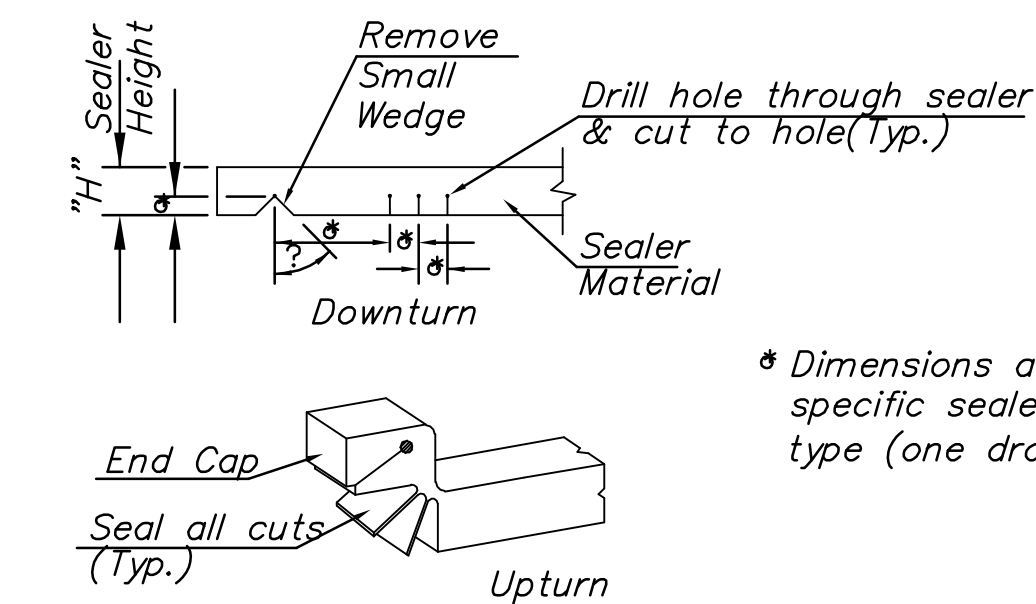


**UTILITY IMPROVEMENTS  
17TH STREET AT BSNF RAIL  
CORRIDOR**

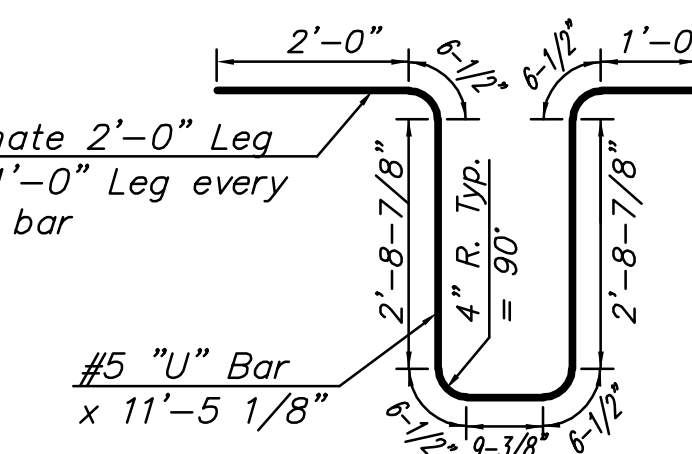
**GENERAL NOTES:**

- Surface of new pavement and crossing material shall be set to identical elevations at their joint of juncture only when the railroad company uses approved mechanical equipment to compact railroad fill and ballast such to preclude track settlement.
- Railroad track and crossing material elevations or pavement elevations shall be adjusted in a range of one-fourth (1/4) inch to one (1) inch to allow for track settlement when the railroad company uses hand methods for compaction of railroad fill and ballast or use of other compaction methods which may not preclude track settlement.
- The exact elevation differential between crossing material and pavement shall be determined by the railroad based on their experience for track settlement with concurrence by the engineer.
- The unit price bid for "Concrete Railroad Approach", measured on a Sq. Yd. basis, shall be full compensation for furnishing all reinforcing steel; for all concrete; for all excavation; and for all labor, tools, equipment, and incidentals necessary to complete the approach, including the lugs.
- The unit price bid for "Concrete Railroad Approach Footing", measured on a Cu. Yd. basis, shall be full compensation for furnishing all reinforcing steel; for all concrete; for all excavation; and for all labor, tools, equipment, and incidentals necessary to complete the footing.

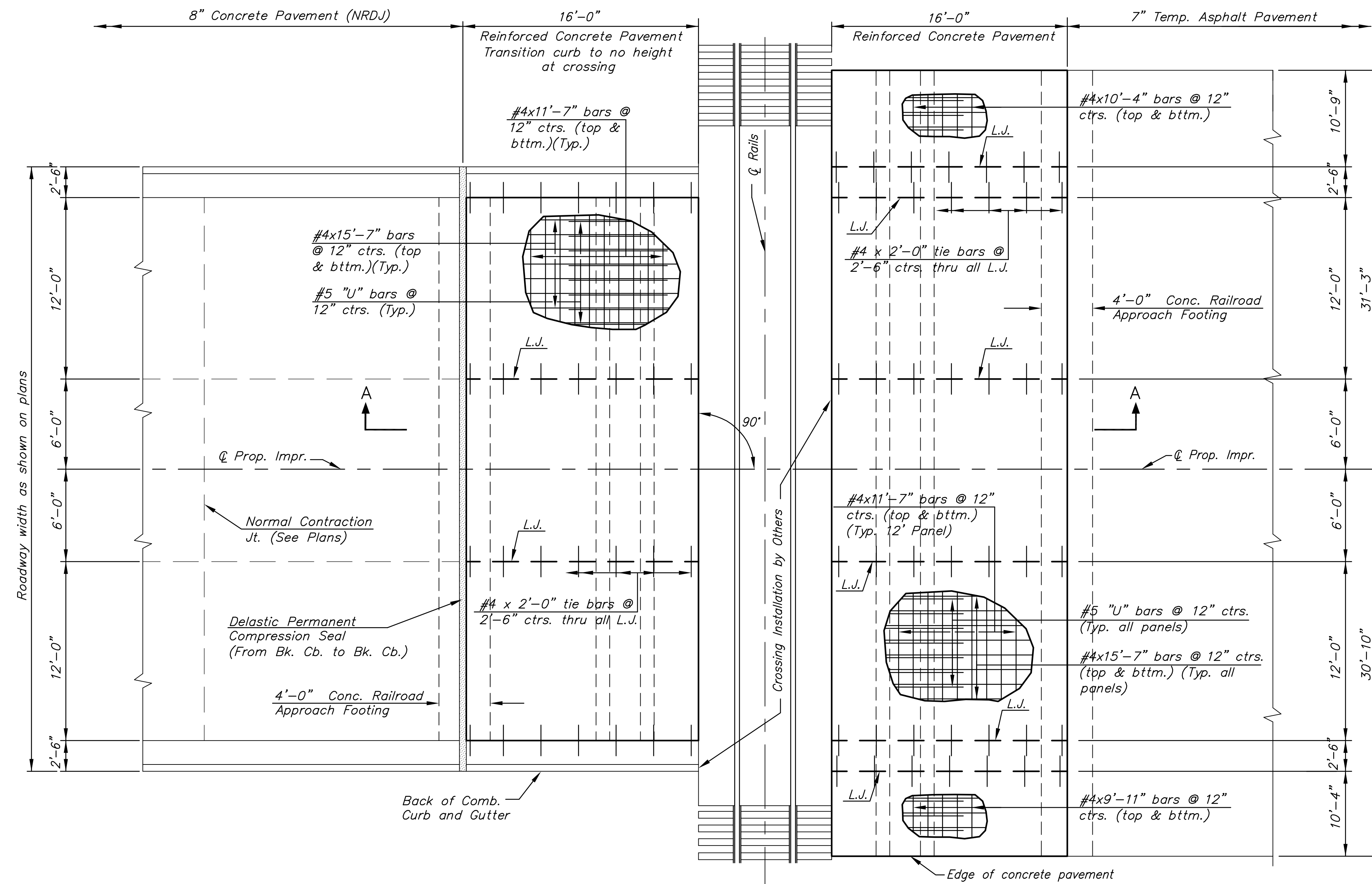


\* Dimensions as part of shop drawing for specific sealer material and project curb type (one drawing for each curb type).

**EXAMPLE CUTTING AND BENDING DETAILS**



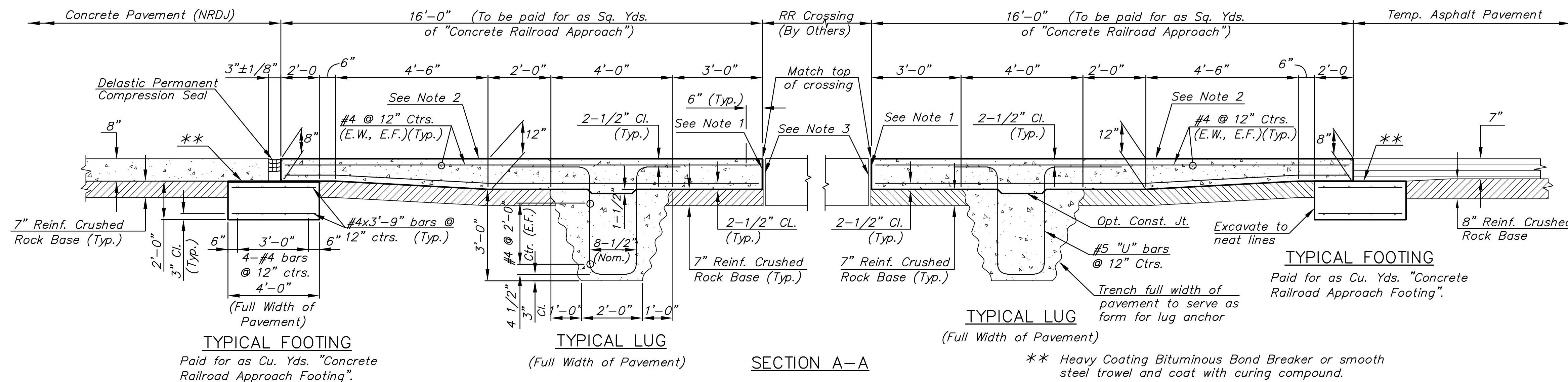
**BENDING DIAGRAM**  
(Dimensions to  $\varnothing$  Bar)



PLAN WITH CONCRETE PAVEMENT ADJACENT

PLAN WITH BITUMINOUS PAVEMENT ADJACENT

**APPROACH PAVING TO RAILROAD CROSSING**



**TYPICAL FOOTING**

Paid for as Cu. Yds. "Concrete Railroad Approach Footing".

**TYPICAL LUG**

(Full Width of Pavement)

**SECTION A-A**

**TYPICAL LUG**  
(Full Width of Pavement)

\*\* Heavy Coating Bituminous Bond Breaker or smooth steel trowel and coat with curing compound.

**NOTES:**

- NOTE 1: One thickness of 90# tar paper for bond breaker on all mating surfaces between pavement, sidewalks, or drives and railroad crossing material; to be installed by Paving Contractor.
- NOTE 2: Top of pavement true to X-slope and grade established by plan elevations and match of crossing installation (See General Notes).
- NOTE 3: Tie Shim Cap Board furnished by Crossing Material Supplier and installed by Railroad

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**UTILITY IMPROVEMENTS  
17TH STREET AT BSNF RAIL CORRIDOR**  
PROJECT NAME

**CONCRETE RAILROAD APPROACH DETAIL**  
SHEET TITLE

JRA DESIGN BY:	DPG DRAWN BY:	JRA CHECKED BY:
SEPTEMBER 2007 DATE	04177 JOB NO.	5 / 28 SHEET/OF