

P.I. STA: 4+78.75
 1. N=1672131.0788, E=1652304.4991
 2. Not Set

P.C. Sta. 4+94.32
 1. N=1672146.6498, E=1652304.2383
 2. Not Set

P.T. Sta. 5+30.52
 1. N=1672182.8394, E=1652304.6398
 2. Not Set

P.C. Sta. 7+69.31
 1. N=1672421.4461, E=1652313.9354
 2. Not Set

P.T. Sta. 8+05.50
 1. N=1672457.6360, E=1652314.3370
 2. Not Set

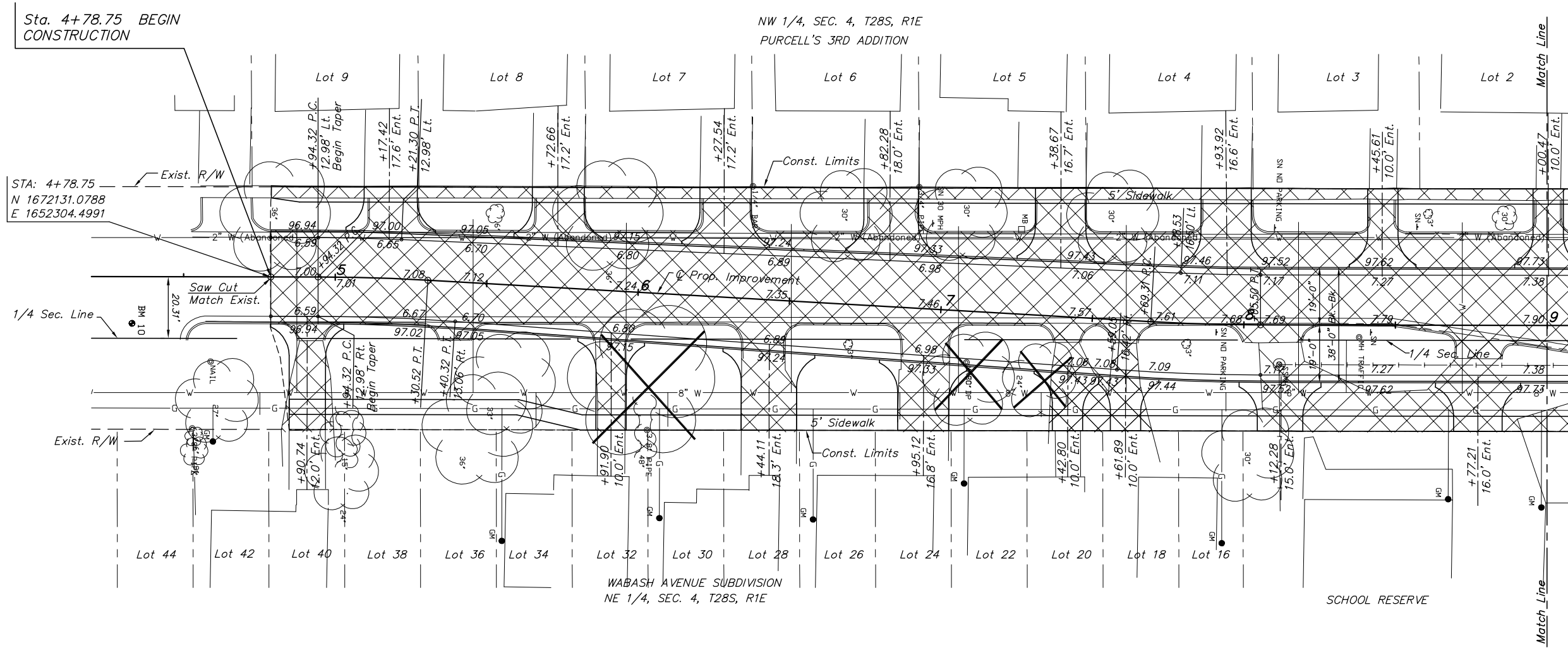
STATE	PROJECT NO.	YEAR	SHEET NO.	SHEETS
KANSAS	87 N-0235-01	2006	17	120

Curve Data Left
 Curb High Edge
 $\Delta = 2'22.39''$ Rt.
 T = 13.15'
 R = 633.50'
 L = 26.29'
 E = 0.14'
 N.C.

P.I. Sta. 7+96.99 (Bk.)=
 P.I. Sta. 7+96.98 (Ahd.)
 $\Delta = 3'11.26''$ Lt.
 T = 18.10'
 R = 650.00'
 L = 36.20'
 E = 0.25'
 N.C.

Curve Data Right
 Curb High Edge
 $\Delta = 4'04.20''$ Rt.
 T = 23.70'
 R = 666.50'
 L = 47.37'
 E = 0.42'
 N.C.

Scale: 1"=20'



Curve Data Left
 Curb High Edge
 $\Delta = 2'22.39''$ Rt.
 T = 13.76'
 R = 662.98'
 L = 27.51'
 E = 0.14'
 N.C.

C/L Curve Data
 P.I. Sta. 5+12.43 (Bk.)=
 P.I. Sta. 5+12.42 (Ahd.)
 $\Delta = 3'11.26''$ Rt.
 T = 18.10'
 R = 650.00'
 L = 36.20'
 E = 0.25'
 N.C.

Curve Data Right
 Curb High Edge
 $\Delta = 4'04.20''$ Rt.
 T = 22.65'
 R = 637.02'
 L = 45.27'
 E = 0.40'
 N.C.

BM #10 Square cut in top of curb S return in Drive
 Sta. 4+32.98, 15.39' Rt. Elev.=97.09

UTILITY ADJUSTMENTS			
DESCRIPTION	STATION/OFFSET	EXIST. TOP	PROP. TOP
Sanitary Sewer Manhole	8+11.66, 12.58' Rt.	97.60	97.27

- LEGEND
- House demolition backfill stabilization. See note, Sheet 2.
 - Pavement, driveway, parking lot and sidewalk removal

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KANSAS DEPARTMENT OF TRANSPORTATION

WASHINGTON STREET
 STA. 4+78.75 TO STA. 9+00

PROJECT NO. 87 N-0235-01 SEDGWICK CO.

M K E C ENGINEERING CONSULTANTS, INC.
 WICHITA, KANSAS

DESIGNED BY: JRA	CHECKED BY: JRA
DRAWN BY: WNJ	DATE: JULY 2004 SHEET 17 OF 120