

IMPROVING 37TH STREET NORTH HYDRAULIC AVE. TO THE C.R.I.&P RAILROAD

GENERAL NOTES

A saw cut of at least one-half the depth of existing surface courses or one-fourth the depth of the existing total pavement thickness shall be provided at locations where proposed construction abuts an existing surface course or pavement for which partial removal of that surface or pavement is required. Sawed joint to facilitate removal within three (3) feet of existing joints will not be permitted and for such instances the limits of removal shall extend to the existing joint. Such saw cuts will not be paid for directly and this cost shall be considered as subsidiary to the removal of the surface or pavement.

Rubble from the removal of miscellaneous structures and excess excavation which is to be wasted shall be disposed of on sites to be provided by the Contractor. These sites shall be approved by the Engineer as to suitability, appearance and site location. Locations that, in the opinion of the Engineer, will leave an unsightly appearance will not be approved.

The Engineer shall take field ties to all quarter section corners. The Contractor shall set a City survey monument in the required location where such quarter section corners fall within the limits of pavement construction. Survey monuments will be furnished by the City. The Engineer will accurately locate and install the iron at the quarter section corner. This work will not be paid for directly, but shall be considered subsidiary to other pay items of work in the contract.

The City of Wichita's Sewer Maintenance Division of the Department of Water and Water Pollution Control has televised sewers within the limits of the project and have found no defects requiring repair. The Division shall be notified and afforded the opportunity to reteleviser sewer lines after subgrade work has been completed and prior to pavement construction to determine if such sewer lines have been damaged by the Contractor's operations. Damaged sewer lines will be repaired by the Contractor, as directed by the Engineer, at the Contractor's expense. Television logs are available for inspection by the Contractor during normal office hours at the Sewer Maintenance Division's office at City Hall.

Contractor will be required to grade existing shoulders adjacent to new pavement. New pavement elevations were established to provide excess pulverized roadbed material to facilitate grading shoulders. Such excess material shall be equally windrowed on each side of new pavement construction or otherwise stockpiled for later use for grading shoulders. Field Engineer may vary shoulder slopes from that shown on typical sections as necessary to match material available for grading such shoulders. Shoulder grading will not be paid for directly and this cost shall be included in the unit price bid for pavement construction.

Utility service lines, poles, valve boxes, meters, and etcetera are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction.

The contractor shall be responsible for preserving property irons. The contractor will be required to re-establish any property irons which are damaged or destroyed by construction operations. Such irons shall be re-established by a licensed land surveyor or a licensed professional engineer in accordance with state laws.

The Contractor shall give all property owners and/or tenants of developed property abutting the project limits a minimum of ten (10) days advance notice prior to start of construction.

ALL DRIVEWAY OVERLAY TO BE PAID AS TONS ASPHALTIC CONC. (5C-1).

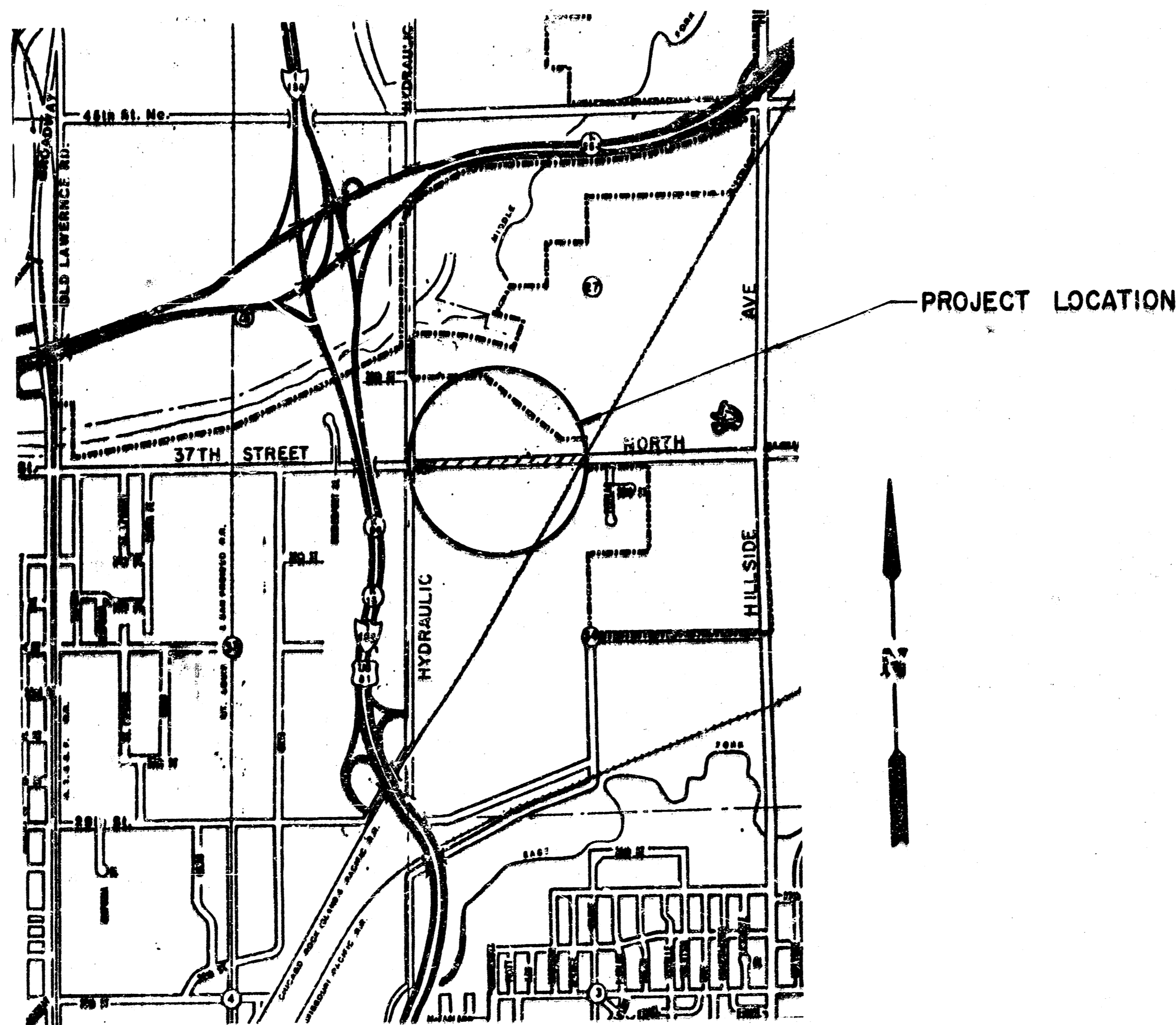
ALL EARTHWORK FOR WHICH THERE IS NOT A PAY ITEM INCLUDED IN THE PROPOSAL SHALL BE CONSIDERED AS SUBSIDIARY TO THE PAY ITEMS OF WORK IDENTIFIED IN THE PROPOSAL.

PLANS INCLUDE SUBGRADE TRANSITIONS ADJACENT TO GRADE CONTROL POINTS WHICH MUST BE LOWERED SUCH THAT THE NEW PAVEMENT SURFACE CAN BE CONSTRUCTED TO ELEVATIONS AS SHOWN ON PLANS. CONTRACTOR WILL BE REQUIRED TO REMOVE AND STOCKPILE EXISTING PAVEMENT IN THESE TRANSITIONS TO FACILITATE EXCAVATION OF EXISTING SUBGRADE SUCH THAT A RECONDITIONED ROADBED AND 6" ASPHALTIC CONCRETE PAVEMENT CAN BE CONSTRUCTED TO PLAN ELEVATIONS. EXISTING ASPHALT PAVEMENT REMOVED SHALL BE REPLACED IN A UNIFORM LIFT FOR THE WIDTH OF ROADBED TO BE RECONDITIONED AS INDICATED BY THE PLANS AND PROCESSED AS REQUIRED FOR RECONDITIONING OF THE EXISTING ROADBED. ADDITIONAL WORK REQUIRED WILL NOT BE PAID FOR SEPARATELY AND THE COST FOR SUCH ADDITIONAL WORK REQUIRED TO LOWER THE SUBGRADE SHALL BE CONSIDERED AS SUBSIDIARY TO OTHER PAY ITEMS OF WORK INCLUDED IN THE CONTRACT.

CONTRACTOR WILL BE REQUIRED TO GRADE EXISTING GRAVEL AND CRUSHED ROCK SURFACED DRIVEWAYS TO MATCH NEW ROADWAY ELEVATIONS. THE COST FOR THIS WORK SHALL BE CONSIDERED AS SUBSIDIARY TO THE OTHER PAY ITEMS OF WORK IN THE CONTRACT.

PROJECT NO. 4775-245-82007-00C-000-001

INDEX NO. 137075



INDEX TO DRAWINGS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS
3-7	PAVING PLANS
8-11	CROSS SECTIONS

**M.E. LINDEBAK CITY ENGINEER
CITY OF WICHITA, KANSAS**

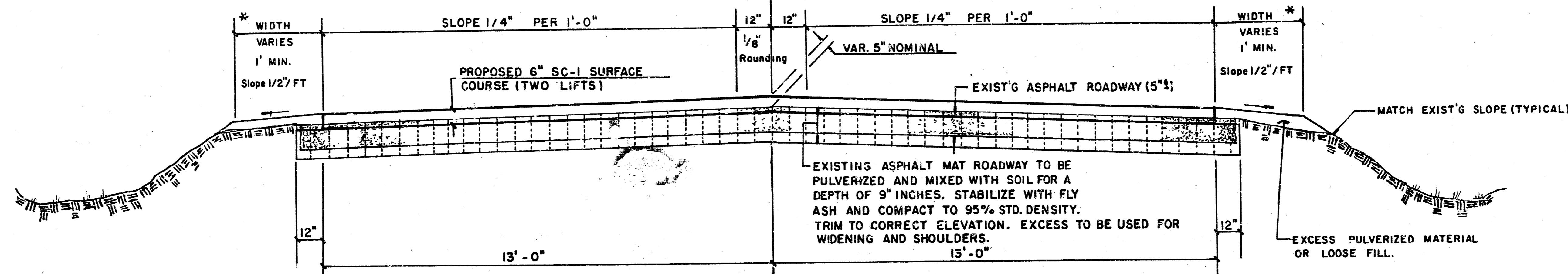
NOTE: ALL EARTHWORK REQUIRED IS SUBSIDIARY

TRAFFIC IS TO BE CARRIED THRU CONSTRUCTION

TYPICAL SECTION 26' ASPHALTIC CONCRETE ROADWAY

6" ASPHALTIC CONCRETE ON RECONDITIONED ROADBED
NORMAL CROWN

* NOTE: FIELD ENGINEER TO VARY SHOULDER WIDTH TO MATCH FILL SLOPE.

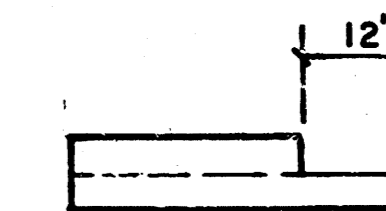


68+30 TO 89+53

A TACK COAT OF EMULSIFIED ASPHALT (SC-1H OR CSS-1H) SHALL BE APPLIED AT AN APPROXIMATE RATE OF 0.10 GALLONS PER SQUARE YARD BETWEEN EACH LIFT OF ASPHALTIC MATERIAL.

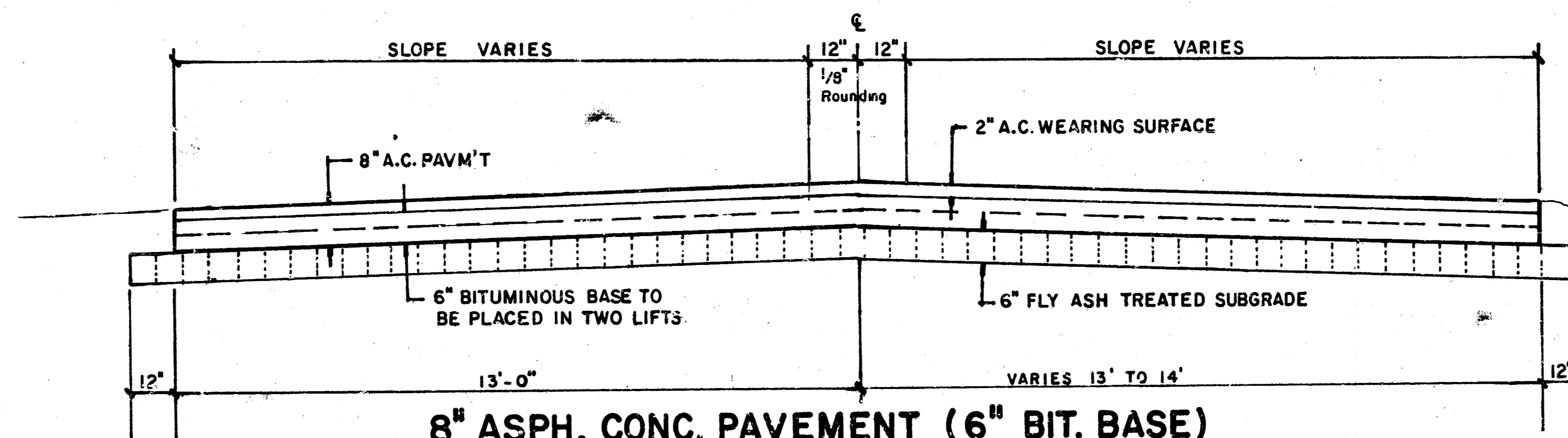
BITUMINOUS BASE AND ASPHALTIC CONCRETE WEARING SURFACE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROL FOR LINE AND GRADE.

TRANSVERSE CONSTRUCTION JOINT

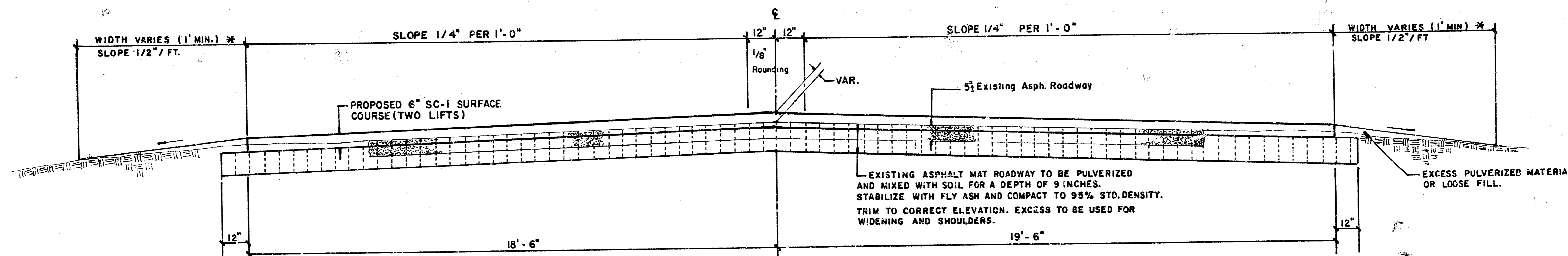


TRANSVERSE CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN FLEXIBLE PAVEMENT AT LOCATIONS WHERE PAVEMENT JOINS EXISTING FLEXIBLE PAVEMENT AS SHOWN BY DETAIL. ALL COSTS ASSOCIATED WITH THE CONSTRUCTION OF THE TRANSVERSE JOINT SHALL BE INCLUDED IN THE BID PRICE FOR TONS ASPHALTIC CONCRETE (SC-1).

CONSTRUCTION JOINTS IN EACH LIFT SHALL BE STAGGERED A MINIMUM OF ONE (1) FOOT FROM JOINTS IN PRECEDING LIFT AND PLACED SO THAT A JOINT WILL BE CONSTRUCTED ON THE CENTERLINE OF THE TOP LIFT.



8" ASPH. CONC. PAVEMENT (6" BIT. BASE)
89+61 TO 90+80

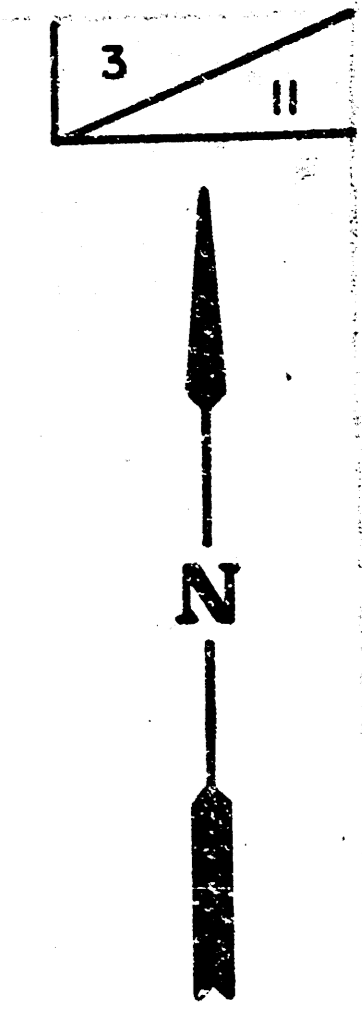
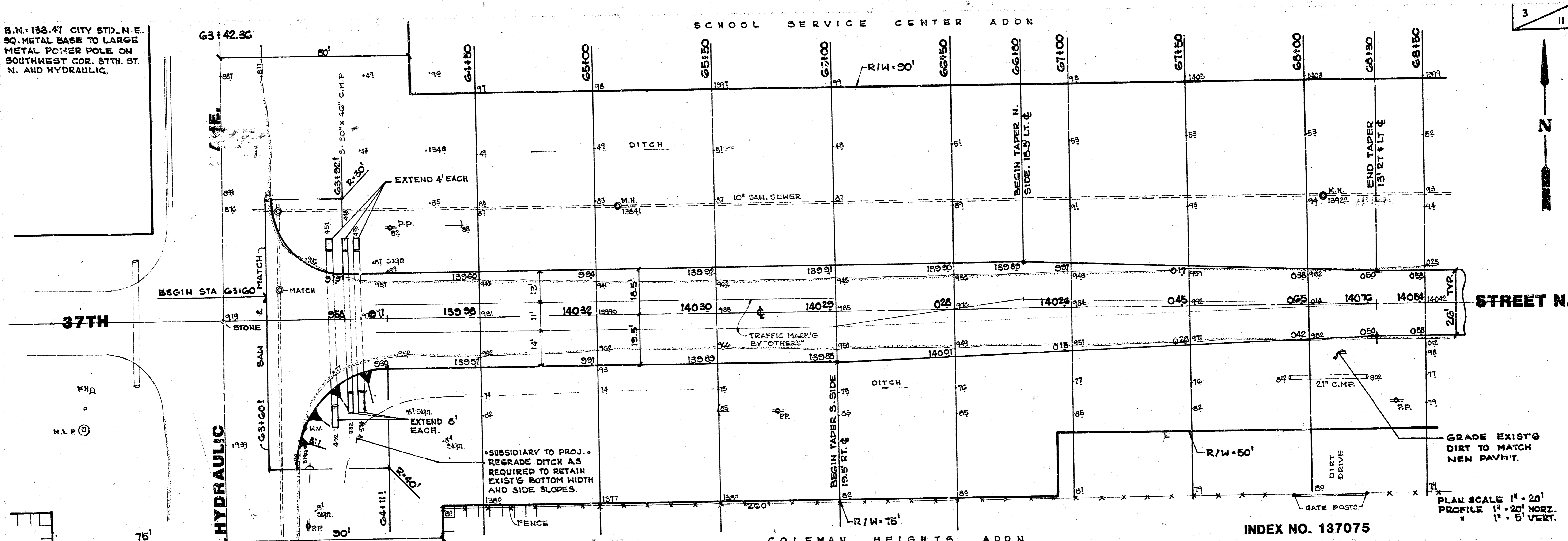


6" ASPH. CONC. ON RECONDITIONED ROADBED (WIDENING)
64+11 TO 66+00

INDEX NO. 137075

PROJECT DESCRIPTION
37TH STREET NORTH
(TYPICAL SECTION)
PROJ. NO. 472-76-245-82007-000-000-001

B.M. 138.47 CITY STD. N.E.
 50' METAL BASE TO LARGE
 METAL POWER POLE ON
 SOUTHWEST COR. 37TH ST.
 N. AND HYDRAULIC.

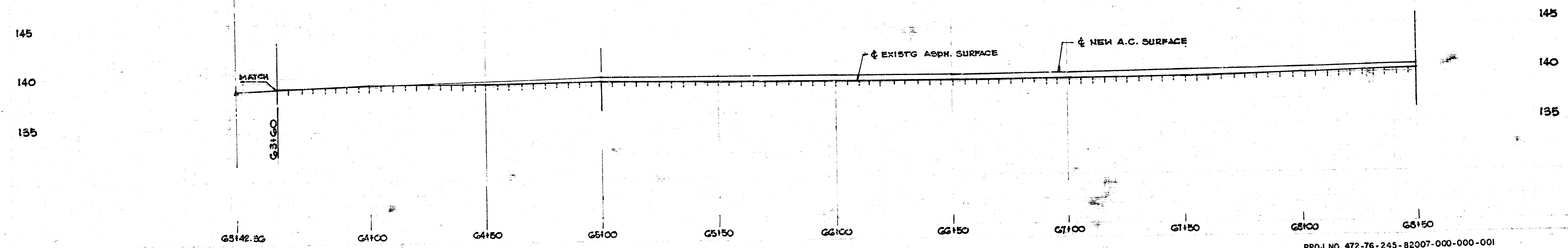


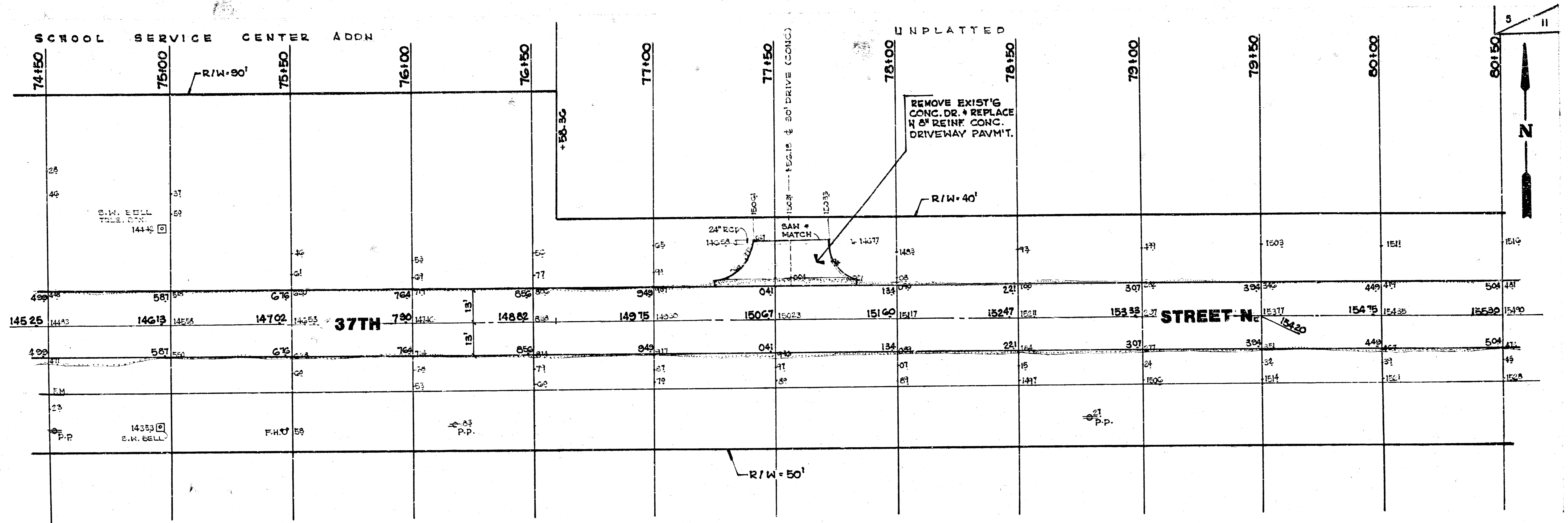
PLAN SCALE 1" = 20'
 PROFILE 1" = 20' HORIZ.
 1" = 5' VERT.

INDEX NO. 137075

6" A.C. PAVEMENT (A-C-1) ON RECONDITIONED ROADBED
 (PLAN ELEV. SET APPROX. 5' ABOVE EXIST'G PAVMT)

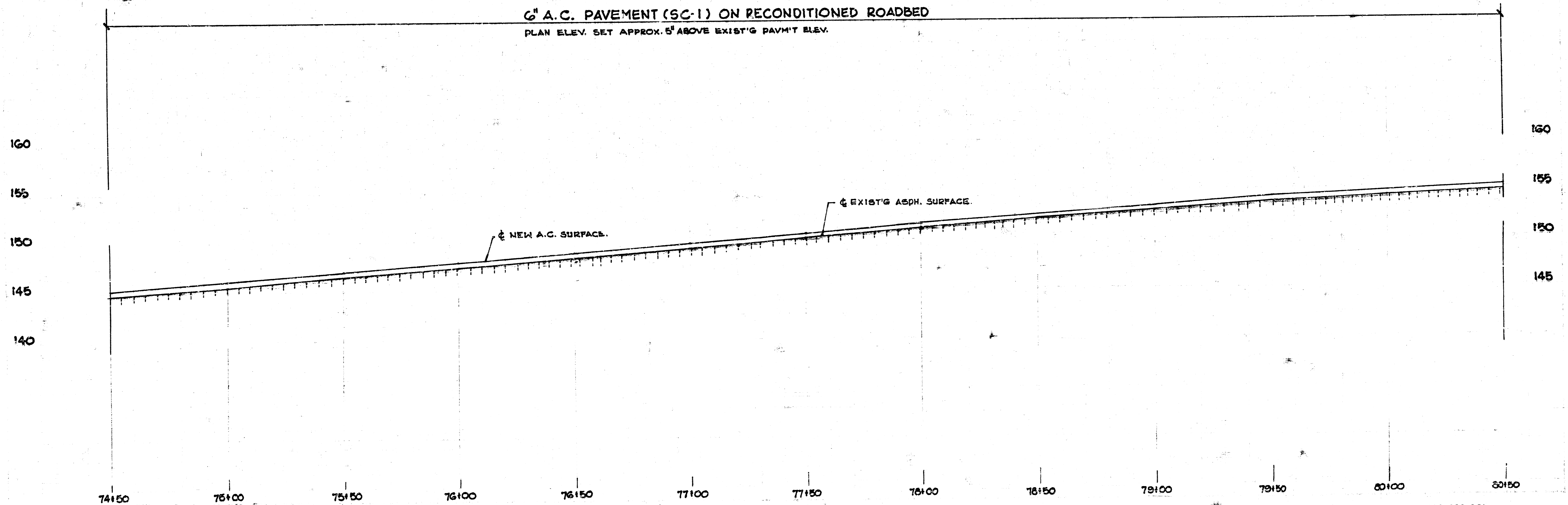
150' TRANSITION FROM EXIST'G PAVMT ELEVATION
 TO 5' ABOVE EXIST'G PAVMT ELEV. APPROX. EXC.
 QUANTITY IS 55 CU. YARDS

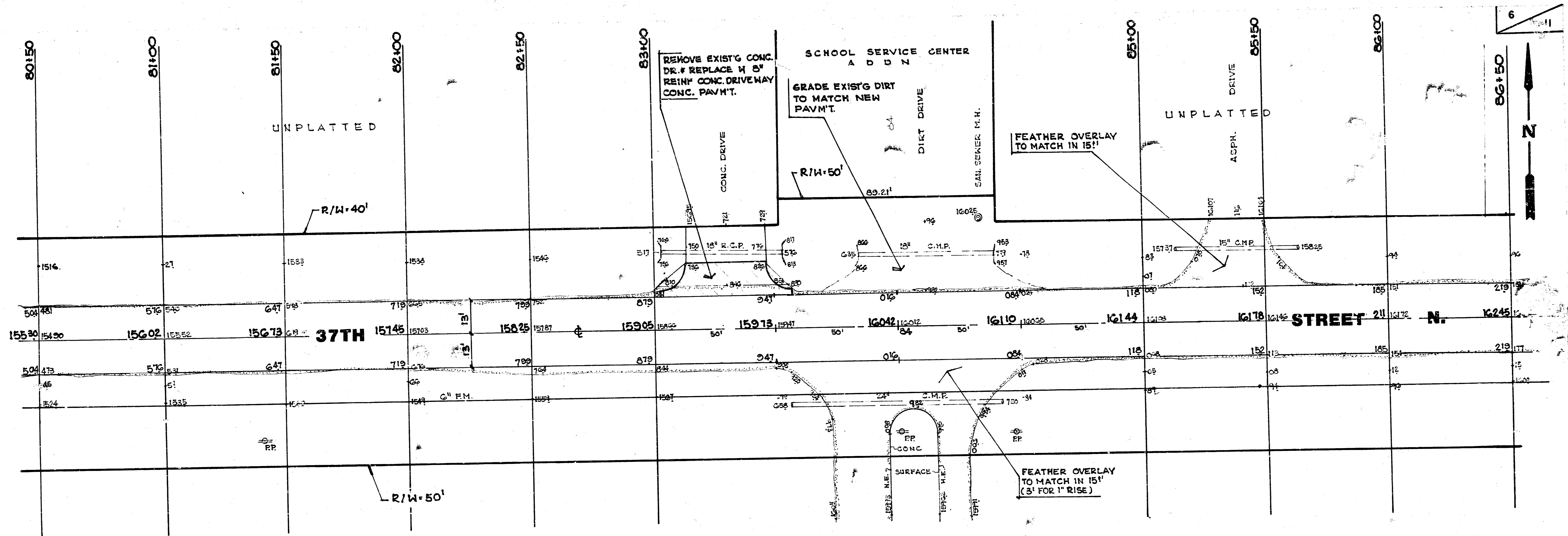




COLEMAN HEIGHTS ADDN.

6" A.C. PAVEMENT (SC-1) ON RECONDITIONED ROADBED
 PLAN ELEV. SET APPROX. 5" ABOVE EXIST'G PAVMT ELEV.

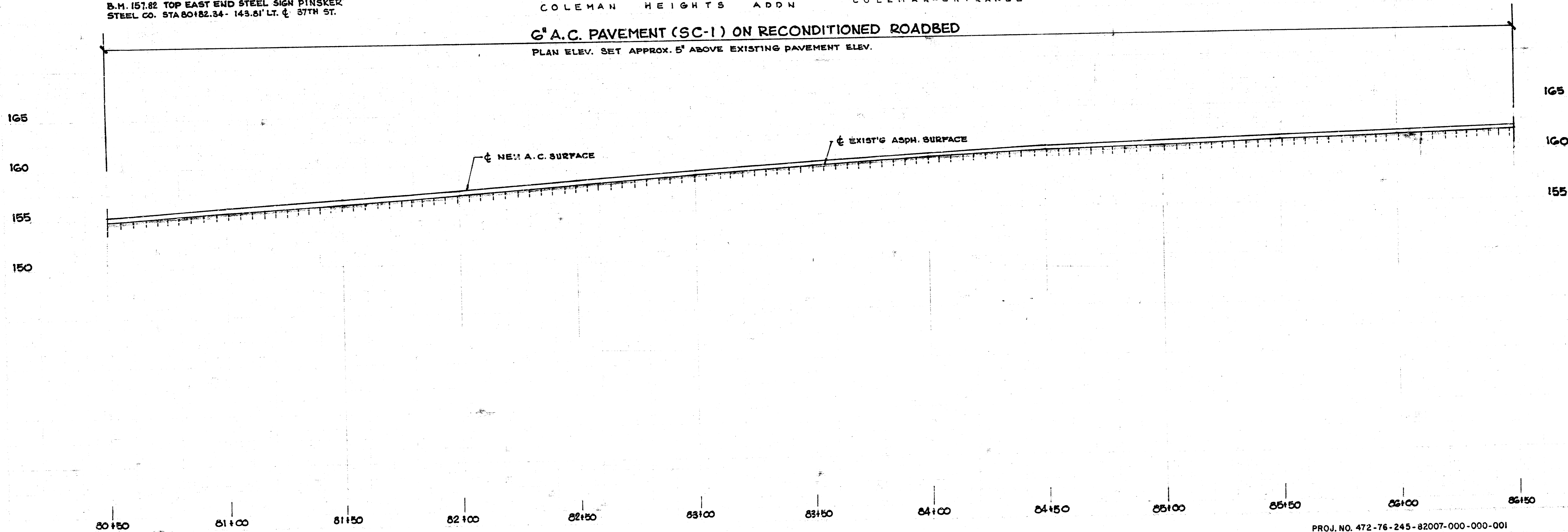


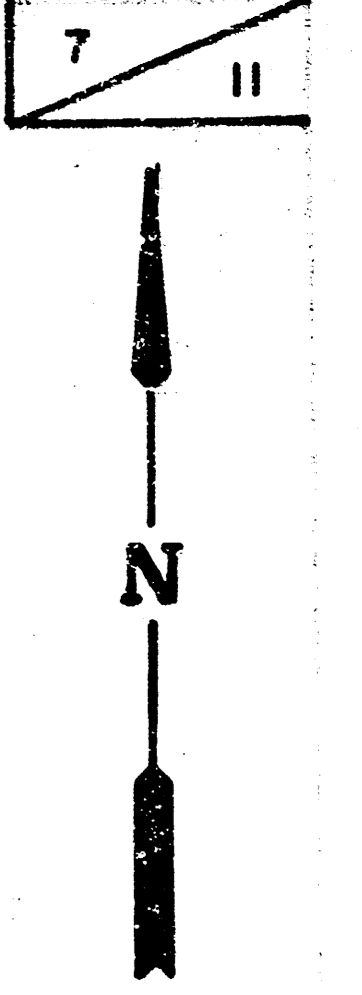
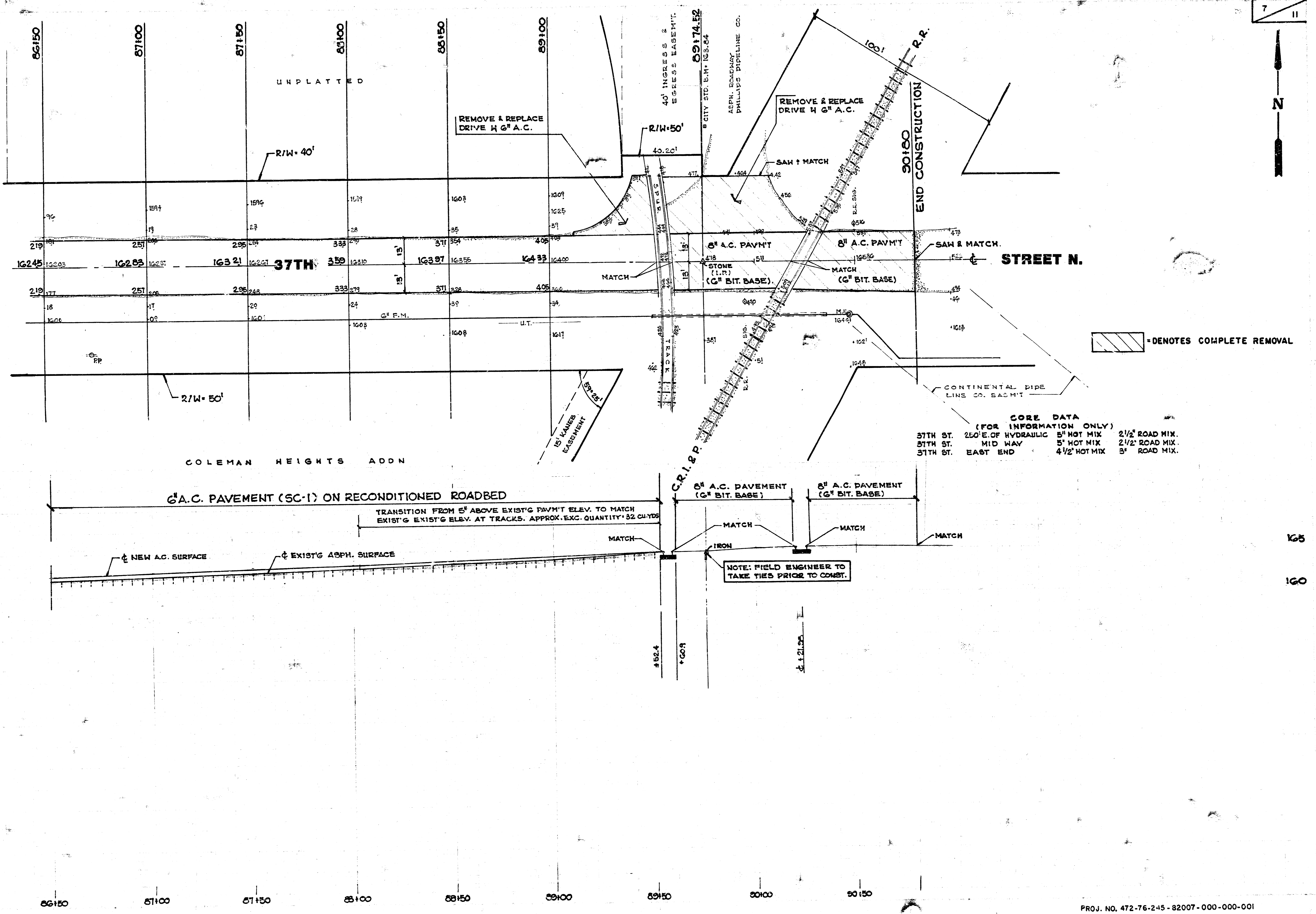


B.M. 157.82 TOP EAST END STEEL SIGN PINSKER
 STEEL CO. STA 80+82.34 - 143.81' LT. & 37TH ST.

COLEMAN HEIGHTS ADDN COLEMAN-ENTRANCE

G³ A.C. PAVEMENT (SC-1) ON RECONDITIONED ROADBED
 PLAN ELEV. SET APPROX. 5' ABOVE EXISTING PAVEMENT ELEV.



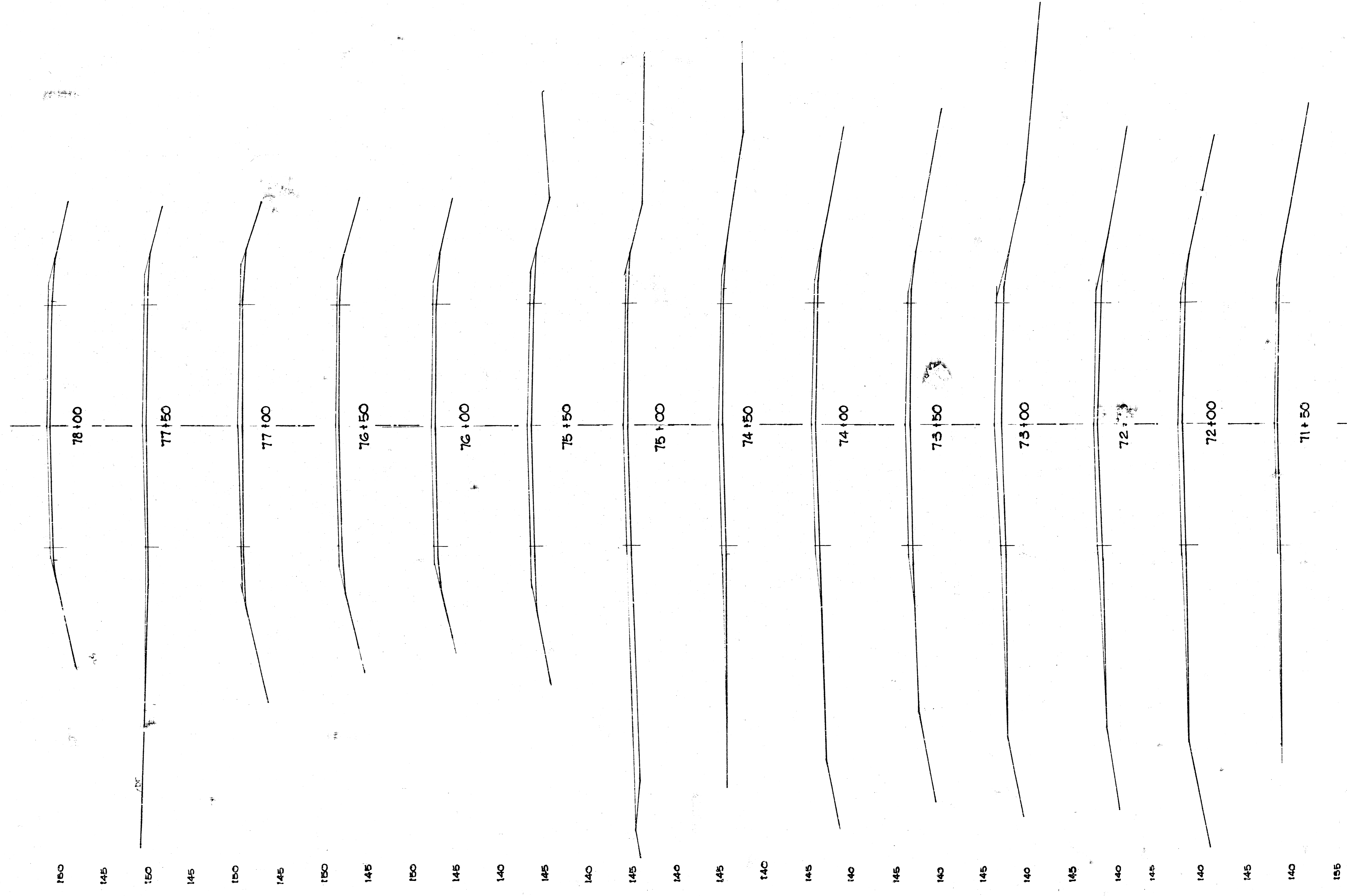


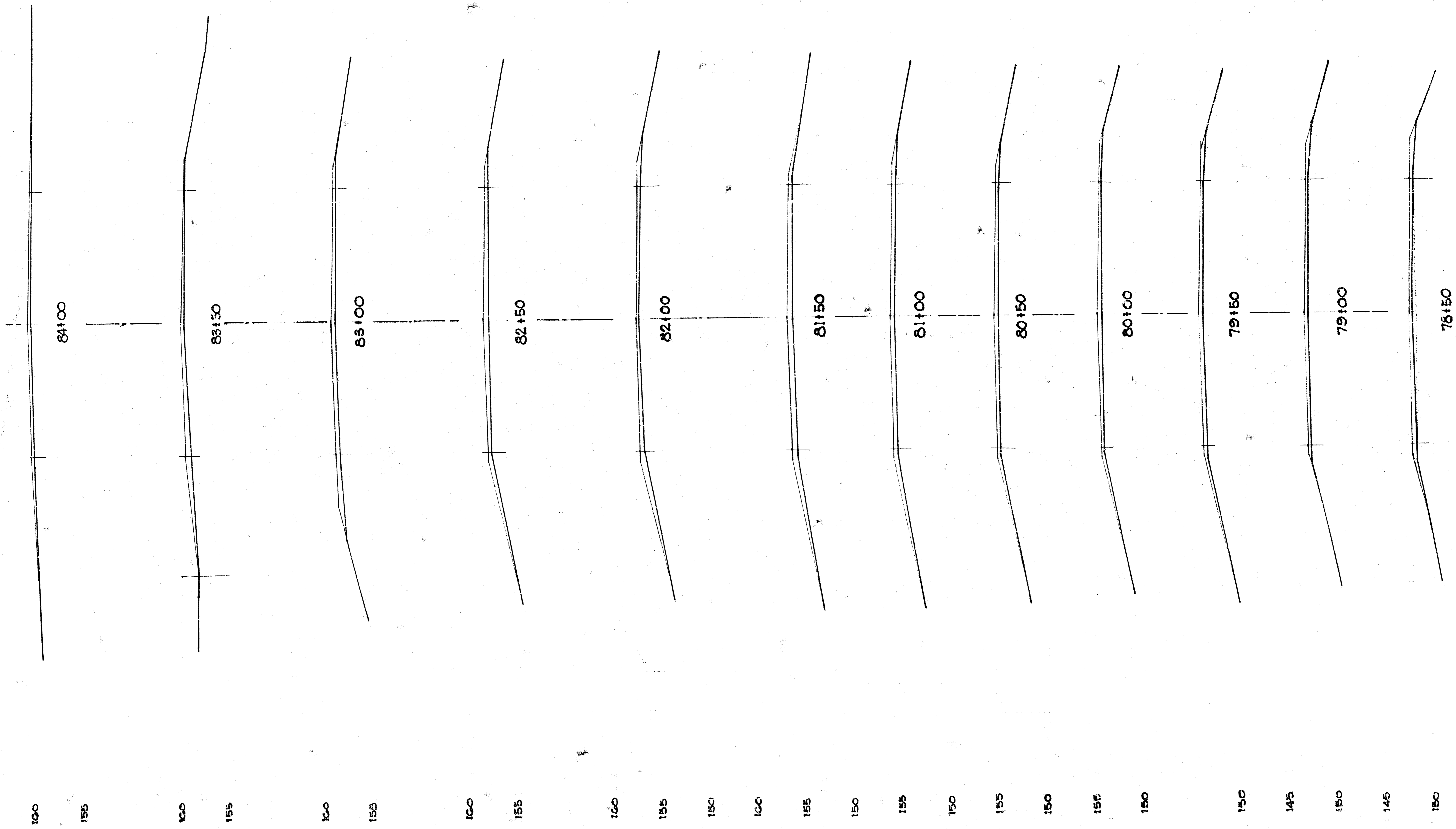
= DENOTES COMPLETE REMOVAL

CORE DATA
(FOR INFORMATION ONLY)

37TH ST. 250' E. OF HYDRAULIC	5" HOT MIX	2 1/2" ROAD MIX.
37TH ST. MID WAY	5" HOT MIX	2 1/2" ROAD MIX.
37TH ST. EAST END	4 1/2" HOT MIX	3" ROAD MIX.

NOTE: FIELD ENGINEER TO TAKE TIES PRIOR TO CONST.





37TH STREET NORTH
Plan. No. 472-16-245-82407-000-000-001

40 30 20 10 0 10 20 30 40

