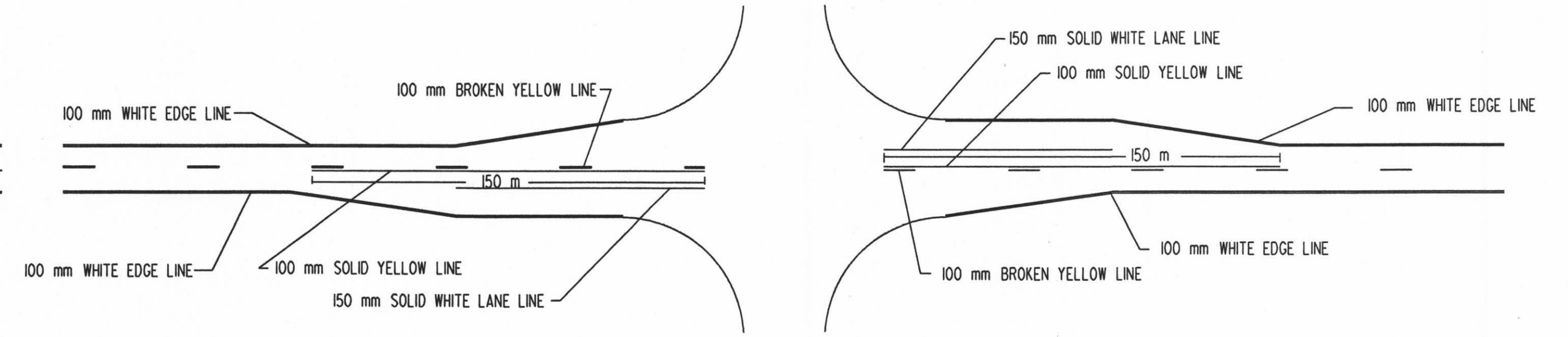
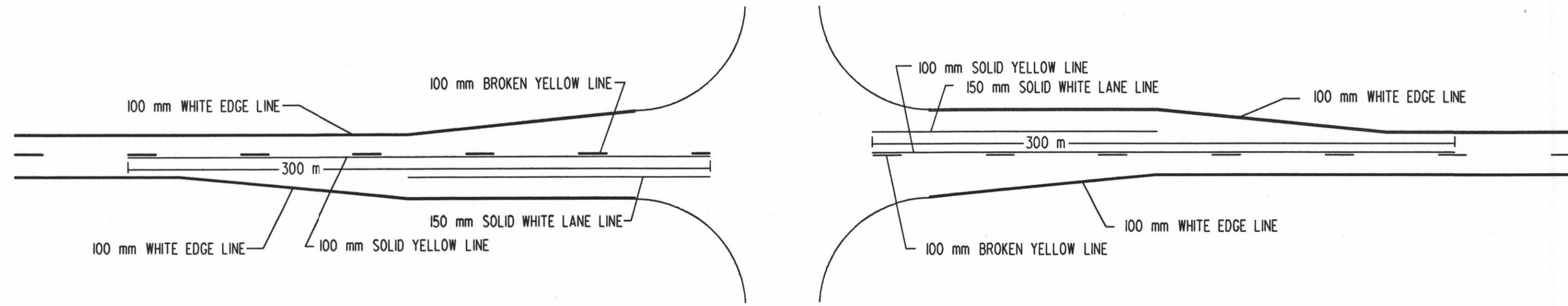


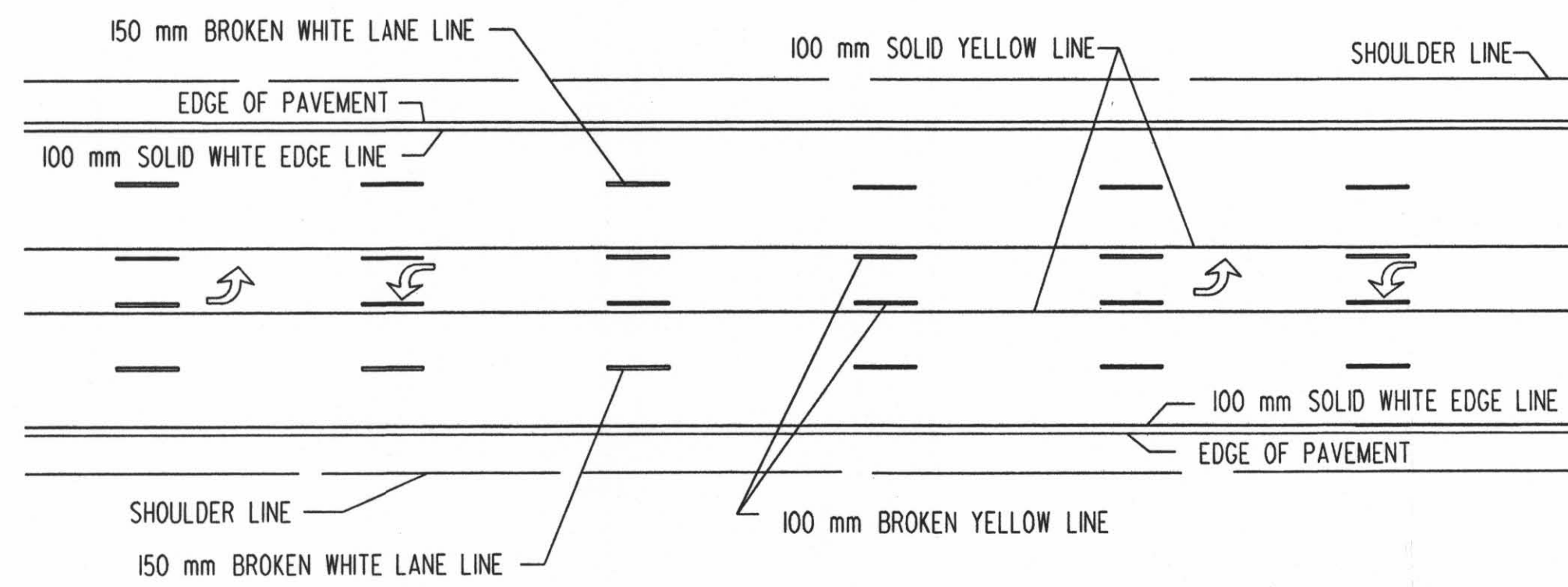
NOTE: ALL PAVEMENT MARKINGS SHALL BE BROKEN AT CROSS ROADS.

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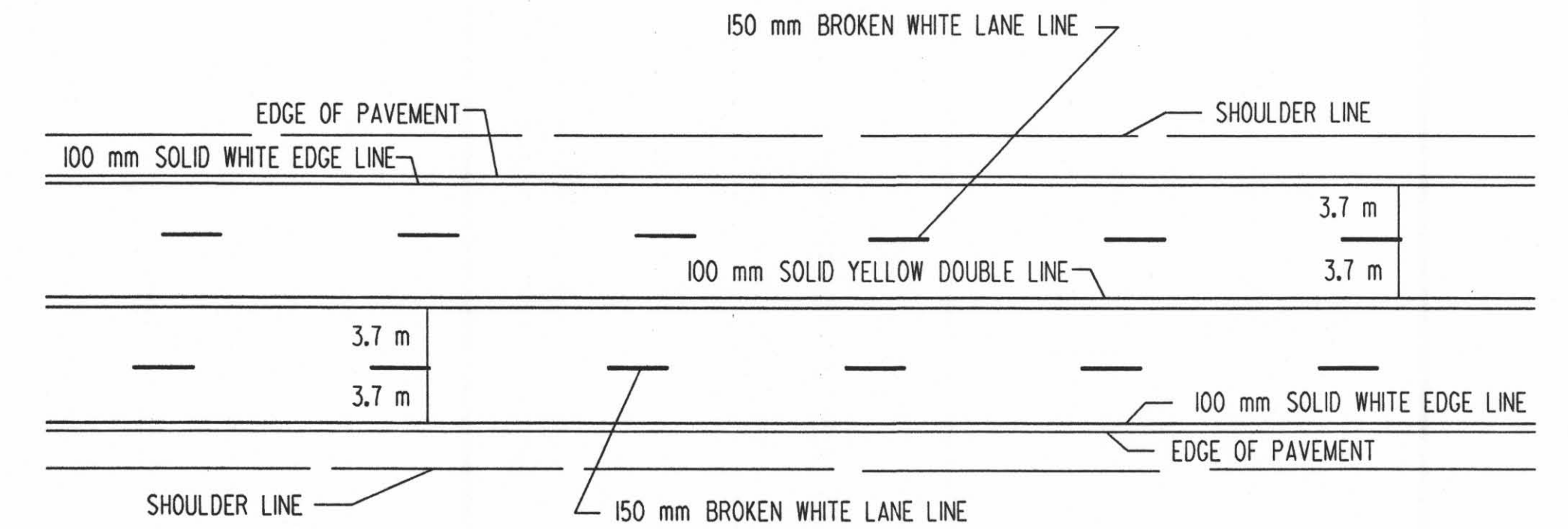


TYPICAL HIGHWAY JUNCTION MARKINGS WITH BYPASS LANES

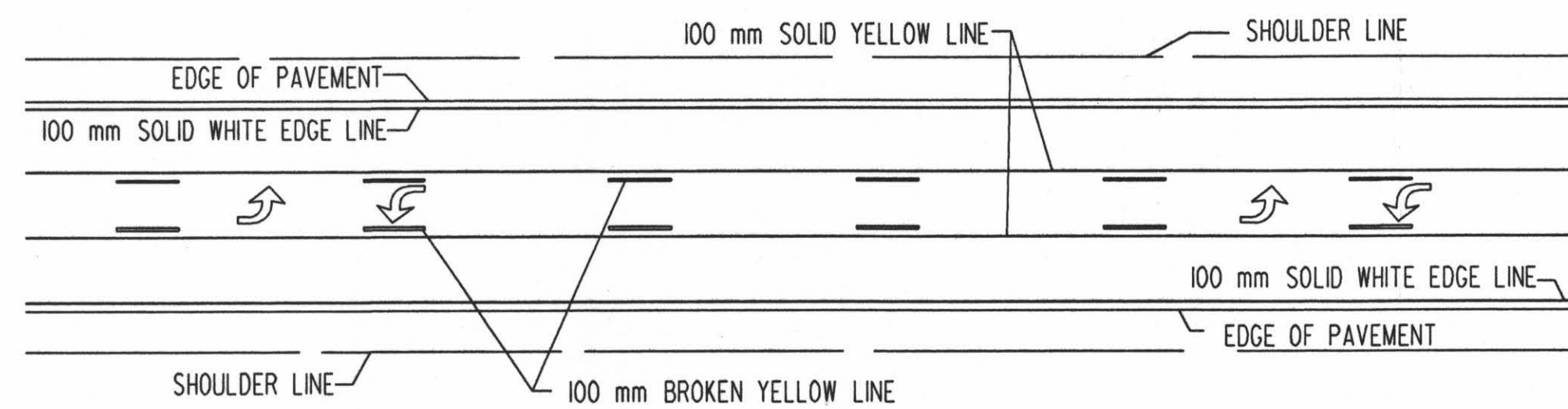
TYPICAL COUNTY ROAD JUNCTION MARKINGS WITH BYPASS LANES



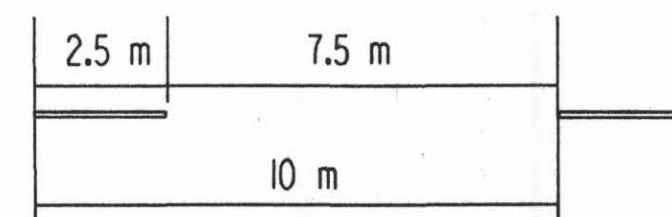
TWO-WAY LEFT TURN DETAIL FOR FIVE LANE ROADWAY



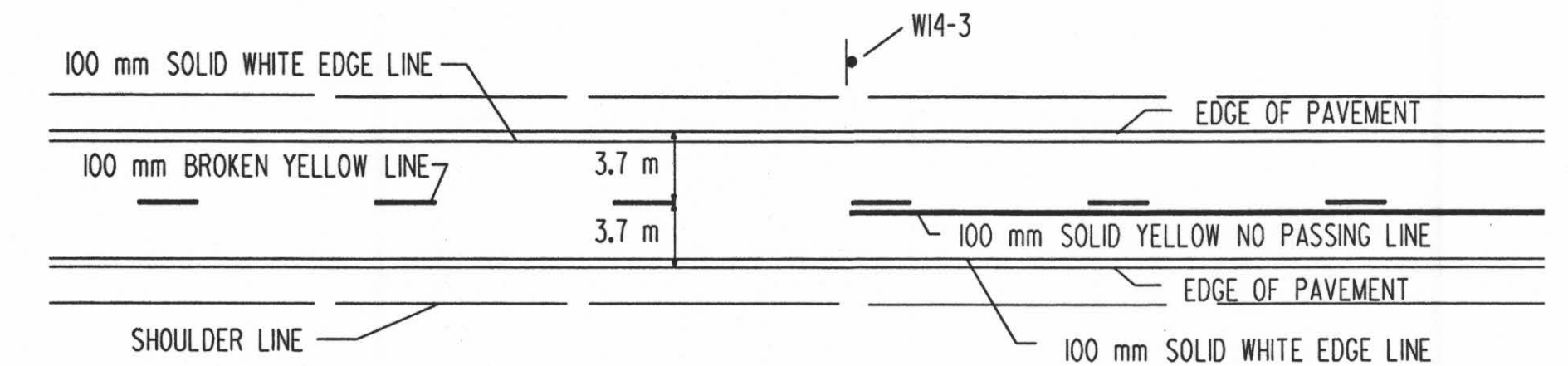
TYPICAL MARKINGS FOR FOUR LANE ROADWAY



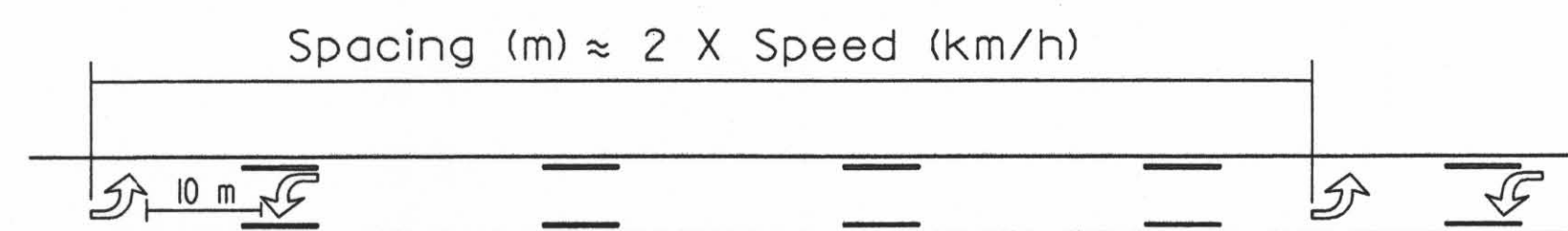
TWO-WAY LEFT TURN DETAIL FOR THREE LANE ROADWAY



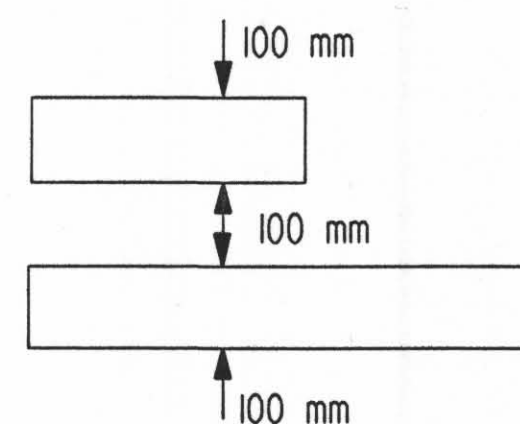
TYPICAL SPACING FOR BROKEN LINES UNLESS OTHERWISE NOTED ON PLANS



TYPICAL TWO LANE MARKINGS



TWO-WAY LEFT TURN ARROW SPACING DETAIL



TYPICAL SPACING FOR NO PASSING LINES UNLESS OTHERWISE NOTED ON PLANS

| SPEED CONVERSION CHART | |
|------------------------|------|
| MPH | Km/h |
| 20 | 30 |
| 25 | 40 |
| 30 | 50 |
| 35 | 60 |
| 40 | 60 |
| 45 | 70 |
| 50 | 80 |
| 55 | 90 |
| 60 | 100 |
| 65 | 110 |
| 70 | 110 |

NOTE: LONGITUDINAL PAVEMENT MARKING LINES SHALL BE OFFSET A MINIMUM OF 50mm FROM LONGITUDINAL PAVEMENT JOINTS.

| | | | | |
|-----|----------|---------------------------|-----|-------|
| 3 | 03/07/98 | Add County Road detail | JFF | LGV |
| 2 | 02/24/98 | REFORMATTED W/NEW DETAILS | JFF | LGV |
| 1 | 03/06/96 | BROKEN LINE DETAIL | JFF | JET |
| NO. | DATE | REVISION | BY | APP'D |

KANSAS DEPARTMENT OF TRANSPORTATION

TYPICAL PAVEMENT MARKING DETAILS FOR UNDIVIDED ROADWAYS

TE308SI 05/12/94

| | | | |
|---------------|----------|------------|------------------|
| FHWA APPROVAL | 05/18/98 | APP'D | JAMES E. TORABEN |
| DESIGNED | JFF | DETAILED | CSB |
| DESIGN CK. | JET | DETAIL CK. | JFF |
| | | QUANTITIES | TRACED |
| | | QUAN. CK. | TRACE CK. |

DSNR: OPER: SVB SCALE: 1/1997/97362/MARKING/DETAILS/TE308SI.DGN 11-19-2001/14:41:05 LAST REV: 1-25-2002 BY: svb