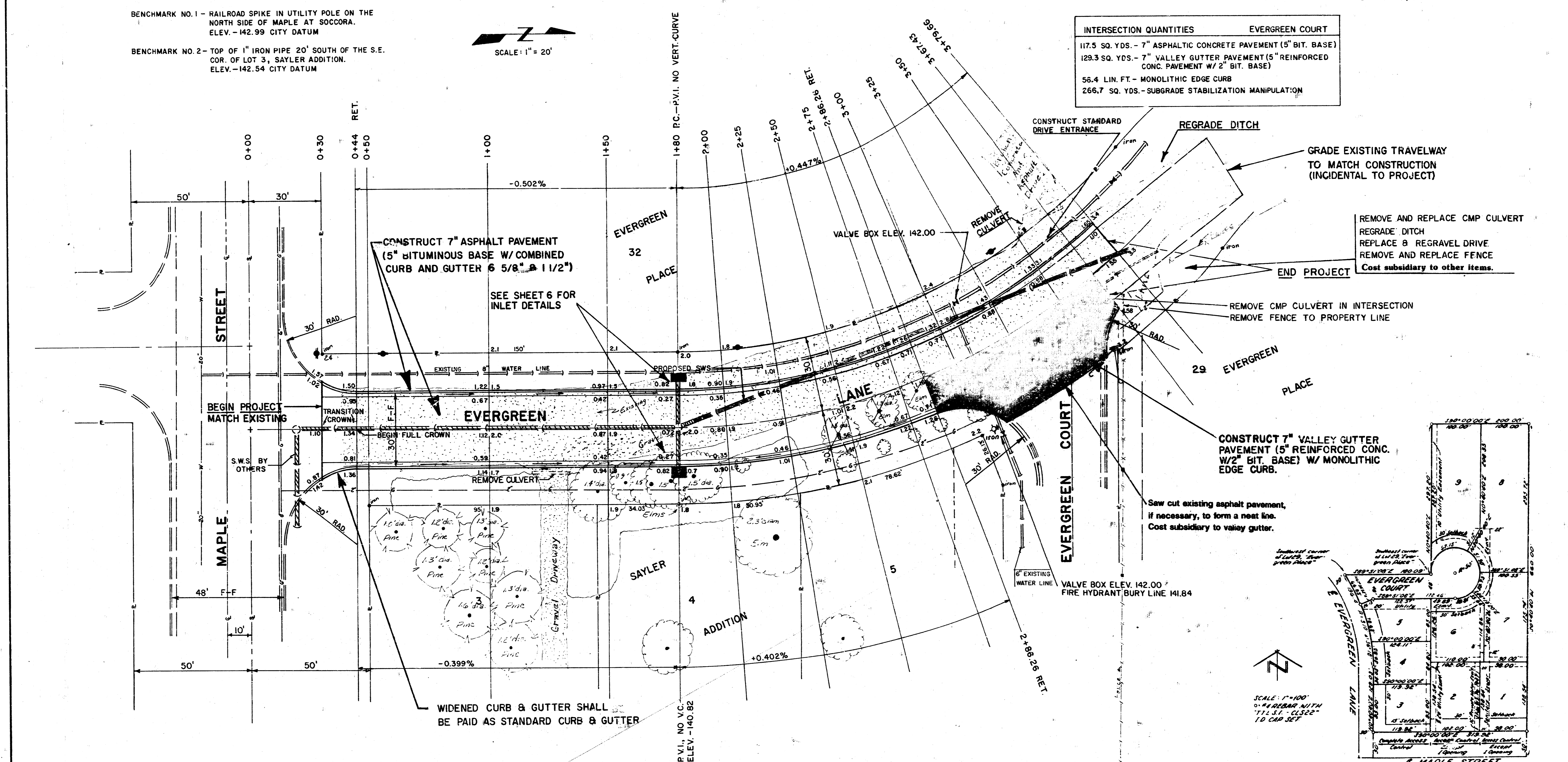
BENCHMARK NO. 2 - TOP OF 1" IRON PIPE 20' SOUTH OF THE S.E. COR. OF LOT 3, SAYLER ADDITION. ELEV. - 142.54 CITY DATUM



NOTE:
SEE SHEET 5 OF 9, INCIDENTAL DRAINAGE, FOR PROPOSED STORM SEWER AND INLETS.

NOTE:
2" GAS LINE IS EXPOSED AT APPROX. STA. 3+57, ± 25' RIGHT.

INTERSECTION QUANTITIES		EVERGREEN COURT
117.5 SQ. YDS.	-	7" ASPHALTIC CONCRETE PAVEMENT (5" BIT. BASE)
129.3 SQ. YDS.	-	7" VALLEY GUTTER PAVEMENT (5" REINFORCED CONC. PAVEMENT W/ 2" BIT. BASE)
56.4 LIN. FT.	-	MONOLITHIC EDGE CURB
266.7 SQ. YDS.	-	SUBGRADE STABILIZATION MANIPULATION



NOTE:
CONTRACTOR SHALL COORDINATE WORK WITH THE MAPLE ST. PAVING CONTRACTOR TO MINIMIZE INCONVENIENCE TO PROPERTY OWNERS.

$\Delta = 40^{\circ} 51' 22''$ $R = 280.00'$ $T = 123.86'$ $L = 199.66'$ $L.C. = 193.86'$ $DEFL./FT. = 0.1623''$

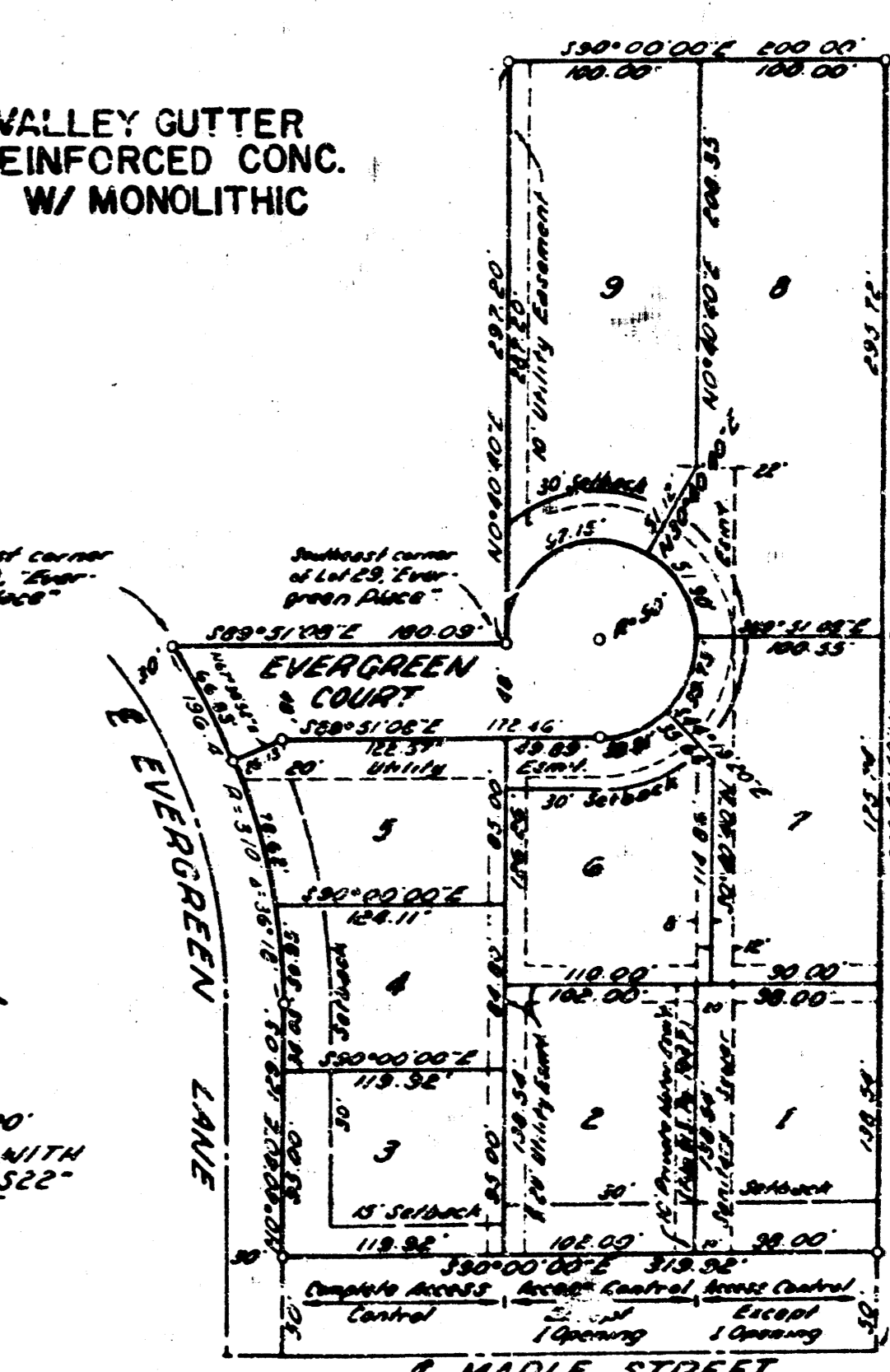
CURVE DATA BASED ON ϕ RADIUS $\Delta/2 = 20^{\circ} 25' 41''$

STA	ARC	CHORD LENGTH		DEFL.	TOTAL DEFL
		7.5' OFF LT CURB	7.5' OFF RT. CURB		
1+80	—	—	—	—	0° 00' 00"
2+00	20.00'	18.35'	21.64'	2° 02' 46"	2° 02' 46"
2+25	25.00'	22.94'	27.04'	2° 33' 29"	4° 36' 15"
2+50	25.00'	22.94'	27.04'	2° 33' 29"	7° 09' 44"
2+75	25.00'	22.94'	27.04'	2° 33' 29"	9° 43' 13"
2+86.26	11.26'	12.18'	14.87'	1° 09' 07"	10° 52' 20"
3+00	13.74'	22.94'	14.87'	1° 24' 20"	12° 16' 40"
3+25	25.00'	22.94'	27.04'	2° 33' 29"	14° 50' 09"
3+50	25.00'	22.94'	27.04'	2° 33' 29"	17° 23' 38"
3+67.43	17.43'	—	18.86'	1° 47' 00"	19° 10' 38"
3+79.66	29.66'	27.21'	—	1° 15' 03"	20° 25' 41"

NOTE: OFFSETS ARE CALCULATED FROM BACK OF CURB.

EXCAVATION 784 CU. YDS.
MANIPULATION 1,3500-SQ. YDS.

The dirt work quantity is based on the full depth asphalt alternate. Any additional dirt work for the crushed rock base alternate shall be incidental to the crushed rock base.



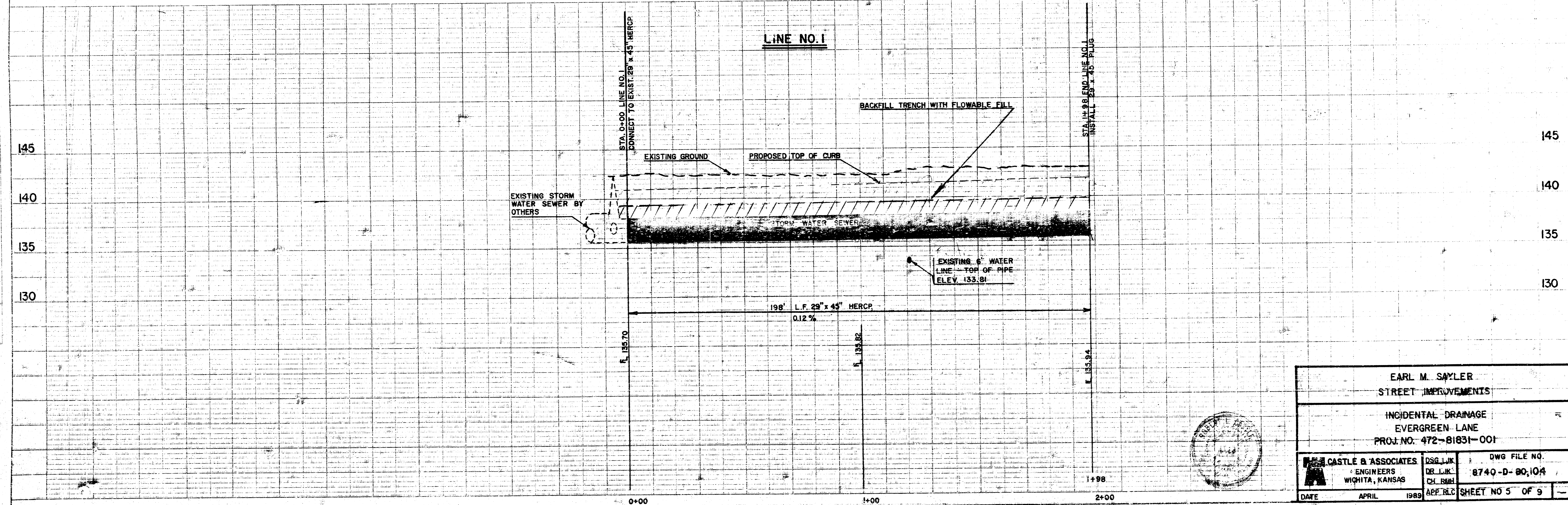
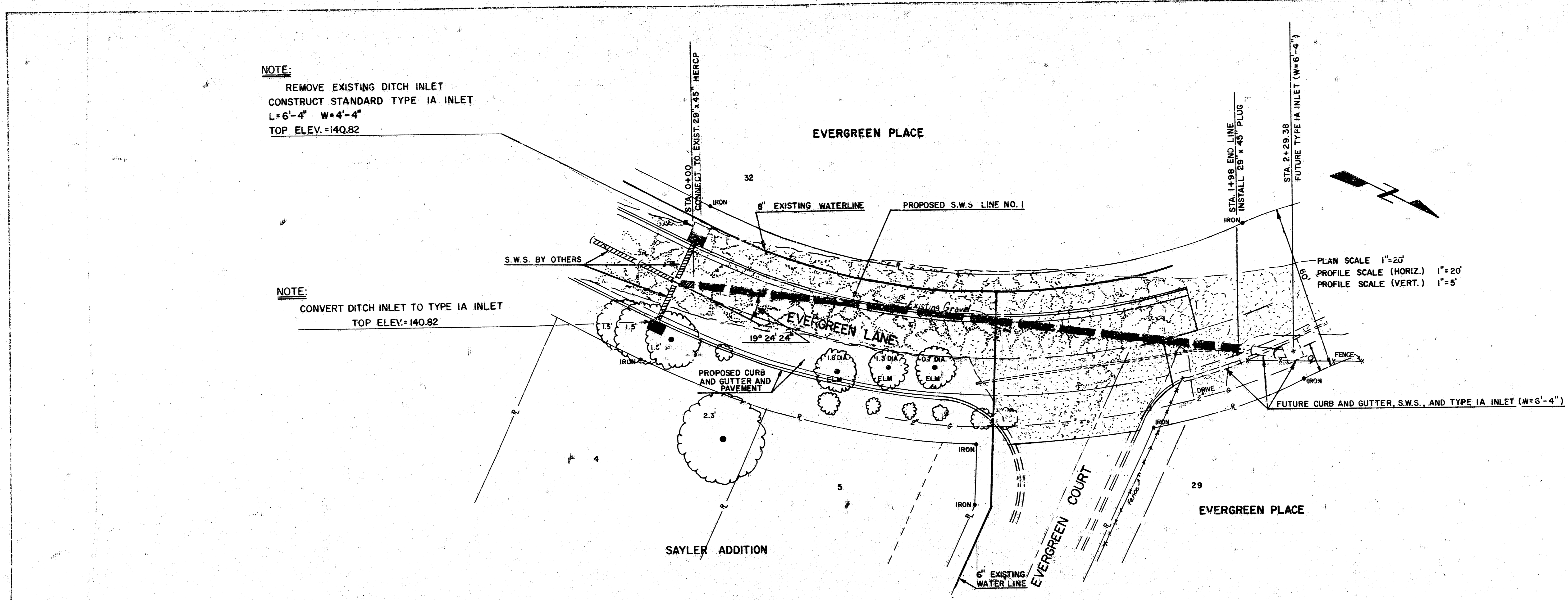
SAYLER ADDITION

EARL M. SAYLER STREET IMPROVEMENTS			
PLAN EVERGREEN LANE PROJ. NO. 472-81831-001			
CASTLE & ASSOCIATES ENGINEERS WICHITA, KANSAS	DSG LJK DR LJK CH RMH	DWG FILE NO. 8740-D-80,103	REV.
DATE APRIL 1988	APP RLC	SHEET NO. 4 OF 9	

NOTE:
 REMOVE EXISTING DITCH INLET
 CONSTRUCT STANDARD TYPE IA INLET
 L=6'-4" W=4'-4"
 TOP ELEV.=140.82

NOTE:
 CONVERT DITCH INLET TO TYPE IA INLET
 TOP ELEV.=140.82

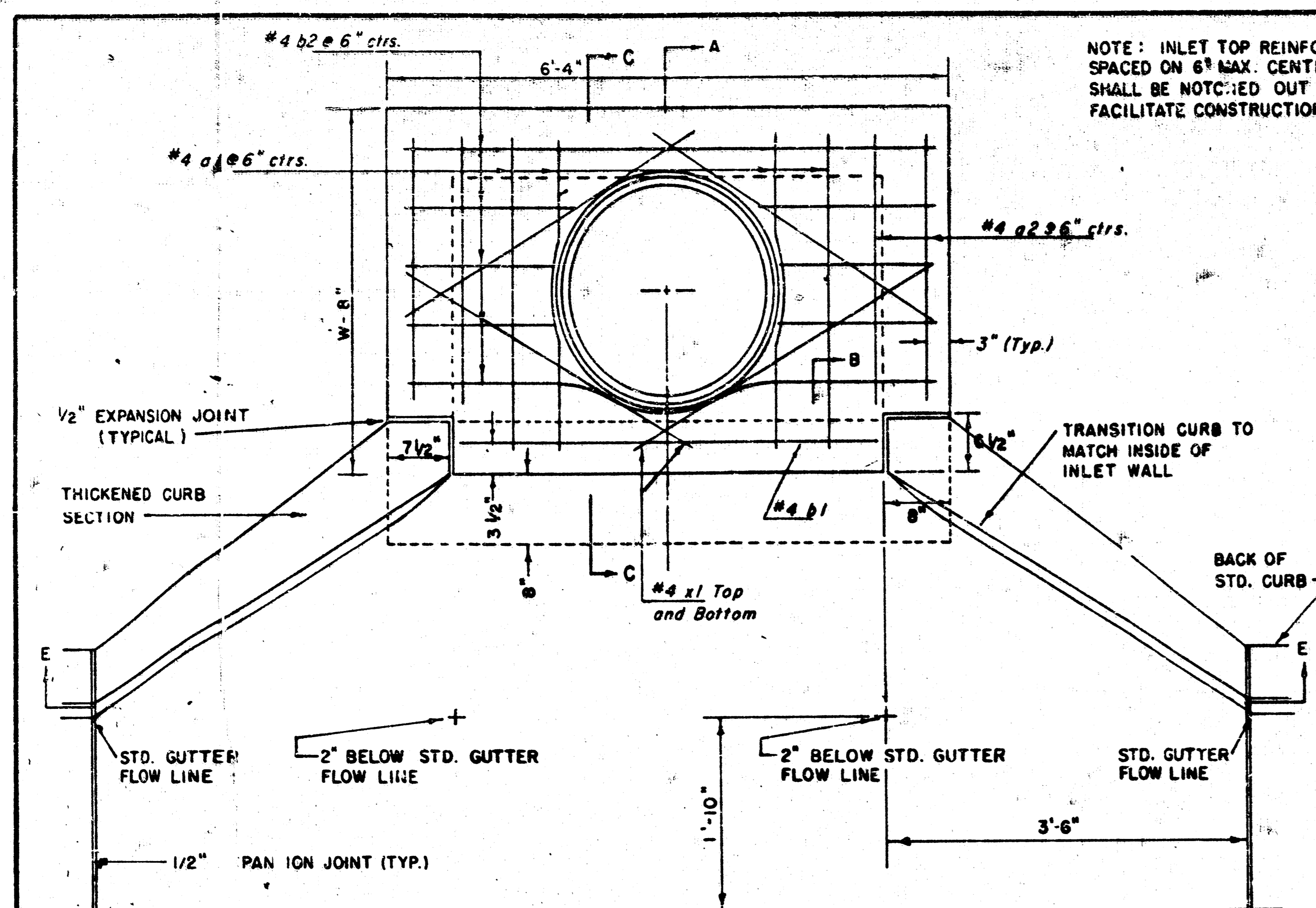
NOTE:
 BACKFILL IN STREET RIGHT-OF-WAY
 SHALL BE COMPACTED ACCORDING TO THE
 CITY OF WICHITA SPECIFICATIONS.



PLAN	DATE	BY	CHKD.

PROFILE	DATE	BY	CHKD.

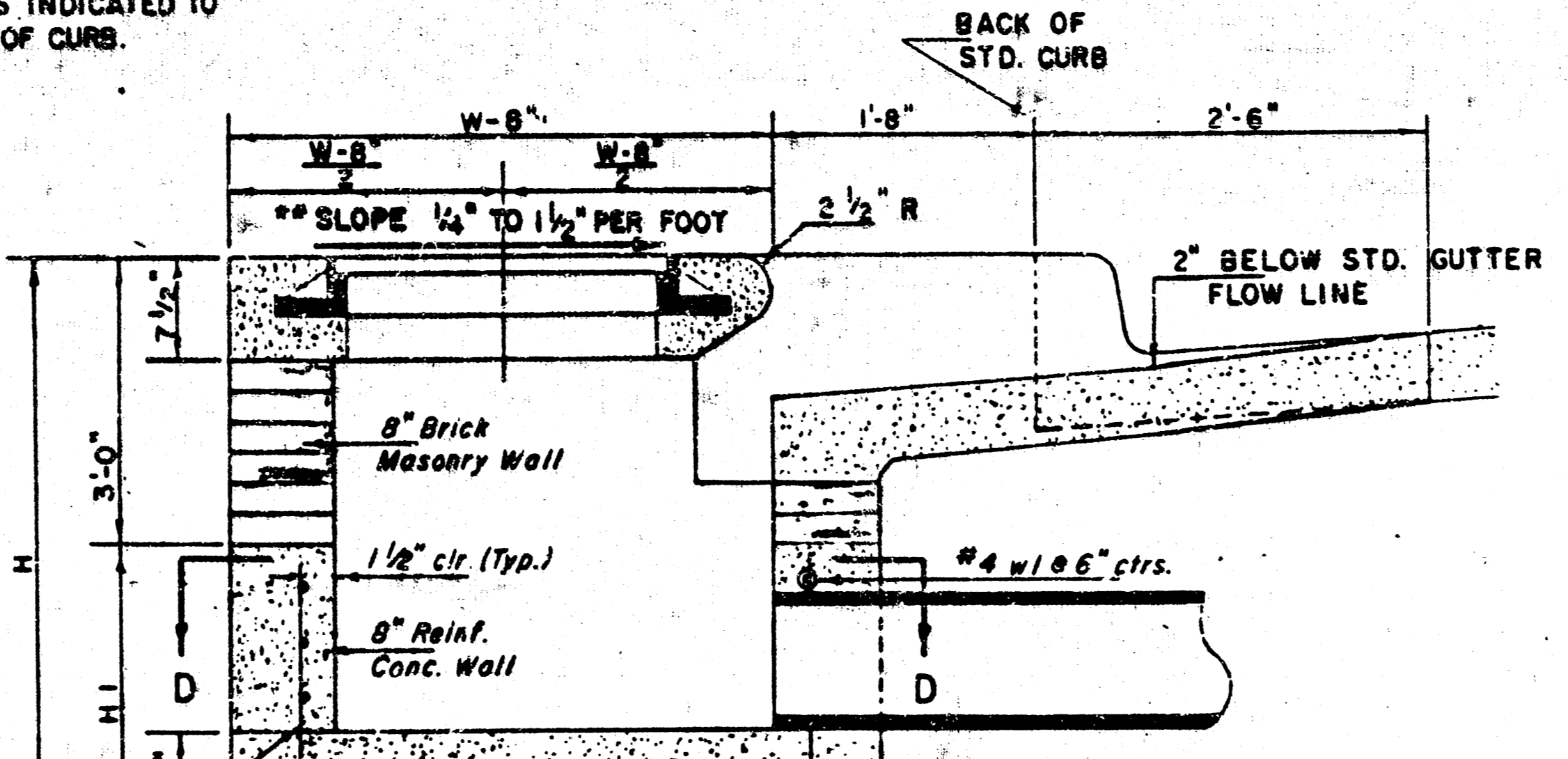
EARL M. SAYLER STREET IMPROVEMENTS	
INCIDENTAL DRAINAGE EVERGREEN LANE PROJ. NO. 472-81831-001	
CASTLE & ASSOCIATES ENGINEERS WICHITA, KANSAS	DRG. LJK CH. BMM APRIL 1989
DWG FILE NO. 8740-D-80,104	SHEET NO 5 OF 9



NOTE: EXPANSION JOINT ONLY IN CURB AREA WITH CONC. PAVEMENT.

PLAN

NOTE: INLET TOP REINFORCING SHALL BE SPACED ON 6" MAX. CENTERS. INLET LIDS SHALL BE NOTICED OUT AS INDICATED TO FACILITATE CONSTRUCTION OF CURB.



SECTION A-A

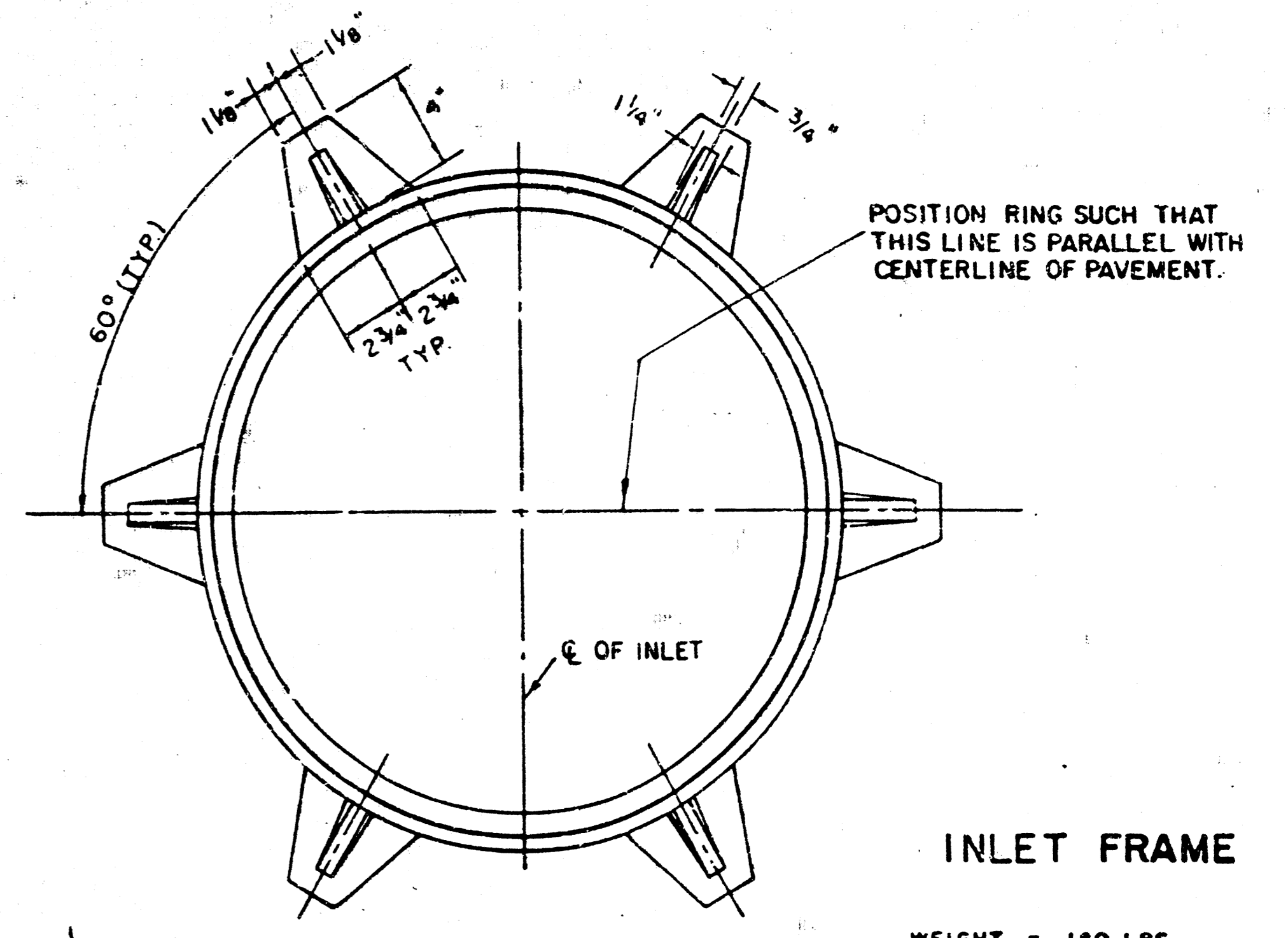
NOTE: CONCRETE TOPS TO BE INSTALLED ON THIN MORTAR CUSHION TO INSURE FULL SUPPORT ALONG BRICK WALLS. CONCRETE TOPS MAY BE CAST IN PLACE OR PRECAST. CONCRETE USED FOR INLET CONSTRUCTION SHALL BE CONCRETE PAVEMENT MIX WITH AIR ENTRAINMENT.

NOTE: CONTRACTOR SHALL HAVE THE OPTION OF CONSTRUCTING 8" BRICK MASONRY WALLS BETWEEN THE CONCRETE INLET BASE AND TOP ON THIS INLET WHEN W = 6'-4" AND H = 7'-0" OR LESS.

ADDITIONAL CURB AND GUTTER CONSTRUCTION NECESSARY TO CONNECT SET-BACK INLET TO PAVEMENT WILL BE PAID FOR AT THE UNIT PRICE BID FOR EACH INLET HOOKUP.

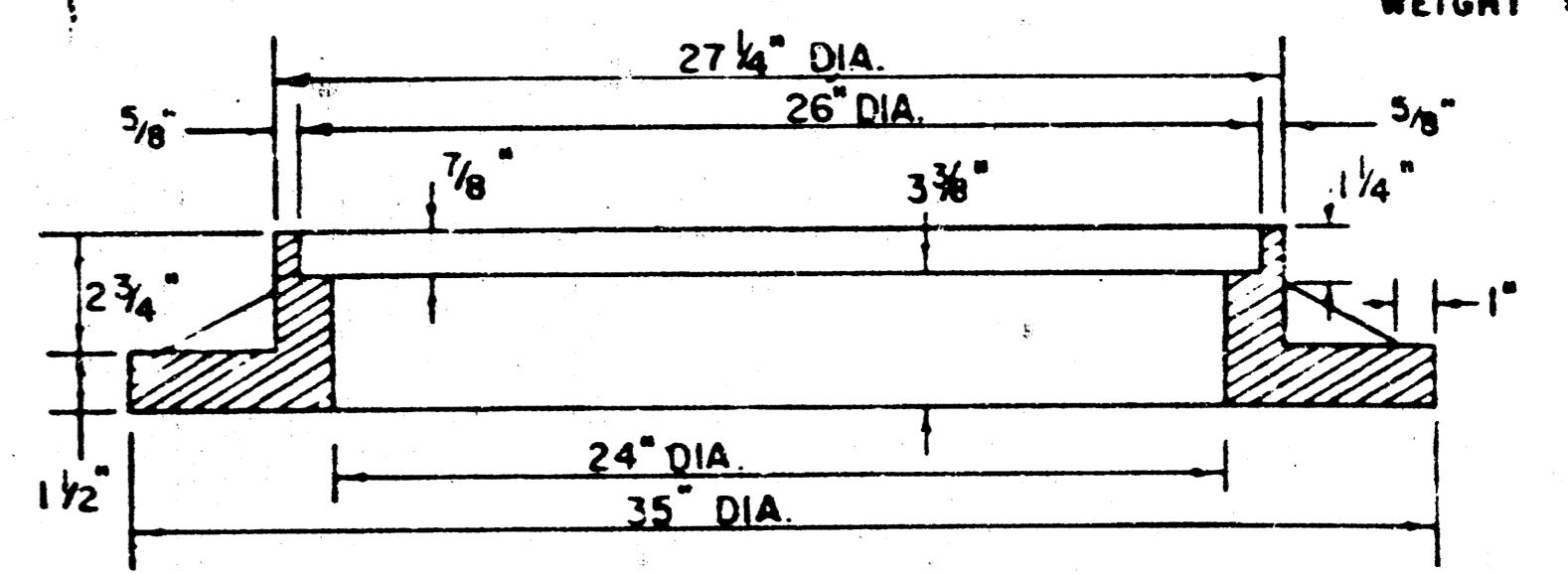
INLET INVERT SHALL BE SHAPED WITH 8 SACK SAND MIX CONCRETE TO CREATE FLOW CHANNELS AND TO INCREASE HYDRAULIC EFFICIENCY SUCH THAT THE INLET WILL BE SELF-CLEANING BETWEEN ALL INLET AND/OR OUTLET PIPES.

THE ENDS OF ALL PIPES INSTALLED IN INLETS SHALL BE CUT OFF FLUSH WITH THE INSIDE FACE OF THE INLET WALL.



INLET FRAME

WEIGHT = 180 LBS.



SEE CITY OF WICHITA STANDARD MANHOLE FRAME AND COVER DETAIL SHEET FOR COVER DETAILS TO BE USED WITH INLET FRAME.

PRECAST SLAB AND FLOOR REINFORCING									
Mark	Size	No.	Len.	No.	Length	No.	Length	No.	Length
a1	#4	6	6'-7"	6	8'-7"	6	10'-7"	6	12'-7"
a2	#4	4	6'-0"	4	8'-0"	4	10'-0"	4	12'-0"
a3	#4	13	4'-1"	13	5'-1"	13	6'-1"	13	7'-1"
b1	#4	1	4'-9"	1	4'-9"	1	4'-9"	1	4'-9"
b2	#4	23	6'-1"	29	6'-1"	35	6'-1"	41	6'-1"
x1	#4	4	3'-10"	8	4'-2"	8	4'-6"	8	4'-10"

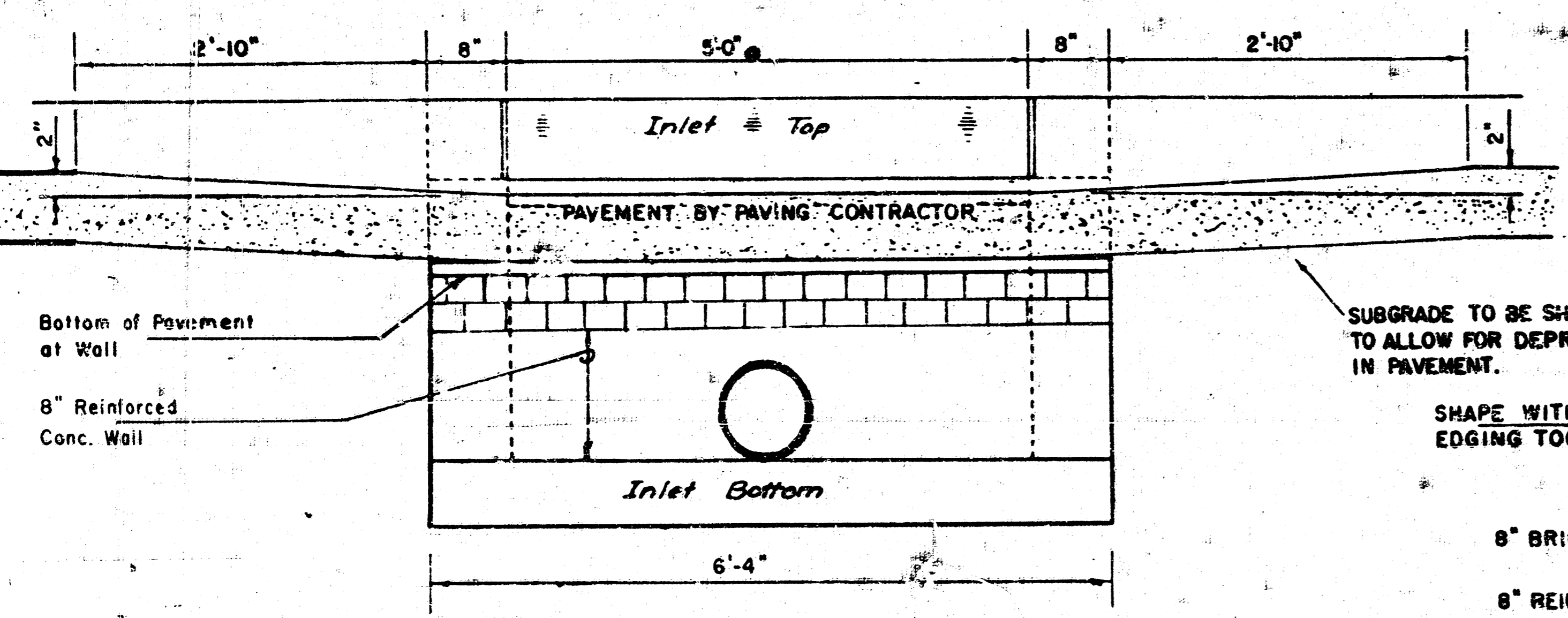
WALL REINFORCING									
Mark	Size	No.	Length	No.	Length	No.	Length	No.	Length
w1	#4	(1)	6'-1"	(1)	6'-1"	(1)	6'-1"	(1)	6'-1"
w2	#4	(1)	4'-1"	(1)	5'-1"	(1)	6'-1"	(1)	7'-1"
w3	#4	(2)	36"	(2)	40"	(2)	44"	(2)	48"

* Field bend or cut Reinforcing as required for clearance.
 ① 4(HI-12") (HI-12") Round down to nearest 0.5"
 ② HI-3"

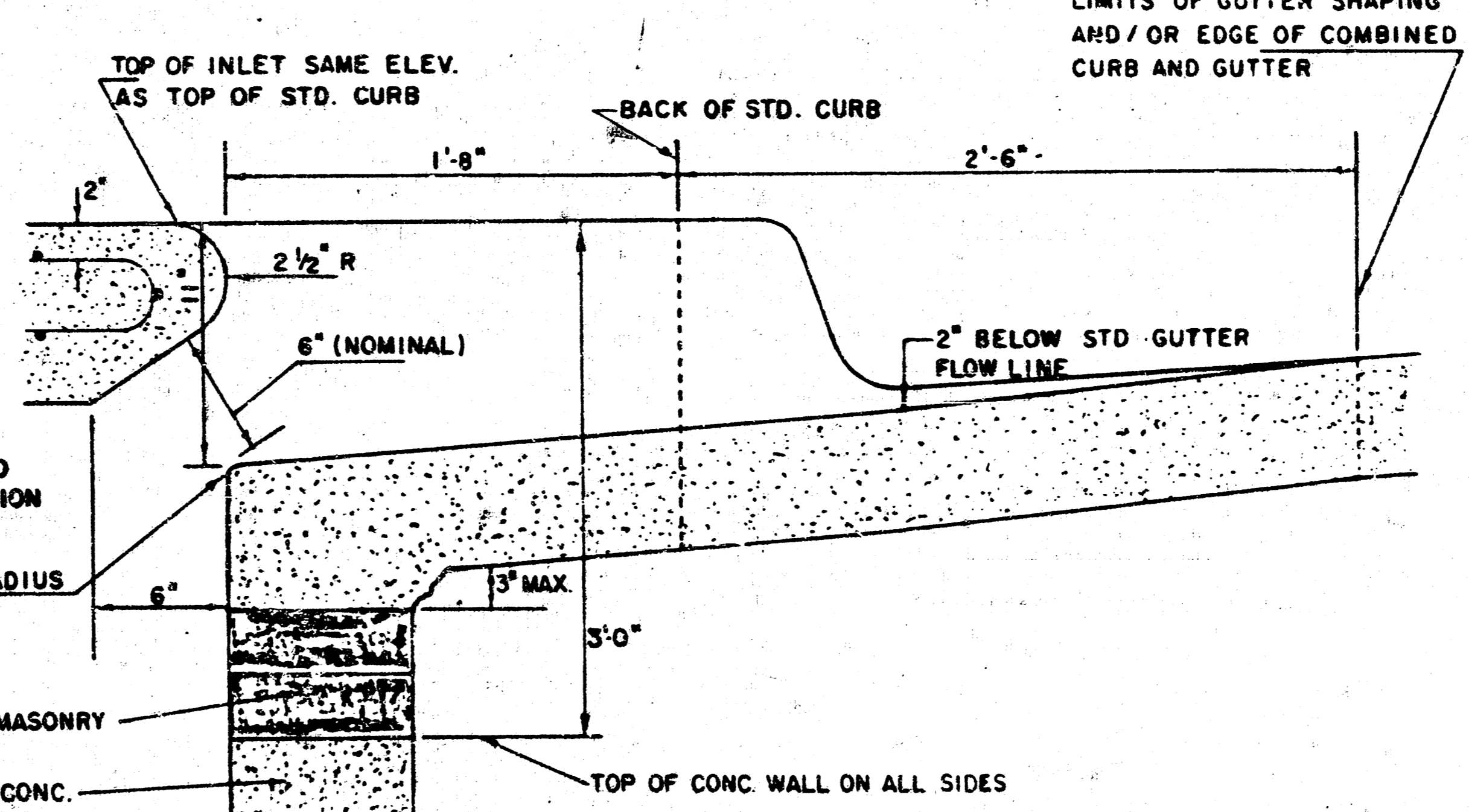
BENDING DIAGRAM

W	PRE-CAST TOP SIZE	PIPE SIZE	CU. YD. CONC.
4'-4"	3'6" x 6'4" x 7 1/2"	21" & SMALLER	0.38 ±
5'-4"	4'6" x 6'4" x 7 1/2"	24" & 30"	0.51 ±
6'-4"	5'6" x 6'4" x 7 1/2"	36" & 42"	0.64 ±
7'-4"	6'6" x 6'4" x 7 1/2"	48" & 54"	0.77 ±
8'-4"	7'6" x 6'4" x 7 1/2"	60" & 66"	0.90 ±

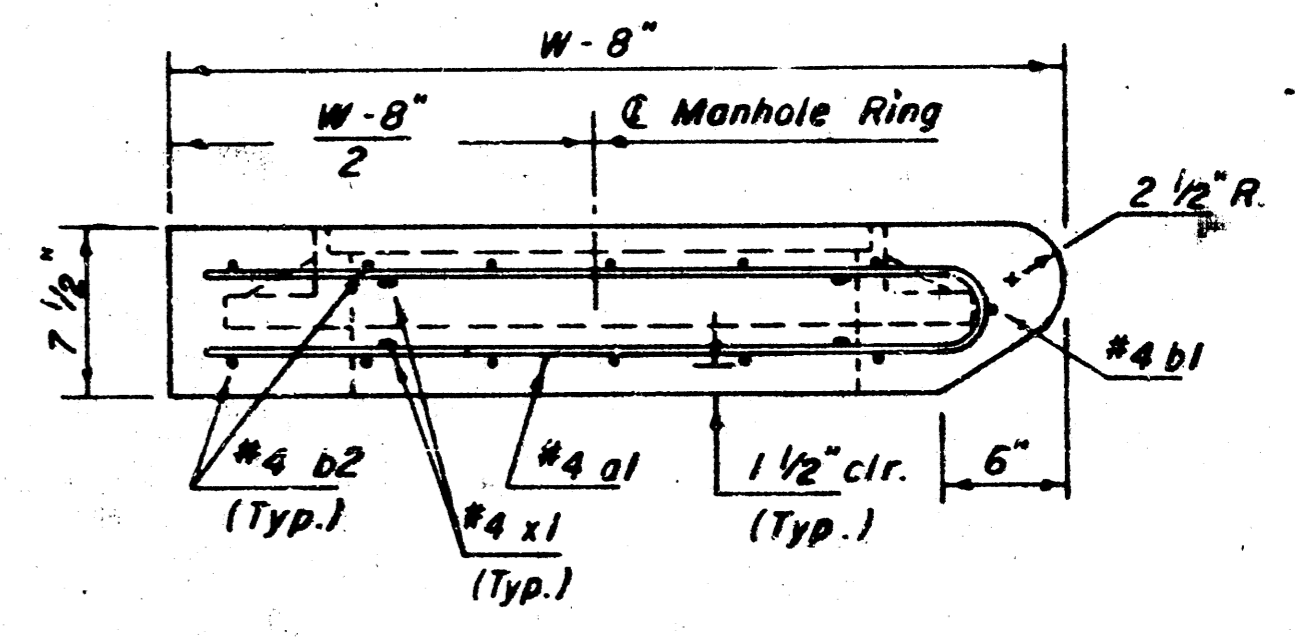
**NOTE: Slope of Inlet Tops to match Sidewalk or Parking Slopes within Limits indicated.



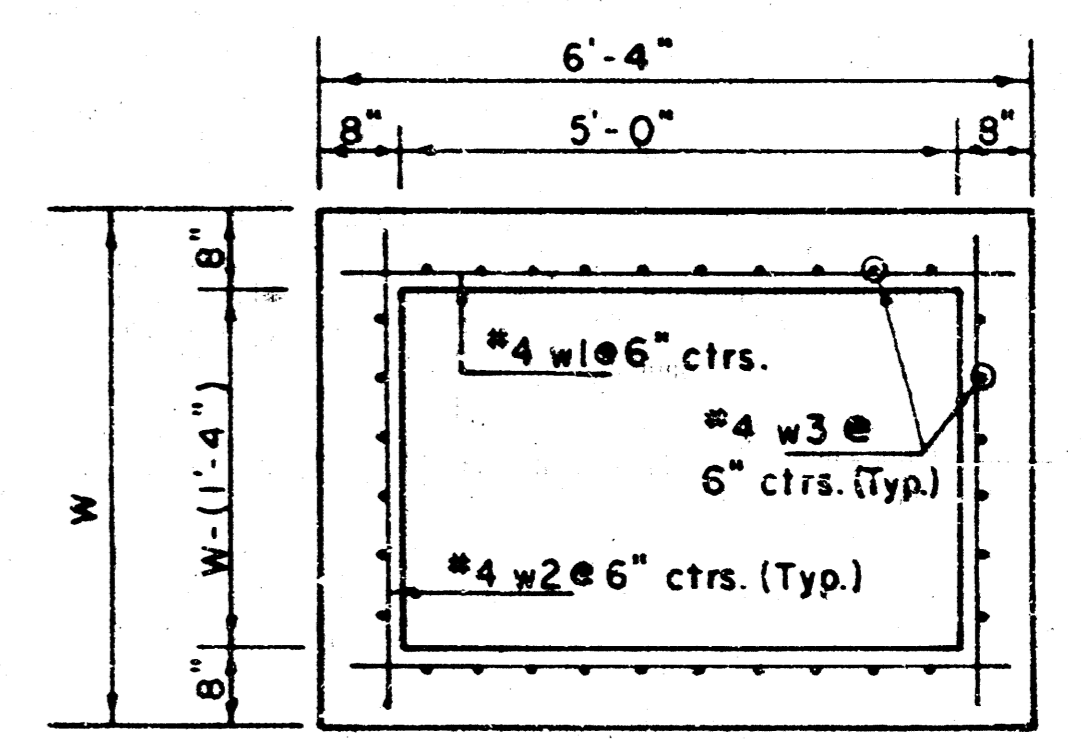
SECTION E-E



SECTION B-B



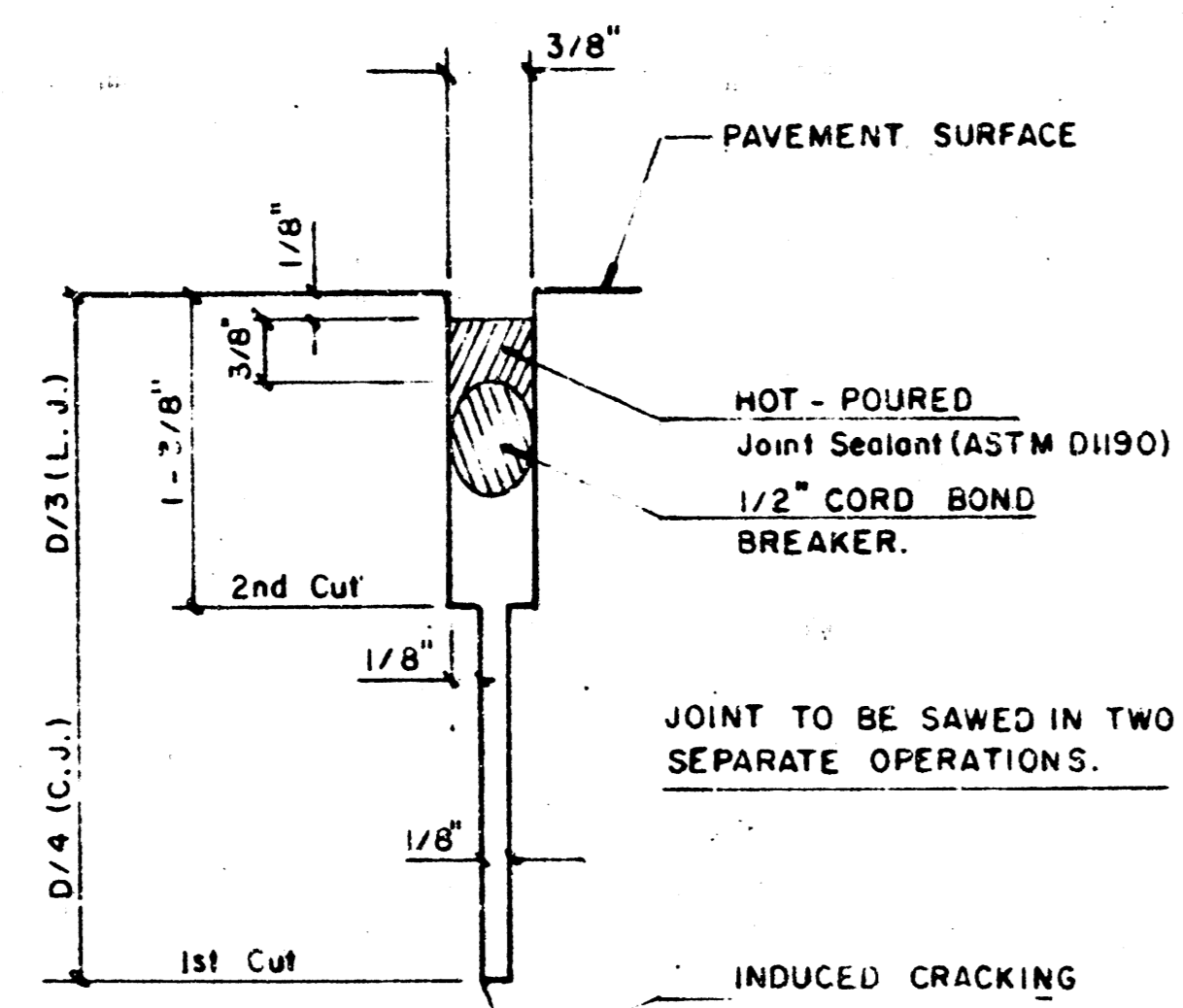
SECTION C-C



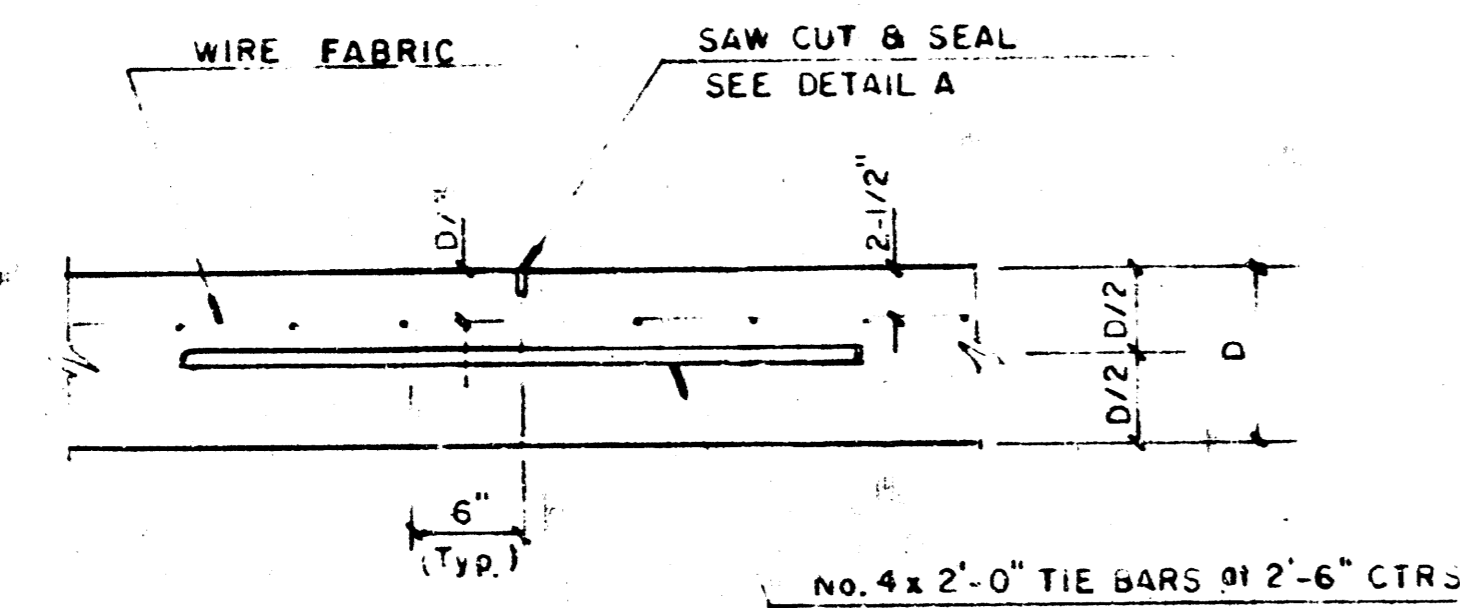
SECTION D-D

REVISED 11-30-1988
 REVISED 12-31-1984
 Revised 2-16-1989

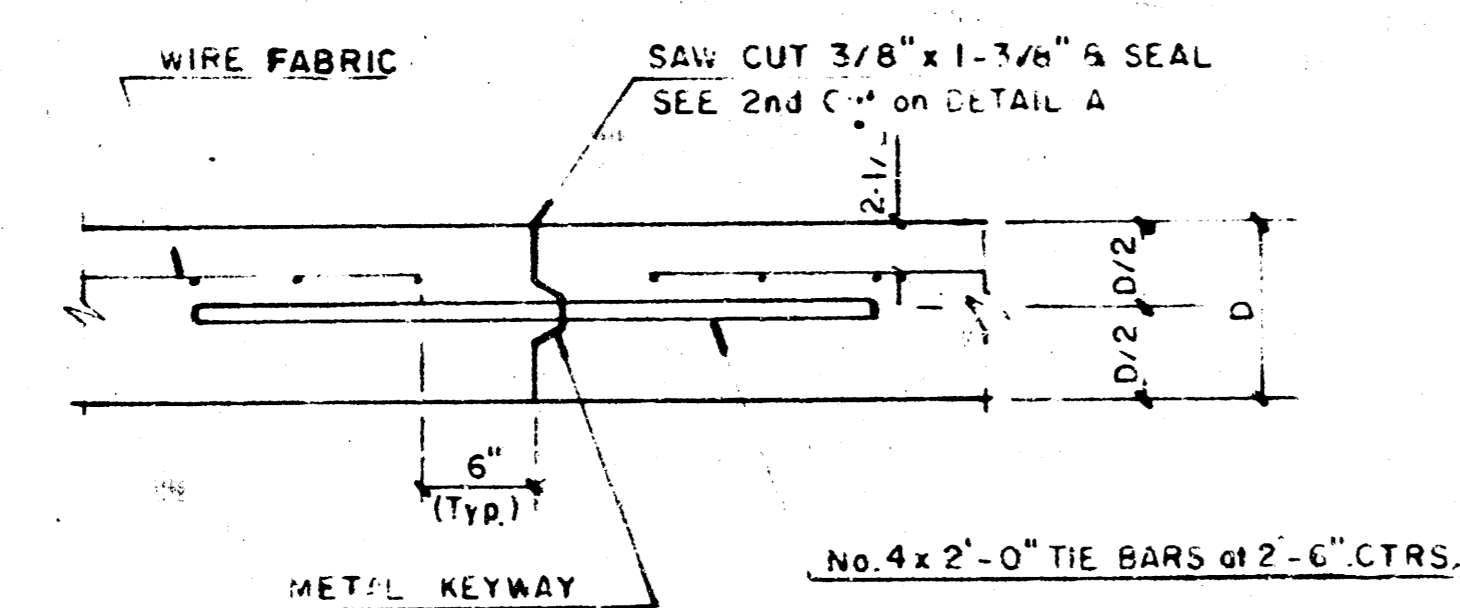
DETAIL STANDARD TYPE IA CURB INLET
 CITY OF WICHITA, KANSAS
 INLET OPENING = 6" x 5' 0"
 PROJ. NO. 472-81831-001
 8740-D-80,105 JUNE 1984 SHEET 6 OF 9



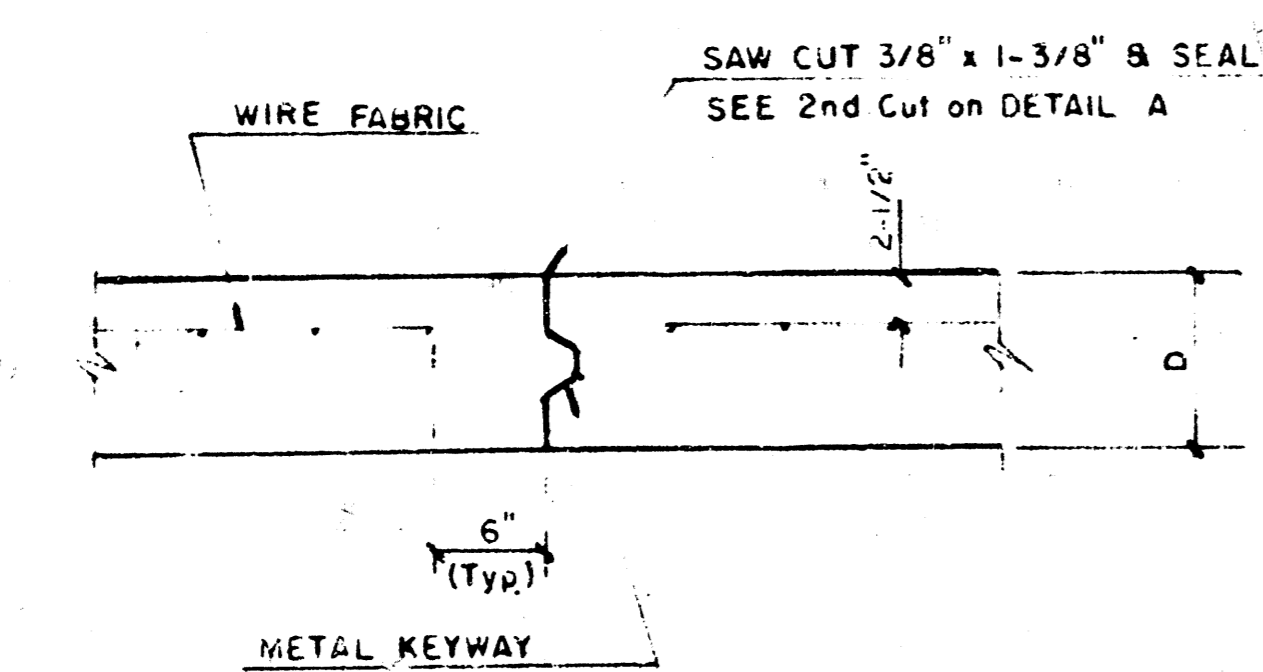
DETAIL A



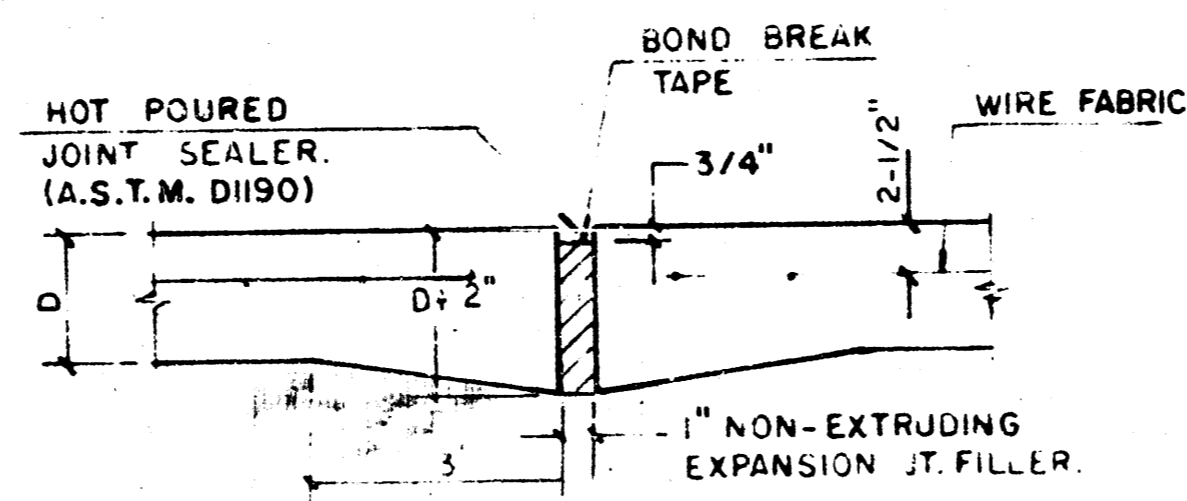
LONGITUDINAL JOINT DETAIL (L.J.)



OPTIONAL LONGITUDINAL CONSTRUCTION JOINT (L.J.) (Alternate L.J.)

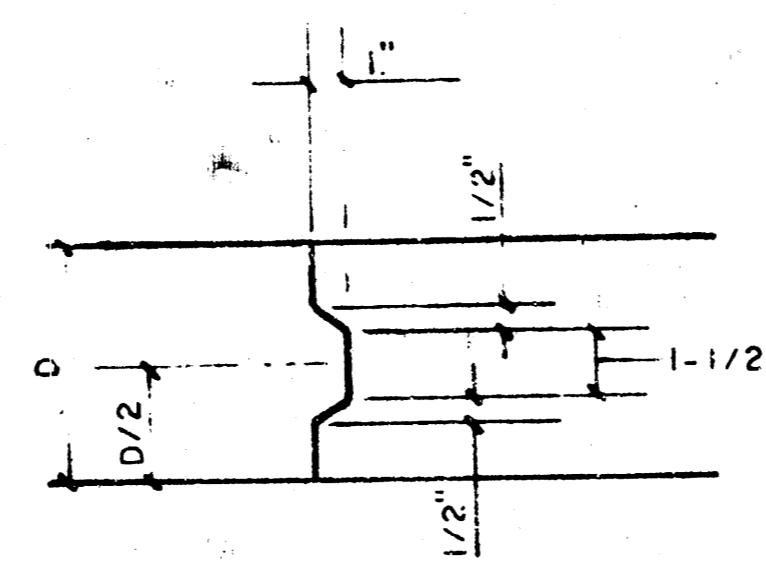


OPTIONAL CONTRACTION JOINT DETAIL (C.J.) (Alternate C.J.)

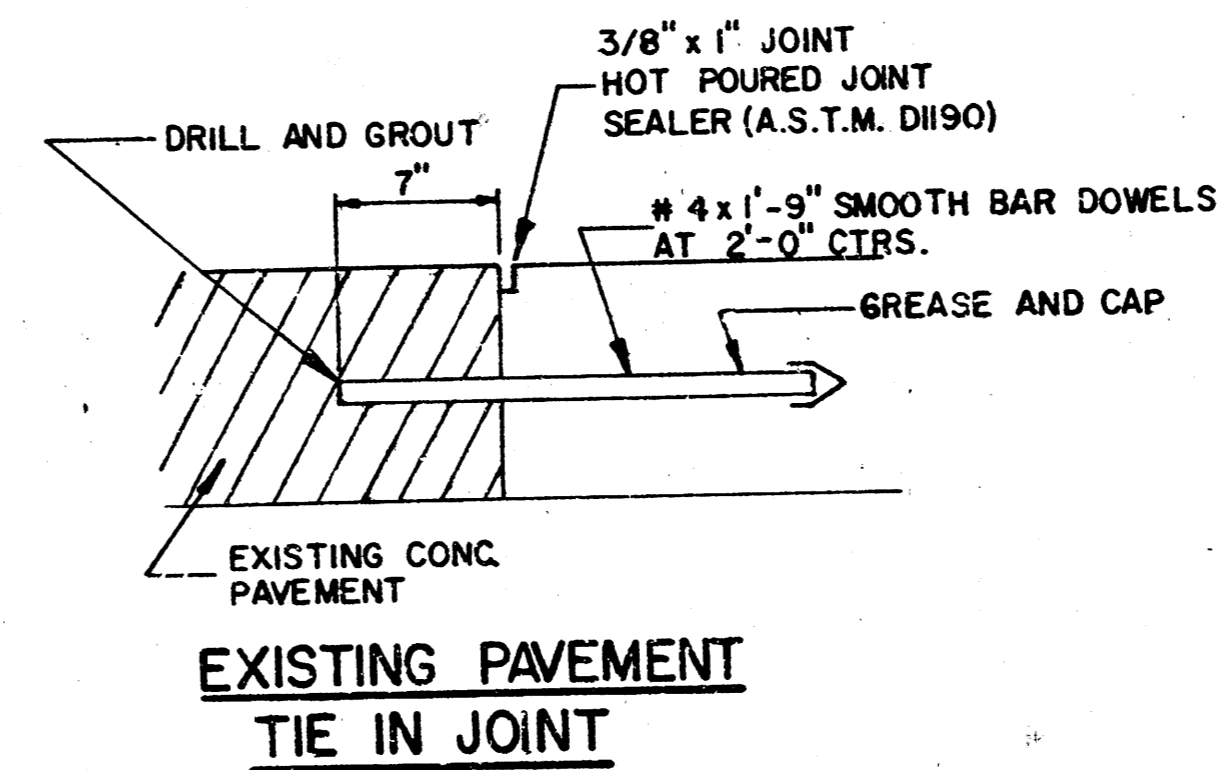


EXPANSION JOINT

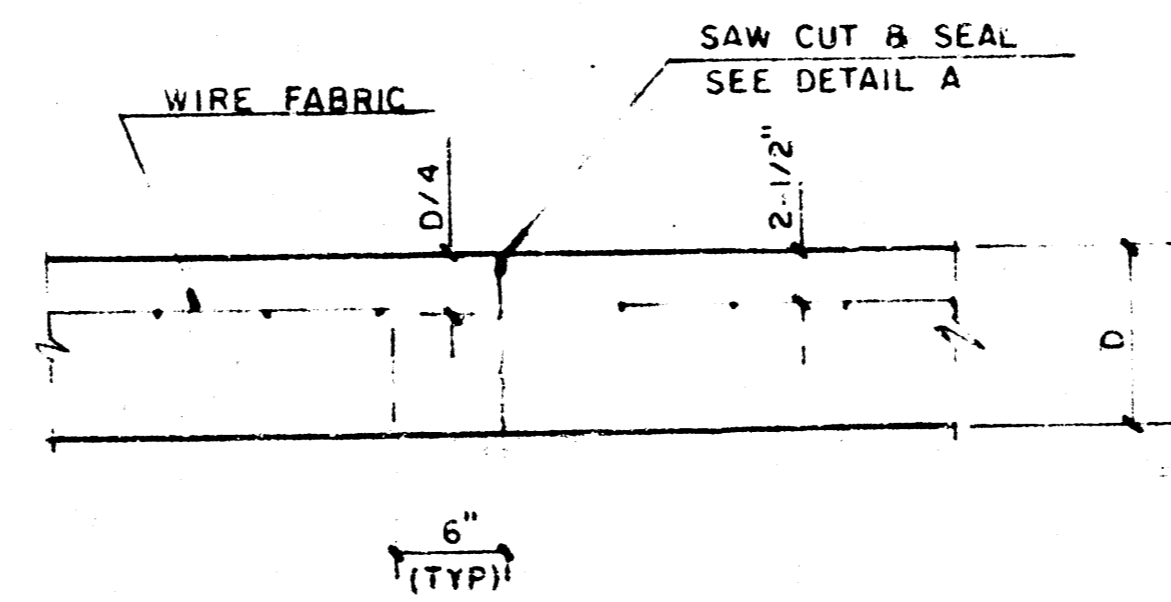
NOTE: EXTRA THICKNESS TO BE SUBSIDIARY TO PRICE OF SQ YDS PAVEMENT.



KEYWAY DETAIL

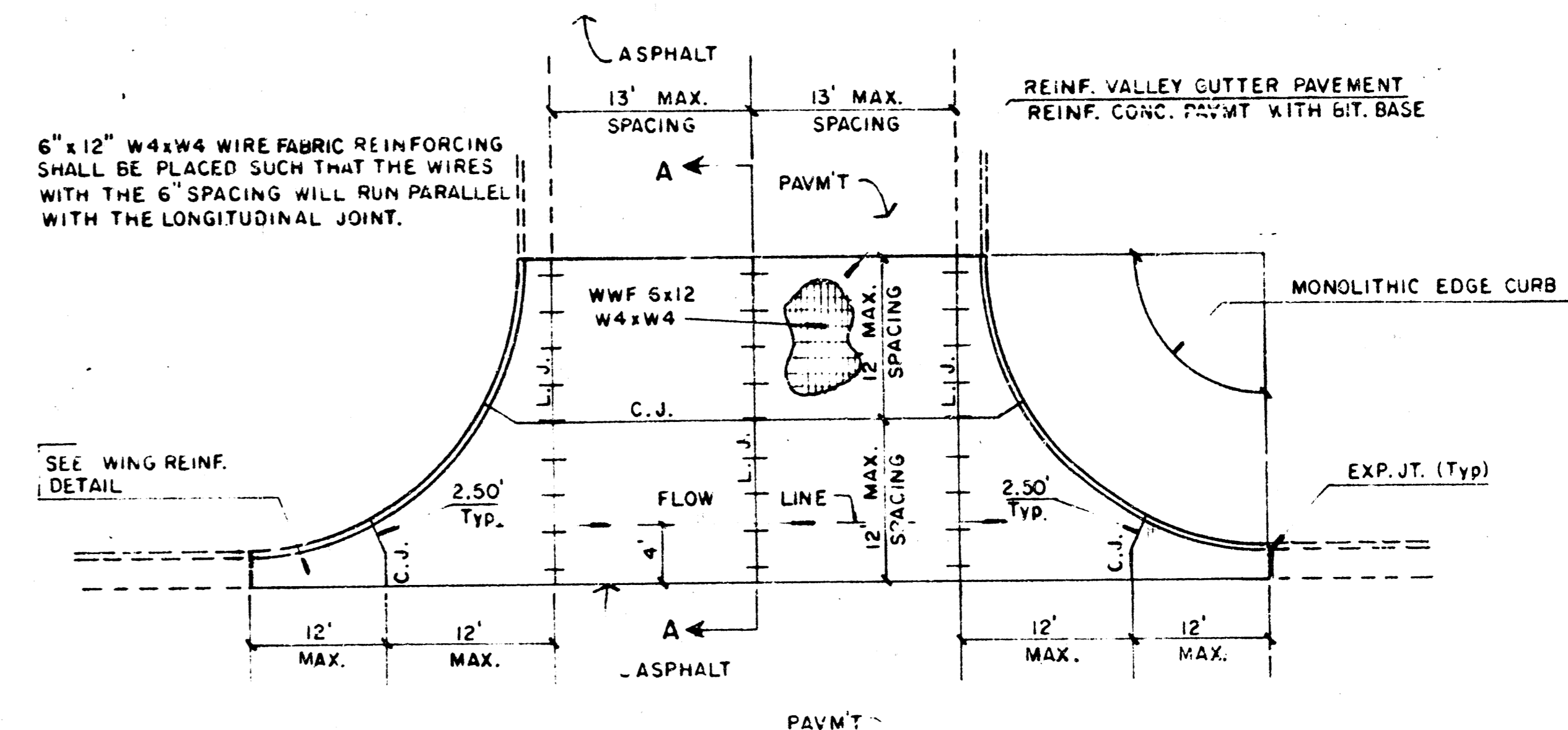


EXISTING PAVEMENT TIE IN JOINT

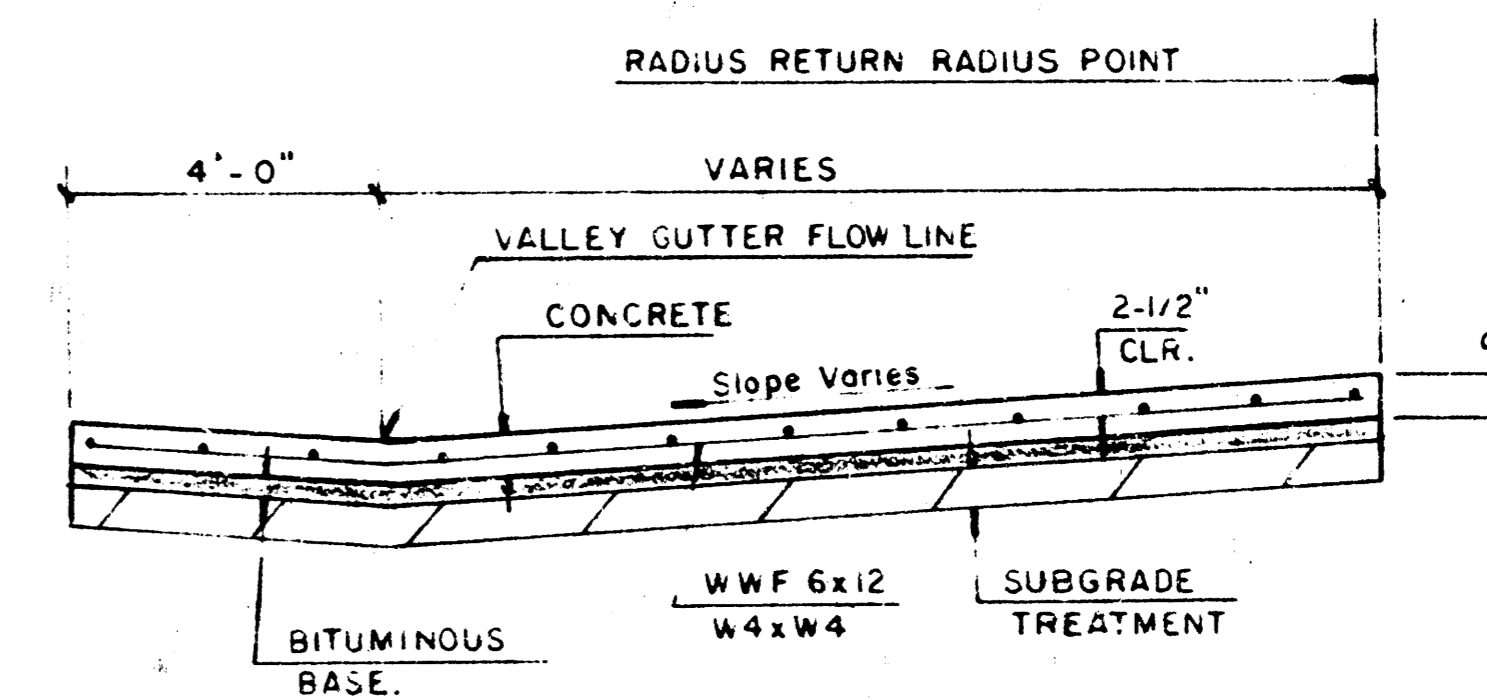


CONTRACTION JOINT DETAIL (C.J.)

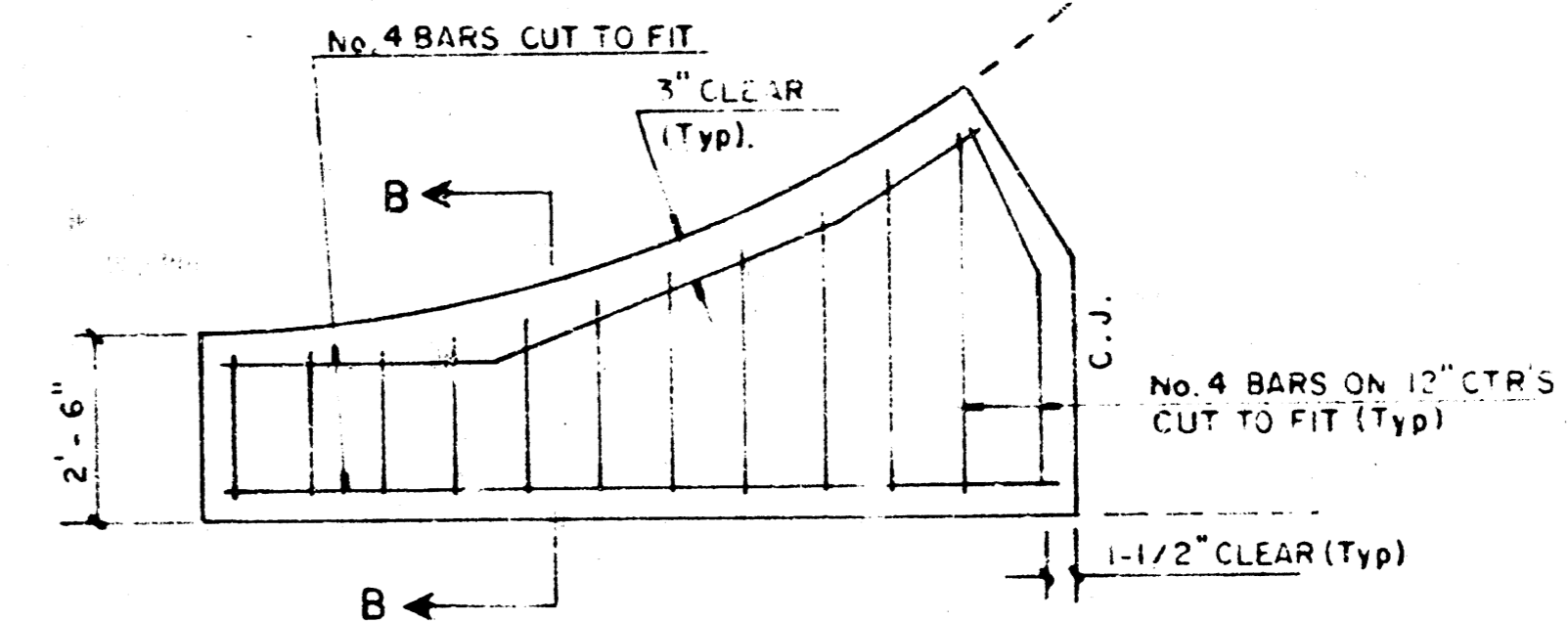
LEGEND
C.J. IDENTIFIES CONTRACTION JOINT
L.J. IDENTIFIES LONGITUDINAL JOINT



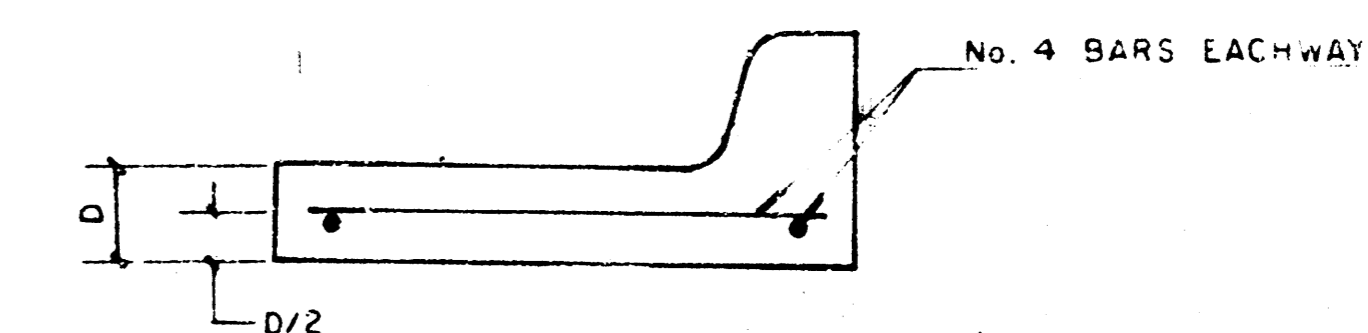
PLAN REINFORCED VALLEY GUTTER



SECTION A-A

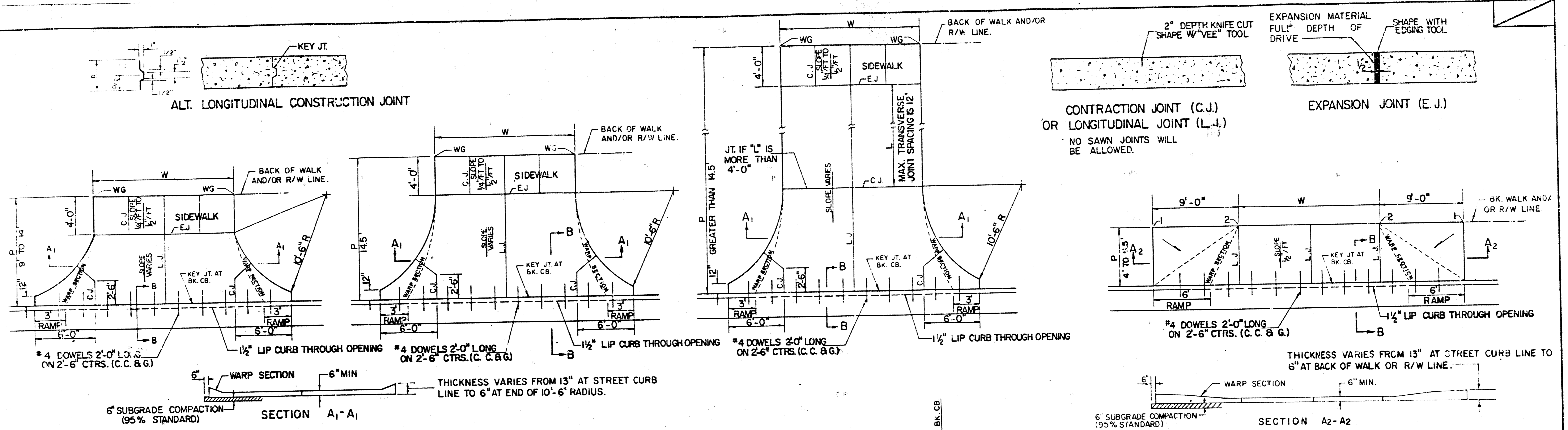


WING REINFORCING DETAIL



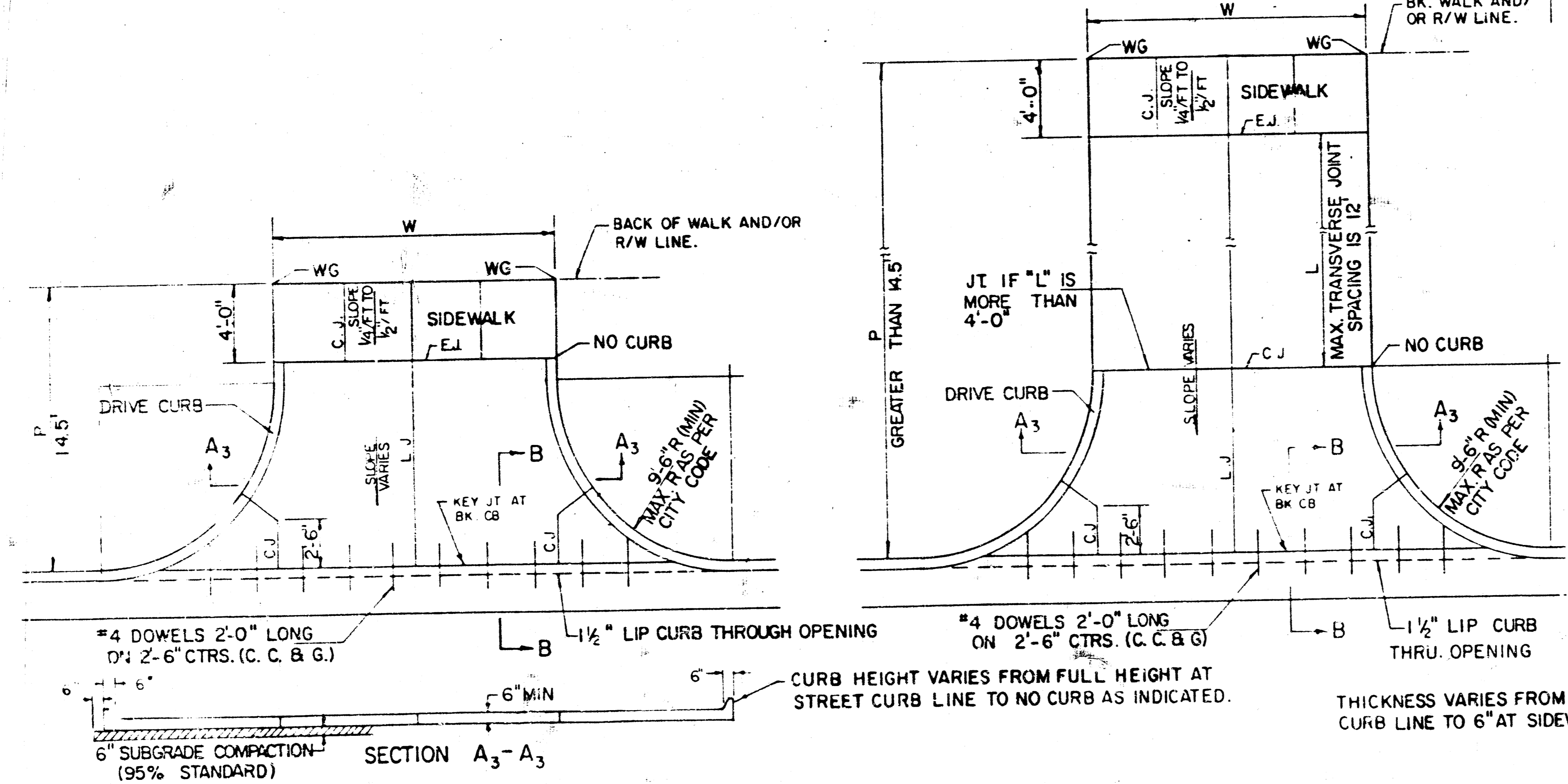
SECTION B-B

NOTE: OMIT WIRE FABRIC REINFORCING IN THIS SECTION.



PARKING WIDTH "P"		9'	10'	11'	12'	13'	14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE	MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35	0.35	0.40	0.45	0.60	0.80	1.35	1.85	2.35	2.85	3.35	3.85	4.35
OPTIMUM	MAX. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.35	0.35	0.40	0.45	0.60	0.70	1.04	1.30	1.56	1.82	2.08	2.34	2.60
OPTIMUM	MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	0.19	0.21	0.23	0.25	0.27	0.30	0.42	0.52	0.62	0.72	0.82	0.92	1.02
ABSOLUTE	MIN. DIST. OF PT. "WG" ABOVE OR BELOW TOP OF FULL CURB	-0.19	-0.16	-0.13	-0.10	-0.08	0.00	0.00	0.15	0.25	0.35	0.45	0.55	0.65

RADIUS RAMP DRIVES (P = 9.0' & GREATER)

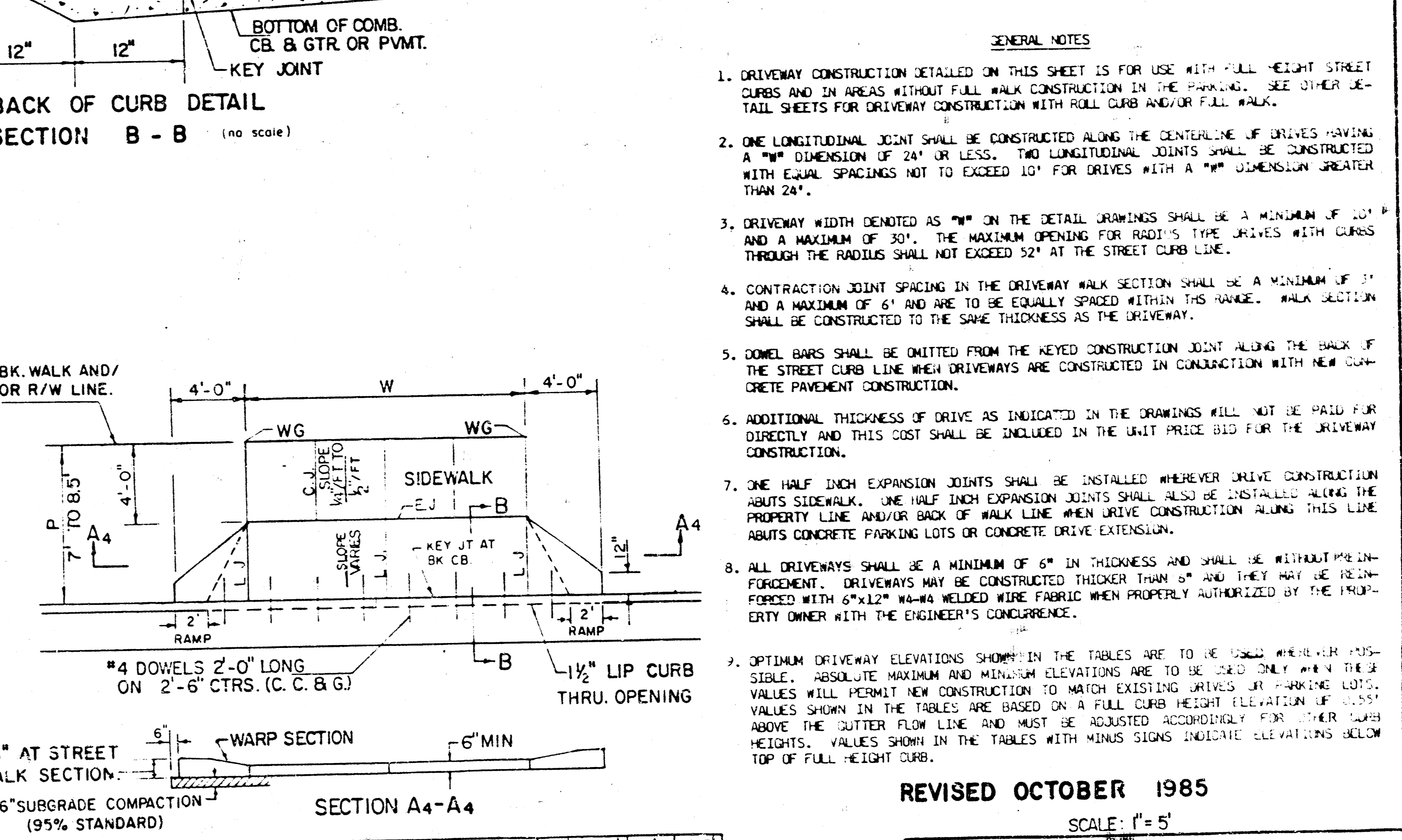


PARKING WIDTH "P"		14.5'	20'	25'	30'	35'	40'	45'	50'
ABSOLUTE	MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.80	1.35	1.85	2.35	2.85	3.35	3.85	4.35
OPTIMUM	MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.70	1.04	1.30	1.56	1.82	2.08	2.34	2.60
OPTIMUM	MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.30	0.42	0.52	0.62	0.72	0.82	0.92	1.02
ABSOLUTE	MIN. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00	0.00	0.15	0.25	0.35	0.45	0.55	0.65

FULL RADIUS DRIVES (P = 14.5' & GREATER)

PARKING WIDTH "P"		4'	4.5'	5'	5.5'	6'	6.5'
DIST. OF PT. "P" ABOVE TOP OF FULL CB.		0.08	0.09	0.10	0.12	0.13	0.14
DIST. OF PT. "P" BELOW TOP OF FULL CB.		-0.26	-0.24	-0.22	-0.20	-0.18	-0.16

FULL RAMP DRIVE (P = 4.0' TO 6.5')



PARKING WIDTH "P"		7'	7.5'	8'	8.5'
ABSOLUTE	MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00	0.10	0.20	0.30
OPTIMUM	MAX. DIST. OF PT. "WG" ABOVE TOP OF FULL CB.	0.00	0.10	0.20	0.30
OPTIMUM	MIN. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-0.15	-0.16	-0.17	-0.17
ABSOLUTE	MAX. DIST. OF PT. "WG" BELOW TOP OF FULL CB.	-0.25	-0.20	-0.20	-0.20

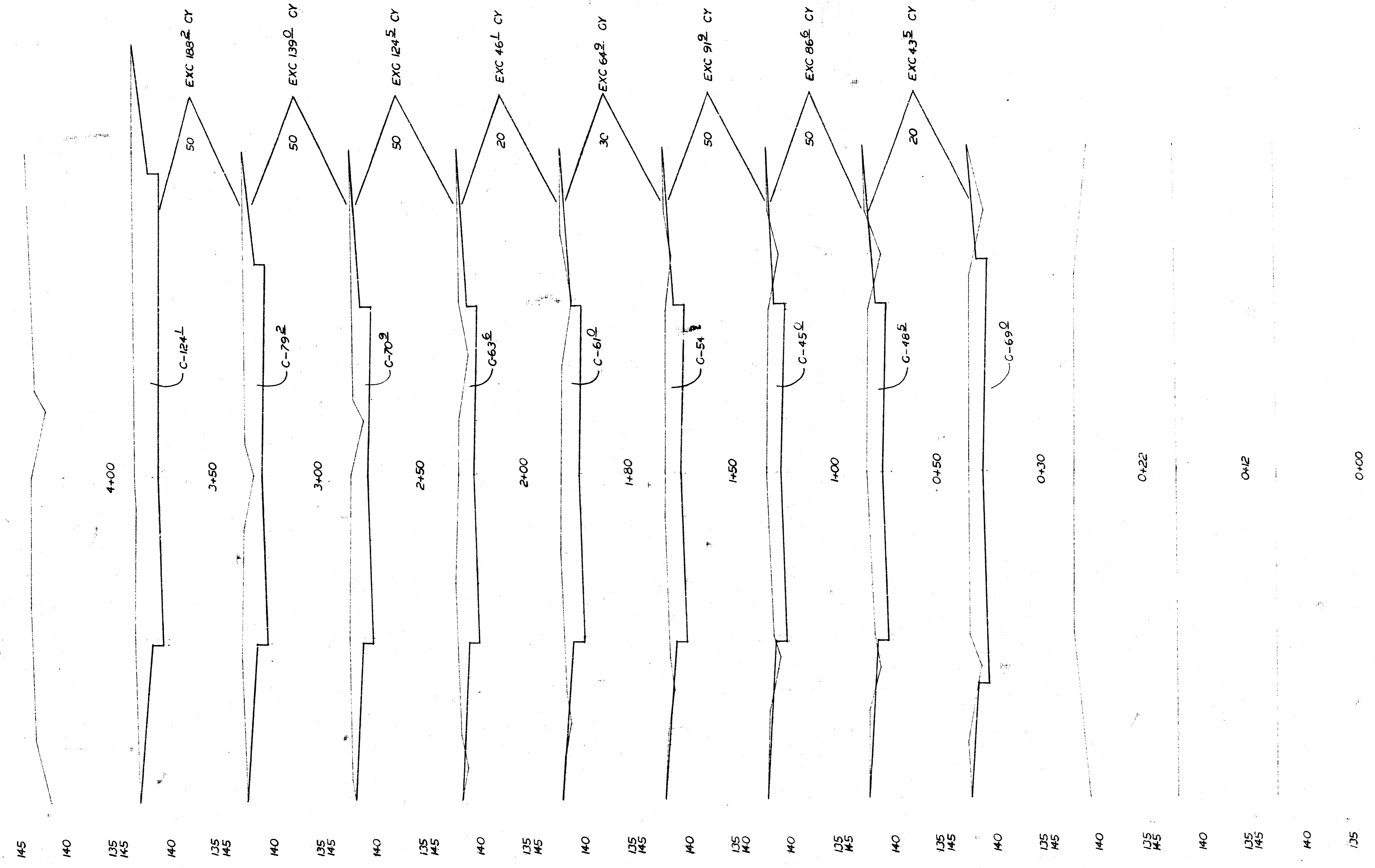
FULL RAMP DRIVE (P = 7.0' TO 8.5')

- GENERAL NOTES**
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING LOTS. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH FULL CURB AND/OR FULL WALK.
 - ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "P" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "P" DIMENSION GREATER THAN 24'.
 - DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS DRIVEWAYS WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
 - CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
 - DOWEL BARS SHALL BE OMITTED FROM THE KEYED CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
 - ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
 - ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
 - ALL DRIVEWAYS SHALL BE A MINIMUM OF 6" IN THICKNESS AND SHALL BE WITHOUT REINFORCEMENT. DRIVEWAYS MAY BE CONSTRUCTED THICKER THAN 6" AND THEY MAY BE REINFORCED WITH 6"x12" W-W WELDED WIRE FABRIC WHEN PROPERLY AUTHORIZED BY THE PROPERTY OWNER WITH THE ENGINEER'S CONFORMANCE.
 - OPTIMUM DRIVEWAY ELEVATIONS SHOWN IN THE TABLES ARE TO BE USED WHENEVER POSSIBLE. ABSOLUTE MAXIMUM AND MINIMUM ELEVATIONS ARE TO BE USED ONLY WHEN THE SEVERAL VALUES WILL PERMIT NEW CONSTRUCTION TO MATCH EXISTING DRIVEWAY OR PARKING LOTS. VALUES SHOWN IN THE TABLES ARE BASED ON A FULL CURB HEIGHT ELEVATION OF 0.55' ABOVE THE OUTER FLOW LINE AND MUST BE ADJUSTED ACCORDINGLY FOR OTHER CURB HEIGHTS. VALUES SHOWN IN THE TABLES WITH MINUS SIGNS INDICATE ELEVATIONS BELOW TOP OF FULL HEIGHT CURB.

REVISED OCTOBER 1985
SCALE: 1" = 5'

STANDARD DRIVE ENTRANCES
FULL HEIGHT CURB
CITY OF WICHITA, KANSAS
8740-D-80,109 PROJECT NUMBER SHEET NO. 8 OF 9
472-81831-001

30 25 20 15 10 5 0 5 10 15 20 25 30



SHEET TOTAL
784.0 CY-EXCAVATION

EARL M. SAYLER STREET IMPROVEMENTS	
CROSS SECTIONS EVERGREEN LANE PROJ. NO. 472-81831-001	
CASTLE & ASSOCIATES ENGINEERS WICHITA, KANSAS	DSG LJK DR. LJK CH. RMH APP. RLC
DATE	8740-D-80,107 SHEET NO 9 OF 9

18-2-3-3