

3RD STREET WATER MAIN REPLACEMENT

FROM THE CENTRAL RAIL CORRIDOR TO I-135

CITY OF WICHITA
SEDGWICK COUNTY, KANSAS

PROJECT NO. 448-90217
GARY L. JANZEN, P.E. - INTERIM CITY ENGINEER
WATER O.C.A. NO. 635808

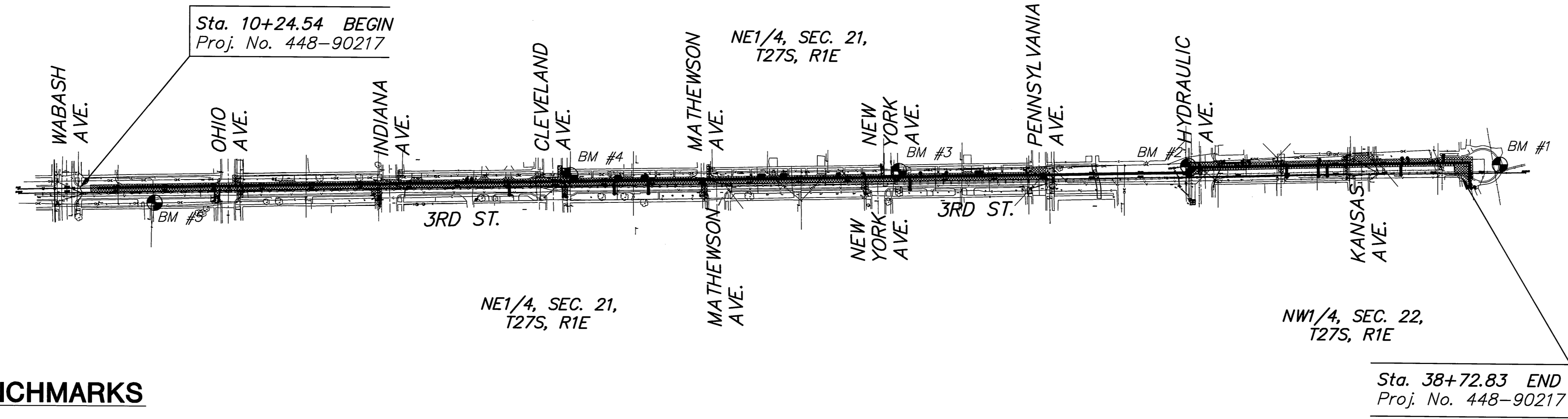
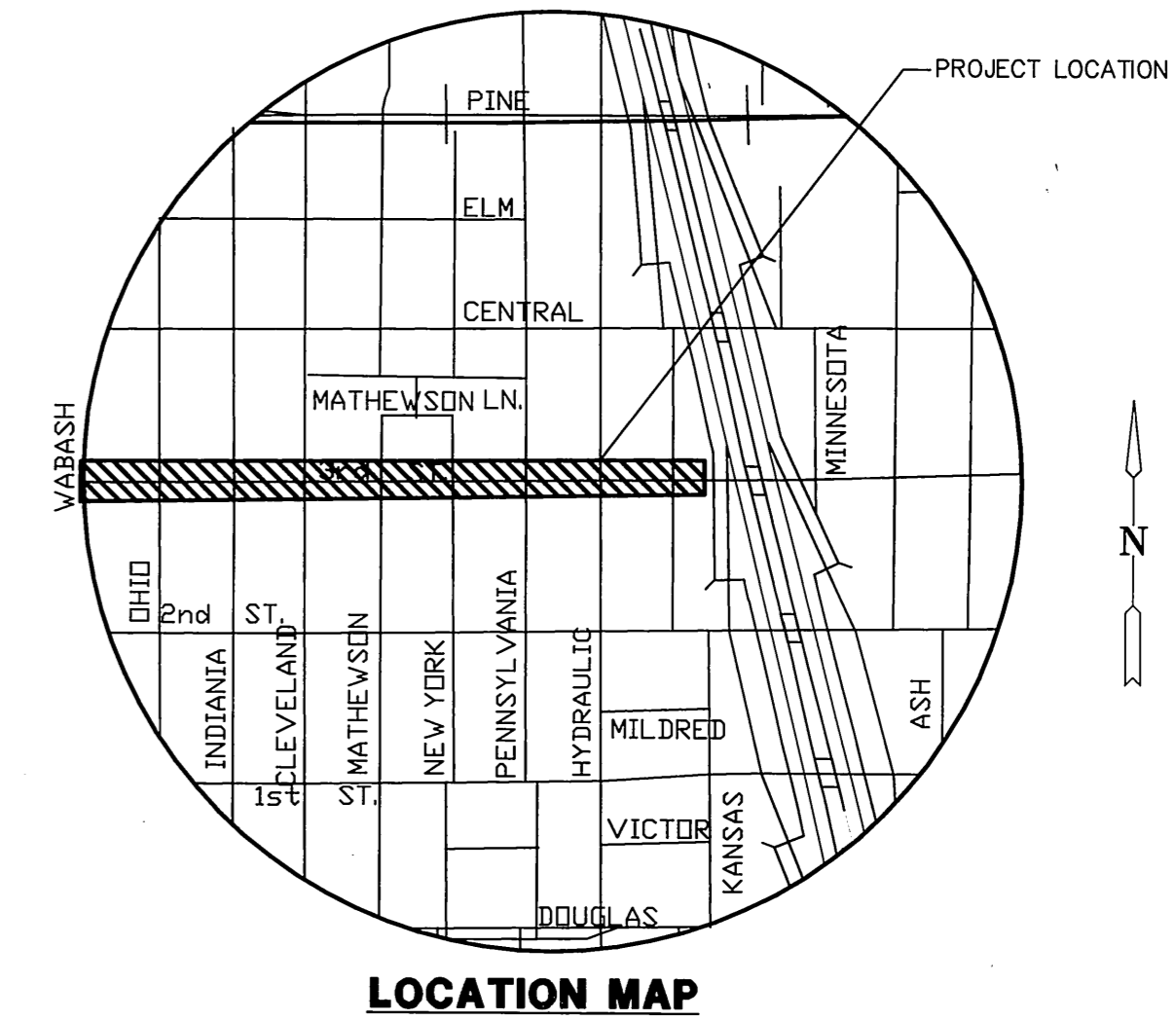
MKEC
ENGINEERING
CONSULTANTS, INC.

411 N. WEBB ROAD
WICHITA, K.S. 67206
316-684-9600



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BENCHMARKS

- BM#1 "□" T.C. E end of Cul-de-sac on 3rd St. @ Canal
Elev.=1299.31
- BM#2 "□" T.C. center inlet on W. side of Hydraulic N. of Q 3rd St.
Elev.=1297.53
- BM#3 RR spike S. fc of PP NE cor. of New York & 3rd St.
Elev.=1300.675
- BM#4 RR spike S. fc of PP NE cor. of Cleveland & 3rd St.
Elev.=1299.185
- BM#5 RR spike N. fc of PP S. side of 3rd St. E. of alley dr. & E. of Wabash
Elev.=1298.295

NOTE: WATER LINE VALVES TO BE OPERATED BY CONTRACTOR ONLY IF WATER INSPECTOR IS ON SITE.

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TITLE SHEET

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JRA / JSB / JRA
DESIGNED / DRAWN / CHECKED

ISSUED
June 2012

REVISED

SHEET NO.

1 of 37

GENERAL NOTES

1. ALL WATER MAINS AND APPURTENANCES SHALL BE INSTALLED IN ACCORDANCE WITH CITY OF WICHITA, KANSAS STANDARD SPECIFICATIONS FOR WATER MAIN INSTALLATIONS.
2. CONTRACTOR WILL BE REQUIRED TO PROVIDE A MINIMUM ADVANCE NOTICE OF SEVENTY-TWO (72) HOURS TO UTILITY COMPANIES PRIOR TO STARTING ANY EXCAVATION AS FOLLOWS:

KANSAS ONE-CALL	1-800-344-7233
OR LOCAL (WICHITA)	687-2470

THE CONTRACTOR MUST NOTIFY THE FOLLOWING IN CASE OF EMERGENCY:

COX COMMUNICATIONS (CABLE)	262-0661
WESTAR (ELECTRIC)	383-8600
KANSAS GAS SERVICE (GAS)	888-482-4950
AT&T (TELEPHONE)	800-870-8390
CITY OF WICHITA WATER & SEWER	262-6000
AQUILA (GAS)	800-303-0357
USD 259 (GARY WELCH)	973-2266
3. EXISTING UTILITIES AND THEIR LOCATIONS, AS SHOWN ON THE PLANS REPRESENT THE BEST INFORMATION OBTAINABLE FOR DESIGN. LOCATION INFORMATION HAS BEEN OBTAINED FROM THE VARIOUS UTILITY COMPANIES AND IS EITHER FROM COMPANY RECORD DRAWINGS OR COMPANY PROVIDED FIELD LOCATIONS. THE PLAN LOCATIONS SHOWN ARE NOT GUARANTEED. ADDITIONAL EXISTING UTILITIES MAY ALSO BE ENCOUNTERED.
4. OPENING AND CLOSING WATER VALVES SHALL BE DONE SLOWLY TO PREVENT DAMAGE TO THE WATER DISTRIBUTION SYSTEM FROM WATER HAMMER. ALL VALVES CLOSED BY THE CONTRACTOR MUST BE REOPENED AS NEW CONSTRUCTION PERMITS. PROJECT INSPECTOR MUST ASCERTAIN THAT ANY VALVE CLOSED BY THE CONTRACTOR IS REOPENED. CONTRACTOR WILL BE PERMITTED TO OPERATE WATER VALVES ONLY WHEN THE PROJECT INSPECTOR ASSIGNED TO THE PROJECT IS PRESENT.
5. CONTRACTOR SHALL NOT START WORK ON THE PROJECT UNTIL THE PROJECT INSPECTOR IS ASSIGNED TO THE PROJECT AND IS PRESENT ON THE SITE. ANY WORK DONE WITHOUT INSPECTION WILL BE REQUIRED TO BE UNCOVERED FOR INSPECTION.
6. THE CONTRACTOR IS REQUIRED TO CALL PATSY (316-219-8915) 24 HOURS IN ADVANCE TO PICKING UP NEW WATER METERS AT 1825 S. McLEAN BLVD. PAPERWORK IS DUE TO PATSY NEXT BUSINESS DAY AFTER METER REPLACEMENT.
7. THE CONTRACTOR SHALL GIVE ALL PROPERTY OWNERS AND/OR TENANTS OF DEVELOPED PROPERTY DIRECTLY ABUTTING CONSTRUCTION OF THIS PROJECT A MINIMUM OF TEN (10) DAYS NOTICE PRIOR TO START OF CONSTRUCTION.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR WILL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS WHICH ARE DAMAGED OR DESTROYED BY HIS CONSTRUCTION OPERATIONS. SUCH IRONS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR IN ACCORDANCE WITH STATE LAWS. ALL COSTS FOR THIS WORK SHALL BE SUBSIDIARY TO SITE RESTORATION.
9. RUBBLE FROM THE REMOVAL OF MISCELLANEOUS STRUCTURES INCLUDING ANY TREES REMOVED AND TREE TRIMMINGS SHALL BE DISPOSED OF ON SITES TO BE PROVIDED BY THE CONTRACTOR. THESE SITES SHALL BE APPROVED BY THE ENGINEER AS TO SUITABILITY, APPEARANCE AND SITE LOCATION. LOCATIONS THAT, IN THE OPINION OF THE ENGINEER, WILL LEAVE AN UNSIGHTLY APPEARANCE WILL NOT BE APPROVED. ALL DISPOSAL SITES MUST BE APPROVED BY THE KANSAS DEPARTMENT OF HEALTH AND ENVIRONMENT. MATERIAL EITHER STOCKPILED OR DISPOSED OF IN A FLOOD PLAIN WOULD REQUIRE A KANSAS STATE BOARD OF AGRICULTURE PERMIT. ANY MATERIAL DUMPED IN WATERS OF THE UNITED STATES OR WETLANDS IS SUBJECT TO U.S. CORPS OF ENGINEERS PERMITTING REGULATIONS. ANY MATERIAL BURIED OR STOCKPILED BEYOND APPROVED CONSTRUCTION LIMITS WOULD REQUIRE ADDITIONAL ARCHAEOLOGICAL INVESTIGATIONS UNLESS BURIED IN A PREVIOUSLY APPROVED BORROW LOCATION.
10. A SAW CUT OF AT LEAST ONE-HALF THE DEPTH OF THE EXISTING SURFACE COURSES OR ONE-FOURTH THE DEPTH OF THE EXISTING TOTAL PAVEMENT THICKNESS SHALL BE PROVIDED AT LOCATIONS WHERE PROPOSED CONSTRUCTION ABUTS AN EXISTING SURFACE OR PAVEMENT FOR WHICH PARTIAL REMOVAL OF THAT SURFACE OR PAVEMENT IS REQUIRED. SAW JOINT TO FACILITATE REMOVAL WITHIN THREE (3) FEET OF EXISTING JOINTS WILL NOT BE PERMITTED AND FOR SUCH INSTANCES THE LIMITS OF REMOVAL SHALL EXTEND TO THE EXISTING JOINT.
11. THE CONTRACTOR SHALL LAY A TRACER WIRE & SET TEST STATIONS ALONG ALL WATER LINE PIPE INSTALLED IN ACCORDANCE WITH CITY OF WICHITA SPECIFICATIONS & TRACER WIRE DETAIL ON DETAIL SHEET. COST IS SUBSIDIARY TO PIPE INSTALLATION.
12. THE CONTRACTOR IS RESPONSIBLE FOR KEEPING WATER SERVICE LINES INTACT AND OPERATIONAL DURING CONSTRUCTION. SPLICING OF EXISTING SERVICE LINES FOR RECONNECTION WILL NOT BE ALLOWED.
13. ALL BUSINESS SIGNS, POSTS AND LANDSCAPING FEATURES IN CONFLICT WITH THE NEW CONSTRUCTION WILL BE RELOCATED BY OTHERS PRIOR TO CONSTRUCTION. UPON THE START OF CONSTRUCTION, ALL ITEMS THAT REMAIN WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CITY. MAILBOXES IN CONFLICT SHALL REMAIN IN SERVICE AND BE TEMPORARILY RELOCATED DURING CONSTRUCTION AND PERMANENTLY RESET AFTER CONSTRUCTION BY THE CONTRACTOR.
14. PROPOSED CURBS MATCHING EXISTING CURBS SHALL BE ALTERED TO MATCH THE SHAPE AND DIMENSIONS OF THE EXISTING CURB. POSITIVE DRAINAGE SHALL BE MAINTAINED THROUGH SUCH TRANSITIONS.
15. BITUMINOUS SURFACE COURSE SHALL BE PLACED WITH A LAYDOWN MACHINE HAVING AUTOMATIC CONTROLS FOR LINE AND GRADE.
16. CRUSHED ROCK SUBGRADE IS TO BE COMPACTED AND SMOOTHED WITH A STEEL FACED ROLLER PRIOR TO PLACEMENT OF PAVEMENT. TACK COAT WILL NOT BE APPLIED TO ROCK BASE.
17. ALL AREAS DISTURBED BY CONSTRUCTION THAT ARE ADJACENT TO DEVELOPED PROPERTIES SHALL BE RESTORED WITH SOD TO MATCH EXISTING TURF TYPE. RESTORATION OF DISTURBED AREAS SHALL INCLUDE, BUT NOT BE LIMITED TO, TOP SOIL PREPARATION AND SODDING. ALL SODDING WORK SHALL BE IN ACCORDANCE WITH CITY STANDARD SPECIFICATIONS AND THE CITY ADMINISTRATIVE REGULATION NO. AR78, WHICH GOVERNS CLEANUP AND RESTORATION OR REPLACEMENT FOLLOWING CONSTRUCTION. SODDING AND ALL TOPSOIL PREPARATION SHALL BE SUBSIDIARY TO "SITE RESTORATION". WHEN THE WEATHER/SEASON PREVENTS THE INSTALLATION OF SOD, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING EROSION CONTROL BLANKET (CURLX I, OR APPROVED EQUAL) AT THE BACK OF CURB (8' WIDE MINIMUM). ALL COSTS FOR EROSION MAT INSTALLATION SHALL BE SUBSIDIARY TO "SITE RESTORATION".

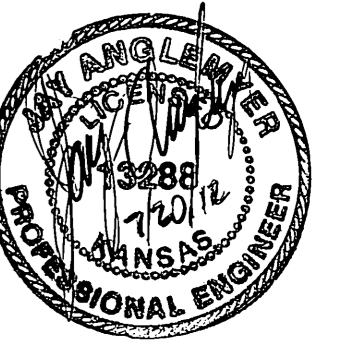
18. THERE WILL BE NO BLOCKING ON THE PROPOSED 24" MAIN WITH THIS PROJECT. THE 24" MAIN SHALL BE RESTRAINED JOINT PIPE, AND THE EXTENT OF RESTRAINING SHALL BE DETERMINED BY THE PIPE MANUFACTURER. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE PIPE MANUFACTURER FOR THEIR RECOMMENDATIONS.
19. AFTER NEW FIRE HYDRANTS HAVE BEEN PLACED IN SERVICE, THE OLD FIRE HYDRANT IS TO BE REMOVED, HYDRANT VALVE ABANDONED IN-PLACE. THE BRANCH CAPPED AND THE HYDRANT DELIVERED TO 1801 S. McLEAN. COST TO BE SUBSIDIARY TO "TRANSPORTATION OF SALVAGED MATERIAL".
20. PAVEMENT PATCH SECTIONS WILL CONFORM TO THE APPROPRIATE TYPICAL PAVEMENT SECTION ON SHEET 3. PAVEMENT PATCHES SHALL BE 2' WIDER THAN THE TOP OF WATER LINE TRENCH AND CENTERED ABOUT THE CENTER OF THE TRENCH. PATCHES WILL BE PAID FOR AS "PAVEMENT REMOVAL" AND THE APPROPRIATE PAVEMENT TYPE.
21. THE CONTRACTOR SHALL CONFIRM THE LOCATION AND ELEVATION OF PROPOSED VALVES TO BE INSTALLED IN OR NEAR PROPOSED WHEEL CHAIR RAMPS. LOCATIONS AND ELEVATIONS GIVEN IN THE PLANS ARE BASED ON VALVES AVOIDING RAMP WINGS.
22. THE "ABANDON EXISTING SYSTEM" BID ITEM WILL COVER ALL COSTS FOR TAKING BOTH THE EXISTING 16" AND 20" WATER MAINS, BETWEEN WABASH AND I-135, OUT OF SERVICE WITH THIS PROJECT. THIS INCLUDES PARTIAL REMOVAL OR CAPPING OF EXISTING LINES, VALVE REMOVAL, CONCRETE FILLING OF VALVE BOXES NO LONGER NEEDED, ETC. CALLED OUT IN THE PLANS FOR MAINS AND LINES CONNECTED TO MAINS. ALL REMAINING ABANDONED PIPE TO BE SAND FILLED.
23. THE "BRICK PAVERS REMOVED AND RESET" BID ITEM WILL INCLUDE ALL WORK TO REMOVE AND STOCKPILE THE EXISTING BRICK SURFACING. ADDITIONAL BRICK NECESSARY TO CONSTRUCT THE NEW SURFACING WILL BE PROVIDED BY THE CITY. COSTS TO TRANSPORT AND INSTALL WILL BE SUBSIDIARY TO THIS PAY ITEM.

RECAPITULATION OF QUANTITIES

LUMP SUM BID ITEMS	Quantity	Unit
Paving Items		
Pavement Removed	1438	sy
AC Pavement 2"	408	sy
Brick Pavers Removed and Reset	3470	sy
Crushed Rock Base 6", Reinforced	5171	sy
Concrete Base 6"	3859	sy
Concrete Pavement 8"	389	sy
Concrete C&G, Type I (6" & 1-1/2")	2300	lf
Wheelchair Ramp W/ Detectable Warnings	14	ea
Valve Box Adjusted	1	ea
Miscellaneous Items		
Transportation of Salvaged Material	1	LS
Mobilization	1	LS
Site Clearing	1	LS
Site Restoration	1	LS
Sodding Items		
Sodding	1	LS
Traffic Items		
Pavement Markings	1	LS
Traffic Control	1	LS
MEASURED QUANTITY BID ITEMS		
Paving Items		
Sidewalk, Drive &/or Pkg Lot Removed	285	sy
Concrete Driveway 8"	718	sf
Concrete Sidewalk 4"	946	sf
Temporary Project Water Pollution Control Items		
BMP, Curb Inlet Protection	32	ea
BMP, Silt Fence	500	lf
BMP, Back of Curb Protection	2230	lf
Traffic Items		
A.C. Pavement 6", Temporary	600	sy
Water Items		
Abandon Existing System	1	LS
WL Pipe 08"	176	lf
WL Pipe 12"	22	lf
WL Pipe 24"	2814	lf
DICL SJ Pipe 06"	28	lf
DICL SJ Pipe 08"	242	lf
DICL SJ Pipe 12"	20	lf
DICL SJ Pipe 24"	60	lf
Valve Assembly 06"	6	ea
Anchor Valve Assembly 08"	13	ea
Anchor Valve Assembly 12"	1	ea
Anchor Valve Assembly 20"	1	ea
Anchor Valve Assembly 24"	3	ea
Fire Hydrant Assembly	5	ea
Blowoff Assembly 2"	5	ea
Air Release Valve Assembly (2")	4	ea
Pipe Plug, 06"	3	ea
Pipe Plug, 08"	5	ea
Pipe Plug, 12"	1	ea
Short Service	30	ea
Long Service	8	ea
Conc. Encasement of Sanitary Sewer	60	lf

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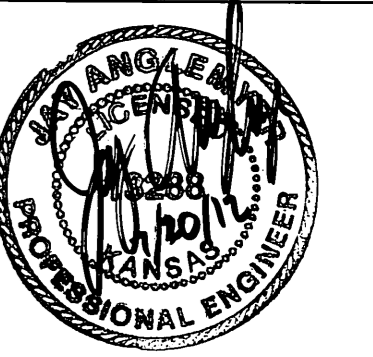
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TYP. SECTIONS & TRENCH DETAILS

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TRAFFIC CONTROL AND CONSTRUCTION SEQUENCING NOTES

3RD STREET MAY BE COMPLETELY CLOSED DURING CONSTRUCTION PROVIDED THAT THE CONTRACTOR ADHERES TO THE CONDITIONS OF THE NOTES ON THIS SHEET.

CONTRACTOR SHALL NOTIFY ADJACENT RESIDENCES AND BUSINESSES OF THE CONSTRUCTION ACTIVITIES AND EXPECTED DURATION OF STREET CLOSURE PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL NOT CLOSE MORE THAN TWO ADJACENT SIDE STREETS AT ANY TIME DURING CONSTRUCTION. A TEMPORARY PAVEMENT CONSISTING OF 5" COMMERCIAL GRADE ASPHALT MAY BE USED, AT THE ENGINEER'S DISCRETION, TO RE-OPEN SIDE STREETS AFTER THE WATER LINE HAS BEEN INSTALLED AND PRIOR TO PERMANENT PAVEMENT REPLACEMENT. THE COST OF PLACING AND REMOVING TEMPORARY PAVEMENT WILL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

HYDRAULIC AVENUE SHALL REMAIN OPEN TO TWO-WAY TRAFFIC AT ALL TIME DURING CONSTRUCTION. SEE TRAFFIC CONTROL DETAILS ON SHEET 36.

IF WORK ON PENNSYLVANIA IS NOT COMPLETED DURING SCHOOL BREAKS, IT SHALL BE KEPT OPEN TO ONE-LANE TRAFFIC AT ALL TIME DURING CONSTRUCTION. SEE TRAFFIC CONTROL DETAIL ON SHEET 36. THE CONTRACTOR WILL BE RESPONSIBLE FOR FLAGGING TRAFFIC DURING WORK ON PENNSYLVANIA.

WATER LINE CONSTRUCTION BETWEEN PENNSYLVANIA AND HYDRAULIC IS TO BE COORDINATED WITH USD 259 (JAY DORSETT, 973-2014). ALL WORK, INCLUDING RESTORATION, IN THIS AREA SHALL BE COMPLETE DURING THE SCHOOL SUMMER BREAK. CONTRACTOR WILL CONSTRUCT A TEMPORARY ENTRANCE OFF OF INDIANA AVENUE FOR UNITED RENTALS. THE TEMPORARY ENTRANCE WILL INCLUDE CURB REMOVAL AND EVENTUAL REPLACEMENT, 32 S.Y. OF 5" COMMERCIAL-GRADE ASPHALT DRIVE AND A TEMPORARY GATE AND FENCE RESTORATION. COST SHALL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

CONTRACTOR SHALL MAINTAIN DRIVE-THROUGH ACCESS AT JET BARBEQUE THROUGHOUT CONSTRUCTION. THE DRIVEWAY SHALL BE CONSTRUCTED HALF-AT-A-TIME. A TEMPORARY PAVEMENT CONSISTING OF 5" COMMERCIAL GRADE ASPHALT SHALL BE USED TO PROVIDE ACCESS FROM THE BUSINESS TO THE INTERSECTION OF 3RD AND WABASH. COST SHALL BE SUBSIDIARY TO "A.C. PAVEMENT 6", TEMPORARY.

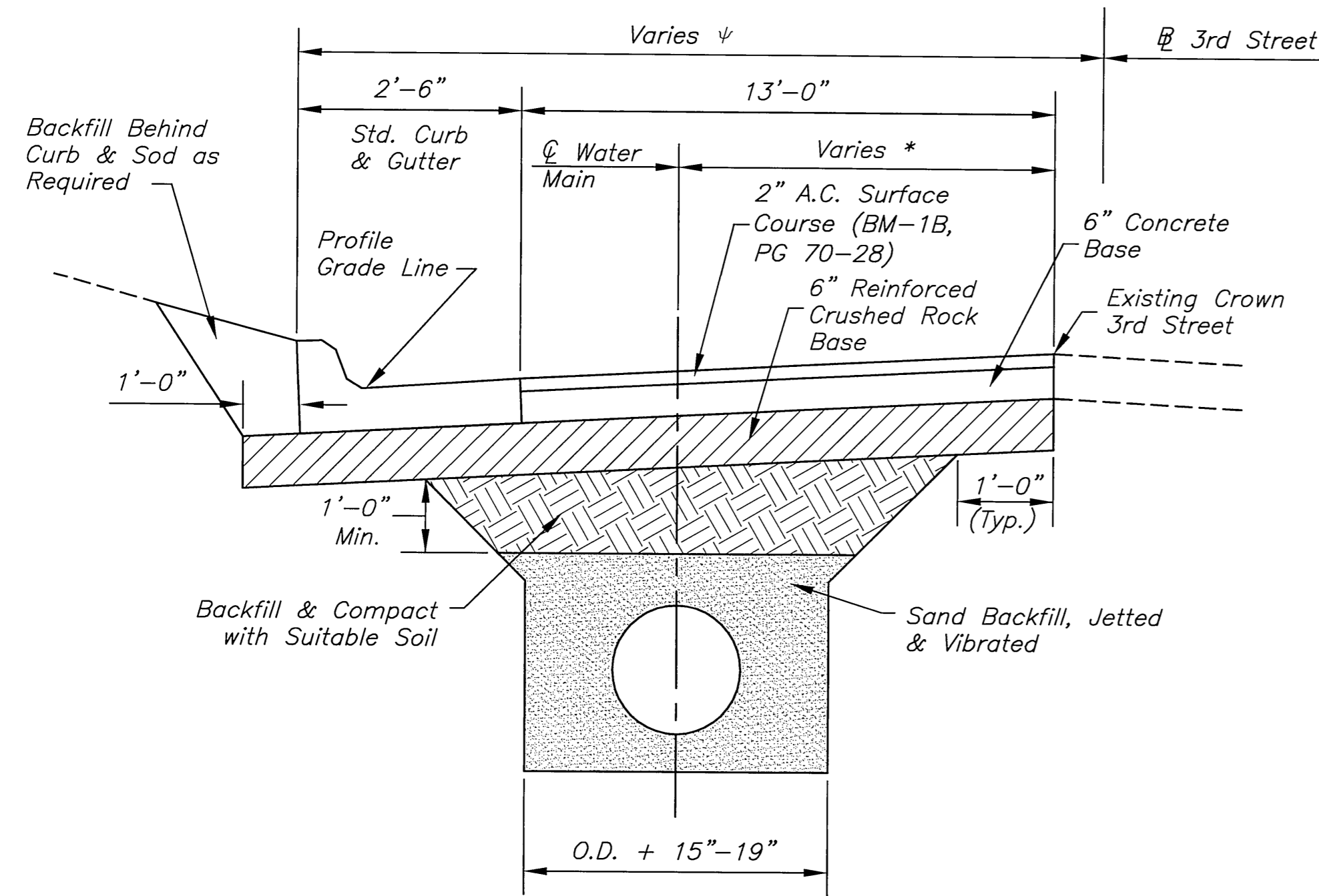
THE PAVEMENT MARKING ON HYDRAULIC SHALL BE REPLACED AFTER THE SURFACE COURSE IS CONSTRUCTED. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC, UNLESS OTHERWISE APPROVED BY THE ENGINEER. PAVEMENT MARKINGS SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND SHALL MEET KDOT'S 2007 EDITION OF THE STANDARD SPECIFICATION SECTION 806. FULL TRAFFIC MAY NOT BE RESTORED (AND SUBSTANTIAL PROJECT COMPLETION ACHIEVED) UNTIL ALL PAVEMENT MARKINGS ARE IN PLACE. SHOULD CONSTRUCTION TIMING BE SUCH THAT RESTORATION OF TRAFFIC BECOMES NECESSARY DURING TEMPERATURES PROHIBITING THE INSTALLATION OF THERMOPLASTIC MARKINGS, THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY MARKINGS UNTIL SUCH TIME THAT THERMOPLASTIC MARKINGS MAY BE PROPERLY INSTALLED. EXCEPT FOR THE MATERIAL REQUIREMENT, TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED EQUIVALENT, IN EVERY MANNER (I.E. DIMENSION, FREQUENCY, SPACING, ETC.), TO THE PERMANENT MARKING LAYOUT. THE COST FOR TEMPORARY PAVEMENT MARKINGS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED SUBSIDIARY TO THE BID ITEM FOR "PAVEMENT MARKINGS."

CONTRACTOR IS RESPONSIBLE FOR REMOVING AND REPLACING ALL OBJECTS CONFLICTING WITH CONSTRUCTION WHEN WORKING ON SCHOOL DISTRICT PROPERTY, INCLUDING FENCES, PLAYGROUND EQUIPMENT, SIDEWALKS, ETC. ANY ITEM DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO COST TO OWNER.

CONTRACTOR TO COORDINATE DRIVEWAY CLOSURES WITH RESIDENTS AND BUSINESSES.

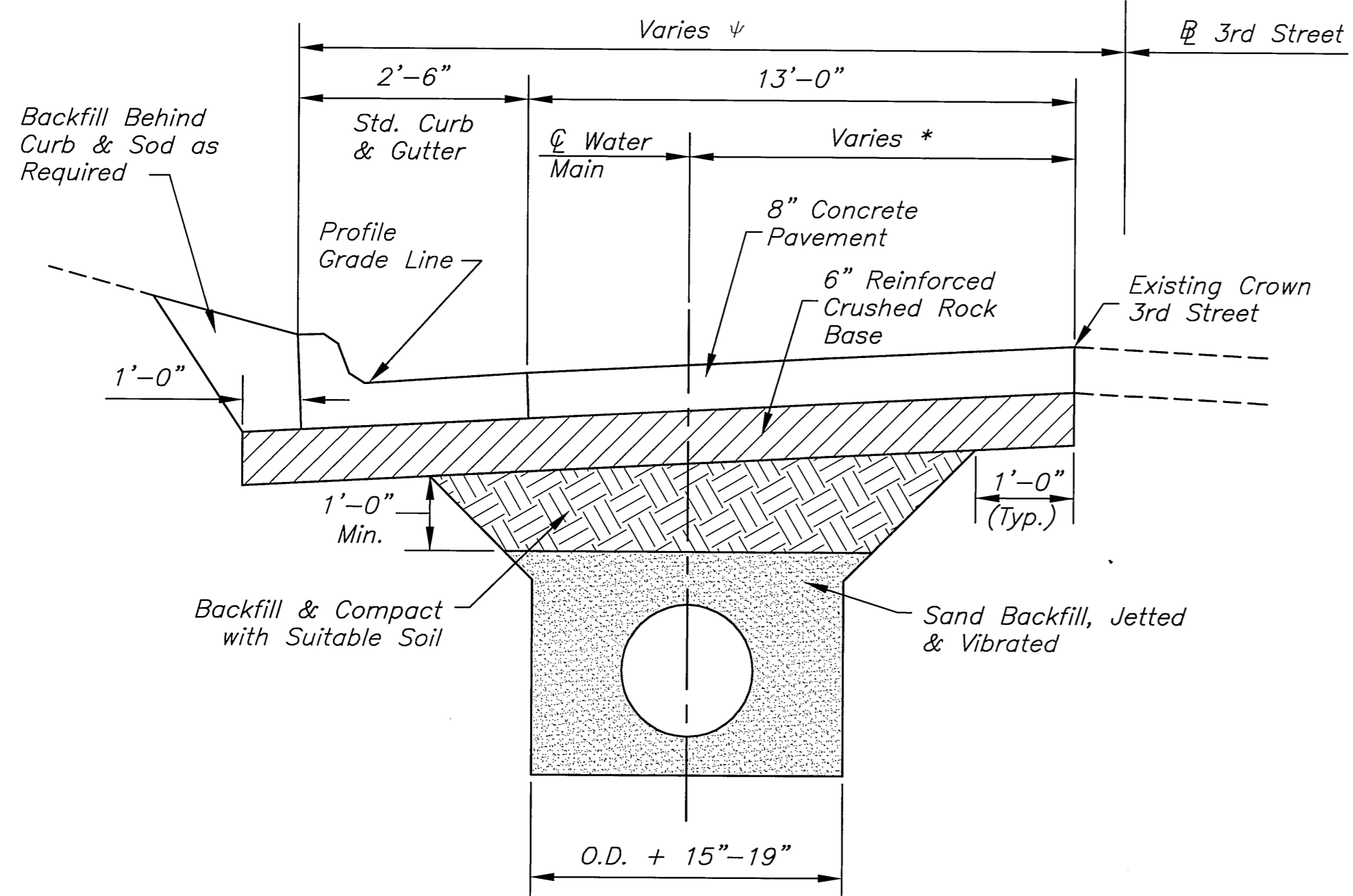
BRICK SURFACE COURSE NOTES:

1. Sand bedding shall meet the specification requirements for fine aggregate. An uncompacted sand laying course shall be spread evenly over the area to be paved and then screeded to a level of approximately 3/4" max. thickness. Once screeded and leveled to the desired elevation, the sand laying course shall not be disturbed in any way.
2. The paving brick shall be installed in a 45° Angled Soldier-Course pattern. Joint spaces shall be kept uniform approximately 1/8 inch thick. The gaps at the edge of the paved surface shall be filled with stones cut to fit. Cutting shall be accomplished to leave a clean edge toward the traffic surface, using a masonry saw. Whenever possible, no cuts should result with a paver less than one-third of its original dimension.
3. Paving brick shall be vibrated to their final level in the sand laying course by two or three passes of a vibrating compactor capable of 3000 to 5000 pounds compaction force with the surface clean and joints open.
4. After vibration, clean concrete sand shall be spread over the paving stone surface, allowed to dry, and vibrated into the joints with additional passes of the plate vibrator so as to completely fill the joints. A light coating of sand shall be swept over the completed surface and left to weather in.



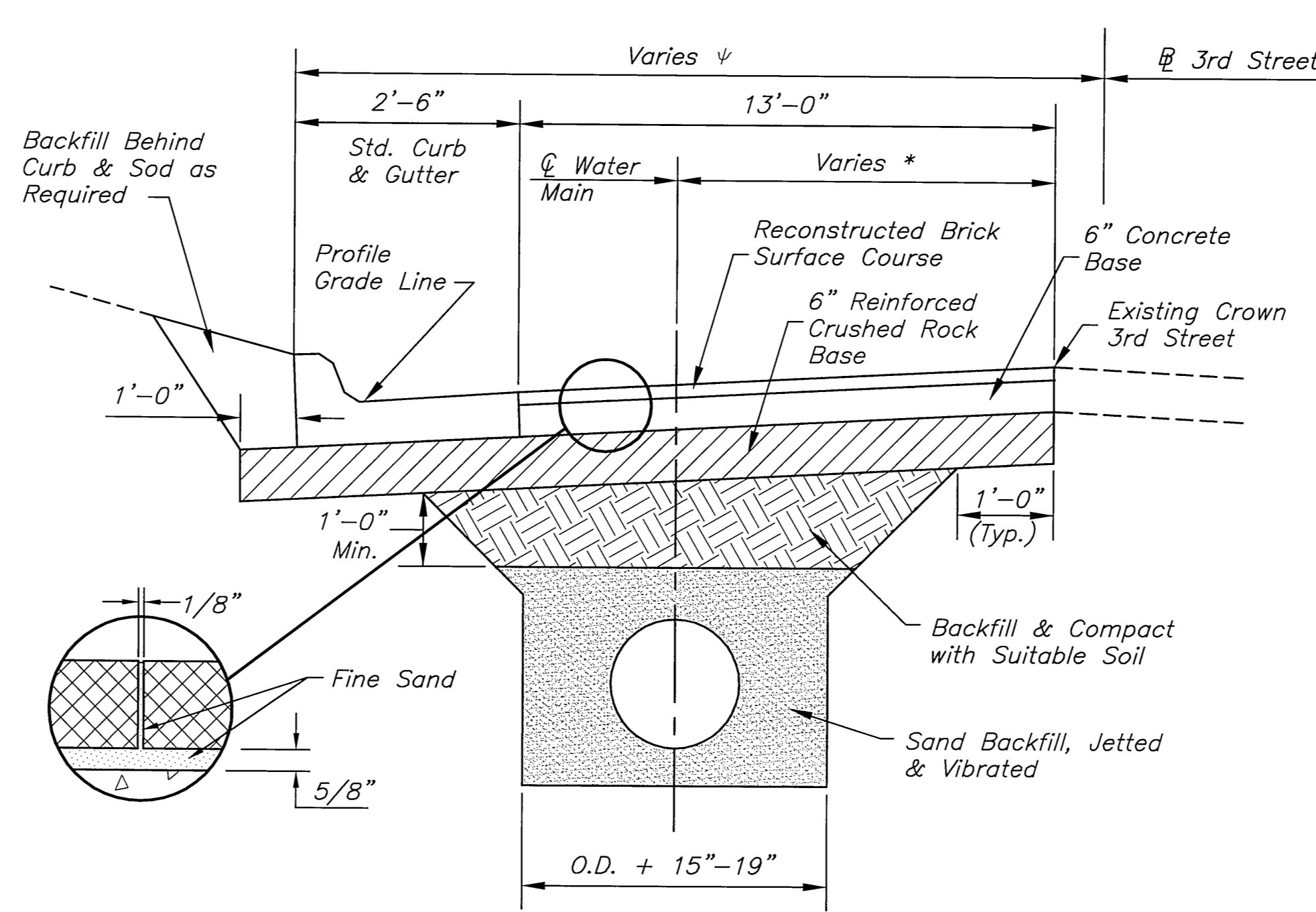
TYPICAL TRENCH AND STREET REPAIR SECTION

STA. 19+62.42 to STA. 20+31.68
STA. 33+01.69 to STA. 34+17.20



TYPICAL TRENCH AND STREET REPAIR SECTION

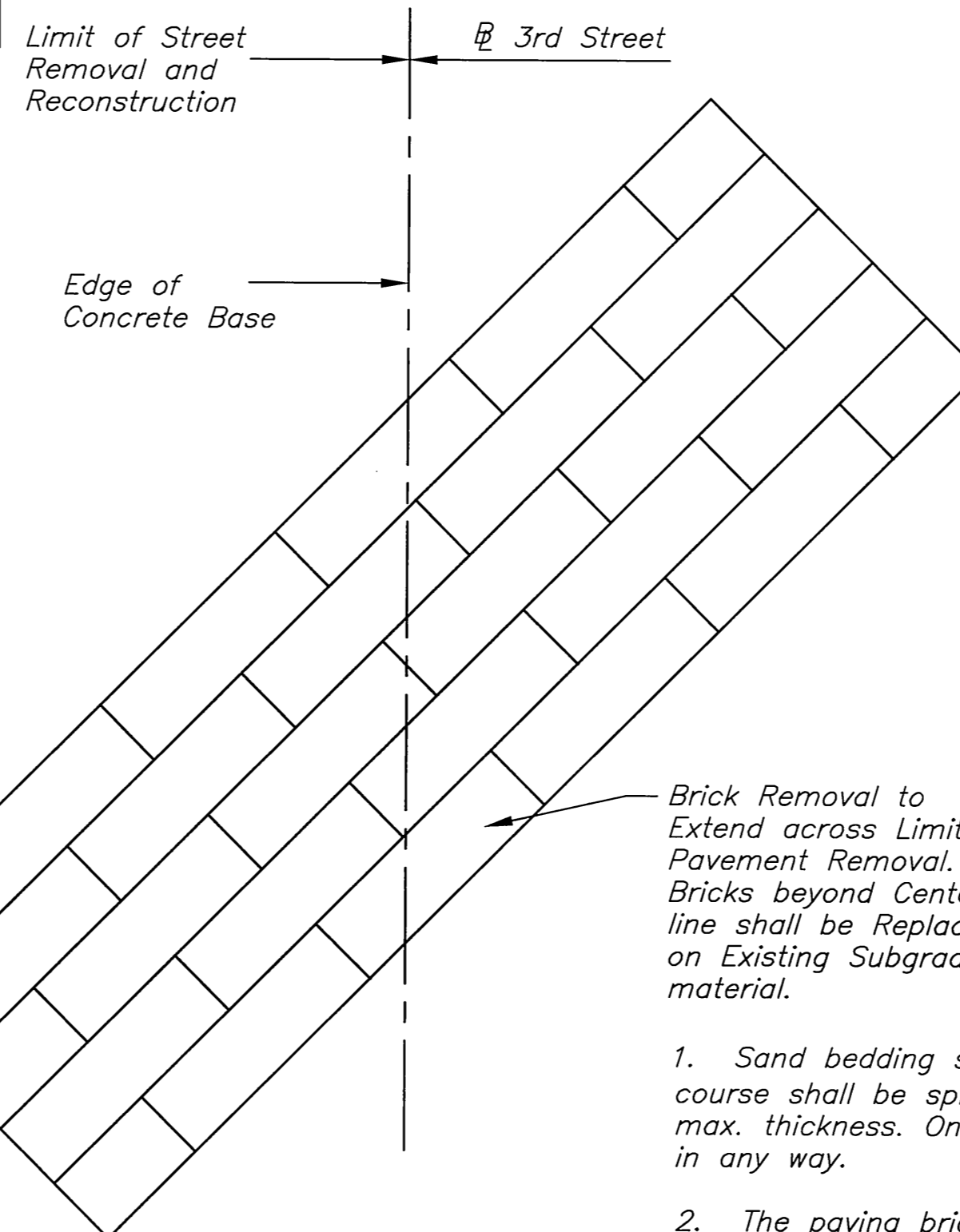
STA. 12+72.94 to STA. 13+71.02
STA. 38+43.43 to STA. 38+81.48



TYPICAL TRENCH AND STREET REPAIR SECTION

Sta. 10+48.74 to Sta. 12+72.94
Sta. 13+71.02 to Sta. 19+62.42
Sta. 20+31.68 to Sta. 30+06.01
Sta. 34+17.20 to Sta. 38+43.43

NOTE:
ANY EARTHWORK ASSOCIATED WITH STREET REPAIR OR REPLACEMENT SHALL BE CONSIDERED SUBSIDIARY TO PAVING ITEMS.



NOTE:
Concrete Base Course shall be constructed slightly rounded near the Crown to avoid an uneven resting surface for bricks crossing the center line.

9'-0" From Sta. 10+48.74 to Sta. 30+06.01
ψ 19'-11" From Sta. 33+01.69 to Sta. 38+81.48
* 9'-0" From Sta. 10+48.74 to Sta. 30+06.01
6'-0" From Sta. 33+01.69 to Sta. 38+81.48